



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

Agenda

City Council Regular Meeting

City Council Chambers | 50 Natoma Street, Folsom CA 95630

May 10, 2022

6:30 PM

Welcome to Your City Council Meeting

We welcome your interest and involvement in the city’s legislative process. This agenda includes information about topics coming before the City Council and the action recommended by city staff. You can read about each topic in the staff reports, which are available on the city website and in the Office of the City Clerk. The City Clerk is also available to answer any questions you have about City Council meeting procedures.

Participation

If you would like to provide comments to the City Council, please:




- Fill out a blue speaker request form, located at the back table.
- Submit the form to the City Clerk before the item begins.
- When it’s your turn, the City Clerk will call your name and invite you to the podium.
- Speakers have three minutes, unless the presiding officer (usually the mayor) changes that time.

Reasonable Accommodations

In compliance with the Americans with Disabilities Act, if you are a person with a disability and you need a disability-related modification or accommodation to participate in this meeting, please contact the City Clerk’s Office at (916) 461-6035, (916) 355-7328 (fax) or CityClerkDept@folsom.ca.us. Requests must be made as early as possible and at least two full business days before the start of the meeting.

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In Person	Online	On TV
		
City Council meetings take place at City Hall, 50 Natoma Street	Watch the livestream and replay past meetings on the city website, www.folsom.ca.us	Watch live and replays of meetings on Sac Metro Cable TV, Channel 14

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City Council Regular Meeting

Folsom City Council Chambers
50 Natoma Street, Folsom, CA
www.folsom.ca.us

Tuesday, May 10, 2022 6:30 PM

Kerri Howell, Mayor

Rosario Rodriguez, Vice Mayor
YK Chalamcherla, Councilmember

Sarah Aquino, Councilmember
Mike Kozlowski, Councilmember

REGULAR CITY COUNCIL AGENDA

Members of the public wishing to participate in this meeting via teleconference may participate either online or by telephone via WebEx.

Meeting Number: 2556 306 6030

Meeting Password: 05 10 2022

Join the meeting by WebEx online:

<https://cityoffolsom.my.webex.com/cityoffolsom.my/j.php?MTID=mb77cd538f3f221303a14ced4efa2b48f>

To make a public comment using the WebEx online platform, please use the “raise hand” feature at the bottom center of the screen. Please make sure to enable audio controls once access has been given by the City Clerk to speak. Please wait to be called upon by the City Clerk.

Join the meeting by WebEx telephone: Dial 1-415-655-0001

*To make a public comment by phone, please press *3 to raise your hand. Please make sure to enable audio controls by pressing *6 once access has been given by the City Clerk to speak. Please wait to be called upon by the City Clerk.*

Verbal comments via virtual meeting must adhere to the principles of the three-minute speaking time permitted for public comment at City Council meetings.

CALL TO ORDER

ROLL CALL:

Councilmembers: Aquino, Chalamcherla, Kozlowski, Rodriguez, Howell

The City Council has adopted a policy that no new item will begin after 10:30 p.m. Therefore, if you are here for an item that has not been heard by 10:30 p.m., you may leave, as the item will be continued to a future Council Meeting.

PLEDGE OF ALLEGIANCE

AGENDA UPDATE

BUSINESS FROM THE FLOOR:

Members of the public are entitled to address the City Council concerning any item within the Folsom City Council's subject matter jurisdiction. Public comments are limited to no more than three minutes. Except for certain specific exceptions, the City Council is prohibited from discussing or taking action on any item not appearing on the posted agenda.

SCHEDULED PRESENTATIONS:

- [1.](#) Proclamation of the Mayor of the City of Folsom Proclaiming May 15-21, 2022 as National Police Week in the City of Folsom
- [2.](#) Proclamation of the Mayor of the City of Folsom Proclaiming May 15 through May 21, 2022 as "National Public Works Week" and May 18, 2022 as "City Works Day" in the City of Folsom
- [3.](#) City Manager's Fiscal Year 2021-22 Third Quarter Financial Report
- [4.](#) Presentation of the City Manager's Fiscal Year 2022-23 Proposed Operating and Capital Budgets for the City of Folsom, the Successor Agency, the Folsom Public Financing Authority, and the Folsom Ranch Public Financing Authority

CONSENT CALENDAR:

Items appearing on the Consent Calendar are considered routine and may be approved by one motion. City Councilmembers may pull an item for discussion.

- [5.](#) Approval of April 12, 2022 Special and Regular Meeting Minutes
- [6.](#) Approval of April 26, 2022 Regular Meeting Minutes
- [7.](#) Resolution No. 10839 – A Resolution of the City Council Ratifying the City Manager's Authorization for Emergency Replacement of the On-Site Fuel Tanks at the City of Folsom Corporation Yard, Determining the Project is Exempt from CEQA, and Appropriation of Funds
- [8.](#) Resolution No. 10840 – A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 2 Village No. 3 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 2 Village No. 3 Subdivision
- [9.](#) Resolution No 10841 – A Resolution Ratifying the City Manager's Execution of a Purchase Agreement with Golden State Fire Apparatus Inc. for a Ladder Truck from Pierce Manufacturing Inc. Pursuant to Folsom Municipal Code Section 2.36.150, and the Approval of an Interfund Loan and Appropriation of Funds
- [10.](#) Resolution No. 10842 – A Resolution Authorizing the City Manager to Execute an Agreement with McGuire and Hester for Construction of the Water Treatment Plant Backwash and Recycled Water Upgrades Project and Appropriation of Funds
- [11.](#) Resolution No. 10843 – A Resolution Authorizing the City Manager to Execute an Agreement with Inferrera Construction Management Group, Inc. for Construction Management and

Inspection Services for the Water Treatment Plant Backwash and Recycled Water Upgrades Project

- [12.](#) Resolution No. 10844 – A Resolution Approving the Preliminary Engineer’s Report for the following Landscaping and Lighting Districts for Fiscal Year 2022-2023 and Setting Public Hearing for American River Canyon North, American River Canyon North No. 2, American River Canyon North No. 3, Blue Ravine Oaks, Blue Ravine Oaks No. 2, Briggs Ranch, Broadstone, Broadstone No. 4, Broadstone Unit No. 3, Cobble Ridge, Cobble Hills Ridge II/Reflections II, Folsom Heights, Folsom Heights No. 2, Hannaford Cross, Lake Natoma Shores, Los Cerros, Natoma Station, Natoma Valley, Prairie Oaks Ranch, Prairie Oaks Ranch No. 2, Prospect Ridge, Sierra Estates, Silverbrook, Steeplechase, The Residences at American River Canyon, The Residences at American River Canyon II, Willow Creek Estates East, Willow Creek Estates East No. 2, Willow Creek Estates South, and Willow Springs
- [13.](#) Resolution No. 10845 – A Resolution Authorizing the City Manager to Execute an Amendment to the Memorandum of Understanding Agreement (Contract No. 174-21 20-062) with the Capital Southeast Connector Joint Powers Authority for the Southeast Connector Segment D3(A) Bike Trail and Appropriation of Funds
- [14.](#) Resolution No. 10846 - A Resolution Authorizing the City Manager to Execute a Design and Consulting Services Contract with Bennett Engineering Services, Inc. for the East Bidwell Street/Iron Point Road and US50 Onramp Improvement Project and Appropriation of Funds
- [15.](#) Resolution No. 10847 - A Resolution Authorizing the City Manager to Execute a Contract Change Order for the Pedestrian/Bike Trail with Goodfellow Brothers, LLC (Contract No. 174-21 20-060) for the Capital SouthEast Connector Segment D3(A), Project No. PW1607, Federal Project No. 5288(046)
- [16.](#) Resolution No. 10848 – A Resolution Adopting a List of Projects for Fiscal Year 2022-23 to be Funded by Senate Bill 1: The Road Repair and Accountability Act
- [17.](#) Resolution No. 10850 - A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 1C South Village No. 4 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 1C South Village No. 4 Subdivision

PUBLIC HEARING:

- [18.](#) Appeal by Igor Semenyuk of a Decision by the Historic District Commission Denying a Conditional Use Permit for the Lakeside Memorial Lawn Crematorium project (PN 19-182) located at 1201 Forrest Street (Continued from April 26, 2022)
- [19.](#) Folsom Corporate Center Apartments – South Side of Iron Point Road, East of the Intersection of Iron Point Road and Oak Avenue Parkway (PN 21-120)
 - i. Resolution No. 10849 - A Resolution to Adopt a Mitigated Negative Declaration, Approve a General Plan Amendment, and Approve a Planned Development Permit for the Folsom Corporate Center Apartments Project
 - ii. Ordinance No. 1327 – An Uncodified Ordinance to Amend the Zoning Designation for a 7.24-acre Parcel (Lot 1) from M-L PD to R-4 PD and to Amend the Zoning Designation for a 4.68-acre Parcel (Lot 6) from BP PD to R-4 PD for the Folsom Corporate Center Apartments Project (Introduction and First Reading)

OLD BUSINESS:

- [20.](#) Resolution No. 10831 - A Resolution of the City Council of the City of Folsom Approving an Affordable Housing Loan in the Amount of \$588,265.55 to Bidwell Place, LP for Construction of the 75-unit Bidwell Place Affordable Multifamily Project, and Appropriation of Funds

NEW BUSINESS:

- [21.](#) Ordinance No. 1326 - An Uncodified Ordinance of the City of Folsom Approving the Folsom Police Department's Military Equipment Use Policy in Compliance with Assembly Bill 481 (Introduction and First Reading)
- [22.](#) Consideration of Letter in Response to Demand Letter Received from Scott Rafferty Regarding Alleged Non-Compliance with the Brown Act

CITY MANAGER REPORTS:

COUNCIL COMMENTS:

ADJOURNMENT

NOTICE: Members of the public are entitled to directly address the City Council concerning any item that is described in the notice of this meeting, before or during consideration of that item. If you wish to address Council on an issue, which is on this agenda, please complete a blue speaker request card, and deliver it to a staff member at the table on the left side of the Council Chambers prior to discussion of the item. When your name is called, stand to be recognized by the Mayor and then proceed to the podium. If you wish to address the City Council on any other item of interest to the public, when the Mayor asks if there is any "Business from the Floor," follow the same procedure described above. Please limit your comments to three minutes or less.

NOTICE REGARDING CHALLENGES TO DECISIONS: Pursuant to all applicable laws and regulations, including without limitation, California Government Code Section 65009 and or California Public Resources Code Section 21177, if you wish to challenge in court any of the above decisions (regarding planning, zoning and/or environmental decisions), you may be limited to raising only those issues you or someone else raised at the public hearing(s) described in this notice/agenda, or in written correspondence delivered to the City at, or prior to, the public hearing.

As presiding officer, the Mayor has the authority to preserve order at all City Council meetings, to remove or cause the removal of any person from any such meeting for disorderly conduct, or for making personal, impertinent, or slanderous remarks, using profanity, or becoming boisterous, threatening or personally abusive while addressing said Council, and to enforce the rules of the Council.

PERSONS INTERESTED IN PROPOSING AN ITEM FOR THE CITY COUNCIL AGENDA SHOULD CONTACT A MEMBER OF THE CITY COUNCIL.

The meeting of the Folsom City Council is being telecast on Metro Cable TV, Channel 14, the Government Affairs Channel, and will be shown in its entirety on the Friday and Saturday following the meeting, both at 9 a.m. The City does not control scheduling of this telecast and persons interested in watching the televised meeting should confirm this schedule with Metro Cable TV, Channel 14. The City of Folsom provides live and archived webcasts of regular City Council meetings. The webcasts can be found on the online services page of the City's website www.folsom.ca.us.

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Clerk's Office at (916) 461-6035, (916) 355-7328 (fax) or CityClerkDept@folsom.ca.us. Requests must be made as early as possible and at least two full business days before the start of the meeting.

Any documents produced by the City and distributed to the City Council regarding any item on this agenda will be made available at the City Clerk's Counter at City Hall located at 50 Natoma Street, Folsom, California and at the Folsom Public Library located at 411 Stafford Street, Folsom, California during normal business hours.

PROCLAMATION
OF THE MAYOR OF THE CITY OF FOLSOM
PROCLAIMING MAY 15 – 21, 2022
as
NATIONAL POLICE OFFICERS MEMORIAL WEEK
in the City of Folsom

WHEREAS, in 1962, President Kennedy proclaimed May 15 as National Peace Officers Memorial Day National Police Week pays special recognition to those law enforcement officers who have lost their lives in the line of duty for the safety and protection of others; and

WHEREAS, there are more than 800,000 law enforcement officers serving in communities across the United States, including 79 sworn members of the City of Folsom Police Department who work devotedly and selflessly on behalf of the citizens of the City of Folsom, regardless of the peril or hazard to themselves; and

WHEREAS, since the first known line of duty death in 1786, more than 23,000 law enforcement officers in the United States have made the ultimate sacrifice; and

WHEREAS, the names of these dedicated public servants are engraved on the walls of the National Law Enforcement Officers Memorial, which was dedicated in 1991 in Washington, D.C.; and

WHEREAS, in 2021 458 federal, state, tribal and local law enforcement officers died in the line-of-duty. This was an increase of 55% from the 295 officers killed in 2020, and is the highest total line-of-duty officer deaths since 1930 when there were 312 fatalities; and

WHEREAS, the service and sacrifice of all officers killed in the line of duty will be honored, and the names of 619 officers newly engraved on the Memorial will be formally dedicated, during the National Law Enforcement Officers Memorial Fund's 34th Annual Candlelight Vigil, to be held on the evening of Friday, May 13, 2022; and

WHEREAS, May 15, 2022 is designated as Peace Officers Memorial Day, in honor of all the fallen officers and their families.

NOW, THEREFORE, I, Kerri M. Howell, Mayor of the City of Folsom, on behalf of the Folsom City Council, do hereby proclaim the week of May 15- 21, 2022 as **National Police Week** in the City of Folsom and publicly salute the service of law enforcement officers in our community and communities across the nation.

PROCLAIMED this 10th day of May 2022.

Attest:

 Kerri M. Howell, MAYOR

 Lydia Konopka, DEPUTY CITY CLERK

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PROCLAMATION

OF THE MAYOR OF THE CITY OF FOLSOM PROCLAIMING May 15 through May 21, 2022

as

“National Public Works Week” and May 18, 2022 as “City Works Day” in the City of Folsom

WHEREAS, public works services provided in the Folsom community are an integral part of our residents’ everyday lives; and

WHEREAS, informed residents are vital to the efficient operation of public works systems and programs such as traffic engineering, street maintenance, storm drainage, solid waste collection, recycling, and hazardous materials; and

WHEREAS, the health, safety, and comfort of this community greatly depends on these operational services; and

WHEREAS, the quality of these programs combined with their planning, design, and construction, are vitally dependent on the skills of City of Folsom staff and officials; and

WHEREAS, the Folsom City Charter and Folsom Municipal Code encourages broad, informed public participation in civic affairs; and

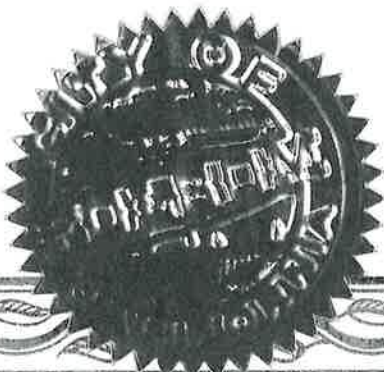
WHEREAS, the City of Folsom seeks and encourages advice and input from the public on matters related to the City; and

WHEREAS, the City of Folsom desires to provide information to the public about municipal operations; and

WHEREAS, City Works Day is designed to inspire residents to discover the City of Folsom’s wide variety of services and programs, and community members are invited to attend the event:

NOW, THEREFORE, I, Kerri M. Howell, Mayor of the City of Folsom, on behalf of the Folsom City Council, do hereby proclaim the week of May 15 through May 21, 2022 as “National Public Works Week” and May 18, 2022 as “City Works Day” in the City of Folsom, and call upon community members to acquaint themselves with the complexities of providing public works and to recognize the contribution that Public Works and other City departments make daily to the health, safety and comfort of our residents.

PROCLAIMED this 10th day of May 2022.



Kerri M. Howell, MAYOR

Attest:

Lydia Konopka, DEPUTY CITY CLERK

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Folsom City Council Staff Report



MEETING DATE:	5/10/2022
AGENDA SECTION:	Scheduled Presentations
SUBJECT:	City Manager’s Fiscal Year 2021-22 Third Quarter Financial Report
FROM:	Finance Department

RECOMMENDATION / CITY COUNCIL ACTION

It is recommended that the City Council receive and file the City Manager’s Fiscal Year 2021-22 Third Quarter Financial Report.

BACKGROUND / ISSUE

Section 5.05R of the Charter of the City of Folsom requires the City Manager submit to the City Council a financial and management report showing the relationship between budgeted and actual revenues, and expenditures and encumbrances on a quarterly basis.

The Financial Report for the third quarter of Fiscal Year (FY) 2021-22 is an analysis of the unaudited financial status of the City’s major funds, covering the nine-month period from July 2021 through March 2022. Please refer to the Appendices of the report for detailed schedules of the City’s key funds for the period ended March 31, 2022.

POLICY / RULE

Section 5.05R of the Charter of the City of Folsom requires the City Manager submit to the City Council a financial and management report showing the relationship between budgeted and actual revenues, and expenditures and encumbrances on a quarterly basis.

Section 3.02.050 (b) of the Folsom Municipal Code states “... within 30 days after the end of each quarter during the fiscal year, and more often if required by the City Council, the City Manager shall submit to the City Council a financial and management report.”

ANALYSIS

Economic Update:

As of March 2022, unemployment was 4.2% and 4.0% in California and Sacramento County respectively and in Folsom, the unemployment rate was 2.7%. As a comparison, in March 2021, the unemployment rates were 8.2% in California, 7.3% in Sacramento County and 4.5% in Folsom. A comparison of home sales during the third quarter of FY 2021-22 and FY 2020-21 shows the number of homes sold decreased by 141 or 18.26%. The average median sales price through the third quarter of FY 2021-22 was \$762,500 which is an increase of 23.11% over FY 2020-21. The number of home sales continue to decline, most likely due to increasing interest rates.

General Fund:

Fiscal Year 2021-22 is currently projected to end the year with a General Fund unassigned fund balance of \$22.98 million. As a comparison, the unassigned fund balance in FY 2020-21 was \$19.20 million. The unassigned fund balance as a percentage of expenditures is projected at 23.38% for FY 2021-22 and for FY 2020-21, it was 21.38%. It is important to note that the 23.38% for FY 2021-22 is inflated when it is compared to the artificially low level of expenditures related to cuts due to COVID-19.

Fiscal Year 2021-22 total projected revenues of \$102.77 million is an increase of \$7.18 million, or 7.51% over the prior fiscal year, and a projected increase of \$7.83 million, or 8.25% over FY 2021-22 budgeted revenues of \$94.94 million. The increase is seen mostly in charges for services and sales tax and is due to continued recovery from the COVID-19 related impacts. The budgeted revenues were projected prior to the reopening of services and venues due to the pandemic and the reopening occurred sooner than anticipated.

Property tax received through the third quarter of the fiscal year was \$16.25 million and compared to the prior fiscal year is an increase of \$594,000 or 3.79%. The current projection is for property tax to end the fiscal year with a slight increase of \$500,000 over the budgeted amount of \$32.49 million, for \$33.01 million. Compared to the prior fiscal year this would be an increase of \$1.76 million or 5.62%.

Sales tax is projected to end the fiscal year at \$27.01 million, an increase of \$2.46 million over the budgeted amount of \$24.55 million or 10.00%. This is due to a better than expected recovery from the impacts of COVID-19. However, when compared to the results for FY 2020-21 sales tax revenues of \$25.85 million, this is only a 4.49% increase year over year.

Through the third quarter, charges for services were \$11.84 million and the current projection for the end of FY 2021-22 is \$16.05 million. The projection of \$16.05 million is an increase from the \$9.84 million budgeted and an increase from the \$13.87 million received in the prior year. The increase over the budgeted amount is mostly related to better than expected Parks and Recreation revenues, up \$2.92 million over the budgeted amount, due to increased program

activity after the re-opening of programs that were discontinued or limited due to COVID-19. Also, development charges are projected to be up \$2.29 million over the budgeted amount due to a faster pace of development in the Folsom Plan Area than projected and charges in Police and Fire are up \$749,000. Charges in the Public Safety Departments include estimated reimbursements from the California Office of Emergency Services (CalOES) for wildfire strike teams. The year over year comparison of charges for services show the Parks and Recreation fees are projected to increase by \$2.06 million from the \$1.93 million received in FY 2020-21. Development charges are projected to increase by \$887,000 when compared to the prior year and Public Safety charges are projected to decrease by approximately \$300,000 mainly due to reduced reimbursements from CalOES.

Fiscal Year 2021-22 total projected General Fund expenditures are \$98.29 million, \$3.35 million (3.53%) more than the appropriated amount of \$94.94 million. The projected expenditure total includes increases in overtime of \$2.00 million in public safety, contracts of \$1.76 million, an increase in services and supplies of \$938,500 and an increase in capital outlay of \$648,600. The increase in overtime in public safety is partially offset by reimbursements from CalOES. The projected increase in all contracts is primarily in the Community Development Department (\$1.96 million) and is mostly due to contract services related to the processing of permits and inspections of new development mostly in the Folsom Plan Area. As a comparison, in FY 2020-21 820 building permits were issued in the Plan Area. Through the third quarter of FY 2021-22, 733 building permits have already been issued. Increases in capital outlay include ambulances, and vehicles for the Police Department and Parks and Recreation. The increase in capital outlay is due to additional vehicles purchased for the Police Department with funding from the American Rescue Plan Act. Services and supplies are continuing to increase as costs increase for most consumables like fuel and parts.

Utility Enterprise Funds:

All three Utility Operating Funds are projected to end the fiscal year with operating revenues exceeding operating expenses. After capital expenses are included and comparing to Fiscal Year 2020-21 the unrestricted net assets are projected to decrease in Water by approximately \$7.06 million, decrease in Solid Waste by approximately \$1.2 million and in Wastewater the change in unrestricted net assets is projected to be flat. In the case of Water and Wastewater, the capital outlay is for capital projects and in Solid Waste, it is the purchase of replacement vehicles.

A year over year comparison of the fiscal year-end projection of expenses and revenues in the combined operating and capital funds shows charges for service revenues in Water are currently projected to decrease by \$329,000 (1.80%) and operating expenses are projected to increase by \$3.11 million (24.68%). The increase in operating expenses is mostly seen in maintenance and contract costs. Wastewater Operating charges for services revenues are projected to increase by \$150,000 (1.77%) and operating expenses are projected to increase by \$328,000 (6.47%). The increase in expenses in Wastewater is seen mostly in contract services. Solid Waste Operating charges for services revenues are projected to increase by \$3.80 million (25.02%) and operating expenses are projected to increase by \$1.52 million (10.92%).

Increases in expenses are mainly due to employee and contract costs and the increase in the projected revenues is due to the recent increase in service rates. The increase in expenses and revenues in Solid Waste are both directly attributable to the regulations regarding food waste and the change in recyclables.

Submitted,



Elaine Andersen
City Manager



Stacey Tamagni
Finance Director/CFO



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

City of Folsom Quarterly Financial Report

Fiscal Year 2021-22 Third Quarter

May 10, 2022

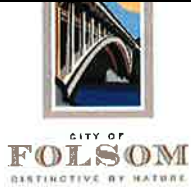
**Prepared by the Office of Management and Budget
Financial Analysis and Reporting Division**

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Third Quarter Financial Report

Fiscal Year 2021-22



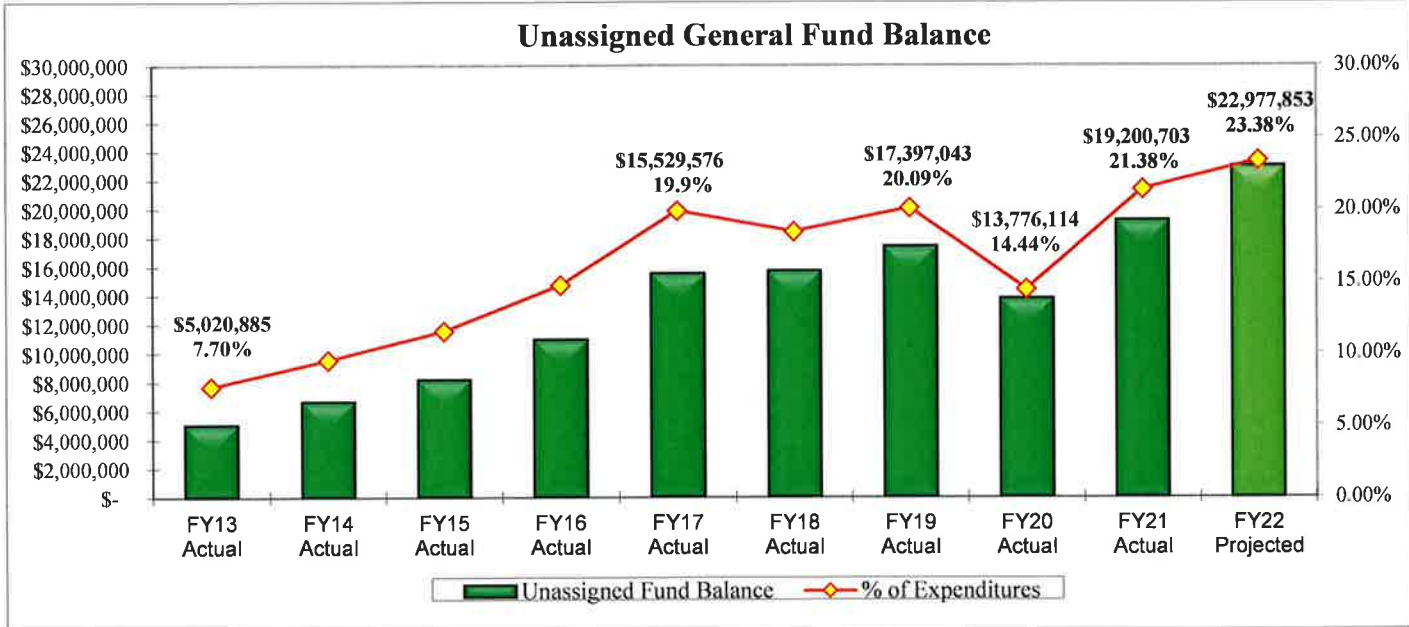
Introduction

This financial report provides an overview of the City’s unaudited financial position through the third quarter of fiscal year (FY) 2021-22 (July 1, 2021, through March 31, 2022) for (1) the General Fund, (2) Housing Special Revenue Fund and L&L Districts, (3) the major enterprise operating funds, and (4) the Risk Management Internal Service Fund. Notable cumulative third quarter to third quarter and budget to actual comparisons are included in this report in addition to year-end projections.

Executive Summary

During first three quarters of FY 2021-22, while still under the COVID-19 public health emergency, most restrictions have been lifted and the economy has started to rebound. Additional influences are causing economic changes. The rise in interest rates and the continued supply chain issues have started to cause costs to increase. The Parks and Recreation Department has continued to rebound with revenues exceeding the budget amount as programming increases to provide services to the community. Parks and Recreation programming revenues are now projected to end FY 2021-22 with an increase of \$2.92 million when compared to the budget and \$2.06 million when compared to the prior fiscal year. The City has received \$4.19 million in American Rescue Plan Act (ARPA) funding of which approximately \$2.2 million has been approved to be used for public safety, facilities and health and welfare, within the category of Provision of Government Services. The current projected year-end General Fund revenues are \$102.77 million and projected expenditures are \$98.29 million.

We project the General Fund’s unassigned fund balance will increase from \$19.20 million to \$22.98 million at the fiscal year end. Below is a chart of the unassigned fund balance over the last ten years and displays the projected change from FY 2020-21 to FY 2021-22.



General Fund: Operating Revenues

The following table includes cumulative revenue comparisons through the third quarter of FY 2020-21 and FY 2021-22 and a revenue budget comparison for FY 2021-22 with year-end projections.

	FY 20-21 Actual Mar. 31, 2021	FY 21-22 Actual Mar. 31, 2022	FY 21-22 Budget	FY 21-22 Projected	Over/Under Budget	% of Budget
Property Tax	\$ 15,656,038	\$ 16,249,998	\$ 32,491,949	\$ 33,010,662	\$ 518,713	102%
Sales Tax	14,763,739	15,921,650	24,551,790	27,008,127	2,456,337	110%
Transient Occupancy Tax	718,276	1,368,351	2,000,000	1,900,000	(100,000)	95%
Charges for Services	8,520,013	11,844,469	9,839,240	16,048,997	6,209,757	163%
License, Permits & VLF	7,411,697	7,752,394	11,916,184	12,959,504	1,043,320	109%
Transfers In	4,083,086	4,214,980	8,393,178	6,008,753	(2,384,425)	72%
All Other	1,886,940	2,024,958	3,550,200	3,635,327	85,127	102%
Subtotal Revenue	\$ 53,039,790	\$ 59,376,801	\$ 92,742,541	\$ 100,571,370	\$ 7,828,829	108.44%
CARES Act / ARPA	1,007,649	2,195,500	2,195,500	2,195,500	-	
Total Revenue	\$ 54,047,439	\$ 61,572,301	\$ 94,938,041	\$ 102,766,870	\$ 7,828,829	108.25%

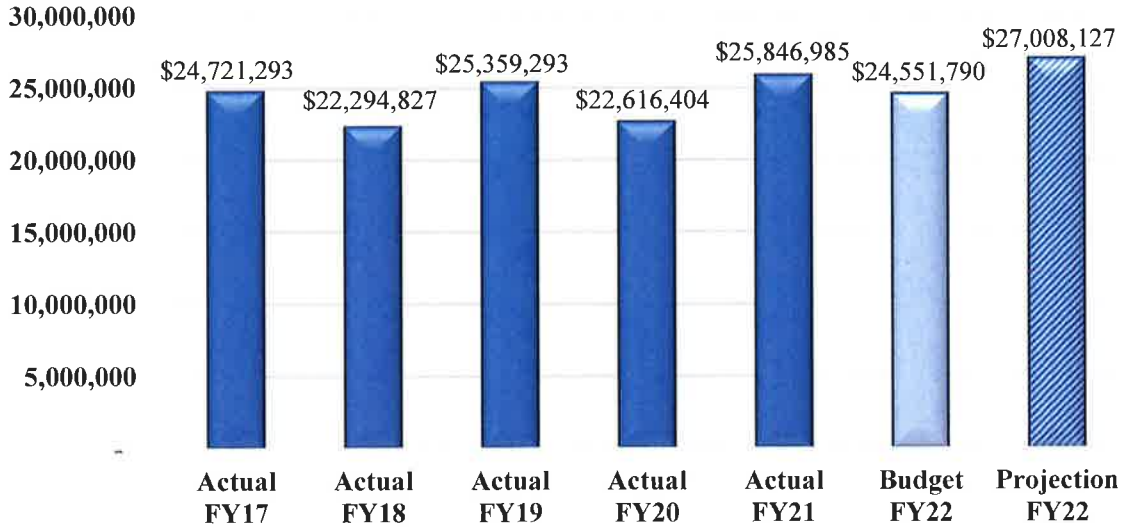
General Fund operating revenues through the third quarter are \$61.57 million, and 13.92% above the same period in FY 2020-21. Revenues are at 64.86% of the budget through the third quarter of the current year, primarily due to the timing associated with receiving some of the larger revenue sources. For instance, property tax is the largest General Fund revenue source, but funding is received in two installments, of which one has been received. The second installment will be received during the fourth quarter of the fiscal year.

The following is an explanation of the notable variances:

- Property tax revenues exceeded last year's cumulative third quarter by 3.79% or \$594,000. The number of home sales from July through March was 631 and a decrease from the prior year of 141 sales (18.26%). The median sales price has increased by approximately 21.72%. The property tax revenue projection for Fiscal Year 2021-22 year-end is \$519,000 greater than the budgeted amount of \$32.49 million, which would exceed the prior year by \$1.76 million or 5.62%.
- Sales tax revenues also exceeded last year's cumulative third quarter by 7.84% or \$1.16 million. During the time frame of July through December 2021, the categories of construction, business to business and the countywide pool show a decrease, all other categories show an increase when compared to the same quarter in the prior year. The categories of apparel stores, restaurants and department stores show increases due to relaxing of pandemic restrictions as consumers return to more normal spending patterns. The threat of a recession now looms in the future due to rising interest rates and higher costs.

Based on the latest sales tax forecast, sales tax is trending to end the year slightly above the budget at \$27.01 million, an increase from the prior year of \$1.16 million or 4.49%. Below is a graph showing sales tax revenue for the current fiscal year and the past five fiscal years. This illustrates the trend of sales tax revenues leveling off.

Sales Tax



- Transient Occupancy Tax (TOT) collections are at \$1.37 million through the third quarter and are projected to end the fiscal year at \$1.90 million, an increase of \$542,500, 39.96% when compared to the FY 2021 amount. The increase is attributed to reduced travel restrictions and the desire to travel.
- Charges for services, including Building and Engineering fees, Parks and Recreation user fees, and Ambulance fees are at \$11.84 million through the third quarter and are projected to end the fiscal year at \$16.05 million. The current projection is \$2.17 million more than the prior year amount of \$13.87 million. The Parks and Recreation charges through the third quarter were \$2.86 million and a comparison to the same quarter in the prior fiscal year shows an increase of \$2.05 million. Ambulance fees through the third quarter were \$2.82 million and compared to the prior fiscal year this is a slight increase of \$9,500 or 0.34%. The projection for Ambulance Fees at fiscal year-end is \$4.40 million, which will be an increase from the prior year by \$111,000 or 2.58%. Community Development charges are at \$4.29 million through the third quarter and are currently projected to end the fiscal year at \$5.18 million. Compared to the prior fiscal year this would be an increase of \$887,000 or 20.65%. The projected increase in Community Development charges is due to an increase in engineering activity, mostly in the plan area, through the third quarter.
- License and permit fees and Vehicle License Fees (VLF) quarter to quarter increased \$448,000 and are projected to end the fiscal year at \$12.33 million which would be an increase of \$818,000 compared to the budget and an increase of \$322,000 when compared to the prior year. The year over year increase is due to increased building permit activity mostly in the Folsom Plan Area, and a corresponding increase in VLF received. To provide a perspective of the building activity currently seen in the Folsom Plan Area, through the third quarter of FY 2021-22 there have been 733 building permits issued, compared to the prior fiscal year when there were 498 building permits issued through the third quarter. Also, business certificates are projected to decrease due to an approved business certificate suspension for small business in the current fiscal year.
- Other revenues decreased 33.19% or \$290,000, through the third quarter when compared to the prior fiscal year.

General Fund: Department Operating Expenditures

The following table includes cumulative third quarter actual expenditure comparisons for FY 2020-21 and FY 2021-22 and an expenditure budget-to-actual comparison for FY 2021-22.

	FY 20-21 Actual Mar. 31, 2021	FY 21-22 Actual Mar. 31, 2022	FY 21-22 Budget	FY 21-22 Projected	Over/Under Budget	Budget
Salaries	\$ 29,361,782	\$ 30,793,804	\$ 40,241,757	\$ 41,235,757	\$ 994,000	102.5%
Benefits	19,995,493	19,672,941	27,569,671	26,472,971	(1,096,700)	96.0%
O&M	13,578,298	17,378,492	23,263,237	26,069,836	2,806,599	112.1%
Capital Outlay	1,559,730	1,195,832	3,080,227	3,728,859	648,632	121.1%
Debt Service	901,381	496,305	783,149	783,149	-	100.0%
Total Expenditures	\$ 65,396,684	\$ 69,537,373	\$ 94,938,041	\$ 98,290,572	\$ 3,352,531	103.5%

Overall, cumulative third quarter General Fund expenditures increased 6.33% compared to the prior year. This is in part due to the increased appropriation from the ARPA funding and an increase in temporary salaries in Parks and Recreation due to programs resuming. General Fund expenditures are coming in at 73.25% percent of the budget through the third quarter of FY 2021-22. The projection for the end of the fiscal year is for expenditures to be at \$98.29 million which would be \$3.35 million more than the budgeted amount or 103.53% of budget. The projected increase to expenditures is primarily due to an increase in services and supplies, contracts and capital outlay.

The table below shows a comparison for FY 2020-21 and FY 2021-22 for each General Fund Department.

	FY 20-21 Actual Mar. 31, 2021	FY 21-22 Actual Mar. 31, 2022	FY 21-22 Budget	FY 21-22 Projected	Over/Under Budget	% of Budget
City Council	\$ 77,414	\$ 79,063	\$ 116,141	\$ 107,241	\$ (8,900)	92.3%
City Manager	851,330	855,075	1,303,034	1,183,034	(120,000)	90.8%
City Clerk	505,666	445,519	596,133	601,633	5,500	100.9%
City Attorney	717,258	735,706	1,058,985	1,001,985	(57,000)	94.6%
Mgmt & Budget	3,808,555	3,717,337	5,399,728	5,262,527	(137,201)	97.5%
Human Resources	454,341	457,691	846,494	625,494	(221,000)	73.9%
Police	17,726,982	17,954,661	25,313,486	24,754,986	(558,500)	97.8%
Fire	17,378,597	17,879,688	22,526,257	24,893,757	2,367,500	110.5%
Community Dev	4,832,403	5,359,856	5,629,217	7,572,217	1,943,000	134.5%
Parks & Recreation	9,929,582	11,098,354	15,688,345	15,053,977	(634,368)	96.0%
Library	1,329,431	1,276,043	1,930,397	1,758,397	(172,000)	91.1%
Public Works	5,194,412	5,626,462	8,355,071	7,870,571	(484,500)	94.2%
Non-Departmental	2,590,711	4,051,919	6,174,753	7,604,753	1,430,000	123.2%
Total Expenditures	\$ 65,396,684	\$ 69,537,373	\$ 94,938,041	\$ 98,290,572	\$ 3,352,531	103.5%

The following is an explanation of the department specific variances of year-end projections as compared to the budget:

- Community Development department is projected to end the fiscal year \$1.94 million (34.52%) over the budgeted amount, which is mostly due to increases in contract costs that are mostly offset by increased revenues.
- Fire department is projected to end the fiscal year \$2.37 million (10.51%) over the budgeted amount, which is mainly seen in overtime costs, vehicle maintenance and capital outlay. Overtime costs have been impacted by employees out after an exposure to COVID-19, wildfire strike teams, retirements and multiple long term vacancies. Capital outlay costs are due to the purchase of an additional two ambulances and will depend on if those ambulances are completed during the current fiscal year. Due to shortages of parts and computer chips, these costs may not materialize until next fiscal year.
- Non-Departmental is projected to end the fiscal year \$1.43 million over the budget amount, which is mainly due to an increase in contract costs and the approval of replacement of the fuel tank at the corporation yard.

Overall General Fund departments' expenditures are trending at budget (in line with the 75% expectation) at this point in the fiscal year.

Enterprise Funds:

Water Fund

The Water Fund is reported on a combined basis and includes the following funds: Water Impact, Water Operating, Water Capital and Water Meters.

The table below includes cumulative third quarter actual revenue and expense comparisons for FY 2020-21 and FY 2021-22 and a budget to actual comparison for FY 2021-22 for the Water Operating Fund.

	FY 20-21 Actual Mar. 31, 2021	FY 21-22 Actual Mar. 31, 2022	FY 21-22 Budget	FY 21-22 Projected	Over/Under Budget	% of Budget
Program Revenues	\$ 12,105,211	\$ 12,036,043	\$ 17,865,000	\$ 17,915,000	\$ 50,000	100.3%
Salaries	2,212,931	2,273,681	3,215,801	3,208,801	(7,000)	99.8%
Benefits	1,651,683	1,622,651	2,273,758	2,268,758	(5,000)	99.8%
Operating Expenses	3,500,621	3,901,037	7,971,887	6,623,887	(1,348,000)	83.1%
Transfers Out	668,248	676,248	1,705,800	1,705,800	-	100.0%
Debt Service	11,175	7,623	1,892,985	1,892,985	-	100.0%
	<u>\$ 8,044,658</u>	<u>\$ 8,481,240</u>	<u>\$ 17,060,231</u>	<u>\$ 15,700,231</u>	<u>\$ (1,360,000)</u>	<u>92.0%</u>
Capital Expenses	\$ 2,005,874	\$ 1,784,082	\$ 16,311,634	\$ 3,993,634	\$ (12,318,000)	24.5%
Working Capital			\$ 21,046,468	\$ 19,267,603		

The Water Fund is projected to end the year with program revenues of \$17.92 million. Total operating expenses, including transfers out and debt service are projected to end the year at \$15.70 million, or 92.0% of budget. This reduction from budgeted amounts is mostly due to savings from budgeted contracts not anticipated to be fully needed this fiscal year. Total expenditures for capital projects are estimated to be \$3.99 million at year-end. The fund will end the year with projected working capital of \$19.27 million.

Wastewater Fund

The Wastewater Fund is reported on a combined basis and includes the Wastewater and Wastewater Capital Funds.

	FY 20-21 Actual Mar. 31, 2021	FY 21-22 Actual Mar. 31, 2022	FY 21-22 Budget	FY 21-22 Projected	Over/Under Budget	% of Budget
Program Revenues	\$ 6,349,791	\$ 7,178,758	\$ 8,525,154	\$ 8,625,154	\$ 100,000	101.17%
Salaries	1,179,926	1,238,853	1,733,816	1,665,816	(68,000)	96.08%
Benefits	903,192	936,198	1,296,782	1,267,782	(29,000)	97.76%
Operating Expenses	798,189	813,631	2,191,084	1,735,084	(456,000)	79.19%
Transfers Out	500,894	516,566	725,198	725,198	-	100.00%
Debt Service					-	-
	<u>\$ 3,382,201</u>	<u>\$ 3,505,248</u>	<u>\$ 5,946,880</u>	<u>\$ 5,393,880</u>	<u>\$ (553,000)</u>	<u>90.70%</u>
Capital Expenses	\$ 289,064	\$ 1,021,131	\$ 16,951,123	\$ 2,036,123	\$ (14,915,000)	12.01%
Working Capital			\$ 16,696,316	\$ 17,891,467		

The Wastewater Fund is projected to end the year with program revenues of \$8.63 million. Total operating expenses, including transfers out, are projected to end the year at \$5.39 million, or 90.70% of budget. This reduction from budgeted

amounts is mostly due to savings in supply and maintenance costs. Total expenditures for capital projects are estimated to be \$2.04 million at year-end. The fund will end the year with projected working capital of \$17.89 million.

Solid Waste Fund

The Solid Waste Fund is reported on a combined basis and includes the Solid Waste Operating, Solid Waste Capital, and Solid Waste Plan Area Capital.

	FY 20-21 Actual Mar. 31, 2021	FY 21-22 Actual Mar. 31, 2022	FY 21-22 Budget	FY 21-22 Projected	Over/Under Budget	% of Budget
Program Revenues	\$ 11,221,276	\$ 15,689,748	\$ 16,241,410	\$ 19,011,410	\$ 2,770,000	117.1%
Salaries	2,394,723	2,569,896	3,871,824	3,520,824	(351,000)	90.9%
Benefits	1,878,396	2,109,863	3,069,820	2,830,820	(239,000)	92.2%
Operating Expenses	4,206,445	4,873,542	6,693,409	7,326,409	633,000	109.5%
Transfers Out	1,420,838	1,251,638	1,742,377	1,742,377	-	100.0%
Debt Service	-	-	-	-	-	0.0%
	<u>\$ 9,900,402</u>	<u>\$ 10,804,939</u>	<u>\$ 15,377,430</u>	<u>\$ 15,420,430</u>	<u>\$ 43,000</u>	<u>100.3%</u>
Capital Expenses	\$ 1,490,261	\$ 799,495	\$ 3,943,709	\$ 2,093,709	\$ (1,850,000)	53.1%
Working Capital			\$ 6,576,585	\$ 8,073,856		

The Solid Waste Fund is projected to end the year with program revenues of \$19.01 million. Total operating expenses, including transfers out, are projected to end the year at \$15.42 million, or 100.3% of budget. The fund is currently projected to be slightly over budget mainly due to the increase in contract costs related to recycling and organics changes. Total expenditures for capital outlay costs are estimated to be \$2.09 million at year-end. The fund will end the year with projected working capital of \$8.07 million.

Other Funds

City Housing Fund

The City Housing Fund as of March 31, 2022 had a cash balance of \$6,180,270. The City Council had also previously approved housing project loans in an amount up to \$3.5 million for the Scholar Way project of which \$2.75 million has now been disbursed.

Risk Management Internal Service Fund

The Risk Management Fund captures the activity associated with employee and retiree health, dental and vision insurance, workers' compensation, and liability insurance expense.

As of March 31, 2022, the City has paid \$5.58 million for health, vision, and dental insurance for active employees and \$3.46 million for retired employees and \$1.97 million for workers' compensation. Liability insurance payments were \$2.45 million with an additional \$760,000 for property premium. The total expenditures for FY 2022 are projected at \$19.43 million, which is an increase from the prior fiscal year of \$1.29 million, which is mostly seen in health insurance, workers compensation, and liability costs.

The projected ending unrestricted net position is \$5.33 million, a \$1.55 decrease from FY 2020-21.

Lighting and Landscape Funds

There are 29 Lighting and Landscape (L&L) Districts in the City of Folsom. Each District has its own budget and maintenance requirements to maintain various types of assets ranging from shrub beds, mini parks, walls, fences, monument signs, streetlights, bollards, landscape lighting, irrigation systems, artwork, a waterfall, walkways/trails, open space, trees, and electrical services.

Some activities that have taken place in the L&L's during this time period include:

District	Project	Date	Cost
American River Canyon North	Kinglet Ct retaining wall and swale repair.	March	\$3,870
Willow Creek Estates East	Entry Sign Replacement	February	\$5,643
Natoma Valley	Quigley Ct. plant replacement and arbor mulch	February	\$390
Willow Creek Estates South	Silberhorn Plant Infill Project	January	\$6,225

Plan Area Impact Fees

Total Plan Area Impact Fees received through the 3rd Quarter of FY 2022 was \$14.33 million. Expenditures during the third quarter total approximately \$712,000 in all Plan Area Impact Fee funds. Expenditures were for the final payment on the Corporation Yard property, ½ of a garbage truck, ½ of a streetsweeper, and design costs for trails, parks and the fire station.

APPENDIX A

City of Folsom, California
Combined General Fund

Revenue and Expense Statement
Quarter Ended March 31, 2022

	FY 2021	FY 2022	FY 2021	FY 2022	FY22 Forecast	VARIANCE		VARIANCE	
	As of	As of	FY 2021	FY 2022	As of	Forecast vs Budget		Acutal vs Budget	
	3/31/2021	3/31/2022	ACTUAL	BUDGET	3/31/2022	\$	%	\$	%
REVENUES:									
Taxes:									
Property	\$ 15,656,038	\$ 16,249,998	\$ 31,253,436	\$ 32,491,949	\$ 33,010,662	\$ 518,713	102%	\$ (16,241,951)	50%
Sales And Use	14,763,739	15,921,650	25,846,985	24,551,790	27,008,127	2,456,337	110%	(8,630,140)	65%
Transient Occupancy	718,276	1,368,351	1,357,550	2,000,000	1,900,000	(100,000)	95%	(631,649)	68%
Real Property Transfer	394,926	570,054	799,193	685,000	1,000,000	315,000	146%	(114,946)	83%
Franchise Fees	2,135	-	738,256	751,800	754,555	2,755	100%	(751,800)	0%
Other	390,966	684,510	710,605	1,000,000	900,000	(100,000)	90%	(315,490)	68%
Licenses And Permits	2,951,186	3,121,916	4,187,991	3,315,700	3,991,783	676,083	120%	(193,784)	94%
Intergovernmental	5,468,161	6,825,978	9,693,482	10,795,984	11,163,221	367,237	103%	(3,970,006)	63%
Charges For Current Services	8,520,013	11,844,469	13,874,002	9,839,240	16,048,997	6,209,757	163%	2,005,229	120%
Fines And Forfeitures	92,065	88,356	155,554	140,000	159,716	19,716	114%	(51,644)	63%
Interest	134,039	98,931	69,519	230,000	106,000	(124,000)	46%	(131,069)	43%
Miscellaneous	872,809	583,106	839,226	743,400	715,056	(28,344)	96%	(160,294)	78%
Operating Transfers In	4,083,086	4,214,980	6,058,559	8,393,178	6,008,753	(2,384,425)	72%	(4,178,198)	50%
TOTAL REVENUES	54,047,439	61,572,301	95,584,357	94,938,041	102,766,870	7,828,829	108.25%	(33,365,740)	65%
EXPENDITURES:									
Current Operating:									
General Government	\$ 8,714,432	\$ 8,960,028	\$ 11,785,161	\$ 13,986,204	\$ 12,353,603	\$ (1,632,601)	88%	\$ 5,026,176	64%
Public Safety	34,957,199	35,672,403	47,103,373	47,561,942	49,413,942	1,852,000	104%	11,889,539	75%
Public Ways and Facilities	5,194,412	5,626,462	7,044,507	8,355,071	7,870,571	(484,500)	94%	2,728,609	67%
Community Services	4,832,403	5,359,856	6,825,605	5,629,217	7,572,217	1,943,000	135%	269,361	95%
Culture and Recreation	9,107,526	9,866,705	13,198,419	13,230,854	13,475,486	244,632	102%	3,364,149	75%
Non-Departmental	2,590,711	4,051,919	3,855,487	6,174,753	7,604,753	1,430,000	123%	2,122,834	66%
Operating Transfers Out	-	-	-	-	-	-	-	-	-
TOTAL EXPENDITURES	65,396,684	69,537,373	89,812,552	94,938,041	98,290,572	3,352,531	103.5%	25,400,668	73%
APPROPRIATION OF FUND BALANCE	(11,349,245)	(7,965,072)	5,771,805	-	4,476,298				
FUND BALANCE, JULY 1	15,697,621	21,469,426	15,697,621	21,469,426	21,469,426				
FUND BALANCE	4,348,377	13,504,354	21,469,426	21,469,426	25,945,724				
NONSPENDABLE FUND BALANCE	(801,458)	(310,673)	(455,629)	(310,673)	(310,673)				
RESTRICTED FUND BALANCE	-	-	-	-	-				
COMMITTED FUND BALANCE	-	-	-	-	-				
ASSIGNED FUND BALANCE	(508,799)	(461,911)	(1,813,094)	-	(2,657,198)				
UNRESTRICTED FUND BALANCE	\$ 3,038,119	\$ 12,731,770	\$ 19,200,704	\$ 21,158,753	\$ 22,977,853				

APPENDIX B

City of Folsom, California

Expenditure Summary - General Fund Departments
Quarter Ended March 31, 2022

	FY 2021	FY 2022	FY 2021	FY 2022	FY22 Forecast	VARIANCE		VARIANCE	
	As of	As of	FY 2021	FY 2022	As of	Forecast vs Budget		Actual vs. Budget	
	3/31/2021	3/31/2022	ACTUAL	BUDGET	3/31/2022	\$	%	\$	%
EXPENDITURES:									
City Council	\$ 77,414	\$ 79,063	\$ 104,152	\$ 116,141	\$ 107,241	\$ (8,900)	92.34%	\$ (37,078)	68%
City Manager	851,330	855,075	1,182,339	1,303,034	1,183,034	(120,000)	90.79%	(447,959)	66%
City Clerk	505,666	445,519	647,613	596,133	601,633	5,500	100.92%	(150,614)	75%
Office of Mgmt & Budget	3,808,555	3,717,337	5,000,741	5,399,728	5,262,527	(137,201)	97.46%	(1,682,391)	69%
City Attorney	717,258	735,706	953,138	1,058,985	1,001,985	(57,000)	94.62%	(323,279)	69%
Human Resources	454,341	457,691	622,182	846,494	625,494	(221,000)	73.89%	(388,803)	54%
Police	17,726,982	17,954,661	23,564,627	25,313,486	24,754,986	(558,500)	97.79%	(7,358,825)	71%
Fire	17,378,597	17,879,688	23,771,961	22,526,257	24,893,757	2,367,500	110.51%	(4,646,569)	79%
Community Development	4,832,403	5,359,856	6,825,605	5,629,217	7,572,217	1,943,000	134.52%	(269,361)	95%
Parks & Recreation	9,929,582	11,098,354	14,401,303	15,688,345	15,053,977	(634,368)	95.96%	(4,589,991)	71%
Library	1,329,431	1,276,043	1,838,898	1,930,397	1,758,397	(172,000)	91.09%	(654,354)	66%
Public Works	5,194,412	5,626,462	7,044,507	8,355,071	7,870,571	(484,500)	94.20%	(2,728,609)	67%
Other	-	-	-	-	-	-	-	-	-
Non Departmental	2,590,711	4,051,919	3,855,487	6,174,753	7,604,753	1,430,000	123.16%	(2,122,834)	66%
Operating Transfers Out	-	-	-	-	-	-	-	-	-
TOTAL EXPENDITURES:	\$ 65,396,684	\$ 69,537,373	\$ 89,812,552	\$ 94,938,041	\$ 98,290,572	\$ 3,352,531	103.53%	\$ (25,400,668)	73%

APPENDIX C

City of Folsom, California
Housing Fund

Revenue and Expense Statement
Quarter Ended March 31, 2022

	FY 2022	FY 2021 ACTUAL	FY 2022 BUDGET	FY22 Forecast	VARIANCE		VARIANCE	
	As of 3/31/2022			As of 3/31/2022	Forecast vs Budget		Actual vs Budget	
					\$	%	\$	%
REVENUES:								
Taxes	\$ -	\$ -	\$ -	\$ -	\$ -		\$ -	
Intergovernmental	-	-	-	-	-		-	
Charges for Current Services	47,265	51,446	20,000	49,000	29,000	245%	27,265	236%
Impact Fee Revenue	4,784,947	4,733,226	250,000	5,500,000	5,250,000	2200%	4,534,947	1914%
Interest Revenue	166,213	344,537	50,000	230,000	180,000	460%	116,213	332%
Other Revenue	-	2,456,121	55,385	-	(55,385)	0%	(55,385)	0%
Operating Transfers In	-	-	-	-	-		-	
TOTAL REVENUES	4,998,425	7,585,330	375,385	5,779,000	5,403,615	1539%	4,623,040	1332%
EXPENDITURES:								
Salary & Benefits	\$ -	\$ -	\$ -	\$ -	\$ -		\$ -	
Services & Supplies	-	-	-	-	-		-	
Contracts	8,652,316	3,564,573	310,000	8,920,000	8,610,000	2877%	(8,342,316)	2791%
Insurance	-	-	-	-	-		-	
Other Operating Expenses	5,500	16,418	40,300	8,300	(32,000)	21%	34,800	14%
Capital Outlay	-	-	-	-	-		-	
Extroardinary Loss on Dissolution of RDAs	-	-	-	-	-		-	
Operating Transfers Out	18,814	112,360	-	-	-		(18,814)	
TOTAL EXPENDITURES	8,676,630	3,693,351	350,300	8,928,300	(8,578,000)	2549%	(8,326,330)	2477%
APPROPRIATION OF FUND BALANCE	(3,678,205)	3,891,979	25,085	(3,149,300)				
FUND BALANCE, JULY 1	29,167,730	25,275,751	29,167,730	29,167,730				
FUND BALANCE	\$ 25,489,525	\$ 29,167,730	\$ 29,192,815	\$ 26,018,430				
NONSPENDABLE FUND BALANCE	(2,261,875)	(2,261,875)	(29,192,815)	(26,018,430)				
RESTRICTED FUND BALANCE	-	-	-	-				
COMMITTED FUND BALANCE	-	-	-	-				
ASSIGNED FUND BALANCE	-	-	-	-				
UNRESTRICTED FUND BALANCE (DEFICIT)	\$ 23,227,650	\$ 26,905,855	\$ -	\$ -				

APPENDIX D

City of Folsom, California
Lighting and Landscaping Districts

Revenue and Expenditure Statement
Quarter Ended March 31, 2022

	Fund 204	Fund 205	Fund 207	Fund 208	Fund 209	Fund 210	Fund 212	Fund 213	Fund 214	Fund 215	Fund 231	Fund 232
	Los Cerros	Briggs Ranch	Natoma Station	Folsom Heights	Broadstone Unit 3	Broadstone	Hannaford Cross	Lake Natoma Shores	Cobble Hills Reflect	Prairie Oaks #2	Sierra Estates	Natoma Valley
Revenues:												
Special Assessment	23,394	46,135	96,561	11,660	12,311	213,893	10,528	12,895	24,477	160,977	4,890	36,359
Interest	797	-	-	152	172	-	-	525	-	196	127	1,235
Other Revenue	-	-	-	-	540	270	3,541	-	-	-	-	-
Total Revenue	\$ 24,190	\$ 46,135	\$ 96,561	\$ 11,812	\$ 13,023	\$ 214,163	\$ 14,069	\$ 13,420	\$ 24,477	\$ 161,173	\$ 5,016	\$ 37,594
Expenditures:												
Communications	-	-	-	-	-	-	-	-	-	-	-	-
Utilities	8,765	12,595	51,742	6,024	1,064	117,958	2,927	2,683	9,374	-	1,111	2,508
Contracts	4,157	5,825	19,342	2,713	23,082	1,251	1,306	848	1,898	-	577	652
Maintenance	16,328	32,643	54,726	-	-	-	6,037	4,407	22,774	1,329	1,899	20,762
Supplies	654	1,120	13,506	1,452	-	-	842	564	1,400	-	165	423
Transfers Out	4,496	12,519	28,877	617	1,077	48,364	2,260	2,852	8,324	-	1,022	6,842
Total Expenditures	\$ 34,399	\$ 64,700	\$ 168,192	\$ 10,807	\$ 25,223	\$ 167,572	\$ 13,371	\$ 11,355	\$ 43,770	\$ 1,329	\$ 4,773	\$ 31,188

APPENDIX D

City of Folsom, California
Lighting and Landscaping Districts

Revenue and Expenditure Statement
 Quarter Ended March 31, 2022

	Fund 234	Fund 236	Fund 237	Fund 249	Fund 250	Fund 251	Fund 252	Fund 253	Fund 260	Fund 262	Fund 266	Fund 267
	Cobble Ridge	Praire Oaks Ranch	Silverbrook	Willow Creek East	Blue Ravine Oaks	Steeplechase	Willow Creek So.	Am River Canyon No.	Willow Springs	Willow Sprgs CFD#11	Broadstone 3 CFD #12	ARC No.2 CFD #13
Revenues:												
Special Assessment	8,057	111,040	-	34,090	20,048	13,974	89,744	59,381	8,075	23,858	331,705	63,289
Interest	617	270	523	-	557	401	3,439	270	26	1,779	8,249	333
Other Revenue	-	-	-	-	-	-	-	-	-	-	-	-
Total Revenue	\$ 8,674	\$ 111,310	\$ 523	\$ 34,090	\$ 20,605	\$ 14,375	\$ 93,183	\$ 59,651	\$ 8,101	\$ 25,637	\$ 339,954	\$ 63,622
Expenditures:												
Communications	-	-	-	-	-	-	-	-	-	-	421	161
Utilities	690	51,329	468	17,258	9,757	3,884	50,096	27,278	4,536	12,856	74,032	11,405
Contracts	1,112	21,093	3,761	1,512	788	1,482	20,399	13,892	3,816	-	60,925	6,116
Maintenance	2,908	95,335	2,185	11,443	957	12,597	31,813	-	-	63,627	191,628	34,167
Supplies	220	9,410	24	2,519	14	273	6,164	5,457	-	7,539	16,142	-
Transfers Out	1,089	49,727	1,193	1,830	1,122	3,645	8,179	3,725	356	13,079	54,634	17,024
Total Expenditures	\$ 6,018	\$ 226,894	\$ 7,630	\$ 34,562	\$ 12,637	\$ 21,882	\$ 116,651	\$ 50,351	\$ 8,709	\$ 97,100	\$ 397,782	\$ 68,872

APPENDIX D

City of Folsom, California
Lighting and Landscaping Districts

Revenue and Expenditure Statement
Quarter Ended March 31, 2022

	Fund 270	Fund 271	Fund 275	Fund 278	Fund 281	Fund 282	Fund 283	Fund 284	Fund 285	Fund 288	Fund 289	Fund 291	
	ARC No. 2	Residences At ARC	ARC North #3	Blue Ravine Oaks No. 2	Folsom Hts #2	Broadstone #4	Islands CFD #16	Willow Creek Estates #2	Prospect Ridge	Maint Dist CFD #18	Maint Dist CFD #19	Maint Dist CFD #23 A1	TOTAL
Revenues:													
Special Assessment	6,932	11,829	140,540	19,557	33,510	52,651	-	42,010	21,840	-	-	-	1,746,208
Interest	901	330	5,647	955	1,471	393	2,114	625	97	4,546	2,273	-	39,019
Other Revenue	-	-	-	-	-	-	-	-	-	-	-	-	4,351
Total Revenue	\$ 7,832	\$ 12,159	\$ 146,187	\$ 20,512	\$ 34,981	\$ 53,045	\$ 2,114	\$ 42,635	\$ 21,937	\$ 4,546	\$ 2,273	\$ -	\$ 1,789,579
Expenditures:													
Communications	-	-	-	-	-	-	-	-	-	-	-	-	581
Utilities	190	2,440	209	-	-	-	5,420	-	1,407	98,383	7,956	1,340	597,686
Contracts	6,425	1,144	1,677	970	5,305	40,095	4,113	67,714	552	7,218	578	-	332,338
Maintenance	-	9,393	63,459	6,807	23,352	110,975	50,235	32,609	4,800	105,694	9,072	357	1,024,317
Supplies	-	923	1,868	635	254	22,204	2,546	375	259	1,957	-	-	98,906
Transfers Out	473	4,698	15,554	3,803	4,656	10,529	11,519	11,070	4,079	1,758	617	-	341,606
Total Expenditures	\$ 7,089	\$ 18,599	\$ 82,767	\$ 12,215	\$ 33,566	\$ 183,804	\$ 73,833	\$ 111,768	\$ 11,096	\$ 215,011	\$ 18,223	\$ 1,697	\$ 2,395,434

APPENDIX E

**City of Folsom, California
Combined Water Funds*
Revenue and Expense Statement
Quarter Ended March 31, 2022**

	FY 2021	FY 2022	FY 2021	FY 2022	FY21 Forecast	VARIANCE		VARIANCE	
	As of	As of	FY 2021	FY 2022	As of	Forecast vs Budget		Actual vs Budget	
	3/31/2021	3/31/2022	ACTUAL	BUDGET	3/31/2022	\$	%	\$	%
OPERATING REVENUES:									
Charges For Services	12,105,211	12,036,043	18,243,570	17,865,000	17,915,000	50,000	100%	(5,828,957)	67%
TOTAL OPERATING REVENUES	12,105,211	12,036,043	18,243,570	17,865,000	17,915,000	50,000	100%	(5,828,957)	67%
OPERATING EXPENSES:									
Salaries	2,212,931	2,273,681	2,930,059	3,215,801	3,208,801	(7,000)	100%	(942,120)	71%
Benefits	1,651,683	1,622,651	2,371,104	2,273,758	2,268,758	(5,000)	100%	(651,107)	71%
Utilities	501,441	506,324	879,769	713,100	715,100	2,000	100%	(206,776)	71%
Supplies	729,614	663,803	983,110	1,197,390	1,059,390	(138,000)	88%	(533,587)	55%
Maintenance and Operation	267,059	828,154	520,036	1,089,757	905,757	(184,000)	83%	(261,603)	76%
Contractual Services	1,322,768	1,265,127	2,089,956	3,923,209	2,731,209	(1,192,000)	70%	(2,658,082)	32%
Depreciation	3,339,649	3,496,873	4,450,702	-	4,450,702	4,450,702	-	3,496,873	-
Other Operating Expenses	679,739	637,629	1,047,743	1,048,431	1,212,431	164,000	116%	(410,802)	61%
TOTAL OPERATING EXPENSES	10,704,885	11,294,242	15,272,480	13,461,446	16,552,148	3,090,702	123%	(2,167,204)	84%
OPERATING INCOME	1,400,326	741,801	2,971,090	4,403,554	1,362,852		31%	589,357	
NONOPERATING REVENUE (EXPENSES):									
Impact Fees	228,769	442,937	268,879	690,225	480,225	(210,000)	70%	(247,288)	64%
Other	13,364	15,889	5,068,659	13,644,640	25,000	(13,619,640)	0%	(13,628,752)	0%
Investment Income	173,647	122,803	75,078	272,000	215,000	(57,000)	79%	(149,197)	45%
Intergovernmental	-	-	455,908	100,000	100,000	-	100%	(100,000)	0%
Proceeds of Financing	-	-	-	-	-	-	-	-	-
Debt Service Expense	(11,175)	(7,623)	(836,898)	(1,892,985)	(1,892,985)	-	100%	1,885,362	0%
Other Reimbursements	-	-	-	-	-	-	-	-	-
Capital Outlay - Projects	(2,005,874)	(1,784,082)	450,139	(16,311,634)	(3,993,634)	12,318,000	24%	14,527,553	11%
TOTAL NONOPERATING REVENUE (EXPENSE)	(1,601,269)	(1,210,076)	5,481,764	(3,497,754)	(5,066,394)	(1,568,640)	145%	2,287,678	35%
INCOME (LOSS) BEFORE CAPITAL CONTRIBUTIONS AND TRANSFERS	(200,942)	(468,274)	8,452,854	905,800	(3,703,542)				
CAPITAL CONTRIBUTIONS AND TRANSFERS:									
Transfers In	53,833	-	94,486	800,000	800,000	-	100%	(800,000)	0%
Transfers Out	(668,248)	(676,248)	(933,797)	(1,705,800)	(1,705,800)	(1,568,640)	100%	1,029,553	40%
TOTAL CAPITAL CONTRIBUTIONS AND TRANSFERS	(614,415)	(676,248)	(839,311)	(905,800)	(905,800)				
CHANGE IN NET ASSETS	(815,357)	(1,144,522)	7,613,544	-	(4,609,342)				
NET ASSETS, JULY 1	102,364,566	109,978,103	102,364,559	109,978,103	109,978,103				
NET ASSETS	101,549,210	108,833,581	109,978,103	109,978,103	105,368,761				
RESTRICTED NET ASSETS	(3,490,719)	(3,591,915)	(1,142,929)	(3,591,915)	(3,591,915)				
UNRESTRICTED NET ASSETS	\$ 98,058,491	\$ 105,241,666	\$ 108,835,174	\$ 106,386,188	\$ 101,776,846				

* Includes the following funds: Water Impact Fee, Water Operating, Water Capital and Water Meters
Prior year includes prior period adjustment for GASB 68

APPENDIX F

City of Folsom, California
 Combined Wastewater Funds*
 Revenue and Expense Statement
 Quarter Ended March 31, 2022

	FY 2021	FY 2022	FY 2021	FY 2022	FY21 Forecast	VARIANCE		VARIANCE	
	As of 3/31/2021	As of 3/31/2022	ACTUAL	BUDGET	As of 3/31/2022	Forecast vs Budget \$	%	Actual vs Budget \$	%
OPERATING REVENUES:									
Charges For Services	6,299,391	7,122,758	8,407,529	8,457,954	8,557,954	100,000	101%	(1,335,196)	84%
Prison Services	50,400	56,000	67,200	67,200	67,200	-	100%	(11,200)	83%
TOTAL OPERATING REVENUES	6,349,791	7,178,758	8,474,729	8,525,154	8,625,154	100,000	101%	(1,346,396)	84%
OPERATING EXPENSES:									
Salaries	1,179,926	1,238,853	1,569,341	1,733,816	1,665,816	(68,000)	96%	(494,963)	71%
Benefits	903,192	936,198	1,357,130	1,296,782	1,267,782	(29,000)	98%	(360,584)	72%
Utilities	60,631	57,586	95,818	90,000	90,000	-	100%	(32,414)	64%
Supplies	176,060	201,738	275,744	530,464	287,464	(243,000)	54%	(328,726)	38%
Maintenance and Operation	128,728	167,607	194,184	376,150	229,150	(147,000)	61%	(208,543)	45%
Contractual Services	165,654	156,658	475,823	782,760	780,760	(2,000)	100%	(626,102)	20%
Depreciation	1,665,199	1,735,638	2,227,348	-	2,227,348	2,227,348	-	1,735,638	-
Other Operating Expenses	267,116	230,042	427,003	411,710	347,710	(64,000)	84%	(181,669)	56%
TOTAL OPERATING EXPENSES	4,546,506	4,724,319	6,622,392	5,221,682	6,896,030	1,674,348	132%	(497,363)	90%
OPERATING INCOME (LOSS)	1,803,285	2,454,439	1,852,337	3,303,472	1,729,124		52%		
NONOPERATING REVENUE (EXPENSES):								177,813	
Impact Fees	31,642	52,037	39,835	186,920	71,920	(115,000)	38%	(134,884)	27.8%
Investment Income	124,976	101,712	102,883	135,000	138,000	3,000	102%	(33,288)	75%
Other	9,095	9,233	3,172,450	14,050,929	60,000	(13,990,929)	0%	(14,041,696)	0%
Debt Service	-	-	-	-	-	-	-	-	-
Capital Outlay - Projects	(289,064)	(1,021,131)	(566,353)	(16,951,123)	(2,036,123)	14,915,000	12%	15,929,992	6%
TOTAL NONOPERATING REVENUE (EXPENSE)	(123,351)	(858,150)	2,748,815	(2,578,274)	(1,766,203)	812,071	69%	1,720,124	33%
INCOME (LOSS) BEFORE CAPITAL CONTRIBUTIONS AND TRANSFERS	1,679,934	1,596,289	4,601,152	725,198	(37,079)				
CAPITAL CONTRIBUTIONS AND TRANSFERS:									
Transfers In	30,133	-	30,133	-	-	-	-	-	-
Transfers Out	(500,894)	(516,566)	(670,845)	(725,198)	(725,198)	-	0%	(208,632)	248%
TOTAL CAPITAL CONTRIBUTIONS AND TRANSFERS	(470,761)	(516,566)	(640,712)	(725,198)	(725,198)				
CHANGE IN NET ASSETS	1,209,173	1,079,723	3,960,440	-	(762,277)				
NET ASSETS, JULY 1	65,009,255	68,969,695	65,009,255	68,969,695	68,969,695				
NET ASSETS	66,218,428	70,049,418	68,969,695	68,969,695	68,207,418				
RESTRICTED NET ASSETS	(2,437,465)	(1,025,604)	(1,943,382)	(1,025,604)	(1,025,604)				
UNRESTRICTED NET ASSETS	\$ 63,780,963	\$ 69,023,814	\$ 67,026,313	\$ 67,944,091	\$ 67,181,814				

* Includes the following funds: Sewer Operating and Sewer Capital
 Prior year includes prior period adjustment for GASB 68

APPENDIX G

City of Folsom, California
 Combined Solid Waste Funds*
 Revenue and Expense Statement
 Quarter Ended March 31, 2022

	FY 2021	FY 2022	FY 2021	FY 2022	FY21 Forecast	VARIANCE		VARIANCE	
	As of	As of	FY 2021	FY 2022	As of	Forecast vs Budget		Actual vs Budget	
	3/31/2021	3/31/2022	ACTUAL	BUDGET	3/31/2022	\$	%	\$	%
OPERATING REVENUES:									
Charges For Services	11,221,276	15,689,748	15,206,531	16,241,410	19,011,410	2,770,000	117%	(551,662)	97%
TOTAL OPERATING REVENUES	11,221,276	15,689,748	15,206,531	16,241,410	19,011,410	2,770,000	117%	(551,662)	97%
OPERATING EXPENSES:									
Salaries	2,394,723	2,569,896	3,151,620	3,871,824	3,520,824	(351,000)	91%	(1,301,928)	66%
Benefits	1,878,396	2,109,863	2,727,918	3,069,820	2,830,820	(239,000)	92%	(959,957)	69%
Utilities	27,332	29,707	38,943	20,500	43,500	23,000	212%	9,207	145%
Supplies	559,189	688,532	909,956	1,219,102	1,248,102	29,000	102%	(530,570)	56%
Maintenance and Operation	808,496	724,862	1,110,328	538,133	994,133	456,000	185%	186,729	135%
Contractual Services	2,456,670	3,010,809	3,631,672	4,285,387	4,400,387	115,000	103%	(1,274,578)	70%
Depreciation	600,643	577,833	802,574	-	802,574	802,574		577,833	
Other Operating Expenses	354,758	419,632	549,205	630,287	640,287	10,000	102%	(210,655)	67%
TOTAL OPERATING EXPENSES	9,080,208	10,131,133	12,922,215	13,635,053	14,480,627	845,574	106%	(3,503,920)	74%
OPERATING INCOME (LOSS)	2,141,068	5,558,615	2,284,316	2,606,357	4,530,783				
NONOPERATING REVENUE (EXPENSE):								1,050,925	
Impact Fees	271,335	433,412	456,683	511,340	566,340	55,000	110.8%	(77,928)	85%
Investment Income	43,617	42,328	19,706	82,000	80,500	(1,500)	98%	(39,672)	52%
Intergovernmental Revenues	498	51,993	30,286	158,097	83,097	(75,000)	53%	(106,104)	33%
Other	175,565	253,879	242,438	2,328,292	310,216	(2,018,076)	13%	(2,074,413)	11%
Debt Service-Expense	-	-	-	-	-	-		-	
Capital Outlay	(1,490,261)	(799,495)	-	(3,943,709)	(2,093,709)	1,850,000	53%	3,144,214	20%
TOTAL NONOPERATING REVENUE (EXPENSE)	(999,246)	(17,883)	749,113	(863,980)	(1,053,556)	(189,576)	122%	846,097	2%
INCOME (LOSS) BEFORE CAPITAL CONTRIBUTIONS AND TRANSFERS	1,141,822	5,540,733	3,033,429	1,742,377	3,477,227				
CAPITAL CONTRIBUTIONS AND TRANSFERS:									
Transfers In	424,889	-	427,089	-	-	-		-	
Transfers Out	(1,420,838)	(1,251,638)	(1,783,148)	(1,742,377)	(1,742,377)	-	0%	490,739	-255%
TOTAL CAPITAL CONTRIBUTIONS AND TRANSFERS	(995,949)	(1,251,638)	(1,356,059)	(1,742,377)	(1,742,377)				
CHANGE IN NET ASSETS	145,874	4,289,095	1,677,370	-	1,734,850				
NET ASSETS, JULY 1	(5,649,391)	(3,972,023)	(5,649,392)	(3,972,020)	(3,972,020)				
NET ASSETS	(5,503,518)	317,072	(3,972,023)	(3,972,020)	(2,237,170)				
RESTRICTED NET ASSETS	(893,289)	(2,923,962)	-	(2,923,962)	(2,923,962)				
UNRESTRICTED NET ASSETS	\$ (6,396,807)	\$ (2,606,889)	\$ (3,972,023)	\$ (6,895,982)	\$ (5,161,132)				

* Includes the following funds: Solid Waste Operating, Solid Waste Capital, and Solid Waste Plan Area Capital
 Prior year includes prior period adjustment for GASB 68



CITY OF
FOLSOM
DISTILLED BY NATURE

Folsom City Council Staff Report



MEETING DATE:	5/10/2022
AGENDA SECTION:	Scheduled Presentations
SUBJECT:	Presentation of the City Manager’s FY 2022-23 Proposed Operating and Capital Budgets for the City of Folsom, the Successor Agency, the Folsom Public Financing Authority, and the Folsom Ranch Public Financing Authority
FROM:	Finance Department

The City Manager’s Fiscal Year 2022-23 proposed budget will be presented. This budget will encompass the 12-month period from July 1, 2022 through June 30, 2023 and will also include the Capital Improvement Plan.

Submitted,

Stacey Tamagni, Finance Director/CFO

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City Council Special Meeting

MINUTES

Tuesday, April 12, 2022 6:15 PM

CALL TO ORDER

The special City Council meeting was called to order at 6:15 p.m. with Mayor Kerri Howell presiding.

ROLL CALL:

Councilmembers Present: Mike Kozlowski, Councilmember
Rosario Rodriguez, Vice Mayor
Sarah Aquino, Councilmember
YK Chalamcherla, Councilmember
Kerri Howell, Mayor

Councilmembers Absent: None

Participating Staff: City Manager Elaine Andersen
City Attorney Steve Wang
City Clerk Christa Freemantle

ADJOURNMENT TO CLOSED SESSION FOR THE FOLLOWING PURPOSES:

1. Conference with Legal Counsel – Existing Litigation – Pursuant to Government Code Section 54956.9(d)(1): Sheldon Sterling v. City of Folsom, Workers' Compensation Appeals Board Case Nos. ADJ12607032 and ADJ12607033

Motion by Councilmember Mike Kozlowski, second by Vice Mayor Rosario Rodriguez to adjourn to Closed Session for the above referenced item. Motion carried with the following roll call vote:

AYES: Councilmember(s): Kozlowski Rodriguez, Aquino, Chalamcherla, Howell
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

RECONVENE

City Attorney Steve Wang announced that no final action was taken during Closed Session.

ADJOURNMENT

The meeting was adjourned to the regular City Council meeting at 6:30 p.m.

SUBMITTED BY:

Christa Freemantle, City Clerk

ATTEST:

Kerri Howell, Mayor

City Council Regular Meeting

MINUTES

Tuesday, April 12, 2022 6:30 PM

CALL TO ORDER

The regular City Council meeting was called to order at 6:31 pm with Mayor Kerri Howell presiding.

ROLL CALL:

Councilmembers Present: Mike Kozlowski, Councilmember
Rosario Rodriguez, Vice Mayor
Sarah Aquino, Councilmember
YK Chalamcherla, Councilmember
Kerri Howell, Mayor

Councilmembers Absent: None

Participating Staff: City Manager Elaine Andersen
City Attorney Steve Wang
City Clerk Christa Freemantle
City Arborist Aimee Nunez
Senior Trails Planner Brett Bolinger
Parks and Recreation Director Lorraine Poggione
CFO/Finance Director Stacey Tamagni
Public Works Director Mark Rackovan

PLEDGE OF ALLEGIANCE

The Pledge of Allegiance was recited.

AGENDA UPDATE

City Attorney Steve Wang announced that items 8 and 14 had additional information, and item 16 was continued to May 10th.

BUSINESS FROM THE FLOOR:

The following speakers addressed the City Council:

1. Jeanne Shuman and Beth Crokan regarding homelessness
2. Sharon Kindel regarding Hinkle Creek Nature Center
3. Janine Ferrer regarding SeeClickFix
4. Margie Donovan (via WebEx teleconference)

SCHEDULED PRESENTATIONS:

1. Proclamation of the Mayor of the City of Folsom Encouraging the Community to Become Involved in Arbor Day

Mayor Kerri Howell presented the proclamation to City Arborist Aimee Nunez.

2. Presentation on the Draft Active Transportation Plan (ATP) and Public Comment

Senior Trails Planner Brett Bolinger introduced consultant Erin David from Alta Planning and Design who made a presentation and responded to questions from the City Council. Senior Trails Planner Brett Bolinger and Parks and Recreation Director Lorraine Poggione also responded to questions.

The following speaker addressed the City Council:

1. Jim Kirstein

CONSENT CALENDAR:

Items appearing on the Consent Calendar are considered routine and may be approved by one motion. City Councilmembers may pull an item for discussion.

3. Approval of March 8, 2022 Special Meeting Workshop Minutes
4. Approval of March 8, 2022 Special/Regular Meeting Minutes
5. Approval of March 22, 2022 Special/Regular Meeting Minutes
6. Resolution No. 10828 - A Resolution Authorizing the City Manager to Execute an Agreement for Food and Beverage Concession at Lembi Community Park with the Folsom Athletic Association
7. Resolution No. 10829 – A Resolution Authorizing the City Manager to Execute Amendment No. 2 to the Agreement (Contract No.172-21 18-009) Between the City of Folsom and the Sacramento Society for the Prevention of Cruelty to Animals for Shelter Services
8. Resolution No. 10832 - A Resolution Authorizing the City Manager to Execute a Contract Amendment with R.E.Y. Engineers, Inc. for the Riley Street Sidewalk Feasibility Study and Appropriation of Funds

- 9. Resolution No. 10833 – A Resolution Authorizing the City Manager to Execute an Agreement with Sacramento County for Reduced Tipping Fees for Municipal Solid Waste at Kiefer Landfill
- 10. Resolution No. 10834 – A Resolution Authorizing the City Manager to Execute an Agreement with Sacramento County for Household Hazardous Waste Collection Program Services Provided to Folsom Residents at Sacramento County Owned Facilities
- 11. Resolution No. 10836 – A Resolution Amending Resolution No. 10479 to Update the Building Valuation Data and Clarify its Use
- 12. Resolution No. 10837 - A Resolution Authorizing the City Manager to Execute an Agreement with Brightview Holdings DBA Brightview Tree Care Services for Shaded Fuel Break Creation and Ladder Fuel Removal

Motion by Councilmember Sarah Aquino, second by Vice Mayor Rosario Rodriguez to approve the Consent Calendar.

Motion carried with the following roll call vote:

AYES: Councilmember(s): Kozlowski, Rodriguez, Aquino, Chalamcherla, Howell
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

JOINT CITY COUNCIL AGENDA

JOINT CITY COUNCIL / FOLSOM RANCH FINANCING AUTHORITY MEETING

ROLL CALL: Council / Board Members: Kozlowski, Rodriguez, Aquino, Chalamcherla, Howell

PUBLIC HEARING:

- 13. Folsom Ranch Financing Authority City of Folsom Community Facilities District No. 23 (Folsom Ranch) Improvement Area No.1 Special Tax Revenue Bonds, Series 2022
 - a. Resolution No. 10835 – A Resolution of the City Council of the City of Folsom Authorizing the Issuance of the City of Folsom Community Facilities District No. 23 (Folsom Ranch) Improvement Area No. 1 Special Tax Bonds, Series 2022, the Execution of a First Supplemental Indenture Providing therefor, Authorizing the Execution of a Local Obligation Purchase Contract, and Authorizing Necessary Actions and the Execution of other Documents in Connection therewith
 - b. Resolution No. 008-Folsom Ranch FA – A Resolution of the Governing Board of the Folsom Ranch Financing Authority Authorizing the Issuance, Sale and Delivery of not to exceed \$17,000,000 Aggregate Principal Amount of City of Folsom Community Facilities District No. 23 (Folsom Ranch) Improvement Area No. 1 Special Tax Revenue Bonds, Series

2022; Approving the Form and Substance of a Trust Agreement, Authorizing Modifications thereof and Execution and Delivery as Modified; Approving a Preliminary Official Statement, Authorizing Changes thereto and Execution and Delivery thereof and of an Official Statement to be Derived therefrom; Approving a Local Obligation Purchase Contract and a Bond Purchase Contract and Execution and Delivery of each; and Authorizing Related Actions Necessary to Implement the Proposed Financing

CFO/Finance Director Stacey Tamagni made a presentation.

Mayor Kerri Howell opened the public hearing. Hearing no public comments, the public hearing was closed.

Motion by Vice Mayor Rosario Rodriguez, second by Councilmember Mike Kozlowski to approve Resolution No. 10835.

- AYES: Councilmember(s): Kozlowski, Rodriguez, Aquino, Chalamcherla, Howell**
- NOES: Councilmember(s): None**
- ABSENT: Councilmember(s): None**
- ABSTAIN: Councilmember(s): None**

Motion by Vice Mayor Rosario Rodriguez, second by Councilmember Mike Kozlowski to approve Resolution No. 008-Folsom Ranch FA.

Motion carried with the following roll call vote:

- AYES: Board Member(s): Kozlowski, Rodriguez, Aquino, Chalamcherla, Howell**
- NOES: Board Member(s): None**
- ABSENT: Board Member(s): None**
- ABSTAIN: Board Member(s): None**

ADJOURNMENT

There being no further business to come before the joint City Council / Folsom Ranch Financing Authority, the meeting was adjourned to the regular City Council meeting at 7:33 pm.

RECONVENE CITY COUNCIL MEETING

OLD BUSINESS:

- 14. Resolution No. 10838 - A Resolution Authorizing the City Manager to Execute an Updated Fiber Network Installation Agreement with SiFi Networks Folsom LLC for the Installation of a Fiber Optic Network in the City of Folsom

Public Works Director Mark Rackovan made a presentation and responded to questions from the City Council with Scott Bradshaw from SiFi responding to additional questions from the City Council.

The following speakers addressed the City Council:

1. George Condon
2. Mike Reynolds

Motion by Vice Mayor Rosario Rodriguez, second by Councilmember YK Chalamcherla to approve Resolution No. 10838.

AYES: Councilmember(s): Kozlowski, Rodriguez, Chalamcherla, Howell
NOES: Councilmember(s): Aquino
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

15. Consideration of Expenditure of Funds from the American Rescue Plan Act and Direction to Staff

CFO/Finance Director Stacey Tamagni made a presentation and responded to questions from the City Council.

The following speakers addressed the City Council:

1. Michael Reynolds
2. Kathleen Cole
3. Jim Snook
4. Will Kempton
5. James Villa
6. Jerry Bernau

Motion by Councilmember Mike Kozlowski, second by Vice Mayor Rosario Rodriguez to approve staff's recommendation of the distribution of funds.

There was discussion by the City Council with clarification provided by City Manager Elaine Andersen.

AYES: Councilmember(s): Kozlowski, Rodriguez, Aquino, Howell
NOES: Councilmember(s): Chalamcherla
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

16. Resolution No. 10831 - A Resolution of the City Council of the City of Folsom Approving an Affordable Housing Grant in the Amount of \$588,265.55 to Bidwell Place, LP for Construction of the 75-unit Bidwell Place Affordable Multifamily Project, and Appropriation of Funds

Item was continued to the May 10th City Council meeting.

NEW BUSINESS:

- 17. Consideration of Letter in Response to Demand Letter Received from Scott Rafferty Regarding Alleged Non-Compliance with the Brown Act

City Attorney Steve Wang made a presentation.

Motion by Councilmember Sarah Aquino, second by Vice Mayor Rosario Rodriguez to send the letter in response and to continue following the Brown Act as the City Council always has.

AYES: Councilmember(s): Kozlowski, Rodriguez, Aquino, Chalamcherla, Howell
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

CITY MANAGER REPORTS:

City Manager Elaine Andersen spoke of upcoming events: the 26th annual Spring Eggstrvaganza and the annual Community Service Day. She congratulated the City's Hazardous Materials Division who were recognized as a top leader in sustainability efforts. She wished Fire Chief Ken Cusano a happy birthday.

CITY COUNCIL COMMENTS:

Vice Mayor Rosario Rodriguez commented regarding the recent Sacramento Transportation Authority Board meeting she attended, the upcoming spring and summer concerts at Zittel Amphitheater and she thanked dispatchers for their service for Telecommunicators Week.

Councilmember YK Chalamcherla commented regarding a recent trip to Washington DC and the Historic District Revitalization meeting he attended. He requested future updates about the Bidwell Studios project and commented regarding his discussion with Congressman Bera regarding water. He stated he would like to eventually see illuminated stop lights in Folsom.

Councilmember Sarah Aquino reported about the Air Quality Management District meeting she attended. She suggested that the City send a letter in support of State surplus funds for a public safety training facility in our region.

Councilmember Mike Kozlowski spoke of the SACOG retreat he attended, a new art gallery in the Historic District behind Snooks, and an upcoming high school track meet.

Mayor Kerri Howell commented about the Regional Transit meeting she attended. She talked about traffic in Folsom and encouraged everyone to drive safely.

ADJOURNMENT

There being no further business to come before the Folsom City Council, Mayor Kerri Howell adjourned the meeting at 8:55 pm.

SUBMITTED BY:

Christa Freemantle, City Clerk

ATTEST:

Kerri Howell, Mayor

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City Council Regular Meeting

MINUTES

Tuesday, April 26, 2022 6:30 PM

Mayor Kerri Howell announced that there was a technical glitch with Webex and those participating via Webex were not able to participate.

CALL TO ORDER

The regular City Council meeting was called to order at 6:33 pm with Mayor Kerri Howell presiding.

ROLL CALL:

Councilmembers Present: Rosario Rodriguez, Vice Mayor
Sarah Aquino, Councilmember
YK Chalamcherla, Councilmember
Mike Kozlowski, Councilmember
Kerri Howell, Mayor

Councilmembers Absent: None

Participating Staff: City Manager Elaine Andersen
Assistant City Attorney Sari Dierking
Deputy City Clerk Lydia Konopka
Police Lieutenant Brian Lockhart
Assistant Planner Josh Kincade
Community Development Director Pam Johns
Public Works Director Mark Rackovan
CFO/Finance Director Stacey Tamagni

PLEDGE OF ALLEGIANCE

The Pledge of Allegiance was recited.

AGENDA UPDATE

Mayor Howell announced that item 8 had additional information.

BUSINESS FROM THE FLOOR:

The following speaker addressed the City Council:

1. Jack Henry regarding Placerville and Sacramento Valley Railroad

SCHEDULED PRESENTATIONS:

1. Resolution of Commendation Honoring 2BGlass for Receiving the 2021 Folsom Arts Achievement Award

Mayor Kerri Howell presented the proclamation to the owners of 2BGlass: Tate Bezdek, Aaron Bezdek and Ash Koss.

2. Presentation by the Sacramento Yolo Mosquito and Vector Control District

This item was moved to a future meeting date due to the presenter not being able to phone-in because of a technical glitch with Webex.

CONSENT CALENDAR:

Items appearing on the Consent Calendar are considered routine and may be approved by one motion. City Councilmembers may pull an item for discussion.

3. Approval of April 12, 2022 Special and Regular Meeting Minutes
4. Resolution No. 10839 – A Resolution of the City Council Ratifying the City Manager’s Authorization for Emergency Replacement of the On-Site Fuel Tanks at the City of Folsom Corporation Yard, Determining the Project is Exempt from CEQA, and Appropriation of Funds
5. Resolution No. 10840 – A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 2 Village No. 3 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 2 Village No. 3 Subdivision
6. Resolution No. 10841 – A Resolution Authorizing the City Manager to Execute a Purchase Agreement with Golden State Fire Apparatus Inc. for a Ladder Truck from Pierce Manufacturing Inc., the Approval of an Interfund Loan and Appropriation of Funds

Motion by Vice Mayor Rosario Rodriguez Councilmember second by Mike Kozlowski, to approve the Consent Calendar.

Motion carried with the following roll call vote:

AYES: Councilmember(s): Rodriguez, Aquino, Chalamcherla, Kozlowski, Howell
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

NEW BUSINESS:

7. Ordinance No. 1326 - An Uncodified Ordinance of the City of Folsom Approving the Folsom Police Department's Military Equipment Use Policy in Compliance with Assembly Bill 481 (Introduction and First Reading)

Lieutenant Brian Lockhart made a presentation and responded to questions from the City Council.

Motion by Vice Mayor Rosario Rodriguez second by Councilmember YK Chalamcherla, to introduce and hold the first reading of Ordinance No. 1326.

Motion carried with the following roll call vote:

AYES: Councilmember(s): Rodriguez, Aquino, Chalamcherla, Kozlowski, Howell
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

Mayor Kerri Howell called for a 15-minute recess at 6:58 pm while staff attempted to connect with Webex. She readjourned the meeting at 7:13 pm and explained that staff had connected with Webex and provided the new meeting information for the public to connect.

Mayor Howell explained that the City Council would proceed with Public Hearing Item 8 but would also continue the item to the regular meeting on May 10th in order to ensure everyone unable to participate at this meeting would be able to at the next meeting.

PUBLIC HEARING:

8. Appeal by Igor Semenyuk of a Decision by the Historic District Commission Denying a Conditional Use Permit for the Lakeside Memorial Lawn Crematorium project (PN 19-182) located at 1201 Forrest Street

Mayor Kerri Howell opened the public hearing.

Assistant Planner Josh Kinkade made a presentation and responded to questions from the City Council. Consultant Lisa Westwood from ECORP Consulting provided information regarding the cultural resources study and Community Development Director Pam Johns provided additional clarification.

The following speakers addressed the City Council:

1. Laura Allbaugh
2. Anita Claney
3. Wendy Schneider
4. Megan McClure
5. Terry Sorensen
6. Dave Hibbins
7. Nancy Oldham

8. Sean Gates
9. Joy Hays
10. Mary Johnson
11. Deborah Grassl
12. Peter Lucyga
13. Helen Walsh
14. Lorin Claney
15. Steve Walsh
16. Andrey Semenyuk
17. Victoria Foster
18. Jennifer Lane
19. June Chan

Hearing no more comments the public hearing was closed.

Motion by Councilmember YK Chalamcherla, second by Vice Mayor Rosario Rodriguez to continue the item to the May 10, 2022 regular City Council meeting.

AYES: Councilmember(s): Rodriguez, Aquino, Chalamcherla, Kozlowski, Howell
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

CITY MANAGER REPORTS:

City Manager Elaine Andersen spoke of upcoming events: the Prescription Drug Takeback Day, the Sacramento Metropolitan Air Quality Management District open house, and the 9th Annual Community Service Day. She announced that the City is conducting a targeted multi-family and mixed-use housing study and is seeking public input.

CITY COUNCIL COMMENTS:

Councilmember Sarah Aquino commented regarding the Wye property and proposed a future agenda item at an upcoming City Council meeting.

Councilmember Mike Kozlowski spoke of the recent SACOG meeting he attended.

Councilmember YK Chalamcherla thanked staff and the appellant for their work on the Lakeside Memorial Lawn crematorium appeal.

Vice Mayor Rosario Rodriguez commented regarding status of hiring an executive director for the Sacramento Transportation Authority, attending the upcoming Cap to Cap trip and upcoming concerts at the Zittel Farm Amphitheater.

Mayor Kerri Howell commented about traffic in Folsom and encouraged everyone to drive safely. She spoke of a recent issue for Regional Transit and announced an upcoming meeting for Sacramento-Placerville Rail JPA.

ADJOURNMENT

There being no further business to come before the Folsom City Council, Mayor Kerri Howell adjourned the meeting at 9:16 pm.

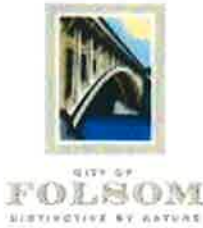
SUBMITTED BY:

Christa Freemantle, City Clerk

ATTEST:

Kerri Howell, Mayor

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Folsom City Council Staff Report



MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10839 – A Resolution of the City Council Ratifying the City Manager’s Authorization for Emergency Replacement of the On-Site Fuel Tanks at the City of Folsom Corporation Yard, Determining the Project is Exempt from CEQA, and Appropriation of Funds
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

The Public Works Department recommends that the City council pass and adopt Resolution No. 10839 – A Resolution of the City Council Ratifying the City Manager’s Authorization for Emergency Replacement of the On-Site Fuel Tanks at the City of Folsom Corporation Yard, Determining the Project is Exempt from CEQA, and Appropriation of Funds.

BACKGROUND / ISSUE

The current Corporation Yard fuel tanks are over 30 years old and have exceeded their useful life. The diesel tank has a slow leak and is unrepairable. Emergency operations planning typically recommends keeping two weeks’ worth of fuel in reserve; the diesel tank allows for less than 12 hours reserve, and the unleaded tank is approximately 75% undersized compared to current industry best practices.

POLICY / RULE

Folsom Municipal Code Section 2.36.150, Emergency Procurement, provides that the City Manager shall make, or authorize others to make, emergency procurement of supplies, equipment, services, or construction items when there exists a threat to public health, welfare, or safety, provided that such emergency procurement shall be made with sufficient competition as is practicable under the circumstances.

ANALYSIS

Current industry best practices and emergency operations planning guidelines recommend keeping a minimum two week fuel reserve. Obtaining the best pricing on fuel requires purchasing a full truck and tanker load (roughly 9,000 gallons) of product. Tanks may only be filled to 80% of their rated capacity to allow for product expansion due to thermal changes. These factors, combined with the City’s average fuel usage over the last five years, result in recommended tank sizes of 18,000 gallons for unleaded gasoline and 23,000 gallons for diesel fuel. Depending on the pricing of prefabricated tanks, purchasing slightly larger capacity tanks may be more cost effective.

The City Corporation Yard’s current fuel station consists of a 4,000 gallon unleaded gasoline tank and a 2,000 gallon diesel tank. Both tanks are repurposed, single-walled former underground storage tanks, mounted above ground, and co-located within a secondary containment structure along with a central tank product fill station and four product dispensing pumps.

Modern, purpose-built aboveground storage tanks are double-walled, with their required secondary containment built into the tank itself. Modern tanks typically do not require an additional secondary containment structure.

The current secondary containment structure and infrastructure can be reused with new equipment but will only accommodate one of the new tanks, most likely the unleaded gasoline tank. The new diesel tank will need to be installed at a different location within the Corporation Yard.

The City Manager authorized the emergency procurement for construction services under the existence of threat to public health, welfare, and safety, pursuant to Folsom Municipal Code Section 2.36.150, Emergency Procurement.

While not required by the Municipal Code, this matter is being presented to the City Council for the Council’s awareness and ratification. Staff is recommending that the City Council ratify the City Manager’s authorization for the emergency work and appropriation of funds with a determination that the current condition of the tanks poses an immediate threat to the public’s health, safety and welfare pursuant to Folsom Municipal Code Section 2.36.150. Staff is recommending that this emergency procurement be used to select a contractor through a competitive bidding process.

FINANCIAL IMPACT

The emergency procurement was not anticipated in the FY 2021-22 General Fund (Fund 010) Operating Budget, therefore staff is requesting an additional appropriation in the General Fund in an amount not-to-exceed \$1,000,000. The additional appropriation will come from the General Funds current fund balance, which is currently available for this procurement.

Given the immediate lower per-gallon cost of fuel that will be purchased less frequently but in greater quantities, the projected return on investment is less than 10 years.

No significant revenue is anticipated from the sale of the existing equipment.

ENVIRONMENTAL REVIEW

This emergency replacement project is exempt from the requirements of the California Environmental Quality Act per Section 15269(b) and (c), Emergency Projects.

ATTACHMENT

Resolution No 10839 - A Resolution of the City Council Ratifying the City Manager's Authorization for Emergency Replacement of the On-Site Fuel Tanks at the City Corporation Yard, Determining the Project is Exempt from CEQA, and Appropriation of Funds

Submitted,



Mark Rackovan, PUBLIC WORKS DIRECTOR

RESOLUTION NO. 10839

A RESOLUTION OF THE CITY COUNCIL RATIFYING THE CITY MANAGER’S AUTHORIZATION FOR EMERGENCY REPLACEMENT OF THE ON-SITE FUEL TANKS AT THE CITY OF FOLSOM CORPORATION YARD, DETERMINING THE PROJECT IS EXEMPT FROM CEQA, AND APPROPRIATION OF FUNDS

WHEREAS, the Corporation Yard fuel tanks are over 30 years old and have exceed their useful life; and

WHEREAS, the diesel fuel tank located at the Corporation Yard has an unreparable slow leak; and

WHEREAS, both of the fuel storage tanks at the Corporation Yard are undersized compared to current industry best practices and emergency operations planning guidelines; and

WHEREAS, the City Manager made a determination for emergency procurement for construction services under the existence of threat to public health, welfare and safety, pursuant to Folsom Municipal Code Section 2.36.150, Emergency Procurement; and

WHEREAS, staff will conduct a competitive bidding process prior to awarding the contract for this project; and

WHEREAS, the project is exempt from CEQA; and

WHEREAS, sufficient funds are available in the General Fund (Fund 010); and

WHEREAS, the agreement will be in a form acceptable to the City Attorney.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom hereby ratify the City Manager’s emergency procurement of construction services for the emergency replacement of the Corporation Yard fuel tanks.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Finance Director is authorized to appropriate the amount bid by the successful contractor in the competitive bidding process, not to exceed \$1,000,000 from the General Fund (Fund 010), from current available fund balance for the Corporation Yard fuel tank emergency replacement procurement.

PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

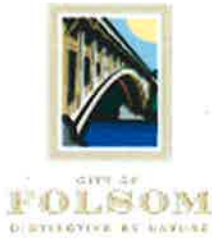
- AYES:** Council Member(s):
- NOES:** Council Member(s):
- ABSENT:** Council Member(s):
- ABSTAIN:** Council Member(s):

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

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Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10840 – A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 2 Village No. 3 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 2 Village No. 3 Subdivision
FROM:	Community Development Department

RECOMMENDATION / CITY COUNCIL ACTION

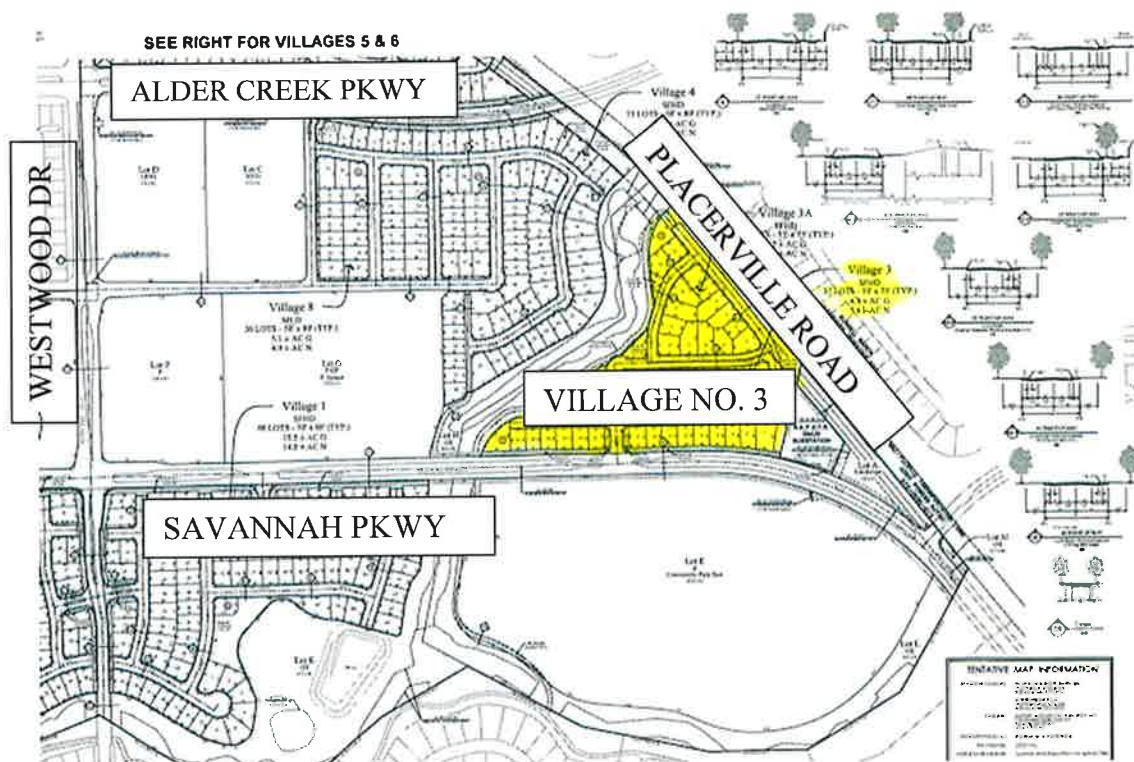
Staff recommends that the City Council move to adopt:

Resolution No. 10840 – A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 2 Village No. 3 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 2 Village No. 3 Subdivision.

BACKGROUND / ISSUE

The Vesting Tentative Subdivision Map (VTSM) for the Mangini Ranch Phase 2 Village No. 3 Subdivision was approved by the City Council on February 13, 2018.

The action for consideration by the City Council is the approval of the Final Map and Subdivision Improvement Agreement for the Mangini Ranch Phase 2 Village No. 3 Subdivision. The Final Map for the Mangini Ranch Phase 2 Village No. 3 Subdivision will create a total of 53 single-family high density (SFHD) residential lots. With the approval of the Final Map, the subdivision process for this project will be complete.



The Mangini Ranch Phase 2 Village No. 3 Subdivision is located on the north side of Savannah Parkway east of Westwood Drive and west of Placerville Road in the Folsom Plan Area (FPA) (see above).

POLICY / RULE

The Subdivision Map Act of the State of California and the City’s Subdivision Ordinance require that the City Council approve Final Maps and Subdivision Improvement Agreements.

ANALYSIS

The Final Map and conditions of approval for the Mangini Ranch Phase 2 Village No. 3 Subdivision have been reviewed by the Community Development Department and other City departments. The Final Map has been found to be in substantial compliance with the approved Vesting Tentative Subdivision Map, and all conditions pertaining to the map have been satisfied.

Attached is a table which includes the conditions of approval for the Mangini Ranch Phase 2 Village No. 3 Vesting Tentative Subdivision Map. The tables include information concerning when the condition is required to be satisfied (e.g. at Final Map, building permit, etc.), which City department is responsible to verify that it has been satisfied, and comments or an explanation on how the condition was satisfied. This subdivision is consistent with the Folsom Plan Area Specific Plan (FPASP) in regards to zoning and unit count.

ENVIRONMENTAL REVIEW

On February 13, 2018, the City Council approved the Mangini Ranch Phase 2 Vesting Tentative Subdivision Map and determined that the Mangini Ranch Phase 2 Subdivision project is entirely consistent with the Folsom Plan Area Specific Plan (FPASP) and Westland Eagle Specific Plan Amendment and therefore exempt from review under the California Environmental Quality Act (CEQA) provided by Government Code section 65457 and CEQA Guidelines sections 15182. No additional environmental review is required.

ATTACHMENTS

1. Resolution No. 10840 - A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 2 Village No. 3 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 2 Village No. 3 Subdivision
2. Mangini Ranch Phase 2 Village No. 3 Subdivision Improvement Agreement
3. Mangini Ranch Phase 2 Village No. 3 Subdivision Final Map
4. Mangini Ranch Phase 2 Village No. 3 Vesting Tentative Subdivision Map
5. Table of Conditions of Approval for the Mangini Ranch Phase 2 Village No. 3 Vesting Tentative Subdivision Map

Submitted,



PAM JOHNS
Community Development Director

ATTACHMENT 1

RESOLUTION NO. 10840 - A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE A SUBDIVISION IMPROVEMENT AGREEMENT AND ACCEPT OFFERS OF DEDICATION FOR THE MANGINI RANCH PHASE 2 VILLAGE NO. 3 SUBDIVISION, AND APPROVAL OF THE FINAL MAP FOR THE MANGINI RANCH PHASE 2 VILLAGE NO. 3 SUBDIVISION

RESOLUTION NO. 10840

A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE A SUBDIVISION IMPROVEMENT AGREEMENT AND ACCEPT OFFERS OF DEDICATION FOR THE MANGINI RANCH PHASE 2 VILLAGE NO. 3 SUBDIVISION, AND APPROVAL OF THE FINAL MAP FOR THE MANGINI RANCH PHASE 2 VILLAGE NO. 3 SUBDIVISION

WHEREAS, the Final Map for the Mangini Ranch Phase 2 Village No. 3 subdivision has been reviewed and approved by the City Engineer as complying with the approved or conditionally approved Vesting Tentative Subdivision Map for the subdivision; and

WHEREAS, the City Council has reviewed the Final Map for the Mangini Ranch Phase 2 Village No. 3 subdivision; and

WHEREAS, the City Council agrees to accept, subject to improvement, any and all offers of dedication as shown on the Final Map for the Mangini Ranch Phase 2 Village No. 3 subdivision.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Folsom that the Final Map for the Mangini Ranch Phase 2 Village No. 3 subdivision is hereby approved.

BE IT FURTHER RESOLVED that the City Manager is hereby authorized to execute the Subdivision Improvement Agreement with Beazer Home Holdings, L.L.C. in a form acceptable to the City Attorney and accept the offers of dedication for the Mangini Ranch Phase 2 Village No. 3 subdivision.

PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

AYES: Councilmember(s)

NOES: Councilmember(s)

ABSENT: Councilmember(s)

ABSTAIN: Councilmember(s)

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

ATTACHMENT 2

MANGINI RANCH PHASE 2 VILLAGE NO. 3 SUBDIVISION IMPROVEMENT AGREEMENT

No Fee Document Pursuant to Government Code Section 6103.

RECORDING REQUESTED BY:

City of Folsom

WHEN RECORDED MAIL TO:

NAME City of Folsom
City Clerk
MAILING ADDRESS 50 Natoma Street
CITY, STATE, ZIP CODE Folsom, CA 95630

SPACE ABOVE THIS LINE RESERVED FOR RECORDER'S USE

CITY OF FOLSOM

SUBDIVISION IMPROVEMENT AGREEMENT

This Agreement is made and entered into this _____ day of _____, 2022, by and between the City of Folsom, hereinafter referred to as "City", and Beazer Homes Holdings, L.L.C., a Delaware Limited Liability Company hereinafter referred to as "Subdivider".

RECITALS

- A. Subdivider has presented to the City a certain Final Map of a proposed subdivision of land located within the corporate limits of the City that has been prepared in accordance with the Subdivision Map Act of the State of California, the subdivision ordinances of the City, and the Tentative Subdivision Map, if any, of the subdivision previously approved by the City Council of the City.
- B. The proposed subdivision of land is commonly known and described as Mangini Ranch Phase 2 Village. No. 3, and is herein referred to as the "subdivision".
- C. Subdivider has requested approval of the Final Map prior to the construction and completion of the public improvements (as shown on the approved improvement plans and listed in Exhibit A), including, but not limited to streets, highways, public ways, sidewalks, curbs, gutters, bikeways, storm drainage facilities, sanitary sewer facilities, domestic water facilities, public utility facilities, landscaping, public lighting facilities, park or recreational improvements and appurtenances thereto, in or required by the Subdivision Map Act, the subdivision ordinances of the City, the Tentative Subdivision Map and development agreement, if any, approved by the City. The foregoing improvements, more specifically listed on Exhibit A attached hereto, are hereinafter referred to as "the required improvements".

D. City Council has required as a condition precedent to the approval of the Final Map, the Subdivider first enters into and executes this subdivision improvement agreement with the City.

NOW, THEREFORE, the parties agree as follows:

1. Performance of Work. Subdivider agrees to furnish, construct, and install at his own expense the required improvements as shown on the approved plans and specifications of the subdivision, a copy of which is on file in the Community Development Department, and is incorporated herein by reference, along with any changes or modifications as may be required by the City Engineer due to errors, omissions, changes in conditions, or changes in facilities as required by the City Engineer. The approved plans and specifications of the required improvements may be modified by the Subdivider as the development progresses, provided that any modification is approved in writing by the City Engineer. The total estimated cost of the required improvements, as shown on Exhibit A, is **ONE MILLION FIVE HUNDRED FIFTY-SIX THOUSAND ONE HUNDRED THIRTY-TWO AND 00/100 DOLLARS (\$1,556,132.00).**
2. Work; Satisfaction of City Engineer. All of the work on the required improvements is to be done at the places, of the materials, and in the manner and at the grades, all as shown upon the approved plans and specifications and as required by the City's Improvement Standards and Standard Construction Specifications and any applicable City ordinances or state and federal laws, and to the satisfaction of the City Engineer.
3. Work; Time for Commencement and Performance. Work on the required improvements shall be completed by the Subdivider on or before twelve (12) months from the date of this Agreement. At least fifteen (15) calendar days prior to the commencement of such work, the Subdivider shall notify the City Engineer in writing of the date fixed by Subdivider for commencement of the work.
4. Time of Essence; Extension.
 - a. Time is of the essence of this Agreement. The date for completion of the work of construction may not be extended, except as provided in Section 16.36.110 of the Folsom Municipal Code.
5. Improvement Security. Concurrently with the execution of this Agreement, the Subdivider shall furnish the City:
 - a. Improvement security in the sum of **ONE MILLION FIVE HUNDRED FIFTY-SIX THOUSAND ONE HUNDRED THIRTY-TWO AND 00/100 DOLLARS (\$1,556,132.00)**, which sum is equal to one hundred percent of the total estimated cost of constructing the required improvements and the cost of any other obligation to be performed by Subdivider under this Agreement, conditioned upon the faithful performance of this Agreement; and

- b. Separate improvement security in the sum of **ONE MILLION FIVE HUNDRED FIFTY-SIX THOUSAND ONE HUNDRED THIRTY-TWO AND 00/100 DOLLARS (\$1,556,132.00)**, which sum is equal to one hundred percent of the estimated cost of constructing the required improvements, securing payment to the contractor, subcontractor and to persons furnishing labor, materials, or equipment to them for the construction of the required improvements.
- c. The Subdivider shall deposit with the City **THREE THOUSAND AND NO/100 DOLLARS (\$3,000.00)** for the Final Map. The deposit may be used at the discretion of the City to correct deficiencies and conditions caused by the Subdivider, contractor, or subcontractors that may arise during or after the construction of the subdivision.
- d. The estimated total cost of required improvements includes a ten percent (10%) construction cost contingency, the cost of the installation of survey monuments in the Subdivision to guarantee and secure the placement of such monuments as provided by Section 66496 of the Government Code of the State of California, and an estimated utility cost in addition to ensure installation of public utilities. In lieu of providing the estimate of total utility costs, the Subdivider may submit, in a form acceptable to the City Engineer, certification from the utility companies that adequate security has been deposited to ensure installation.
6. Plan Checking and Inspection Fees. The Subdivider shall pay to the City fees for the checking, filing, and processing of improvement plans and specifications, and for inspecting the construction of the required improvements in the amounts and at the times established by the City.
7. Indemnification and Hold Harmless. The Subdivider shall indemnify, protect, defend, save and hold the City harmless from any and all claims or causes of action for death or injury to persons, or damage to property resulting from intentional or negligent acts, errors, or omissions of Subdivider or Subdivider's officers, employees, volunteers, and agents during performance of this Agreement, or in connection with Subdivider's work, or from any violation of any federal, state, or municipal law or ordinance, to the extent caused, in whole or in part, by the willful misconduct, negligent acts, or omissions of Subdivider or its employees, subcontractors, or agents, or by the quality or character of Subdivider's work. It is understood that the duty of Subdivider to indemnify and hold harmless includes the duty to defend as set forth in Section 2778 of the California Civil Code. Acceptance by City of insurance certificates and endorsements required under this Agreement does not relieve Subdivider from liability under this indemnification and hold harmless clause. This indemnification and hold harmless clause shall apply to any damages or claims for damages whether or not such insurance policies shall have been determined to apply, and shall further survive the expiration or termination of this Agreement. By execution of this Agreement, Subdivider acknowledges and agrees to the provisions of this Section and that it is a material element of consideration. Subdivider shall, at his own cost and expense, defend any and all actions, suits, or legal proceedings that may be brought or instituted against the City, its officers and employees, on any such claim or demand, and pay or satisfy any judgement that may be

rendered against the City in any such actions, suits or legal proceedings, or result thereof.

8. Insurance. Subdivider and any contractors hired by Subdivider to perform any of the Required Improvements shall, at their expense, maintain in effect for the duration of this Agreement or until the required improvements are accepted by the City, whichever first occurs, not less than the following coverage and limits of insurance, which shall be maintained with insurers and under forms of policy satisfactory to the City. The maintenance by Subdivider and its contractors of the following coverage and limits of insurance is a material element of this Agreement. The failure of Subdivider or any of its contractors to maintain or renew coverage or to provide evidence of renewal may be treated by the City as a material breach of this Agreement.

a. Minimum Limits of Insurance. Subdivider shall maintain limits not less than:

- 1. Comprehensive General Liability: \$1,000,000 combined single limit per occurrence for bodily injury, personal injury and property damage.
- 2. Automobile Liability: \$1,000,000 combined single limit per accident for bodily injury, personal injury and property damage.
- 3. Worker’s Compensation and Employers Liability: Worker’s Compensation limits as required by the Labor Code of the State of California and Employers Liability limits of \$1,000,000 per accident.

b. Deductibles and Self-Insured Retentions. Any deductibles or self-insured retentions shall be declared to and approved by the City. At the option of the City, either: the insurer shall reduce or eliminate such deductibles or self-insured retentions as respects to a bond guaranteeing payment of losses and related investigations, claim administration and defense expenses.

c. Other Insurance Provisions. The policies are to contain, or be endorsed to contain, the following provisions:

1. General Liability and Automobile Liability Coverages

- A. The City, its officers, officials, employees and volunteers are to be covered as insureds as respects: liability arising out of activities performed by or on behalf of the Subdivider; products and completed operations of the Subdivider; premises owned, leased or used by the Subdivider; or automobiles owned, leased, hired or borrowed by the Subdivider. The coverage shall contain no special limitations on the scope of protection afforded to the City, its officers, officials, employees or volunteers.
- B. The Subdivider’s insurance coverage shall be primary insurance

as respects the City, its officers, officials, employees and volunteers. Any insurance of self-insurance maintained by the City, its officials, employees or volunteers shall be excess of the Subdivider's insurance and shall not contribute with it.

- C. Any failure to comply with reporting provisions of the policies shall not affect coverage provided to the City, its officers, officials, employees or volunteers.
 - D. The Subdivider's insurance shall apply separately to each insured against whom claim is made or suit is brought, except with respect to the limits of the insurer's liability.
2. Worker's Compensation and Employers Liability Coverage. The insurer shall agree to waive all rights or subrogation against the City, its officers, officials, employees and volunteers for losses arising from work performed by Subdivider for the City.
 3. All Coverages. Each insurance policy required by this clause shall be endorsed to state that coverage shall not be suspended, voided or cancelled by either party, reduced in coverage or in limits except after thirty (30) days prior written notice by certified mail, return receipt requested, has been given to the City.
- d. Acceptability of Insurers. Insurance is to be placed with insurers with a Best's rating of not less than A: VII.
 - e. Verification of Coverage. Concurrently with the execution of this Agreement, the Subdivider shall furnish the City with original endorsements affecting coverage required by this clause. The endorsements for each insurance policy are to be signed by a person authorized by that insurer to bind coverage on its behalf. The City reserves the right to require complete, certified copies of all required insurance policies at any time.
9. Title to Improvements. Title to and ownership of the required public improvements constructed under this Agreement by Subdivider shall vest absolutely in the City upon completion and written acceptance of such improvements by the City Engineer. The City Engineer shall not accept the required improvements unless Subdivider certifies that such improvements have been constructed in conformity with the approved plans and specifications, approved modifications, if any, the approved Final Map, City Improvement Standards and Standard Construction Specifications, any applicable City Ordinances or State and Federal laws and after 35 days from the date of filing of a Notice of Completion.
 10. Warranty Security. Prior to acceptance of the required improvements by the City Engineer, the Subdivider shall provide security in the amount and in the form as required by the City Engineer to guarantee the improvements against any defective work or labor done or defective materials used in the performance of the required improvements (Warranty Security) throughout the warranty security period which shall be the period of one year following

completion and written acceptance of the improvements (Warranty Security Period). The amount of the Warranty Security shall not be less than 10 percent of the cost of the construction of the improvements, including the cash deposit required in paragraph 5C of this agreement, which shall be retained for the Warranty Security Period.

11. Repair or Reconstruction of Defective Work or Materials. If, within the Warranty Security Period or the applicable statute of limitations, whichever is longer, any improvement or part of any improvement furnished and/or installed or constructed by Subdivider or any of the work done under this Agreement fails to fulfill any of the requirements of the Agreement or the specifications referred to herein as determined by the City, Subdivider shall without delay and without any cost to the City, repair, replace, or reconstruct any defective or otherwise unsatisfactory part or parts of the required improvements. If the Subdivider fails to act promptly or in accordance with this requirement, or if the exigencies of the situation require repairs or replacements to be made before the Subdivider can be notified, then the City may, at its option, make the necessary repairs or replacements or perform the necessary work, and Subdivider shall pay to City the actual cost of such repairs plus fifteen percent (15%) within thirty (30) days of the date of billing for such work by City. The parties further understand and agree that the Warranty Security furnished pursuant to paragraph 10 of this Agreement shall guarantee and secure the faithful performance and payment of the provisions of this paragraph during the Warranty Security Period.
12. Subdivider Not Agent of City. Neither Subdivider nor any of Subdivider's agents or contractors are or shall be considered to be agents of City in connection with the performance of Subdivider's obligations under this Agreement.
13. Notice of Breach and Default. If Subdivider refuses or fails to prosecute the work, or any part thereof, with such diligence as will ensure its completion within the time specified, or any extension thereof, or fails to complete the work within such time, or if Subdivider should be adjudged a bankruptcy, or Subdivider should make a general assignment for the benefit of his creditors, or if a receiver should be appointed in the event of Subdivider's insolvency, or if Subdivider or any of Subdivider's contractors, subcontractors, agents or employees should violate any of the provisions of this Agreement and the City may, but is under no obligation to, serve written notice upon Subdivider and Subdivider's surety, if any, of breach of this Agreement, or of any portion thereof.
14. Breach of Agreement; Performance By Surety or City. In the event of any such notice, Subdivider's surety, if any, shall have the duty to take over and complete the work and the required improvements; provided, however, that if the surety within fifteen (15) days after the serving of such notice of breach upon it does not give the City written notice of its intention to take over the performance thereof within fifteen (15) days after notice to the City of such election, then the City may take over the work and prosecute the same to completion by contract, or by any other method the City may deem advisable, for the account and at the expense of the Subdivider, and the Subdivider's surety shall be liable to City for any excess costs of damages incurred by the City; and in such event, the City, without liability for so doing, may take possession of and utilize in completing the work, such materials, appliances, plant or other property belonging to Subdivider as may be on the site of the work and necessary therefor.

If the form of improvement security is other than a bond, then the City, after giving notice of breach of the Agreement, may proceed to collect against the improvement security in the manner provided by law and by the terms of the security instrument.

- 15. Notices. All notices required under this Agreement shall be in writing, and delivered in person or sent by registered or certified mail, postage prepaid.

Notices required to be given to City shall be addressed as follows:

**City of Folsom
Community Development Department
50 Natoma Street
Folsom, CA 95630
ATTN: City Engineer**

Notices required to be given to Subdivider shall be addressed as follows:

**Beazer Homes Holdings, L.L.C.
12317 Edyth Lake Way
Rancho Cordova, CA 95742
ATTN; Lucas C. Wissmann, President & Taylor R. Bollinger, VP**

Notices required to be given surety, if any, of Subdivider shall be addressed as follows:

Any party of the surety may change such address by notice in writing to the other party and thereafter notices shall be addressed and transmitted to the new address.

- 16. Attorney's Fees. In the event any legal action is brought to enforce or interpret this Agreement, the prevailing party shall be entitled to an award of reasonable attorney's fees, in addition to any other relief to which he may be entitled.
- 17. Assignment. This Agreement shall bind and inure to the benefit of the assigns, successors in interest, heirs, executors, and administrators of the parties, and the parties agree that the City may cause a copy of this Agreement to be recorded in the Sacramento County Recorder's Office.

IN WITNESS WHEREOF, the parties have executed this Agreement as follows:

SUBDIVIDER

**Beazer Home Holdings, L.L.C.,
A Delaware Limited Liability Company**

BY: _____ BY: _____

Print Name: _____ Print Name: _____

Title: _____ Title: _____

DATE _____ DATE _____

CITY OF FOLSOM, a Municipal Corporation

Elaine Andersen
CITY MANAGER
DATE _____

ATTEST:

Christa Freemantle
CITY CLERK
DATE _____

APPROVED AS TO CONTENT:

Pam Johns
COMMUNITY DEVELOPMENT DIRECTOR
DATE _____

APPROVED AS TO FORM:

Steven Wang
CITY ATTORNEY
DATE _____

NOTICE: SIGNATURE(S) ON BEHALF OF "SUBDIVIDER" MUST BE NOTARIZED
Certificate of Acknowledgement pursuant to Civil Code, Section 1189, must be attached.
SUBDIVISION AGREEMENT – Mangini Ranch Phase 2 Village No. 3

BOND # _____
PREMIUM: _____

PERFORMANCE BOND
for
Subdivision Improvement Agreement

WHEREAS, The City Council of the City of Folsom, a Municipal Corporation in the State of California, and **Beazer Home Holdings, L.L.C., a Delaware Limited Liability Company**, (hereinafter designated as “Principal”) have entered into an agreement where by principal agrees to install and complete certain designated public improvements, which said agreement, dated _____ 2022, and identified as the **Mangini Ranch Phase 2 Village No. 3 Subdivision Improvement Agreement** is hereby referred to and made a part hereof; and,

WHEREAS, Said Principal is required under the terms of said agreement to furnish a bond for the faithful performance of said agreement;

NOW THEREFORE, We, the principal, and _____, as surety, are held and firmly bound unto the City of Folsom, hereinafter referred to as the City; in the penal sum of **ONE MILLION FIVE HUNDRED FIFTY-SIX THOUSAND ONE HUNDRED THIRTY-TWO AND 00/100 DOLLARS (\$1,556,132.00)**, lawful money of the United States, for the payment of which sum well and truly to be made, we bind ourselves, our heirs, successors, executors, and administrators, jointly and severally firmly by these presents.

The condition of this obligation is such that if the above bounded principal, its heirs, executors, administrators, successors or assigns, shall in all things stand to and abide by, and well and truly keep and perform the covenants, conditions, and provisions in the said agreement and any alteration thereof made as therein provided, on its part, to be kept and performed at the time and in the manner therein specified, and in all respects according to their true intent and meaning, and shall indemnify and save harmless the City, its officers, agents, and employees, as therein stipulated, then this obligation shall become null and void; otherwise it shall be and remain in full force and effect.

As a part of the obligation secured hereby and in addition to the face amount specified therefor, there shall be included costs and reasonable expenses and fees, including reasonable attorney’s fees, incurred by City in successful enforcing such obligation, all to be taxed as costs and included in any judgment rendered.

The surety hereby stipulates and agrees that no change, extension of time, alteration or addition to the terms of the agreement or to the work to be performed thereunder or the specifications accompanying the same shall in any way affect its obligations on this bond, and it does hereby waive notice of any such change, extension of time, alteration or addition to the terms of the agreement or to the work or to the specifications.

IN WITNESS WHEREOF, this instrument has been duly executed by the principal and surety above named, on _____, 2022.

BY _____
(PRINCIPAL)

BY _____
(PRINCIPAL)

BY _____
(SURETY)

(ADDRESS)

(CITY, STATE, ZIP)

(TELEPHONE)

APPROVED AS TO FORM

CITY ATTORNEY

BOND # _____
PREMIUM: _____

LABOR & MATERIALS BOND
for
Subdivision Improvement Agreement

WHEREAS, The City Council of the City of Folsom, a Municipal Corporation of the State of California, and **Beazer Home Holdings, L.L.C., a Delaware Limited Liability Company** (hereinafter designated as "Principal"), have entered into an agreement whereby principal agrees to install and complete certain designated public improvements, which said agreement, dated _____ 2022, and identified as the **Mangini Ranch Phase 2 Village No. 3 Subdivision Improvement Agreement** is hereby referred to and made a part hereof; and,

WHEREAS, under the terms of said agreement, principal is required before entering upon the performance of the work, to file a good and sufficient payment bond with the City of Folsom to secure the claims to which reference is made in Title 15 (commencing with Section 3082) of Part 4 of Division 3 of the Civil Code of the State of California;

NOW THEREFORE, said principal and the undersigned as corporate surety, are held firmly bound unto the City of Folsom and all contractors, subcontractors, laborers, materialmen and other persons employed in the performance of the aforesaid agreement and referred to in the aforesaid Code of Civil Procedure, in the sum of **ONE MILLION FIVE HUNDRED FIFTY-SIX THOUSAND ONE HUNDRED THIRTY-TWO AND 00/100 DOLLARS (\$1,556,132.00)** for materials furnished or labor thereon of any kind, or for amounts due under the Unemployment Insurance Act with respect to such work or labor, that said surety will pay the same in an amount not exceeding the amount hereinabove set forth, and also in case suit is brought upon this bond, will pay, in addition to the face amount thereof, cost and reasonable expenses and fees, including reasonable attorney's fees, incurred by City in successfully enforcing such obligation, to be awarded and fixed by the court, and to be taxed as costs and to be included in the judgment therein rendered.

It is hereby expressly stipulated and agreed that this bond shall inure to the benefit of any and all persona, companies and corporations entitled to file claims under Title 15 (commencing with Section 3082) of Part 4 of Division 3 of the Civil Code, so as to give a right of action to them or their assigns in any suit brought upon this bond.

Should the condition of this bond be fully performed, then this obligation shall become null and void, otherwise it shall be and remain in full force and effect.

The surety hereby stipulates and agrees that no change, extension of time, alteration, or addition to the terms of said agreement or the specifications accompanying the same shall in any manner affect its obligations on this bond, and it does hereby waive notice of any such change, extension, alteration, or addition.

IN WITNESS WHEREOF, this instrument has been duly executed by the principal and surety above named, on _____, 2022.

BY _____
(PRINCIPAL)

BY _____
(PRINCIPAL)

BY _____
(SURETY)

(ADDRESS)

(CITY, STATE, ZIP)

(TELEPHONE)

APPROVED AS TO FORM

CITY ATTORNEY

ATTACHMENT 3

MANGINI RANCH PHASE 2 VILLAGE NO. 3 FINAL MAP

OWNER'S STATEMENT

THE UNDERSIGNED DOES HEREBY STATE THAT WE ARE THE ONLY PARTIES HAVING ANY RECORD TITLE INTEREST IN THE REAL PROPERTY INCLUDED WITHIN THE BOUNDARIES OF THIS FINAL MAP OF 'MANGINI RANCH PHASE 2 - VILLAGE 3' AND DO HEREBY DECLARE THE CONSENT FROM NO OTHER PERSON IS NECESSARY, AND WE CONSENT TO THE PREPARATION AND RECORDATION OF THIS FINAL MAP, AND OFFER FOR DEDICATION AND DO HEREBY DEDICATE AS PUBLIC RIGHT-OF-WAY AND AS PUBLIC UTILITY EASEMENTS TO THE CITY OF FOLSOM, SAVANNAH PARKWAY, BITTERTROOT WAY, DANDELION WAY, PERSIMMON WAY AND SNAPDRAGON WAY AS SHOWN HEREON.

WE DO HEREBY DEDICATE FOR SPECIFIC PURPOSES THE FOLLOWING:

1. A PUBLIC EASEMENT FOR THE INSTALLATION AND MAINTENANCE OF DRAIN, GAS, SEWER AND WATER PIPES, AND FOR UNDERGROUND WIRES AND CONDUITS FOR ELECTRICAL, TELEVISION AND COMMUNICATIONS SERVICES, TOGETHER WITH ANY AND ALL APPURTENANCES PERTAINING THERETO ON, OVER, UNDER AND ACROSS LOTS A, B, C, D, AND E AND THOSE STRIPS OF LAND SHOWN HEREON AND DESIGNATED "PUBLIC UTILITY EASEMENT" (P.U.E.).
2. A PUBLIC EASEMENT FOR PEDESTRIAN ACCESS ON, OVER AND ACROSS THOSE STRIPS OF LAND SHOWN HEREON AND DESIGNATED "PEDESTRIAN ACCESS EASEMENT" (P.A.E.).
3. A PUBLIC EASEMENT AND RIGHT-OF-WAY FOR THE INSTALLATION, REPAIR, REMOVAL OR REPLACEMENT OF LANDSCAPING TOGETHER WITH ANY AND ALL APPURTENANCES PERTAINING THERETO ON, OVER, UNDER, ACROSS AND ABOVE THOSE STRIPS OF LANDS SHOWN HEREON AND DESIGNATED "LANDSCAPE EASEMENT" (L.E.).
4. A PUBLIC EASEMENT FOR CONSTRUCTION AND MAINTAINING CENTRALIZED MAIL DELIVERY BOXES, PEDESTALS AND SLABS, TOGETHER WITH ANY AND ALL APPURTENANCES PERTAINING THERETO INCLUDING PEDESTRIAN ACCESS FOR DELIVERY AND RECEIPT OF MAIL ON, OVER, AND ACROSS STRIPS OF LAND FIVE (5) FEET IN WIDTH CONTIGUOUS TO ALL RIGHT-OF-WAYS.
5. AN EASEMENT FOR INGRESS AND EGRESS FOR SUPPORT AND USE BY LAW ENFORCEMENT, FIRE PROTECTION, WELFARE AND OTHER PUBLIC AGENCIES, THEIR VEHICLES AND PERSONNEL OVER LOT C SHOWN HEREON AND DESIGNATED "EMERGENCY VEHICLE ACCESS EASEMENT" (E.V.A.).

BEAZER HOMES HOLDINGS, LLC,
A DELAWARE LIMITED LIABILITY COMPANY

BY: _____ BY: _____
NAME: TAYLOR R. BOLLINGER NAME: LUCAS C. WISSMANN
TITLE: VP DIV LA & FORWARD PLANNING TITLE: PRESIDENT DIVISIONAL
- NORTHERN CALIFORNIA - NORTHERN CALIFORNIA

NOTARY'S ACKNOWLEDGMENT

A NOTARY PUBLIC OR OTHER OFFICER COMPLETING THIS CERTIFICATE VERIFIES ONLY THE IDENTITY OF THE INDIVIDUAL WHO SIGNED THE DOCUMENT TO WHICH THIS CERTIFICATE IS ATTACHED, AND NOT THE TRUTHFULNESS, ACCURACY OR VALIDITY OF THAT DOCUMENT.

STATE OF CALIFORNIA
COUNTY OF _____
ON _____ BEFORE ME, _____, A NOTARY PUBLIC

PERSONALLY APPEARED
WHO PROVED TO ME ON THE BASIS OF SATISFACTORY EVIDENCE TO BE THE PERSON(S) WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE WITHIN INSTRUMENT AND ACKNOWLEDGED TO ME THAT HE/SHE/THEY EXECUTED THE SAME IN HIS/HER/THEIR AUTHORIZED CAPACITY(IES), AND THAT BY HIS/HER/THEIR SIGNATURE(S) ON THE INSTRUMENT THE PERSON(S), OR THE ENTITY UPON BEHALF OF WHICH THE PERSON(S) ACTED, EXECUTED THE INSTRUMENT.

I CERTIFY UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE STATE OF CALIFORNIA THAT THE FOREGOING PARAGRAPH IS TRUE AND CORRECT.

WITNESS MY HAND AND OFFICIAL SEAL.

SIGNATURE _____ PRINTED NAME _____

MY PRINCIPAL PLACE OF BUSINESS IS _____ COUNTY

MY COMMISSION EXPIRES: _____ MY COMMISSION NUMBER: _____



VICINITY MAP
N.T.S.

NAVD88 BENCHMARK-CITY OF FOLSOM

BENCHMARK '72' ELEVATION = 502.07 NAVD88

BRASS DISK STAMPED 'CITY OF FOLSOM BM 72' ON THE WEST CORNER OF A CONCRETE DRAINAGE STRUCTURE. LOCATION OF SITE IS APPROXIMATELY 350 FEET SOUTH OF ALDER CREEK PARKWAY AND 80 FEET EAST OF THE SPTC/PA RAILROAD TRACKS. APPROXIMATE LATITUDE: N38D 38' 13.33" LONGITUDE: W121D 06' 08.22"

THE BASIS FOR ELEVATIONS WERE RUN FROM COUNTY BENCHMARK U01B-009 STAMPED 'K-856' IN FEBRUARY 2014 BY MACKAY AND SOMPS CIVIL ENGINEERS, INC.

Preliminary
04/13/2022 7:31:02 AM

SURVEYOR'S STATEMENT

THIS MAP WAS PREPARED BY ME OR UNDER MY DIRECTION AND IS BASED UPON A FIELD SURVEY IN CONFORMANCE WITH THE REQUIREMENTS OF THE SUBDIVISION MAP ACT AND LOCAL ORDINANCE AT THE REQUEST OF FOLSOM REAL ESTATE SOUTH, LLC, A DELAWARE LIMITED LIABILITY COMPANY AND CARPENTER EAST, LLC, A DELAWARE LIMITED LIABILITY COMPANY IN MAY 2018. I HEREBY STATE THAT THIS FINAL MAP SUBSTANTIALLY CONFORMS TO THE CONDITIONALLY APPROVED TENTATIVE MAP THAT THE MONUMENTS WILL BE OF THE CHARACTER AND WILL OCCUPY THE POSITIONS AS INDICATED AND WILL BE SET BY DECEMBER 31, 2023; AND THAT SAID MONUMENTS WILL BE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.

MACKAY & SOMPS CIVIL ENGINEERS, INC.



PAUL FERGUSON, JR.
PLS 9265 EXP. 03-31-2024

DATE: _____

CITY ENGINEER'S STATEMENT

I HEREBY STATE THAT I HAVE EXAMINED THIS FINAL MAP OF 'MANGINI RANCH PHASE 2 - VILLAGE 3' AND FIND IT TO BE SUBSTANTIALLY THE SAME AS THE TENTATIVE MAP APPROVED BY THE CITY COUNCIL OF THE CITY OF FOLSOM, AND THAT ALL PROVISIONS OF THE SUBDIVISION MAP ACT AND ALL APPLICABLE CITY ORDINANCES HAVE BEEN COMPLIED WITH.

STEVEN R. KRAHN, RCE 49291
CITY ENGINEER
CITY OF FOLSOM
LICENSE EXPIRES: 9/30/2022

DATE: _____

CITY SURVEYOR'S STATEMENT

I HEREBY STATE THAT I HAVE EXAMINED THIS FINAL MAP OF 'MANGINI RANCH PHASE 2 - VILLAGE 3' AND I AM SATISFIED THAT SAID MAP IS TECHNICALLY CORRECT.

GERALD A. YOUNG, L.S. 3852
CITY SURVEYOR
LICENSE EXPIRES: 6/30/2022

DATE: _____

CITY CLERK'S STATEMENT

I HEREBY STATE THAT THE CITY COUNCIL OF THE CITY OF FOLSOM HAS APPROVED THIS FINAL MAP OF 'MANGINI RANCH PHASE 2 - VILLAGE 3', AND HAS ACCEPTED, ON BEHALF OF THE PUBLIC, SUBJECT TO IMPROVEMENTS, ALL RIGHT-OF-WAYS AND EASEMENTS OFFERED HEREON FOR DEDICATION IN ACCORDANCE WITH THE TERMS OF THAT OFFER AND HAS APPROVED THE ABANDONMENT OF THE EASEMENTS LISTED HEREON.

CHRISTA FREEMANTLE
CITY CLERK
DATE: _____

RECORDER'S STATEMENT

FILED THIS _____ DAY OF _____, 2022, AT _____ M. IN BOOK _____ OF MAPS, AT PAGE _____ AT THE REQUEST OF MACKAY & SOMPS CIVIL ENGINEERS, INC. TITLE TO THE LAND INCLUDED IN THIS FINAL MAP BEING VESTED AS PER CERTIFICATE NO. _____ ON FILE IN THIS OFFICE.

DOANNA ALLRED
SACRAMENTO COUNTY RECORDER
STATE OF CALIFORNIA
DOCUMENT NO.: _____

BY: _____ DEPUTY FEE: \$ _____

**FINAL MAP (PN 17-307.03)
MANGINI RANCH
PHASE 2 - VILLAGE 3**

BEING A MERGER AND RESUBDIVISION OF LOT 3 AND LOT 3A OF THAT CERTAIN FINAL MAP TITLED 'MANGINI RANCH PHASE 2 LARGE LOT', FILED FOR RECORD ON JULY 2, 2019, IN BOOK 412 OF MAPS AT PAGE 7, SACRAMENTO COUNTY RECORDS, BEING A PORTION OF SECTION 16, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

CITY OF FOLSOM • SACRAMENTO COUNTY • CALIFORNIA



APRIL 2022
SHEET 1 OF 6

TRUSTEE'S STATEMENT

PLACER TITLE COMPANY, A CALIFORNIA CORPORATION, AS TRUSTEE UNDER THAT CERTAIN DEED OF TRUST, ASSIGNMENT OF LEASES AND RENTS, SECURITY AGREEMENT AND FIXTURE FILING, RECORDED NOVEMBER 10, 2021, (INSTRUMENT) 202111100839, OFFICIAL RECORDS OF SACRAMENTO COUNTY, HEREBY CONSENTS TO THE RECORDATION OF THIS MAP AND THE SUBDIVISION OF THE LANDS SHOWN HEREON.

DATE

NOTARY'S ACKNOWLEDGMENT

A NOTARY PUBLIC OR OTHER OFFICER COMPLETING THIS CERTIFICATE VERIFIES ONLY THE IDENTITY OF THE INDIVIDUAL WHO SIGNED THE DOCUMENT TO WHICH THIS CERTIFICATE IS ATTACHED, AND NOT THE TRUTHFULNESS, ACCURACY OR VALIDITY OF THAT DOCUMENT.

STATE OF CALIFORNIA

COUNTY OF _____

ON _____ BEFORE ME, _____, A NOTARY PUBLIC

PERSONALLY APPEARED

WHO PROVED TO ME ON THE BASIS OF SATISFACTORY EVIDENCE TO BE THE PERSON(S) WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE WITHIN INSTRUMENT AND ACKNOWLEDGED TO ME THAT HE/SHE/THEY EXECUTED THE SAME IN HIS/HER/THEIR AUTHORIZED CAPACITY(IES), AND THAT BY HIS/HER/THEIR SIGNATURE(S) ON THE INSTRUMENT THE PERSON(S), OR THE ENTITY UPON BEHALF OF WHICH THE PERSON(S) ACTED, EXECUTED THE INSTRUMENT.

I CERTIFY UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE STATE OF CALIFORNIA THAT THE FOREGOING PARAGRAPH IS TRUE AND CORRECT.

WITNESS MY HAND AND OFFICIAL SEAL.

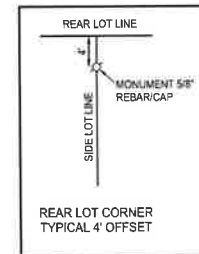
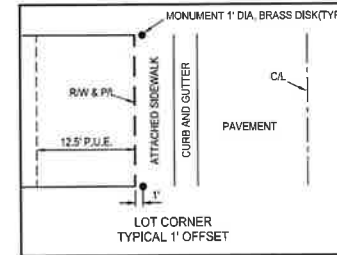
SIGNATURE _____ PRINTED NAME _____

MY PRINCIPAL PLACE OF BUSINESS IS _____ COUNTY _____

MY COMMISSION EXPIRES: _____ MY COMMISSION NUMBER: _____

NOTES

1. ALL CURVE DIMENSIONS ARE RADIUS, DELTA AND ARC LENGTH. ALL DISTANCES SHOWN ARE GROUND DISTANCES AND ARE IN FEET AND DECIMALS THEREOF. DUE TO ROUNDING THE SUM OF INDIVIDUAL DIMENSIONS MAY NOT EQUAL THE OVERALL DIMENSION.
2. THIS FINAL MAP CONTAINS 11.103± ACRES GROSS CONSISTING OF 53 RESIDENTIAL LOTS AND 5 LETTERED LOTS.
3. A PRELIMINARY GEOTECHNICAL ENGINEERING REPORT FOR THE MANGINI RANCH (PROJECT NO. E07145.001) WAS PREPARED BY YOUNGDAHL CONSULTING GROUP, INC ON JULY, 2014 IS AVAILABLE FOR PUBLIC INSPECTION AT THE CITY OF FOLSOM COMMUNITY DEVELOPMENT DEPARTMENT.
4. ALL FRONT LOT CORNERS WILL BE SET WITH A 1" DIAMETER BRASS DISC STAMPED "LS 9265" ON A 1.00 FOOT OFFSET ONTO THE SIDEWALK ON THE SIDE PROPERTY LINE EXTENDED (SEE DETAIL THIS SHEET).
5. REAR LOT CORNERS WILL BE SET AS FOLLOWS (UNLESS SHOWN OTHERWISE)
 - 5.1. FOR THE COMMON LINES OF LOTS 17/19, 48/49, 45/48/49, G151 AND 51/52 WILL BE SET WITH A 5/8" REBAR AND PLASTIC CAP STAMPED "LS 9265".
 - 5.2. FOR THE COMMON LINES OF LOTS 18/19, AND 38/50 WILL BE SET WITH A 4.00 FOOT OFFSET ON THE SIDE LOT LINE WITH A 5/8" REBAR AND PLASTIC CAP STAMPED "LS 9265" (SEE DETAIL THIS SHEET).
 - 5.3. FOR LOTS 1-16, 20-37, 39-45 AND THE COMMON LINES OF 46/47 AND 46/48 WHICH FALL WITHIN A MASONRY WALL WILL BE SET WITH A 3/4" BRASS TAG STAMPED "LS 9265" ON THE FACE OF WALL 2.00 FOOT ABOVE GROUND OR ON TOP OF THE WALL.
6. PROPERTY SUBJECT TO COMMUNITY FACILITIES DISTRICT 2013-1 (WATER FACILITIES AND SUPPLY), IN ACCORDANCE WITH THE "MELLO ROOS COMMUNITY FACILITIES ACT OF 1982". SAID ASSESSMENTS ARE/OR WILL BE COLLECTED WITH THE ANNUAL TAX BILL PER 20131230 O.R. 0311
7. PROPERTY SUBJECT TO COMMUNITY FACILITIES DISTRICT NO. 17 (WILLOW HILL PIPELINE) PER 20150325 O.R. 0353.
8. PROPERTY SUBJECT TO THE SCHOOL FACILITIES IMPROVEMENT DISTRICT NO. 3, AS DISCLOSED BY THAT CERTAIN ASSESSMENT MAP, RECORDED JULY 07, 2006, AS BOOK 103, PAGE 9 PER 20060707 O.R. 0662.
9. PROPERTY SUBJECT TO THE CITY OF FOLSOM COMMUNITY FACILITIES DISTRICT NO. 2014-1 (POWER PACE PROGRAM), AS DISCLOSED BY THAT CERTAIN ASSESSMENT MAP, RECORDED DECEMBER 23, 2013, AS BOOK 116, PAGE 7 PER 20131223 O.R. 0305.
10. PROPERTY SUBJECT TO THE CALIFORNIA HOME FINANCE AUTHORITY COMMUNITIES FACILITIES DISTRICT NO. 2014-1 (CLEAN ENERGY), AS DISCLOSED BY THAT CERTAIN ASSESSMENT MAP, RECORDED OCTOBER 05, 2015, AS (BOOK) 20151005, (PAGE) 0763.
11. PROPERTY SUBJECT TO COMMUNITY FACILITIES DISTRICT NO. 18 (FOLSOM PLAN AREA - AREA WIDE IMPROVEMENTS AND SERVICES) PER 20151209 O.R. 0427 AND 20161116 O.R. 465.
12. PROPERTY SUBJECT TO COMMUNITY FACILITIES DISTRICT NO. 23 (FOLSOM RANCH) PER BOOK 130 OF ASSESSMENT MAPS AT PAGE 27 AND DN 202008081509.
13. PROPERTY SUBJECT TO "WATER SUPPLY AND FACILITIES FINANCING PLAN AND AGREEMENT" PER 20130124 O.R. 1382 AND DOCUMENTS DECLARING MODIFICATIONS THEREOF PER 20130326 O.R. 1518, 20130521 O.R. 0391, 20140865 O.R. 0959.
14. PROPERTY SUBJECT TO AN EASEMENT OVER SAID LAND FOR AVIGATION AND INCIDENTAL PURPOSES AS GRANTED TO THE OF COUNTY OF SACRAMENTO AND CITY OF FOLSOM PER 20140715 O.R. 518 AND 525.
15. PROPERTY SUBJECT TO THE TERMS, CONDITIONS AND PROVISIONS AS CONTAINED IN THE AGREEMENT ENTITLED "FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT AGREEMENT, RELATIVE TO THE "FOLSOM SOUTH SPECIFIC PLAN" BY AND BETWEEN THE CITY OF FOLSOM, AND CARPENTER EAST, LLC, RECORDED JULY 15, 2014, AS BOOK 20140715, PAGE 522, OFFICIAL RECORDS, AND AS AMENDED BY DOCUMENT RECORDED, JANUARY 29, 2016 AS BOOK 20160129, PAGE 0388, OFFICIAL RECORDS, AND AS AMENDED BY DOCUMENT RECORDED, SEPTEMBER 17, 2019, DN 201909171340, O.R. AND MODIFIED BY DOCUMENT RECORDED, NOVEMBER 10, 2021, DN 202111100840.
16. PROPERTY SUBJECT TO THE TERMS, CONDITIONS AND PROVISIONS AS CONTAINED IN THE DOCUMENT ENTITLED "BUILDERS AGREEMENT", BY AND BETWEEN EAST CARPENTER IMPROVEMENT COMPANY, LLC, AND BEAZER HOMES HOLDINGS, LLC, A DELAWARE LIMITED LIABILITY COMPANY, RECORDED NOVEMBER 10, 2021, AS (INSTRUMENT) 202111100841, OFFICIAL RECORDS.
17. LOTS A, B, C, D, AND E AS SHOWN PER THIS MAP ARE TO BE DEEDED TO THE CITY OF FOLSOM EITHER CONCURRENT OR FOLLOWING RECORDATION OF THIS MAP.
18. PROPERTY IS SUBJECT TO THE "FUEL MODIFICATION PLAN, FOLSOM RANCH PHASE 2, EAST CARPENTER IMPROVEMENT COMPANY" PREPARED BY WOOD RODGERS AND ON FILE IN THE CITY OF FOLSOM COMMUNITY DEVELOPMENT DEPARTMENT.
19. PROPERTY LIES WITHIN THE BOUNDARIES OF PENDING SCHOOL FACILITIES IMPROVEMENT DISTRICT NO. 2.
20. PROPERTY SUBJECT TO AN "AGREEMENT AFFECTING REAL PROPERTY (INCLUSIONARY HOUSING AGREEMENT)" PER 20191217 O.R. 0909.
21. PURSUANT TO SECTION 66434(G) OF THE SUBDIVISION ACT THE FILING OF THIS FINAL MAP SHALL CONSTITUTE ABANDONMENT OF THE EASEMENTS LISTED BELOW, NOT SHOWN HEREON, WITHIN THE BOUNDARY OF THIS MAP.
 - A. THE LANDSCAPE EASEMENT, PEDESTRIAN ACCESS EASEMENT AND PUBLIC UTILITY EASEMENT WITHIN LOT 3 OF 4-12 B.M. 7.
 - B. PLACEVILLE ROAD RIGHT-OF-WAY PER 848 O.R. 188
 - C. SAVANNAH PARKWAY RIGHT-OF-WAY DEDICATED PER 412 B.M. 7 (REDEDICATED ON THIS MAP)



**FINAL MAP (PN 17-307.03)
MANGINI RANCH
PHASE 2 - VILLAGE 3**

BEING A MERGER AND RESUBDIVISION OF LOT 3 AND LOT 3A OF THAT CERTAIN FINAL MAP TITLED "MANGINI RANCH PHASE 2 LARGE LOT", FILED FOR RECORD ON JULY 2, 2019, IN BOOK 412 OF MAPS AT PAGE 7, SACRAMENTO COUNTY RECORDS, BEING A PORTION OF SECTION 16, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

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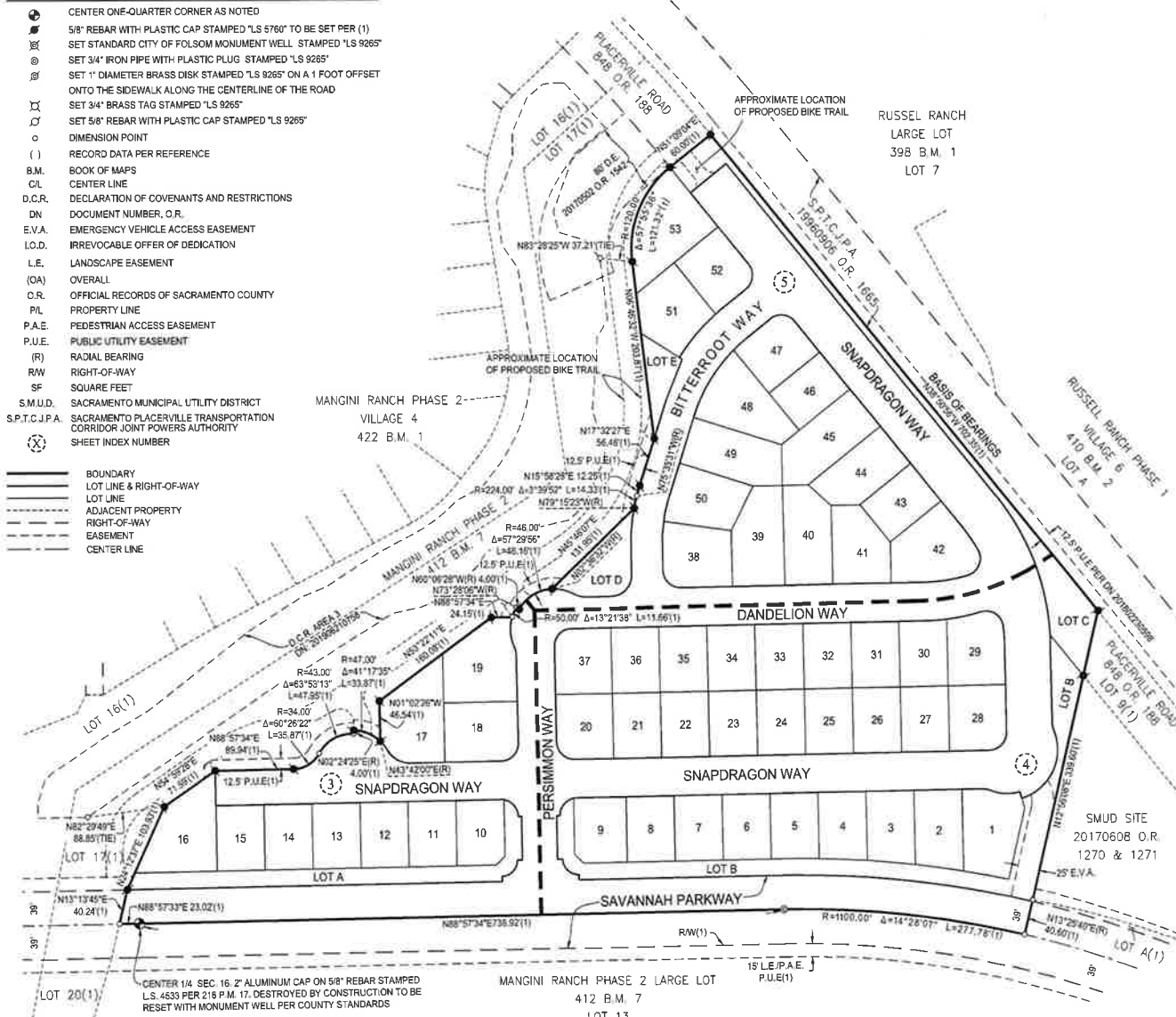
Mackay & Somp
ENGINEERS PLANNERS SURVEYORS
1025 Creekside Ridge Drive, Suite 150, Roseville, CA 95678 (916) 775-1118

APRIL 2022
SHEET 2 OF 6

SEE THIS SHEET FOR NOTES AND SEE SHEET 3 FOR BASIS OF BEARINGS, LEGEND AND REFERENCES.

LEGEND

- ⊕ CENTER ONE-QUARTER CORNER AS NOTED
 - ⊕ 5/8" REBAR WITH PLASTIC CAP STAMPED "LS 5760" TO BE SET PER (1)
 - ⊕ SET STANDARD CITY OF FOLSOM MONUMENT WELL. STAMPED "LS 9265"
 - ⊕ SET 3/4" IRON PIPE WITH PLASTIC PLUG STAMPED "LS 9285"
 - ⊕ SET 1" DIAMETER BRASS DISK STAMPED "LS 9265" ON A 1 FOOT OFFSET ONTO THE SIDEWALK ALONG THE CENTERLINE OF THE ROAD
 - ⊕ SET 3/4" BRASS TAG STAMPED "LS 9265"
 - ⊕ SET 5/8" REBAR WITH PLASTIC CAP STAMPED "LS 9265"
 - DIMENSION POINT
 - () RECORD DATA PER REFERENCE
 - B.M. BOOK OF MAPS
 - CL CENTER LINE
 - D.C.R. DECLARATION OF COVENANTS AND RESTRICTIONS
 - DN DOCUMENT NUMBER, O.R.
 - E.V.A. EMERGENCY VEHICLE ACCESS EASEMENT
 - I.O.D. IRREVOCABLE OFFER OF DEDICATION
 - L.E. LANDSCAPE EASEMENT
 - (OA) OVERALL
 - O.R. OFFICIAL RECORDS OF SACRAMENTO COUNTY
 - P.L. PROPERTY LINE
 - P.A.E. PEDESTRIAN ACCESS EASEMENT
 - P.U.E. PUBLIC UTILITY EASEMENT
 - (R) RADIAL BEARING
 - RW RIGHT-OF-WAY
 - SF SQUARE FEET
 - S.M.U.D. SACRAMENTO MUNICIPAL UTILITY DISTRICT
 - S.P.T.C.J.P.A. SACRAMENTO PLACERVILLE TRANSPORTATION CORRIDOR JOINT POWERS AUTHORITY
 - ⊗ SHEET INDEX NUMBER
-
- BOUNDARY
 - LOT LINE & RIGHT-OF-WAY
 - LOT LINE
 - ADJACENT PROPERTY
 - RIGHT-OF-WAY
 - EASEMENT
 - CENTER LINE



SEE SHEET 2 FOR NOTES AND SEE THIS SHEET FOR BASIS OF BEARINGS, LEGEND AND REFERENCES.

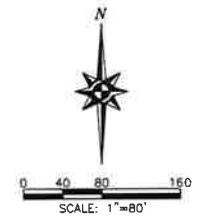
I.O.D. CITY OF FOLSOM
20210402 O.R. 0622

BASIS OF BEARINGS

THE BASIS OF BEARING FOR THIS SURVEY IS THE NORTHEASTERLY LINE OF LOT 3A AS SHOWN ON THE MANGINI RANCH PHASE 2 LARGE LOT MAP FILED FOR RECORD ON JULY 2, 2019 IN BOOK 412 OF MAPS, AT PAGE 7, SACRAMENTO COUNTY RECORDS AND HAVING A BEARING OF SOUTH 38°50'56" EAST AS DETERMINED FROM THE MONUMENTS SHOWN AS FOUND.

REFERENCES

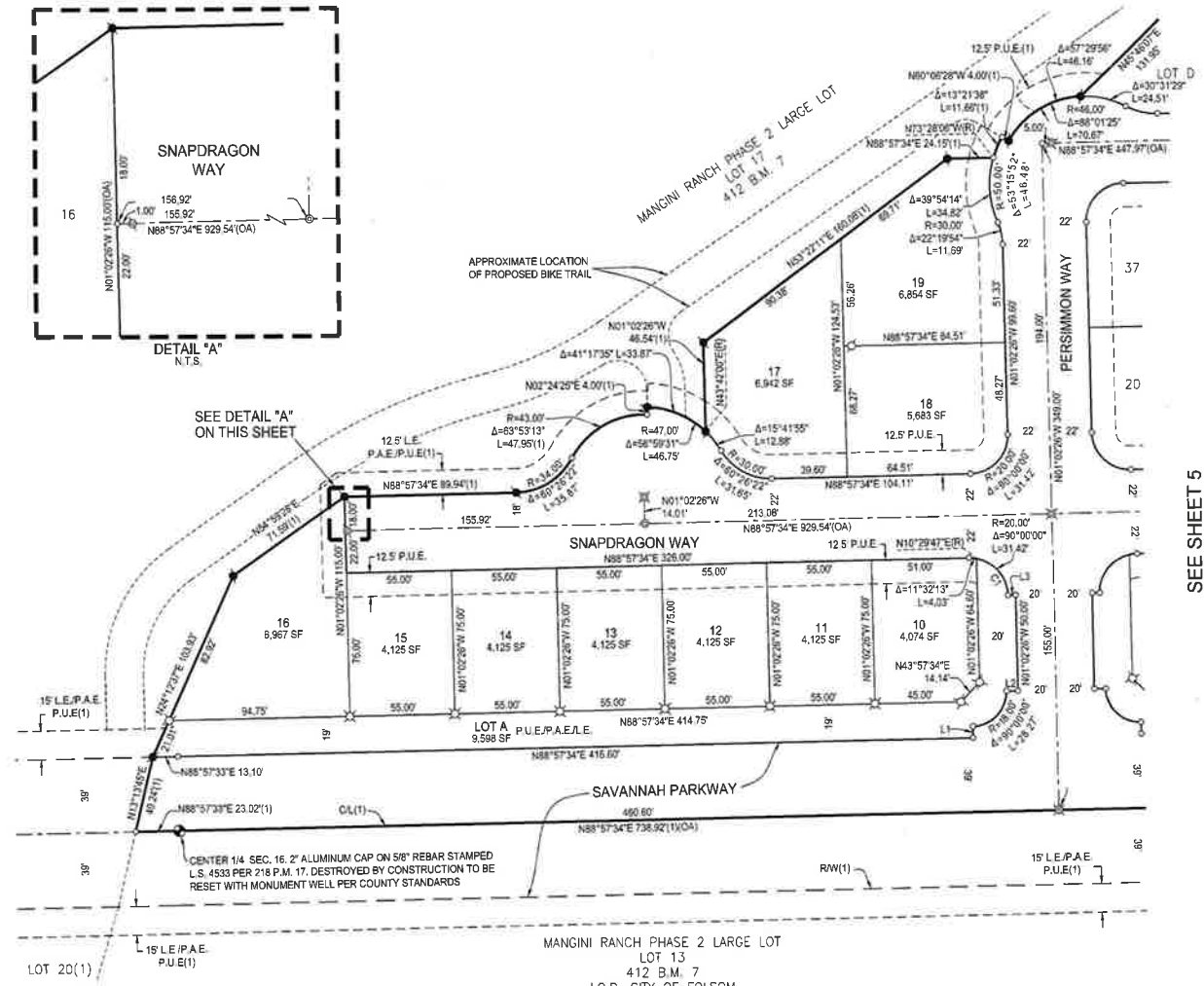
- (1) 412 B.M.7



**FINAL MAP (PN 17-307.03)
MANGINI RANCH
PHASE 2 - VILLAGE 3**

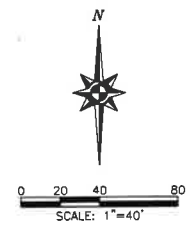
BEING A MERGER AND RESUBDIVISION OF LOT 3 AND LOT 3A OF THAT CERTAIN FINAL MAP TITLED "MANGINI RANCH PHASE 2 LARGE LOT", FILED FOR RECORD ON JULY 2, 2019, IN BOOK 412 OF MAPS AT PAGE 7, SACRAMENTO COUNTY RECORDS, BEING A PORTION OF SECTION 16, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

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SHEET 3 OF 6



Line Table		
Line #	Bearing	Length
L1	N01°02'26\"/>	

Curve Table			
Curve #	Radius	Delta	Length
C1	20.00'	76°27'47\"/>	

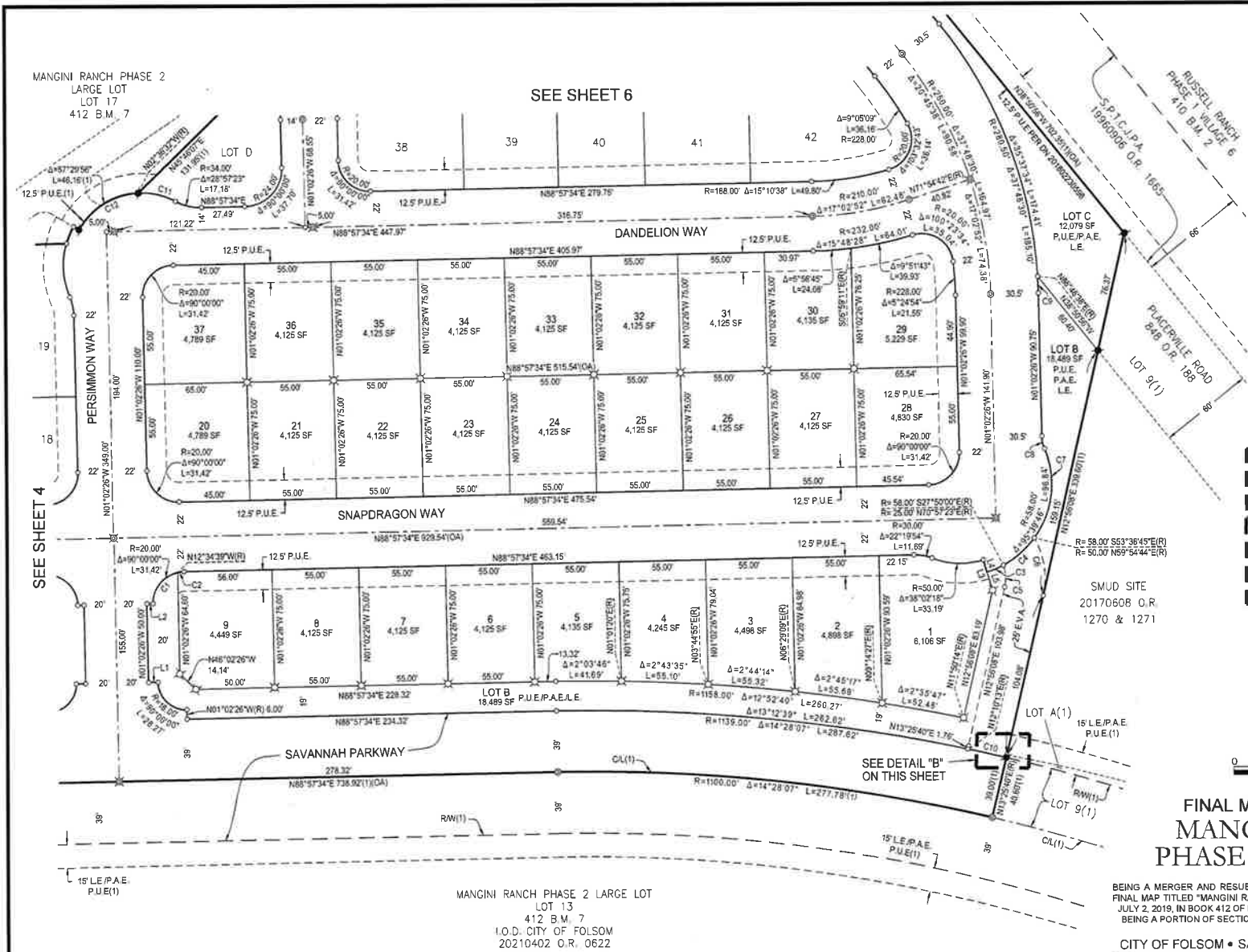


**FINAL MAP (PN 17-307.03)
MANGINI RANCH
PHASE 2 - VILLAGE 3**

BEING A MERGER AND RESUBDIVISION OF LOT 3 AND LOT 3A OF THAT CERTAIN FINAL MAP TITLED "MANGINI RANCH PHASE 2 LARGE LOT", FILED FOR RECORD ON JULY 2, 2019, IN BOOK 412 OF MAPS AT PAGE 7, SACRAMENTO COUNTY RECORDS, BEING A PORTION OF SECTION 16, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

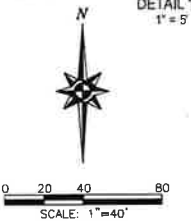
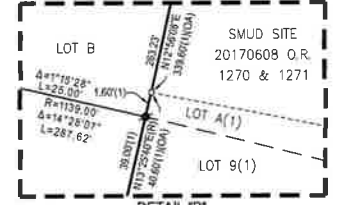
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SHEET 4 OF 6

SEE SHEET 2 FOR NOTES AND SEE SHEET 3 FOR BASIS OF BEARINGS, LEGEND AND REFERENCES.



Line Table		
Line #	Bearing	Length
L1	N88°57'34"E (R)	6.00'
L2	N88°57'34"E (R)	4.00'
L3	N16°44'50"W (R)	20.01'
L4	N16°44'50"W (R)	8.00'
L5	N16°44'50"W (R)	12.01'

Curve Table			
Curve #	Radius	Delta	Length
C1	20.00'	78°27'47"	27.38'
C2	20.00'	11°32'13"	4.03'
C3	58.00'	11°05'09"	11.22'
C4	58.00'	25°48'45"	26.13'
C5	25.00'	31°58'45"	13.95'
C6	50.00'	43°01'24"	37.54'
C7	58.00'	58°45'51"	59.49'
C8	22.00'	21°22'10"	8.21'
C9	280.50'	2°10'55"	10.68'
C10	1139.00'	1°15'28"	25.00'
C11	46.00'	30°3'29"	24.51'
C12	46.00'	68°01'25"	70.67'



**FINAL MAP (PN 17-307.03)
MANGINI RANCH
PHASE 2 - VILLAGE 3**

BEING A MERGER AND RESUBDIVISION OF LOT 3 AND LOT 3A OF THAT CERTAIN FINAL MAP TITLED "MANGINI RANCH PHASE 2 LARGE LOT", FILED FOR RECORD ON JULY 2, 2019, IN BOOK 412 OF MAPS AT PAGE 7, SACRAMENTO COUNTY RECORDS, BEING A PORTION OF SECTION 16, TOWNSHIP 9 NORTH, RANGE 6 EAST, M.D.B.M.

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SEE SHEET 2 FOR NOTES AND SEE SHEET 3 FOR BASIS OF BEARINGS, LEGEND AND REFERENCES.

MANGINI RANCH PHASE 2 LARGE LOT
LOT 13
412 B.M. 7
I.O.D. CITY OF FOLSOM
20210402 O.R. 0622

SEE SHEET 2 FOR NOTES AND SEE SHEET 3 FOR BASIS OF BEARINGS, LEGEND AND REFERENCES.

FINAL MAP (PN 17-307.03)
MANGINI RANCH
PHASE 2 - VILLAGE 3

BEING A MERGER AND RESUBDIVISION OF LOT 3 AND LOT 3A OF THAT CERTAIN FINAL MAP TITLED "MANGINI RANCH PHASE 2 LARGE LOT", FILED FOR RECORD ON JULY 2, 2019, IN BOOK 412 OF MAPS AT PAGE 7, SACRAMENTO COUNTY RECORDS, BEING A PORTION OF SECTION 16, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

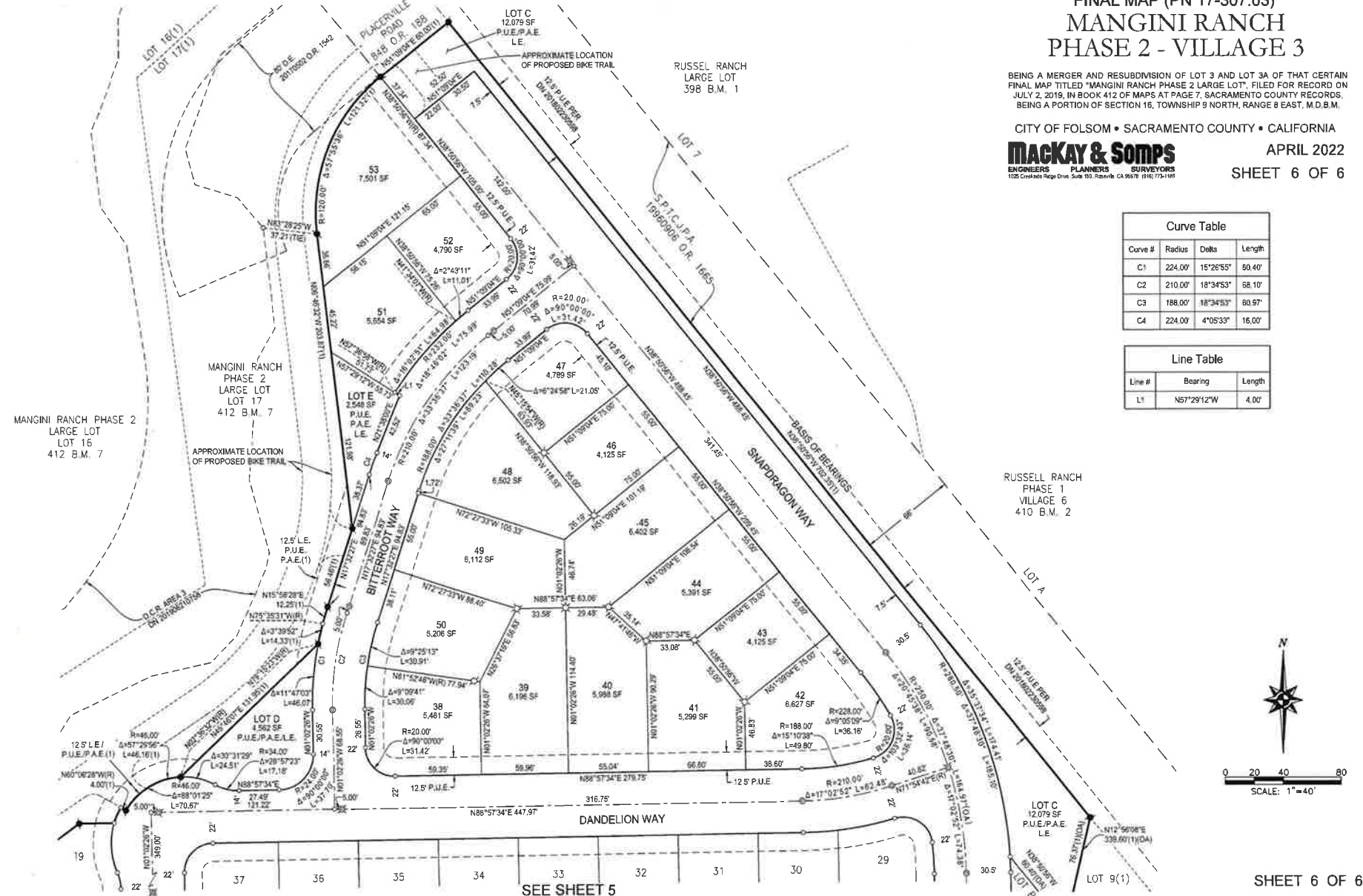
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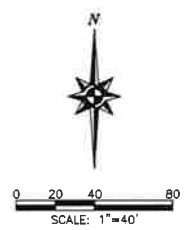
APRIL 2022
 SHEET 6 OF 6

Curve Table			
Curve #	Radius	Delta	Length
C1	224.00'	15°26'55"	60.40'
C2	210.00'	18°34'53"	58.10'
C3	188.00'	18°34'53"	60.97'
C4	224.00'	4°05'33"	16.00'

Line Table		
Line #	Bearing	Length
L1	N57°29'12"W	4.00'



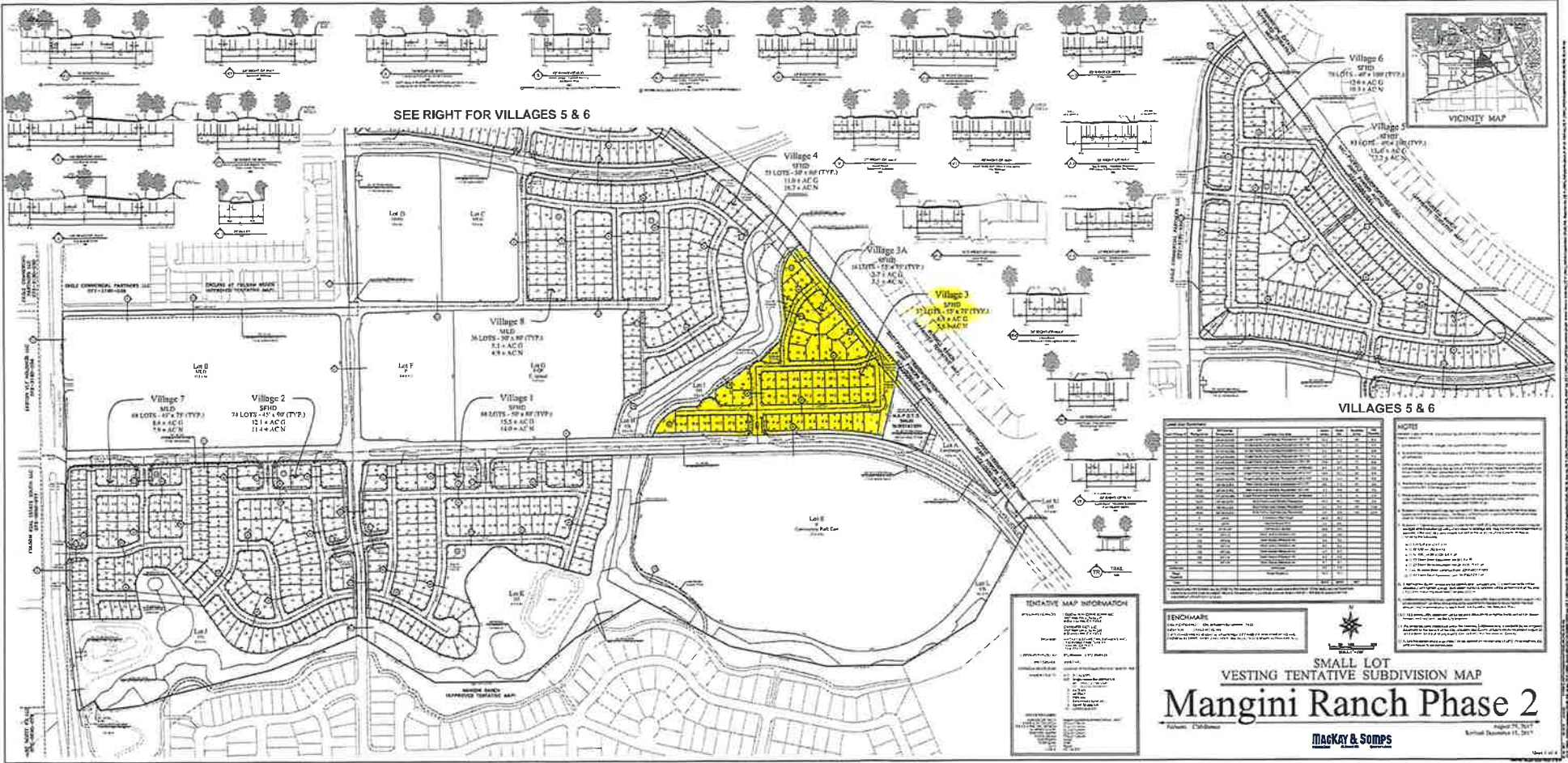
RUSSELL RANCH
 PHASE 1
 VILLAGE 6
 410 B.M. 2



SHEET 6 OF 6

ATTACHMENT 4

MANGINI RANCH PHASE 2 VILLAGE NO. 3 VESTING TENTATIVE SUBDIVISION MAP



ATTACHMENT 5

TABLE OF CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 VILLAGE NO. 3 VESTING TENTATIVE SUBDIVISION MAP

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
1.	<p>Final Development Plans The owner/applicant shall submit final site development plans to the Community Development Department that shall substantially conform to the exhibits referenced below:</p> <ol style="list-style-type: none"> 2. Vicinity Map 3. Illustrative Master Plan Exhibit, dated December 15, 2017 4. Large-Lot Vesting Tentative Subdivision Map, dated December 15, 2017 5. Small-Lot Vesting Tentative Subdivision Map, dated December 15, 2017 6. Preliminary Grading and Drainage Plan, dated December 15, 2017 7. Preliminary Utility Plan, dated December 15, 2017 8. Conceptual Phasing Plan, dated December 15, 2017 9. On-Site Infrastructure Phasing Exhibit, dated December 15, 2017 10. On-Site Infrastructure Phasing Narrative, dated December 14, 2017 11. Preliminary Phased Off-Site Utility Plan, dated September, 2017 12. Off-Site Infrastructure Triggers, dated December, 2017 13. Interim Off-Site Intersection Design, dated December 15, 2017 14. Trail System Modification Exhibit, dated December 15, 2017 15. Noise Mitigation Exhibit and Conceptual Wall and Fencing Exhibit, dated December 15, 2017 16. Inclusionary Housing Plan, dated September 26, 2017 17. Parks and Open Space Ownership/Maintenance Summary, dated December 14, 2017 18. Minor Administrative Modification Exhibits 19. Folsom Ranch Central District Guidelines Addendum, dated December, 2017 <p>The Small-Lot Vesting Tentative Subdivision Map is approved for the development of a 545-unit residential subdivision (Mangini Ranch Phase 2 Subdivision). Implementation of the project shall be consistent with the above referenced items and these conditions of approval.</p>	G, I, M, B	CD (P)(E)	<p>The Community Development Department has reviewed and approved the improvement plans and the final map for the project. The approved improvement plans are in substantial compliance with the grading and drainage plans, the site and utility plans, offsite infrastructure exhibit, the preliminary landscape plans and the design guidelines.</p> <p>The final map for Village 3 is in substantial compliance with the approved Vesting Tentative Subdivision Map.</p> <p>The Vesting Tentative Subdivision Map (PN17-307) was approved by the City Council on February 13, 2018. (Resolution No. 10069)</p>	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
2.	Plan Submittal All civil engineering, improvement, and landscape and irrigation plans, shall be submitted to the Community Development Department for review and approval to ensure conformance with this approval and with relevant codes, policies, standards and other requirements of the City of Folsom.	G, I, M,	CD (P)(E)(B)	Improvement plans for the Village 3 subdivision have been reviewed and approved by the Community Development Department. Grading and construction commenced in this subdivision in the Spring of 2022. Landscape and Irrigation plans for this subdivision reviewed and approved prior to the first building permit issuance in the subdivision.	Yes
3.	Validity This approval of the Vesting Small Lot Tentative Subdivision Map shall be valid for a period of twenty-four months pursuant to Section 16.16.110A of the Folsom Municipal Code and the Subdivision Map Act. The term of the approved Inclusionary Housing Plan shall track the term of the Vesting Small Lot Tentative Subdivision Map, as may be extended from time to time pursuant to Section 16.16.110.A and 16.16.120 of the Folsom Municipal Code and the Subdivision Map Act. The term of the Project Design Guidelines shall track the term of the First Amended and Restated Tier 1 Development Agreement.	OG	CD (P)	The City Council approved the Small Lot Vesting Tentative Map on February 13, 2018. (Resolution No. 10069)	Yes
4.	Vesting Tentative Subdivision Map Approval The Vesting Tentative Subdivision Map for the Mangini Ranch Phase 2 Subdivision project shall be subject to review and approval by the City Council.	M	CD (P)(E)	The Owner/applicant has complied with all applicable mitigation measures from the FEIR/EIS prior to the issuance of a grading permit. Additionally, construction inspection and monitoring is being conducted throughout construction by the City and/or its Consultants.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
5.	<p>Improvements in the PFFP</p> <p>The owner/applicant shall be subject to all thresholds, timelines and deadlines for the construction and final completion of various improvements for the entire Folsom Plan Area. The various improvements are outlined and detailed in the Folsom Plan Area Specific Plan Public Facilities Financing Plan (PFFP) dated January 28, 2014 and adopted by City of Folsom Resolution No. 9298. These improvements in the PFFP include, but are not limited to, the backbone infrastructure water (water reservoirs, water transmission mains, booster pump stations, pressure reducing valve stations, etc.), sanitary sewer (lift stations and forced mains) systems, recycled water mains and associated infrastructure, roadway and transportation (future interchanges, major arterial roadways, etc.) improvements, aquatic center (community pool), parks, fire stations, municipal services center, community library, etc The thresholds and timelines included in the PFFP require facilities to be constructed and completed based on number of building permits issued and in some cases, number of residential units that are occupied. The owner/applicant shall be required to address these thresholds and timelines as the project moves forward through the various developments stages and shall be subject to the various fair share requirements, subject to the provisions of the PFFP, the ARDA and any amendment thereto.</p>	M	CD(E)(P)(B), PW, FD, EWR, PR	The owner/applicant is in compliance with all plan area wide obligations. These conditions of approval require certain improvements during buildout of this Tentative Map. The owner/applicant has been required to construct these improvements as needed and in conjunction with the other subdivisions under development. Grading and construction in this subdivision commenced in the Spring of 2020.	Yes
6.	<p>Street Names</p> <p>The street names identified below shall be used for the Final Small-Lot Map: Savannah Parkway, Alder Creek Parkway, Westwood Drive, Dandelion Lane, Golden Wave Drive, Gardner Street, Persimmon Way, Sassafras Trail, Hackberry Lane, Arbor View Drive, Emerald Knoll Lane, Copper Ridge Drive, Field View Trail, Eagle Ridge Lane, Spice Wood Court, Willow Grove Street, Cimarron Trail, Vista Grande Drive, Sienna Bluff Trail, Lilac Circle, Butterfly Ridge, Spring Harvest Trail, Barnwood Drive, Barn Owl Drive, Triple Creek Way, Crooked Bed Trail, Deer Point Court, Blacktail Way, Willowick Lane, Shadow Creek Circle, Dragonfly Way, Old Ranch Road, Whispering Brook Way, Shakers Ridge Way, Quakie Glen Drive, Shale Rock Court, Wind River Range Lane, Owl Creek Drive, Drowsy Water Way, Cackleberry Court, Quail Meadow Way, Tall Oaks Bend, Stone View Trail, Snapdragon Lane, Timberline Meadow Way, Trails End, Broken Oak Lane, Bitterroot Lane.</p>	M	CD (E)(P)	The Final Map for Village 3 includes street names chosen from the approved list in this condition.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
7.	<p>Indemnity for City</p> <p>The owner/applicant shall protect, defend, indemnify, and hold harmless the City and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul any approval by the City or any of its agencies, departments, commissions, agents, officers, employees, or legislative body concerning the project, which claim, action or proceeding is brought within the time period provided therefore in Government Code Section 66499.37 or other applicable statutes of limitation. The City will promptly notify the owner/applicant of any such claim, action or proceeding, and will cooperate fully in the defense. If the City should fail to cooperate fully in the defense, the owner owner/applicant shall not thereafter be responsible to defend, indemnify and hold harmless the City or its agents, officers, and employees, pursuant to this condition. The City may, within its unlimited discretion, participate in the defense of any such claim, action or proceeding if both of the following occur:</p> <ul style="list-style-type: none"> • The City bears its own attorney's fees and costs; and • The City defends the claim, action or proceeding in good faith <p>The owner/applicant shall not be required to pay or perform any settlement of such claim, action or proceeding unless the settlement is approved by the owner/applicant. The owner/applicant's obligations under this condition shall apply regardless of whether a Final Map is ultimately recorded with respect to this project.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD	The City standard subdivision improvement agreement includes language that satisfies this condition. The subdivision improvement agreement will be executed by the City Manager upon approval by the City Council.	Yes
8.	<p>Small-Lot Vesting Tentative Subdivision Map</p> <p>The Small-Lot Vesting Tentative Subdivision map is expressly conditioned upon compliance with all environmental mitigation measures in the Folsom Plan Area Specific Plan (FEIR/EIS) as amended by the Westland/Eagle Specific Plan Amendment CEQA Addendum, and the Folsom South of U.S. Highway 50 Specific Plan Revised Proposed Off-Site Water Facility Alternative CEQA Addendum as well as compliance with the mitigation measures in the South of U.S. Highway 50 Backbone Infrastructure Project Mitigated Negative Declaration for those portions of Mangini Ranch Phase 2 that are included as part of the South of U.S. Highway 50 Backbone Infrastructure Project.</p>	OG	CD	The Owner/applicant has complied with all applicable mitigation measures from the FEIR/EIS prior to the issuance of a grading permit for this subdivision in the Spring of 2022. Additionally, construction inspection and monitoring is conducted throughout construction by the City and/or its Consultants to verify compliance.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
9.	ARDA and Amendments The owner/applicant shall comply with all provisions of Amendments No. 1 and 2 to the First Amended and Restated Tier 1 Development Agreement and any approved amendments thereafter by and between the City and the owner/applicant of the project.	G, I, M, B	CD (E)	The owner/applicant has complied with all applicable provisions of the respective Amended and Restated Tier 1 Development Agreement.	Yes
10.	Mitigation Monitoring The owner/applicant shall participate in a mitigation monitoring and reporting program pursuant to City Council Resolution No. 2634 and Public Resources Code 21081.6. The mitigation monitoring and reporting measures identified in the Folsom Plan Area Specific Plan FEIR/EIS, the South of 50 Backbone Infrastructure Project MND, the Westland/Eagle Specific Plan Amendment to the FPASP and Addendum to the FPASP EIR/EIS, and the Folsom South of U.S. Highway 50 Specific Plan Project Revised Proposed Off-Site Water Facility Alternative Amendment to the FPASP and Addendum to the FPASP EIR/EIS have been incorporated into these conditions of approval in order to mitigate or avoid significant effects on the environment. These mitigation monitoring and reporting measures are identified in the mitigation measure column. Applicant shall fund on a Time and Materials basis all mitigation monitoring (e.g., staff and consultant time).	OG	CD (P)	The owner has funded and participated in a MMRP reporting program performed by the City's consultant (Helix) and/or staff throughout the course of grading and construction.	Yes
POLICE/SECURITY REQUIREMENT					
11.	The owner/applicant shall consult with the Police Department in order to incorporate all reasonable crime prevention measures. The following security/safety measures shall be considered: <ul style="list-style-type: none"> • A security guard on-duty at all times at the site or a six-foot security fence shall be constructed around the perimeter of construction areas. • Security measures for the safety of all construction equipment and unit appliances. • Landscaping shall not cover exterior doors or windows, block line-of-sight at intersections or screen overhead lighting. 	G, I, B	PD	The owner provided onsite security during construction and has incorporated line of sight guidelines into landscaping plans at intersections	Yes
DEVELOPMENT COSTS AND FEE REQUIREMENTS					
12.	Taxes and Fees The owner/applicant shall pay all applicable taxes, fees and charges for the project at the rate and amount required by the Public Facilities Financing Plan and Amendment No. 1 to the Amended and Restated Tier 1 Development Agreement.	OG	CD (P)(E)	The owner has paid all current taxes and fees associated with this subdivision.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
13.	Assessments If applicable, the owner/applicant shall pay off any existing assessments against the property, or file necessary segregation request and pay applicable fees.	OG	CD (E)	The owner has paid all taxes and fees associated with this subdivision and filed a tax segregation request for applicable taxes.	Yes
14.	FPASP Development Impact Fees The owner/applicant shall be subject to all Folsom Plan Area Specific Plan Area development impact fees in place at the time of approval or subsequently adopted consistent with the Public Facilities Financing Plan (PFFP), Development Agreement and amendments thereto, unless exempt by previous agreement. The owner/applicant shall be subject to all applicable Folsom Plan Area plan-wide development impact fees in effect at such time that a building permit is issued. These fees may include, but are not limited to, the Folsom Plan Area Specific Plan Fee, Specific Plan Infrastructure Fee (SPIF), Solid Waste Fee, Corporation Yard Fee, Transportation Management Fee, Transit Fee, Highway 50 Interchange Fee, General Park Equipment Fee, Housing Trust Fee, etc. Any protest to such for all fees, dedications, reservations or other exactions imposed on this project will begin on the date of final approval (February 13, 2018), or otherwise shall be governed by the terms of Amendments No. 1 and 2 to ARDA. The fees shall be calculated at the fee rate set forth in the PFFP and the ARDA.	B	CD (P), PW, PK	The Owner/Applicant shall pay all required City fees and Plan Area wide fees prior to issuance of building permits.	Yes
15.	Legal Counsel The City, at its sole discretion, may utilize the services of outside legal counsel to assist in the implementation of this project, including, but not limited to, drafting, reviewing and/or revising agreements and/or other documentation for the project. If the City utilizes the services of such outside legal counsel, the City shall provide notice to the owner/applicant of the outside counsel selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for all outside legal fees and costs incurred and documented by the City for such services. The owner/applicant may be required, at the sole discretion of the City Attorney, to submit a deposit to the City for these services prior to initiation of the services. The owner/applicant shall be responsible for reimbursement to the City for the services regardless of whether a deposit is required.	OG	CD (P)(E)	The City has not yet utilized any outside services for any type of legal issues for this subdivision. If at any time during the development of this subdivision, any outside legal services were necessary, the owner/applicant would be required to conform to this condition.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
16.	<i>Consultant Services</i> If the City utilizes the services of consultants to prepare special studies or provide specialized design review or inspection services for the project, the City shall provide notice to the owner/applicant of the outside consultant selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for actual costs incurred and documented in utilizing these services, including administrative costs for City personnel. A deposit for these services shall be provided prior to initiating review of the Grading Plan, Final Map, improvement plans, or beginning inspection, whichever is applicable.	G, I, M, B	CD (P)(E)	The City has provided notice to the owner/applicant for various Consultants performing services for the development of this subdivision. The City has collected deposits from the owner/applicant in advance of such work for these services.	Yes
17.	<i>Walls/Fences/Gates</i> The final location, design, height, materials, and colors of the walls, fences, and gates shall be subject to review and approval by the Community Development Department to ensure consistency with the Folsom Ranch Central District Design Guidelines.	B	CD (P)(E)	The owner/applicant will submit landscape and streetscape plans to the Community Development Department. The Community Development Department will verify that the landscape and streetscape plans include the required wall, fences and gates and that these improvements are in compliance with the Folsom Ranch Central District Design Guidelines.	Condition will be satisfied prior to building permit issuance in the subdivision.

**CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307)
WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50
SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP**

Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
18.	<p>Development Phase 1 (Villages 1, 2, 7) Plan The owner/applicant shall construct the following improvements as shown on the Vesting Tentative Subdivision Map with each applicable phase. Roadways shall be to the ultimate horizontal and vertical alignment unless otherwise noted.</p> <ul style="list-style-type: none"> o Roads <ul style="list-style-type: none"> ▪ Alder Creek Parkway (East Bidwell Street to Placerville Road) <ul style="list-style-type: none"> • One lane of travel in each direction (These roadway improvements are existing improvements being constructed with FPA Phase 1 Backbone Improvements) • Intersection and median pocket improvements are described in following three phases ▪ East Bidwell Street (Alder Creek Parkway to the interface of Mangini Ranch Phase 1 Subdivision Project) <ul style="list-style-type: none"> • One lane of travel in each direction (These roadway improvements are existing-improvements constructed with FPA Phase 1 Backbone Improvements) • Intersection of Savannah Parkway and East Bidwell Street including turn lanes (as shown on Attachment 11/Off-Site Infrastructure Triggers, December-2017/Required Prior to First Building Permit) <ul style="list-style-type: none"> o Control: Stop-Sign control at the westbound approach to East Bidwell Street from Savannah Parkway with full access. o Southbound Approach to Savannah Parkway from East Bidwell Street: One thru-lane, and one left-turn lane with a 200-foot long transition, 60-foot-long taper, and 100 feet of vehicle storage. o Northbound Approach to Savannah Parkway from East Bidwell Street: One shared thru/right-turn lane. o Westbound Approach to East Bidwell Street from Savannah Parkway: One shared left/right-turn lane, and a striped out left-turn pocket with a 125-foot-long taper and 60 feet of vehicle storage. 	G, I, M	CD (E), EWR, PW, FD	<p>The owner has submitted and obtained approval by the City of a phasing plan for Development Phase 1 of the Mangini Ranch Phase 2 Vesting Tentative Subdivision Map, which includes Village 3. The following addresses the improvements necessary to serve Village 3: Alder Creek Parkway is not required to provide access to the Village 3 subdivision.</p> <p>Roadway improvements to provide access to the Village 3 subdivision have been constructed with the approved Village 1, 2 and 7 improvement plans and the approved Enclave at Folsom Ranch Off-site Improvement Plans. These roadway improvements included East Bidwell Street, Old Ranch Way, Westwood Drive and Savannah Parkway. These improvements have been constructed and have been accepted by the City in the Spring of 2022. The extension of Savannah Parkway to Grand Prairie Road is currently under construction and will provide vehicular access to Village 3. It is expected that the Village 3 subdivision improvements and the extension of Savannah Parkway will be complete in the Fall of 2022.</p>	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
18. Cont.	<ul style="list-style-type: none"> ○ Between Old Ranch Way and the southern boundary of the project site, East Bidwell Street shall be constructed as a two-lane arterial on the eastern “half segment” of its ultimate configuration. This two-lane segment shall have a striped 2-foot-wide median south of Old Ranch Way, consistent with the California Manual of Uniform Traffic Control Devices (MUTCD) Figure 3A-107 (CA), or similar standard. The southbound left-turn pocket shall be developed in accordance with the Highway Design Manual (HDM) Figure 405-2A, or similar standard. Savannah Parkway shall have a raised median curb. ● Intersection of Old Ranch Way and East Bidwell Street including turn lanes <ul style="list-style-type: none"> ○ Control: All-Way Stop-Sign control at the intersection of East Bidwell Street and Old Ranch Way with full access. ○ Southbound Approach to Old Ranch Way from East Bidwell Street: One thru-lane and one left-turn lane. ○ Northbound Approach to Old Ranch Way from East Bidwell Street: One shared thru/right-turn lane. ○ Westbound Approach to East Bidwell Street from Old Ranch Way: One shared left/right-turn lane. ● Village 7 Entrance/Exit on East Bidwell Street <ul style="list-style-type: none"> ○ Control: Stop-Sign control at the Village 7 exit to East Bidwell Street. ○ Access to Village 7 from East Bidwell Street limited to right-turns in and right-turns out only. Measures to enforce the right-turn in and right-turn out restriction at this location shall be implemented to the satisfaction of the Community Development Department. ● Frontage Improvements on East Bidwell Street <ul style="list-style-type: none"> ○ Frontage Improvements including curb, gutter, sidewalk, and landscaping along the east side of East Bidwell Street from Savannah Parkway to the southern project boundary with the Mangini Ranch Phase 1 Subdivision project. 	G, I, M	CD (E), EWR, PW, FD		

**CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307)
WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50
SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP**

Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
18. Cont.	<ul style="list-style-type: none"> ▪ Savannah Parkway (East Bidwell Street East Bidwell Street to the eastern edge of Open Space Lots I and L) <ul style="list-style-type: none"> • One lane of travel in each direction and a landscaped median of varying widths. • Intersection of Savannah Parkway and Westwood Drive including turn lanes <ul style="list-style-type: none"> ○ Westbound approach to Westwood Drive from Savannah Parkway: One through lane, one right-turn lane, and one left-turn lane. The right-turn pocket will have a 125-foot-long taper and 210 feet of vehicle storage. The left-turn lane will have a 125-foot-long taper with 60 feet of vehicle storage. ○ Eastbound approach to Westwood Drive from Savannah Parkway: One shared through/right-turn lane and one left-turn lane. The left-turn lane will have a 125-foot-long taper with 60 feet of vehicle storage. ○ Northbound approach to Savannah Parkway from Westwood Drive: One shared through lane/right-turn lane and one left-turn lane. The left-turn lane will have a 125-foot-long taper with 60 feet of vehicle storage. A 60-foot-long taper will be provided for the right-turn movement. • Village 1 Entrance/Exit and Turn Pockets <ul style="list-style-type: none"> ○ Control: Stop-Sign control at the Village 1 exit to Savannah Parkway. ○ Westbound left-turn pocket into Village 1 entrance from Savannah Parkway with a 125-foot-long taper and 60 feet of vehicle storage. ○ Eastbound left-turn pocket at Savannah Parkway/Village 1 intersection with 125-foot-long taper and 60 feet of vehicle storage. • Frontage Improvements on Savannah Parkway <ul style="list-style-type: none"> ○ Frontage Improvements along the southern side of Savannah Parkway including curb, gutter, sidewalk, medians, and landscaping along with the remainder of paving required to complete the roadway sections K and K1 to the eastern edge of Open Space Lots I and J as shown on the Small-Lot Vesting Tentative Subdivision Map. ▪ Westwood Drive (Savannah Parkway to the interface of Mangini Ranch Phase 1, including the Alder Creek Tributary creek crossing) <ul style="list-style-type: none"> • One lane of travel in each direction • Frontage improvements on Westwood Drive including curb, gutter, sidewalk, landscaping, medians along with remainder of paving required to complete the roadway Sections U, V1 and V-2 as shown on the Small-Lot Vesting Tentative Subdivision Map 	G, I, M	CD (E), EWR, PW, FD	<p>Roadway improvements to Savannah Parkway have been constructed with the Village 1, 2 and 7 subdivision improvement plans. These improvements have been completed and accepted by the City in the Spring of 2022.</p> <p>Additional roadway improvements to Savannah Parkway east of the westerly Village 2 subdivision boundary to the intersection of Grand Prairie Road have been approved by the City and are currently under construction. These roadway improvements will provide vehicular access to Village 3 and will be complete in the Fall of 2022.</p>	Yes

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CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
18. Cont.	<ul style="list-style-type: none"> ▪ Westwood Drive (Alder Creek Parkway to Old Ranch Way) <ul style="list-style-type: none"> • One lane of travel in each direction (The Enclave at Folsom Ranch Subdivision project (PN 16-025) is currently conditioned to complete these improvements) • Intersection of Alder Creek Parkway and Westwood Drive including turn lanes <ul style="list-style-type: none"> ○ Control: All-Way Stop-Sign control at the intersection of Westwood Drive and Alder Creek Parkway with full access. ○ Westbound Approach to Westwood Drive from Alder Creek Parkway: One shared through/right-turn lane, and one left-turn lane with a 125-foot long taper and 60 feet of vehicle storage. ○ Northbound Approach to Alder Creek Parkway from Westwood Drive: One shared through/right-turn lane, and one left-turn lane with a 125-foot long taper and 210 feet of vehicle storage. A 60-foot-long taper will be provided for the right-turn movement. ○ Eastbound Approach to Westwood Drive from Alder Creek Parkway: One through lane, one right-turn lane and one left-turn lane. • Intersection of Old Ranch Way and Westwood Drive including turn lanes <ul style="list-style-type: none"> ○ Eastbound Approach to Westwood Drive from Old Ranch Way: One shared through/right-turn lane, and one left-turn lane with a 125-foot long taper and 60 feet of vehicle storage. ○ Southbound Approach to Old Ranch Way from Westwood Drive: One shared through/right-turn lane, and one left-turn lane with a 125-foot long taper and 60 feet of vehicle storage. • Frontage Improvements on Westwood Drive <ul style="list-style-type: none"> ○ Frontage Improvements including curb, gutter, sidewalk, median, and landscaping along with the remainder of paving required to complete the roadway section K2 as shown on the Small-Lot Vesting Tentative Subdivision Map. ▪ Old Ranch Way (East Bidwell Street to Westwood Drive) <ul style="list-style-type: none"> • One lane of travel in each direction (The Enclave at Folsom Ranch Subdivision project (PN 16-025) is currently conditioned to complete these improvements) • Frontage improvements along the southern side including curb, gutter and sidewalk and landscaping along with remainder of paving required to complete the roadway Section S and S1 as shown on the Small-Lot Vesting Tentative Subdivision Map. 	G, I, M	CD (E), EWR, PW, FD	<p>Alder Creek Parkway is not required to provide access to the Village 3 subdivision.</p> <p>Roadway improvements to provide access to the Village 3 subdivision have been constructed with the approved Village 1, 2 and 7 improvement plans and the approved Enclave at Folsom Ranch Off-site Improvement Plans. These roadway improvements included East Bidwell Street, Old Ranch Way, Westwood Drive and Savannah Parkway. These improvements have been constructed and have been accepted by the City in the Spring of 2022.</p>	Yes

**CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307)
WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50
SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP**

Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
18. Cont.	<ul style="list-style-type: none"> o Utility Infrastructure <ul style="list-style-type: none"> ▪ The utilities shall be constructed concurrent with the roadway phasing, as deemed appropriate and necessary to support the particular phase by the City Engineer. ▪ A particular development phase may be developed into sub-phases in which the roadway and utility phasing may change. If sub-phasing is proposed, the City Engineer shall determine what roadway and utility improvements are appropriate and necessary to serve the sub-phase. ▪ Hydromodification Basins No. 19 and No. 23 shall be constructed with Phase 1, unless already constructed by Others. o Trails in Open Space <ul style="list-style-type: none"> ▪ No trails within Phase 1 o Park Grading <ul style="list-style-type: none"> ▪ Community Park East will serve as a spoils site during Phase 1 grading. Grading of the park will be completed in subsequent phases. 	G, I, M	CD (E), EWR, PW, FD	<p>Underground water, sanitary sewer, storm drainage and public utility infrastructure will be constructed as a part of the Savannah Parkway roadway construction extension which are required to serve Village 3, Hydro-modification Basin 19 has been included in the Enclave Offsite improvements plans and is required to be completed prior to the issuance of any building permits in Village 3.</p> <p>There are no trails being constructed in the open space adjoining the Village 3 subdivision. The trails are being rough graded by the Subdivider and the City will construct the trails in the future.</p> <p>The owner/applicant commenced with grading the entire Mangini Ranch Phase 2 subdivision in the Spring of 2019. Throughout the grading in the future years, excess spoils will be placed in the future Community Park East.</p>	Yes

<p align="center">CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP</p>					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
19.	<p>Development Phase 2 (Villages 3, 4, 8, and Lots C & D) Plan Developer shall construct the following improvements as shown on the Mangini Ranch Phase 2 Vesting Tentative Subdivision Map & Conceptual Development Phasing Diagram unless otherwise noted; Roadways shall be constructed to the ultimate horizontal and vertical alignment unless otherwise noted.</p> <ul style="list-style-type: none"> o Roads <ul style="list-style-type: none"> ▪ Savannah Parkway (Eastern edge of Open Space Lots I and L to SMUD Substation) <ul style="list-style-type: none"> • One lane of travel in each direction and median • Village 3 Entrance/Exit on Savannah Parkway <ul style="list-style-type: none"> o Control: Stop-Sign control at the Village 3 exit to Savannah Parkway. o Eastbound left-turn lane from Savannah Parkway into Village 3 entrance with 125-foot-long taper and 60 feet of vehicle storage. o Frontage improvements along the northern side of Savannah Parkway including curb, gutter and sidewalk, median, and landscaping along with remainder of paving required to complete the roadway Section K as shown on the Small-Lot Vesting Tentative Subdivision Map. o Park Frontage improvements are the responsibility of the City. ▪ Westwood Drive (Savannah Parkway to Old Ranch Way) <ul style="list-style-type: none"> • One lane of travel in each direction • Intersection of Old Ranch Way and Westwood Drive including turn lanes <ul style="list-style-type: none"> o Northbound Approach to Old Ranch Way from Westwood Drive: One shared through/right-turn lane, and one left-turn lane with a 125-foot long taper and 60 feet of vehicle storage. A 60-foot-long taper will be provided for the right-turn movement. o Westbound Approach to Westwood Drive from Old Ranch Way: One shared through/right-turn lane, and one left-turn lane with a 125-foot long taper and 60 feet of vehicle storage. • Intersection of Savannah Parkway and Westwood Drive including turn lanes <ul style="list-style-type: none"> o Southbound Approach to Savannah Parkway from Westwood Drive: One shared through/right-turn lane, and one left-turn lane with a 125-foot long taper and 90 feet of vehicle storage. A 60-foot-long taper will be provided for the right-turn movement. 	G, I, M	CD (E), EWR, PW, FD	<p>The improvement plans for the roadway improvements for Savannah Parkway from the easterly boundary of Village 2 to Grand Prairie Road necessary to serve Village 3 have been reviewed and approved by the City. These roadway improvements are currently under construction and are expected to be complete in the Fall of 2022.</p> <p>The roadway improvements to Westwood Drive between Savannah Parkway and Old Ranch Way were included in the approved improvement plans for Village 2. These improvements commenced in the Summer of 2020 and have been completed and accepted by the City in the Spring of 2022.</p> <p>The required improvements to Old Ranch Way and Alder Creek Parkway in this condition have been constructed and accepted by the City. These roadway improvements were required as a condition of approval for the Village 4 & 8 subdivisions approved by the City Council in July of 2020.</p>	Yes

**CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307)
WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50
SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP**

Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
19. Cont.	<ul style="list-style-type: none"> • Frontage Improvements on Westwood Drive <ul style="list-style-type: none"> ○ Frontage improvements along the western side of Westwood Drive including curb, gutter and sidewalk, median, and landscaping along with remainder of paving required to complete the roadway Section K as shown on the Small-Lot Vesting Tentative Subdivision Map. ▪ Old Ranch Way (Westwood Drive to Village 4) <ul style="list-style-type: none"> • One lane of travel in each direction • Frontage improvements including curb, gutter, sidewalk, median and landscaping along with remainder of paving required to complete the roadway Section V-4 as shown on the Small-Lot Vesting Tentative Subdivision Map • Park & School frontage including curb, gutter, and 10-foot-wide sidewalk with remainder of paving required to complete the roadway Section V-4 as shown on the Small-Lot Vesting Tentative Subdivision Map. Park frontage improvements are the responsibility of the City. ▪ Alder Creek Parkway (Westwood Drive to Placerville Road) <ul style="list-style-type: none"> • One lane of travel in each direction and median (Existing travel lanes on Alder Creek Parkway constructed with FPA Phase 1 Backbone Improvements) • Village 8 Entrance/Exit on Savannah Parkway <ul style="list-style-type: none"> ○ Westbound left-turn lane from Alder Creek Parkway into Village 8 entrance with 125-foot-long taper and 60 feet of vehicle storage. • Intersection of East Bidwell Street and Alder Creek Parkway (as shown on Attachment 11/Off-Site Infrastructure Triggers, December-2017/Required Prior to 236th Building Permit) <ul style="list-style-type: none"> ○ Control: Signalize with a protected southbound East Bidwell Street left-turn, westbound Alder Creek Parkway split phasing, and westbound Alder Creek Parkway right-turn overlap. U-Turns prohibited. ○ Southbound Approach to Alder Creek Parkway from East Bidwell Street: One thru-lane, and two left-turn lanes, with a 300-foot-long single lane left-turn pocket excluding tapers for the most easterly of the left turning lanes. ○ Northbound Approach to Alder Creek Parkway from East Bidwell Street: One thru lane and one shared thru/right-turn lane with a striped 500-foot long right-turn pocket excluding tapers for the shared thru/right-turn lane. 	G, I, M	CD (E), EWR, PW, FD	<p>The improvement plans for the roadway improvements for Savannah Parkway from the easterly boundary of Village 2 to Grand Prairie Road necessary to serve Village 3 have been reviewed and approved by the City. These roadway improvements are currently under construction and are expected to be complete in the Fall of 2022.</p> <p>The roadway improvements to Westwood Drive between Savannah Parkway and Old Ranch Way were included in the approved improvement plans for Village 2. These improvements commenced in the Summer of 2020 and have been completed and accepted by the City in the Spring of 2022.</p> <p>The required improvements to Old Ranch Way and Alder Creek Parkway in this condition have been constructed and accepted by the City. These roadway improvements were required as a condition of approval for the Village 4 & 8 subdivisions approved by the City Council in July of 2020.</p>	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
19. Cont.	<ul style="list-style-type: none"> ○ Westbound Approach to East Bidwell Street from Alder Creek Parkway: One right-turn lane and one left-turn lane, with a 200-foot left-turn pocket excluding tapers for the left-turn lane. ○ Eastbound Alder Creek Parkway Departure: Two receiving lanes shall be provided, the second receiving lane shall be dropped after 300 feet excluding tapers. ○ East Bidwell Street shall be constructed as a four-lane divided arterial between Alder Creek Parkway and the U.S. Highway 50 Interchange, with a 38-foot-wide median at Alder Creek Parkway that tapers back to match the existing four-lane arterial segment at the eastbound U.S. Highway 50 slip onramp. East Bidwell Street shall be constructed as a two-lane divided arterial between Alder Creek Parkway and Old Ranch Way, with a 38-foot-wide raised median at Alder Creek Parkway that tapers back to match the two-lane half segment. Alder Creek Parkway between East Bidwell Street and Westwood Drive shall be constructed as a two-lane divided roadway with a 38-foot-wide raised median. ● Frontage Improvements on Alder Creek Parkway <ul style="list-style-type: none"> ○ Frontage improvements along the southern side of Alder Creek Parkway including curb, gutter, sidewalk, median, and landscaping along with remainder of paving required to complete the roadway Section C1 as shown on the Small-Lot Vesting Tentative Subdivision Map. ■ Intersection of East Bidwell Street and White Rock Road <ul style="list-style-type: none"> ● Prior to issuance of the 281st building permit, the owner/applicant shall be responsible for either Option A or Option B below as follows: <ul style="list-style-type: none"> ○ Option A: The Capital Southeast Connector Joint Powers Authority (JPA) project proposes to relocate and signalize the East Bidwell Street/White Rock Road intersection: If the proposed JPA project at this location is fully funded and construction is underway by the time the 281st building permit is issued, the project shall pay the Sacramento County Transportation Development Fees, toward the JPA project. 	G, I, M	CD (E), EWR, PW, FD	The required improvements to the intersection of East Bidwell Street and White Rock Road have been fully funded by the Connector JPA are currently under construction. It is anticipated that the new signal and the improvements at the intersection will be complete and operational in the Spring of 2022. This condition is therefore satisfied.	Yes

**CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307)
WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50
SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP**

Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
19. Cont.	<ul style="list-style-type: none"> ○ Option B: Signalize the existing East Bidwell Street/White Rock Road intersection with Mangini Ranch Phase 1 improvements: If the JPA project to relocate and signalize the East Bidwell Street/White Rock Road intersection is not fully funded and under construction prior to issuances of the 281st building permit, the owner/applicant shall be responsible to signalize the existing intersection with improvements described in Condition No. 127 of the Mangini Ranch Phase 1 conditions of approval. Mangini Ranch Phase 1 improvements at this location consist of “Southbound on East Bidwell Street construct a free southbound right turn lane consisting of 315 feet of deceleration length plus 50 feet storage length, excluding appropriate tapers and a 300 foot receiving /acceleration lane, excluding tapers along westbound White Rock Road. Westbound on White Rock Road, construct a free right-turn lane consisting of 315 feet of deceleration length plus 50 feet of storage length, excluding appropriate tapers, and a 300-foot receiving lane excluding appropriate tapers along northbound East Bidwell Street. ○ The JPA currently has more than seven million dollars programed toward relocation and signalization of the East Bidwell Street/White Rock Road intersection, and is planning to begin acquiring right-of-way during the winter of 2018, and begin construction during the summer of 2019. The projected absorption Schedule for the Mangini Ranch Phase 2 project estimates that the 281 dwelling units will not be constructed until sometime in the second quarter of 2020. Option A above is the preferred improvement, Option B would be a throwaway improvement. ○ Utility Infrastructure <ul style="list-style-type: none"> ▪ The utilities shall be constructed concurrent with the roadway phasing, as deemed appropriate and necessary to support the particular phase by the City Engineer ▪ A particular development phase may be developed into sub-phases in which the roadway and utility phasing may change. If sub-phasing is proposed, the City Engineer shall determine what roadway and utility improvements are appropriate and necessary to serve the sub-phase ○ Trails in Open Space <ul style="list-style-type: none"> ▪ The proposed trail within Lot I open space will be graded with Village 3 ▪ The proposed trail within Lot H open space adjacent to the school will be graded with Village 4 ○ Park Grading <ul style="list-style-type: none"> ▪ Community Park East will serve as a spoils site during Phase 2 grading. Grading of the park will be completed in subsequent phases. 	G, I, M	CD (E), EWR, PW, FD	<p>The required improvements to the intersection of East Bidwell Street and White Rock Road have been fully funded by the Connector JPA are currently under construction. It is anticipated that the new signal and the improvements at the intersection will be complete and operational in the Spring of 2022. This condition is therefore satisfied.</p> <p>Underground water, sanitary sewer, storm drainage and public utility infrastructure will be constructed as a part of the Savannah Parkway extension roadway construction to serve Village 3, Hydro-modification Basin 19 has been included in the Enclave Offsite improvements plans and is required to be completed prior to the issuance of any building permits in Village 3.</p> <p>There are no trails being constructed in the open space adjoining the Village 3 subdivision. The trails are being rough graded by the Subdivider and the City will construct the trails in the future.</p> <p>The owner/applicant commenced with grading the entire Mangini Ranch Phase 2 subdivision in the Spring of 2019. Throughout the grading in the future years, excess spoils will be placed in the future Community Park East.</p>	Yes

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CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
20.	<p>Development Phase 3 (Villages 5 and 6, and Lots B, F, and G) Developer shall construct the following improvements as shown on the Mangini Ranch Phase 2 Vesting Tentative Subdivision Map & Conceptual Development Phasing Diagram unless otherwise noted; Roadways shall be constructed to the ultimate horizontal and vertical alignment unless otherwise noted.</p> <ul style="list-style-type: none"> o Roads <ul style="list-style-type: none"> ▪ East Bidwell Street (Savannah Parkway to the Alder Creek Parkway) <ul style="list-style-type: none"> • Intersection of Savannah Parkway and East Bidwell Street including turn lanes (as shown on Attachment 11/Off-Site Infrastructure Triggers, December-2017/Required Prior to 496th Building Permit) <ul style="list-style-type: none"> o Control: Signal control with split phasing. o Southbound Approach to Savannah Parkway from East Bidwell Street: One thru-lane, and one left-turn lane with a 100-foot-long left-turn pocket excluding tapers for the left-turn lane. o Northbound Approach to Savannah Parkway from East Bidwell Street: One shared thru/right-turn lane. o Westbound Approach to East Bidwell Street from Savannah Parkway: One right-turn lane, and one left-turn lane with a 60-foot left-turn pocket excluding tapers for the left-turn lane. o Between Old Ranch Way and the southern boundary of the project site, East Bidwell Street shall be constructed as a two-lane arterial on the eastern “half-segment” of its ultimate configuration. This two-lane segment shall have a striped 2-foot-wide median south of Old Ranch Way, consistent with the California Manual on Traffic Control Devices (MUTCD) Figure 3A-107 (CA), or similar standard. The southbound left-turn pocket shall be developed in accordance with the Highway Design Manual (HDM) Figure 405.2A, or similar standard. Savannah Parkway shall have a 4-foot-wide raised median. • Frontage improvements including curb, gutter, sidewalk, median, and landscaping along with remainder of paving required to complete the roadway Section I as shown on the Small-Lot Vesting Tentative Subdivision Map. 	G, I, M	CD (E), EWR, PW, FD	<p>The improvement plans for the roadway improvements for Savannah Parkway from the easterly boundary of Village 2 to Grand Prairie Road necessary to serve Village 3 have been reviewed and approved by the City. These roadway improvements are currently under construction and are expected to be complete in the Fall of 2022.</p> <p>The roadway improvements to Westwood Drive between Savannah Parkway and Old Ranch Way were included in the approved improvement plans for Village 2. These improvements commenced in the Summer of 2020 and have been completed and accepted by the City in the Spring of 2022.</p> <p>The required improvements to Old Ranch Way and Alder Creek Parkway in this condition have been constructed and accepted by the City. These roadway improvements were required as a condition of approval for the Village 4 & 8 subdivisions approved by the City Council in July of 2020.</p>	Yes

**CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307)
WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50
SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP**

Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
20. Cont.	<ul style="list-style-type: none"> o Savannah Parkway (SMUD Substation to Grand Prairie Road) o One lane of travel in each direction and a landscaped median of varying widths. o Intersection of Savannah Parkway and Grand Prairie Road including turn lanes o Northbound approach to Grand Prairie Road from Savannah Parkway: One shared through/right-turn/left-turn lane. o Southbound approach to Grand Prairie Road from Savannah Parkway: One shared through/right-turn lane and one left-turn lane. The left-turn lane will have a 125-foot-long taper with 60 feet of vehicle storage. <ul style="list-style-type: none"> ▪ Westbound approach to Savannah Parkway from Grand Prairie Road: One shared right-turn lane and one left-turn lane. o Frontage improvements along the northern side of Savannah Parkway including curb, gutter, sidewalk, median, and landscaping along with remainder of paving required to complete the roadway Section K as shown on the Small-Lot Vesting Tentative Subdivision Map. o Park frontage improvements are the responsibility of the City. o Westwood Drive (Alder Creek Parkway to Placerville Road) o One lane of travel in each direction with median o Intersection of Alder Creek Parkway and Westwood Drive including turn lanes o Southbound Approach to Alder Creek Parkway from Placerville Road: One shared through/right-turn lane, and one left-turn lane with a 125-foot long taper and 60 feet of vehicle storage. o Intersection of Alder Creek Parkway and Placerville Road including turn lanes o Southbound Approach to Placerville Road from Alder Creek Parkway: One right-turn lane. <ul style="list-style-type: none"> ▪ Eastbound Approach to Placerville Road from Alder Creek Parkway: One shared through/left-turn lane. o Village 6 Entrance/Exit on Westwood Drive <ul style="list-style-type: none"> o Control: Stop-Sign control at the Village 6 exit to Westwood Drive. Northbound Approach to Village 6 entrance from Westwood Drive: o One shared through/right-turn lane. Southbound Approach to Village 6 entrance from Westwood Drive: One shared through/left-turn lane o Frontage improvements including curb, gutter, sidewalk, median, and landscaping along with remainder of paving required to complete the roadway Sections U, U-2, and V1 as shown on the Small-Lot Vesting Tentative Subdivision Map. 	G, I, M	CD (E), EWR, PW, FD	<p>The improvement plans for the roadway improvements for Savannah Parkway from the easterly boundary of Village 2 to Grand Prairie Road necessary to serve Village 3 have been reviewed and approved by the City. These roadway improvements are currently under construction and are expected to be complete in the Fall of 2022.</p> <p>The roadway improvements to Westwood Drive between Savannah Parkway and Old Ranch Way were included in the approved improvement plans for Village 2. These improvements commenced in the Summer of 2020 and have been completed and accepted by the City in the Spring of 2022.</p> <p>The required improvements to Old Ranch Way and Alder Creek Parkway in this condition have been constructed and accepted by the City. These roadway improvements were required as a condition of approval for the Village 4 & 8 subdivisions approved by the City Council in July of 2020.</p>	Yes

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CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
20. Cont.	<ul style="list-style-type: none"> ▪ Alder Creek Parkway (Westwood Drive to Placerville Road) <ul style="list-style-type: none"> • Village 5 Entrance/Exit on Alder Creek Parkway <ul style="list-style-type: none"> ○ Control: Stop-Sign control at the Village 5 exit to Alder Creek Parkway. ○ Eastbound Approach to Village 5 entrance from Alder Creek Parkway: Left-turn lane with 125-foot-long taper and 60 feet of vehicle storage. ○ Frontage improvements along the northern side of Alder Creek Parkway including curb, gutter, sidewalk, median, and landscaping along with remainder of paving required to complete the roadway Section C1 as shown on the Small-Lot Vesting Tentative Subdivision Map. ▪ Intersection of Placerville Road and White Rock Road <ul style="list-style-type: none"> • Prior to the 496th building permit, the owner/applicant shall be responsible for prohibiting southbound left-turns from Placerville Road to eastbound White Rock Road by construction of a raised median on Placerville Road to channelize all southbound traffic onto westbound White Rock Road. ○ Utility Infrastructure <ul style="list-style-type: none"> ▪ The utilities shall be constructed concurrent with the roadway phasing, as deemed appropriate and necessary to support the particular phase by the City Engineer ▪ A particular development phase may be developed into sub-phases in which the roadway and utility phasing may change. If sub-phasing is proposed, the City Engineer shall determine what roadway and utility improvements are appropriate and necessary to serve the sub-phase ○ Trails in Open Space <ul style="list-style-type: none"> ▪ The proposed trail within Community Park East, including the connection with Mangini Ranch Phase 1, will be graded with Phase 3 ○ Park Grading <ul style="list-style-type: none"> ▪ Community Park East grading will be completed. 	G, I, M	CD (E), EWR, PW, FD	These improvements to East Bidwell Street, Alder Creek Parkway and the intersection of Placerville Road and White Rock Road in this condition are not required to provide access and utilities to the Village 3 subdivision. The required improvements to Placerville Road at the intersection of White Rock Road will be completed with future phases in the Mangini Ranch Phase 2 subdivision	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
21.	Off-site improvements / Rights of Entry For any improvements constructed on private property that are not under the ownership or control of the owner/applicant, all rights-of-entry, and if necessary, a permanent easement shall be obtained and provided to the City. All rights of entry, construction easements, either permanent or temporary and other easements shall be obtained as set forth in Amendments No. 1 and 2 to ARDA, which shall be fully executed by all affected parties and shall be recorded with the Sacramento County Recorder, where applicable, prior to approval of grading and/or improvement plans.	G	CD (E)	The owner/applicant obtained all Rights of Entry, construction easements and grant deeds for all off-site improvements prior to commencement of grading. All rights-of-entry are on file with the Community Development Department.	Yes
22.	Mine Shaft Remediation The owner/applicant shall locate and remediate all antiquated mine shafts, drifts, open cuts, tunnels, and water conveyance or impoundment structures existing on the project site, with specific recommendations for the sealing, filling, or removal of each that meet all applicable health, safety and engineering standards. Recommendations shall be prepared by an appropriately licensed engineer or geologist. All remedial plans shall be reviewed and approved by the City prior to approval of grading plans.	G	CD (E)	During the course of grading and construction for this subdivision no mine shafts or tunnels were located or discovered.	Yes
23.	Prepare Traffic Control Plan. Prior to construction, a Traffic Control Plan for roadways and intersections affected by construction shall be prepared. The Traffic Control Plan shall designate haul routes and comply with requirements in the encroachment permits issued by the City of Rancho Cordova, Sacramento County, and Caltrans and any other local agencies, including but not limited to the City, if applicable. The Traffic Control Plan to be prepared by the project construction contractor(s) shall, at minimum, include the following measures: <ul style="list-style-type: none"> • Maintaining the maximum amount of travel lane capacity during non-construction periods, possible, and advanced notice to drivers through the provision of construction signage. • Maintaining alternate one-way traffic flow past the lay down area and site access when feasible. • Heavy trucks and other construction transport vehicles shall avoid the busiest commute hours (7 a.m. to 8 a.m. and 5 p.m. to 6 p.m. on weekdays). • A minimum 72-hour advance notice of access restrictions for residents, businesses, and local emergency response agencies. This shall include the identification of alternative routes and detours to enable for the avoidance of the immediate construction zone. • A phone number and community contact for inquiries about the schedule of the construction throughout the construction period. This information will be posted in a local newspaper, via the City's web site, or at City Hall and will be updated on a monthly basis. 	G	CD (E)	The owner/applicant has jointly participated with the adjacent property owner (Enclave at Folsom Ranch) for the traffic control, planning and construction of roadway improvements to East Bidwell Street, Alder Creek Parkway, Savannah Parkway and Placerville Road. The improvements to Alder Creek Parkway, East Bidwell Street, Savannah Parkway, Old Ranch Way and Westwood Drive are expected to be completed in the Spring of 2022.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
24.	State and Federal Permits The owner/applicant shall obtain all required State and Federal permits and provide evidence that said permits have been obtained, or that the permit is not required, subject to staff review prior to approval of any grading or improvement plan.	G, I	CD (P)(E)	The owner/applicant has obtained all required State and Federal permits and copies are available from the Community Development Department	Yes
25.	Water Quality Certification A water quality certification pursuant to Section 401 of the Clean Water Act is required before issuance of the record of decision and before issuance of the Section 404 permit. Before construction in any areas containing wetland features, the owner/applicant shall obtain water quality certification for the project. Any measures required as part of the issuance of water quality certification shall be implemented pursuant to the permit conditions.	G	CD (E)	The City and/or the Owner obtained a Section 401 Water Quality certification for the backbone and project specific improvements. All required measures were implemented prior to grading and construction in the Spring of 2019.	Yes
26.	Landslide /Slope Failure The owner/applicant shall retain an appropriately licensed engineer during the grading activities to identify existing landslides and potential slope failure hazards. The said engineer shall be notified a minimum of two days prior to any site clearing or grading to facilitate meetings with the grading contractor in the field.	G	CD (E) PW	The owner/applicant retained a geotechnical engineer and implemented recommendations for this mitigation measure. A geotechnical report outlining these recommendations is on file with Community Development Department.	Yes
IMPROVEMENT PLAN REQUIREMENTS					
27.	Improvement Plans The improvement plans for the required public and private subdivision improvements necessary to serve any and all phases of development shall be reviewed and approved by the Community Development Department prior to approval of a Final Map.	M	CD (E)	The Community Development Department has reviewed and approved the improvement plans for this subdivision.	Yes
28.	Standard Construction Specifications and Details Public and private improvements, including roadways, curbs, gutters, sidewalks, bicycle lanes and trails, streetlights, underground infrastructure and all other improvements shall be provided in accordance with the latest edition of the City of Folsom <i>Standard Construction Specifications and Details</i> and the <i>Design and Procedures Manual and Improvement Standards</i> .	I	CD (P)(E)	The Community Development Department has reviewed and approved the improvement plans for this subdivision. The improvement plans and the required improvements are being constructed in accordance with the current City Standards and Specifications.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
29.	<p><i>Water and Sewer Infrastructure</i></p> <p>All City-owned water and sewer infrastructure shall be placed within the street right of way. In the event that a City-maintained public water or sewer main needs to be placed in an area other than the public right of way, such as through an open space corridor, landscaped area, etc., the following criteria must be met;</p> <ul style="list-style-type: none"> • The owner/applicant shall provide public sewer and water main easements • An access road shall be designed and constructed to allow for the operations, maintenance and replacement of the public water or sewer line by the City along the entire water and/or sewer line alignment. • In no case shall a City-maintained public water or public sewer line be placed on private residential property. • The domestic water and irrigation system owned and maintained by the City shall be separately metered per City of Folsom <u><i>Standard Construction Specifications and Details</i></u>. 	I	CD (E)	The owner/applicant has installed all sewer and water infrastructure within the street right of way or within existing public water, sewer and public utility easements for this subdivision.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
30.	<p>Lighting Plan The owner/applicant of all project phases shall submit a lighting plan for the project to the Community Development Department. The lighting plan shall be consistent with the Folsom Ranch Central District Design Guidelines:</p> <ul style="list-style-type: none"> Shield or screen lighting fixtures to direct the light downward and prevent light spill on adjacent properties; Place and shield or screen flood and area lighting needed for construction activities, nighttime sporting activities, and/or security so as not to disturb adjacent residential areas and passing motorists; For public lighting in residential neighborhoods, prohibit the use of light fixtures that are of unusually high intensity or that blink or flash; Use appropriate building materials (such as low-glare glass, low-glare building glaze or finish, neutral, earthtoned colored paint and roofing materials), shielded or screened lighting, and appropriate signage in the office/commercial areas to prevent light and glare from adversely affecting motorists on nearby roadways; and Design exterior on-site lighting as an integral part of the building and landscaping design in the Specific Plan Area. Lighting fixtures shall be architecturally consistent with the overall site design. Lights used on signage should be directed to light only the sign face with no off site glare. 	I	CD (P)	The owner/applicant submitted a Lighting Plan for all backbone roadways and subdivisions in accordance with the Design Guidelines and City Standards for Street Lighting. A copy of the lighting plans are available from the Community Development Department.	Yes
31.	<p>Utility Coordination The owner/applicant shall coordinate the planning, development and completion of this project with the various utility agencies (i.e., SMUD, PG&E, etc.). The owner/applicant shall provide the City with written confirmation of public utility service prior to approval of all final maps.</p>	M	CD (P)(E)	The owner/applicant has coordinated with all public utilities that will provide service to the subdivision. Bonding for the construction of the joint trench facilities to serve this subdivision are provided in the subdivision improvement agreement for this subdivision.	Yes
32.	<p>Replacing Hazardous Facilities The owner/applicant shall be responsible for replacing any and all damaged or hazardous public sidewalk, curb and gutter, and/or bicycle trail facilities along the site frontage and/or boundaries, including pre-existing conditions and construction damage, to the satisfaction of the Community Development Department.</p>	I, OG	CD (E)	No existing improvements were existing as part of this subdivision.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
33.	Future Utility Lines All future utility lines lower than 69 KV that are to be built within the project, shall be placed underground within and along the perimeter of the project at the developer's cost. The owner/applicant shall dedicate to SMUD all necessary underground easements for the electrical facilities that will be necessary to service development of the project.	B	CD (E)	All utility lines below 69 kV have been designed to be placed underground and Public Utility Easements have been dedicated on the final map for this subdivision.	Yes
34.	Water Meter Fixed Network System The owner owner/applicant shall pay for, furnish and install all infrastructure associated with the water meter fixed network system for any City-owned and maintained water meter within the project.	I	CD (E), EWR	The Owner/applicant has completed the infrastructure allowing for the water meter fixed network system. Meters will be furnished and installed during home construction for each individual metered connection.	Yes
35.	Vertical Curb All curbs located adjacent to landscaping, whether natural or manicured, and where parking is allowed shall be vertical.	I	CD (P)(B)	The improvement plans for the subdivision improvements and backbone roadways provide vertical curbing as required.	Yes
36.	Class II Bike Lanes All Class II bike lanes shall be striped and the legends painted green. No parking shall be permitted within the Class II bike lanes.	I	CD (E)(P)	All Class II bike lanes have been constructed in accordance with the Specific Plan, Design Guidelines and City Standards and Specifications.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
37.	<p>Noise Barriers Based on the Environmental Noise Assessment prepared by Bollard Acoustical Consultants on August 23, 2017, the following measures shall be implemented to the satisfaction of the Community Development Department:</p> <ul style="list-style-type: none"> • Solid noise barriers or similar natural features (earthen berm, etc.) shall be required to reduce future traffic noise levels to below the City of Folsom exterior criteria of 60 dB Ldn at the proposed residential backyards. Barrier heights are specified relative to backyard elevations. The following barrier heights and locations are required to the satisfaction of the Community Development Department (see Figure 2 in the August 23, 2017 Assessment): <ul style="list-style-type: none"> ○ 6-foot noise barrier at residences adjacent to Highway 50 ○ 10-foot noise barrier at residences adjacent to East Bidwell Street ○ 7-foot noise barrier at residences adjacent to Alder Creek Parkway ○ 6-foot noise barrier at residences adjacent to Savannah Parkway • Suitable materials for the traffic noise barriers shall include masonry and precast concrete panels. Other materials may be acceptable but shall be reviewed by an acoustical consultant prior to use. The final design, materials, and colors of the barriers shall be to the satisfaction of the Community Development Department. • Mechanical ventilation (air conditioning) shall be provided for all residences within the Mangini Ranch Phase 2 Subdivision to allow the occupants to close doors and windows as desired to achieve compliance with the applicable interior noise level criteria. • All second-floor windows of residences located adjacent to East Bidwell Street from which the roadway is visible shall have a minimum STC rating of 32. Figure 2 of the Noise Assessment shows the specific lots where upgrades are required. • All second-floor windows of residences located adjacent to Alder Creek Parkway from which the roadway is visible shall have a minimum STC rating of 30. Figure 2 of the Noise Assessment shows the specific lots where upgrades are required. 	I	CD (E)(P)	The owner/applicant has designed the subdivision and sound walls along Savannah Parkway in accordance with these conditions. The sound walls in the required location and height will be constructed as part of the improvements for this subdivision.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
38.	<p><i>Master Plan Updates</i> The City has approved the Folsom Plan Area Storm Drainage Master Plan, Wastewater Master Plan, and Water Master Plan. The owner/applicant shall submit complete updates to the approved master plans, if applicable, for the proposed changes to the master plans as a result of the proposed project. The updates to the master plans for the proposed project shall be reviewed and approved by the City prior to approval of grading and/or improvement plans.</p> <p>The plans shall be accompanied by engineering studies supporting the sizing, location, and timing of the proposed facilities. Improvements shall be constructed in phases as the project develops in accordance with the approved master plans, including any necessary off-site improvements to support development of a particular phase or phases, subject to prior approval by the City. Off-site improvements may include roadways to provide secondary access, water transmission lines or distribution facilities to provide a looped water system, sewer trunk mains and lift stations, water quality facilities, non-potable water pipelines and infrastructure, and drainage facilities including on or off-site detention. No changes in infrastructure from that shown on the approved master plan shall be permitted unless and until the applicable master plan has been revised and approved by the City. Final lot configurations may need to be modified to accommodate the improvements identified in these studies to the satisfaction of the City.</p> <p>The owner/applicant shall provide sanitary sewer, water and storm drainage improvements with corresponding easements, as necessary, in accordance with these studies and the latest edition of the City of Folsom <u><i>Standard Construction Specifications and Details</i></u>, and the <u><i>Design and Procedures Manual and Improvement Standards</i></u>.</p> <p>The storm drainage design shall provide for no net increase in run-off under post-development conditions.</p>	G, I	CD(E), EWR, PW	The owner/applicant has provided updated Master Plans for approval prior to the issuance of a grading permit. Copies of the Master Plans are available from the Community Development Department.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
39.	<p>Best Management Practices</p> <p>The storm drain improvement plans shall provide for “Best Management Practices” that meet the requirements of the water quality standards of the City’s National Pollutant Discharge Elimination System Permit issued by the State Regional Water Quality Control Board.</p> <p>In addition to compliance with City ordinances, the owner/applicant shall prepare a Stormwater Pollution Prevention Plan (SWPPP), and implement Best Management Practices (BMPs) that comply with the General Construction Stormwater Permit from the Central Valley RWQCB, to reduce water quality effects during construction. Detailed information about the SWPPP and BMPs are provided in Chapter 3A.9, “Hydrology and Water Quality.”</p> <p>Each proposed project development shall result in no net change to peak flows into Alder Creek and associated tributaries, or to Buffalo Creek, Carson Creek, and Coyote Creek. The owner/applicant shall establish a baseline of conditions for drainage on-site. The baseline-flow conditions shall be established for 2-, 5-, and 100-year storm events. These baseline conditions shall be used to develop monitoring standards for the stormwater system on the Specific Plan Area. The baseline conditions, monitoring standards, and a monitoring program shall be submitted to USACE and the City for their approval. Water quality and detention basins shall be designed and constructed to ensure that the performance standards, which are described in Chapter 3A.9, “Hydrology and Water Quality,” are met and shall be designed as off-stream detention basins.</p> <p>Discharge sites into Alder Creek and associated tributaries, as well as tributaries to Carson Creek, Coyote Creek, and Buffalo Creek, shall be monitored to ensure that pre-project conditions are being met. Corrective measures shall be implemented as necessary. The mitigation measures will be satisfied when the monitoring standards are met for 5 consecutive years without undertaking corrective measures to meet the performance standard.</p>	G, I	CD (E)	The Owner/Applicant is in compliance with the Storm Water Pollution Prevention Plan (SWPPP) for the subdivision. General notes have been included on the approved grading and public infrastructure plans to address this condition. Compliance has been monitored through construction inspection.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
40.	Litter Control During Construction, the owner/applicant shall be responsible for litter control and sweeping of all paved surfaces in accordance with City standards. All on-site storm drains shall be cleaned immediately before the commencement of the rainy season (October 15).	OG	CD (E)	The owner/applicant has complied with this provision and completed periodic on-site cleaning and sweeping of the project site. Compliance has been monitored through construction inspection	Yes
FIRE DEPT REQUIREMENTS					
41.	Prepare fuel modification plan (FMP). If applicable, the owner/applicant shall submit a Fuel Modification Plan consistent with the FPA Open Space Management Plan to the City for review and preliminary approval from the Fire Code Official prior to any Final and/or Parcel Map. Final approval of the plan by the Fire Code Official shall occur prior to the issuance of a permit for any new construction. A Fuel Modification Plan shall consist of a set of scaled plans showing fuel modification zones indicated with applicable assessment notes, a detailed landscape plan and an irrigation plan. A fuel modification plan submitted for approval shall be prepared by one of the following: a California state licensed landscape architect, or state licensed landscape contractor, or a landscape designed, or an individual with expertise acceptable to the Fire Code Official. The owner/applicant shall obtain off-site easements for the required for the fuel modification buffer. The owner/applicant agree to be responsible for the long-term maintenance of the Fuel Modification Plan. Notification of fuel modification requirements are to be made upon sale to new property owners. Proposed changes to the approved Fuel Modification Plan shall be submitted to the Fire Code Official for approval prior to implementation.	G, I, M, B	CD (P), FD	The owner/applicant has submitted a Fuel Modification Plan (FMP) and the City Fire Department and the Community Development Department has reviewed and approved the plan. The FMP is for all open space areas adjacent to residential land uses adjoining the subdivision.	Yes

**CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307)
WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50
SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP**

Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
42.	<p><i>All-Weather Access and Fire Hydrants</i> The owner/applicant shall provide all-weather access and fire hydrants before combustible materials are allowed on any project site or other approved alternative method as approved by the Fire Code Official/Fire Chief. All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material or vertical construction is allowed on any project site or other approved alternative method as approved by the Fire Code Official/Fire Chief. (All-weather access is defined as six inches of compacted aggregate base from May 1 to September 30 and two inch asphalt concrete over six inch aggregate base from October to April 30). The building shall have illuminated addresses visible from the street or drive fronting the property. Size and location of address identification shall be reviewed and approved by the Fire Marshal.</p> <ul style="list-style-type: none"> • Commercial Fire-Flow with Automatic Fire Sprinkler System: The required fire-flow for the general commercial portion of the project is determined to be 750 GPM for three hours. The reduced fire-flow shall not be less than 1,000 GPM for commercial buildings with automatic sprinkler systems per Section 903.1.1 of the CFC, and shall not be less than 1,500 GPM for commercial buildings with automatic sprinkler systems per Section 903.3.1.2 of the CFC. • Residential Fire-Flow with Automatic Fire Sprinkler System: The required fire-flow for the proposed residential portion of the project is determined to be 875 GPM for one hour. • All public streets shall meet City of Folsom Street Standards unless an alternative is specifically included within this approval. • The maximum length of any dead end street shall not exceed 500 feet in accordance with the Folsom Fire Code (unless approved by the Fire Department). Several streets indicated on the plans are dead ends greater than 500 feet. In such cases, a second emergency access will be required. • All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material storage or vertical construction is allowed. All-weather access is defined as 6" of compacted AB from May 1 to September 30 and 2"AC over 6" AB from October 1 to April 30 • The first Fire Station planned for the Folsom Plan Area shall be completed and operational at the time that the threshold of 1,500 occupied homes within the Folsom Plan Area is met. 	G, I, M, B	CD (P), FD	The owner/applicant has designed and received approval for all weather access improvements and fire hydrants for the entire subdivision. Building permits will not be issued prior to these improvements being completed to the satisfaction of the Community Development Department and the Fire Department.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
43.	<p>Landscaping Plans Final landscape plans and specifications shall be prepared by a registered landscape architect and approved by the City prior to the approval of the first building permit or the Small Lot Final Map, whichever occurs first. Said plans shall include all on-site landscape specifications and details, and shall comply with all State and local rules, regulations, Governor's declarations and restrictions pertaining to water conservation and outdoor landscaping.</p> <p>Landscaping shall meet shade requirements as outlined in the Folsom Plan Area Specific Plan where applicable. The landscape plans shall comply and implement water efficient requirements as adopted by the State of California (Assembly Bill 1881) (State Model Water Efficient Landscape Ordinance) until such time the City of Folsom adopts its own Water Efficient Landscape Ordinance at which time the owner/applicant shall comply with any new ordinance. Shade and ornamental trees shall be maintained according to the most current American National Standards for Tree Care Operations (ANSI A-300) by qualified tree care professionals. Tree topping for height reduction, view protection, light clearance or any other purpose shall not be allowed. Specialty-style pruning, such as pollarding, shall be specified within the approved landscape plans and shall be implemented during a 5-year establishment and training period. Landscaping installed in open spaces located between tiers of lots shall be chosen for resistance to fire and limited fuel production. Furthermore, the owner/applicant shall comply with city-wide landscape rules or regulations on water usage. Owner/applicant shall comply with any state or local rules and regulations relating to landscape water usage and landscaping requirements necessitated to mitigate for drought conditions on all landscaping in the Mangini Ranch Phase 2 project.</p>	B, M	CD(P), PW	The owner/applicant will submit the landscape and irrigation plans for this subdivision and the Community Development Department will review and approve the landscape and irrigation plans prior to issuance of the first building permit in the subdivision. The landscape and irrigation improvements are bonded for in the subdivision improvement agreement. In accordance with long established City policy, the landscape and irrigation improvements for the subdivision are required to be complete prior to the first certificate of occupancy in the subdivision. The long established City policy related to building permits and landscape and irrigation issuance and timing is contrary to this condition. However, the Community Development Department will verify compliance with the long established City policy.	Condition will be satisfied prior to issuance of a building permit in the subdivision.
44.	<p>Right of Way Landscaping Landscaping along all road rights of way and in public open space lots shall be installed when the adjoining road or lots are constructed.</p>	I, OG	CD (P), PW	Landscaping and irrigation plans for the Village 3 subdivision frontage on Savannah Parkway and are bonded for the subdivision improvement agreement for Village 3.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
MAP REQUIREMENTS					
45.	<i>Subdivision Improvement Agreement</i> Prior to the approval of any Final Map, the owner/applicant shall enter into a subdivision improvement agreement with the City, identifying all required improvements, if any, to be constructed with each proposed phase of development. The owner/applicant shall provide security acceptable to the City, guaranteeing construction of the improvements.	M	CD (E)	The required subdivision improvement agreement is included as part of the City staff report accompanying the final map for City Council approval. The resolution approving the final map for this subdivision includes a statement authorizing the City Manager to execute the subdivision improvement agreement for the subdivision along with approval of the final map.	Yes
46.	<i>The Final Inclusionary Housing Plan</i> The Final Inclusionary Housing Plan shall be approved by the City Council, and the Inclusionary Housing Agreement approved by the City Attorney shall be executed prior to recordation of the first Small-Lot Final Map for the Mangini Ranch Phase 2 Subdivision.	M	CD (P)(E)	The owner/applicant has executed an Inclusionary Housing Agreement with the City. The agreement allows the owner/applicant to provide an in-lieu fee assigned to each building permit in the subdivision. The in-lieu housing fee will be paid at the time of building permit issuance.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
47.	<p>Department of Real Estate Public Report The owner/applicant shall disclose to the homebuyers in the Department of Real Estate Public Report</p> <ol style="list-style-type: none"> 1) Future public parks and public schools are located in relatively close proximity to the proposed subdivision, and that the public parks may include facilities (basketball courts, a baseball field, softball fields, soccer fields, and playground equipment) that may generate noise impacts during various times, including but not limited to evening and nighttime hours. The owner/applicant shall also disclose that the existing public parks include nighttime sports lighting that may generate lighting impacts during evening and nighttime hours. 2) The soil in the subdivision may contain naturally occurring asbestos and naturally occurring arsenic. 3) The collecting, digging, or removal of any stone, artifact, or other prehistoric or historic object located in public or open space areas, and the disturbance of any archaeological site or historic property, is prohibited. 4) The project site is located within close proximity to the Mather Airport flight path and that overflight noise may be present at various times. 5) That all properties located within one mile of an on- or off-site area zoned or used for agricultural use (including livestock grazing) shall be accompanied by written disclosure from the transferor, in a form approved by the City of Folsom, advising any transferee of the potential adverse odor impacts from surrounding agricultural operations which disclosure shall direct the transferee to contact the County of Sacramento concerning any such property within the County zoned for agricultural uses within one mile of the subject property being transferred. 	M	CD (P) PK	The owner/applicant has provided copies of their proposed CC&R's, which contain provisions in accordance with Items #1-5 listed in this condition of approval. The Community Development Department has reviewed and approved the C.C. & R.'s and verified that they include the required disclosures.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
48.	Public Utility Easements The owner/applicant shall dedicate public utility easements for underground facilities on properties adjacent to the streets. A minimum of twelve and one-half-foot (12.5') wide Public Utility Easements for underground facilities (i.e., SMUD, Pacific Gas and Electric, cable television, telephone) shall be dedicated adjacent to all public street rights-of-way. The owner/applicant shall dedicate additional width to accommodate extraordinary facilities as determined by the City. The width of the public utility easements adjacent to public right of way may be reduced with prior approval from public utility companies.	M	CD (E)	The owner/applicant has dedicated a 12.5' PUE along backbone roadway utility corridors as well as internal streets within the subdivision.	Yes
49.	Final Map Phasing Should multiple Final Maps be filed by the owner/applicant, the phasing of maps shall be to the satisfaction of the Community Development Department.	M	CD (E)	There is no phasing proposed for the Village 3 final map.	Yes
50.	Backbone Infrastructure As provided for in the ARDA and the Amendment No. 1 thereto, the owner/applicant shall provide fully executed grant deeds, legal descriptions, and plats for all necessary Backbone Infrastructure to serve the project, including but not limited to lands, public rights of way, public utility easements, public water main easements, public sewer easements, irrevocable offers of dedication and temporary construction easements. All required easements as listed necessary for the Backbone Infrastructure shall be reviewed and approved by the City and recorded with the Sacramento County Recorder pursuant to the timing requirements set forth in Section 3.8 of the ARDA, and any amendments thereto.	M	CD (E)	The owner/applicant (or its previous owner) provided all necessary public utility easements, grant deeds, offers of dedication or temporary construction easements required to build all of the required Backbone Infrastructure needed to serve the subdivision. These were recorded with Sacramento County Recorder within the Large Lot Final Map or by separate instrument.	Yes
51.	New Permanent Benchmarks The owner/applicant shall provide and establish new permanent benchmarks on the (NAVD 88) datum in various locations within the subdivision or at any other locations in the vicinity of the off-site Backbone Infrastructure as directed by the City Engineer. The type and specifications for the permanent benchmarks shall be provided by the City. The new benchmarks shall be placed by the owner/applicant within 6 months from the date of approval of the vesting tentative subdivision map.	M	CD (E)	The owner/applicant has installed new benchmarks per the direction of the City Engineer. The required benchmarks are in place and currently in use.	Yes
52.	Centralized Mail Delivery Units All Final Maps shall show easements or other mapped provisions for the placement of centralized mail delivery units. The owner/applicant shall provide a concrete base for the placement of any centralized mail delivery unit. Specifications and location of such base shall be determined pursuant to the applicable requirements of the U. S. Postal Service and the City of Folsom Community Development Department, with due consideration for street light location, traffic safety, security, and consumer convenience.	M	CD (E)	The Final Map includes an easement that allows for the construction and maintenance of centralized mail delivery boxes.	Yes

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
53.	Recorded Final Map Prior to the issuance of building permits, the owner/applicant shall provide a digital copy of the recorded Final Map (in AutoCAD format) to the Community Development Department. The exception to this requirement are model homes; subject to approval of the Community Development Department, building permits for model homes only may be issued prior to recording of the Final Map.	B	CD (E)	The Community Development Department will require the copies of the recorded final map to be submitted prior to approval of the first building permit in the subdivision.	Condition will be satisfied prior to issuance of a building permit.
54.	Recorded Final Map Prior to issuance of building permits, the owner/applicant shall provide the Folsom-Cordova Unified School District with a copy of the recorded Final Map.	B	CD (P), FCUSD	The Community Development Department will require the copies of the recorded final map to be submitted to the Folsom-Cordova Unified School District prior to approval of the first building permit in the subdivision. This will be satisfied at the issuance of the first building permit.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
55.	<p><i>Design Review Approval</i> Prior to issuance of a building permit for any residential units within the subdivision, the owner/applicant shall obtain Design Review and/or Planned Development approval from the Planning Commission for all residences to be built within the subdivision. If the architecture is not consistent with the Folsom Ranch Central District Design Guidelines, the owner applicant may modify the plans or apply for a modification to the Design Guidelines to be reviewed by the Planning Commission.</p>	B	CD (P)	The owner/applicant has not submitted a Design Review application at this time. The Planning Commission will evaluate a future application for conformance with the Folsom Ranch Central District Guidelines. The Community Development Department will present the Design Review approval for residential units in this subdivision to the Planning Commission prior to issuance of a building permit.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 2 SUBDIVISION PROJECT (PN 17-307) WEST OF PLACERVILLE ROAD, NORTH OF WHITE ROCK ROAD, EAST OF SCOTT ROAD, AND SOUTH OF US HIGHWAY 50 SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP					
Condition No.	Condition of Approval	When Required	Responsible Department	Comments	Condition Satisfied?
TRAFFIC, ACCESS, CIRCULATION, AND PARKING REQUIREMENTS					
56.	<p><u>East Bidwell Street/Savannah Parkway</u> Prior to issuance of the first building permit, the owner/applicant shall be responsible for configuring the East Bidwell Street/Savannah Parkway Intersection as follows:</p> <ul style="list-style-type: none"> o Southbound Approach to Savannah Parkway from East Bidwell Street: One thru-lane, and one left-turn lane with a 200-foot long transition, 60-foot-long taper, and 100 feet of vehicle storage. o Northbound Approach to Savannah Parkway from East Bidwell Street: One shared thru/right-turn lane. o Westbound Approach to East Bidwell Street from Savannah Parkway: One shared left/right-turn lane, and a striped out left-turn pocket with a 125-foot-long taper and 60 feet of vehicle storage. o Control: Stop-Sign control at the westbound approach to East Bidwell Street from Savannah Parkway with full access. o Between Old Ranch Way and the southern boundary of the project site, East Bidwell Street shall be constructed as a two-lane arterial on the eastern "half segment" of its ultimate configuration. This two-lane segment shall have a striped 2-foot-wide median south of Old Ranch Way, consistent with the California Manual of Uniform Traffic Control Devices (MUTCD) Figure 3A-107 (CA), or similar standard. The southbound left-turn pocket shall be developed in accordance with the Highway Design Manual (HDM) Figure 405-2A, or similar standard. Savannah Parkway shall have a raised median curb. 	B	CD (E)	The owner/applicant has constructed the required intersection improvements and the City has formally accepted these improvements in the Spring of 2022. The required improvements were included on either the approved Enclave at Folsom Ranch Off-Site Improvement Plans or the approved Village 2 Improvement Plans.	Yes

<p>57.</p>	<p><u>East Bidwell Street/Alder Creek Parkway</u> Prior to issuance of the 236th building permit, the owner/applicant shall be responsible for expanding and signalizing the East Bidwell Street/Alder Creek Parkway Intersection as follows:</p> <ul style="list-style-type: none"> o Southbound Approach to Alder Creek Parkway from East Bidwell Street: One thru-lane, and two left-turn lanes, with a 300-foot-long single lane left-turn pocket excluding tapers for the most easterly of the left turning lanes. o Northbound Approach to Alder Creek Parkway from East Bidwell Street: One thru lane and one shared thru/right-turn lane with a striped 500-foot long right-turn pocket excluding tapers for the shared thru/right-turn lane. o Westbound Approach to East Bidwell Street from Alder Creek Parkway: One right-turn lane and one left-turn lane, with a 200-foot left-turn pocket excluding tapers for the left-turn lane. o Eastbound Alder Creek Parkway Departure: Two receiving lanes shall be provided, the second receiving lane shall be dropped after 300 feet excluding tapers. o Control: Signalize with a protected southbound East Bidwell Street left-turn, westbound Alder Creek Parkway split phasing, and westbound Alder Creek Parkway right-turn overlap. U-Turns prohibited. o East Bidwell Street shall be constructed as a four-lane divided arterial between Alder Creek Parkway and the U.S. Highway 50 Interchange, with a 38-foot-wide median at Alder Creek Parkway that tapers back to match the existing four-lane arterial segment at the eastbound U.S. Highway 50 slip onramp. East Bidwell Street shall be constructed as a two-lane divided arterial between Alder Creek Parkway and Old Ranch Way, with a 38-foot-wide raised median at Alder Creek Parkway that tapers back to match the two-lane half segment described in the East Bidwell Street/Savannah Parkway Condition No. 57 above. Alder Creek Parkway between East Bidwell Street and Westwood Drive shall be constructed as a two-lane divided roadway with a 38-foot-wide raised median. 	<p>B</p>	<p>CD (E)</p>	<p>The Community Development Department has reviewed and approved the traffic signal plans for the proposed signal at the intersection of East Bidwell Street and Alder Creek Parkway. The traffic signal is expected to be complete and operational in the Spring of 2022. The Community Development Department is monitoring building permit issuance in the Mangini Ranch Phase 2 development to verify completion of the signal prior to issuance of the 236th building permit.</p>	<p>Condition will be satisfied prior to issuance of the 236th building permit in the Mangini Ranch Phase 2 subdivision.</p>
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<p>58.</p>	<p><u>East Bidwell Street/White Rock Road</u> Prior to issuance of the 281st building permit, the owner/applicant shall be responsible for either Option A or Option B below as follows:</p> <ul style="list-style-type: none"> o Option A: The Capital Southeast Connector Joint Powers Authority (JPA) project proposes to relocate and signalize the East Bidwell Street/White Rock Road intersection: If the proposed JPA project at this location is fully funded and construction is underway by the time the 281st building permit is issued, the project shall pay the Sacramento County Transportation Development Fees, toward the JPA project. o Option B: Signalize the existing East Bidwell Street/White Rock Road intersection with Mangini Ranch Phase 1 improvements: If the JPA project to relocate and signalize the East Bidwell Street/White Rock Road intersection is not fully funded and under construction prior to issuances of the 281st building permit, the owner/applicant shall be responsible to signalize the existing intersection with improvements described in Condition No. 127 of the Mangini Ranch Phase 1 conditions of approval. Mangini Ranch Phase 1 improvements at this location consist of “Southbound on East Bidwell Street construct a free southbound right turn lane consisting of 315 feet of deceleration length plus 50 feet storage length, excluding appropriate tapers and a 300 foot receiving /acceleration lane, excluding tapers along westbound White Rock Road. Westbound on White Rock Road, construct a free right-turn lane consisting of 315 feet of deceleration length plus 50 feet of storage length, excluding appropriate tapers, and a 300 foot receiving lane excluding appropriate tapers along northbound East Bidwell Street. o The JPA currently has more than seven million dollars programed toward relocation and signalization of the East Bidwell Street/White Rock Road intersection, and is planning to begin acquiring right-of-way during the winter of 2018, and begin construction during the summer of 2019. The projected absorption Schedule for the Mangini Ranch Phase 2 project estimates that the 281 dwelling units will not be constructed until sometime in the second quarter of 2020. Option A above is the preferred improvement, Option B would be a throwaway improvement. 	<p>B</p>	<p>CD (E)</p>	<p>The required improvements to the intersection of East Bidwell Street and White Rock Road have been fully funded by the Connector JPA are currently under construction. It is anticipated that the new signal and the improvements at the intersection will be complete and operational in the Spring of 2022. This condition is therefore satisfied.</p>	<p>Yes</p>
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<p>59.</p>	<p><u>White Rock Road/Placerville Road</u> Prior to the 496th building permit, the owner/applicant shall be responsible for prohibiting southbound left-turns from Placerville Road to eastbound White Rock Road by construction of a raised median on Placerville Road to channelize all southbound traffic onto westbound White Rock Road.</p>	<p>B</p>	<p>CD (E)</p>	<p>This condition is not a requirement of this Village 3 subdivision since this subdivision will only increase the number of mapped lots to 422 in the Mangini Ranch Phase 2 subdivision which is below the requirement to complete the improvement prior to issuance of the 496th building permit.</p>	<p>Condition will be satisfied prior to issuance of the 496th building permit in the Mangini Ranch Phase 2 subdivision.</p>
<p>60.</p>	<p><u>East Bidwell Street/Savannah Parkway</u> Prior to issuance of the 496th building permit and concurrent with implementation of Condition 64 above, the owner/applicant shall signalize the East Bidwell Street/Savannah Parkway intersection as follows:</p> <ul style="list-style-type: none"> ○ Southbound Approach to Savannah Parkway from East Bidwell Street: One thru-lane, and one left-turn lane with a 100-foot-long left-turn pocket excluding tapers for the left-turn lane. ○ Northbound Approach to Savannah Parkway from East Bidwell Street: One shared thru/right-turn lane. ○ Westbound Approach to East Bidwell Street from Savannah Parkway: One right-turn lane, and one left-turn lane with a 60-foot left-turn pocket excluding tapers for the left-turn lane. ○ Control: Signal control with split phasing. ○ Between Old Ranch Way and the southern boundary of the project site, East Bidwell Street shall be constructed as a two-lane arterial on the eastern “half-segment” of its ultimate configuration. This two-lane segment shall have a striped 2-foot-wide median south of Old Ranch Way, consistent with the California Manual on Traffic Control Devices (MUTCD) Figure 3A-107 (CA), or similar standard. The southbound left-turn pocket shall be developed in accordance with the Highway Design Manual (HDM) Figure 405.2A, or similar standard. Savannah Parkway shall have a 4-foot-wide raised median. 	<p>B</p>	<p>CD (E)</p>	<p>This condition is not a requirement of this Village 3 subdivision since this subdivision will only increase the number of mapped lots to 422 in the Mangini Ranch Phase 2 subdivision which is below the requirement to complete the improvement prior to issuance of the 496th building permit.</p>	<p>Condition will be satisfied prior to issuance of the 496th building permit in the Mangini Ranch Phase 2 subdivision.</p>

61.	<i>Credit Reimbursement Agreement</i> Prior to the recordation of the first Small-Lot Final Map, the owner/applicant and City shall enter into a credit and reimbursement agreement for constructed improvements that are included in the Folsom Plan Area's Public Facilities Financing Plan.	M	CD (E)	The owner/applicant has entered into a SPIF Credit/Reimbursement Agreements with the City for eligible improvements constructed by the owner/applicant.	Yes
ARCHITECTURE/SITE DESIGN REQUIREMENTS					
62.	<i>Landscaping Plan</i> Owner/applicant shall submit a landscape plan for all areas (by phase or subdivision) of the project where owner/applicant proposes to install landscaping on residential lots. The landscape plan shall take into account the then existing state or local rules and regulations related to landscape water usage and water wise landscape principles. The landscape plans shall be submitted and approved by the Community Development Director prior to the issuance of a building permit in the phase or subdivision. The owner/applicant shall comply with any state or local rules and regulations relating to landscape water usage and landscaping requirements necessitated to mitigate for drought conditions.	B	CD (P) (E)	The Community Development Department will review all future residential site plans in the subdivision to verify compliance with this condition.	Condition will be satisfied prior to issuance of a building permit.
63.	<i>Mechanical Equipment Screening</i> All mechanical equipment shall be concealed from view of public streets, neighboring properties and nearby higher buildings where practicable to the satisfaction of the Community Development Department.	B	CD (P) (E)	The Community Development Department will review all future residential site plans in the subdivision to verify compliance with this condition.	Condition will be satisfied prior to issuance of a building permit.

64.	<p><i>Bicycle Trail System Modifications</i> The owner/applicant shall incorporate the design and grading for the proposed Class I bike trails located within Lot H into the improvement plans consistent with the Mangini Ranch Phase 2 Proposed Trail System Modification Exhibit dated December 15, 2017.</p>	I	CD (P)	<p>The Mangini Ranch Phase 2 Grading Plans approved by the City reflect the modifications to the trail system as shown in trail system modification exhibit dated December 15, 2017. The trail system modifications are adjoining future villages in the Mangini Ranch Phase 2 subdivision.</p>	Yes
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CITY CORPORATION YARD					
65.	<p>The location tentatively identified for acquisition by the City for the City's new Corporation Yard in Section 2.2.3.4 of the First Amended and Restated Tier I Development Agreement (ARDA) between the City and the project developer may not be feasible for use as a Corporation Yard due to challenges in obtaining County entitlements and utility services. Subject to the application of this condition to other Participating Landowners (as defined in the ARDA) as provided herein, the following condition is added with respect to resolution of the location of the corporation yard:</p> <p>Prior to approval of the First Final Small Lot Map in the FPA (or first building permit if development may occur without any subdivision), a site consistent with the requirements of Section 2.2.3.4 of the ARDA, as may be amended or as otherwise agreed to between the City and the Participating Landowners, shall be identified as acceptable to the City as suitable and feasible for use as the new Corporation Yard, with access to sewer, water and all required utility services. The City's determination of feasibility may include the identification of an alternative site consistent with the forgoing, as a back-up for the primary site, as well as an evaluation of the time, cost and likelihood of obtaining any necessary entitlements or other governmental approvals for use of the land as a corporation yard, with the final determination of feasibility subject to the sole and reasonable discretion of the City Council.</p> <p>The City intends to impose this condition equitably throughout the Plan Area as and to each and every Participating Landowner who seeks any future specific plan amendment, tentative subdivision map or ARDA amendment in connection with its proposed development. If the City fails to impose such a condition, when required, with at least substantially similar terms, although precise language may differ (whether through a tentative subdivision map condition of approval, amendment to the specific plan or to a development agreement, or other agreement between the City and a Participating Landowner), this condition of approval shall be null and void as to Owner/Applicant's Project, and shall not be used as a reason to prevent approval of any final small lot map for Owner/Applicant's Project. If the City approves any other final small lot map for a project within the Plan Area and the Corporation Yard site has not been approved as provided for herein, Owner/Applicant may seek relief from the terms of this condition by appeal to the City Manager, with the right to review by the City Council.</p>	M, B	CD (P)	<p>The City has completed the annexation process with LAFCO to add the future corporation site to the City of Folsom Limits. The grant deed for the transfer of the Corporation Yard to the City has recorded in the Official Records of Sacramento County and LAFCO has recorded the Certificate of Completion in Book 20181207 at Page 0779 in the Official Records of Sacramento County completing the annexation process.</p>	Yes

SCHOOL SITES				
66.	<p>The locations of the elementary school sites and the combined middle school/high school were initially established during the City's processing and approval of the Folsom Specific Plan, at which time all Plan Area participants were engaged in the review of land planning and land uses. In 2015, the Folsom Cordova Unified School District raised concerns that the planned location of the future combined middle school/high school site may not be preferred.</p> <p>Prior to approval of First Final Small Lot Map in the FPA (or first building permit if development may occur without any subdivision), the site(s) for the future high school and middle school in the Folsom Plan Area will be identified and approved by the City, in consultation with the Folsom Cordova Unified School District.</p> <p>The City intends to impose this condition equitably throughout the Plan Area as and to each and every Participating Landowner who seeks any future specific plan amendment, tentative subdivision map or ARDA amendment in connection with its proposed development. If the City fails to impose such a condition, when required, with at least substantially similar terms, although precise language may differ (whether through a tentative subdivision map condition of approval, amendment to the specific plan or to a development agreement, or other agreement between the City and a Participating Landowner), this condition of approval shall be null and void as to Owner/Applicant's Project, and shall not be used as a reason to prevent approval of any final small lot map for Owner/Applicant's Project. If the City approves any other final small lot map for a project within the Plan Area and the high school and middle school site(s) has not been approved as provided for herein, Owner/Applicant may seek relief from the terms of this condition by appeal to the City Manager, with the right to review by the City Council.</p> <p>If Owner/Applicant proposes final maps in phases, Owner/Applicant may apply to the City Manager to permit individual phases to move forward to final map if substantial progress is being made to identify an acceptable site(s) as described above. The City Manager's determination of substantial progress shall be in his/her sole discretion.</p>	M, B	CD (P) Folsom Cordova Unified School District	<p>The City, the Folsom Cordova Unified School District (FCUSD) and the Landowners in the Folsom Plan Area successfully negotiated the proposed locations of the combined middle school/high school site. The City Manager informed the Landowners that this condition of approval has been satisfied with a letter dated June 12, 2018. The letter is on file with the City.</p> <p>Yes</p>

Mitigation Measures					
67.	<i>Westland/Eagle Specific Plan Amendment (W/E SPA) Mitigation Monitoring Reporting Program (MMRP) for the Westland/Eagle Area of the Folsom Plan Area Specific Plan (FPASP).</i> Table 1 below describes the mitigation measures from the FPASP (May 2011) MMRP, as amended by the Revised Proposed Water Supply Facility Alternative (November 2012) and by W/E SPA (September 2015).				
Table 1.					
FPASP EIR/EIS, as amended by the W/E SPA Addendum, Mitigation Measures Applicable to the Mangini Ranch Phase 2 Tentative Subdivision Map Project.*					
*The MMRP for the W/E SPA Addendum to the FPASP EIR/EIS is included as Attachment 20.					
Condition	Mitigation Measures Applicable to the Project	Timing	Responsible Agency	Comments	Condition Satisfied?
Aesthetics					
67-1	3A.1-1: Construct and Maintain a Landscape Corridor Adjacent to U.S. 50.	G, O	CD	This subdivision does not have frontage on US HWY 50	Yes
67-2	3A.1-4: Screen Construction Staging Areas.	G	CD (P)(E)(B)	The construction staging area does not require screening due to distance from existing residences (>1000 feet)	Yes
67-3	3A.1-5: Establish and Require Conformance to Lighting Standards and Prepare and Implement a Lighting Plan.	B	CD (P)	CDD has approved the lighting plan for this subdivision	Yes
Air Quality					
67-4	3A.2-1a: Implement Measures to Control Air Pollutant Emissions Generated by Construction of On-Site Elements.	G, C	CD	(2) The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Spring of 2022. Compliance table is on file with the City.	Yes
67-5	3A.2-1b: Pay Off-site Mitigation Fee to SMAQMD to Off-Set NOX Emissions Generated by Construction of On-Site Elements.	G, C	CD (P)(E) SMAQMD	See (2) above	Yes
67-6	3A.2-1c: Analyze and Disclose Projected PM10 Emission Concentrations at Nearby Sensitive Receptors Resulting from Construction of On-Site Elements.	G	CD (E)(P)	See (2) above	Yes
67-7	3A.2-1d: Implement SMAQMD's Basic Construction Emission Control Practices during Construction of all Off-site Elements located in Sacramento County	G	SMAQMD	See (2) above. No off-site elements outside the City limits required for this subdivision	Yes
67-8	3A.2-1f: Implement SMAQMD's Enhanced Exhaust Control Practices during Construction of all Off-site Elements.	G	Sacramento County El Dorado County or Cal Trans	See (2) above. No off-site elements outside the City limits required for this subdivision	Yes

67-9	3A.2-1g: Pay Off-site Mitigation Fee to SMAQMD to Off-Set NOX Emissions Generated by Construction of Off-site Elements.	G	SMAQMD	See (2) above	Yes
67-10	3A.2-1h: Analyze and Disclose Projected PM10 Emission Concentrations at Nearby Sensitive Receptors Resulting from Construction of Off-site Elements	G		See (2) above	Yes
67-11	3A.2-2: Implement All Measures Prescribed by the Air Quality Mitigation Plan to Reduce Operational Air Pollutant Emissions.	M, I	CD	See (2) above	Yes
67-12	3A.2-4a: Develop and Implement a Plan to Reduce Exposure of Sensitive Receptors to Construction-Generated Toxic Air Contaminant Emissions.	G, C	CD	See (2) above	Yes
67-13	3A.2-4b: Implement Measures to Reduce Exposure of Sensitive Receptors to Operational Emissions of Toxic Air Contaminants	G, C	CD SMAQMD	See (2) above	Yes
67-14	3A.2-5: Implement A Site Investigation to Determine the Presence of NOA and, if necessary, Prepare and Implement an Asbestos Dust Control Plan.	G, C	CD	See (2) above	Yes
67-15	3A.2-6: Implement Measures to Control Exposure of Sensitive Receptors to Operational Odorous Emissions	B, C	CD	See (2) above	Yes
Biological Resources					
67-16	3A.3-1a (as amended by W/E SPA): Mitigation for erosion impacts.	I, OG	PW USACE	See (2) above	Yes
67-17	3A.3-1b (as amended by W/E SPA): Implement Clean Water Act Section 404 Permits and Section 401 Water Quality Certifications.	G, I, OG	CD USACE	See (2) above	Yes
67-18	3A.3-2a (as amended by W/E SPA): Avoid Direct Loss of Swainson's Hawk and Other Raptor Nests.	G, I, C	CD CDFW	See (2) above	Yes
67-19	W/E SPA Mitigation Measure 4.4-4: Conduct preconstruction Swainson's Hawk and other raptor surveys	G	CD (P)(E) CDFW	See (2) above	Yes
67-20	3A.3-2b: Prepare and Implement a Swainson's Hawk Mitigation Plan.	G, I, C	CD	See (2) above	Yes
67-21	W/E SPA Mitigation Measure 4.4-5: Prepare and Implement Swainson's hawk mitigation plan.	G	CD (P)(E) CDFW	See (2) above	Yes
67-22	3A.3-2c (as amended by W/E SPA): Conduct preconstruction Tricolored Blackbird Nesting surveys.	G	CD CDFW	See (2) above	Yes
67-23	W/E SPA Mitigation Measure 4.4-6: Conducting preconstruction burrowing owl survey.	G	CD (P)(E) CDFW	See (2) above	Yes
67-24	W/E SPA Mitigation Measure 4.4-7: Preconstruction nesting bird survey.	G	CD (P)(E) CDFW	See (2) above	Yes
67-25	3A.3-2d (as amended by W/E SPA): Conduct preconstruction bat roosting survey.		CD	See (2) above	Yes
67-26	3A.3-2e: Obtain an Incidental Take Permit under Section 10(a) of ESA; Develop and Implement a Habitat Conservation Plan to Compensate for the Loss of Vernal Pool Habitat.	G, I, OG	USFWS CD	See (2) above	Yes
67-27	3A.3-2f: Obtain an Incidental Take Permit under Section 10(a) of ESA; Develop and Implement a Habitat Conservation	G, I, OG	USFWS	See (2) above	Yes

	Plan to Compensate for the Loss of VELB Habitat.		USACE CD		
67-28	3A.3-2g: Secure Take Authorization for Federally Listed Vernal Pool Invertebrates and Implement All Permit Conditions.	G, I, C	USACE CD	See (2) above	Yes
67-29	3A.3-2h (as amended by W/E SPA): Valley Elderberry Longhorn Beetle avoidance and minimization measures.	G, I, OG	USACE USFWS CD	See (2) above	Yes
67-30	3A.3-3: Conduct Special-Status Plant Surveys; Implement Avoidance and Mitigation Measures or Compensatory Mitigation.	G, I, B	USFWS CDFW CD	See (2) above	Yes
67-31	3A.3-4a (as amended by W/E SPA): Implement Section 1602 Master Streambed Alteration Agreement.	G, I, C	CDFW CD	See (2) above	Yes
67-32	3A.3-4b (as amended by W/E SPA): Valley Needlegrass Grassland Avoidance and Minimization Measures.	G, I, B	CDFW CD	See (2) above	Yes
67-33	3A.3-5 (as amended by W/E SPA): Oak woodlands mitigation.	G, I, C	CD, PW	See (2) above	Yes
67-34	W/E SPA Mitigation Measure 4.4-1: Conduct environmental awareness training for construction employees.	G, C	CD	See (2) above	Yes
67-35	W/E SPA Mitigation Measure 4.4-2: Conduct preconstruction western spadefoot survey	G	CD (E) (P) CDFW	See (2) above	Yes
67-36	W/E SPA Mitigation Measure 4.4-3: Conduct preconstruction western pond turtle survey.	G	CD (E)(P) CDFW	See (2) above	Yes
Climate Change					
67-37	3A.4-1: Implement Additional Measures to Control Construction-Generated GHG Emissions.	M, B, C	CD SMAQMD	See (2) above	Yes
67-38	3A.4-2a: Implement Additional Measures to Reduce Operational GHG Emissions.	M, B	CD SMAQMD	See (2) above	Yes
67-39	3A.4-2b: Participate in and Implement an Urban and Community Forestry Program and/or Off-Site Tree Program to Off-Set Loss of On-Site Trees.	M, B	CD	No trees are present in the subdivision.	Yes
Cultural Resources					
67-40	3A.5-1a (as amended by W/E SPA): Comply with the Programmatic Agreement	G	CD USACE	See (2) above	Yes
67-41	3A.5-1b (as amended by W/E SPA): Perform an Inventory and Evaluation of Cultural Resources for the California Register of Historic Places, Minimize or Avoid Damage or Destruction, and Perform Treatment Where Damage or Destruction Cannot be Avoided.	G	CD USACE	See (2) above	Yes
67-42	3A.5-2 (as amended by W/E SPA): Conduct Construction Personnel Education, Conduct On-Site Monitoring if Required, Stop Work if Cultural Resources are Discovered, Assess the Significance of the Find, and Perform Treatment or Avoidance as Required.	G, C	CD USACE	See (2) above	Yes

67-43	3A.5-3 (as amended by W/E SPA): Suspend Ground-Disturbing Activities if Human Remains are Encountered and Comply with California Health and Safety Code Procedures.	OG	CD (P)(E) Sacramento County Coroner Native American Heritage Commission	See (2) above. No human remains have been encountered in the subdivision during grading and construction.	Yes
Geology, Soils, Minerals, And Paleontological Resources					
67-44	3A.7-1a: Prepare Site-Specific Geotechnical Report per CBC Requirements and Implement Appropriate Recommendations.	B	CD (E)	Owner/applicant has provided Geotechnical Report to the City. The Geotechnical report for the subdivision is on file with the City.	Yes
67-45	3A.7-1b: Monitor Earthwork during Earthmoving Activities	B	CD (P)(E)(B)	Compliance monitored through construction inspection.	Yes
67-46	3A.7-3: Prepare and Implement the Appropriate Grading and Erosion Control Plan.	G	CD (E)	Compliance monitored through construction inspection.	Yes
67-47	3A.7-5: Divert Seasonal Water Flows Away from Building Foundations.	B	CD (B)(P)	CDD will review all future building permits in the subdivision to verify compliance with mitigation measure	Condition will be satisfied prior to building permit issuance.
67-48	3A.7-10: Conduct Construction Personnel Education, Stop Work if Paleontological Resources are Discovered, Assess the Significance of the Find, and Prepare and Implement a Recovery Plan as Required.	C	CD	See (2) above. No human remains or paleontological resources have been encountered in the subdivision during grading and construction.	Yes
Hazards and Hazardous Materials					
67-49	3A.8-2: Complete Investigations Related to the Extent to Which Soil and/or Groundwater May Have Been Contaminated in Areas Not Covered by the Phase I and II Environmental Site Assessments and Implement Required Measures	G, C	CD	See (2) above. No hazardous and/or contaminated soil or groundwater has been discovered in the subdivision.	Yes
67-50	3A.8-5: Prepare and Implement a Blasting Safety Plan in Consultation with a Qualified Blaster.	M	CD	The owner/applicant has retained a blasting contractor, BTI, Inc., to conduct all blasting in the subdivision. A Blasting Safety Plan was provided to the City and the contractor has been issued a blasting permit for the subdivision. BTI, Inc. notifies the City prior to all blasts in accordance with the Blasting Permit requirements.	Yes

67-51	Mitigation Measure 3A.8-6: Prudent Avoidance and Notification of EMF Exposure.	M	CD Folsom Cordova Unified School District	There are no existing overhead power lines within or adjoining the subdivision	Yes
67-52	3A.8-7: Prepare and Implement a Vector Control Plan in Consultation with the Sacramento-Yolo Mosquito and Vector Control District.	G	CD Sacramento- Yolo Mosquito and Vector Control District	A Vector Control Plan was prepared and submitted to the City for review and approval. The plan incorporated various Best Management Practices in consultation with the Sacramento-Yolo Mosquito and Vector Control District. A copy of the Vector Control Plan is available from the Community Development Department.	Yes
Hydrology and Water Quality					
67-53	3A.9-1: Acquire Appropriate Regulatory Permits and Prepare and Implement SWPPP and BMPs.	G, C	CD (E) CVRWQB	Owner/applicant has been issued a WDID # and has submitted a SWPPP approved by the RWQCB. SWPPP is on file at the City.	Yes
67-54	3A.9-2: Prepare and Submit Final Drainage Plans and Implement Requirements Contained in Those Plans.	G, B	CD (E)	The City has reviewed and approved the storm drain plans for this subdivision. The storm drain improvements are in compliance with the approved Folsom Plan Storm Drain Master Plan approved by the City.	Yes
67-55	3A.9-3: Develop and Implement a BMP and Water Quality Maintenance Plan.	G, C	CD (E), PW	Owner/applicant has been issued a WDID # and has submitted a SWPPP approved by the RWQCB. SWPPP is on file at the City.	Yes
67-56	3A.9-4: Inspect and Evaluate Existing Dams Within and Upstream of the Project Site and Make Improvements if Necessary.	M, I	PW	There are no existing dams upstream from this subdivision.	Yes

Noise					
67-57	3A.11-1: Implement Noise-Reducing Construction Practices, Prepare and Implement a Noise Control Plan, and Monitor and Record Construction Noise near Sensitive Receptors.	G, C	CD	The owner/applicant has implemented noise reducing construction practices included as part of the required Noise Control Plan. Compliance with these requirements has been monitored through construction inspection.	Yes
67-58	3A.11-3: Implement Measures to Prevent Exposure of Sensitive Receptors to Groundborne Noise or Vibration from Project Generated Construction Activities.	C	CD (E)(P)	The owner/applicant has been working closely with their contractors during the course of grading and construction to minimize ground borne noise and vibration. The owner/applicant has been challenged due to the extent of the blasting needed for the grading of the subdivision. The owner/applicant and their contractors are working very hard to minimize the vibration and disturbance to existing residents in the vicinity of the blasting. The owner/applicant as provided monitoring reports to the City verifying that all ground borne noise and vibration is within allowable thresholds and eliminating any possibility of structural damage to existing residential units.	Yes
67-59	3A.11-4: Implement Measures to Prevent Exposure of Sensitive Receptors to Increases in Noise from Project-Generated Operational Traffic on Off-site and On-Site Roadways.	C	CD (E)(P)	The owner/applicant continues to make efforts to reduce noise from grading and construction in the vicinity of existing residents. Night work is necessary due to impacts to the travelling public during the normal workday. The night work has been disruptive to some existing residences. The CDD is working closely to monitor the noise impacts in the night time hours to minimize impacts to existing residents.	Yes
67-60	3A.11-5: Implement Measures to Reduce Noise from Project-Generated Stationary Sources.	I, OG	CD (E)	All stationary construction equipment, if present, is muffled to reduce noise in accordance with noise requirements.	Yes
67-61	W/E SPA Mitigation Measure 4.12-1: Implement measures to prevent exposure of sensitive receptors to increases in noise from project-generated operational traffic on offsite and onsite roadways	M	CD	The owner/applicant continues to make efforts to reduce noise from grading and	Yes

				construction in the vicinity of existing residents. Night work is necessary due to impacts to the travelling public during the normal workday. The night work has been disruptive to some existing residences. The CDD is working closely to monitor the noise impacts in the night time hours to minimize impacts to existing residents.	
Public Services					
67-62	3A.14-1 (as amended by W/E SPA): Prepare and Implement a Construction Traffic Control Plan.	G, I, B, C	PW	The CDD has reviewed and approved all traffic control plans for the subdivision to verify compliance with City ordinances and to minimize delays to the travelling public.	Yes
67-63	3A.14-2: Incorporate California Fire Code; City of Folsom Fire Code Requirements; and EDHFD Requirements, if Necessary, into Project Design and Submit Project Design to the City of Folsom Fire Department for Review and Approval.	B, O	CD, FD	The City Fire Department has reviewed and approved all proposed improvements for the subdivision. The City FD will verify adequate fire flow prior to building permit issuance in the subdivision.	Yes
67-64	3A.14-3: Incorporate Fire Flow Requirements into Project Designs.	B, O	CD, FD	The City Fire Department has reviewed and approved all proposed improvements for the subdivision. The City FD will verify adequate fire flow prior to building permit issuance in the subdivision.	Yes

Traffic and Transportation

It should be noted that many of the Transportation, Traffic, and Circulation mitigation measures identified below will be satisfied through the payment of fees. Below is a brief summary of the fee types and their purpose. The acronyms for each fee type noted below are further noted in the Implementation Schedule column of each applicable mitigation measure to clarify how each mitigation measure is anticipated to be satisfied.

Public Facilities Financing Plan (PFFP):

In January of 2014, the City of Folsom adopted the PFFP for the Folsom Plan Area which detailed all the infrastructure components to address full build out of the Plan Area. The PFFP includes various techniques including development fees to fund the necessary infrastructure. The City is currently in the process of preparing and adopting implementing ordinances and a nexus study required by State law to impose the associated development fees.

Included in the PFFP are a number roadway projects including the Highway Interchanges that the Mangini Ranch Phase 2 Subdivision project will have cumulative impacts on within the Folsom Plan Area. The PFFP was designed to satisfy the “fair share” financing of all the Plan Area’s backbone roadway system. Participating in this fee program will satisfy numerous roadway mitigation measures as shown in the MMRP table.

Sacramento County Transportation Development Fee (SCTDF) contribution:

The City is establishing a “fair share” fee to mitigate roadway impacts outside the project boundaries and within unincorporated Sacramento County. This fee will be included in the City Facilities portion of the Public Facilities Financing Plan program and will be collected at the time of building permit issuance. The basis for the calculation of the fee is a report entitled, “Fair Share Cost Allocation Sacramento County & City of Folsom” dated January 2, 2014.

Cal Trans/City Memorandum of Understanding (Cal Trans MOU):

The City of Folsom and Cal Trans entered into an MOU on December 17, 2014 to establish a fee mechanism to address the "fair share" impacts to Highway 50. The MOU identifies all the highway improvements for which there are mitigation measures and potential construction projects to address them. The City will establish a fee in the City Facilities portion of the Public Facilities Financing Plan and it will be collected at the time of building permit issuance

67-65	3A.15-1: Project Participation in Funding Transportation Improvements	B	CD (E), PW	(1) This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Yes
67-66	3A.15-1a: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Folsom Boulevard/Blue Ravine Road Intersection (Intersection 1).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-67	3A.15-1b: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements at the Sibley Street/ Blue Ravine Road Intersection (Intersection 2).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-68	3A.15-1c: The Applicant Shall Fund and Construct Improvements to the Scott Road (West)/White Rock Road Intersection (Intersection 28).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-69	3A.15-1e: Fund and Construct Improvements to the Hillside Drive/Easton Valley Parkway Intersection (Intersection 41).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-70	3A.15-1f: Fund and Construct Improvements to the Oak Avenue Parkway/Middle Road Intersection (Intersection 44).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-71	3A.15-1h: Participate in Fair Share Funding of Improvements to Reduce Impacts to the Hazel Avenue/Folsom Boulevard Intersection (Sacramento County Intersection 2).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-72	3A.15-1j: Participate in Fair Share Funding of Improvements to Reduce Impacts on Hazel Avenue between Madison Avenue and Curragh Downs Drive (Roadway Segment 10).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-73	3A.15-1o: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 as an alternative to improvements at the Folsom Boulevard/U.S. 50 Eastbound Ramps Intersection (Caltrans Intersection 4).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-74	3A.15-1p: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/ State Route 16 Intersection (Caltrans Intersection 12).	B (Caltrans MOU/pay SCTDF)	CD (E), PW	See (1) above	Yes
67-75	3A.15-1q: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-76	3A.15-1r: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Hazel Avenue and Folsom Boulevard (Freeway Segment 3).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-77	3A.15-1s: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 4).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-78	3A.15-1u: Participate in Fair Share Funding of Improvements to Reduce Impacts on Westbound U.S. 50 between Prairie City Road and Folsom Boulevard (Freeway Segment 16).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes

67-79	3A.15-1v: Participate in Fair Share Funding of Improvements to Reduce Impacts on Westbound U.S. 50 between Hazel Avenue and Sunrise Boulevard (Freeway Segment 18).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-80	3A.15-1w: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Folsom Boulevard Ramp Merge (Freeway Merge 4).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-81	3A.15-1x: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Diverge (Freeway Diverge 5).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-82	3A.15-1y: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Direct Merge (Freeway Merge 6).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-83	3A.15-1z: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Flyover On-Ramp to Oak Avenue Parkway Off-Ramp Weave (Freeway Weave 8).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-84	3A.15-1aa: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Oak Avenue Parkway Loop Merge (Freeway Merge 9).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-85	3A.15-1dd: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Empire Ranch Road Loop Ramp Merge (Freeway Merge 23).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-86	3A.15-1ee: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Oak Avenue Parkway Loop Ramp Merge (Freeway Merge 29).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-87	3A.15-1ff: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Prairie City Road Loop Ramp Merge (Freeway Merge 32).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-88	3A.15-1gg: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Prairie City Road Direct Ramp Merge (Freeway Merge 33).	B Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-89	3A.15-1hh: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Folsom Boulevard Diverge (Freeway Diverge 34).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-90	3A.15-1ii: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Hazel Avenue Direct Ramp Merge (Freeway Merge 38).	B (Caltrans MOU)	CD (E), PW	See (1) above	Yes
67-91	3A.15-2a: Develop Commercial Support Services and Mixed-use Development Concurrent with Housing Development, and Develop and Provide Options for Alternative Transportation Modes.	I (pay PFFP fee and Transit fee)	PW	All properties in the FPASP pay an annual Transportation Management Fee via CFD 18	Yes
67-92	3A.15-2b: Participate in the City's Transportation System Management Fee Program.	B (pay City fee)	CD (E), PW	All properties in the FPASP pay an annual Transportation Management Fee via CFD 18	Yes
67-93	3A.15-2c: Participate with the 50 Corridor Transportation Management Association	B (pay City fee)	PW	All properties in the FPASP pay an annual Transportation Management Fee via CFD 18	Yes
67-94	3A.15-3: Pay Full Cost of Identified Improvements that Are Not Funded by the City's Fee Program.	B (Caltrans MOU, PFFP fee, SCTDF)	CD (E), PW	See (1) above	Yes
67-95	3A.15-4a: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Sibley Street/Blue Ravine Road Intersection (Folsom Intersection 2).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-96	3A.15-4b: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Oak Avenue Parkway/East Bidwell Street Intersection (Folsom Intersection 6).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes

67-97	3A.15-4c: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the East Bidwell Street/College Street Intersection (Folsom Intersection 7).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-98	3A.15-4d: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the East Bidwell Street/Iron Point Road Intersection (Folsom Intersection 21).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-99	3A.15-4e: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Serpa Way/ Iron Point Road Intersection (Folsom Intersection 23)	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-100	3A.15-4f: The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Empire Ranch Road/Iron Point Road Intersection (Folsom Intersection 24).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-101	3A.15-4g: The Applicant Shall Fund and Construct Improvements to the Oak Avenue Parkway/Easton Valley Parkway Intersection (Folsom Intersection 33).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-102	3A.15-4i: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/White Rock Road Intersection (Sacramento County Intersection 3).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-103	3A.15-4j: Participate in Fair Share Funding of Improvements to Reduce Impacts on Grant Line Road between White Rock Road and Kiefer Boulevard (Sacramento County Roadway Segments 5-7).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-104	3A.15-4k: Participate in Fair Share Funding of Improvements to Reduce Impacts on Grant Line Road between Kiefer Boulevard and Jackson Highway (Sacramento County Roadway Segment 8).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-105	3A.15-4l: Participate in Fair Share Funding of Improvements to Reduce Impacts on Hazel Avenue between Curragh Downs Drive and U.S. 50 Westbound Ramps (Sacramento County Roadway Segments 12-13).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-106	3A.15-4m: Participate in Fair Share Funding of Improvements to Reduce Impacts on White Rock Road between Grant Line Road and Prairie City Road (Sacramento County Roadway Segment 22).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-107	3A.15-4n: Participate in Fair Share Funding of Improvements to Reduce Impacts on White Rock Road between Empire Ranch Road and Carson Crossing Road (Sacramento County Roadway Segment 28).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-108	3A.15-4o: Participate in Fair Share Funding of Improvements to Reduce Impacts on the White Rock Road/Carson Crossing Road Intersection (El Dorado County 1).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-109	3A.15-4p: Participate in Fair Share Funding of Improvements to Reduce Impacts on the Hazel Avenue/U.S. 50 Westbound Ramps Intersection (Caltrans Intersection 1).	B (pay SCTDF)	CD (E), PW	See (1) above	Yes
67-110	3A.15-4q: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).	B (pay SCTDF)	Capitol Southeast Connector JPA	See (1) above	Yes
67-111	3A.15-4r: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Rancho Cordova Parkway and Hazel Avenue (Freeway Segment 3).	B (pay SCTDF)	Capitol Southeast Connector JPA	See (1) above	Yes
67-112	3A.15-4s: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 5).	B (pay SCTDF)	Capitol Southeast Connector JPA	See (1) above	Yes
67-113	3A.15-4t: Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Prairie City Road and Oak Avenue Parkway (Freeway Segment 6).	B (pay PFFP/Interchange fee)	CD (E), PW	See (1) above	Yes

67-114	3A.15-4u: Participate in Fair Share Funding of Improvements to Reduce Impacts on the U.S. 50 Eastbound / Prairie City Road Slip Ramp Merge (Freeway Merge 6).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-115	3A.15-4v: Participate in Fair Share Funding of Improvements to Reduce Impacts on the U.S. 50 Eastbound / Prairie City Road Flyover On Ramp to Oak Avenue Parkway Off Ramp Weave (Freeway Weave 7).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-116	3A.15-4w: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound / Oak Avenue Parkway Loop Ramp Merge (Freeway Merge 8).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-117	3A.15-4x: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound / Empire Ranch Road Loop Ramp Merge (Freeway Merge 27).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-118	3A.15-4y: Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound / Prairie City Road Loop Ramp Merge (Freeway Merge 35).	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-119	W/E SPA Mitigation Measure 4.16-1: Participate in Fair Share Funding of modification of the Iron Point Road/East Bidwell Street Intersection.	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
67-120	W/E SPA Mitigation Measure 4.16-2: Participate in Fair Share Funding of improvements to the Scott Road/Easton Valley Parkway Intersection.	B (pay PFFP fee)	CD (E), PW	See (1) above	Yes
Utilities and Service Systems					
67-121	3A.16-1: Submit Proof of Adequate On- and Off-Site Wastewater Conveyance Facilities and Implement On- and Off-Site Infrastructure Service Systems or Ensure That Adequate Financing Is Secured	M, B	CD, PW	The Phase 1 Sanitary Sewer infrastructure including the off-site sewer trunk main, the Alder Creek Parkway sewer lift station and forced main to serve this subdivision have been constructed by the FPA landowners and have been completed and accepted by the City and are currently in operation.	Yes
67-122	3A.16-3: Demonstrate Adequate SRWTP Wastewater Treatment Capacity	M, B	CD, PW	The City obtained a letter from Regional San which provides verification that there is adequate capacity in the existing Regional San conveyance and treatment system to accommodate the entire Folsom Plan Area at buildout. Confirmation from Regional San was required because the Folsom Plan Area is served by the existing Regional San Lift Station on Iron Point Road. The City Sewer Lift Station and Forced Main which connects to the Regional San Lift Station has been accepted by the City and is currently in operation.	Yes
Water Supply					
67-123	3A.18-1: Submit Proof of Surface Water Supply Availability	M, B	CD, PW	The owner/applicant has constructed the necessary infrastructure to provide potable	Yes

				water to the subdivision. The potable Phase 1 water infrastructure for the Folsom Plan Area has been reviewed, approved and accepted by the City and is currently in operation.	
67-124	3A.18-2a: Submit Proof of Adequate Off-Site Water Conveyance Facilities and Implement Off-Site Infrastructure Service System or Ensure That Adequate Financing Is Secured.	M, B	CD, PW	The off-site potable water infrastructure to serve the subdivision has been reviewed, approved and accepted by the City and is currently in operation. In addition, the City has verified that the off-site potable water infrastructure is adequate to serve the subdivision.	Yes
Cumulative					
67-125	AIR-1-Land: Implement East Sacramento Regional Aggregate Mining Truck Management Plan or Other Measures to Reduce Exposure of Sensitive Receptors to Operational Emissions of Toxic Air Contaminants from Quarry Truck Traffic.	M	CD	The owner/applicant is a participant in the Truck Management Plan at such time there is traffic generated from the future quarries south of the Folsom Plan Area.	Yes
67-126	NOISE-1-Land: Implement East Sacramento Regional Aggregate Mining Truck Management Plan or Other Measures to Reduce Exposure of Sensitive Receptors to Operational Noise from Quarry Truck Traffic.	M	CD	The owner/applicant is a participant in the Truck Management Plan at such time there is traffic generated from the future quarries south of the Folsom Plan Area.	Yes
68.	Folsom South of U.S. Highway 50 Backbone Infrastructure Mitigated Negative Declaration (Backbone MND) Mitigation Monitoring Reporting Program (MMRP) for the Folsom Plan Area Specific Plan (FPASP). Table 2 Below describes the mitigation measures from the Backbone MND (December 2014) MMRP				
Table 2.					
Backbone MND Mitigation Measures Applicable to the Mangini Ranch Phase 2 Tentative Subdivision Map Project.*					
*The mitigation measures specific to the 2014 Backbone Infrastructure MND (designated by roman numerals, e.g., IV-1) apply only to the portions of the Mangini Ranch Phase 2 Project that are included as part of the South of Highway 50 Backbone Infrastructure Project. The MMRP for the Backbone Infrastructure Project is included as Attachment 20.					
Condition	Mitigation Measures Applicable to the Project	Timing	Responsible Agency	Comments	Condition Satisfied?
Aesthetics					
68-1	Backbone MND Mitigation Measure I-1: Design above ground pump station and storage tank facilities to reduce visual impacts.	I	CD, EWR	There are no above ground facilities required to be constructed in Mangini Ranch Phase 2	Yes
68-2	Backbone MND Mitigation Measure I-2: Develop and implement a landscaping plan for pump station and storage tank facilities to reduce visual impacts.	I	CD, EWR	There are no above ground facilities required to be constructed in Mangini Ranch Phase 2	Yes
Air Quality					

68-3	Backbone MND Mitigation Measure III-1: Prepare and Implement NOX Reduction Plan	G, C	CD SMAQMD	(2) The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Spring of 2022. Compliance table is on file with the City.	Yes
68-4	Backbone MND Mitigation Measure III-2: Pay Off-site Mitigation Fee to SMAQMD to off-set NOX Emissions Generated by Construction.	G, C	CD SMAQMD	See (2) above	Yes
68-5	Backbone MND Mitigation Measure III-4: Implement A Site Investigation to Determine the Presence of NOA and, if necessary, Prepare and Implement an Asbestos Dust Control Plan.	G, C	CD SMAQMD	See (2) above	Yes
Biological Resources					
68-6	Backbone MND Mitigation Measure IV-1: Conduct Special-Status Plant Surveys; Implement Avoidance and Mitigation Measures or Compensatory Mitigation	G, C	CD	See (2) above	Yes
68-7	Backbone MND Mitigation Measure IV-2: Implement Conditions of the Biological Opinion (BO) for Federally Listed Vernal Pool Invertebrates.	G, C	CD USFWS	See (2) above	Yes
68-8	Backbone MND Mitigation Measure IV-3: Implement Conditions of the Biological Opinion for Impacts on Valley Elderberry Longhorn Beetle.	G, C	CD USFWS	See (2) above	Yes
68-9	Backbone MND Mitigation Measure IV-4: Western Spadefoot Toad	G, C	CD CDFW	See (2) above	Yes
68-10	Backbone MND Mitigation Measure IV-5: Western Pond Turtle	G, C	CD CDFW	See (2) above	Yes
68-11	Backbone MND Mitigation Measure IV-6(a): Swainson's Hawk Nesting Habitat	G, I, C	CD	See (2) above	Yes
68-12	Backbone MND Mitigation Measure IV-6(b): Swainson's Hawk Foraging Habitat	G, I, C	CD	See (2) above	Yes
68-13	Backbone MND Mitigation Measure IV-7: Tricolored Blackbird	G, C	CD CDFW	See (2) above	Yes
68-14	Backbone MND Mitigation Measure IV-8: Nesting Raptors	G, I	CD CDFW	See (2) above	Yes
68-15	Backbone MND Mitigation Measure IV-9: Nesting Special Status Birds and Migratory Birds	G, C	CD CDFW	See (2) above	Yes
68-16	Backbone MND Mitigation Measure IV-10: Special-Status Bats		CD CDFW	See (2) above	Yes
68-17	Backbone MND Mitigation Measure IV-12: Implement Section 1602 Master Streambed Alteration Agreement	G, C	CD CDFW	See (2) above	Yes
68-18	Backbone MND Mitigation Measure IV-13: Conduct Surveys to Identify and Map Valley Needlegrass Grassland; Implement Avoidance and Minimization Measures or Compensatory Mitigation, if necessary	G, C	CD CDFW	See (2) above	Yes

68-19	Backbone MND Mitigation Measure IV-14: Secure Amended Clean Water Act Section 404 Permit and Section 401 Permit and Implement All Permit Conditions; Ensure No Net Loss of Functions of Wetlands, Other Waters of the U.S., and Waters of the State	G, I, C	CD USACE	See (2) above	Yes
68-20	Backbone MND Mitigation Measure IV-15: Conduct Tree Survey, Prepare and Implement an Oak Woodland Mitigation Plan, Replace Native Oak Trees Removed, and Implement Measures to Avoid and Minimize Indirect Impacts on Oak Trees and Oak Woodland Habitat Retained On-Site.	G, I, C	CD, PW	See (2) above	Yes
68-21	Backbone MND Mitigation Measure IV-11: American Badger	G, C	CD CDFW	See (2) above	Yes
Cultural Resources					
68-22	Backbone MND Mitigation Measure V-1: Comply with the applicable procedures in the FAPA and implementation of applicable historic property treatment plans	G, C	CD USACE	See (2) above	Yes
68-23	Backbone MND Mitigation Measure V-2: Conduct Construction Personnel Education, Conduct On-Site Monitoring if Required, Stop Work if Cultural Resources are Discovered, Assess the Significance of the Find, and Perform Treatment or Avoidance as Required.	G, C	CD USACE	See (2) above	Yes
68-24	Backbone MND Mitigation Measure V-3: Suspend Ground-Disturbing Activities if Human Remains are Encountered and Comply with California Health and Safety Code Procedures.	OG	CD Sacramento County Coroner Native American Heritage Commission	See (2) above	Yes
Geology, Soils, and Paleontological Resources					
68-25	Backbone MND Mitigation Measure VI-1: Prepare Site-Specific Geotechnical Report per CBC Requirements and Implement Appropriate Recommendations.	G, C	CD EWR	See (2) above	Yes
68-26	Backbone MND Mitigation Measure VI-3: Monitor Earthwork during Earthmoving Activities.	G, C	CD	See (2) above	Yes
68-27	Backbone MND Mitigation Measure VI-5(a): Prepare and Implement the Appropriate Grading and Erosion Control Plan.	G	PW	See (2) above	Yes
Hydrology and Water Quality					
68-28	Backbone MND Mitigation Measure VI-5(b): Prepare and Implement the appropriate Grading and Erosion Control Plan for the detention basin West of Prairie City Road.	G	PW	See (2) above	Yes
68-29	Backbone MND Mitigation Measure IX-1: Acquire Appropriate Regulatory Permits and Prepare and Implement SWPPP and BMPs.	G	CD (E) Central Valley Reg. Water Quality Control	See (2) above	Yes

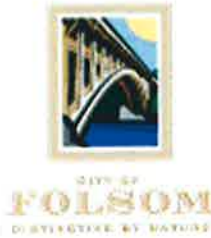
Condition	Mitigation Measures Applicable to the Project	Timing	Responsible Agency	Comments	Condition Satisfied?
Climate Change					
68-30	Backbone MND Mitigation Measure VII-1: Greenhouse Gas Emissions	Prior to releasing RFB to contractors	SMAQMD	See (2) above	
Public Services					
68-31	Backbone MND Mitigation Measure XVI-1: Prepare and Implement a Construction Traffic Control Plan.	G, I, B, C	PW	See (2) above	
Water Improvements					
68-32	Backbone MND Mitigation Measure III-3: North of U.S. Highway 50 Water Improvements	G, I	SMAQMD CD (E)	See (2) above	
68-33	Backbone MND Mitigation Measure V-4 North of U.S. Highway 50 Water Improvements	G, I	CD (E)	See (2) above	
68-34	Backbone MND Mitigation Measure VI-2 North of U.S. Highway 50 Water Improvements	G, I	CD (E)	See (2) above	
68-35	Backbone MND Mitigation Measure VI-4 North of U.S. Highway 50 Water Improvements	G, I	CD (E)	See (2) above	
68-36	Backbone MND Mitigation Measure XII-1 North of U.S. Highway 50 Water Improvements	G, I, B	PW	See (2) above	
69.	Revised Proposed Off-Site Water Facility Alternative Mitigation Monitoring Reporting Program (MMRP) for the Folsom Plan Area Specific Plan (FPASP). Table 3 below describes the mitigation measures from the Revised Water Alternative (November 2012) MMRP.				
Table 3.					
Revised Proposed Off-Site Water Facility Alternative Addendum to the FPASP EIR/EIS Mitigation Measures Applicable to the Mangini Ranch Phase 2 Tentative Subdivision Map Project.*					
*The Folsom South of U.S. Highway 50 Specific Plan Project: Revised Proposed Off-Site Water Facility Alternative was approved December 11, 2012. The November 2012 MMRP for the Revised Proposed Off-Site Water Facility Alternative is included as Attachment 21.					
Aesthetics					
69-1	3B 1-2a: Enhance Exterior Appearance of Structural Facilities.	G, B	CD	(3) The Folsom Plan Area (FPA) landowners did not construct the previously contemplated off-site water system to serve the FPA. Instead, the FPA landowners contributed funding to conserve 20% of the existing potable water being utilized in the City. This conservation effort has been completed and validated in the Sacramento County court system and will serve the FPA throughout buildout. The following mitigation measures were related to the previously contemplated off-site water system to be constructed to serve the FPA and therefore do not apply to this subdivision.	Yes
69-2	3B 1-2b: Prepare Landscaping Plan.	G, B	CD	See (3) above	Yes
69-3	3B 1-3a: Conformance to Construction Lighting Standards.	G, B	CD	See (3) above	Yes
69-4	3B 1-3b: Prepare and Submit a Lighting Master Plan.	G, B	CD	See (3) above	Yes
Air Quality					
69-5	3B 2-1a: Develop and Implement a Construction NOX Reduction Plan.	G, C	CD SMAQMD	See (3) above	Yes
69-6	3B 2-1c: Implement Fugitive Dust Control Measures and a Particulate Matter	G, C	CD	See (3) above	Yes

	Monitoring Program during Construction.		SMAQMD		
69-7	3B 2-3a: Cite Pump Siting Buffers Away from Sensitive Receptors.	G, B	CD SMAQMD	See (3) above	Yes
69-8	3B 2-3b: Conduct Project-Level DPM Screening and Implement Measures to Reduce Annual DPM to Acceptable Concentrations.	G, B	CD SMAQMD	See (3) above	Yes
Climate Change					
69-9	3B.4-1a: Implement GHG Reduction Measures during Construction.	G, B	CD SMAQMD	See (3) above	Yes
69-10	3B.4-1b Prepare and Implement an Off-site Water Facilities Climate Action Plan.	G, B	CD SMAQMD	See (3) above	Yes
Cultural Resources					
69-11	3A.5-1a: Comply with the Programmatic Agreement.	G	CD (E) USACE	See (3) above	Yes
69-12	3A.5-1b: Perform an Inventory and Evaluation of Cultural Resources for the California Register of Historic Places, Minimize or Avoid Damage or Destruction, and Perform Treatment Where Damage or Destruction Cannot be Avoided.	B, C	CD	See (3) above	Yes
69-13	3A.5-2: Conduct Construction Personnel Education, Conduct On-Site Monitoring if Required, Stop Work if Cultural Resources are Discovered, Assess the Significance of the Find, and Perform Treatment or Avoidance as Required.	G, C	CD USACE	See (3) above	Yes
69-14	3A.5-3: Suspend Ground-Disturbing Activities if Human Remains are Encountered and Comply with California Health and Safety Code Procedures.	C	CD USACE	See (3) above	Yes
Geology, Soils, Minerals, and Paleontological Resources					
69-15	3B 7-1a: Prepare Geotechnical Report(s) for the Revised Proposed Off-site Water Facilities and Implement Required Measures.	engineering plans	CD	See (3) above	Yes
69-16	3B 7-1b: Incorporate Pipeline Failure Contingency Measures Into Final Pipeline Design.	engineering plans	CD	See (3) above	Yes
69-17	3B.7-4: Implement Corrosion Protection Measures.	engineering plans	CD	See (3) above	Yes
69-18	3B 7-5: Conduct Construction Personnel Education, Stop Work if Paleontological Resources are Discovered, Assess the Significance of the Find, and Prepare and Implement a Recovery Plan as Required.	C	CD	See (3) above	Yes
Hazards and Hazardous Materials					
69-19	3B 8-1a: Transport, Store, and Handle Construction-Related Hazardous Materials in Compliance with Relevant Regulations and Guidelines.	G, C	CD	See (3) above	Yes
69-20	3B 8-1b: Prepare and Implement a Hazardous Materials Management Plan.	G, C	CD	See (3) above	Yes
69-21	3B 8-5a: Conduct Phase 1 Environmental Site Assessment for Selected Alignment.	G, C	CD	See (3) above	Yes
69-22	3B.8-5b: Develop and Implement a Remediation Plan.	G, C	CD	See (3) above	Yes
69-23	3B 8-7a: Keep Construction Area Clear of Combustible Materials.	G, C	CD	See (3) above	Yes

69-24	3B 8-7b: Provide Accessible Fire Suppression Equipment.	G, C	CD	See (3) above	Yes
Hydrology and Water Quality					
69-25	3B 9-1a: Acquire Appropriate Regulatory Permits and Prepare and Implement SWPPP and BMPs.	G, C	CD CVRWQB	See (3) above	Yes
69-26	3B 9-1b: Properly Dispose of Hydrostatic Test Water and Construction Dewatering in Accordance with the Central Valley Regional Water Quality Control Board.	G, C	CD CVRWQB	See (3) above	Yes
69-27	3B 9-3a: Prepare and Implement Drainage Plan(s) for Structural Facilities.	G, C	CD CVRWQB	See (3) above	Yes
69-28	3B 9-3b: Ensure the Provision of Sufficient Outlet Protection and On-site Containment.	G, C	CD CVRWQB	See (3) above	Yes
Noise					
69-29	3B 11-1a: Limit Construction Hours.	C	CD	See (3) above	Yes
69-30	3B 11-1b: Minimize Noise from Construction Equipment and Staging.	C	CD	See (3) above	Yes
69-31	3B 11-1c: Maximize the Use of Noise Barriers.	G, C	CD	See (3) above	Yes
69-32	3B 11-1d: Prohibit Non-Essential Noise Sources During Construction.	G, C	CD	See (3) above	Yes
69-33	3B 11-1e: Monitor Construction Noise and Provide a Mechanism for Filing Noise Complaints.	G, C	CD	See (3) above	Yes
69-34	3B 11-3: Implement Operational Noise Minimization Measures.	engineering plans	CD	See (3) above	Yes
Parks and Recreation					
69-35	3B 12-1: Provide for Continued Recreational Access as Identified in Mitigation Measure 3.14-1a.	G, C	CD	See (3) above	Yes
Traffic and Transportation					
69-36	3B 15-1a: Prepare Traffic Control Plan.	G, C	CD	See (3) above	Yes
69-37	3B 15-1b: Assess Pre-Off-site Water Facilities Roadway Conditions.	G, C	CD	See (3) above	Yes
Utilities and Service Systems					
69-38	3B 16-3a: Minimize Utility Conflicts by Implementing an Underground Services Alert.	G, C	CD	See (3) above	Yes
69-39	3B 16-3b: Coordinate with Utility Providers and Implement Appropriate Installation Methods to Minimize Potential Utility Service Disruptions.		CD	See (3) above	Yes
Groundwater					
69-40	3B 17-1a: Implement Construction Dewatering Best Management Practices.	G, C	CD CDFW or CVRWQB	See (3) above	Yes

69-41	3B.17-1b: Implement a Dewatering Discharge Monitoring Program.	G, C	CD CDFW or CVRWQB	See (3) above	Yes
Water Supply					
69-42	3A.18-1: Submit Proof of Surface Water Supply Availability.	M, B	CD, PW	See (3) above	Yes
69-43	3A.18-2a: Submit Proof of Adequate Off-Site Water Conveyance Facilities and Implement Off-Site Infrastructure Service System or Ensure That Adequate Financing Is Secured.	M, B	CD, PW	See (3) above	Yes
MISCELLANEOUS					
70.	The Owner/Applicant acknowledges that the State adopted amendments to Section 65850 of the California Government Code (specifically Section 65850(g)), effective January 1, 2018, to allow for the implementation of inclusionary housing requirements in residential rental units, upon adoption of an ordinance by the City. Under the First Amended and Restated Tier 1 Development Agreement, the City vested the Owner/Applicant's rights in the affordable housing provisions contained in the City's then-existing Housing Element and Municipal Code until January 1, 2020. In the event the City amends its Inclusionary Housing Ordinance after January 1, 2020 with respect to rental housing pursuant to Section 65850(g), the Project shall be subject to such amendments should any residential rental project be proposed within the Project.	OG	CD	The owner/applicant has acknowledged that they are currently subject to the inclusionary housing requirements in residential rental units and further acknowledged that they will be subject to any amendments to the FMC-Housing Element at such time amendments are made after January 1, 2020.	Yes

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Folsom City Council Staff Report



MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No 10841 – A Resolution Ratifying the City Manager’s Execution of a Purchase Agreement with Golden State Fire Apparatus Inc. for a Ladder Truck from Pierce Manufacturing Inc. Pursuant to Folsom Municipal Code Section 2.36.150, and the Approval of an Interfund Loan and Appropriation of Funds
FROM:	Finance Department

RECOMMENDATION / CITY COUNCIL ACTION

Staff recommends the City Council approve Resolution No. 10841 - A Resolution Ratifying the City Manager’s Execution of a Purchase Agreement with Golden State Fire Apparatus, Inc for a Ladder Truck from Pierce Manufacturing, Inc. Pursuant to Folsom Municipal Code Section 2.36.150, and the Approval of an Interfund Loan and Appropriation of Funds.

UPDATE

This item was considered and approved by the Council at its regular meeting on April 26, 2022. Unfortunately, the public was not able to participate in that meeting remotely due to technical difficulties. As a result, staff is requesting that the Council reconsider and approve this item on May 10, 2022.

Before the City Council meeting on April 26th, the manufacturer notified staff that new purchases will be subject to a seven percent (7%) increase starting on April 28, 2022. Consequently, the purchase price of this ladder truck would increase by \$140,000 unless the contract was signed before April 28th. Pursuant to Folsom Municipal Code section 2.36.150, the contract was signed on April 27, 2022 to avoid that significant cost increase.

BACKGROUND / ISSUE

The City of Folsom Fire Department's current 100-foot aerial ladder truck was purchased in 2014 and currently has 54,732 road miles. The vehicle has seen an increase in annual vehicle maintenance and repair costs causing it to be out of service for an extended period.

Staff was notified by Pierce Manufacturing, Inc. that a 7% price increase on new purchases would go into effect on April 28, 2022. In order to secure the purchase prior to the price increase staff is requesting approval for an interfund loan in a not to exceed amount of \$2.0 million.

POLICY / RULE

In accordance with Folsom Municipal Code Section 2.36.080, contracts for supplies, equipment, services, and construction with a value of \$66,141 or greater shall be awarded by City Council.

ANALYSIS

The Fire Department operates one aerial ladder truck in its fleet. The apparatus is staffed on a daily basis to support the mission and tasks it routinely performs throughout the City and region. The truck is equipped with a hydraulic aerial ladder device, which allows firefighters to reach the roofs of commercial buildings up to 100 feet in height. The aerial ladder also allows firefighters to direct water on a large fire from an elevated position which is critical in keeping large fires contained to their building of origin. The apparatus also carries a large amount of equipment to meet the fire, rescue, and emergency medical tasks that it may be assigned.

The build time for this type of apparatus is approximately two years. Due to the length of time needed for the build, the increasing annual maintenance costs, prolonged out of service time on the current truck, and the upcoming price increase staff is requesting approval for the purchase of the ladder truck in the current fiscal year. An additional appropriation for this purchase will be required.

Staff is also requesting approval for an interfund loan from the Highway 50 Improvements Fund (Fund 443) to the General Fund (Fund 010) in a not to exceed amount of \$2.0 million. The Highway 50 Improvements Fund has sufficient funds available for the loan and will be repaid with interest earnings it would have received had there not been a loan, in order to not have a loss to the fund. The loan will be for a period of no more than 10 years and the rate of interest will be the quarterly rate of return the City receives on its pooled investments.

FINANCIAL IMPACT

The purchase of the ladder truck will be for a not-to-exceed amount of \$2.0 million. This purchase will require an appropriation in the current fiscal year in the General Fund (Fund 010)

Fire Department Operating Budget for the purchase of the truck and in the Highway 50 Improvements Fund (Fund 443) for the loan to the General Fund.

The loan from the Highway 50 Improvements Fund will provide the upfront funding for the purchase and the General Fund will repay the loan over no more than 10 years at a rate of interest equal to the quarterly rate of return as shown on the City's investment report each quarter. On April 12, 2022 the City Council approved the use of \$235,000 of American Rescue Plan Act funding which will be utilized for the first payment due as repayment of the interfund loan.

ATTACHMENTS

1. Resolution No 10841 – A Resolution Ratifying the City Manager's Execution of a Purchase Agreement with Golden State Fire Apparatus Inc. for a Ladder Truck from Pierce Manufacturing Inc. Pursuant to Folsom Municipal Code Section 2.36.150, and the Approval of an Interfund Loan and Appropriation of Funds
2. Golden State Fire Apparatus Proposal

Submitted,



Stacey Tamagni, Finance Director

ATTACHMENT 1

RESOLUTION NO. 10841**A RESOLUTION RATIFYING THE CITY MANAGER'S EXECUTION OF A PURCHASE AGREEMENT WITH GOLDEN STATE FIRE APPARATUS INC. FOR A LADDER TRUCK FROM PIERCE MANUFACTURING, INC. PURSUANT TO FOLSOM MUNICIPAL CODE SECTION 2.36.150, AND THE APPROVAL OF AN INTERFUND LOAN AND APPROPRIATION OF FUNDS**

WHEREAS, the Fire Department staff has validated the need to acquire a replacement aerial ladder truck for use within the City; and

WHEREAS, staff reviewed and recommended participation in a cooperative purchasing agreement with the Houston-Galveston Area Council utilizing Golden State Fire Apparatus Inc., for purchase of a new ladder truck to ensure purchase of the best product at the best price; and

WHEREAS, the current ladder truck is experiencing extended periods of down time due to maintenance and repairs; and

WHEREAS, the build time for this type of vehicle is approximately two years; and

WHEREAS, the manufacturer has notified the City that a 7% increase on new purchases will occur on April 28, 2022; and

WHEREAS, an interfund loan will allow the City to secure the production of the ladder truck prior to the cost increase; and

WHEREAS, an interfund loan would provide the upfront funding, and the loan would be between the General Fund (Fund 010) and the Highway 50 Improvements Fund (Fund 443) for a not-to-exceed amount of \$2.0 million, and will be repaid over no more than 10 years; and

WHEREAS, the interest rate on the interfund loan will be at the quarterly rate of return as shown on the City's investment report each quarter to not cause any loss to the Highway 50 Improvements Fund; and

WHEREAS, sufficient funds are available in the Highway 50 Improvements Fund (Fund 443) to provide the loan to the General Fund in a not-to-exceed amount of \$2.0 million; and

WHEREAS, the agreement will be in a form acceptable to the City Attorney; and

WHEREAS, this item was considered and approved by the Council on April 26, 2022 but the public was not able to participate remotely in the meeting due to technical difficulties; and

WHEREAS, the City Manager signed the agreement on April 27, 2022 pursuant to Folsom Municipal Code section 2.36.150, Emergency Procurement, to avoid a cost increase of \$140,000;

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom hereby ratifies the City Manager's execution of a purchase agreement with Golden State Fire Apparatus, Inc. for a ladder truck from Pierce Manufacturing, Inc. pursuant to Folsom Municipal Code section 2.36.150.

BE IT FURTHER RESOLVED that the City Council of the City of Folsom authorizes an interfund loan to the General Fund (Fund 010) from the Highway 50 Improvements Fund (Fund 443) in a not-to-exceed amount of \$2.0 million to be repaid over not more than 10 years at the rate of interest equal to the quarterly rate of return as shown on the City's investment report each quarter.

BE IT FURTHER RESOLVED the Finance Director is authorized to appropriate in Fiscal Year 2021-22 in the Highway 50 Improvements Fund (Fund 443) a loan for the not-to-exceed amount of \$2.0 million and to appropriate an additional \$2.0 million in the General Fund (Fund 010) for the purchase of the ladder truck in Fiscal Year 2021-22.

PASSED AND ADOPTED this 10th day of May, 2022, by the following roll-call vote:

- AYES:** Councilmember(s):
- NOES:** Councilmember(s):
- ABSENT:** Councilmember(s):
- ABSTAIN:** Councilmember(s):

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

ATTACHMENT 2



**GOLDEN
STATE**
FIRE APPARATUS

PROPOSAL PREPARED FOR
City of Folsom
Pierce Manufacturing, Inc.
107' Velocity Ascendant Tiller
HGAC FS12-19, Code FS19VA11
April 15, 2022

SALES CONSULTANT
Ryan Wright
Golden State Fire Apparatus, Inc.
7400 Reese Road
Sacramento, CA 95828
209.613.3809 Cell
ryan@goldenstatefire.com

PARTS, SERVICE & SUPPORT
Golden State Emergency Vehicle Service, Inc.
7400 Reese Road
Sacramento, CA 95828
916.330.1638 Office
parts@goldenstatefire.com



www.goldenstatefire.com
 7400 Reese Road
 Sacramento, CA 95828
 Office 916.330.1638
 Fax 916.330.1649

PROPOSAL PREPARED FOR:

City of Folsom
 535 Glenn Drive
 Folsom, CA 95630

Submitted Date:	April 15, 2022
Proposal Number:	60415-22A
Expiration Date:	April 28, 2022
Sales Consultant:	Ryan Wright

We hereby propose and agree to furnish, after your acceptance of this proposal and the proper execution by the CITY OF FOLSOM, hereinafter called "Customer" and an officer of Golden State Fire Apparatus, Inc., hereinafter called "GSFA", the following fire apparatus and equipment, hereinafter called "Product":

#	Description	Unit Price
A	One (1) Pierce Manufacturing, Inc. 107' Velocity Ascendant Tiller	1,809,362.08
B	Fire Fighting Equipment	183,000.00
C	Discount for HGAC FS12-19, Product Code FS19VA11	(54,530.00)
D	Pre-Payment Discount for 100% Payment at Time of Order	(81,696.58)
E	SUBTOTAL	1,856,135.50
F	7.75% State Sales Tax	143,850.50
G	California Tire Fee	14.00
H	GRAND TOTAL	2,000,000.00



PROPOSAL SUMMARY

This proposal includes the following items in accordance with the specifications hereto attached:

- Fire apparatus and equipment
- Delivery to GSFA service center in Sacramento
- Final Delivery to Customer head quarters
- Demonstration and familiarization of the Product

PRODUCT COMPLETION

Product shall be built in accordance with the specifications hereto attached, delays due to acts of God, strikes, war, or intentional conflict, failures to obtain chassis, materials, unusual weather conditions or other causes beyond GSFA's control not preventing, within approximately **750 to 780 CALENDAR DAYS** after receipt of this order and the acceptance thereof at our Sacramento, California office. Within thirty (30) calendar days after receipt of this order and acceptance thereof, GSFA shall submit to Customer a production schedule including tentative pre-construction conference, final inspection and final delivery dates.

DELIVERY LOCATION

Product shall be shipped in accordance with the specifications hereto attached and be delivered to you at **FOLSOM, CALIFORNIA**. Proof of insurance must be demonstrated by the Customer to GSFA prior to transferring of the Product(s).

ACCEPTING THIS PROPOSAL

In the event Customer wishes to purchase the Product described in this Proposal and the attached specifications, then, prior to the expiration date listed on page 2 of this Proposal, Customer shall sign and return this Proposal. Thereafter, GSFA and Customer will endeavor to enter into a purchase agreement incorporating this Proposal and including additional terms (a "Purchase Agreement"). If Customer returns a signed copy of this Proposal alone, GSFA will send Customer its form of Purchase Agreement for Customer's review and signature. **If Customer desires to use its standard form of purchase order as the Purchase Agreement, then Customer should return a signed copy of this Proposal along with a copy of such purchase order. All purchase orders shall be made out to GSFA.** GSFA will review such purchase order and contact the Customer regarding any required revisions. Only upon a full execution of a Purchase Agreement shall GSFA and Customer be obligated to purchase and sell the Product set forth in this Proposal.

TERMS AND CONDITIONS

The following Terms and Conditions are hereby made part of this Proposal:

1. Payment Terms (100% Pre-Payment at Time of Order) – Customer shall pay the amount listed on page one of this Proposal, which includes: (i) the total price for the Product (the "Purchase Price"), (ii) the estimated state sales tax on the Product, and (iii) the California tire fee (together with the Purchase Price and estimated state sales tax, the "Grand Total") within fifteen (15) calendar days from the date on which the Purchase Agreement is fully executed. The proposed delivery timeframe for the Product, which is outlined on page one of this Proposal, shall not begin until full payment of the Grand Total is received. In the event Customer does not pay GSFA the Grand Total in the timeframe set forth in this Section 1, GSFA may, in its sole discretion, cancel the Purchase Agreement entered into between the parties.

2. Multiple Unit Purchase – If the Purchase Price includes pricing for multiple units, the price stated on this Proposal shall only be valid if the quantity of Products being proposed are purchased at the same time, pursuant to the same Purchase Agreement.

3. Stock / Demo Units – If applicable, any stock/demo units, including those identified by this Proposal, are available for sale on an as-is, first-come and first served-basis. Regardless of this Proposal, the first Customer to enter into a Purchase Agreement identifying any such stock/demo units shall obtain said units.

4. Order Changes – The Customer may request that GSFA incorporate a change to the Product or the Specifications for the Product by delivering a written change order to GSFA, which shall include a description of the proposed change sufficient to permit GSFA to evaluate the feasibility of such change (a "Change Order"). GSFA will provide Customer a written response (a "Response") stating (i) whether GSFA will accommodate such Change Order (which GSFA may decide in its sole and absolute discretion) and (ii) the terms of the modification to the order, including any increase or decrease in the Purchase Price resulting from such Change Order, and any effect on production scheduling or Delivery resulting from such Change Order. Customer shall have seven (7) days after receipt of the Response to notify GSFA as to whether Customer desires to make the changes GSFA has approved in the Response. In the event Customer counter-signs GSFA's Response, Customer shall pay the increase (or be refunded the decrease) in the Purchase Price prior to final delivery to Customer location.

5. Force Majeure – GSFA shall not be responsible nor deemed to be in default on account of delays in performance due to causes which are beyond GSFA's and manufacturer's control and which make GSFA's performance impracticable, including but not limited to wars, insurrections, strikes, riots, fires, storms, floods, other acts of nature, explosions, earthquakes, accidents, any act of government, delays in transportation, inability to obtain necessary labor supplies or manufacturing facilities, allocation regulations or orders affecting materials, equipment, facilities or completed products, failure to obtain any required license or certificates, acts of God or the public enemy or terrorism, failure of transportation, epidemics, quarantine restrictions, failure of vendors (due to causes similar to those within the scope of this clause) to perform their contracts or labor troubles causing cessation, slowdown, or interruption of work.

6. Cancellation/Termination – In the event Customer and GSFA enter into a Purchase Agreement and Customer thereafter cancels or terminates the Purchase Agreement, GSFA will charge a cancellation fee as follows: (a) 10% of the Purchase Price after order is accepted and entered by GSFA; (b) 20% of the Purchase Price after completion of the pre-construction phase of the order process; and (c) 50% of the Purchase Price after the requisition of any materials or commencement of any manufacturing or assembly of the Product by either GSFA or the manufacturer of the Product. The tier of cancellation fee applicable to any cancellation shall be in the sole and absolute discretion of GSFA.

7. State Sales Tax – Customer shall be responsible for the cost of state sales tax associated with, or attributable to the Product. The taxes owed by Customer for the Product is subject to adjustment for the applicable state sales tax rate in effect when the Product is delivered to the Customer. Therefore, the sales tax will be increased or decreased at the time of delivery if a change in the sales tax rate has occurred, in which case Customer shall pay GSFA (or be refunded by GSFA) the applicable change in sales tax.

8. Proposal Expiration – After the Expiration Date shown on page one of this Proposal, Customer shall require GSFA's written consent to accept this Proposal.

9. Governing Law – This Proposal is to be governed by and under the laws of the state of California.

Thank you for providing Golden State Fire Apparatus, Inc. with the opportunity to provide this proposal. If you have any questions regarding the options presented or need additional options, please contact me.

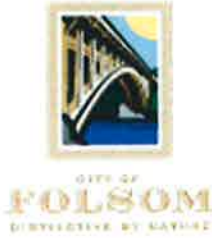
Sincerely,

I, _____ authorized representative of CITY OF FOLSOM agrees to purchase the proposed Product(s) and agree to the terms and conditions of this proposal and the specifications hereto attached.

Ryan Wright
Golden State Fire Apparatus, Inc.

SIGNATURE: _____

TITLE: _____ DATE: _____



Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10842 - A Resolution Authorizing the City Manager to Execute an Agreement with McGuire and Hester for the Construction of the Water Treatment Plant Backwash and Recycled Water Upgrades Project and Appropriation of Funds
FROM:	Environmental and Water Resources Department

RECOMMENDATION / CITY COUNCIL ACTION

The Environmental and Water Resources Department recommends the City Council pass and adopt Resolution No. 10842 - A Resolution Authorizing the City Manager to Execute an Agreement with McGuire and Hester for the Construction of the Water Treatment Plant Backwash and Recycled Water Upgrades Project and Appropriation of Funds.

BACKGROUND / ISSUE

The Environmental and Water Resources (EWR) Department identifies infrastructure rehabilitation and replacement projects through water and sewer master plans, ongoing condition assessment programs, and regulatory changes. EWR staff completed an analysis of potential Water Treatment Plant capacity and reliability projects. Through these efforts, EWR staff identified the Water Treatment Plant Backwash and Recycled Water Upgrades Project as a priority project.

There are two Reclaimed Backwash (RBW) ponds (RBW #1 and RBW #2) at the Water Treatment Plant (WTP) that store backwash water, which is water used to clean the filters. This backwash water can be recycled back to the headworks of the WTP. The US Environmental Protection Agency (EPA) allows water agencies to recycle up to 10% of the backwash water based on the water treatment plant capacity. The WTP's current firm capacity is 35 million gallons per day (MGD), and 10% of the current firm capacity, or 3.5 MGD of backwash water,

can be recycled back into the system. The WTP's future firm capacity will be 50 MGD. This project will increase redundancy and reliability to allow for 10% of the future firm capacity, or 5.0 MGD of backwash water, to be recycled back into the WTP system. Additionally, increasing the recycled water capacity greatly reduces the City's risk for the RBW ponds to potentially overflow.

The current piping configuration at the inlet of the RBW ponds only feeds RBW #2. This project will upgrade the piping arrangement to feed either RBW #1 or RBW #2 with isolation valves. Having this operational flexibility in feeding either pond will allow for the maintenance and cleaning of one pond at a time, while the other is still operating. This project also involves upgrading the submersible pumps in the decant pump station (DPS) at the outlet of the RBW ponds to meet the peak flow of approximately 5.0 MGD. Additionally, the DPS control system will be upgraded to provide improved operational controls and flexibility. The DPS wet well also has aging and leaking slide gates that will be replaced.

This resolution will authorize the City Manager to execute an agreement with McGuire and Hester for the construction of the Water Treatment Plant Backwash and Recycled Water Upgrades Project in the amount of \$1,834,000, and the budget for this agreement to include a 10% contingency in the amount of \$183,400 and Appropriation of Funds.

POLICY / RULE

In accordance with Chapter 2.36 of the Folsom Municipal Code, supplies, equipment, services, and construction with a value of \$62,657 or greater shall be awarded by City Council.

ANALYSIS

The City completed plans and specifications for the Water Treatment Plant Backwash and Recycled Water Upgrades Project and publicly advertised for bids on February 16, 2022. In addition, the City provided these documents to www.ciplist.com which is also picked up by several area builders' exchanges and the City of Folsom's website. The Environmental and Water Resources Department received the following bids on March 25, 2022, for construction of the Water Treatment Plant Backwash and Recycled Water Upgrades Project:

Contractor	Bid Amount
McGuire and Hester	\$1,834,000
T & S Construction Co., Inc.	\$2,110,000
Lorang Brothers Construction, Inc.	\$2,280,329
Syblon Reid	\$2,290,000
TNT Industrial Contractors, Inc.	\$2,297,956

Staff has reviewed the bids submitted and has determined that McGuire and Hester is the lowest responsible and responsive bidder who meets the requirements and specifications set forth in the invitation for bids.

FISCAL IMPACT

The Environmental and Water Resources Department recommends that the contract be awarded to the lowest responsible, responsive bidder, McGuire and Hester for \$1,834,000 with the project budgeted for this agreement in the amount of \$2,017,400 which will include a 10% contingency in the amount of \$183,400.

Approximate project costs to date, which include design, construction administration services, construction management and inspection services total approximately \$362,625. The Water Treatment Plant Backwash and Recycled Water Upgrades Project was included in the FY 2021-22 Capital Improvement Plan with a total project budget of \$1,072,200, which was the original assumed project cost from the initial inclusion in the FY 2017/18 Capital Improvement Plan. Therefore, an additional appropriation will be needed for construction costs, and staff is requesting an appropriation of \$1,307,825 of which \$850,086 will be appropriated in the Water Operating Fund (Fund 520) and \$457,739 in the Water Capital Fund (Fund 521) for a total project budget of \$2,380,025. Funds are available in both Fund 520 and Fund 521 for this additional appropriation.

In February of 2021, the City was awarded a financial assistance agreement from the U.S. Department of the Interior, Bureau of Reclamation (BOR) WaterSMART Drought Response Program for \$750,000 to be utilized on three City water projects: Ashland Water System Rehabilitation Project No.1, The Empire Ranch Non-Potable Well Project, and the Water Treatment Plant Backwash and Recycled Water Upgrades Project. Of the \$750,000 grant, \$250,000 was reserved for the construction of the Water Treatment Plant Backwash and Recycled Water Upgrades Project. Following completion of construction of the Water Treatment Plant Backwash and Recycled Water Upgrades Project, the City will submit to the BOR for reimbursement of the allotted \$250,000 with \$162,500 to be applied to the Water Operating Fund (Fund 520) and \$87,500 to the Water Capital Fund (Fund 521).

ENVIRONMENTAL REVIEW

This project is replacement and/or improvement of existing infrastructure with negligible or no expansion of use and therefore is categorically exempt from environmental review under the California Environmental Quality Act as noted in Title 14 – California Code of Regulations, Chapter 3 – Guidelines for Implementation of the California Environmental Quality Act, Article 19 – Categorical Exemptions, Sections 15301 (Existing Facilities), 15302 (Replacement or Reconstruction), and/or 15304 (Minor Alterations to Land).

ATTACHMENT

Resolution No. 10842 - A Resolution Authorizing the City Manager to Execute an Agreement with McGuire and Hester for the Construction of the Water Treatment Plant Backwash and Recycled Water Upgrades Project and Appropriation of Funds

Submitted,

Marcus Yasutake, Director
ENVIRONMENTAL AND WATER RESOURCES DEPARTMENT

RESOLUTION NO. 10842**A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE AN AGREEMENT WITH MCGUIRE AND HESTER FOR THE CONSTRUCTION OF THE WATER TREATMENT PLANT BACKWASH AND RECYCLED WATER UPGRADES PROJECT AND APPROPRIATION OF FUNDS**

WHEREAS, the City of Folsom has identified this project as a priority to maintain the integrity and operation of the water treatment system; and

WHEREAS, the project is categorically exempt from environmental review under the California Environmental Quality Act; and

WHEREAS, plans and specifications were prepared for this work and publicly advertised for bids on February 16, 2022; and

WHEREAS, McGuire and Hester was the lowest responsible and responsive bidder, with an amount of \$1,834,000; and

WHEREAS, this project was included in the FY 2021-22 Capital Improvement Plan with a project budget of \$1,072,200 staff is now projecting the total project costs will be \$2,380,025; and

WHEREAS, an additional appropriation of funds in the amount of \$1,307,825 is needed for a revised project budget of \$2,380,025 and sufficient funds are available in the Water Operating Fund (Fund 520) and the Water Capital Fund (Fund 521) for this additional appropriation; and

WHEREAS, the agreement will be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the City Manager to execute an agreement with McGuire and Hester for the Construction of the Water Treatment Plant Backwash and Recycled Water Upgrades Project for a not-to-exceed amount of \$1,834,000 with the budgeted amount to include a 10% contingency of \$183,400 for a total of \$2,017,400.

BE IT FURTHER RESOLVED that the Finance Director is authorized to appropriate \$1,307,825 for this agreement. The appropriation will be from the Water Operating Fund (Fund 520) in the amount of \$850,086 and from the Water Capital Fund (Fund 521) in the amount of \$457,739, for a total project budget of \$2,380,025.

PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

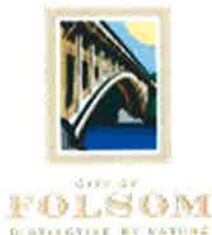
AYES: Councilmember(s):
NOES: Councilmember(s):

ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK



Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10843 - A Resolution Authorizing the City Manager to Execute an Agreement with Inferrera Construction Management Group, Inc. for Construction Management and Inspection Services for the Water Treatment Plant Backwash and Recycled Water Upgrades Project
FROM:	Environmental and Water Resources Department

RECOMMENDATION / CITY COUNCIL ACTION

The Environmental and Water Resources Department recommends the City Council pass and adopt Resolution No. 10843 - A Resolution Authorizing the City Manager to Execute an Agreement with Inferrera Construction Management Group, Inc. for Construction Management and Inspection Services for the Water Treatment Plant Backwash and Recycled Water Upgrades Project.

BACKGROUND / ISSUE

The Environmental and Water Resources (EWR) Department identifies infrastructure rehabilitation and replacement projects through water and sewer master plans, ongoing condition assessment programs, and regulatory changes. EWR staff completed an analysis of potential Water Treatment Plant capacity and reliability projects. Through these efforts, EWR staff identified the Water Treatment Plant Backwash and Recycled Water Upgrades Project as a priority project.

There are two Reclaimed Backwash (RBW) ponds (RBW #1 and RBW #2) at the Water Treatment Plant (WTP) that store backwash water, which is water used to clean the filters. This backwash water can be recycled back to the headworks of the WTP. The US Environmental Protection Agency (EPA) allows water agencies to recycle up to 10% of the backwash water

based on the water treatment plant capacity. The WTP’s current firm capacity is 35 million gallons per day (MGD), and 10% of the current firm capacity, or 3.5 MGD of backwash water, can be recycled back into the system. The WTP’s future firm capacity will be 50 MGD. This project will increase redundancy and reliability to allow for 10% of the future firm capacity, or 5.0 MGD of backwash water, to be recycled back into the WTP system. Additionally, increasing the recycled water capacity greatly reduces the City’s risk for the RBW ponds to potentially overflow.

The current piping configuration at the inlet of the RBW ponds only feeds RBW #2. This project will upgrade the piping arrangement to feed either RBW #1 or RBW #2 with isolation valves. Having this operational flexibility in feeding either pond will allow for the maintenance and cleaning of one pond at a time, while the other is still operating. This project also involves upgrading the submersible pumps in the decant pump station (DPS) at the outlet of the RBW ponds to meet the peak flow of approximately 5.0 MGD. Additionally, the DPS control system will be upgraded to provide improved operational controls and flexibility. The DPS wet well also has aging and leaking slide gates that will be replaced.

This resolution will authorize the City Manager to execute an agreement with Inferrera Construction Management Group, Inc. for construction management and inspection services for the Water Treatment Plant Backwash and Recycled Water Upgrades Project in the amount of \$184,951.

POLICY / RULE

In accordance with Chapter 2.36 of the Folsom Municipal Code, supplies, equipment, services, and construction with a value of \$66,141 or greater shall be awarded by City Council.

ANALYSIS

In October of 2018, the EWR Department completed a pre-qualification process for consultants for construction management and inspection services. Inferrera Construction Management Group, Inc., HydroScience Engineers, Inc., and NV5, Inc. were three of the consulting firms selected to provide these services for this type of project through the pre-qualification process.

On April 1, 2022, the City requested proposals from these consultants for construction management and inspection services for the Water Treatment Plant Backwash and Recycled Water Upgrades Project. The EWR department received proposals from Inferrera Construction Management Group, Inc., and HydroScience Engineers, Inc., on April 15, 2022, and NV5, Inc. chose to not submit a proposal.

The fee schedules from the proposals received are outlined below:

Consultant	Fee Amount
Inferrera Construction Management Group, Inc.	\$184,951
HydroScience Engineers, Inc.	\$226,640
NV5, Inc.	N/A

EWR staff evaluated the consultant submittals based on relevant project experience, the consultant's understanding of the background and requirements of the project, qualifications and experience of the consultant's construction management team, previous work with municipalities, cost and best value to the City.

Inferrera Construction Management Group, Inc. was determined to provide the best value to the City based on the fee amount, past municipal project experience involving work of similar scope and complexity, staff coordination, and abilities for performing these types of construction management and inspection services.

The construction management and inspection for this project will require approximately 872 person-hours of highly technical and specialized services, including full-time construction inspection. The proposed fee for construction management and inspection services is consistent with recent City construction projects with respect to the construction duration. Construction management services require full-time, on-site inspection, and include other necessary tasks such as project schedule tracking, review and/or coordination of project submittals, coordination with the other on-going City construction projects, materials testing, and overall owner representation throughout project construction.

This resolution will authorize the City Manager to execute an agreement with Inferrera Construction Management Group, Inc. for construction management and inspection services for the Water Treatment Plant Backwash and Recycled Water Upgrades Project in the amount of \$184,951.

FINANCIAL IMPACT

The Water Treatment Plant Backwash and Recycled Water Upgrades Project was included in the FY 2021-22 Capital Improvement Plan with a total project budget of \$1,072,200. Sufficient funds are available in both the Water Operating Fund (520) and Water Capital Fund (521) and the project for this agreement.

ENVIRONMENTAL REVIEW

This project is replacement and/or improvement of existing infrastructure with negligible or no expansion of use and therefore is categorically exempt from environmental review under the California Environmental Quality Act as noted in Title 14 – California Code of Regulations, Chapter 3 – Guidelines for Implementation of the California Environmental Quality Act, Article 19 – Categorical Exemptions, Sections 15301 (Existing Facilities), 15302 (Replacement or Reconstruction), and/or 15304 (Minor Alterations to Land).

ATTACHMENT

Resolution No. 10843 - A Resolution Authorizing the City Manager to Execute an Agreement with Inferrera Construction Management Group, Inc. for Construction Management and

Inspection Services for the Water Treatment Plant Backwash and Recycled Water Upgrades
Project

Submitted,

Marcus Yasutake, Director
ENVIRONMENTAL AND WATER RESOURCES DEPARTMENT

RESOLUTION NO. 10843

A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE AN AGREEMENT WITH INFERRERA CONSTRUCTION MANAGEMENT GROUP, INC. FOR CONSTRUCTION MANAGEMENT AND INSPECTION SERVICES FOR THE WATER TREATMENT PLANT BACKWASH AND RECYCLED WATER UPGRADES PROJECT

WHEREAS, the City has identified this project as a priority to maintain integrity and operation of the water treatment system; and

WHEREAS, Inferrera Construction Management Group, Inc. by reason of their past experience and abilities for performing these types of services, are qualified to perform the required construction management and inspection services for the project and provide the best value to the City; and

WHEREAS, the Water Treatment Plant Backwash and Recycled Water Upgrades Project was included in the FY 2021-22 Capital Improvement Plan; and

WHEREAS, sufficient funds are available in the both the Water Operating Fund (Fund 520) and the Water Capital Fund (Fund 521) for this agreement; and

WHEREAS, the agreement will be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the City Manager to execute an agreement with Inferrera Construction Management Group, Inc. for construction management and inspection services for the Water Treatment Plant Backwash and Recycled Water Upgrades Project for a not-to-exceed amount of \$184,951.

PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

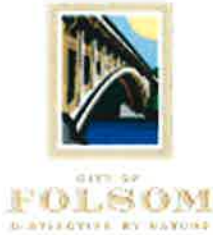
AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

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Folsom City Council Staff Report



MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10844 – A Resolution Approving the Preliminary Engineer’s Report for the following Landscaping and Lighting Districts for Fiscal Year 2022-2023 and Setting Public Hearing for American River Canyon North, American River Canyon North No. 2, American River Canyon North No. 3, Blue Ravine Oaks, Blue Ravine Oaks No. 2, Briggs Ranch, Broadstone, Broadstone No. 4, Broadstone Unit No. 3, Cobble Ridge, Cobble Hills Ridge II/Reflections II, Folsom Heights, Folsom Heights No. 2, Hannaford Cross, Lake Natoma Shores, Los Cerros, Natoma Station, Natoma Valley, Prairie Oaks Ranch, Prairie Oaks Ranch No. 2, Prospect Ridge, Sierra Estates, Silverbrook, Steeplechase, The Residences at American River Canyon, The Residences at American River Canyon II, Willow Creek Estates East, Willow Creek Estates East No. 2, Willow Creek Estates South, and Willow Springs
FROM:	Parks and Recreation Department

RECOMMENDATION / CITY COUNCIL ACTION

Staff recommends the City Council adopt Resolution No. 10844 – A Resolution Approving the Preliminary Engineer’s Report for the following Landscaping and Lighting Districts for Fiscal Year 2022-2023 American River Canyon North, American River Canyon North No. 2, American River Canyon North No. 3, Blue Ravine Oaks, Blue Ravine Oaks No. 2, Briggs Ranch, Broadstone, Broadstone No. 4, Broadstone Unit No. 3, Cobble Ridge, Cobble Hills Ridge II/Reflections II, Folsom Heights, Folsom Heights No. 2, Hannaford Cross, Lake Natoma Shores, Los Cerros, Natoma Station, Natoma Valley, Prairie Oaks Ranch, Prairie Oaks Ranch No. 2, Prospect Ridge, Sierra Estates, Silverbrook, Steeplechase, The Residences at American River Canyon, The Residences at American River Canyon II, Willow Creek Estates East, Willow Creek Estates East No. 2, Willow Creek Estates South, and Willow Springs.

BACKGROUND / ISSUE

The City of Folsom has thirty existing Landscaping and Lighting Districts. Each year, as part of the annual assessment process, an Engineer's Report must be prepared in accordance with the requirement of Article 4 of Chapter 1 of Part 2 of Division 15 of the *Streets and Highways Code* and the *Landscaping and Lighting Act of 1972*.

The Engineer's Report for Fiscal Year 2022-2023 will address all thirty districts in one report and will be submitted for final approval to the City Council.

On March 22, 2022, the City Council approved Resolution No. 10825 – A Resolution Directing the Preparation of the Engineer's Report for American River Canyon North, American River Canyon North No. 2, American River Canyon North No. 3, Blue Ravine Oaks, Blue Ravine Oaks No. 2, Briggs Ranch, Broadstone, Broadstone No. 4, Broadstone Unit No. 3, Cobble Ridge, Cobble Hills Ridge II/Reflections II, Folsom Heights, Folsom Heights No. 2, Hannaford Cross, Lake Natoma Shores, Los Cerros, Natoma Station, Natoma Valley, Prairie Oaks Ranch, Prairie Oaks Ranch No. 2, Prospect Ridge, The Residences at American River Canyon, The Residences at American River Canyon II, Sierra Estates, Silverbrook, Steeplechase, Willow Creek Estates East, Willow Creek Estates East No. 2, Willow Creek Estates South, and Willow Springs Landscaping and Lighting Assessment Districts within the City of Folsom for FY 2022-23. The attached resolution approves the Preliminary Engineer's Report for the thirty districts, declares the continued assessment for each district and sets the date of the public hearing for final approval of the Engineer's Report. The attached Preliminary Engineer's Report for FY 2022-23 addresses all thirty districts in one report and is submitted for City Council review and approval. Included within the report are the following for each district:

- A. Plans and specifications for the maintenance of the improvements (on file in the Parks and Recreation Department).
- B. Cost estimates of maintaining the improvements.
- C. Diagram of the assessment districts.
- D. Estimated costs for maintaining the improvements.

Under the provision of Section 54954.6 of the Government Code, each year a public meeting and public hearing are to be held on the levy of assessments. The attached resolution sets the public hearing for the July 12, 2022 City Council meeting.

POLICY / RULE

The City Council is required to adopt a resolution approving the Preliminary Engineer's Report as part of the annual assessment process pursuant to Article 4 of Chapter 1 of Part 2 of Division 15 of the *Streets and Highways Code* (*Landscaping and Lighting Act of 1972*). The City Council is also required to adopt a resolution declaring intention to levy and collect assessments pursuant to Section 54954.6 of the Government Code and Section 22624 of the *Streets and Highways Code*.

ANALYSIS

The attached Preliminary Engineer's Report (Attachment 2) prepared by the Engineer of Record, SCI Consulting Group, is for all thirty Landscaping and Lighting Districts for FY 2022-23. This report (one for each district and combined into one document) is submitted for City Council review and has been prepared in accordance with the Streets and Highways Code and includes the following: plans and specifications, estimated costs and budgets, method of apportionment, the proposed assessment for FY 2022-23, and the assessment diagram.

Assessment to Properties

Assessments to properties within each district are the same as FY 2021-22, with the exception of two districts; Willow Creek Estates East No. 2, and Broadstone 4. Willow Creek Estates East No. 2 has 3 zones, zones A & B have an increase of \$3.98 from \$99.53 last year to \$103.51 this year. Zone C has an increase of \$3.66 over last year bringing their rate from \$91.49 last year to \$95.15 this year. Broadstone 4 has 4 zones, zone A has an increase of \$1.17 this year from \$38.81 last year to \$39.98 this year. Zone B has an increase of \$1.10 over last years rate and increased from 36.96 last year to 38.06 this year. Zone C has an increase of \$1.10 over last years rate and increased from \$36.42 to \$37.52. Zone D had an increase of \$1.06 this year and increased from \$35.74 last year to \$36.80 this year. There are another ten districts that have escalators and are eligible for CPI increases that will not be utilized this year. Those districts are American River Canyon North No. 3, Blue Ravine Oaks No. 2, Broadstone 3, Cobble Ridge, Folsom Heights No.2, Natoma Valley, Prospect Ridge, The Residences, The Residences II, and Sierra Estates.

Installment Summary

The installment summaries describe short-term installments collected pursuant to Section 22660 of the Streets and Highways Code to meet the districts' future repair and replacement needs anticipated to occur within an approximate five-year time frame, as well as long-term installments collected to meet those future needs anticipated to occur within 5 to 30-year time frames.

Comparison to Last Year

District budgets for this upcoming year will continue focusing on improvements and restorations that enhance each district's commitment to water conservation, prolonging assets life, drought tolerant landscaping improvements, fire safety, and tree stewardship. As such, some districts will be retrofitting and centralizing irrigation controllers, inventorying street trees, changing out plant materials to water wise varietals, and converting over to LED Streetlights. Many of the City's districts are over 20 years old and do not have escalators built

into their rates to track with cost-of-living increases and economic changes. As such, districts being monitored for future outreach regarding a new assessment overlay district are Briggs Ranch (31 years old), and Hannaford Cross (31 years old), and Cobble Ridge II / Reflections II (28 years old). Lastly the City will be starting the first stages of outreach for an increase in Natoma Station in the 2021-22, and 2022-2023 Fiscal Years.

FINANCIAL IMPACT

Each Landscaping and Lighting District levies and collects funds to cover operating and maintenance costs. There is no fiscal impact to the City of Folsom General Fund. Below is a summary of the proposed assessments for FY 2022-23. There are twenty-eight (28) districts in which the assessments remain the same, two (2) districts with increased assessments, and two (2) districts that are being removed from the tax roll (Union Square because it has an HOA that manages the landscape areas and Silverbrook because there is capacity in the fund balance).

District	Annual Assessment per unit	Credit or increase*	Annual Net Assessment
American River Canyon North	\$102.94	0	\$102.94
American River Canyon North No. 2	\$77.70	0	\$77.70
American River Canyon North No. 3	\$269.86	0	\$269.86
Blue Ravine Oaks	\$218.60	0	\$218.60
Blue Ravine Oaks No. 2	\$213.26	0	\$213.26
Briggs Ranch	\$122.28	0	\$122.28
Broadstone	\$164.99	0	\$164.99
Broadstone Unit No. 3	\$28.07	0	\$28.07
Broadstone No. 4			
• Zone A	\$38.81	\$1.17*	\$39.98
• Zone B	\$36.96	\$1.10*	\$38.06
• Zone C	\$36.42	\$1.10*	\$37.52
• Zone D	\$35.74	\$1.06*	\$36.80
Cobble Ridge	\$139.64	0	\$139.64
Cobble Hills Ridge II/Reflections II	\$113.14	0	\$113.14
Folsom Heights	\$70.88	0	\$70.88
Folsom Heights No. 2*			
• California Hills	\$196.42	0	\$196.42
• Folsom Heights No. 2 (Enclave)	\$208.38	0	\$208.38
Hannaford Cross	\$195.78	0	\$195.78
Lake Natoma Shores	\$183.58	0	\$183.58
Los Cerros	\$121.18	0	\$121.18
Natoma Station			

<ul style="list-style-type: none"> Natoma Station Union Square 	\$91.71 \$228.88	0 (taking off tax roll)	\$91.71 \$0.00
Natoma Valley	\$856.37	0	\$856.37
Prairie Oaks Ranch	\$213.61	0	\$213.61
Prospect Ridge	\$1,173.86	0	\$1,173.86
The Residences at ARC			
<ul style="list-style-type: none"> The Residences at ARC The Residences at ARC II 	\$536.67 \$1,169.97	0 0	\$536.67 \$1,169.97
Sierra Estates	\$363.68	0	\$363.68
Silverbrook	\$138.32	(taking off tax roll)	0.00
Steeplechase	\$157.68	0	\$157.68
Willow Creek Estates East	\$80.40	0	\$80.40
Willow Creek Estates East No. 2			
<ul style="list-style-type: none"> Zone A Zone B Zone C 	\$99.53 \$99.53 \$91.49	\$3.98* \$3.98* \$3.66*	\$103.51 \$103.51 \$95.15
Willow Creek Estates South	\$109.88	0	\$109.88
Willow Springs	\$28.14	0	\$28.14

ENVIRONMENTAL REVIEW

N/A (This does not apply as there is no environmental review aspect to the engineer's report.)

ATTACHMENTS

1. Resolution No. 10844 – A Resolution Approving the Preliminary Engineer's Report for the following Landscaping and Lighting Districts for Fiscal Year 2022-2023 American River Canyon North, American River Canyon North No. 2, American River Canyon North No. 3, Blue Ravine Oaks, Blue Ravine Oaks No. 2, Briggs Ranch, Broadstone, Broadstone No. 4, Broadstone Unit No. 3, Cobble Ridge, Cobble Hills Ridge II/Reflections II, Folsom Heights, Folsom Heights No. 2, Hannaford Cross, Lake Natoma Shores, Los Cerros, Natoma Station, Natoma Valley, Prairie Oaks Ranch, Prairie Oaks Ranch No. 2, Prospect Ridge, Sierra Estates, Silverbrook, Steeplechase, The Residences at American River Canyon, The Residences at American River Canyon II, Willow Creek Estates East, Willow Creek Estates East No. 2, Willow Creek Estates South, and Willow Springs
2. Preliminary Engineer's Report – The City of Folsom Landscaping and Lighting Districts, May 2022

Submitted,

Lorraine Poggione, Parks & Recreation Director

ATTACHMENT NO. 1

RESOLUTION NO. 10844

A RESOLUTION APPROVING THE PRELIMINARY ENGINEER'S REPORT FOR THE FOLLOWING LANDSCAPING AND LIGHTING DISTRICTS FOR FISCAL YEAR 2022-2023 AND SETTING PUBLIC HEARING FOR AMERICAN RIVER CANYON NORTH, AMERICAN RIVER CANYON NORTH NO. 2, AMERICAN RIVER CANYON NORTH NO. 3, BLUE RAVINE OAKS, BLUE RAVINE OAKS NO. 2, BRIGGS RANCH, BROADSTONE, BROADSTONE NO. 4, BROADSTONE UNIT NO. 3, COBBLE RIDGE, COBBLE HILLS RIDGE II/REFLECTIONS II, FOLSOM HEIGHTS, FOLSOM HEIGHTS NO. 2, HANNAFORD CROSS, LAKE NATOMA SHORES, LOS CERROS, NATOMA STATION, NATOMA VALLEY, PRAIRIE OAKS RANCH, PRAIRIE OAKS RANCH NO. 2, PROSPECT RIDGE, SIERRA ESTATES, SILVERBROOK, STEEPLECHASE, THE RESIDENCES AT AMERICAN RIVER CANYON, THE RESIDENCES AT AMERICAN RIVER CANYON II, WILLOW CREEK ESTATES EAST, WILLOW CREEK ESTATES EAST NO. 2, WILLOW CREEK ESTATES SOUTH, AND WILLOW SPRINGS

WHEREAS, the City Council of the City of Folsom, County of Sacramento, California, is the governing body for the following Assessment Districts (collectively the "Assessment Districts"). The proposed assessment rates for FY 2022-23 are as follows:

District	Annual Assessment per unit	Credit or increase*	Annual Net Assessment
American River Canyon North	\$102.94	0	\$102.94
American River Canyon North No. 2	\$77.70	0	\$77.70
American River Canyon North No. 3	\$269.86	0	\$269.86
Blue Ravine Oaks	\$218.60	0	\$218.60
Blue Ravine Oaks No. 2	\$213.26	0	\$213.26
Briggs Ranch	\$122.28	0	\$122.28
Broadstone	\$164.99	0	\$164.99
Broadstone Unit No. 3	\$28.07	0	\$28.07
Broadstone No. 4			
• Zone A	\$38.81	\$1.17*	\$39.98
• Zone B	\$36.96	\$1.10*	\$38.06
• Zone C	\$36.42	\$1.10*	\$37.52
• Zone D	\$35.74	\$1.06*	\$36.80
Cobble Ridge	\$139.64	0	\$139.64
Cobble Hills Ridge II/Reflections II	\$113.14	0	\$113.14
Folsom Heights	\$70.88	0	\$70.88
Folsom Heights No. 2*			
• California Hills	\$196.42	0	\$196.42
• Folsom Heights No. 2 (Enclave)	\$208.38	0	\$208.38

Hannaford Cross	\$195.78	0	\$195.78
Lake Natoma Shores	\$183.58	0	\$183.58
Los Cerros	\$121.18	0	\$121.18
Natoma Station			
• Natoma Station	\$91.71	0	\$91.71
• Union Square	\$228.88	(taking off tax roll)	\$0.00
Natoma Valley	\$856.37	0	\$856.37
Prairie Oaks Ranch	\$213.61	0	\$213.61
Prospect Ridge	\$1,173.86	0	\$1,173.86
The Residences at ARC			
• The Residences at ARC	\$536.67	0	\$536.67
• The Residences at ARC II	\$1169.97	0	\$1169.97
Sierra Estates	\$363.68	0	\$363.68
Silverbrook	\$138.32	(taking off tax roll)	0.00
Steeplechase	\$157.68	0	\$157.68
Willow Creek Estates East	\$80.40	0	\$80.40
Willow Creek Estates East No. 2			
• Zone A	\$99.53	\$3.98*	\$103.51
• Zone B	\$99.53	\$3.98*	\$103.51
• Zone C	\$91.49	\$3.66*	\$95.15
Willow Creek Estates South	\$109.88	0	\$109.88
Willow Springs	\$28.14	0	\$28.14

WHEREAS, the Engineer's Report for the Assessment Districts has been made, filed with the City Clerk and duly considered by the Council and is hereby deemed sufficient and preliminarily approved. The Engineer's Report shall stand as the Engineer's Report for all subsequent proceedings under and pursuant to this Resolution, Section 22565, et. seq., of the California Streets and Highways Code and Article XIID of the California Constitution; and

WHEREAS, it is the intention of the City Council to levy and collect assessments within the Assessment Districts for FY 2022-23. Within the Assessment Districts, the existing and proposed improvements are generally described as follows:

The improvements to be undertaken by the Assessment Districts are described as installation, maintenance and servicing of public facilities, including but not limited to, turf, ground cover, shrubs and trees, irrigation systems, drainage systems, street lighting, fencing, sound walls, sidewalks, monuments, statuary, fountains, water quality ponds, park facilities, open space, bike trails, walkways, drainage swales and other ornamental structures and facilities, entry signage, street pavers, art work, and all necessary appurtenances, and labor, materials, supplies, utilities and equipment, as applicable, for property owned or maintained by the City of Folsom. Services provided include all necessary service, operations and maintenance of the above-mentioned improvements, as applicable, for any property owned or maintained by the City of Folsom.

WHEREAS, the Assessment Districts consist of the lots and parcels shown on the boundary maps of the Assessment Districts on file with the City Clerk of the City of Folsom, and reference is hereby made to such maps for further particulars; and

WHEREAS, reference is hereby made to the Engineer's Report, on file with the City Clerk, for a full and detailed description of the improvements, the boundaries of the assessment districts and any zones therein, and the estimated cost of the improvements and the proposed assessments upon assessable lots and parcels of land within the Assessment Districts; and

WHEREAS, prior to the conclusion of the hearing, any interested person may file a written protest with the City Clerk, or, having previously filed a protest, may file a written withdrawal of that protest. A written protest shall state all grounds of objection. A protest by a property owner shall contain a description sufficient to identify the property owned by such owner; and

WHEREAS, the City Clerk shall cause a notice of the hearing to be given by publishing a notice once, at least ten (10) days prior to the date of the hearing above specified, in a newspaper circulated in the City of Folsom.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes:

1. **APPROVAL OF PRELIMINARY ENGINEER'S REPORT:** The City Council of the City of Folsom hereby approves, as submitted, the preliminary Engineer's Report for the City of Folsom Landscaping and Lighting Districts for FY 2022-23.

PROPOSED ASSESSMENT: The City Council of the City of Folsom intends to continue to levy and collect assessments during FY 2022-23 within the City of Folsom Landscaping and Lighting Districts. Annual Assessments are the same as FY 2021-22 for all Assessments Districts, with the exception of Broadstone No. 4, and Willow Creek Estates East No. 2 who will have increases in their assessments as shown in Annual Assessment Table above. Two districts, Natoma Station-Union Square and Silverbrook are being taken off the tax rolls for Fiscal Year 2022-23.

2. **REVIEW OF PRELIMINARY ENGINEER'S REPORT:** Affected property owners and interested persons may review the Engineer's Report, which contains a full and detailed description of each of the Assessment District boundaries, within the City of Folsom Landscaping and Lighting Districts, the improvements, and the proposed maintenance budget and assessments upon each parcel within each Assessment District, at the City of Folsom located at 50 Natoma Street, Folsom, California 95630 between the hours of 8:00 a.m. and 5:00 p.m., Monday through Friday.

- 3. **PUBLIC HEARING:** The City Council has scheduled a public hearing on the proposed assessments within each Assessment District on July 12 2022, at 6:30 p.m., at the City of Folsom, City Council Chambers, 50 Natoma Street, Folsom, California 95630, for the purpose of allowing public testimony regarding the proposed assessments and for the Council’s final action upon the Engineer’s Report and proposed assessments.

PASSED AND ADOPTED this 10th day of May 2022 by the following vote:

AYES: Councilmembers:

NOES: Councilmembers:

ABSENT: Councilmembers:

ABSTAIN: Councilmembers:

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

ATTACHMENT NO. 2

FY 2022-23

ENGINEER'S REPORT

City of Folsom

Landscaping and Lighting Assessment Districts

May 2022

Preliminary Engineer's Report

Engineer of Work:



4745 Mangels Boulevard
Fairfield, California 94534
707.430.4300
www.sci-cg.com

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City of Folsom

City Council

Kerri Howell, Mayor
Rosario Rodriguez, Vice Mayor
Sarah Aquino, Member
YK Chalamcherla, Member
Mike Koslowski, Member

City Manager

Elaine Andersen, City Manager

City Attorney

Steve Wang, City Attorney

Folsom Parks & Recreation Director

Lorraine Poggione

Municipal Landscape Services Manager

Zachary Perras

Engineer of Work

SCI Consulting Group

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Introduction

Overview

The City of Folsom Landscaping and Lighting Districts (the "Districts") provides funding for the installation, maintenance and servicing of landscaping, sidewalks, fences, walls, parks, open space, signage, soundwalls, street lighting, and other public improvements in the City of Folsom. Thirty such districts exist as follows:

- American River Canyon North
- American River Canyon North No. 2
- American River Canyon North No. 3
- Blue Ravine Oaks
- Blue Ravine Oaks No. 2
- Briggs Ranch
- Broadstone
- Broadstone Unit No.3
- Broadstone No. 4
- Cobble Ridge
- Cobble Hills Ridge II/Reflections II
- Folsom Heights
- Folsom Heights No. 2
- Hannaford Cross
- Lake Natoma Shores
- Los Cerros
- Natoma Station (Including Union Square Annexation)
- Natoma Valley (Formerly Lakeridge Estates)
- Prairie Oaks Ranch
- Prairie Oaks Ranch No. 2
- Prospect Ridge
- Sierra Estates
- Silverbrook
- The Residences at American River Canyon
- The Residences at American River Canyon II
- Steeplechase
- Willow Creek Estates East
- Willow Creek Estates East No. 2
- Willow Creek Estates South
- Willow Springs

These assessments were established in previous fiscal years. In each subsequent year for which the assessments will be continued, the City Council ("Council") must direct the preparation of an Engineer's Report, budgets and proposed assessments for the upcoming fiscal year. The Engineer's Report also identifies future planned projects. After the Engineer's Report is completed, the Council may preliminarily approve the Engineer's Report and proposed assessments and establish the date for a public hearing on the continuation of the assessments. This Engineer's Report ("Report") was prepared pursuant to the direction of the Council.

This Report was prepared to establish the budget for the improvements that would be funded by the proposed 2022-23 assessments and to define the benefits received from the improvements by property within the Districts and the method of assessment apportionment to lots and parcels. This Report and the proposed assessments have been made pursuant to the Landscaping and Lighting Act of 1972, Part 2 of Division 15 of the California Streets and Highways Code (the "Act") and Article XIID of the California Constitution (the "Article").

If the Council approves this Engineer's Report and the proposed assessments by resolution, a notice of the proposed assessment levies must be published in a local paper at least 10 days prior to the date of the public hearing. The resolution preliminarily approving the Engineer's Report and establishing the date for a public hearing typically directs that this notice be published.

Following the minimum 10-day time period after publishing the notice, a public hearing is held for the purpose of allowing public testimony about the proposed continuation of the assessments. This hearing is currently scheduled for July 12, 2022. At this hearing, the Council would consider approval of a resolution confirming the assessments for fiscal year 2022-23. If so confirmed and approved, the assessments would be submitted to the County Auditor/Controller for inclusion on the property tax rolls for Fiscal Year 2022-23.

Included is a separate but integral tool: the City of Folsom Landscaping and Lighting District Improvement Plan ("Improvement Plan"). It is a separate planning document that identifies the type of upcoming improvement (e.g. re-landscaping a corridor or painting a wall); the estimated cost; any installments required for short-term (less than five years) and/or long term (not greater than 30 years) improvements, and the approximate schedule for completion of the improvement. The City intends to continually update and revise the Improvement Plan throughout each year to reflect the current status of improvement projects, budget updates and/or changes in priorities.

The concept of the Improvement Plan arose from the City's commitment to comply with the requirements of the Act as well as produce a valuable instrument that enables the City to schedule, prioritize, and plan for needed maintenance and servicing improvements in the districts. It also serves as a user-friendly means for members of the public to review and understand the use of the assessment revenues generated from each district.

Legal Analysis

Silicon Valley Taxpayers Association, Inc. v Santa Clara County Open Space Authority

In July of 2008, the California Supreme Court issued its ruling on the Silicon Valley Taxpayers Association, Inc. v. Santa Clara County Open Space Authority ("SVTA vs. SCCOSA"). This ruling is the most significant court case in further legally clarifying the substantive assessment requirements of Proposition 218 which was approved by California voters in 1996. Several of the most important elements of the ruling included further emphasis that:

- Benefit assessments are for special, not general, benefit
- The services and/or improvements funded by assessments must be clearly defined
- Special benefits are directly received by and provide a direct advantage to property in each district

This Engineer's Report is consistent with the SVTA vs. SCCOSA decision and with the requirements of Article XIII C and XIII D of the California Constitution based on the following factors:

1. Most of the Districts were formed prior to the passage of Proposition 218 and/or with unanimous approval of property owners. Although these assessments are consistent with Proposition 218, the California judiciary has generally referred to pre-Proposition 218 assessments as "grandfathered assessments" and held them to a different standard than post Proposition 218 assessments.
2. The Districts are narrowly drawn to only include the specially benefiting parcels, and the assessment revenue derived from real property in each District is expended only on specifically identified improvements and/or maintenance and servicing of those improvements in that District that confer special benefits to property in that District.
3. The use of unique and narrowly drawn Districts ensures that the improvements constructed and maintained with assessment proceeds are located in close proximity to the real property subject to the assessment, and that such improvements provide direct and special benefit to the property in that District.
4. Due to their proximity to the assessed parcels, the improvements and maintenance thereof financed with assessment revenues in the District provide a direct advantage to properties in that District, and the benefits conferred on such property in each District are more extensive and direct than a general increase in property values.
5. The assessments paid in each District are proportional to the special benefit that each parcel within that District receives from such improvements and the maintenance thereof because:

- a. The specific improvements and maintenance and utility costs thereof in each District and their respective costs are specified in this Engineer's Report; and
- b. Such improvement and maintenance costs in each District are allocated among different types of property located within each District, and equally among those properties which have similar characteristics and receive similar special benefits.

Therefore, given the factors highlighted above, this Engineer's Report is consistent with the SVTA vs. SCCOSA decision and with the requirements of Article XIII C & XIII D of the California Constitution.

Dahms v. Downtown Pomona Property

On June 8, 2009, the 4th District Court of Appeal upheld a benefit assessment for property in the downtown area of the City of Pomona. On July 22, 2009, the California Supreme Court denied review. In Dahms the court upheld an assessment that was 100% special benefit (i.e. 0% general benefit) on the rationale that the services and improvements funded by the assessments were directly provided to property in the assessment district. The Court also upheld discounts and exemptions from the assessment for certain properties.

Bonander v. Town of Tiburon

On December 31, 2009, the 1st District Court of Appeal overturned a benefit assessment approved by property owners to pay for placing overhead utility lines underground in an area of the Town of Tiburon. The Court invalidated the assessments on the grounds that the assessments had been apportioned to assessed property based in part on relative costs within sub-areas of the assessment district instead of proportional special benefits.

Beutz v. County of Riverside

On May 26, 2010 the 4th District Court of Appeals issued a decision on the Steven Beutz v. County of Riverside ("Beutz") appeal. This decision overturned an assessment for park maintenance in Wildomar, California, primarily because the general benefit associated with improvements and services were not explicitly calculated and quantified and separated from the special benefits.

Golden Hill Neighborhood Association v. City of San Diego

On September 22, 2011, the San Diego Court of Appeal issued a decision on the Golden Hill Neighborhood Association V. City of San Diego appeal. This decision overturned an assessment for street and landscaping maintenance in the Greater Golden Hill neighborhood of San Diego, California. The court described two primary reasons for its decision. First, like in *Beutz*, the court found the general benefits associated with services were not explicitly calculated, quantified and separated from the special benefits. Second, the court found that the City had failed to record the basis for the assessment on its own parcels.

Compliance with Current Law

This Engineer's Report is consistent with the requirements of Article XIIC and XIID of the California Constitution and with the *SVTA* decision because the Improvements to be funded are clearly defined; the Improvements are directly available to and will directly benefit property in the Assessment Districts; and the Improvements provide a direct advantage to property in each of the Assessment Districts that would not be received in absence of the Assessments.

This Engineer's Report is consistent with *Beutz, Dahms and Greater Golden Hill* because the Improvements will directly benefit property in each of the Assessment Districts and the general benefits have been explicitly calculated and quantified and excluded from the Assessments. The Engineer's Report is consistent with *Bonander* because the Assessments have been apportioned based on the overall cost of the Improvements and proportional special benefit to each property.

Impact of Recent Proposition 218 Decisions

This Engineer's Report is consistent with the *SVTA vs. SCCOSA, Dahms, Bonander – Beutz* and *Greater Golden Hill* decisions and with the requirements of Article XIIC and XIID of the California Constitution based on the following factors:

1. The assessment revenue derived from real property in each assessment District within the City of Folsom is extended only on specific landscaping and other improvements and/or maintenance and servicing of those improvements in that assessment district
2. The use of various assessment districts ensures that the landscaping and other improvements constructed and maintained with assessment proceeds are located in close proximity to the real property subject to the assessment, and that such improvements provide a direct advantage to the property in the assessment district.

3. Due to their proximity to the assessed parcels, the improvements and maintenance thereof financed with assessment revenues in each assessment district benefits the properties in that assessment district in a manner different in kind from the benefit that other parcels of real property in the City of Folsom derive from such improvements, and the benefits conferred on such property in each assessment district are more extensive and direct than a general increase in property values.
4. The assessments paid in each assessment district are proportional to the special benefit that each parcel within that assessment district receives from such improvements and the maintenance thereof because:
 - a. The specific landscaping and other improvements and maintenance and utility costs thereof in each assessment district and the costs thereof are specified in this Engineer's Report; and
 - b. Such improvement and maintenance costs in each assessment district are allocated among different types of property located within each assessment district, and equally among those properties which have similar characteristics and receive similar special benefits.

There have been a number of clarifications made to the analysis, findings and supporting text in this Report to ensure that this consistency is well communicated.

Plans & Specification

The work and improvements proposed to be undertaken by the City of Folsom Landscaping and Lighting Districts (the "Districts") and the cost thereof paid from the continuation of the annual assessment provide special benefit to parcels within the Districts defined in the Method of Assessment herein. Consistent with the Landscaping and Lighting Act of 1972, (the "Act") the work and improvements (the "Improvements") are generally described as follows:

Within these districts, the existing and proposed improvements are generally described as the installation, maintenance and servicing of turf, ground cover, shrubs and trees, irrigation systems, drainage systems, street lighting, fencing, soundwalls, sidewalks, monuments, statuary, fountains, water quality ponds, park facilities, open space, bike trails, walkways, drainage swales and other ornamental structures and facilities, entry signage, street pavers, art work, and monuments and all necessary appurtenances, and labor, materials, supplies, utilities and equipment, as applicable, for property owned or maintained by the City of Folsom. Any plans and specifications for these improvements have been filed with the City of Folsom and are incorporated herein by reference.

"Maintain" or "maintenance" means the furnishing of services and materials for the ordinary and usual maintenance, operation, and servicing of any improvement, including:

- (a) Repair, removal, or replacement of all or any part of any improvement.*
- (b) Providing for the life, growth, health, and beauty of landscaping, including cultivation, irrigation, trimming, spraying, fertilizing, or treating for disease or injury.*
- (c) The removal of trimmings, rubbish, debris, and other solid waste.*
- (d) The cleaning, sandblasting, and painting of walls and other improvements to remove or cover graffiti.*

"Service" or "servicing" means the furnishing of:

- (a) Electric current or energy, gas, or other illuminating agent for any public lighting facilities or for the lighting or operation of any other improvements.*
- (b) Water for the irrigation of any landscaping, the operation of any fountains, or the maintenance of any other improvements.*

The assessment proceeds from each District will be exclusively used for Improvements within that District plus Incidental expenses. Reference is made to the Estimate of Cost and Budget, Appendix A and to the additional plans and specifications, including specific expenditure and improvement plans by District, which are on file with the City.

Estimate of Cost and Budget

Description of Improvements

Following are descriptions of improvements for the various City of Folsom Landscaping and Lighting Districts.

American River Canyon North

- Maintenance of lawns and trees within landscape medians, corridors, and open spaces.
- Purchase of irrigation water from San Juan Suburban Water District.
- Maintenance of Irrigation system, entry fountain, plantings, sidewalks and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- No planned projects.

Future Improvement Projects:

- Water fall pump, autofill, filters, chlorination systems.
- Waterfall pond liner.

American River Canyon North No. 2

- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- No planned projects.

Future Improvement Projects:

- Paint and replace streetlight poles.

American River Canyon North No. 3

- Purchase of electric power.
- Purchase of irrigation water from San Juan Suburban Water District.
- Maintenance of landscaping, open space, lighting, signs, sidewalk and walls, waterfalls, including turf, ground cover, shrubs and trees, irrigation systems, drainage systems, street lighting, walls, signs.

Planned Improvement Projects for 2022-23:

- Centralized irrigation controller upgrade.
- Signage replacement.
- Mystic Hills replace missing landscape.

Future Improvement Projects:

- Tree and Landscape Improvements (or replacements).
- ARC Drive/Canyon Falls (Cascade Perimeter) landscape remove/replace trees, mow band replacement.
- Main Walking Trail – landscaping, irrigation, stairs and clean up.
- Baldwin Dam path repair.
- Waterfall rock repair.

Blue Ravine Oaks

- Maintenance of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from City of Folsom.
- Maintenance of irrigation system, plantings, sidewalks, fences, walls and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- No planned projects.

Future Improvement Projects:

- Blue ravine wall repair.

Blue Ravine Oaks No. 2

- Maintenance of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from City of Folsom.
- Maintenance of irrigation system, plantings, sidewalks, fences, walls and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Tree removal/replacement.

Future Improvement Projects:

- Riley Street Fence Replacement.
- Tree removal/replacement.
- Signage replacement.

Briggs Ranch

- Maintenance of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from City of Folsom.
- Maintenance of irrigation system, plantings, sidewalks, trails, walls, fences, open space area, signage and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Shrub and tree upgrades (Blue Ravine/E. Natoma).
- Fence/Wall repair/replacement.
- Fence repair/replacement (E. Natoma Partial).
- Pet station repair/replacement.

Future Improvement Projects:

- Bollard repair/replacement.
- Fence repair/replacement (Blue Ravine/E Natoma)
- Fence repair/replacement (E. Natoma Partial).
- Entry sign replacement (brass lettering)
- Irrigation upgrades/replacement (3 controllers).
- Landscape lighting upgrades or replacement.
- Tree and Landscape Improvements (partial collection).

Broadstone

- Maintenance of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from City of Folsom.
- Maintenance of irrigation system, plantings, sidewalks, trails, sound walls, water quality ponds and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Tree and Landscape Improvements (partial fund collection).
- Bollard repair/replacement.
- Light pole/fixture replacement of KW.

Future Improvement Projects:

- Repair irrigation/replace shrubs – Iron Point median.
- Tree and Landscape Improvements (or replacements).
- Shrub replacement throughout (some irrigation repair) 28 acres.
- Landscape light replacement.
- Pet station replacement (7).
- Signage repair/replacement.
- Turf removal/irrigation retrofit.
- Irrigation upgrades and flow (15 controllers).

Broadstone Unit No. 3

- Purchase of electric power.
- Maintenance of street light fixtures.

Planned Improvement Projects for 2022-23:

- No planned projects.

Future Improvement Projects:

- Paint streetlight poles (350 poles).

Broadstone No. 4

- Maintenance of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from City of Folsom.
- Maintenance of irrigation system, plantings, sidewalks, trails, sound walls, water quality ponds and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Repair irrigation, replant shrubs: Rathbone, Knofler, other interior areas
- Landscape light repair/replacement (60 lights).

Future Improvement Projects:

- No planned projects.

Cobble Hills Ridge II/Reflections II

- Maintenance of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from the City of Folsom.

- Maintenance of Irrigation system, plantings, sidewalks, soundwalls, signage, parks, park facilities, open space and streetlights.
- Purchase of Electric Power from SMUD.
- Maintenance of public lighting fixtures.

Planned Improvement Projects for 2022-23:

- Tree and Landscape Improvements (or replacements).
- Mini Park and path to Lembi turf and shrub repair/replacement.

Future Improvement Projects:

- Fence repair/replacement (225 feet)
- Wall repairs and painting.
- Signage improvement/replacement.
- Tree and Landscape Improvements (or replacements).
- Shrub replacement – Sibley/Corner, Glenn/Oxborough.

Cobble Ridge

- Maintenance of shrubs and trees within landscape medians and corridors.
- Purchase of irrigation water from the City of Folsom.
- Maintenance of irrigation system, plantings, open space areas, soundwalls, sidewalks and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Fence repair and painting.
- Tree pruning.

Future Improvement Projects:

- No planned projects.

Folsom Heights

- Maintenance of shrubs and trees within landscape medians and corridors, corridors, bike trails, walkways, and open space areas.
- Purchase of irrigation water from the City of Folsom.
- Maintenance of irrigation system, plantings, fences, walls, sidewalks and streetlights.
- Purchase of Electric Power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Ladder fuel work.

Future Improvement Projects:

- No planned projects.

Folsom Heights No. 2

- Maintenance of lawns and trees within landscape medians and corridors, corridors, bike trails, walkways, and open space areas.
- Purchase of irrigation water from the City of Folsom.
- Maintenance of irrigation system, plantings, fences, walls, sidewalks and streetlights.
- Purchase of Electric Power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Tree care in open space.
- Tree pruning.

Future Improvement Projects (if funded with new District):

- No planned projects.

Hannaford Cross

- Maintenance of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water and electric power for the two mini parks in a 70%/30%: City/District contribution. (based on maintenance assignments)
- Maintenance of Irrigation system, bike trails, walkways, fences, walls, guard shack, drainage swale, plantings, sidewalks and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Lakeside fence repair.
- Repairs at guard shack.
- Tree pruning.

Future Improvement Projects:

- No planned projects.

Lake Natoma Shores

- Maintenance of lawns and trees within landscape medians and corridors.
- Maintenance of Irrigation system, soundwalls, signage, street pavers, plantings, sidewalks and streetlights.
- Purchase of Irrigation water from the City of Folsom.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- LED conversion.
- Tree pruning.

Future Improvement Projects:

- No planned projects.

Los Cerros

- Maintenance of landscape medians and corridors.
- Purchase of irrigation water from the City of Folsom.
- Maintenance of Irrigation system, walls, plantings, sidewalks and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Ladder fuel work.
- Tree replacement.
- Tree pruning
- Paint street light poles.

Future Improvement Projects:

- Install flow package and master valve.
- Upgrade irrigation controllers

Natoma Station

- Maintenance of Irrigation system, walls, signage, art work, open space areas, parks, plantings and streetlights.
- Purchase of irrigation water from the City of Folsom.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Ladder fuel work
- Light pole replacement
- Shrub & tree replacement & concrete work on turnpike
- Iron Point Rd shrub & tree replacement
- Bigfoot mini park tree replacement
- Tree replacement / wall damage on back diamond
- Shrub / tree replacement on Blue Ravine
- Tree pruning.

Future Improvement Projects:

- Tree & Landscape Improvements (or replacements)
- Wetland area improvements
- Wall repair and painting (7800 linear feet)
- Mini park replanting / bark (2 parks @ .5 acre)
- Road paver replacement
- Signage repair / replacement
- Sidewalk repair
- Irrigation upgrades
- Art repair

Natoma Station-Union Square

Note: Union Square which is a Benefit zone of Natoma Station will be providing its own landscaping and lighting maintenance via an existing homeowner's association and servicing for 2022-23.

Natoma Valley

- Installation, maintenance and servicing of turf, ground cover, shrubs and trees, irrigation systems, drainage systems, street lighting, soundwalls, retaining walls, fencing and all necessary appurtenances, and labor, materials, supplies, utilities and equipment

Planned Improvement Projects for 2022-23:

- Interior landscape improvements.
- Tree pruning.

Future Improvement Projects:

- No planned projects.

Prairie Oaks Ranch

- Maintenance and servicing of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from the City of Folsom.
- Maintenance of Irrigation system, walls, signage, fences, open space areas, trellises, and streetlights along Grover Road, Russi Road, Willard Drive, Stewart Street and the interior public roadways within the subdivisions.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- No planned projects.

Future Improvement Projects:

- No planned projects.

Prairie Oaks Ranch No. 2

- Maintenance and servicing of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from the City of Folsom.
- Maintenance of Irrigation system, walls, signage, fences, open space areas, trellises, and streetlights along Grover Road, Russi Road, Willard Drive, Stewart Street and the interior public roadways within the subdivisions.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Tree pruning.
- Tree replacement (replace empty areas).
- LED Landscape Lights.
- Landscape replacement.
- Fence replacement.
- Ladder fuel.
- Repair damaged walls (stucco half walls).

Future Improvement Projects:

- Landscape replacement on Blue Ravine
- Landscape Replacement on Riley
- Landscape Replacement on Prairie City
- Landscape replacement on Iron Point
- Fence replacement

- Post and cable replacement
- Repair damaged wall

Prospect Ridge

- Maintenance of Irrigation system, walls, signage, open space areas, parks, plantings and streetlights.
- Purchase of irrigation water from the City of Folsom.
- Purchase of electric power.
- Maintenance of street lighting fixtures.
- Maintenance of landscape corridors.

Planned Improvement Projects for 2022-23:

- Tree pruning.

Future Improvement Projects:

- Plant replacement.

Sierra Estates

- Maintenance of landscaping, lighting and soundwalls along Rowland Court, Dolan Court and Riley Street including turf, ground cover, shrubs and trees, irrigation systems, drainage systems, street lighting, fencing, soundwalls, monuments, statuary, fountains, and other ornamental structures and facilities, entry monuments and all necessary appurtenances
- Purchase of water from the City of Folsom
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Tree replacement.

Future Improvement Projects:

- No planned projects.

Silverbrook

- Note: Silverbrook will not be levied for fiscal year 2022-23, due to a surplus in revenue.
- Maintenance of lawns and trees within landscape median.
- Purchase of irrigation water from City of Folsom.
- Maintenance of Irrigation system, entry median, plantings, sidewalks and streetlights.

- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Tree replacement.

Future Improvement Projects:

- No planned projects.

Steeplechase

- Maintenance of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from the City of Folsom.
- Maintenance of Irrigation system, plantings, sidewalks and streetlights.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Fence replacement.
- Tree pruning.

Future Improvement Projects:

- No planned projects.

The Residences at American River Canyon

- Maintenance landscaping, lighting and soundwalls along American River Canyon Drive and Oak Avenue including turf, ground cover, shrubs and trees, irrigation systems, drainage systems, street lighting, sound-walls, and all necessary appurtenances.
- Purchase of water from San Juan Water District.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Landscape replacement.

Future Improvement Projects:

- No planned projects.

The Residences at American River Canyon II

- Installation, maintenance and servicing of turf, ground cover, shrubs, and trees, irrigation systems, drainage systems, street lighting, walls, signage and all necessary appurtenances, and labor, materials, supplies, utilities, and equipment

Planned Improvement Projects for 2022-23:

- Landscape replacement.

Future Improvement Projects:

- No planned projects.

Willow Creek Estates East

- Maintenance of lawns and trees within landscape medians and corridors.
- Purchase of irrigation water from the City of Folsom.
- Maintenance of irrigation system, walls, plantings, sidewalks and streetlights, as well as weed abatement.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- No planned projects.

Future Improvement Projects: (if funding available)

- No planned projects.

Willow Creek Estates East No. 2

- Maintenance of irrigation system, walls, plantings, sidewalks and streetlights, as well as weed abatement.
- Purchase of irrigation water from the City of Folsom.
- Purchase of electric power.
- Maintenance of street lighting fixtures.
- Maintenance of lawns and trees within landscape medians and corridors.

Planned Improvement Projects for 2022-23:

- 2 flow packages & Master Valve Install
- Light pole replacement (3)
- Tree pruning
- Oleander replacement on Blue Ravine frontage.

Future Improvement Projects:

- Landscape replacement on Oak Avenue.
- Landscape replacement on Blue Ravine.
- Irrigation controller upgrade (4 controllers).
- Tree and landscape improvement / replacement.

Willow Creek Estates South

- Purchase of irrigation water from the City of Folsom.
- Maintenance of Irrigation system, walls, entry signage, drainage way, parks, sidewalks and streetlights, as well as weed abatement.
- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- Tree pruning.
- New planting around replaced signs.
- Interior sign replacement.

Future Improvement Projects:

- No planned projects.

Willow Springs

- Purchase of electric power.
- Maintenance of street lighting fixtures.

Planned Improvement Projects for 2022-23:

- LED retrofits..

Future Improvement Projects:

- Paint/repair Lamp Posts.

Budgets

Below is a summary of the Budgets for the various districts. Refer to Appendix A - Budgets, for detailed budgets for each district.

District	Improvement Costs	Incidental Costs	Total Improvement Costs
American River Canyon North	\$146,500.00	\$6,309.54	\$152,809.54
American River Canyon North 2	\$56,000.00	\$1,475.40	\$57,475.40
American River Canyon North 3	\$373,600.00	\$542.54	\$374,142.54
Blue Ravine Oaks	\$17,700.00	\$2,343.35	\$20,043.35
Blue Ravine Oaks No. 2	\$48,800.00	\$5,917.35	\$54,717.35
Briggs Ranch	\$84,500.00	\$16,831.41	\$101,331.41
Broadstone	\$192,500.00	\$66,632.70	\$259,132.70
Broadstone Unit No.3	\$24,500.00	\$2,885.00	\$27,385.00
Broadstone No. 4	\$347,500.00	\$15,210.38	\$362,710.38
Cobble Hills II/Reflections II	\$41,350.00	\$12,078.51	\$53,428.51
Cobble Ridge	\$13,900.00	\$2,259.82	\$16,159.82
Folsom Heights	\$42,150.00	\$1,754.72	\$43,904.72
Folsom Heights No. 2	\$63,275.00	\$6,952.00	\$70,227.00
Hannaford Cross	\$20,900.00	\$5,329.77	\$26,229.77
Lake Natoma Shores	\$25,450.00	\$4,619.67	\$30,069.67
Natoma Valley (formerly) Lakeridge	\$46,625.00	\$9,919.61	\$56,544.61
Los Cerros	\$82,400.00	\$6,943.83	\$89,343.83
Natoma Station	\$200,500.00	\$40,371.37	\$240,871.37
Prospect Ridge	\$19,775.00	\$6,208.65	\$25,983.65
Prairie Oaks Ranch	\$80,000.00	\$67,593.99	\$147,593.99
Prairie Oaks Ranch No. 2	\$275,000.00	\$5,950.00	\$280,950.00
The Residences at ARC	\$24,950.00	\$5,312.03	\$30,262.03
The Residences at ARC II	\$24,950.00	\$0.00	\$24,950.00
Sierra Estates	\$13,775.00	\$2,127.75	\$15,902.75
Silverbrook	\$9,513.17	\$2,508.17	\$12,021.33
Steeplechase	\$39,200.00	\$6,493.00	\$45,693.00
Willow Creek East	\$26,000.00	\$3,190.00	\$29,190.00
Willow Creek East Estates No 2	\$125,000.00	\$15,950.00	\$140,950.00
Willow Creek South	\$255,000.00	\$12,517.57	\$267,517.57
Willow Springs	\$14,000.00	\$1,530.03	\$15,530.03
TOTALS	\$2,735,313.17	\$337,758.15	\$3,073,071.32

District	Current Projects	Benefit Units (SFEs)	Rate	Total Assessment
American River Canyon North	\$65,000.00	1,022	\$102.94	\$105,204.68
American River Canyon North 2	\$25,000.00	160	\$77.70	\$12,432.00
American River Canyon North 3	\$250,000.00	1,022	\$269.86 *	\$275,796.92
Blue Ravine Oaks	\$0.00	165	\$218.60	\$36,069.00
Blue Ravine Oaks No. 2	\$20,000.00	165	\$213.26 *	\$35,187.90
Briggs Ranch	\$0.00	660	\$122.28	\$80,706.02
Broadstone	\$0.00	2,369	\$164.99	\$390,859.66
Broadstone Unit No.3	\$10,000.00	812	\$28.07 *	\$22,779.65
Broadstone No. 4	\$90,000.00	2,065	\$39.98 *	\$82,560.53
Cobble Hills II/Reflections II	\$0.00	389	\$113.14	\$44,011.46
Cobble Ridge	\$5,500.00	98	\$139.64 *	\$13,684.72
Folsom Heights	\$28,000.00	308	\$70.88	\$21,831.04
Folsom Heights No. 2	\$37,000.00	299	\$208.38 *	\$62,386.89
Hannaford Cross	\$0.00	103	\$195.78	\$20,165.34
Lake Natoma Shores	\$8,000.00	113	\$183.58	\$20,744.54
Natoma Valley (formerly) Lakeridge	\$0.00	79	\$856.37 *	\$67,653.23
Los Cerros	\$45,000.00	337	\$121.18	\$40,837.66
Natoma Station	\$0.00	1,897	\$91.70	\$173,976.36
Prospect Ridge	\$0.00	27	\$1,173.86	\$31,400.76
Prairie Oaks Ranch	\$0.00	919	\$213.61	\$196,228.55
Prairie Oaks Ranch No. 2	\$0.00	919	\$313.91 *	\$288,367.14
The Residences at ARC	\$4,000.00	17	\$536.67 *	\$9,123.39
The Residences at ARC II	\$4,000.00	10	\$1,169.97 *	\$11,699.70
Sierra Estates	\$7,500.00	25	\$363.68 *	\$9,092.00
Silverbrook	\$0.00	114	\$0.00	\$0.00
Steeplechase	\$13,000.00	154	\$157.68	\$24,282.72
Willow Creek East	\$0.00	747	\$80.40	\$60,058.80
Willow Creek East Estates No 2	\$50,000.00	741	\$103.51 *	\$76,748.01
Willow Creek South	\$120,000.00	1462	\$109.88	\$160,642.36
Willow Springs	\$0.00	517	\$28.14	\$14,548.38
TOTALS	\$782,000.00			\$2,389,079.41

* Subject to CPI increase

Method of Apportionment

This section of the Engineer's Report includes an explanation of the benefits to be derived from the installation, maintenance and servicing of the Improvements throughout the Districts, and the methodology used to apportion the total assessment to properties within the City of Folsom Landscaping and Lighting Districts.

The City of Folsom Landscaping and Lighting Districts consist of all Assessor Parcels within the boundaries of each District defined as defined by Assessment Diagram included within this report and the Assessor Parcel Numbers listed within the included Levy roll. The parcels include all privately or publicly owned parcels within said boundaries. The method used for apportioning the assessment is based upon the proportional special benefits to be derived by the properties in the City of Folsom Landscaping and Lighting Districts over and above general benefits conferred on real property or to the public at large. The apportionment of special benefit is a two-step process: the first step is to identify the types of special benefit arising from the improvements, and the second step is to allocate the assessments to property based on the estimated relative special benefit for each type of property.

Discussion of Benefit

In summary, the assessments can only be levied based on the special benefit to property. This benefit is received by property over and above any general benefits. With reference to the requirements for assessments, Section 22573 of the Landscaping and Lighting Act of 1972 states:

"The net amount to be assessed upon lands within an assessment district may be apportioned by any formula or method which fairly distributes the net amount among all assessable lots or parcels in proportion to the estimated benefits to be received by each such lot or parcel from the improvements."

Proposition 218, as codified in Article XIID of the California Constitution, has confirmed that assessments must be based on the special benefit to property and the assessment must not exceed the reasonable cost of the proportional benefit upon the assessed parcel:

"No assessment shall be imposed on any parcel which exceeds the reasonable cost of the proportional special benefit conferred on that parcel."

The following benefit categories summarize the types of special benefit to residential, commercial, industrial and other lots and parcels resulting from the Improvements to be provided with the assessment proceeds. These categories of special benefit are supported by various California legislation and supporting studies which describe the types of special benefit received by property from Improvements such as those proposed by the City of Folsom Landscaping and Lighting Districts. These types of special benefit are summarized as follows:

- Proximity to improved landscaped areas within each District.
- Access to improved landscaped areas within each District.
- Improved Views within each District.
- Extension of a property's outdoor areas and green spaces for properties within close proximity to the Improvements.
- Creation of individual lots for residential and commercial use that, in absence of the District and the services provided by the District, would not have been created.

In this regard, the recent the SVTA v. SCCOSA decision provides enhanced clarity to the definitions of special benefits to properties in three distinct areas:

- Proximity
- Expanded or improved access
- Views

The SVTA v. SCCOSA decision also clarifies that a special benefit is a service or improvement that provides a direct advantage to a parcel, and that indirect or derivative advantages resulting from the overall public benefits from a service or improvement are general benefits. The SVTA v. SCCOSA decision also provides specific guidance that park improvements are a direct advantage and special benefit to property that is proximate to a park improved by an assessment:

The characterization of a benefit may depend on whether the parcel receives a direct advantage from the improvement (e.g. proximity to a park) or receives an indirect, derivative advantage resulting from the overall public benefits of the improvement (e.g. general enhancement of the district's property values).

Proximity, improved access and views, in addition to the other special benefits listed above further strengthen the basis of these assessments.

Benefit Factors

The special benefits from the Improvements are further detailed below:

Proximity to improved landscaped areas within the District

Only the specific properties within close proximity to the Improvements are included in each District. Therefore, property in the Districts enjoys unique and valuable proximity and access to the Improvements that the public at large and property outside the Districts do not share.

In absence of the assessments, the Improvements would not be provided and the landscaping areas in the Districts would be degraded due to insufficient funding for maintenance, upkeep and repair. Therefore, the assessments provide Improvements that are over and above what otherwise would be provided. Improvements that are over and above what otherwise would be provided do not by themselves translate into special benefits but when combined with the unique proximity and access enjoyed by parcels in the Districts, they provide a direct advantage and special benefit to property in the Districts.

Access to improved landscaped areas within the District

Since the parcels in each District are the only parcels that enjoy close access to the Improvements, they directly benefit from the unique close access to improved landscaping areas that are provided by the Assessments. This is a direct advantage and special benefit to property in that District.

Improved views within the District

The District, by maintaining these landscaped areas, provides improved views to properties in each District. The properties in a District enjoy close and unique proximity, access and views of the Improvements; therefore, the improved and protected views provided by the Assessments are another direct and tangible advantage that is uniquely conferred upon property in a District.

Extension of a property's outdoor areas and green spaces for properties within close proximity to the Improvements

In large part because it is generally cost prohibitive to provide large open land areas in development projects, the residential, commercial and other benefiting properties in each District do not have large outdoor areas and green spaces. The landscaped areas within each District provide additional outdoor areas that serve as an effective extension of the land area for properties that are in close proximity to the Improvements. The Improvements, therefore, provide an important, valuable and desirable extension of usable land area, which confers a direct advantage and special benefit to properties in close proximity to the Improvements.

Creation of individual lots for residential and commercial use that, in absence of the assessments, would not have been created

Typically, the original owner/developer of the property within the Districts can petition the City to establish the assessment districts. As parcels were sold, new owners were informed of the assessments through the title reports, and in some cases, through Department of Real Estate "White Paper" reports that the parcels were subject to assessment. Purchase of property was also an "agreement" to pay the assessment. In absence of the assessments, the lots within the Districts would probably not have been subdivided and created. These lots, and the improvements they support, are a special benefit to the property owners.

General versus Special Benefit

The assessments from the City of Folsom Landscaping and Lighting Districts are used to fund improvements and increased levels of maintenance to the grounds adjoining the properties in the Districts. In absence of those Districts, such Improvements would not be provided and the properties would not have been subdivided and improved to the same extent. The Districts were specifically proposed for formation to provide additional and improved improvements, and services in the Districts. In absence of the assessments, these public resources could not be created and revenues would not be available for their continued maintenance and improvement. Therefore, the assessments solely provide special benefit to property in the Districts over and above the general benefits conferred by the general facilities of the City.

Although these Improvements may be available to the general public at large because the Districts are accessible by members of the public, the Improvements within each District were specifically designed, located and created to provide additional and improved public resources for property inside the Districts, and not the public at large. Other properties that are either outside the Districts or within the Districts and not assessed, do not enjoy the unique proximity, access, views and other special benefit factors described previously. These Improvements are of special benefit to properties located within the Districts because they provide a direct advantage to properties in the Districts that would not be provided in absence of the assessments.

Although the analysis used to support these assessments concludes that the benefits are solely special, as described above, consideration is made for the suggestion that a portion of the benefits are general. General benefits cannot be funded by these assessments - the funding must come from other sources.

The maintenance and servicing of these improvements is also partially funded, directly and indirectly from other sources including City of Folsom, the County of Sacramento and the State of California. This funding comes in the form of grants, development fees, special programs, and general funds, as well as direct maintenance and servicing of facilities (e.g. curbs, gutters, streets, drainage systems, and other infrastructure maintenance items such as pond clean outs and street sweeping, etc.) This funding from other sources more than compensates for general benefits, if any, received by the properties within the districts.

In the 2009 Dahms case, the court upheld an assessment that was 100% special benefit on the rationale that the services funded by the assessments were directly provided within the assessment district over and above the services already provided by the City within the boundaries of the assessment district. It is also important to note that certain services funded by the assessments in Pomona are similar to the services funded by the Assessments described in this Engineer’s Report and the Court found these services to be 100% special benefit. Similar to the assessments in Pomona, the Assessments described in this Engineer’s Report fund improvements and services directly provided within the Assessment District to benefit properties within the assessment district and not to the public at large, and these properties enjoy close proximity and access to the Improvements. Therefore, Dahms establishes a basis for minimal or zero general benefits from the Assessments.

Step 1: Calculation of the General Benefit

The general benefits from this assessment may be quantified as illustrated in the following table.

Benefit Factor	Relative Weight	General Benefit Contribution	Relative General Benefit
Creation of parcels	90	0%	0
Improved views	5	10%	0.5
Improved nighttime visibility and safety from streetlights	5	20%	1
	100		1.5
Total Calculated General Benefit =			1.5%

As a result, the City of Folsom will contribute at least 1.5% of the total budget from sources other than the assessment. The contribution offsets any general benefits from the Assessment Services.

Step 2: Calculation of Current General Benefit Contribution from City

The general benefit contribution is satisfied from the sum of the following components:

The City of Folsom owns, maintains, rehabilitates and replaces curb and gutter along the border of the Assessment Districts improvements. This curb and gutter serves to support, contain, retain, manage irrigation flow and growth, and provide a boundary for the improvements. The contribution from the City of Folsom toward general benefit from the maintenance, rehabilitation and replacement of the curb gutter is conservatively estimated to be 1%.

The City of Folsom owns and maintains storm drainage systems along the border of the Assessment Districts improvements. This system serves to prevent flooding and associated damage to the improvements, and manage urban runoff including local pollutants loading from the improvements. The contribution from the City of Folsom towards general benefit from the maintenance, and operation of the local storm drainage systems are conservatively estimated to be 1%.

The City of Folsom owns and maintains local public streets along the border of the Assessment District improvements. These public streets proved access to the improvements for its enjoyment as well as efficient maintenance. The contribution from the City of Folsom towards general benefit from the maintenance of local public streets is conservatively estimated to contribute 1%.

The Improvements were constructed by the original owner/developer(s) as a condition of development. The value of the construction of the improvements can be quantified and monetized as an annuity. Since this construction was performed and paid by non-assessment funds, this "annuity" can be used to offset general benefit costs, and is conservatively estimated to contribute 25%.

Therefore, the total General Benefit that is conservatively quantified at 1.5% is more than offset by the total non-assessment contribution towards general benefit of 28%.

Method of Assessment

The second step in apportioning assessments is to determine the relative special benefit for each property. This process involves determining the relative benefit received by each property in relation to a single family home, or, in other words, on the basis of Single Family Equivalents (SFE). This SFE methodology is commonly used to distribute assessments in proportion to estimated special benefit and is generally recognized as providing the basis for a fair and appropriate distribution of assessments. For the purposes of this Engineer's Report, all properties are designated a SFE value, which is each property's relative benefit in relation to a single family home on one parcel. In this case, the "benchmark" property is the single family detached dwelling which is one Single Family Equivalent or one SFE.

Assessment Apportionment

The improved properties within the Districts consist of primarily of single family, multi-family, commercial and non-assessed parcels, with the vast majority being single family. Since all single family residential parcels in the Districts are deemed to have good proximity to the improvements, such single family properties receive similar benefit from the proposed improvements and are assigned 1.0 SFE units. The benefits for other types of properties are further defined as follows.

General Case

Many of the City of Folsom Districts contain only single family residences and non-assessed properties such as parks and green spaces. These districts are:

Blue Ravine Oaks	165 residential lots
Blue Ravine Oaks No. 2	165 residential lots
Cobble Ridge	98 residential lots

Cobble Hills Ridge II/Reflections II	389 residential lots
Hannaford Cross	103 residential lots
Lake Natoma Shores	113 residential lots
Los Cerros	337 residential lots
Natoma Station – (Union Square Annexation)	116 residential lots
Natoma Valley	72 residential lots
Sierra Estates	25 residential lots
Silverbrook	122 residential lots
Steeplechase	154 residential lots
The Residences at American River Canyon	17 residential lots
The Residences at ARC II Annexation	10 residential lots
Willow Creek East	747 residential lots
Willow Springs	<u>517</u> residential lots
Total	3,150

These Districts are assessed per Assessment Table 1, next page.

Assessment Table 1

Description	SFEs
Single Family Parcel	1.00
Non Assessed (e.g. open space, park land etc.)	0.00

Note: In 2006-07, a general case SFE rate was established for condominiums in districts in which the original Engineer’s Report did not anticipate condominium development. This rate is 0.67 SFEs.

American River Canyon North

There are 410.124 acres in American River Canyon North. There are 1006 residential lots and each one is assigned 1 benefit unit (SFE.) The 2.00 acres of currently undeveloped property is assigned 2.63 SFEs per acre from a rate determined at the time of formation of this district:

American River Canyon North properties are assessed per Assessment Table 2, below, as per the original formation documents:

Assessment Table 2

Description	SFEs
Single Family Parcel	1.00
Undeveloped Property, per acre	2.63
Non Assessed (e.g. open space, park land etc.)	0.00

American River Canyon North No. 2

There are 130.805 acres in American River Canyon North No. 2. American River Canyon No. 2 lies completely within American River Canyon North. There are 161 residential lots and each one is assigned 1 benefit unit (SFE).

American River Canyon North No. 2 properties are assessed per Assessment Table 3, below, as per the original formation documents:

Assessment Table 3

Description	SFEs
Single Family Parcel	1.0000
Undeveloped Residential Property, per lot	0.3273
Non Assessed (e.g. open space, park land etc.)	0.0000

American River Canyon North No 3

There are 410.124 acres in American River Canyon North. There are 1006 residential lots.

Each assessable parcel in the Assessment District receives a special and direct benefit from the improvements in the Assessment District. Since the Assessment District is comprised of residential single family improved properties and all properties have good proximity to the improvements, all assessable parcels within the Assessment District are estimated to benefit equally from the improvements associated with the Assessment District, and the costs associated with the improvements are apportioned equally to all parcels on the basis of current or proposed dwelling units. Each parcel is assigned SFE units relative to the number of current or proposed dwelling units on the parcel.

The procedure used to arrive at each parcel's annual levy amount is:

$$\text{Balance to Levy} / \text{Total SFE Benefit Units in District} = \text{Assessment Amount Per Benefit Unit}$$

There are three Zones of Benefit. In Zone A each parcel is assigned 1 benefit unit (SFE), in Zone B, each parcel is assigned 0.83 benefit unit (SFE) and Zone C each parcel is assigned 0.50 benefit unit (SFE.) Properties in Zone B and Zone C receive lower benefit units because they currently pay for common open space areas within their zone. In 2007, when the American River Canyon North District No. 3 was formed, an analysis of the associated landscaping improvements was performed to determine the relative benefit to each zone from this new assessment. It was estimated that Zone B receives 17% of the special benefit, and Zone C receives 50% of the special benefit. Therefore, the SFE units for Zone B and Zone C have been adjusted accordingly.

American River Canyon North properties are assessed per Assessment Table 4, below:

Assessment Table 4

Description	SFEs
Zone A – Original ARCN Area	1.0000
Zone B – Canyon Falls Village Area	0.8300
Zone C – ARCN No. 2 Area	0.5000

Broadstone

According to the Broadstone Landscaping and Lighting “Method of Spread,” there are 895.301 assessable acres in Broadstone. Of these, 416.1455 acres are divided into 1,682 single family residential lots (4.2 lots per acre average) and 479.156 acres are divided into multi-family and commercial lots. The multi-family parcels are APN 0721070002 through APN 0721070100 are known as Bentley Square West (99 units); and APN 0721610001 through APN 072161053 are known as Bentley Square East (53 units). [In addition to these properties listed in the “Method of Spread,” other multi-family complexes are also assessed, including Vessona, Sherwood, Haldon.] Although these projects were designed as single family small lot divisions, the density is consistent with the multi-family land use designation. These projects are consistent with both the Multi-Family Low Density General Plan Land Use Designation (MLD) and the Multi-Family zoning (R-M_PD) of the project site. There are 1530 single family residential lots and each one is assigned 1 SFEs. There are 312.555 developed, non-single family acres and each is assigned 2.1 SFEs per acre [This is the rate applied to commercial properties, as implicitly indicated in the Method of Spread]. (4.2 units * 0.5). Unrecorded single family residential lots are assigned .65 SFEs.

There are 134.387 undeveloped, non-single family residential acres and each one will be assigned 0.704 SFEs per acre. (4.2 units *.0.5 * 0.335). There are 152 lots with Bentley Square East and West and each is assigned .0962 SFEs per lot.

Broadstone properties are assessed per Assessment Table 4, below, as per the original formation documents:

Assessment Table 5

Description	SFEs
Single Family Parcel	1.0000
Multi-Family Parcels, per unit	0.0962
Developed Non-Single Family, per acre	2.1000
Undeveloped Non-Single Family, per acre	0.7040
Non Assessed (e.g. open space, park land etc.)	0.0000

Broadstone No. 3

There are 559.36 acres in Broadstone No. 3. Of these, 325 acres are single family residential lots (2.034 lots per acre average) and 11.48 acres are divided into multi-family residential and 26.93 acres are non-assessed for use as parks, open space, etc. There are 382 single family residential lots and each one is assigned 1 SFE. There are 28.09 developed, non-single family residential acres and each one is assigned 2.034 SFEs per acre. There are 283 undeveloped, single family lots and each one is assigned 0.326 SFEs. There are 171.71 undeveloped, non-single family residential acres and each one is assigned 0.326 x 2.034 SFEs.

Broadstone No.3 properties are assessed per Assessment Table 5, below, as per the original formation documents:

Assessment Table 6

Description	SFEs
Single Family Parcel	1.0000
Undeveloped Single Family Parcel	0.3260
Developed Non-Single Family, per acre	2.0340
Undeveloped Non-Single Family, per acre	0.6630
Non Assessed (e.g. open space, park land etc.)	0.0000

Broadstone No. 4**Residential**

Certain residential properties in the Assessment District that contain a single residential dwelling unit are assigned one Single Family Equivalent or 1.0 SFE. Detached or attached houses, zero-lot line houses and town homes are included in this category of single family residential property. If there is more than one single family detached dwelling on a parcel, it will be charged one SFE per single family detached dwelling.

Properties with more than one residential unit (other than parcels with more than one detached single family dwelling as described above) are designated as multi-family residential properties. These properties benefit from the Improvements in proportion to the number of dwelling units that occupy each property, the average number of people who reside in multi-family residential units versus the average number of people who reside in a single family home and the relative size of each type of residential dwelling unit. The population density factors for the area in Sacramento County encompassing the Assessment District, as depicted in the following table, provide the basis for determining the SFE factors for residential properties. Using the total population in a certain property type in the area from the 2010 Census and dividing it by the total number of such households, finds that approximately 2.91 persons occupy each single family residence, whereas an average of 2.12 persons occupy each condominium. The ratio of 2.91 people on average for a single family residence and 2.12 people per dwelling unit in a condominium unit results in a population density equivalent of 0.73 for condominiums. Next, the relative building areas are factored into the analysis because special benefits are related to the average size of a property, in addition to average population densities. For a condominium, this calculation results in an SFE factor of 0.40 per dwelling unit. A similar calculation is used for the SFE Rates for other residential property types.

Commercial

SFE values for commercial and industrial land uses are based on the equivalence of special benefit on a land area basis between single family residential property and the average commercial/industrial property. The SFE values for various commercial and industrial land uses are further defined by using average employee densities because the special benefit factors described previously can be measured by the average number of people who work at commercial/industrial properties.

In order to determine employee density factors, the findings from the San Diego Association of Governments Traffic Generators Study (the "SANDAG Study") are used because these findings were approved by the State Legislature as being a good representation of the average number of employees per acre of land area for commercial and industrial properties. As determined by the SANDAG Study, the average number of employees per acre for commercial and industrial property is 24.

In comparison, the average number of people residing in a single family home in the area is 2.91. Since the average lot size for a single family home in the Assessment District is approximately 0.20 acres, the average number of residents per acre of residential property is 14.55.

The employee density per acre is generally 1.65 times the population density of single family residential property per acre (24 employees per acre / 14.55 residents per acre). Therefore, the average employee density can be used as the basis for allocating benefit to commercial or industrial property since a commercial/industrial property with 4.8 employees receives generally similar special benefit to a residential property with 1 resident. This factor of equivalence of benefit between 1 resident to 4.8 employees is the basis for allocating commercial/industrial benefit. Table 2 below shows the average employees per acre of land area or portion thereof for commercial and industrial properties and lists the relative SFE factors per quarter acre for properties in each land use category.

Commercial and industrial properties in excess of 5 acres generally involve uses that are more land intensive relative to building areas and number of employees (lower coverage ratios). As a result, the benefit factors for commercial and industrial property land area in excess of 5 acres is determined to be the SFE rate per quarter acre for the first 5 acres and the relevant SFE rate per each additional acre over 5 acres.

Institutional properties that are used for residential, commercial or industrial purposes are also assessed at the appropriate residential, commercial or industrial rate.

Vacant

The benefit to vacant properties is determined to be proportional to the corresponding benefits for similar type developed properties; however, at a lower rate due to the lack of improvements on the property. A measure of the benefits accruing to the underlying land is the average value of land in relation to Improvements for developed property. The SFE factor for vacant/undeveloped parcels is 0.25 per parcel.

The benefit to undeveloped properties is determined to be proportional to the corresponding benefits for similar type developed properties, but at a lower rate due to the lack of improvements on the property. A measure of the benefits accruing to the underlying land is the average value of land in relation to Improvements for developed property. An analysis of the assessed valuation data from the County of Sacramento found that approximately 25% of the assessed value of improved properties is classified as the land value. It is reasonable to assume, therefore, that approximately 25% of the benefits are related to the underlying land and 75% are related to the improvements and the day-to-day use of the property. Using this ratio, the SFE factor for vacant/undeveloped parcels is 0.25 per parcel.

Other Properties

Article XIID stipulates that publicly owned properties must be assessed unless there is clear and convincing evidence that those properties receive no special benefit from the assessment.

All properties that are specially benefited are assessed. Other publicly owned property that is used for purposes similar to private residential, commercial, industrial or institutional uses is benefited and assessed at the same rate as such privately owned property.

Miscellaneous, public right-of-way parcels, well, reservoir or other water rights parcels, limited access open space parcels, watershed parcels and common area parcels typically do not generate employees, residents, customers or guests. Moreover, many of these parcels have limited economic value and, therefore, do not benefit from specific enhancement of property value. Such parcels are, therefore, not specially benefited and are not assessed.

In 2015, when the Broadstone No. 4 was formed, an analysis was performed of the associated lighting and landscaping improvements to determine the relative benefit to each zone from this new assessment. As a result, four Zones of Benefit were created within Broadstone No. 4. Parcels in Zone B are determined to receive 95.25% of the level of special benefit of those within Zone A, parcels in Zone C are determined to receive 93.87% of the level of special benefit of those within Zone A, and parcels in Zone D are determined to receive 92.23% of the level of special benefit of those within Zone A.

Broadstone No. 4 properties are assessed per Assessment Table 4, below:

Assessment Table 7

Description	SFEs
Single Family Parcel	1.00
Multi-Family Parcels, per unit (2 to 4 units)	0.27
Multi-Family Parcels, per unit (5+ units)	0.22
Condo	0.40
Mobile Home (separate lot)	0.20
Commercial, shopping center	0.50
Office	1.42
Vacant	0.25

Briggs Ranch

There are 642 residential lots and each one is assigned 1 benefit unit (SFE). Undeveloped residential parcels APN: 071-1190-007, 008, 010, 011 and 012 are assessed based on 2.2 SFEs per acre.

Briggs Ranch properties are assessed per Assessment Table 6, below, as per the original formation documents:

Assessment Table 8

Description	SFEs
Single Family Parcel	1.0000
Undeveloped Single Family, per acre	2.2000
Non Assessed (e.g. open space, park land etc.)	0.0000

Folsom Heights

There are 288 residential lots and each one is assigned 1 benefit unit (SFE.) APN 071-1050-050 is assessed 4.1 SFEs per acre.

Folsom Heights properties are assessed per Assessment Table 7, below, as per the original formation documents:

Assessment Table 9

Description	SFEs
Single Family Parcel	1.0000
Undeveloped Single Family, per acre	4.1000
Multi Family, per unit	0.5000
Non Assessed (e.g. open space, park land etc.)	0.0000

Natoma Station

There are 1272 single family residential lots and each one is assigned 1 SFEs. There are 94.99 acres of Commercial and each one is assigned .6299 SFEs per acre. There are 21.03 acres of Multi Family and each one is assigned 3.2337 SFEs per acre.

Natoma Station properties are assessed per Assessment Table 8, below, as per the original formation documents:

Assessment Table 10

Description	SFEs
Single Family Parcel	1.0000
Commercial outside of Lot X, per acre	0.6299
Commercial inside of Lot X, per acre	4.2487
Multi Family, per acre	3.2337
Non Assessed (e.g. open space, park land etc.)	0.0000

Prospect Ridge

Residential

Certain residential properties in the Assessment District that contain a single residential dwelling unit are assigned one Single Family Equivalent or 1.0 SFE. Detached or attached houses, zero-lot line houses and town homes are included in this category of single family residential property. If there is more than one single family detached dwelling on a parcel, it will be charged one SFE per single family detached dwelling.

Properties with more than one residential unit (other than parcels with more than one detached single family dwelling as described above) are designated as multi-family residential properties. These properties benefit from the Improvements in proportion to the number of dwelling units that occupy each property, the average number of people who reside in multi-family residential units versus the average number of people who reside in a single family home and the relative size of each type of residential dwelling unit. The population density factors for the area in Sacramento County encompassing the Assessment District, as depicted in the following table, provide the basis for determining the SFE factors for residential properties. Using the total population in a certain property type in the area from the 2010 Census and dividing it by the total number of such households, finds that approximately 2.91 persons occupy each single family residence, whereas an average of 2.12 persons occupy each condominium. The ratio of 2.91 people on average for a single family residence and 2.12 people per dwelling unit in a condominium unit results in a population density equivalent of 0.73 for condominiums. Next, the relative building areas are factored into the analysis because special benefits are related to the average size of a property, in addition to average population densities. For a condominium, this calculation results in an SFE factor of 0.40 per dwelling unit. A similar calculation is used for the SFE Rates for other residential property types.

The single family equivalency factor of 0.22 per dwelling unit for multifamily residential properties of 5 or more units applies to such properties with 20 or fewer units. Properties in excess of 20 units typically offer on-site recreational amenities and other facilities that tend to offset some of the benefits provided by the improvements. Therefore the benefit for properties in excess of 20 units is determined to be 0.22 SFE per unit for the first 20 units and 0.10 SFE per each additional unit in excess of 20 dwelling units.

Commercial/Industrial Properties

SFE values for commercial and industrial land uses are based on the equivalence of special benefit on a land area basis between single family residential property and the average commercial/industrial property. The SFE values for various commercial and industrial land uses are further defined by using average employee densities because the special benefit factors described previously can be measured by the average number of people who work at commercial/industrial properties.

In order to determine employee density factors, the findings from the San Diego Association of Governments Traffic Generators Study (the "SANDAG Study") are used because these findings were approved by the State Legislature as being a good representation of the average number of employees per acre of land area for commercial and industrial properties. As determined by the SANDAG Study, the average number of employees per acre for commercial and industrial property is 24.

In comparison, the average number of people residing in a single family home in the area is 2.91. Since the average lot size for a single family home in the Assessment District is approximately 0.20 acres, the average number of residents per acre of residential property is 14.55.

The employee density per acre is generally 1.65 times the population density of single family residential property per acre (24 employees per acre / 14.55 residents per acre). Therefore, the average employee density can be used as the basis for allocating benefit to commercial or industrial property since a commercial/industrial property with 4.8 employees receives generally similar special benefit to a residential property with 1 resident. This factor of equivalence of benefit between 1 resident to 4.8 employees is the basis for allocating commercial/industrial benefit. Table 2 below shows the average employees per acre of land area or portion thereof for commercial and industrial properties and lists the relative SFE factors per quarter acre for properties in each land use category.

Commercial and industrial properties in excess of 5 acres generally involve uses that are more land intensive relative to building areas and number of employees (lower coverage ratios). As a result, the benefit factors for commercial and industrial property land area in excess of 5 acres is determined to be the SFE rate per quarter acre for the first 5 acres and the relevant SFE rate per each additional acre over 5 acres.

Institutional properties that are used for residential, commercial or industrial purposes are also assessed at the appropriate residential, commercial or industrial rate.

Vacant Properties

The benefit to vacant properties is determined to be proportional to the corresponding benefits for similar type developed properties; however, at a lower rate due to the lack of improvements on the property. A measure of the benefits accruing to the underlying land is the average value of land in relation to Improvements for developed property. The SFE factor for vacant/undeveloped parcels is 0.25 per parcel.

The benefit to undeveloped properties is determined to be proportional to the corresponding benefits for similar type developed properties, but at a lower rate due to the lack of improvements on the property. A measure of the benefits accruing to the underlying land is the average value of land in relation to Improvements for developed property. An analysis of the assessed valuation data from the County of Sacramento found that approximately 25% of the assessed value of improved properties is classified as the land value. It is reasonable to assume, therefore, that approximately 25% of the benefits are related to the underlying land and 75% are related to the improvements and the day-to-day use of the property. Using this ratio, the SFE factor for vacant/undeveloped parcels is 0.25 per parcel.

Other Properties

Article XIID stipulates that publicly owned properties must be assessed unless there is clear and convincing evidence that those properties receive no special benefit from the assessment.

All properties that are specially benefited are assessed. Other publicly owned property that is used for purposes similar to private residential, commercial, industrial or institutional uses is benefited and assessed at the same rate as such privately owned property.

Miscellaneous, public right-of-way parcels, well, reservoir or other water rights parcels, limited access open space parcels, watershed parcels and common area parcels typically do not generate employees, residents, customers or guests. Moreover, many of these parcels have limited economic value and, therefore, do not benefit from specific enhancement of property value. Such parcels are, therefore, not specially benefited and are not assessed.

Prairie Oaks Ranch

There are 856 residential lots and each one is assigned 1 benefit unit (SFE). There is one multi-family parcel and it is being assessed 57 SFEs. There is one proposed school site and it is being assessed 5.62 SFEs, or the cost of maintaining its' frontage.

Prairie Oaks Ranch properties are assessed per Assessment Table 9, below, as per the original formation documents:

Assessment Table 11

Description	SFEs
Single Family Parcel	1.0000
Proposed School Site, per parcel	5.6300
Multi Family, per unit	1.0000
Non Assessed (e.g. open space, park land etc.)	0.0000

Prairie Oaks Ranch No. 2

The proposed assessments will provide additional maintenance and servicing of existing improvements that will clearly confer special benefits to properties in the Assessment District. The allocation of special benefits to property is partially based on the type of property and the size of property. These benefits can also partially be measured by the occupants on property in the Assessment District because such parcel population density is a measure of the relative benefit a parcel receives from the improvements. It should be noted that many other types of "traditional" assessments also use parcel population densities to apportion the assessments. For example, the assessments for sewer systems, roads and water systems are typically allocated based on the population density of the parcels assessed. Therefore, the apportionment of benefit is reasonably based the type of parcel, the size of parcels and the population density of parcels.

The primary step in apportioning assessments is to determine the relative special benefit for each property. This process involves determining the relative benefit received by each property in relation to a single-family home, or, in other words, on the basis of Single Family Equivalents (SFE). This SFE methodology is commonly used to distribute assessments in proportion to estimated special benefit and is generally recognized as providing the basis for a fair and appropriate distribution of assessments. For the purposes of this Engineer's Report, all properties are designated an SFE value, which is each property's relative benefit in relation to a single-family home on one parcel. In this case, the "benchmark" property is the single-family detached dwelling which is one Single Family Equivalent or one SFE that currently total to 899.4.

Moreover, a fixed or flat assessment for all commercial properties of similar type was deemed to be inappropriate because larger commercial properties receive a higher degree of benefit than other similarly used properties that are significantly smaller. (For two properties used for commercial purposes, there is clearly a higher benefit provided to the larger property in comparison to a smaller commercial property because the larger property generally supports a larger building and has higher numbers of employees, customers and guests that would benefit from proximity and improved access to well maintained and improved landscaped areas. So the potential population of employees or residents is a measure of the special benefits received by the property.) Larger parcels, therefore, receive an increased benefit from the assessments.

Finally, the special benefits to be derived from the proposed assessments will be conferred on property and are not based on a specific property owner's use of the improvements, or a specific property owner's occupancy of property or the property owner's demographic status such as age or number of dependents. However, it is ultimately people who value the special benefits described above and use and enjoy the Assessment District's landscaped areas. In other words, the benefits derived to property are related to the average number of people who could potentially live on, work at, or otherwise could use a property, not how the property is currently used by the present owner. Therefore, the number of people who could or potentially live on, work at or otherwise use a property is one indicator of the relative level of benefit received by a property.

In conclusion, the Assessment Engineer determined that the appropriate method of assessment apportionment should be based on the type and use of property, the relative size of the property, its relative population and usage potential and its proximity to landscaped areas. This method is further described below.

Residential Properties

Assessment Table 12

Type of Residential Property	Pop. Density Equivalent	SqR Factor	SFE Factor
Single Family Residential	1.00	1.00	1.00
Condominium	0.73	0.58	0.42
Duplex, Triplex, Fourplex	0.77	0.42	0.32
Multi-Family Residential (5+ Units)	0.72	0.30	0.22
Mobile Home on Separate Lot	0.58	0.43	0.25

Commercial/Industrial Properties

Assessment Table 13

Type of Commercial/Industrial Land Use	Average Employees Per Acre ¹	SFE Units per Quarter Acre ²	SFE Units per Acre After 5
Commercial	24	0.500	0.500
Office	68	1.420	1.420
Shopping Center	24	0.500	0.500
Office	24	0.500	0.500
Self Storage or Parking Lot	1	0.021	
Golf Course	0.80	0.033	
Cemeteries	0.10	0.004	
Agriculture	0.05	0.002	

Vacant Properties

The benefit to undeveloped properties is determined to be proportional to the corresponding benefits for similar type developed properties, but at a lower rate due to the lack of improvements on the property. A measure of the benefits accruing to the underlying land is the average value of land in relation to improvements for developed property. An analysis of the assessed valuation data from the County of Sacramento found that approximately 25% of the assessed value of improved properties is classified as the land value. It is reasonable to assume, therefore, that approximately 25% of the benefits are related to the underlying land and 75% are related to the improvements and the day-to-day use of the property. Using this ratio, the SFE factor for vacant/undeveloped parcels is 0.25 per parcel.

Willow Creek Estates East No. 2

Zones of Benefit

As part of the engineering work for this assessment, an analysis was conducted on the relationship (including proximity, level of service, etc.), between properties and the primary improvements located throughout the Assessment District. Parcels in Zone A (on Garrett Drive, Ferrera Drive and Whitmer Drive) receive direct special benefit from the proximate landscaping and trees adjacent to the properties as well as less proximate streetlighting. Parcels in Zone B receive direct special benefit from the proximate streetlighting as well as landscaping particularly along the street entrances into the neighborhood. Zone C receive direct special benefit from the proximate streetlighting but less benefit from the landscaping because they are less proximate to the landscaped areas.

Thus, three zones (A, B, and C) were created as shown on the assessment diagram. Parcels in Zone A are determined to receive same level of the level of special benefit of those within Zone B and parcels in Zone C are determined to receive 92.08% of the level of special benefit of those within Zone A and Zone B.

The SVTA decision indicates:

In a well-drawn district — limited to only parcels receiving special benefits from the improvement — every parcel within that district receives a shared special benefit. Under section 2, subdivision (i), these benefits can be construed as being general benefits since they are not “particular and distinct” and are not “over and above” the benefits received by other properties “located in the district.”

We do not believe that the voters intended to invalidate an assessment district that is narrowly drawn to include only properties directly benefiting from an improvement. Indeed, the ballot materials reflect otherwise. Thus, if an assessment district is narrowly drawn, the fact that a benefit is conferred throughout the district does not make it general rather than special. In that circumstance, the characterization of a benefit may depend on whether the parcel receives a direct advantage from the improvement (e.g., proximity to

park) or receives an indirect, derivative advantage resulting from the overall public benefits of the improvement (e.g., general enhancement of the district's property values).

In the Assessment District, the advantage that each parcel receives from the Improvements is direct, and the boundaries are narrowly drawn to include only parcels that benefit from the assessment. Therefore, the even spread of assessment throughout each narrowly drawn Zone of Benefit is indeed consistent with the SVTA decision and satisfies the "direct relationship to the "locality of the improvement" standard.

Residential Properties

Assessment Table 14

Type of Residential Property	Pop. Density Equivalent	SqFt Factor	Proposed Rate
Single Family Residential	1.00	1.00	1.00
Condominium	0.73	0.55	0.40
Duplex Triplex, Fourplex	0.64	0.42	0.27
Multi-Family Residential (5+ Units)	0.64	0.34	0.22
Mobile Home on Separate Lot	0.45	0.45	0.20

Commercial/Industrial Properties

Assessment Table 15

Type of Commercial/Industrial Land Use	Average Employees Per Acre ¹	SFE Units per Quarter Acre ²	SFE Units per Acre After 5
Commercial	24	0.500	0.500
Office	68	1.420	1.420
Shopping Center	24	0.500	0.500
Office	24	0.500	0.500
Self Storage or Parking Lot	1	0.021	
Golf Course	0.80	0.033	
Cemeteries	0.10	0.004	
Agriculture	0.05	0.002	

Vacant Properties

The benefit to vacant properties is determined to be proportional to the corresponding benefits for similar type developed properties; however, at a lower rate due to the lack of improvements on the property. A measure of the benefits accruing to the underlying land is the average value of land in relation to Improvements for developed property. The SFE factor for vacant/undeveloped parcels is 0.25 per parcel.

The benefit to undeveloped properties is determined to be proportional to the corresponding benefits for similar type developed properties, but at a lower rate due to the lack of improvements on the property. A measure of the benefits accruing to the underlying land is the average value of land in relation to Improvements for developed property. An analysis of the assessed valuation data from the County of Sacramento found that approximately 25% of the assessed value of improved properties is classified as the land value. It is reasonable to assume, therefore, that approximately 25% of the benefits are related to the underlying land and 75% are related to the improvements and the day-to-day use of the property. Using this ratio, the SFE factor for vacant/undeveloped parcels is 0.25 per parcel.

Other Properties

Article XIIID stipulates that publicly owned properties must be assessed unless there is clear and convincing evidence that those properties receive no special benefit from the assessment.

All properties that are specially benefited are assessed. Other publicly owned property that is used for purposes similar to private residential, commercial, industrial or institutional uses is benefited and assessed at the same rate as such privately owned property.

Miscellaneous, public right-of-way parcels, well, reservoir or other water rights parcels, limited access open space parcels, watershed parcels and common area parcels typically do not generate employees, residents, customers or guests. Moreover, many of these parcels have limited economic value and, therefore, do not benefit from specific enhancement of property value. Such parcels are, therefore, not specially benefited and are not assessed.

Willow Creek Estates South

There are 1102 residential lots in Village 1, 2, 3 (lots 41-93 and 155-165), 4-7 and 9A and each one is assigned 1 benefit unit (SFE.) There are 243 residential lots in Village 8 and 9b and each one is assigned 1.086 benefit unit (SFE). There are 64 residential lots in Village 3 (lots 41-93 and 155-165), and each one is assigned 1.256 benefit unit (SFE). There are 10 Lexington Business Park parcels and they are assessed at 0.618 SFEs per parcel. There are 3 Lexington Square parcels and they are assessed at 2.4710 SFEs per parcel.

Willow Creek Estates South properties are assessed per Assessment Table 10, below, as per the original formation documents:

Assessment Table 16

Description	SFEs
Single Family Parcel Village 1,2,3 (lots 41-93 and 155-165),4-7 and 9A	1.0000
Single Family Parcel Village 8 and 9b	1.0870
Single Family Parcel Village 3 (lots 41-93 and 155-165)	1.2560
Business Park Parcel	0.6180
Commercial Parcel	2.4710
Non Assessed (e.g. open space, park land etc.)	0.0000

Other Property Types

Public right-of-way parcels, well, reservoir or other water rights parcels, limited access open space parcels, watershed parcels and common area parcels typically do not generate employees, residents, customers or guests. Moreover, many of these parcels have limited economic value and, therefore, do not benefit from specific Improvement of property value. Such parcels are, therefore, not specially benefited and are not assessed.

Assessment

WHEREAS, the City of Folsom, County of Sacramento, California, pursuant to the provisions of the Landscaping and Lighting Act of 1972 and Article XIID of the California Constitution (collectively "the Act"), initiated the preparation of an Engineer's Report for the City of Folsom Landscaping and Lighting Districts;

WHEREAS, the City of Folsom directed the undersigned Engineer of Work to prepare and file a report presenting an estimate of costs, a diagram for the Districts and an assessment of the estimated costs of the improvements upon all assessable parcels within the Districts, to which the description of said proposed improvements therein contained;

NOW, THEREFORE, the undersigned, by virtue of the power vested in me under said Act and the order of the City Council of said City of Folsom, hereby make the following assessment to cover the portion of the estimated cost of said improvements, and the costs and expenses incidental thereto to be paid by the Districts.

The amount to be paid for said improvements and the expense incidental thereto, to be paid by the City of Folsom Landscaping and Lighting Districts for the fiscal year 2022-23 is generally as follows:

Summary Cost Estimates	
Improvement Costs	\$2,540,313.17
Incidental Costs	\$399,402.15
Other Costs	<u>\$782,000.00</u>
Total Improvement Costs	<u>\$3,721,715.31</u>

As required by the Act, an Assessment Diagram is hereto attached and made a part hereof showing the exterior boundaries of said City of Folsom Landscaping and Lighting Districts. The distinctive number of each parcel or lot of land in the said City of Folsom Landscaping and Lighting Districts is its Assessor Parcel Number appearing on the Assessment Roll.

And I do hereby assess and apportion said net amount of the cost and expenses of said improvements, including the costs and expenses incident thereto, upon the parcels and lots of land within said City of Folsom Landscaping and Lighting Districts, in accordance with the special benefits to be received by each parcel or lot, from the improvements, and more particularly set forth in the Cost Estimate and Method of Assessment hereto attached and by reference made a part hereof.

The assessments are made upon the parcels or lots of land within the City of Folsom Landscaping and Lighting Districts in proportion to the special benefits to be received by the parcels or lots of land, from said improvements.

The Sierra Estates, The Residences at American River Canyon, The Residences at American Canyon II Annexation to the Residences at American River Canyon, Cobble Ridge, Broadstone 3, and Natoma Valley, Willow Creek Estates East No. 2, Prospect Ridge are subject to an annual adjustment tied to the Consumer Price Index for the San Francisco Area, with a maximum annual adjustment not to exceed 4% and American River Canyon North 3, Blue Ravine No. 2, Folsom Heights No. 2, and Prairie Oaks Ranch No.2 are subject a maximum annual adjustment not to exceed 3%.

Any change in the CPI in excess of the maximum annual increase shall be cumulatively reserved as the "Unused CPI" and shall be used to increase the maximum authorized assessment rate in years in which the CPI is less than 4% for Sierra Estates, The Residences at American River Canyon, The Residences at American Canyon II Annexation to the Residences at American River Canyon, Cobble Ridge, Broadstone 3, and Natoma Valley, Willow Creek Estates East No. 2, Prospect Ridge; and is less than 3% for American River Canyon North 3, Blue Ravine No. 2, Folsom Heights No. 2 and Prairie Oaks Ranch No. 2.

The proposed assessments for the Districts that are eligible for the CPI increase will be assessed at the rate used in fiscal year 2021-2022 but are less than the maximum authorized rates. Broadstone No.4 and Willow Creek East Estates No. 2 will be assessed at the maximum authorized rate for fiscal year 2022-23.

District	Authorized Rate	Proposed Rate 22-23
American River Canyon North No. 3	\$294.90	\$269.86
Blue Ravine Oaks No. 2	\$232.90	\$213.26
Broadstone 3	\$38.83	\$28.07
Broadstone No. 4-Zone A	\$39.98	\$39.98
Broadstone No. 4-Zone B	\$38.06	\$38.06
Broadstone No. 4-Zone C	\$37.52	\$37.52
Broadstone No. 4-Zone D	\$36.80	\$36.80
Cobble Ridge	\$227.73	\$139.64
Folsom Heights No.2	\$227.70	\$208.38
Natoma Valley	\$972.56	\$856.37
Prospect Ridge	\$1,220.81	\$1,173.86
Prairie Oaks Ranch No. 2	\$323.32	\$323.32
The Residences	\$722.63	\$536.67
The Residences II	\$1,499.93	\$1,169.97
Sierra Estates	\$413.02	\$363.68
Willow Creek East Estates No 2-Zone A&B	\$103.51	\$103.51
Willow Creek East Estates No 2-Zone C	\$95.15	\$95.15

Silverbrook is subject to an annual assessment for \$132.32. However, there will be a credit in 2022-23 due to sufficiency of fund balance for current maintenance needs.

On April 9, 2013 by Resolution No. 9137, the Fieldstone Meadows Landscaping and Lighting District was dissolved. The City will no longer be responsible for maintain the improvements nor providing services within the Fieldstone Meadows Landscaping and Lighting District.

Union Square a benefit zone of Natoma Station will be maintained and serviced by their Home Owner’s Association and has not been levied since fiscal year 2009-10.

Each parcel or lot of land is described in the Assessment Roll by reference to its parcel number as shown on the Assessor's Maps of the County of Sacramento for the fiscal year 2022-23. For a more particular description of said property, reference is hereby made to the deeds and maps on file and of record in the office of the County Recorder of said County.

I hereby place opposite the Assessor Parcel Number for each parcel or lot within the Assessment Roll, the amount of the assessment for the fiscal year 2022-23 for each parcel or lot of land within the said City of Folsom Landscaping and Lighting Districts.

Dated: April 26, 2022



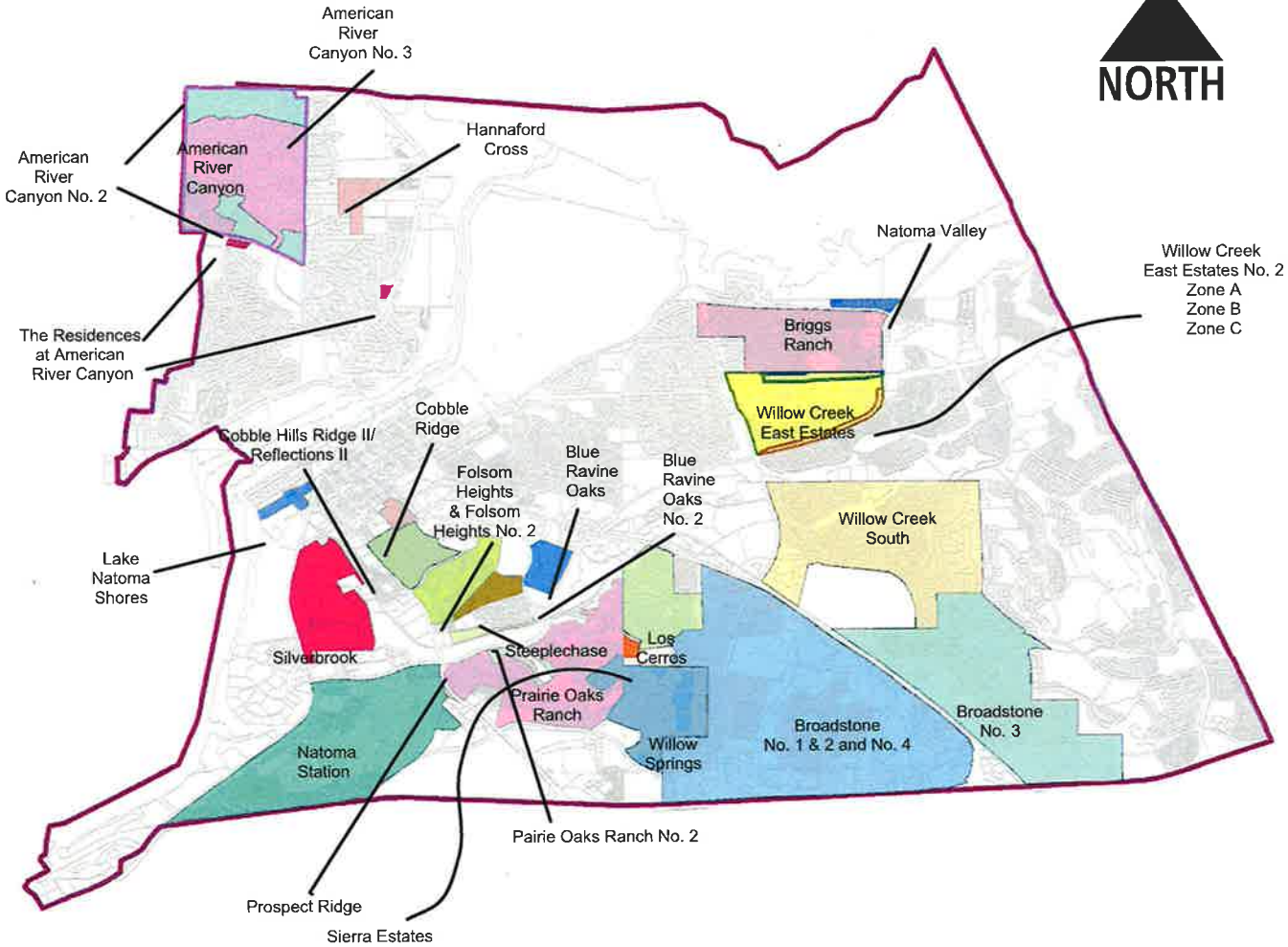
Engineer of Work
John W. Bliss, License No. C52091



Assessment Diagram

The boundaries of the City of Folsom Landscaping and Lighting Districts are displayed on the following Assessment Diagram.

The specific lines and dimensions of each lot or parcel are on file at the City.



FILED IN THE OFFICE OF THE CITY CLERK OF THE CITY OF FOLSOM, COUNTY OF SACRAMENTO, CALIFORNIA, THIS ____ DAY OF _____, 2022.

CITY CLERK

RECORDED IN THE OFFICE OF THE CITY CLERK OF THE CITY OF FOLSOM, COUNTY OF SACRAMENTO, CALIFORNIA, THIS ____ DAY OF _____, 2022.

CITY CLERK

AN ASSESSMENT WAS CONFIRMED AND LEVIED BY THE CITY COUNCIL OF THE CITY OF FOLSOM ON THE LOTS, PIECES AND PARCELS OF LAND ON THIS ASSESSMENT DIAGRAM ON THE ____ DAY OF _____, 2022 FOR FISCAL YEAR 2022-23 AND SAID ASSESSMENT DIAGRAM AND THE ASSESSMENT ROLL FOR SAID FISCAL YEAR WERE FILED IN THE OFFICE OF THE COUNTY AUDITOR OF THE COUNTY OF SACRAMENTO ON THE ____ DAY OF _____, 2022. REFERENCE IS HEREBY MADE TO SAID RECORDED ASSESSMENT ROLL FOR THE EXACT AMOUNT OF EACH ASSESSMENT LEVIED AGAINST EACH PARCEL OF LAND.

CITY CLERK

FILED THIS ____ DAY OF _____, 2022, AT THE HOUR OF ____ O'CLOCK ____ M. IN THE OFFICE OF THE COUNTY AUDITOR OF THE COUNTY OF SACRAMENTO, STATE OF CALIFORNIA, AT THE REQUEST OF THE GOVERNING BOARD OF THE CITY OF FOLSOM.

COUNTY AUDITOR, COUNTY OF SACRAMENTO

Note:
REFERENCE IS HEREBY MADE TO THE MAPS AND DEEDS OF RECORD IN THE OFFICE OF THE ASSESSOR OF THE COUNTY OF SACRAMENTO FOR A DETAILED DESCRIPTION OF THE LINES AND DIMENSIONS OF ANY PARCELS SHOWN HEREIN. THOSE MAPS SHALL GOVERN FOR ALL DETAILS CONCERNING THE LINES AND DIMENSIONS OF SUCH PARCELS. EACH PARCEL IS IDENTIFIED IN SAID MAPS BY ITS DISTINCTIVE ASSESSOR'S PARCEL NUMBER.

Appendix A -- Budgets

The attached budgets for Fiscal Year 2022-23 are included for each of the Districts.

Category Descriptions

Fund Balance Calculation:

This calculation determines funds available in a district. This calculation includes the included funds remaining after being allocated to the estimated reserve.

Estimated Reserves

Estimated Reserve to finance approximately 6 months of the following year: This is approximately 45% of the operating and incidental costs of a Landscaping and Lighting District to fund the operations until collected revenue is received from the County.

Short-Term Installments

Funds listed here are monies collected in prior years and set aside for future proposed improvements projected to be completed within the next five years.

Long-Term Installments

Funds listed here are monies collected in prior years and set aside for future proposed improvements projected to be completed within five to thirty years.

Improvement Costs

General Maintenance Costs

- Scheduled: monthly landscape maintenance and service
- Unscheduled: unscheduled but potential costs for repairs (i.e. broken sprinklers and irrigation systems), replacements (i.e. remove and replace dead tree or irrigation controller), and other services (i.e. repair fence post or treat for a specific pest) not included in monthly maintenance and service costs
- Streetlights: repair and replace bulbs and ballasts in streetlights

Service Costs

- Electrical: electric costs for streetlight maintenance and power to irrigation controllers
- Water: water costs to irrigate landscaping

Current Year Improvement Projects

Funded improvements planned to occur in the upcoming fiscal year

Incidental Costs

- Professional Services: consultant cost for Engineer's Report and Improvement Plan
- Contract Services: other contracts or professional services such as backflow testing (yearly tests), vector control, graffiti removal, and streetlight pole replacement
- Publications/Mailings/Communications: yearly notices in public hearings, mailings to Advisory Committee Members, and telephone expenses
- Staff: Landscaping and Lighting District Manager and/or inspector, clerical support, and/or other city staff.
- Overhead: General overhead (Landscaping and Lighting Districts' share of general overhead categories such as City Clerk, City Attorney, City Manager, etc.) and Department overhead (Landscaping and Lighting Districts' share of department overhead categories such as City Attorney, City Clerk and Finance Dept. Costs).
- County Auditor Fee: Per Parcel Fee charged by County to put levy on tax bills

Total Improvement Costs

This is the total of all improvement costs budgeted for the upcoming year. This cost includes current improvements that are funded by fund balance monies. Fund balance monies are monies that have been collected in prior years in anticipation of being used for specific improvements and/or intended for replacement or improvement of capital items within a district.

Assessment to Property (Current)

This calculation takes the number of single-family equivalent benefit units and multiplies it by the amount that each property within a district is will be assessed for the upcoming year. This is the total assessment amount that will be generated by the properties within the District.

District Balance

The purpose of this calculation is to describe all costs expected to occur in the upcoming year, any installments being collected as part of the upcoming year's assessment and contributions from other sources. The outcome of the calculation is the total assessment for the district. A surplus would be applied and/or credited to the upcoming year's assessment. If there are insufficient funds in the fund balance to cover the 6-month reserve, or the current and/or proposed improvements, then a deficit would exist. A deficit generally indicates that an increase in assessment may be necessary (requiring voter approval with a simple majority), however there may be a one-time reason for the deficit and an increase may not be necessary. Deficit situations are reviewed and analyzed on a case-by-case basis.

Net Assessment Calculation

This calculation determines the net assessment after the surplus or deficit is factored into the calculation. If a deficit exists, the net assessment will indicate that the assessment for the district might be too low. If a surplus exists, the net assessment will indicate that the assessment for the district might be too high. Any increased adjustments require voter approval (simple majority).

Allocated Net Assessment to Property

This calculation takes the net assessment for the district that was calculated above (i.e. factoring in a surplus or deficit) and divides it by the number of single-family equivalent benefit units. The outcome of the calculation is the total allocated net assessment per single-family equivalent benefit unit. This calculation is generally the same as the allocated assessment however if there is a deficit it will indicate the revised amount that would be required to eliminate the deficit. Conversely if there is a surplus the calculation would show the amount that the assessment could be reduced by and still cover the anticipated costs for current and future years.

Comparison of Net Assessment and Assessment

Shows a comparison of the net assessment and the current assessment and indicates a per parcel deficit or surplus.

City of Folsom American River North Landscaping and Lighting District Fund 253 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$106,204.19	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$46,981.82)	
Available Funds		\$59,222.37
Improvement Costs		
<u>General Maintenance Costs</u>		
1. Scheduled	\$0.00	
2. Unscheduled*	\$0.00	
3. Streetlights*	\$5,500.00	
4. Irrigation	\$11,000.00	
<u>Service Costs</u>		
5. Electrical*	\$30,000.00	
6. Water*	\$35,000.00	
<u>Current Year Improvement Projects</u>		
7. Sign rehab, ladder fuel removal	\$65,000.00	
	Subtotal of Item 7	\$65,000.00
Subtotal		\$146,500.00
<u>Incidental Costs</u>		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)*	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$0.00	
12. Overhead	\$4,966.00	
13. County Auditor Fee	\$593.54	
Subtotal		\$6,309.54
Total Improvement Costs		\$152,809.54
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$102.94	
Single Family Equivalent Benefit Units	1,022	
Total Assessment		\$105,204.68
Installation Costs (see Installation Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$14,695.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installation Costs		\$14,695.00
District Balance		
Total Assessment to property	\$105,204.68	
Total Improvement Costs	(\$152,809.54)	
Subtotal	(\$47,604.86)	
Total Available Funds	\$59,222.37	
Total Funds	\$11,617.51	
Total Installation Costs	(\$14,695.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$3,077.49)	
District Balance (surplus is +; deficit is ())		(\$3,077.49)
Net Assessment Calculation		
Assessment	\$105,204.68	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$3,077.49	
Net Assessment		\$108,282.17
Allocated Net Assessment to Property		
Net Assessment	\$108,282.17	
Single Family Equivalent Benefit Units	1022	
Allocated Net Assessment to Property		\$105.95
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		(\$105.95)
Allocated Assessment to Property		\$102.94
Per Parcel Surplus (+) or Deficit (-)		(\$3.01)

*Funds from fund balance

American River Canyon North - Installment Summary

District:	American River Canyon North							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$65,179							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Waterfall Pond Liner (concrete and other)	\$1,200	\$14,695	\$3,333	\$13,333	\$16,667	\$33,333	\$33,333	\$100,000
Waterfall pumps, autofill, filters, clorination system			\$2,500	\$10,000	\$12,500	\$25,000	\$25,000	\$75,000
Totals:	\$1,200	\$14,695	\$5,833	\$23,333	\$29,167	\$0	\$0	\$175,000

City of Folsom American River Canyon North No. 2 Landscaping and Lighting District Fund 270 - lights Only 2022-23		
		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$168,618.22	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$5,551.82)	
Available Funds		\$163,066.40
Improvement Costs		
<u>General Maintenance Costs</u>		
1. Scheduled	\$0.00	
2. Unscheduled	\$0.00	
3. Streetlights	\$30,000.00	
<u>Service Costs</u>		
4. Electrical	\$1,000.00	
5. Water	\$0.00	
<u>Current Year Improvement Projects</u>		
6. LED conversion	\$25,000.00	
	Subtotal of Item 6	\$25,000.00
Subtotal		\$56,000.00
<u>Incidental Costs</u>		
7. Professional Services (Engineer's Report and IP)	\$750.00	
8. Contract Services (all other contracts and services)	\$0.00	
9. Publications/Mailings/Communications	\$0.00	
10. Staff	\$0.00	
11. Overhead	\$631.00	
12. County Auditor Fee	\$94.40	
Subtotal		\$1,475.40
Total Improvement Costs		\$57,475.40
<u>Assessment to Property (Current)</u>		
Assessment per Single Family Equivalent	\$77.70	
Single Family Equivalent Benefit Units	160	
Total Assessment		\$12,432.00
<u>Installment Costs (see installment Plan and Summary next page)</u>		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$203,688.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$16,000.00	
Total Installment Costs		\$219,688.00
<u>District Balance</u>		
Total Assessment	\$12,432.00	
Total Improvement Costs	(\$57,475.40)	
Subtotal	(\$45,043.40)	
Total Available Funds	\$163,066.40	
Total Funds	\$118,023.00	
Total Installment Costs	(\$219,688.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$101,665.00)	
District Balance (surplus is +; deficit is -)		(\$101,665.00)
<u>Net Assessment Calculation</u>		
Assessment	\$12,432.00	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$101,665.00	
Net Assessment		\$114,097.00
<u>Allocated Net Assessment to Property</u>		
Net Assessment	\$114,097.00	
Single Family Equivalent Benefit Units	160	
Allocated Net Assessment to Property		\$713.11
<u>Comparison of Net Assessment and Assessment</u>		
Allocated Net Assessment to Property	(\$713.11)	
Allocated Assessment to Property	\$77.70	
Per Parcel Surplus (+) or Deficit (-)		(\$635.41)
<small>*Funds from fund balance</small>		

American River Canyon North No. 2 - Installment Summary

District:	American River Canyon North #2		(lights)					
Fiscal Year:	2022-23							
Fund Balance (2022)	\$170,458							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Paint light poles (approx. 250 poles)	\$2,000	\$57,587	\$2,000	\$8,000	\$10,000	\$20,000	\$20,000	\$60,000
Pole Replacement	\$8,000	\$66,101	\$8,000	\$32,000	\$40,000	\$80,000	\$80,000	\$240,000
Totals:	\$10,000	\$123,688	\$10,000	\$40,000	\$50,000	\$100,000	\$100,000	\$300,000

City of Folsom American River Canyon North No. 3 Landscaping and Lighting District Fund 275 2022-23		
		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$1,011,292.01	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$110,818.77)	
Available Funds		\$900,473.24
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$70,000.00	
2. Unscheduled	\$50,000.00	
3. Streetlights	\$0.00	
4. Irrigation Parts	\$3,300.00	
Service Costs		
5. Electrical	\$300.00	
6. Water	\$0.00	
Current Year Improvement Projects		
7. Landscape Improvements	\$250,000.00	
	<i>Subtotal</i>	<u>\$250,000.00</u>
Subtotal		\$373,600.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$15,351.00	
12. Overhead	\$5,387.00	
13. County Auditor Fee	\$542.54	
	<i>Subtotal</i>	<u>\$22,030.54</u>
Total Improvement Costs		\$395,630.54
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$269.86	
Single Family Equivalent Benefit Units	919.56	
Total Assessment		\$248,152.46
Installment Costs (see installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$128,000.00	
Long-Term Installment Plan (previously collected)	\$597,000.00	
Short-Term Installment Plan (collected this year)	\$12,000.00	
Long-Term Installment Plan (collected this year)	\$32,000.00	
Total Installment Costs		\$769,000.00
District Balance		
Total Assessment	\$248,152.46	
Total Improvement Costs	(\$395,630.54)	
<i>Subtotal</i>	<u>(\$147,478.08)</u>	
Total Available Funds	\$900,473.24	
Total Funds	\$752,995.16	
Annual Installment (collected this year)	(\$769,000.00)	
Contributions from other sources	\$0.00	
<i>Net Balance</i>	<u>(\$16,004.84)</u>	
District Balance (surplus is +; deficit is {})		(\$16,004.84)
Net Assessment Calculation		
Assessment	\$248,152.46	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$16,004.84	
Net Assessment		\$264,157.30
Allocated Net Assessment to Property		
Net Assessment	\$264,157.30	
Single Family Equivalent Benefit Units	920	
Allocated Net Assessment to Property		\$287.26
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	(\$287.26)	
Allocated Assessment to Property	\$269.86	
Per Parcel Surplus (+) or Deficit (-)		(\$17.40)

Funds from fund balance

American River Canyon North No. 3 - Installment Summary

District:	American River Canyon North #3							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$1,078,365							
Short Term Installment Summary								
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Approx.
	Installment	Collections	2018	2019	2020	2021	2022	Total
								Required
Irrigation Controller Upgrade-centralized (1 controller)								\$15,000
Signage Replacement								\$60,000
Mystic Hills replace missing landscape								\$25,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$100,000
Long Term Installment Summary								
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Approx.
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Total
								Required
Waterfall Rock Repair								\$75,000
Baldwin Dam Path Repair								\$350,000
Tree and landscape improvement (or replacement)								\$300,000
ARC Drive/Canyon Falls (Cascade perimeter), landscape/ remove, replace trees, mow band replacement								\$135,000
Main Walking Trail landscaping, irrigation, stairs, clean-up								\$150,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$1,010,000

City of Folsom Blue Ravine Oaks Landscaping and Lighting District Fund 250 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$100,274.58	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$16,107.53)	
Available Funds		\$84,167.05
Improvement Costs		
<u>General Maintenance Costs</u>		
1. Scheduled	\$0.00	
2. Unscheduled	\$0.00	
3. Streetlights	\$1,200.00	
<u>Service Costs</u>		
4. Electrical	\$4,000.00	
5. Water	\$12,500.00	
<u>Current Year Improvement Projects</u>		
6. No Planned Projects	\$0.00	
Subtotal of Item 6	\$0.00	
Subtotal		\$17,700.00
<u>Incidental Costs</u>		
7. Professional Services (Engineer's Report and IP)	\$750.00	
8. Contract Services (all other contracts and services)	\$0.00	
9. Publications/Mailings/Communications	\$0.00	
10. Staff	\$0.00	
11. Overhead	\$1,496.00	
12. County Auditor Fee	\$97.35	
Subtotal		\$2,343.35
Total Improvement Costs		\$20,043.35
<u>Assessment to Property (Current)</u>		
Assessment per Single Family Equivalent	\$218.60	
Single Family Equivalent Benefit Units	165	
Total Assessment		\$36,069.00
<u>Installation Costs (see installment Plan and Summary next page)</u>		
Short-Term Installment Plan (previously collected)	\$11,000.00	
Long-Term Installment Plan (previously collected)	\$0.00	
Short-Term Installment Plan (collected this year)	\$1,000.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installation Costs		\$12,000.00
<u>District Balance</u>		
Total Assessment	\$36,069.00	
Total Improvement Costs	(\$20,043.35)	
Subtotal	\$16,025.65	
Total Available Funds	\$84,167.05	
Total Funds	\$100,192.70	
Total Installment Cost	(\$12,000.00)	
Contributions from other sources	\$0.00	
Net Balance	\$88,192.70	
District Balance (surplus is +; deficit is (-))		\$88,192.70
<u>Net Assessment Calculation</u>		
Assessment	\$36,069.00	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$88,192.70)	
Net Assessment		(\$52,123.70)
<u>Allocated Net Assessment to Property</u>		
Net Assessment	(\$52,123.70)	
Single Family Equivalent Benefit Units	165	
Allocated Net Assessment to Property		(\$315.90)
<u>Comparison of Net Assessment and Assessment</u>		
Allocated Net Assessment to Property	\$315.90	
Allocated Assessment to Property	\$218.60	
Per Parcel Surplus (+) or Deficit (-)		\$534.50

*Funds from Fund balance

Blue Ravine Oaks - Installment Summary

District:	Blue Ravine Oaks (The Shores)							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$108,044							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Blue Ravine wall repair	\$5,000	\$5,000	\$5,000	\$50,000	\$5,000	\$5,000	\$5,000	\$130,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom Blue Ravine Oaks No. 2 Landscaping and Lighting District Fund 278 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$183,683.79	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$15,714.05)	
Available Funds		\$167,969.74
Improvement Costs		
<u>General Maintenance Costs</u>		
1. Scheduled	\$12,500.00	
2. Unscheduled	\$15,000.00	
3. Streetlights	\$0.00	
4. Irrigation	\$1,300.00	
<u>Service Costs</u>		
5. Electrical	\$0.00	
6. Water	\$0.00	
<u>Current Year Improvement Projects</u>		
7. Drip conversion/Fence replacement	\$20,000.00	
Subtotal of Item 8	\$20,000.00	
Subtotal		\$48,800.00
<u>Incidental Costs</u>		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$4,080.00	
12. Overhead	\$990.00	
13. County Auditor Fee	\$97.35	
Subtotal		\$5,917.35
Total Improvement Costs		\$54,717.35
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$213.26	
Single Family Equivalent Benefit Units	165	
Total Assessment		\$35,187.90
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$1,000.00	
Long-Term Installment Plan (previously collected)	\$73,600.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$9,200.00	
Total Installment Costs		\$83,800.00
District Balance		
Total Assessment	\$35,187.90	
Total Improvement Costs	(\$54,717.35)	
Subtotal	(\$19,529.45)	
Total Available Funds	\$167,969.74	
Total Funds	\$148,440.29	
Total Installment Cost	(\$83,800.00)	
Contributions from other sources	\$0.00	
Net Balance	\$64,640.29	
District Balance (surplus is +; deficit is -)		\$64,640.29
Net Assessment Calculation		
Assessment	\$35,187.90	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$64,640.29)	
Net Assessment		(\$29,452.39)
Allocated Net Assessment to Property		
Net Assessment	(\$29,452.39)	
Single Family Equivalent Benefit Units	165	
Allocated Net Assessment to Property		(\$178.50)
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		\$178.50
Allocated Assessment to Property		\$213.26
Per Parcel Surplus (+) or Deficit (-)		\$391.76

Blue Ravine Oaks No. 2 - Installment Summary

District:	Blue Ravine Oaks No. 2							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$180,675							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Tree Removal/Replacement (Blue Ravine Road)	\$1,000	\$1,000	\$0	\$0	\$0	\$0	\$0	\$35,000
Totals:	\$1,000	\$1,000	\$0	\$0	\$0	\$0	\$0	\$35,000
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Fence Replacement on Riley	\$1,200	\$1,200	\$1,200	\$1,200	\$1,200	\$1,200	\$1,200	\$65,000
Tree Removal/Replacement	\$2,500	\$2,500	\$2,500	\$2,500	\$2,500	\$2,500	\$2,500	\$80,000
Signage Replacement	\$500	\$500	\$500	\$500	\$500	\$500	\$500	\$4,000
Totals:	\$4,200	\$4,200	\$4,200	\$4,200	\$4,200	\$4,200	\$4,200	\$149,000

City of Folsom Briggs Ranch Landscaping and Lighting District Fund 205 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	(\$19,473.48)	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$36,041.32)	
Available Funds		(\$55,514.80)
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$45,000.00	
2. Unscheduled	\$15,000.00	
3. Streetlights	\$4,500.00	
4. Irrigation	\$2,500.00	
Service Costs		
5. Electrical	\$9,000.00	
6. Water	\$8,500.00	
Current Year Improvement Projects		
7. No planned projects	\$0.00	
Subtotal	Subtotal of Item 7	\$0.00
		\$84,500.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$12,727.00	
12. Overhead	\$2,965.00	
13. County Auditor Fee	\$389.41	
Subtotal		\$16,831.41
Total Improvement Costs		\$101,331.41
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$122.28	
Single Family Equivalent Benefit Units	660.01	
Total Assessment		\$80,706.02
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$59,000.00	
Long-Term Installment Plan (previously collected)	\$101,890.00	
Short-Term Installment Plan (collected this year)	\$2,000.00	
Long-Term Installment Plan (collected this year)	\$3,410.00	
Total Installment Costs		\$166,300.00
District Balance		
Total Assessment	\$80,706.02	
Total Improvement Costs	(\$101,331.41)	
Subtotal	(\$20,625.38)	
Total Available Funds	(\$55,514.80)	
Total Funds	(\$76,140.18)	
Total Installment Cost	(\$166,300.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$242,440.18)	
District Balance (surplus is +; deficit is ())		(\$242,440.18)
Net Assessment Calculation		
Assessment	\$80,706.02	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$242,440.18	
Net Assessment	\$323,146.21	
Allocated Net Assessment to Property		
Net Assessment	\$323,146.21	
Single Family Equivalent Benefit Units	660	
Allocated Net Assessment to Property	\$489.61	
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	(\$489.61)	
Allocated Assessment to Property	\$122.28	
Per Parcel Surplus (+) or Deficit (-)	(\$367.33)	
*Funds from fund balance		

Briggs Ranch - Installment Summary

District:	Briggs Ranch							
Fiscal Year:	2022-23							
Fund Balance (2022)	(\$9,396)							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Shrub and tree upgrades (E. Natoma & Blue Ravine)	\$10,000	\$17,000	\$500	\$500	\$500	\$500	\$500	\$50,000
Fence/Wall repair/replacement	\$10,000	\$17,000	\$500	\$500	\$500	\$500	\$500	\$50,000
Fence repair/replacement (E. Natoma partial)	\$2,000	\$500	\$500	\$500	\$500	\$500	\$500	\$50,000
Pet Stations repair/replacement	\$2,000	\$6,000	\$500	\$500	\$500	\$500	\$500	\$6,000
Totals:	\$24,000	\$40,500	\$2,000	\$2,000	\$2,000	\$2,000	\$2,000	\$156,000
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Bollard repair/replacement	\$2,000	\$39,020	\$410	\$410	\$410	\$410	\$410	\$60,000
Fence repair/replacement (Blue Ravine Partial)	\$4,000	\$8,000	\$500	\$500	\$500	\$500	\$500	\$60,000
Fence repair/replacement (E. Natoma Partial)	\$4,000	\$8,000	\$500	\$500	\$500	\$500	\$500	\$60,000
Entry Sign Replacement (brass lettering)	\$1,000	\$3,500	\$500	\$500	\$500	\$500	\$500	\$10,000
Irrigation upgrades/replacement (3 controllers)	\$2,000	\$5,000	\$500	\$500	\$500	\$500	\$500	\$10,000
Landscape lighting upgrades or replacement	\$5,000	\$9,500	\$500	\$500	\$500	\$500	\$500	\$75,000
Tree and landscape improvement (partial collection)	\$2,000	\$5,000	\$500	\$500	\$500	\$500	\$500	\$10,000
Totals:	\$20,000	\$78,020	\$3,410	\$3,410	\$3,410	\$3,410	\$3,410	\$285,000

**City of Folsom
Broadstone Landscaping and Lighting District
Fund 210
2022-23**

	Total Budget
Fund Balance Calculation	
Starting Fund Balance (as of April 2022)	(\$112,322.00)
Estimated Reserve to finance approx. first 6 months of 22-23	(\$174,548.29)
Available Funds	(\$286,870.29)

Improvement Costs	
General Maintenance Costs	
1. Scheduled	\$0.00
2. Unscheduled	\$0.00
3. Streetlights	\$0.00
Service Costs	
4. Electrical	\$47,500.00
5. Water	\$145,000.00
Current Year Improvement Projects	
6. No Planned Projects	\$0.00
Subtotal	\$192,500.00
Incidental Costs	
7. Professional Services (Engineer's Report and IP)	\$750.00
8. Contract Services (all other contracts and services)	\$0.00
9. Publications/Mailings/Communications	\$0.00
10. Staff	\$54,909.00
11. Overhead	\$9,576.00
12. County Auditor Fee	\$1,397.70
Subtotal	\$66,632.70
Total Improvement Costs	\$259,132.70

Assessment to Property (Current)	
Assessment per Single Family Equivalent	\$164.99
Single Family Equivalent Benefit Units	2,368.99
Total Assessment	\$390,859.66

Installment Costs (see Installment Plan and Summary next page)	
Short-Term Installment Plan (previously collected)	\$0.00
Long-Term Installment Plan (previously collected)	\$0.00
Short-Term Installment Plan (collected this year)	\$0.00
Long-Term Installment Plan (collected this year)	\$0.00
Total Installment Costs	\$0.00

District Balance	
Total Assessment	\$390,859.66
Total Improvement Costs	(\$259,132.70)
Subtotal	\$131,726.96
Total Available Funds	(\$286,870.29)
Total Funds	(\$155,143.33)
Total Installment Cost	\$0.00
Contributions from other sources	\$0.00
Net Balance	(\$155,143.33)
District Balance (surplus is +; deficit is {})	(\$155,143.33)

Net Assessment Calculation	
Assessment	\$390,859.66
Surplus or Deficit (surplus is subtracted; deficit is added)	\$155,143.33
Net Assessment	\$546,002.99

Allocated Net Assessment to Property	
Net Assessment	\$546,002.99
Single Family Equivalent Benefit Units	2369
Allocated Net Assessment to Property	\$230.48

Comparison of Net Assessment and Assessment	
Allocated Net Assessment to Property	(\$230.48)
Allocated Assessment to Property	\$164.99
Per Parcel Surplus (+) or Deficit (-)	(\$65.49)

*Funds from fund balance

Broadstone 1 and 2 - Installment Summary

District:	Broadstone								
Fiscal Year:	2022-23								
Fund Balance (2022)	(\$112,322)								
Short Term Installment Summary									
									Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total	
	Installment	Collections	2018	2019	2020	2021	2022	Required	
Tree & Landscape Improvements (partial fund collection)									\$25,000
Bollar repair/replacement (40)									\$15,000
Light pole/fixture replacement of KW									\$10,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$50,000
Long Term Installment Summary									
									Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total	
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required	
Repair irrigation/replace shrubs Iron Point median									\$50,000
Tree & landscape improvement (or replacement)									\$160,000
Shrub replacement - throughout (some irrigation repair) 28 acres									\$1,000,000
Landscape light replacement									\$10,000
Pet station replacement (7)									\$11,000
Signage repair/replacement									\$40,000
Turf removal/irrigation retrofit									\$200,000
Irrigation upgrades and flow (15 controllers)									\$45,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$1,516,000

City of Folsom Broadstone 3 Landscaping and Lighting District Fund 209 - Streetlights Only 2022-23		
		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$57,738.73	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$10,172.83)	
Available Funds		\$47,565.90
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$0.00	
2. Unscheduled	\$0.00	
3. Streetlights	\$12,500.00	
Service Costs		
4. Electrical	\$2,000.00	
5. Water	\$0.00	
Current Year Improvement Projects		
6. LED Conversion	\$10,000.00	
	<i>Subtotal of Item 6</i>	<u>\$10,000.00</u>
Subtotal		\$24,500.00
Incidental Costs		
7. Professional Services (Engineer's Report and IP)	\$750.00	
8. Contract Services (all other contracts and services)	\$0.00	
9. Publications/Mailings/Communications	\$0.00	
10. Staff	\$0.00	
11. Overhead	\$1,436.00	
12. County Auditor Fee	\$699.00	
		<u>\$2,885.00</u>
Subtotal		\$2,885.00
Total Improvement Costs		\$27,385.00
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$28.07	
Single Family Equivalent Benefit Units	811.53	
Total Assessment		\$22,779.65
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$5,000.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$5,000.00
District Balance		
Total Assessment	\$22,779.65	
Total Improvement Costs	(\$27,385.00)	
Subtotal	(\$4,605.35)	
Total Available Funds	\$58,105.90	
Total Funds	\$53,500.55	
Total Installment Cost	(\$5,000.00)	
Contributions from other sources	\$0.00	
Net Balance	\$48,500.55	
District Balance (surplus is +; deficit is ())		\$48,500.55
Net Assessment Calculation		
Assessment	\$22,779.65	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$48,500.55)	
Net Assessment		(\$25,720.90)
Allocated Net Assessment to Property		
Net Assessment	(\$25,720.90)	
Single Family Equivalent Benefit Units	812	
Allocated Net Assessment to Property		(\$31.69)
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	\$31.69	
Allocated Assessment to Property	\$28.07	
Per Parcel Surplus (+) or Deficit (-)		\$59.76

*Funds from fund balance

Broadstone 3 - Installment Summary

District:	Broadstone #3							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$39,886							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Total	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Paint Streetlight Poles (350 poles)	\$5,000	\$0	\$1,667	\$6,667	\$8,333	\$16,667	\$16,667	\$50,000
Total	\$5,000	\$0	\$1,667	\$6,667	\$8,333	\$16,667	\$16,667	\$50,000

City of Folsom Broadstone No. 4 Landscaping and Lighting District Fund 282 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$166,588.00	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$36,869.50)	
Available Funds		\$129,718.50
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$165,000.00	
2. Unscheduled	\$15,000.00	
3. Streetlights	\$42,500.00	
4. Irrigation	\$35,000.00	
Service Costs		
5. Electrical	\$0.00	
6. Water	\$0.00	
Current Year Improvement Projects		
7. Tree pruning	\$90,000.00	
Subtotal	Subtotal of Item 6 \$90,000.00	\$347,500.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$6,999.00	
12. Overhead	\$7,040.00	
13. County Auditor Fee	\$421.38	
Subtotal		\$15,210.38
Total Improvement Costs		\$362,710.38
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$39.98	
Single Family Equivalent Benefit Units	2,065.05	
Total Assessment		\$82,560.53
Installation Costs (see installation Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$0.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installation Costs		\$0.00
District Balance		
Total Assessment	\$82,560.53	
Total Improvement Costs	(\$362,710.38)	
Subtotal	(\$280,149.85)	
Total Available Funds	\$129,718.50	
Total Funds	(\$150,431.35)	
Total Installation Cost	\$0.00	
Contributions from other sources	\$0.00	
Net Balance	(\$150,431.35)	
District Balance (surplus is +; deficit is ())		(\$150,431.35)
Net Assessment Calculation		
Assessment	\$82,560.53	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$150,431.35	
Net Assessment		\$232,991.88
Allocated Net Assessment to Property		
Net Assessment	\$232,991.88	
Single Family Equivalent Benefit Units	2065	
Allocated Net Assessment to Property		\$112.83
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		(\$112.83)
Allocated Assessment to Property		\$39.98
Per Parcel Surplus (+) or Deficit (-)		(\$72.85)

*Funds from fund balance

Broadstone 4 - Installment Summary

District:	Broadstone No. 4							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$166,588							
Short Term Installment Summary								
Project	Yearly Installment	Prior Years Collections	Year 1 2018	Year 2 2019	Year 3 2020	Year 4 2021	Year 5 2022	Approx. Total Required
Repair irrigation/replant shrubs Rathbone, Knofler, other interior areas	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$50,000
Landscape light repair/replacement (60 lights)	\$0	\$0	\$0	\$0	\$0	\$0	\$0	6000
Total	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$56,000
Long Term Installment Summary								
Project	Yearly Installment	Prior Years Collections	Year 1 2018	Years 2-5 2019-2022	Years 5-10 2022-2027	Years 10-20 2027-2037	Years 20-30 2037-2047	Approx. Total Required
Total	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom Cobble Hills II/Reflections II Landscaping and Lighting District Fund 214 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	(\$17,604.11)	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$19,654.43)	
Available Funds		(\$37,258.54)
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$20,000.00	
2. Unscheduled	\$5,000.00	
3. Streetlights	\$1,250.00	
4. Irrigation	\$2,600.00	
Service Costs		
5. Electrical	\$2,000.00	
6. Water	\$10,500.00	
Current Year Improvement Projects		
7. No Planned Projects	\$0.00	
Subtotal	<i>Subtotal of Item 7</i>	\$0.00
		\$41,350.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$9,151.00	
12. Overhead	\$1,948.00	
13. County Auditor Fee	\$229.51	
Subtotal		\$12,078.51
Total Improvement Costs		\$53,428.51
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$113.14	
Single Family Equivalent Benefit Units	389	
Total Assessment		\$44,011.46
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$43,856.00	
Long-Term Installment Plan (previously collected)	\$54,000.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$97,856.00
District Balance		
Total Assessment	\$44,011.46	
Total Improvement Costs	(\$53,428.51)	
Subtotal	(\$9,417.05)	
Total Available Funds	(\$37,258.54)	
Total Funds	(\$46,675.59)	
Total Installment Cost	(\$97,856.00)	
Contributions from other sources	\$5,000.00	
Net Balance	(\$139,531.59)	
District Balance (surplus is +; deficit is -)		(\$139,531.59)
Net Assessment Calculation		
Assessment	\$44,011.46	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$139,531.59	
Net Assessment		\$183,543.05
Allocated Net Assessment to Property		
Net Assessment	\$183,543.05	
Single Family Equivalent Benefit Units	389	
Allocated Net Assessment to Property		\$471.83
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	(\$471.83)	
Allocated Assessment to Property	\$113.14	
Per Parcel Surplus (+) or Deficit (-)		(\$358.69)

*Funds from fund balance

Cobble Hills Ridge - Installment Summary

District:	Cobble Hills II/Reflections II							
Fiscal Year:	2022-23							
Fund Balance (2022)	(\$15,807)							
Short Term Installment Summary								
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Approx.
	Installation	Collections	2018	2019	2020	2021	2022	Total Required
Tree & landscape improvements (or replacement)	\$5,000	\$38,856	\$0	\$0	\$0	\$0	\$0	\$50,000
Mini Park & Path to Lembi Turf and Shrub Repair/Replacement	\$10,000	\$5,000	\$0	\$0	\$0	\$0	\$0	\$40,000
Totals:	\$15,000	\$43,856	\$0	\$0	\$0	\$0	\$0	\$90,000
Long Term Installment Summary								
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Approx.
	Installation	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Total Required
Fence Repair/Replacment (225 ft)	\$1,000	\$6,500	\$0	\$0	\$0	\$0	\$0	\$10,000
Wall Repairs and Painting (628 ft)	\$1,000	\$35,500	\$0	\$0	\$0	\$0	\$0	\$40,000
Shrub Replacement-Glenn/Oxborough	\$5,000	\$7,500	\$0	\$0	\$0	\$0	\$0	\$65,000
Shrub Replacement-Sibley and Corner	\$1,000	\$0	\$0	\$0	\$0	\$0	\$0	\$15,000
Signage Repair/Replacement	\$1,000	\$1,500	\$0	\$0	\$0	\$0	\$0	\$8,000
Tree & landscape improvements (or replacement)	\$2,000	\$3,000	\$0	\$0	\$0	\$0	\$0	\$53,600
Totals:	\$11,000	\$54,000	\$0	\$0	\$0	\$0	\$0	\$191,600

City of Folsom Cobble Ridge Landscaping and Lighting District Fund 234 2022-23		
		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$104,313.62	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$6,111.26)	
Available Funds		\$98,202.36
Improvement Costs		
<u>General Maintenance Costs</u>		
1. Scheduled	\$2,500.00	
2. Unscheduled	\$3,000.00	
3. Streetlights	\$750.00	
4. Irrigation	\$400.00	
<u>Service Costs</u>		
5. Electrical	\$500.00	
6. Water	\$1,250.00	
<u>Current Year Improvement Projects</u>		
7. Fence repair/Plant & bark	\$5,500.00	
	Subtotal of Item 6	\$5,500.00
Subtotal		\$13,900.00
<u>Incidental Costs</u>		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$1,030.00	
12. Overhead	\$422.00	
13. County Auditor Fee	\$57.82	
Subtotal		\$2,259.82
Total Improvement Costs		\$16,159.82
<u>Assessment to Property (Current)</u>		
Assessment per Single Family Equivalent	\$139.64	
Single Family Equivalent Benefit Units	98	
Total Assessment		\$13,684.72
<u>Installment Costs (see installment Plan and Summary next page)</u>		
Short-Term Installment Plan (previously collected)	\$31,518.00	
Long-Term Installment Plan (previously collected)	\$29,000.00	
Short-Term Installment Plan (collected this year)	\$1,000.00	
Long-Term Installment Plan (collected this year)	\$1,000.00	
Total Installment Costs		\$62,518.00
<u>District Balance</u>		
Total Assessment	\$13,684.72	
Total Improvement Costs	(\$16,159.82)	
Subtotal	(\$2,475.10)	
Total Available Funds	\$98,202.36	
Total Funds	\$95,727.26	
Total Installment Cost	(\$62,518.00)	
Contributions from other sources	\$0.00	
Net Balance	\$33,209.26	
District Balance (surplus is +; deficit is {})		\$33,209.26
<u>Net Assessment Calculation</u>		
Assessment	\$13,684.72	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$33,209.26)	
Net Assessment		(\$19,524.54)
<u>Allocated Net Assessment to Property</u>		
Net Assessment	(\$19,524.54)	
Single Family Equivalent Benefit Units	98	
Allocated Net Assessment to Property		(\$199.23)
<u>Comparison of Net Assessment and Assessment</u>		
Allocated Net Assessment to Property	\$199.23	
Allocated Assessment to Property	\$139.64	
Per Parcel Surplus (+) or Deficit (-)		\$338.87

*Funds from fund balance

Cobble Ridge – Installment Summary

District:	Cobble Ridge							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$177,559							
Short Term Installment Summary								
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Approx.
	Installment	Collections	2018	2019	2020	2021	2022	Total
								Required
year 2 pruning								\$3,000
year 3 pruning								\$3,000
year 4 pruning								\$3,000
year 5 pruning								\$3,000
Fence repair and painting								\$5,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$17,000
Long Term Installment Summary								
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Approx.
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Total
								Required
Fence Repair/Replacement (340 ft)	\$2,000	\$14,750	\$250	\$250	\$250	\$250	\$250	\$20,000
Shrub Replacement	\$2,000	\$4,750	\$250	\$250	\$250	\$250	\$250	\$20,000
Tubular Fence repair/replacement	\$400	\$3,000	\$250	\$250	\$250	\$250	\$250	\$4,000
Tree & landscape improvements (or replacements)	\$500	\$2,500	\$250	\$250	\$250	\$250	\$250	\$3,000
Totals:	\$4,900	\$25,000	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$47,000

City of Folsom		Total Budget
Folsom Heights Landscaping and Lighting District		
Fund 208		
2022-23		
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$25,081.07	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$9,749.20)	
Available Funds		\$15,331.87
Improvement Costs		
General Maintenance Costs		
1. Scheduled**	\$0.00	
2. Unscheduled	\$0.00	
3. Streetlights	\$2,000.00	
4. Irrigation	\$2,750.00	
Service Costs		
5. Electrical	\$3,650.00	
6. Water	\$5,750.00	
Current Year Improvement Projects		
7. Open space/Tree work/Ladder fuel removal	\$28,000.00	
Subtotal	<i>Subtotal of item 7</i>	\$42,150.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$0.00	
12. Overhead	\$823.00	
13. County Auditor Fee	\$181.72	
Subtotal		\$1,754.72
Total Improvement Costs		\$43,904.72
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$70.88	
Single Family Equivalent Benefit Units	308	
Total Assessment		\$21,831.04
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$56,000.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$56,000.00
District Balance		
Total Assessment	\$21,831.04	
Total Improvement Costs	(\$43,904.72)	
Subtotal	(\$22,073.68)	
Total Available Funds	\$15,331.87	
Total Funds	(\$6,741.81)	
Total Installment Cost	(\$56,000.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$62,741.81)	
District Balance (surplus is +; deficit is {})		(\$62,741.81)
Net Assessment Calculation		
Assessment	\$21,831.04	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$62,741.81	
Net Assessment		\$84,572.85
Allocated Net Assessment to Property		
Net Assessment	\$84,572.85	
Single Family Equivalent Benefit Units	308	
Allocated Net Assessment to Property		\$274.59
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	(\$274.59)	
Allocated Assessment to Property	\$70.88	
Per Parcel Surplus (+) or Deficit (-)		(\$203.71)
<small>**Regular Maintenance reduced by 1/2 to 2 times care/month</small>		

Folsom Heights - Installment Summary

District:	Folsom Heights							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$31,405							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Ladder fuel work								\$25,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$25,000
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom Folsom Heights No. 2 Landscaping and Lighting District Fund 281 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$265,432.27	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$27,860.45)	
Available Funds		\$237,571.82
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$16,275.00	
2. Unscheduled	\$10,000.00	
3. Streetlights	\$0.00	
Service Costs		
4. Electrical	\$0.00	
5. Water	\$0.00	
Current Year Improvement Projects		
6. Tree pruning/Ladder fuel removal	\$37,000.00	
	Subtotal of Item 6	\$37,000.00
Subtotal		\$63,275.00
Incidental Costs		
7. Professional Services (Engineer's Report and IP)	\$750.00	
8. Contract Services (all other contracts and services)	\$0.00	
9. Publications/Mailings/Communications	\$0.00	
10. Staff	\$5,100.00	
11. Overhead	\$1,102.00	
12. County Auditor Fee	\$0.00	
Subtotal		\$6,952.00
Total Improvement Costs		\$70,227.00
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$208.38	
Single Family Equivalent Benefit Units	299.99	
Total Assessment		\$62,386.89
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$0.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$0.00
District Balance		
Total Assessment	\$62,386.89	
Total Improvement Costs	(\$70,227.00)	
Subtotal	(\$7,840.11)	
Total Available Funds		\$237,571.82
Total Funds	\$229,731.71	
Total Installment Cost	\$0.00	
Contributions from other sources	\$0.00	
Net Balance	\$229,731.71	
District Balance (surplus is +; deficit is {})		\$229,731.71
Net Assessment Calculation		
Assessment	\$62,386.89	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$229,731.71)	
Net Assessment		(\$167,344.82)
Allocated Net Assessment to Property		
Net Assessment	(\$167,344.82)	
Single Family Equivalent Benefit Units	299	
Allocated Net Assessment to Property		(\$558.95)
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	\$558.95	
Allocated Assessment to Property	\$208.38	
Per Parcel Surplus (+) or Deficit (-)		\$767.33

*Funds from fund balance

Folsom Heights No. 2 - Installment Summary

District:	Folsom Heights No. 2							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$290,736							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
year 3 pruning								\$8,000
year 4 pruning								\$8,000
year 5 pruning								\$8,000
fence work								\$2,000
Tree care in open space								\$3,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$29,000
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Glenn wall repair			\$0	\$0	\$0	\$0	\$0	\$0
Tree and landscape imprv (Vierra Cir)			\$0	\$0	\$0	\$0	\$0	\$0
New landscape (Glenn)			\$0	\$0	\$0	\$0	\$0	\$0
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom Hannafor Cross Landscaping and Lighting District Fund 212 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$18,376.10	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$9,005.34)	
Available Funds		\$9,370.76
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$11,500.00	
2. Unscheduled	\$2,500.00	
3. Streetlights	\$1,250.00	
4. Irrigation	\$650.00	
Service Costs		
5. Electrical	\$2,500.00	
6. Water	\$2,500.00	
Current Year Improvement Projects		
7. No Planned Projects	\$0.00	
Subtotal	Subtotal of Item B \$0.00	\$20,900.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mallings/Communications	\$0.00	
11. Staff	\$3,776.00	
12. Overhead	\$743.00	
13. County Auditor Fee	\$60.77	
Subtotal		\$5,329.77
Total Improvement Costs		\$26,229.77
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$195.78	
Single Family Equivalent Benefit Units	103	
Total Assessment		\$20,165.34
Installment Costs (see installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$7,500.00	
Long-Term Installment Plan (previously collected)	\$28,440.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$544.00	
Total Installment Costs		\$36,484.00
District Balance		
Total Assessment	\$20,165.34	
Total Improvement Costs	(\$26,229.77)	
Subtotal	(\$6,064.43)	
Total Available Funds	\$11,530.76	
Total Funds	\$5,466.33	
Total Installment Cost	(\$36,484.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$31,017.67)	
District Balance (surplus is +; deficit is {})		(\$31,017.67)
Net Assessment Calculation		
Assessment	\$20,165.34	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$31,017.67	
Net Assessment		\$51,183.01
Allocated Net Assessment to Property		
Net Assessment	\$51,183.01	
Single Family Equivalent Benefit Units	103	
Allocated Net Assessment to Property		\$496.92
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		(\$496.92)
Allocated Assessment to Property		\$195.78
Per Parcel Surplus (+) or Deficit (-)		(\$301.14)

*Funds from Fund Balance

Hannaford Cross - Installment Summary

District:	Hannaford Cross							
Fiscal Year:	2022-23							
Fund Balance (2022)	(\$5,571)							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Lakeside fence repair								\$25,000
year 2 pruning								\$5,000
year 3 pruning								\$5,000
year 4 pruning								\$50,000
year 5 pruning								\$5,000
Repairs at guardshack								\$3,500
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$93,500
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom		
Lake Natoma Shores Landscaping and Lighting District		
Fund 213		
2022-23		
		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$97,213.26	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$9,264.00)	
Available Funds		\$87,949.26
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$7,000.00	
2. Unscheduled	\$5,000.00	
3. Streetlights	\$750.00	
4. Irrigation	\$1,000.00	
Service Costs		
5. Electrical	\$700.00	
6. Water	\$3,000.00	
Current Year Improvement Projects		
7. LED conversion/Tree work	\$8,000.00	
	Subtotal of Item 7	\$8,000.00
Subtotal		\$25,450.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$2,860.00	
12. Overhead	\$943.00	
13. County Auditor Fee	\$66.67	
Subtotal		\$4,619.67
Total Improvement Costs		\$30,069.67
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$183.58	
Single Family Equivalent Benefit Units	113	
Total Assessment		\$20,744.54
Installment Costs (see installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$26,857.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$1,250.00	
Total Installment Costs		\$28,107.00
District Balance		
Total Assessment	\$20,744.54	
Total Improvement Costs	(\$30,069.67)	
Subtotal	(\$9,325.13)	
Total Available Funds	\$87,949.26	
Total Funds	\$78,624.13	
Total Installment Cost	(\$28,107.00)	
Contributions from other sources	\$0.00	
Net Balance	\$50,517.13	
District Balance (surplus is +; deficit is (-))		\$50,517.13
Net Assessment Calculation		
Assessment	\$20,744.54	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$50,517.13)	
Net Assessment		(\$29,772.59)
Allocated Net Assessment to Property		
Net Assessment		(\$29,772.59)
Single Family Equivalent Benefit Units	113	
Allocated Net Assessment to Property		(\$263.47)
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		\$263.47
Allocated Assessment to Property		\$183.58
Per Parcel Surplus (+) or Deficit (-)		\$447.05

*Funds from fund balance

Lake Natoma Shores - Installment Summary

District:	Lake Natoma Shores							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$101,218							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
LED landscape lights								\$5,000
year 3 pruning								5000
year 4 pruning								5000
year 5 pruning								5000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$20,000
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Signage Repair/Replacement	\$1,000	\$14,607	\$500	\$500	\$500	\$500	\$500	\$15,000
Turf repair/irrigation upgrades	\$1,000	\$2,750	\$250	\$250	\$250	\$250	\$250	\$22,000
Tree & landscape improvements (or replacements)	\$1,000	\$4,500	\$500	\$500	\$500	\$500	\$500	\$16,000
Totals:	\$3,000	\$21,857	\$1,250	\$1,250	\$1,250	\$1,250	\$1,250	\$53,000

City of Folsom Los Cerros Landscaping and Lighting District Fund 204 2022-23		
		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$152,489.98	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$18,237.09)	
Available Funds		\$134,252.89
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$12,500.00	
2. Unscheduled	\$10,000.00	
3. Streetlights	\$3,000.00	
Service Costs		
4. Electrical	\$7,150.00	
5. Water	\$4,750.00	
Current Year Improvement Projects		
6. Paint light poles/Tree pruning	\$45,000.00	
	<i>Subtotal of Item 6</i>	\$45,000.00
Subtotal		\$82,400.00
Incidental Costs		
7. Professional Services (Engineer's Report and IP)	\$750.00	
8. Contract Services (all other contracts and services)	\$0.00	
9. Publications/Mailings/Communications	\$0.00	
10. Staff	\$4,690.00	
11. Overhead	\$1,305.00	
12. County Auditor Fee	\$198.83	
Subtotal		\$6,943.83
Total Improvement Costs		\$89,343.83
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$121.18	
Single Family Equivalent Benefit Units	337	
Total Assessment		\$40,837.66
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$3,000.00	
Long-Term Installment Plan (previously collected)	\$125,400.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$2,400.00	
Total Installment Costs		\$130,800.00
District Balance		
Total Assessment	\$40,837.66	
Total Improvement Costs	(\$89,343.83)	
Subtotal	(\$48,506.17)	
Total Available Funds	\$134,252.89	
Total Funds	\$85,746.72	
Total Installment Cost	(\$130,800.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$45,053.28)	
District Balance (surplus is +; deficit is ())		(\$45,053.28)
Net Assessment Calculation		
Assessment	\$40,837.66	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$45,053.28	
Net Assessment		\$85,890.94
Allocated Net Assessment to Property		
Net Assessment	\$85,890.94	
Single Family Equivalent Benefit Units	337	
Allocated Net Assessment to Property		\$254.87
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		(\$254.87)
Allocated Net Assessment to Property		\$121.18
Per Parcel Surplus (+) or Deficit (-)		(\$133.69)

*Funds from Fund Balance

Los Cerros – Installment Summary

District:	Los Cerros							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$101,218							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2¹	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Ladder fuel work								\$10,000
Tree replacement								\$10,000
year 3 pruning								\$10,000
year 4 pruning								\$10,000
year 5 pruning								\$10,000
Paint street light poles								\$27,300
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$77,300
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Install flow package and master valve								\$30,000
Upgrade irrigation controllers								\$35,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$65,000

City of Folsom Natoma Station Landscaping and Lighting District Fund 207 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	(\$251,590.48)	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$77,693.55)	
Available Funds		(\$329,284.03)
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$95,000.00	
2. Unscheduled	\$12,500.00	
3. Streetlights	\$12,000.00	
4. Irrigation	\$17,500.00	
Service Costs		
5. Electrical	\$18,500.00	
6. Water	\$45,000.00	
Current Year Improvement Projects		
7. No planned projects	\$0.00	
Subtotal	Subtotal of Item 7 \$0.00	\$200,500.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$32,602.00	
12. Overhead	\$5,900.00	
13. County Auditor Fee	\$1,119.37	
Subtotal		\$40,371.37
Total Improvement Costs		\$240,871.37
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$91.70	
Single Family Equivalent Benefit Units	1,897.23	
Total Assessment		\$173,976.36
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$30,000.00	
Long-Term Installment Plan (previously collected)	\$98,020.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$800.00	
Total Installment Costs		\$128,820.00
District Balance		
Total Assessment	\$173,976.36	
Total Improvement Costs	(\$240,871.37)	
Subtotal	(\$66,895.01)	
Total Available Funds	(\$247,754.03)	
Total Funds	(\$314,649.04)	
Total Installment Cost	(\$128,820.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$443,469.04)	
District Balance (surplus is +; deficit is (-))		(\$443,469.04)
Net Assessment Calculation		
Assessment	\$173,976.36	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$443,469.04	
Net Assessment	\$617,445.40	
Allocated Net Assessment to Property		
Net Assessment	\$617,445.40	
Single Family Equivalent Benefit Units	1897	
Allocated Net Assessment to Property	\$325.45	
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		(\$325.45)
Allocated Assessment to Property		\$91.70
Per Parcel Surplus (+) or Deficit (-)		(\$233.75)
<small>*Funds from fund balance</small>		

Natoma Station - Installment Summary

District:	Natoma Station							
Fiscal Year:	2022-23							
Fund Balance (2022)	(\$246,857)							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Ladder fule work								\$10,000
Light pole replacement								\$25,000
Shrub & tree replacement and concrete work on turnpike								\$40,000
Iron Point Rd shrub & tree replacement								\$40,000
Bigfood mini park tree replacement								\$10,000
Tree replacement/wall damage on back diamond								\$10,000
Shrub/Tree replacement on Blue Ravine								\$40,000
year 1 tree pruning								\$25,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$200,000
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Tree & landscape improvements (or replacements)								\$160,000
Wetland area improvement								\$30,000
Wall Repair/Painting (7800 feet)								\$234,000
Mini Park-replanting/bark (2 parks at 1/2 acre)								\$60,000
Road Paver replacement								\$40,000
Signage Repair Replacement								\$23,000
Sidewalk repair								\$80,000
Irrigation upgrades								\$56,000
Art repair								\$100,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$783,000

City of Folsom Natoma Valley Landscaping and Lighting District Fund 232 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$219,829.55	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$30,212.26)	
Available Funds		\$189,617.29
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$32,500.00	
2. Unscheduled	\$7,500.00	
3. Streetlights	\$375.00	
4. Irrigation	\$1,000.00	
Service Costs		
5. Electrical	\$1,500.00	
6. Water	\$3,750.00	
Current Year Improvement Projects		
7. No planned projects	\$0.00	
Subtotal	\$0.00	\$46,625.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$7,436.00	
12. Overhead	\$1,687.00	
13. County Auditor Fee	\$46.61	
Subtotal		\$9,919.61
Total Improvement Costs		\$56,544.61
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$856.37	
Single Family Equivalent Benefit Units	79	
Total Assessment		\$67,653.23
Installment Costs (see installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$0.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$0.00
District Balance		
Total Assessment	\$67,653.23	
Total Improvement Costs	(\$56,544.61)	
Subtotal	\$11,108.62	
Total Available Funds	\$189,617.29	
Total Funds	\$200,725.91	
Total Installment Cost	\$0.00	
Contributions from other sources	\$0.00	
Net Balance	\$200,725.91	
District Balance (surplus is +; deficit is -)		\$200,725.91
Net Assessment Calculation		
Assessment	\$67,653.23	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$200,725.91)	
Net Assessment	(\$133,072.68)	
Allocated Net Assessment to Property		
Net Assessment	(\$133,072.68)	
Single Family Equivalent Benefit Units	79	
Allocated Net Assessment to Property	(\$1,684.46)	
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	\$1,684.46	
Allocated Assessment to Property	\$856.37	
Per Parcel Surplus (+) or Deficit (-)	\$2,540.83	
*Funds from fund balance		

Natoma Valley - Installment Summary

District:	Natoma Valley								
Fiscal Year:	2022-23								
Fund Balance (2022)	\$238,959								
Short Term Installment Summary									
									Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5		Total
	Installment	Collections	2018	2019	2020	2021	2022		Required
year 3 pruning									\$5,500
year 4 pruning									\$5,500
year 5 pruning									\$5,500
Interior landscape improvements									\$5,500
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$22,000
Long Term Installment Summary									
									Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30		Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047		Required
Wall Repair/Replacement	\$5,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$50,000
Totals:	\$5,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$50,000

City of Folsom Prospect Ridge Landscaping and Lighting District Fund 285 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$14,690.66	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$14,022.80)	
Available Funds		<u>\$667.86</u>
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$7,500.00	
2. Unscheduled	\$8,300.00	
3. Streetlights	\$500.00	
4. Irrigation	\$600.00	
Service Costs		
5. Electrical	\$375.00	
6. Water	\$2,500.00	
Current Year Improvement Projects		
7. No Planned Projects	\$0.00	
Subtotal	<i>Subtotal of Item 6</i>	<u>\$0.00</u>
		<u>\$19,775.00</u>
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$5,219.00	
12. Overhead	\$219.00	
13. County Auditor Fee	\$20.65	
Subtotal		<u>\$6,208.65</u>
Total Improvement Costs		<u>\$25,983.65</u>
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$1,173.86	
Single Family Equivalent Benefit Units	26.75	
Total Assessment		<u>\$31,400.76</u>
Installment Costs (see installment plan and summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$0.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		<u>\$0.00</u>
District Balance		
Total Assessment	\$31,400.76	
Total Improvement Costs	(\$25,983.65)	
Subtotal	<u>\$5,417.11</u>	
Total Available Funds	<u>\$667.86</u>	
Total Funds	\$6,084.96	
Total Installment Cost	\$0.00	
Contributions from other sources	\$0.00	
Net Balance	<u>\$6,084.96</u>	
District Balance (surplus is +; deficit is !)		<u>\$6,084.96</u>
Net Assessment Calculation		
Assessment	\$31,400.76	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$6,084.96)	
Net Assessment	<u>\$25,315.79</u>	
Allocated Net Assessment to Property		
Net Assessment	\$25,315.79	
Single Family Equivalent Benefit Units	27	
Allocated Net Assessment to Property		<u>\$946.38</u>
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	(\$946.38)	
Allocated Assessment to Property	<u>\$1,173.86</u>	
Per Parcel Surplus (+) or Deficit (-)		<u>\$227.48</u>

*Funds from Bond balance

Prospect Ridge - Installment Summary

District:	Prospect Ridge							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$23,213							
Short Term Installment Summary								
								Approx.
Project	Yearly Installment	Prior Years Collections	Year 1 2018	Year 2 2019	Year 3 2020	Year 4 2021	Year 5 2022	Total Required
year 1 pruning								
year 2 pruning								
year 3 pruning								
year 4 pruning								
year 5 pruning								
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Long Term Installment Summary								
								Approx.
Project	Yearly Installment	Prior Years Collections	Year 1 2018	Years 2-5 2019-2022	Years 5-10 2022-2027	Years 10-20 2027-2037	Years 20-30 2037-2047	Total Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom Prairie Oaks Ranch Landscaping and Lighting District Fund 236 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	(\$458,829.72)	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$87,630.83)	
Available Funds		(\$546,460.55)
Improvement Costs		
<u>General Maintenance Costs</u>		
1. Scheduled	\$0.00	
2. Unscheduled	\$0.00	
3. Streetlights	\$10,000.00	
4. Irrigation-Parts	\$0.00	
<u>Service Costs</u>		
5. Electrical	\$10,000.00	
6. Water	\$60,000.00	
<u>Current Year Improvement Projects</u>		
7. No planned projects	\$0.00	
	Subtotal of Item 7	\$0.00
Subtotal		\$80,000.00
<u>Incidental Costs</u>		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$59,484.00	
12. Overhead	\$6,818.00	
13. County Auditor Fee	\$541.99	
Subtotal		\$67,593.99
Total Improvement Costs		\$147,593.99
<u>Assessment to Property (Current)</u>		
Assessment per Single Family Equivalent	\$213.61	
Single Family Equivalent Benefit Units	918.63	
Total Assessment		\$196,228.55
<u>Installment Costs (see Installment Plan and Summary next page)</u>		
Short-Term installment Plan (previously collected)	\$117,200.00	
Long-Term installment Plan (previously collected)	\$97,711.00	
Short-Term installment Plan (collected this year)	\$0.00	
Long-Term installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$214,911.00
<u>District Balance</u>		
Total Assessment	\$196,228.55	
Total Improvement Costs	(\$147,593.99)	
Subtotal	\$48,634.56	
Total Available Funds	(\$546,460.55)	
Total Funds	(\$497,825.99)	
Total Installment Cost	(\$214,911.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$712,736.99)	
District Balance (surplus is +; deficit is !)		(\$712,736.99)
<u>Net Assessment Calculation</u>		
Assessment	\$196,228.55	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$712,736.99	
Net Assessment		\$908,965.55
<u>Allocated Net Assessment to Property</u>		
Net Assessment	\$908,965.55	
Single Family Equivalent Benefit Units	919	
Allocated Net Assessment to Property		\$989.48
<u>Comparison of Net Assessment and Assessment</u>		
Allocated Net Assessment to Property		(\$989.48)
Allocated Assessment to Property		\$213.61
Per Parcel Surplus (+) or Deficit (-)		(\$775.87)

*Funds from Fund Balance

Prairie Oaks Ranch - Installment Summary

District:	Prairie Oaks Ranch							
Fiscal Year:	2022-23							
Fund Balance (2022)	(\$451,538)							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom Prairie Oaks Ranch No. 2 Landscaping and Lighting District Fund 215 2022-23		
		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$159,647.18	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$128,777.66)	
Available Funds		<u>\$30,869.52</u>
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$135,000.00	
2. Unscheduled	\$20,000.00	
3. Streetlights	\$0.00	
4. Irrigation-Parts	\$20,000.00	
Service Costs		
5. Electrical	\$0.00	
6. Water	\$0.00	
Current Year Improvement Projects		
7. Tree replacement (replace empty areas)		
8. LED landscape lights		
9. Landscape replacement		
10. Fence replacement		
11. Ladder fuel		
12. Repair damage walls (stucco half walls)		
Subtotal of Item 7-12	\$100,000.00	
Subtotal		<u>\$275,000.00</u>
Incidental Costs		
13. Professional Services (Engineer's Report and IP)	\$750.00	
14. Contract Services (all other contracts and services)	\$0.00	
15. Publications/Mailings/Communications	\$0.00	
16. Staff	\$0.00	
17. Overhead	\$5,200.00	
18. County Auditor Fee	\$0.00	
Subtotal		<u>\$5,950.00</u>
Total Improvement Costs		<u>\$280,950.00</u>
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$313.91	
Single Family Equivalent Benefit Units	918.63	
Total Assessment		<u>\$288,367.14</u>
Installment Costs (see installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$0.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		<u>\$0.00</u>
District Balance		
Total Assessment	\$288,367.14	
Total Improvement Costs	(\$280,950.00)	
Subtotal	\$7,417.14	
Total Available Funds	\$30,869.52	
Total Funds	\$38,286.67	
Total Installment Cost	\$0.00	
Contributions from other sources	\$0.00	
Net Balance	\$38,286.67	
District Balance (surplus is +; deficit is !)		<u>\$38,286.67</u>
Net Assessment Calculation		
Assessment	\$288,367.14	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$38,286.67)	
Net Assessment		<u>\$250,080.48</u>
Allocated Net Assessment to Property		
Net Assessment	\$250,080.48	
Single Family Equivalent Benefit Units	919	
Allocated Net Assessment to Property		<u>\$272.23</u>
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		(\$272.23)
Assessment to Property		<u>\$313.91</u>
Per Parcel Surplus (+) or Deficit (-)		<u>\$41.68</u>

Prairie Oaks Ranch No. 2 - Installment Summary

District:	Prairie Oaks Ranch No. 2							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$159,647							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
year 1 pruning								\$25,000
year 2 pruning								\$25,000
year 3 pruning								\$25,000
year 4 pruning								\$25,000
year 5 pruning								\$25,000
Tree replacment (replace empty areas)								\$15,000
LED landscape lights								\$25,000
Landscape replacement								\$25,000
Fence replacement								\$20,000
Ladder fuel								\$75,000
Repair damage walls (stucco half walls)								\$8,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$210,000
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Landscape replacement on Blue Ravine								\$250,000
Landscape Replacement on Riley								\$250,000
Landscape Replacement on Prairie City								\$150,000
Landscape replacment on Iron Point								\$100,000
Fence replacement								\$100,000
Post and cable replacement								\$75,000
Repair damaged walls								\$40,000
Totals:								\$965,000

City of Folsom		Total Budget
The Residences At ARC (Oak Ave) Landscaping and Lighting District		
Fund 271		
2022-23		
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$64,044.30	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$4,074.28)	
Available Funds		\$59,970.02
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$12,000.00	
2. Unscheduled	\$3,000.00	
3. Streetlights	\$750.00	
4. Irrigation Parts	\$1,200.00	
Service Costs		
5. Electrical	\$700.00	
6. Water	\$3,300.00	
Current Year Improvement Projects		
7. Landscape replacement	\$4,000.00	
Subtotal	<i>Subtotal of Item 8</i>	\$24,950.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$375.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$4,012.00	
12. Overhead	\$915.00	
13. County Auditor Fee	\$10.03	
Subtotal		\$5,312.03
Total Improvement Costs		\$30,262.03
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$536.67	
Single Family Equivalent Benefit Units	17	
Res 1 - Assessment		\$9,123.39
Total Assessment		\$9,123.39
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$48,000.00	
Long-Term Installment Plan (previously collected)	\$14,000.00	
Short-Term Installment Plan (collected this year)	\$2,000.00	
Long-Term Installment Plan (collected this year)	\$1,000.00	
Total Installment Costs		\$65,000.00
District Balance		
Total Assessment	\$9,123.39	
Total Improvement Costs	(\$30,262.03)	
Subtotal	(\$21,138.64)	
Total Available Funds	\$59,970.02	
Total Funds	\$38,831.38	
Total Installment Cost	(\$65,000.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$26,168.62)	
District Balance (surplus is +; deficit is {})		(\$26,168.62)
Net Assessment Calculation		
Assessment	\$9,123.39	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$26,168.62	
Net Assessment		\$35,292.01
Allocated Net Assessment to Property		
The Residences at ARC I		
Net Assessment	\$35,292.01	
Single Family Equivalent Benefit Units	17	
Allocated Net Assessment to Property		\$2,076.00
Comparison of Net Assessment and Assessment		
The Residences at ARC I		
Allocated Net Assessment to Property		(\$2,076.00)
Allocated Assessment to Property		\$336.67
Per Parcel Surplus (+) or Deficit (-)		(\$1,539.33)

Results from fund balance

The Residences at ARC - Installment Summary

District:	The Residences at ARC							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$67,680							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
The Residences at ARC								
Landscape replacement								\$4,500
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$4,500
Long Term Installment Summary								
								Total
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
The Residences at ARC								
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom The Residences At ARC II (Folsom-Auburn Blvd) Landscaping and Lighting District Fund 271 2022-23		
		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$64,044.30	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$5,224.80)	
Available Funds		\$58,819.50
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$12,000.00	
2. Unscheduled	\$3,000.00	
3. Streetlights	\$750.00	
4. Irrigation Parts	\$1,200.00	
Service Costs		
5. Electrical	\$700.00	
6. Water	\$3,300.00	
Current Year Improvement Projects		
7. Landscape replacement	\$4,000.00	
Subtotal	<i>Subtotal of Item 8</i> \$4,000.00	\$24,950.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$375.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$4,012.00	
12. Overhead	\$915.00	
13. County Auditor Fee	\$5.90	
Subtotal		\$5,307.90
Total Improvement Costs		\$30,257.90
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$1,169.97	
Single Family Equivalent Benefit Units	10	
Res 2 - Assessment		\$11,699.70
Total Assessment		\$11,699.70
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$37,000.00	
Long-Term Installment Plan (previously collected)	\$14,000.00	
Short-Term Installment Plan (collected this year)	\$2,000.00	
Long-Term Installment Plan (collected this year)	\$1,000.00	
Total Installment Costs		\$54,000.00
District Balance		
Total Assessment	\$11,699.70	
Total Improvement Costs	(\$30,257.90)	
Subtotal	(\$18,558.20)	
Total Available Funds	\$58,819.50	
Total Funds	\$40,261.30	
Total Installment Cost	(\$54,000.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$13,738.70)	
District Balance (surplus is +; deficit is {})		(\$13,738.70)
Net Assessment Calculation		
Assessment	\$11,699.70	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$13,738.70	
Net Assessment		\$25,438.40
Allocated Net Assessment to Property		
The Residences at ARC II		
Net Assessment	\$25,438.40	
Single Family Equivalent Benefit Units	10	
Allocated Net Assessment to Property		\$2,543.84
Comparison of Net Assessment and Assessment		
The Residences at ARC II		
Allocated Net Assessment to Property	(\$2,543.84)	
Allocated Assessment to Property	\$1,169.97	
Per Parcel Surplus (+) or Deficit (-)		(\$1,373.87)

*Funds from Fund Balance

The Residences at ARC II - Installment Summary

District:	The Residences at ARC II							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$67,680							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
The Residences at ARCII								
Landscape replacement								\$4,500
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$4,500
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
The Residences at ARC II								
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom Silverbrook Landscaping and Lighting District Fund 237 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$102,479.31	
Estimated Reserve to finance approx. first 6 months of 22-23	\$0.00	
Available Funds		\$102,479.31
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$3,155.00	
2. Unscheduled*	\$1,500.00	
3. Streetlights*	\$1,200.00	
4. Irrigation Parts	\$150.00	
Service Costs		
5. Electrical*	\$500.00	
6. Water*	\$500.00	
Current Year Improvement Projects		
6. No planned projects	\$0.00	
Subtotal of Item 7	\$0.00	
Subtotal		\$7,005.00
Incidental Costs		
7. Professional Services (Engineer's Report and IP)	\$750.00	
8. Contract Services (all other contracts and services)*	\$0.00	
9. Publications/Mailings/Communications	\$100.00	
10. Staff	\$1,144.00	
11. Overhead	\$447.00	
12. County Auditor Fee	\$67.17	
Subtotal		\$2,508.17
Total Improvement Costs		\$9,513.17
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$0.00	
Single Family Equivalent Benefit Units	113.84	
Total Assessment		\$0.00
Installment Plan Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$57,000.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$57,000.00
District Balance		
Total Assessment to property	\$0.00	
Total Improvement Costs	(\$9,513.17)	
Subtotal	(\$9,513.17)	
Total Available Funds	\$102,479.31	
Total Funds	\$92,966.14	
Total Installment Costs	(\$57,000.00)	
Contributions from other sources	\$0.00	
Net Balance	\$35,966.14	
District Balance (surplus is +; deficit is {})		\$35,966.14
Net Assessment Calculation		
Assessment	\$0.00	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$35,966.14)	
Net Assessment		(\$35,966.14)
Allocated Net Assessment to Property		
Net Assessment	(\$35,966.14)	
Single Family Equivalent Benefit Units	114	
Allocated Net Assessment to Property		(\$315.94)
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	\$315.94	
Allocated Assessment to Property	\$0.00	
Per Parcel Surplus (+) or Deficit (-)		\$315.94

*These are estimated costs based on prior year's costs and/or anticipated expenses upcoming year.

Assessments will not be levied for 2022-23.

Silverbrook - Installment Summary

District:	Silverbrook							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$102,449							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Tree replacement								\$7,500
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$7,500
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Median Relandscaping	\$5,000	\$57,000	\$0	\$0	\$0	\$0	\$0	\$150,000
Totals:	\$5,000	\$57,000	\$0	\$0	\$0	\$0	\$0	\$150,000

City of Folsom Steeplechase Landscaping and Lighting District Fund 251 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$87,508.24	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$10,844.06)	
Available Funds		\$76,664.18
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$11,000.00	
2. Unscheduled	\$7,500.00	
3. Streetlights	\$1,250.00	
4. Irrigation Parts	\$600.00	
Service Costs		
5. Electrical	\$2,100.00	
6. Water	\$3,750.00	
Current Year Improvement Projects		
7. Fence replacement/Tree work	\$13,000.00	
	<i>Subtotal of Item 7</i>	\$13,000.00
Subtotal		\$39,200.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$4,019.00	
12. Overhead	\$841.00	
13. County Auditor Fee	\$883.00	
Subtotal		\$6,493.00
Total Improvement Costs		\$45,693.00
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$157.68	
Single Family Equivalent Benefit Units	154	
Total Assessment		\$24,282.72
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$73,376.00	
Long-Term Installment Plan (previously collected)	\$85,500.00	
Short-Term Installment Plan (collected this year)	\$5,000.00	
Long-Term Installment Plan (collected this year)	\$6,500.00	
Total Installment Costs		\$170,376.00
District Balance		
Total Assessment	\$24,282.72	
Total Improvement Costs	(\$45,693.00)	
Subtotal	(\$21,410.28)	
Total Available Funds	\$76,664.18	
Total Funds	\$55,253.90	
Total Installment Cost	(\$170,376.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$115,122.10)	
District Balance (surplus is +; deficit is !)		(\$115,122.10)
Net Assessment Calculation		
Assessment	\$24,282.72	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$115,122.10	
Net Assessment		\$139,404.82
Allocated Net Assessment to Property		
Net Assessment	\$139,404.82	
Single Family Equivalent Benefit Units	154	
Allocated Net Assessment to Property		\$905.23
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		(\$905.23)
Assessment		\$157.68
Per Parcel Surplus (+) or Deficit (-)		(\$747.55)

*Funds from Fund Balance

Steeplechase - Installment Summary

District:	Steeplechase								
Fiscal Year:	2022-23								
Fund Balance (2022)	\$83,778								
Short Term Installment Summary									
									Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total	
	Installment	Collections	2018	2019	2020	2021	2022	Required	
fence replacments near park									\$8,000
year 2 tree pruning									\$5,000
year 3 tree pruning									\$5,000
year 4 tree pruning									\$5,000
year 5 tree pruning									\$5,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$28,000
Long Term Installment Summary									
									Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total	
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required	
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom Sierra Estates Landscaping and Lighting District Fund 231 2022-23		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$22,289.80	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$4,060.26)	
Available Funds		<u>\$18,229.54</u>
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$3,000.00	
2. Unscheduled	\$1,500.00	
3. Streetlights	\$250.00	
4. Irrigation Parts	\$350.00	
Service Costs		
5. Electrical	\$325.00	
6. Water	\$850.00	
Current Year Improvement Projects		
7. Tree replacement	\$7,500.00	
Subtotal	<i>Subtotal of Item 7</i>	<u>\$7,500.00</u>
		<u>\$13,775.00</u>
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$1,144.00	
12. Overhead	\$219.00	
13. County Auditor Fee	\$14.75	
Subtotal		<u>\$2,127.75</u>
Total Improvement Costs		<u>\$15,902.75</u>
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$363.68	
Single Family Equivalent Benefit Units	25	
Total Assessment		<u>\$9,092.00</u>
Installment Costs (see installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$8,858.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$1,100.00	
Total Installment Costs		<u>\$9,958.00</u>
District Balance		
Total Assessment	\$9,092.00	
Total Improvement Costs	(\$15,902.75)	
Subtotal	(\$6,810.75)	
Total Available Funds	\$18,229.54	
Total Funds	\$11,418.79	
Total Installment Cost	(\$9,958.00)	
Contributions from other sources	\$0.00	
Net Balance	\$1,460.79	
District Balance (surplus is +; deficit is (-))		<u>\$1,460.79</u>
Net Assessment Calculation		
Assessment	\$9,092.00	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$1,460.79)	
Net Assessment		<u>\$7,631.21</u>
Allocated Net Assessment to Property		
Net Assessment	\$7,631.21	
Single Family Equivalent Benefit Units	25	
Allocated Net Assessment to Property		<u>\$305.25</u>
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		(\$305.25)
Allocated Assessment to Property		<u>\$363.68</u>
Per Parcel Surplus (+) or Deficit (-)		<u>\$58.43</u>

*Funds from Fund Balance

Sierra Estates - Installment Summary

District:	Sierra Estates							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$24,919							
Short Term Installment Summary								
Project	Yearly Installment	Prior Years Collections	Year 1 2018	Year 2 2019	Year 3 2020	Year 4 2021	Year 5 2022	Approx. Total Required
Tree replacement								\$7,500
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$7,500
Long Term Installment Summary								
Project	Yearly Installment	Prior Years Collections	Year 1 2018	Years 2-5 2019-2022	Years 5-10 2022-2027	Years 10-20 2027-2037	Years 20-30 2037-2047	Approx. Total Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom Willow Creek Estates East Landscaping and Lighting District Fund 249 2022-23		
		Total Budget
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	(\$156,417.79)	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$26,820.78)	
Available Funds		(\$183,238.57)
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$0.00	
2. Unscheduled	\$0.00	
3. Streetlights	\$0.00	
4. Irrigation	\$0.00	
Service Costs		
5. Electrical	\$11,000.00	
6. Water	\$15,000.00	
Current Year Improvement Projects		
7. No planned projects	\$0.00	
Subtotal	Subtotal of Item 7 \$0.00	\$26,000.00
Incidental Costs		
8. Professional Services (Engineer's Report and IP)	\$750.00	
9. Contract Services (all other contracts and services)	\$0.00	
10. Publications/Mailings/Communications	\$0.00	
11. Staff	\$0.00	
12. Overhead	\$2,440.00	
13. County Auditor Fee	\$0.00	
Subtotal		\$3,190.00
Total Improvement Costs		\$29,190.00
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$80.40	
Single Family Equivalent Benefit Units	747	
Total Assessment		\$60,058.80
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$34,000.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$34,000.00
District Balance		
Total Assessment	\$60,058.80	
Total Improvement Costs	(\$29,190.00)	
Subtotal	\$30,868.80	
Total Available Funds	(\$137,898.57)	
Total Funds	(\$107,029.77)	
Total Installment Cost	(\$34,000.00)	
Contributions from other sources	\$0.00	
Net Balance	(\$141,029.77)	
District Balance (surplus is +; deficit is -)		(\$141,029.77)
Net Assessment Calculation		
Assessment	\$60,058.80	
Surplus or Deficit (surplus is subtracted; deficit is added)	\$141,029.77	
Net Assessment		\$201,088.57
Allocated Net Assessment to Property		
Net Assessment	\$201,088.57	
Single Family Equivalent Benefit Units	747	
Allocated Net Assessment to Property		\$269.19
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	(\$269.19)	
Allocated Assessment to Property	\$80.40	
Per Parcel Surplus (+) or Deficit (-)		(\$188.79)
*Funds from fund balance		

Willow Creek Estates East – Installment Summary

District:	Willow Creek Estates East							
Fiscal Year:	2022-23							
Fund Balance (2022)	(\$153,767)							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	Installment	Collections	2018	2019	2020	2021	2022	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

City of Folsom		Total Budget
Willow Creek Estates East No. 2 Landscaping and Lighting District		
Fund 284		
2022-23		
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$123,634.47	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$34,273.77)	
Available Funds		\$89,360.70
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$40,000.00	
2. Unscheduled	\$20,000.00	
3. Streetlights	\$15,000.00	
Service Costs		
4. Electrical	\$0.00	
5. Water	\$0.00	
Current Year Improvement Projects		
6. Pruning, planting, flow sensor installation	\$50,000.00	
Subtotal of Item 7	\$50,000.00	
Subtotal		\$125,000.00
Incidental Costs		
7. Professional Services (Engineer's Report and IP)	\$750.00	
8. Contract Services (all other contracts and services)	\$0.00	
9. Publications/Mailings/Communications	\$0.00	
10. Staff	\$13,789.00	
11. Overhead	\$971.00	
12. County Auditor Fee	\$440.00	
Subtotal		\$15,950.00
Total Improvement Costs		\$140,950.00
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$103.51	
Single Family Equivalent Benefit Units	741.46	
Total Assessment		\$76,748.01
Installment Costs (see installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$0.00	
Long-Term Installment Plan (previously collected)	\$0.00	
Short-Term Installment Plan (collected this year)	\$0.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$0.00
District Balance		
Total Assessment	\$76,748.01	
Total Improvement Costs	(\$140,950.00)	
Subtotal	(\$64,201.99)	
Total Available Funds	\$89,360.70	
Total Funds	\$25,158.71	
Total Installment Cost	\$0.00	
Contributions from other sources	\$0.00	
Net Balance	\$25,158.71	
District Balance (surplus is +; deficit is !)		\$25,158.71
Net Assessment Calculation		
Assessment	\$76,748.01	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$25,158.71)	
Net Assessment		\$51,589.30
Allocated Net Assessment to Property		
Net Assessment	\$51,589.30	
Single Family Equivalent Benefit Units	741	
Allocated Net Assessment to Property		\$69.58
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property		(\$69.58)
Allocated Assessment to Property		\$103.51
Per Parcel Surplus (+) or Deficit (-)		\$33.93

*Funds from fund balance

Willow Creek Estates East No. 2 - Installment Summary

District:		Willow Creek Estates East No 2							
Fiscal Year:		2022-23							
Fund Balance (2022)		\$155,461							
Short Term Installment Summary									
								Approx.	
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total	
	Installment	Collections	2018	2019	2020	2021	2022	Required	
2 flow packages & Master Valve Install								\$7,500	
Light pole replacement (3)								\$20,000	
year 3 tree pruning								\$10,000	
year 4 tree pruning								\$10,000	
year 5 tree pruning								\$10,000	
Oleander replacement on blue ravine frontage									
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$50,000	
Long Term Installment Summary									
								Approx.	
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total	
	Installment	Collections	2018	2019	2020	2021	2022	Required	
Landscape replacement on Oak Avenue								\$30,000	
Landscape replacement on Blue Ravine								\$60,000	
Irrigation controller upgrade (4 controllers)								\$50,000	
Tree and landscape improvement / replacement								\$100,000	
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$240,000	

City of Folsom Willow Creek Estates South Landscaping and Lighting District Fund 252 2022-23			Total Budget
Fund Balance Calculation			
Starting Fund Balance (as of April 2022)	\$653,714.29		
Estimated Reserve to finance approx. first 6 months of 22-23	(\$71,738.92)		
Available Funds			\$581,975.37
Improvement Costs			
General Maintenance Costs			
1. Scheduled	\$17,500.00		
2. Unscheduled	\$20,000.00		
3. Streetlights	\$12,500.00		
4. Irrigation	\$12,500.00		
Service Costs			
5. Electrical	\$35,000.00		
6. Water	\$37,500.00		
Current Year Improvement Projects			
7. Interior sign renovation	\$120,000.00		
	<i>Subtotal of Item 8</i>	\$120,000.00	
Subtotal			\$255,000.00
Incidental Costs			
8. Professional Services (Engineer's Report and IP)	\$750.00		
9. Contract Services (all other contracts and services)	\$0.00		
10. Publications/Mailings/Communications	\$0.00		
11. Staff	\$5,308.00		
12. Overhead	\$5,597.00		
13. County Auditor Fee	\$862.57		
Subtotal			\$12,517.57
Total Improvement Costs			\$267,517.57
Assessment to Property (Current)			
Assessment per Single Family Equivalent	\$109.88		
Single Family Equivalent Benefit Units	1,461.98		
Total Assessment			\$160,642.36
Installation Costs (see Installation Plan and Summary next page)			
Short-Term Installment Plan (previously collected)	\$0.00		
Long-Term Installment Plan (previously collected)	\$394,000.00		
Short-Term Installment Plan (collected this year)	\$0.00		
Long-Term Installment Plan (collected this year)	\$15,000.00		
Total Installation Costs			\$409,000.00
District Balance			
Total Assessment	\$160,642.36		
Total Improvement Costs	(\$267,517.57)		
Subtotal	(\$106,875.21)		
Total Available Funds	\$581,975.37		
Total Funds	\$475,100.17		
Total Installation Cost	(\$409,000.00)		
Contributions from other sources	\$0.00		
Net Balance	\$66,100.17		
District Balance (surplus is +; deficit is -)			\$410,965.96
Net Assessment Calculation			
Assessment	\$160,642.36		
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$410,965.96)		
Net Assessment			(\$250,323.60)
Allocated Net Assessment to Property			
Net Assessment	(\$250,323.60)		
Single Family Equivalent Benefit Units	1462		
Allocated Net Assessment to Property			(\$171.22)
Comparison of Net Assessment and Assessment			
Allocated Net Assessment to Property	\$171.22		
Allocated Assessment to Property	\$109.88		
Per Parcel Surplus (+) or Deficit (-)			\$281.10

*Funds from fund balance

Willow Creek South - Installment Summary

District:	Willow Creek Estates South							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$686,214							
Short Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Year 2	Year 3	Year 4	Year 5	Total
	installment	Collections	2018	2019	2020	2021	2022	Required
year 3 tree pruning								\$20,000
year 4 tree pruning								\$20,000
year 5 tree pruning								\$20,000
new planting around replaced signs								\$15,000
interior sign replacement								\$85,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$160,000
Long Term Installment Summary								
								Approx.
Project	Yearly	Prior Years	Year 1	Years 2-5	Years 5-10	Years 10-20	Years 20-30	Total
	Installment	Collections	2018	2019-2022	2022-2027	2027-2037	2037-2047	Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

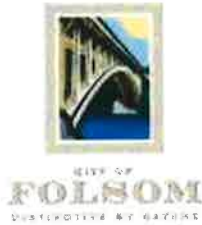
City of Folsom		Total Budget
Willow Springs Landscaping and Lighting District		
Fund 260 - Streetlights Only		
2022-23		
Fund Balance Calculation		
Starting Fund Balance (as of April 2022)	\$48,542.05	
Estimated Reserve to finance approx. first 6 months of 22-23	(\$6,496.95)	
Available Funds		\$42,045.10
Improvement Costs		
General Maintenance Costs		
1. Scheduled	\$0.00	
2. Unscheduled	\$0.00	
3. Streetlights	\$7,500.00	
Service Costs		
4. Electrical	\$6,500.00	
5. Water	\$0.00	
Current Year Improvement Projects		
6. No planned projects	\$0.00	
Subtotal of Item 6	\$0.00	
Subtotal		\$14,000.00
Incidental Costs		
7. Professional Services (Engineer's Report and IP)	\$750.00	
8. Contract Services (all other contracts and services)	\$0.00	
9. Publications/Mailings/Communications	\$0.00	
10. Staff	\$0.00	
11. Overhead	\$475.00	
12. County Auditor Fee	\$305.03	
Subtotal		\$1,530.03
Total Improvement Costs		\$15,530.03
Assessment to Property (Current)		
Assessment per Single Family Equivalent	\$28.14	
Single Family Equivalent Benefit Units	517	
Total Assessment		\$14,548.38
Installment Costs (see Installment Plan and Summary next page)		
Short-Term Installment Plan (previously collected)	\$12,500.00	
Long-Term Installment Plan (previously collected)	\$0.00	
Short-Term Installment Plan (collected this year)	\$1,000.00	
Long-Term Installment Plan (collected this year)	\$0.00	
Total Installment Costs		\$13,500.00
District Balance		
Total Assessment	\$14,548.38	
Total Improvement Costs	(\$15,530.03)	
Subtotal	(\$981.65)	
Total Available Funds	\$49,260.10	
Total Funds	\$48,278.45	
Total Installment Cost	(\$13,500.00)	
Contributions from other sources	\$0.00	
Net Balance	\$34,778.45	
District Balance (surplus is +; deficit is {})		\$42,682.48
Net Assessment Calculation		
Assessment	\$14,548.38	
Surplus or Deficit (surplus is subtracted; deficit is added)	(\$42,682.48)	
Net Assessment		(\$28,134.10)
Allocated Net Assessment to Property		
Net Assessment	(\$28,134.10)	
Single Family Equivalent Benefit Units	517	
Allocated Net Assessment to Property		(\$54.42)
Comparison of Net Assessment and Assessment		
Allocated Net Assessment to Property	\$54.42	
Allocated Assessment to Property	\$28.14	
Per Parcel Surplus (+) or Deficit (-)		\$82.56
<small>*Funds from fund balance</small>		

Willow Springs - Installment Summary

District:	Willow Springs							
Fiscal Year:	2022-23							
Fund Balance (2022)	\$7,537							
Short Term Installment Summary								
Project	Yearly Installment	Prior Years Collections	Year 1 2018	Year 2 2019	Year 3 2020	Year 4 2021	Year 5 2022	Approx. Total Required
LED retrofits (contempo)								\$15,000
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$15,000
Long Term Installment Summary								
Project	Yearly Installment	Prior Years Collections	Year 1 2018	Years 2-5 2019-2022	Years 5-10 2022-2027	Years 10-20 2027-2037	Years 20-30 2037-2047	Approx. Total Required
Totals:	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

Appendix B – Assessment Roll, FY 2022-23

Reference is hereby made to the Assessment Roll in and for the assessment proceedings on file with the City of Folsom City Clerk, as the Assessment Roll is too voluminous to be bound with this Engineer's Report.



Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10845 – A Resolution Authorizing the City Manager to Execute an Amendment to the Memorandum of Understanding Agreement (Contract No. 174-21 20-062) with the Capital Southeast Connector Joint Powers Authority for the Southeast Connector Segment D3(A) Bike Trail and Appropriation of Funds
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

The Public Works Department recommends that the City Council pass and adopt Resolution No. 10845 - A Resolution Authorizing the City Manager to Execute an Amendment to the Memorandum of Understanding Agreement (Contract No. 174-21 20-062) with the Capital Southeast Connector Joint Powers Authority for the Southeast Connector Segment D3(A) Bike Trail and Appropriation of Funds.

BACKGROUND / ISSUE

The Capital SouthEast Connector is a planned 34-mile limited-access roadway spanning from U.S. 50 at Silva Valley Parkway interchange in El Dorado County to Interstate 5 at the Hood-Franklin Interchange in Elk Grove. The Connector is planned to be constructed in segments as funding and priorities allow. Segment D3, referred to as the “Folsom segment,” includes the length of the Connector that borders the City of Folsom and Sacramento County from Prairie City Road to the El Dorado County Line. Segment E1 is the El Dorado County Segment that Segment D3 ties into.

Segment D3 has been further segmented into two additional segments: D3(A) and D3(B). Segment D3(A) will upgrade existing White Rock Road to a four-lane expressway beginning near the intersection of Prairie City Road and continuing through the intersection of East Bidwell Street. The project entails constructing four lanes between these two major

intersections, including a bridge over Alder Creek. The bridge also serves as a “wildlife crossing,” as contemplated in the South Sacramento Habitat Conservation Plan.

The project is a complete reconstruction that will create a new alignment of White Rock Road adjacent to and immediately south of the existing White Rock Road. The existing White Rock Road will remain open to traffic during construction. Currently under construction, Segment D(3A) is expected to be complete by the Summer of 2022.

The City is currently managing the SouthEast Connector Segment D3(A) on behalf of the Capital SouthEast Connector Joint Powers Authority (JPA) and in collaboration with Sacramento County. The City will also manage the Scott Road Realignment on behalf of the JPA in collaboration with Sacramento County.

During the design process, the Class 1 Bike Trail and Bridge was pulled from the project due to budgetary constraints, with an intent to construct it in a later phase. The project had the ability to add the rough grading for the bike trail and bike/pedestrian bridge through the contingency fund. In order to provide finish grade, base, paving, and shoulder backing an amendment with the JPA is required to provide additional funding.

POLICY / RULE

Execution of Amendments to the Memorandum of Understanding Agreement between Agencies require City Council approval.

ANALYSIS

The Memorandum of Understanding provides the framework for transactions between the City of Folsom and the JPA. Amendment two will allow for the project to complete the bike trail on Segment D3(A) which also completes the Segment D3(A).

The existing Memorandum of Understanding was approved by City Council at the October 27, 2020, Council Meeting. The First Amendment to the Memorandum of Understanding to add the Scott Road Realignment was approved by City Council on October 26, 2021.

FINANCIAL IMPACT

The Capital SouthEast Connector Segment D3(A) is eligible to receive funds from FAST Act funding based on a SACOG Regional Surface Transportation Program Grant and SB-1 Funding. Funding for construction and construction engineering is shown below:

Fund	Fund Type	Amount
FAST Act	Surface Transportation Block Grant to match SB-1 Funding 1 to 1 at \$10,000,000	\$15,000,000
SB1	Competitive	\$10,000,000
Local Transportation Fund (446)	To match Surface Transportation Block Grant of \$15,000,000 at 11.47%	\$1,720,500
Local Transportation Fund (446)	To Cover Balance of Project Funding	\$1,195,147
Local Transportation Fund (446)	SouthEast Connector Joint Powers Authority to provide funding for Pedestrian/Bike Trail through Invoicing	\$400,000
	Project Budget for Construction	\$28,315,396

The FAST Act funding requires a local match of 11.47%. The Transportation Fund (Fund 446) will be utilized to meet the required match of \$1,720,500. The SB1 funding requires a \$100% match, of which \$10,000,000 of the FAST Act funding will be utilized as the match.

The previously approved agreement with Goodfellow Brothers, LLC is for \$22,368,765 with a budgeted contingency of \$2,236,876.50 (10%) for a total contract budget amount of \$24,605,641.50.

The contract costs for construction and construction management have been previously approved by City Council in separate resolutions as follows:

Company	Description	Cost
Goodfellow Brothers, LLC	Construction	\$22,368,765.00
	10% Contingency for Construction	\$2,236,876.50
	Additional Contingency Funded by SouthEast Connector Joint Powers Authority	\$400,000
Salaber Associates, Inc.	Construction Engineering, Inspection and Materials Testing	\$2,992,848.00
Dokken Engineering, Inc.	Design Support and Environmental Services	\$317,157.28
	Project Budget for Construction	\$28,315,646.78

The Contract Change Order for the Pedestrian/Bike Trail will utilize the existing contingency in the amount of \$285,802. \$400,000 which has been appropriated through Amendment 2 of the Memorandum of Understanding with the SouthEast Connector Joint Powers Authority will be appropriated to the project. The total cost for the Pedestrian/Bike Trail is \$685,802 and the additional \$400,00 from the JPA will increase the project budget to \$28,443,000. No additional City of Folsom funding is needed.


ENVIRONMENTAL REVIEW

In 2016 the JPA approved a California Environmental Quality Act (CEQA) Tiered Initial Study with Mitigated Negative Declaration for the Capital Southeast Connector Segment D3/E1 which includes the Scott Road Realignment Project and the Class 1 Bike Trail. The project is not located in an environmentally sensitive area and would not result in potential impacts to the environment, including traffic, noise, air quality, and water quality.

ATTACHMENTS

1. Resolution No. 10845 – A Resolution Authorizing the City Manager to Execute an Amendment to the Memorandum of Understanding Agreement (Contract No. 174-21 20-062) with the Capital Southeast Connector Joint Powers Authority for the Southeast Connector Segment D3(A) Bike Trail and Appropriation of Funds
2. Second Amendment to Memorandum of Understanding Agreement
3. First Amendment to Memorandum of Understanding Agreement-Executed
4. Memorandum of Understanding Agreement-Executed

Submitted,



Mark Rackovan, PUBLIC WORKS DIRECTOR

Attachment 1

RESOLUTION NO. 10845

**A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE AN
AMENDMENT TO THE MEMORANDUM OF UNDERSTANDING AGREEMENT
(CONTRACT NO. 174-21 20-062) WITH THE CAPITAL SOUTHEAST CONNECTOR
JOINT POWERS AUTHORITY FOR THE SOUTHEAST CONNECTOR SEGMENT
D3(A) AND APPROPRIATION OF FUNDS**

WHEREAS, the Capital SouthEast Connector Project is the Sacramento region's largest single transportation project, ultimately extending over 34 miles from Elk Grove to El Dorado County; and

WHEREAS, the City of Folsom is a Member Jurisdiction of the Capital SouthEast Connector Joint Powers Authority and will implement the Project; and

WHEREAS, the Capital SouthEast Connector Project has, to date, been funded primarily through a Sacramento County sales-tax measure approved in 2004 by 75 percent of voters; and

WHEREAS, the Capital SouthEast Connector Project has developed a program development budget that requires the use of Federal and State funds to advance the project towards timely construction; and

WHEREAS, the City of Folsom desires to construct the Capital SouthEast Connector Segment D3(A) Pedestrian/Bike Trail; and

WHEREAS, the Capital SouthEast Connector Joint Powers Authority has funding to assist the construction of the Capital SouthEast Connector Segment D3(A) Project Pedestrian/Bike Trail; and

WHEREAS, the balance needed of \$285,801.96 is within the project budget contingency to fully fund the Pedestrian/Bike Trail; and

WHEREAS, the City of Folsom and Capital SouthEast Connector Joint Powers Authority desires to amend the existing Memorandum of Understanding to add the Pedestrian/Bike Trail; and

WHEREAS, the Second Amendment to the Memorandum of Understanding will identify responsibilities and financial transactions between the City of Folsom and Capital SouthEast Connector Joint Powers Authority; and

WHEREAS, funds in the amount of \$400,000 are available in the Transportation Improvement Fund (Fund 446) for Fiscal Year 2021-22; and

WHEREAS, reimbursements received from the Joint Powers Authority will be credited to the Transportation Improvement Fund (Fund 446); and

WHEREAS, the agreement will be in a form acceptable to the City Attorney;

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the City Manager to execute an Amendment to the Memorandum of Understanding Agreement (Contract No. 174-21 20-062) with the Capital SouthEast Joint Powers Authority for the Capital SouthEast Connector Segment D3(A) Pedestrian/Bike Trail.

NOW, THEREFORE BE IT FURTHER RESOLVED that the Finance Director is authorized to appropriate \$400,000 from the Transportation Improvement Fund (Fund 446) for the Capital Southeast Connector Segment D3(A) Pedestrian/Bike Trail, to be offset by an additional reimbursement from the JPA.

PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

Attachment 2

ITEM 14 b

**SECOND AMENDMENT TO MEMORANDUM OF UNDERSTANDING
BETWEEN THE CAPITAL SOUTHEAST CONNECTOR JPA AND THE CITY OF
FOLSOM RELATED TO A PORTION OF SEGMENT D3 OF THE CAPITAL
SOUTHEAST CONNECTOR EXPRESSWAY**

This Second Amendment (“Amendment”) to the November 10, 2020, Memorandum of Understanding (“MOU”) is made and entered into on this ___ day of _____, 2022 by and between the Capital SouthEast Connector Joint Powers Authority, a joint powers authority, (“JPA”) and the City of Folsom, a Municipal Corporation (“City”).

RECITALS

WHEREAS, the City and JPA entered into a MOU regarding a portion of Segment D3 of the Capital SouthEast Connector expressway beginning at the intersection of Prairie City Road and continuing to the intersection of East Bidwell Street (the “Project”); and

WHEREAS, the Project involves the construction of four lanes and a bridge over Alder Creek, which will result in a new alignment of White Rock Road adjacent and immediately south of its existing location; and

WHEREAS, the City and JPA entered into an amendment to the MOU on December 9, 2021 to provide funding for the realignment of existing Scott Road to the signalized intersection at White Rock Road/Prairie City Road within the City’s right of way (the “Scott Road Project”); and

WHEREAS, the City and JPA now desire to further amend the MOU to no longer provide for reimbursement of funds in connection with a Grant issued to the JPA by CalRecycle as the Project experienced significant cost savings, in excess of the grant award, by using alternate construction materials than those required by the Grant; and

WHEREAS, the City and JPA further desire to provide additional funding for the construction of Class 1 multi-use path improvements within the Project limits (the “Multi-Use Path”); and

NOW, THEREFORE, the parties hereto, in consideration of the mutual covenants, promises, and agreements herein contained, and for other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged and agreed, do hereby agree to amend the MOU as follows:

1. **Recitals**. The above recitals are true and correct and are hereby made a part of the MOU.
2. **Term**. The MOU shall terminate on March 1, 2023.
3. **Elimination Grant Funding**. The JPA and City hereby agree that Sections 4 and 5 of the MOU are hereby deleted in their entirety and that these Sections of the MOU shall now read “[RESERVED]”.

4. Funding for Multi-Use Path. The JPA will reimburse the City up to Four Hundred Thousand dollars (\$400,000.00) for the Multi-Use Path (“Multi-Use Path Funds”). The JPA shall reimburse the City for Multi-Use Path improvements within thirty (30) days of receipt of an invoice from the City.
5. City Obligations. The City agrees to include the JPA in Multi-Use Path related decisions affecting construction cost, including those related to any design revisions or contract change order requests. The City shall submit monthly invoices related to the Multi-Use Path. Invoices shall include dates and description of the work performed, summary of 1) total Multi-Use Path Funds; 2) amount of current bill; 3) amount billed to date; 4) amount of remaining Multi-Use Path Funds, and documentation of reimbursable expenses and billed items.
6. No Additional Funding. Except as otherwise provided in this MOU and the existing Federal Funding of the project, neither party shall be responsible for contributing additional funds.
7. Except as expressly amended herein, all terms and conditions of the MOU shall remain in full force and effect.

[SIGNATURES ON NEXT PAGE]

IN WITNESS WHEREOF, the parties hereto have executed this Amendment in the County of Sacramento, State of California, on the date set forth above.

CITY OF FOLSOM

Elaine Andersen, City Manager

Approved as to Form:

Steven Wang, City Attorney

Attest:

Mark Rackovan, Public Works Director

Stacey Tamagni, Finance Director

Christa Freemantle, City Clerk

CAPITAL SOUTHEAST CONNECTOR AUTHORITY

Derek Minnema, Executive Director

Approved as to Form:

Sloan Sakai Yeung & Wong, LLP
Legal Counsel to JPA

Attachment 3

**FIRST AMENDMENT TO MEMORANDUM OF UNDERSTANDING
BETWEEN THE CAPITAL SOUTHEAST CONNECTOR JPA AND THE CITY OF
FOLSOM RELATED TO A PORTION OF SEGMENT D3 OF THE CAPITAL
SOUTHEAST CONNECTOR EXPRESSWAY**

This First Amendment (“Amendment”) to the November 10, 2020 Memorandum of Understanding (“MOU”) is made and entered into on this 9 day of ~~November~~ ^{December}, 2021 by and between the Capital SouthEast Connector Joint Powers Authority, a joint powers authority, (“JPA”) and the City of Folsom, a Municipal Corporation (“City”).

RECITALS

WHEREAS, the City and JPA entered into a MOU regarding a portion of Segment D3 of the Capital SouthEast Connector expressway beginning at the intersection of Prairie City Road and continuing to the intersection of East Bidwell Street (the “Project”); and

WHEREAS, the Project involves the construction of four lanes and a bridge over Alder Creek, which will result in a new alignment of White Rock Road adjacent and immediately south of its existing location; and

WHEREAS, the City and JPA now desire to realign existing Scott Road to the signalized intersection at White Rock Road/Prairie City Road within the City’s right of way (the “Scott Road Project”); and

WHEREAS, the City and JPA have met regularly throughout the design phase and are in agreement on the scope and nature of the work which will include a realigned roadway upgraded to current design standards, including shoulders and drainage improvements; and

WHEREAS, the JPA approved the final bid package on August 27, 2021 (“Final Bid Package”) and the City and JPA now desire to advertise the project for construction and work collaboratively to ensure the successful completion of the Project.

NOW, THEREFORE, the parties hereto, in consideration of the mutual covenants, promises, and agreements herein contained, and for other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged and agreed, do hereby agree to amend the MOU as follows:

1. **Recitals**. The above recitals are true and correct and are hereby made a part of the MOU.
2. **Term**. The MOU shall terminate on March 1, 2023.
3. **Funding Contingency**. The City and JPA hereby acknowledge that the obligations of the parties in this Amendment are contingent upon the JPA’s receipt of Construction Funds, as such term is defined in Section 4 herein, from the City of Elk Grove pursuant to a Memorandum of Understanding between the JPA and the City of Elk Grove Related to the Exchange of Funding.



4. JPA Obligations.


- a. The JPA will reimburse the City up to three million three hundred and fifty thousand dollars (\$3,350,000.00) for construction of the Scott Road Project (“Construction Funds”). The JPA shall reimburse the City for costs related to the construction of the Scott Road Project and for construction management services within thirty (30) days of receipt of an invoice from the City.
- b. During construction of the Project the JPA will provide engineering support and environmental monitoring services.

5. City Obligations. The City hereby agrees to the following:

- a. The City will publicly advertise for bids the Final Bid Package for construction of the Scott Road Project to allow for construction to commence in Spring of 2022.
 - b. The City will administer construction of the Scott Road Project and will release a request for proposals for construction management services for the Scott Road Project. The City will include the JPA in the request for proposal response review, evaluation, and selection process.
 - c. The City shall only award the Scott Road Project and execute a contract for construction management services if the total costs do not exceed the Construction Funds available for reimbursement by the JPA. The City agrees to include the JPA in Project related decisions affecting construction cost, including those related to any Project design revisions or contract change order requests. In the event bids for the Scott Road Project and the cost of construction management services exceed the amount of Construction Funds, the parties may agree in writing to provide additional funding to address any shortfall.
 - d. The City shall submit monthly invoices related to construction of the Scott Road Project to the JPA. Invoices shall include dates and description of the work performed, summary of 1) total Construction Funds; 2) amount of current bill; 3) amount billed to date; 4) amount of remaining construction funds, and documentation of reimbursable expenses and billed items.
6. No Additional Funding. Except as otherwise provided in this MOU, neither party shall be responsible for contributing additional funds.
7. Except as expressly amended herein, all terms and conditions of the MOU shall remain in full force and effect.

IN WITNESS WHEREOF, the parties hereto have executed this Amendment in the County of Sacramento, State of California, on the date set forth above.

CITY OF FOLSOM


Elaine Andersen, City Manager

Approved as to Form:


Steven Wang, City Attorney

Attest:


Mark Rackovan, Public Works Director


Stacey Tamagni, Finance Director


Christa Freemantle, City Clerk

CAPITAL SOUTHEAST CONNECTOR AUTHORITY


Derek Minnema, Executive Director

Approved as to Form:


Sloan Sakai Yeung & Wong, LLP
Legal Counsel to JPA

Attachment 4

MEMORANDUM OF UNDERSTANDING
BETWEEN THE CAPITAL SOUTHEAST CONNECTOR JPA AND THE CITY OF
FOLSOM RELATED TO A PORTION OF SEGMENT D3 OF THE CAPITAL
SOUTHEAST CONNECTOR EXPRESSWAY

This Memorandum of Understanding (“MOU”) is made and entered into on this 10th day of November 2020 by and between the Capital SouthEast Connector Joint Powers Authority, a joint powers authority, (“JPA”) and the City of Folsom, a Municipal Corporation (“City”).

RECITALS

WHEREAS, the City and JPA have collaborated on the design of a portion of Segment D3 of the Capital Southeast Connector expressway beginning at the intersection of Prairie City Road and continuing to the intersection of East Bidwell Street (the “Project”); and

WHEREAS, the Project involves the construction of four lanes and a bridge over Alder Creek, which will result in a new alignment of White Rock Road adjacent and immediately south of its existing location; and

WHEREAS, on May 13, 2020, the California Transportation Commission authorized \$25 million for construction of the Project; and

WHEREAS, the City received construction authorization from the California Department of Transportation (“Caltrans”) on June 25, 2020 and subsequently released the Project for bid; and

WHEREAS, the City and JPA desire to address certain ancillary issues related to costs associated with preliminary grading for the Project and the use of funds awarded to the JPA in connection with the Department of Resources Recycling and Recovery (“CalRecycle”) Fiscal Year 2019-20 Rubberized Pavement Grant Program, Grant Number TRP12-19-0002 (the “Grant”).

NOW, THEREFORE, the parties hereto, in consideration of the mutual covenants, promises, and agreements herein contained, and for other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged and agreed, do hereby agree as follows:

1. **Recitals**. The above recitals are true and correct and are hereby made a part of this MOU.
2. **Term**. This MOU shall terminate on April 1, 2022 or upon completion of the Project, whichever is sooner.
3. **Project Utility Grading**. Within sixty (60) days of execution of this MOU, the JPA shall pay the City thirty thousand dollars (\$30,000.00) for use in connection with certain Project costs associated with preliminary grading related to utility relocations.
4. **Reimbursement of Grant Funds**. The parties acknowledge the JPA has been awarded Grant funds in an amount not to exceed two hundred twenty thousand dollars (\$220,000.00) and that the Grant is reimbursement based in accordance with rates established in the Grant agreement. In connection with the Project and consistent with the terms of the Grant



agreement, the JPA shall seek reimbursement of all eligible costs and expenses from Grant funds awarded to the Project and remit any such Grant reimbursement received to the City. The City shall provide all requisite information and supporting documentation necessary for the completion of any forms, reports and documents required in connection with seeking reimbursement of Grant funds. The JPA and City acknowledge that determination of eligibility of costs and expenses for reimbursement is made exclusively by CalRecycle in accordance with the terms of the Grant agreement and that the JPA is only obligated to remit to the City as reimbursement any Grant funds received by the JPA.

5. Eligibility for Receipt of Grant Funds. The City hereby certifies that California Labor Code Section 1782 does not prohibit the City from receiving state funds for the Project. The City further certifies that it complies with California Labor Code Section 1770-1782.
6. Amendment. This MOU and all of the covenants and conditions set forth herein, may be modified, amended or terminated only by a writing duly authorized and executed by both the City and the JPA.
7. Construction and Interpretation. It is agreed and acknowledged by the parties hereto that the provisions of this MOU have been arrived at through negotiation, and that each of the parties has had a full and fair opportunity to revise the provisions of this MOU and to have such provisions reviewed by legal counsel. Therefore, the normal rule of construction that any ambiguities are to be resolved against the drafting party shall not apply in construing or interpreting this MOU.
8. Venue. This MOU and all matters relating to it shall be governed by the laws of the State of California and any action brought relating to this MOU shall be held exclusively in a state court in Sacramento County.
9. Severability. If any provision of this MOU is held by a court of competent jurisdiction to be invalid, void or unenforceable, the remainder of this MOU shall not be affected, except as necessarily required by the invalid provisions, and shall remain in full force and effect unless amended or modified by mutual consent of the parties.
10. Notices. All notices, requests, certifications or other correspondence required to be provided by the parties to this MOU shall be in writing and shall be personally delivered or delivered by first class mail to the respective parties at the following addresses:

CITY
 City of Folsom
 Attn: City Manager
 50 Natoma Street
 Folsom, CA 95630

JPA
Capital SouthEast Connector JPA
Attn: Executive Director
10640 Mather Blvd., Ste. 120
Mather, CA 95655


Notice by personal delivery shall be effective immediately upon delivery. Notice by mail shall be effective upon receipt or three days after mailing, whichever is earlier.

11. Counterparts. This MOU may be executed in multiple counterparts, each of which shall constitute an original, and all of which taken together shall constitute one and the same instrument. Documents executed, scanned, and transmitted electronically and electronic signatures shall be deemed original signatures for purposes of this Agreement and all matters related thereto, with such scanned and electronic signatures having the same legal effect as original signatures.

[SIGNATURES ON NEXT PAGE]

IN WITNESS WHEREOF, the parties hereto have executed this MOU in the County of Sacramento, State of California, on the date set forth above.

CITY OF FOLSOM



Elaine Andersen, City Manager 11/10/2020

Approved as to Form:



Steven Wang, City Attorney 11/10/2020

Attest:



Dave Nugen, Public Works Director



Stacey Tamagni, Finance Director



Christa Freemantle, City Clerk 11/12/20

CAPITAL SOUTHEAST CONNECTOR JOINT POWERS AUTHORITY



Derek Minnema, Executive Director

Approved as to Form:



Sloan Sakai Yeung & Wong, LLP
Legal Counsel to JPA



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CITY OF
FOLSOM
DISTINCTIVE BY NATURE

Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10846 - A Resolution Authorizing the City Manager to Execute a Design and Consulting Services Contract with Bennett Engineering Services, Inc. for the East Bidwell Street/Iron Point Road and US50 Onramp Improvement Project and Appropriation of Funds
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

The Public Works Department recommends that the City Council pass and adopt Resolution No. 10846 - A Resolution Authorizing the City Manager to Execute a Design and Consulting Services Contract with Bennett Engineering Services, Inc. for the East Bidwell Street/Iron Point Road and US50 Onramp Improvement Project and Appropriation of Funds.

BACKGROUND / ISSUE

In December 2020, City Council approved Resolution No. 10571 - A Resolution Authorizing the Authorizing Staff to Submit Grant Applications to the Sacramento Area Council of Governments for the 2020-21 Funding Round. One of the projects that received Sacramento Area Council of Governments (SACOG) funding, is the subject of this engineering design project.

The East Bidwell and Iron Point / US50 Onramp Improvement Project anticipates converting one westbound through lane on Iron Point Road to a third left turn lane, modifying the East Bidwell Street and westbound US50 onramp to create a westbound US50-only lane in the number 3 lane and modify and improve onramp operations by converting the westbound high-occupancy vehicle (HOV) lane into a mixed flow lane and installing a metering system. The project will also evaluate the storage capacity of the left turn pocket on Iron Point Road at East Bidwell Street using forecasted models in Folsom and the SACOG region. The project will be a coordinated effort between the City of Folsom and Caltrans Freeway Operations, who have

been briefed on the project and partnered with the city to define the scope prior to advertisement.

POLICY / RULE

Section 2.36.080 of the Folsom Municipal Code states, in part, that contracts for supplies, equipment, services, and construction with an estimated value of \$66,141 or greater shall be awarded by the City Council.

ANALYSIS

Staff publicly advertised a Request for Proposals to provide Professional Engineering Services on February 25, 2022 and received one proposal on March 22, 2022.

Bennett Engineering Services, Inc. was the sole submission for this project. City staff is comfortable recommending Bennett Engineering Services, Inc. for the award of this contract based on their prior experience with the city, their high-quality team consisting of Fehr & Peers, Jacobs, and UNICO, and their submitted proposal for this project which was built off their original design and analysis effort that was instrumental in the city being successfully awarded the grant for this project.

FINANCIAL IMPACT

The city was awarded SACOG Maintenance and Modernization funds in the amount of \$5,167,000 for this project. After subsequent discussions with Caltrans, staff demonstrated that nearly the same project result would be achieved by converting the existing HOV lane for the westbound US50 onramp to a metered mix flow lane and eliminating the widening of the onramp would substantially reduce the overall cost of the project by an anticipated \$4.3 million. Staff now believes that the use of local funds to accelerate project delivery would better serve the community and region. Staff is currently in discussions to reprogram the current SACOG award to other projects in the city.

The design contract with Bennett Engineering Services, Inc. would be authorized for a not to exceed amount of \$246,454. Staff is requesting an appropriation in the amount of \$246,454 from the Transportation Improvement Fund (Fund 446) for this project.

ENVIRONMENTAL REVIEW

Bennett Engineering Services, Inc will work with City Staff for environmental evaluations and a technical memorandum that will be submitted to Caltrans for approval of the project as it pertains to the California Environmental Quality Act (CEQA).

ATTACHMENTS

1. Resolution No. 10846 - A Resolution Authorizing the City Manager to Execute a Design and Consulting Services Contract with Bennett Engineering Services, Inc. for the East Bidwell Street/Iron Point Road and US50 Onramp Improvement Project and Appropriation of Funds
2. Preliminary configuration of East Bidwell Street/Iron Point Road and US50 Onramp

Submitted,



Mark Rackovan, PUBLIC WORKS DIRECTOR

Attachment 1

RESOLUTION NO. 10846

A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE A DESIGN AND CONSULTING SERVICES CONTRACT WITH BENNETT ENGINEERING SERVICES, INC. FOR THE EAST BIDWELL STREET/IRON POINT ROAD AND US50 ONRAMP IMPROVEMENT PROJECT AND APPROPRIATION OF FUNDS

WHEREAS, using the recommendations from Resolution 10571 of the City Council, the Public Works Department applied for and received funds from the Sacramento Area Council of Governments (SACOG) for the 2020-2021 Funding Round in the amount of \$5,167,000 for the design and construction of roadway improvements at East Bidwell Street/Iron Point Road and the US50 westbound onramp; and

WHEREAS, subsequent discussions with Caltrans, changed the proposed design which significantly reduced the cost of the project and still provided increased traffic flow; and

WHEREAS, the awarded SACOG funding will most likely be re-programmed to other City of Folsom projects; and

WHEREAS, a Request for Proposal to provide Professional Engineering Services was publicly advertised on February 25, 2022, and on March 22, 2022, the lone proposal was received from Bennett Engineering Services, Inc.; and

WHEREAS, the proposal was reviewed by a proposal review panel, with Bennett Engineering Services, Inc. being chosen as the most qualified firm; and

WHEREAS, there is sufficient funding available for the contract in the Transportation Improvement Fund (Fund 446); and

WHEREAS, the agreement will be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the City Manager to Execute a Design and Consulting Services Contract with Bennett Engineering Services, Inc. for the East Bidwell Street/Iron Point Road and US50 Onramp Improvement Project for the not-to-exceed amount of \$246,454.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Finance Director is authorized to appropriate an additional \$246,454 from the Transportation Improvement Fund (Fund 446) for this contract.

PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

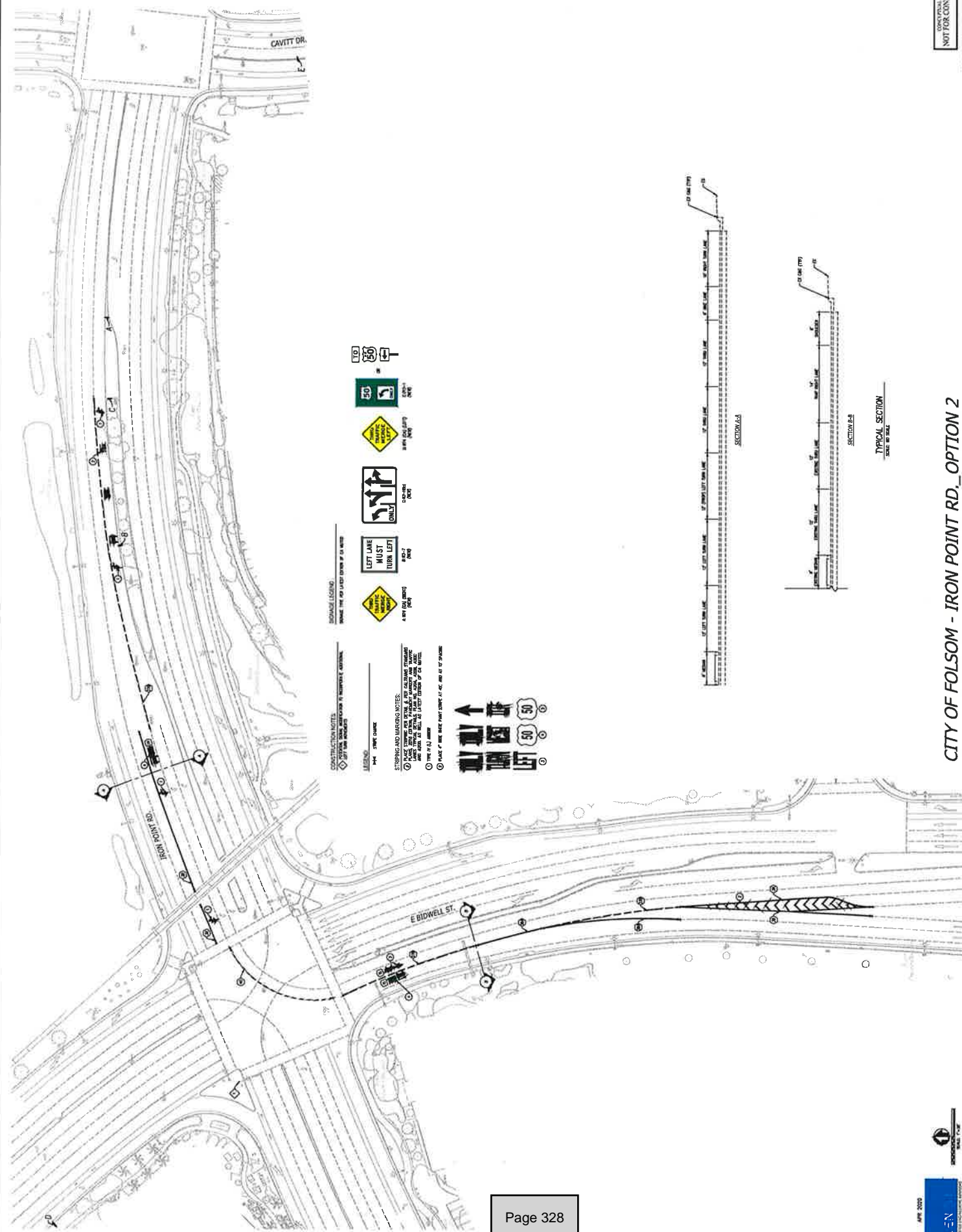
- AYES:** Councilmember(s):
- NOES:** Councilmember(s):
- ABSENT:** Councilmember(s):
- ABSTAIN:** Councilmember(s):

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

Attachment 2



DISPATCH LEGEND:
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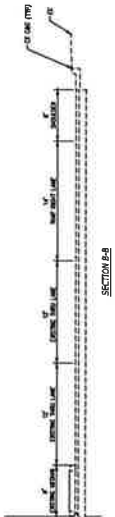
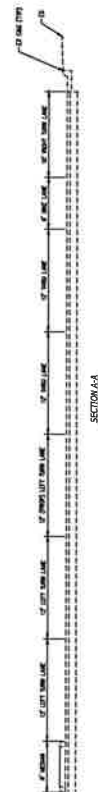
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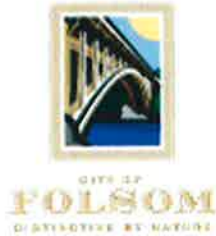
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TYPICAL SECTION
SCALE: AS SHOWN

CITY OF FOLSOM - IRON POINT RD., OPTION 2



Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10847 - A Resolution Authorizing the City Manager to Execute a Contract Change Order for the Pedestrian/Bike Trail with Goodfellow Brothers, LLC (Contract No. 174-21 20-060) for the Capital SouthEast Connector Segment D3(A), Project No. PW1607, Federal Project No. 5288(046)
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

The Public Works Department recommends that the City Council pass and adopt Resolution No. 10847 - A Resolution Authorizing the City Manager to Execute a Contract Change Order for the Pedestrian/Bike Trail with Goodfellow Brothers, LLC (Contract No. 174-21 20-060) for the Capital SouthEast Connector Segment D3(A), Project No. PW1607, Federal Project No. 5288(046).

BACKGROUND / ISSUE

The planned Capital SouthEast Connector is a 34-mile limited access roadway spanning from U.S. 50 at Silva Valley Parkway interchange in El Dorado County to Interstate 5 at the Hood-Franklin Interchange in Elk Grove. The Connector is planned to be constructed in segments as funding and priorities allow. Segment D3 includes the length of the Connector that borders the City of Folsom and Sacramento County from Prairie City Road to the El Dorado County Line. Segment E1 is the El Dorado County Segment that Segment D3 ties into.

Segment D3 has been further segmented into two additional segments: D3(A) and D3(B). Segment D3(A) will upgrade the existing White Rock Road to a four-lane expressway beginning near the intersection of Prairie City Road and continuing through the intersection of East Bidwell Street. The project involves constructing four lanes between these two major intersections, including a bridge over Alder Creek. The bridge also serves as a "wildlife crossing," as contemplated in the South Sacramento Habitat Conservation Plan.

The project is a complete reconstruction that will create a new alignment of White Rock Road adjacent to and immediately south of the existing White Rock Road. The existing White Rock Road will remain open to traffic during construction.

The City of Folsom is leading the construction of this project on behalf of the Capital SouthEast Connector Joint Powers Authority (JPA) and in collaboration with Sacramento County.

The City has secured the State Transportation Block Grant Program (STBG) and SB1 funding from the Sacramento Area Council of Governments (SACOG) in partnership with the JPA. A request for authorization to proceed with construction was submitted to Caltrans on May 15, 2020. The Authorization to Proceed (E-76) was received on June 25, 2020, and the subsequent Supplemental Agreement was received on August 8, 2020.

The project was initially designed to include a Class 1 Bike Trail and interconnection between traffic signals. These two items of work were removed from the project due to funding constraints, with an intent to construct them in a later phase. Funding for completion of the Pedestrian/Bike bridge and trail has been supplemented by Amendment No. 2 with the SouthEast Connector Joint Powers Authority.

POLICY / RULE

Section 2.36.080 of the Folsom Municipal Code states, in part, that contracts for supplies, equipment, services, and construction with an estimated value of \$66,141 or greater shall be awarded by the City Council.

ANALYSIS

This project was publicly advertised on July 13, 2020, and bids were opened publicly on September 9, 2020, at 2:00 p.m. in the City Council Chamber.

Six bids were received with Goodfellow Brothers, LLC being the lowest responsive responsible bid as follows:

Contractor	Ranking	Bid Amount
Goodfellow Brothers, LLC	Lowest Responsive Responsible Bid	\$22,368,765.00

The lowest responsive responsible bid proposal provided by Goodfellow Brothers, LLC was reviewed by staff and found to be in good order.

FINANCIAL IMPACT

The Capital SouthEast Connector Segment D3(A) is eligible to receive funds from FAST Act funding based on a SACOG Regional Surface Transportation Program Grant and SB-1 Funding. Funding for construction and construction engineering is shown below:

Fund	Fund Type	Amount
FAST Act	Surface Transportation Block Grant to match SB-1 Funding 1 to 1 at \$10,000,000	\$15,000,000
SB1	Competitive	\$10,000,000
Local Transportation Fund (446)	To match Surface Transportation Block Grant of \$15,000,000 at 11.47%	\$1,720,500
Local Transportation Fund (446)	To Cover Balance of Project Funding	\$1,195,147
Local Transportation Fund (446)	SouthEast Connector Joint Powers Authority to provide funding for Pedestrian/Bike Trail through Invoicing	\$400,000
	Project Budget for Construction	\$28,315,396

The FAST Act funding requires a local match of 11.47%. The Transportation Fund (Fund 446) will be utilized to meet the required match of \$1,720,500. The SB1 funding requires a 100% match, of which \$10,000,000 of the FAST Act funding will be utilized as the match.

The agreement with Goodfellow previously approved is for \$22,368,765 with a budgeted contingency of \$2,236,876.50 (10%) for a total contract budget amount of \$24,605,641.50.

The contract costs for construction and construction management have been previously approved by City Council in separate resolutions as follows:

Company	Description	Cost
Goodfellow Brothers, LLC	Construction	\$22,368,765.00
	10% Contingency for Construction	\$2,236,876.50
	Additional Contingency Funded by SouthEast Connector Joint Powers Authority	\$400,000
Salaber Associates, Inc.	Construction Engineering, Inspection and Materials Testing	\$2,992,848.00
Dokken Engineering, Inc.	Design Support and Environmental Services	\$317,157.28
	Project Budget for Construction	\$28,315,646.78

The Contract Change Order for the Pedestrian/Bike Trail will utilize the existing contingency in the amount of \$285,802 and \$400,000 which has been appropriated through Amendment 2 of the Memorandum of Understanding with the SouthEast Connector Joint Powers Authority. The total cost of the Pedestrian/Bike Trail is \$685,802. The current project budget is \$28,443,000. No additional City of Folsom funding is needed.

ENVIRONMENTAL REVIEW

In 2016 the Capital SouthEast Connector JPA approved a California Environmental Quality Act (CEQA) Tiered Initial Study with Mitigated Negative Declaration for the Capital SouthEast Connector Segment D3/E1.

Caltrans approved the National Environmental Policy Act (NEPA) Categorical Exclusion for Segment D3/E1.

The project is not located in an environmentally sensitive area and would not result in potential impacts to the environment, including traffic, noise, air quality and water quality.

ATTACHMENT

Resolution No. 10847 - A Resolution Authorizing the City Manager to Execute a Contract Change Order for the Pedestrian/Bike Trail with Goodfellow Brothers, LLC (Contract No. 174-21 20-060) for the Capital SouthEast Connector Segment D3(A), Project No. PW1607, Federal Project No. 5288(046)

Submitted,

Mark Rackovan, PUBLIC WORKS DIRECTOR

RESOLUTION NO. 10847

**A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE A
CONTRACT CHANGE ORDER FOR THE PEDESTRIAN/BIKE TRAIL WITH
GOODFELLOW BROTHERS, LLC (CONTRACT NO. 174-21 20-060) FOR THE
CAPITAL SOUTHEAST CONNECTOR SEGMENT D3(A), PROJECT NO. PW1607,
FEDERAL PROJECT NO. 5288(046)**

WHEREAS, the Capital SouthEast Connector Project is the Sacramento region's largest single transportation project; and

WHEREAS, the City of Folsom is a Member Jurisdiction of the Capital SouthEast Connector Joint Powers Authority and will implement the Project; and

WHEREAS, the Capital SouthEast Connector Project has, to date, been funded primarily through a Sacramento County sales-tax measure approved in 2004 by 75 percent of voters; and

WHEREAS, the Capital SouthEast Connector Project has established a program development budget that requires the use of Federal and State funds to advance the project towards timely construction; and

WHEREAS, the City of Folsom desires to construct the Capital SouthEast Connector Project Segment D3(A) along the City's border to four lanes with two eight-foot shoulders; and

WHEREAS, the City of Folsom received Surface Transportation Block Grant Program funding, and is eligible for federal reimbursement, up to \$15,000,000; and

WHEREAS, the City of Folsom received SB-1 funding, and is eligible for state reimbursement up to \$10,000,000; and

WHEREAS, this project was publicly advertised on July 13, 2020, with six bids received on September 9, 2020, with Goodfellow Brothers, LLC being the lowest responsive responsible bidder; and

WHEREAS, the City of Folsom entered into a contract with Goodfellow Brothers, LLC for construction, of the Capital SouthEast Connector Segment D3(A); and

WHEREAS, a Contract Change Order will be utilized to add a pedestrian/bike trail; and

WHEREAS, the cost of completion for the Pedestrian/Bike Trail is \$685,801.96; and

WHEREAS, the City of Folsom received \$400,000 from the SouthEast Connector Joint Powers Authority through Amendment Two of the Memorandum of Understanding to construct the Class 1 Bike Trail; and

WHEREAS, the \$400,000 received from the SouthEast Connector Joint Powers Authority is to be added to the project contingency budget; and

WHEREAS, the balance needed of \$285,801.96 is within the existing project budget contingency; and

WHEREAS, the agreement will be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the City Manager to Execute a Contract Change Order for the Pedestrian/Bike Trail with Goodfellow Brothers, LLC (Contract No. 174-21 20-060) for the Capital SouthEast Connector Segment D3(A), Project No. PW1607, Federal Project No. 5288(046), in the amount of \$685,801.96.

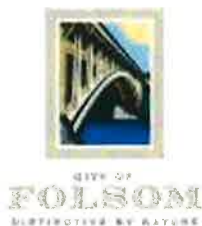
PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK



Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10848 - A Resolution Adopting a List of Projects for Fiscal Year 2022-23 to be Funded by Senate Bill 1: The Road Repair and Accountability Act
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

The Public Works Department recommends that the City Council pass and adopt Resolution No. 10848 - A Resolution Adopting a List of Projects for Fiscal Year 2022-23 to be Funded by Senate Bill 1: The Road Repair and Accountability Act.

BACKGROUND / ISSUE

On April 28, 2017, California Governor Brown signed into law Senate Bill 1 (SB-1) which establishes the Road Maintenance and Rehabilitation Account (RMRA) to address deferred maintenance on the state highway and local street and road systems. The Bill states that funds shall be used for projects that include, but are not limited to, the following:

- Road maintenance and rehabilitation
- Safety projects
- Railroad grade separations
- Complete street components, including active transportation purposes, pedestrian and bicycle safety projects, transit facilities, and drainage and stormwater capture projects
- Traffic control devices
- Match for state/federal funds for eligible projects

The City of Folsom is estimated to receive approximately \$1.8 million of RMRA funds for Fiscal Year 2022-23.

The Fiscal Year 2021-22 SB-1 funds will be utilized for the Pavement Resurfacing Project Fiscal Year 2021-22 which includes Iron Point Road between East Bidwell Street and Oak Avenue Parkway and Broadstone Parkway between East Bidwell Street and Iron Point Road.

The City of Folsom currently has an overall Pavement Condition Index (PCI) value of 71, which is described as “Good-Excellent” according to StreetSaver, the software program that is utilized by the Public Works Department to manage the City’s Pavement Management Program. Future projections show that at the pre-SB-1 funding level, this PCI value would degrade to 42 (Poor) by year 2037, and to value 32 (Very Poor) by the year 2045. Due to this rapid, yet expected, degradation of the pavement system, this SB-1 funding source is valuable in helping to maintain the City of Folsom’s overall pavement network.

In order to receive SB-1 funds, local agencies are required by the California Transportation Commission (CTC) to annually submit a list of projects that may be constructed and funded with RMRA funds, and the list must be part of an approved Resolution. The list of projects allows the City to scope the Capital Improvements to meet the available funding.

The Public Works Capital Improvement Projects identified to utilize SB-1 funding are as follows:

Project	Project Completion	Estimated Useful Life
Pavement Resurfacing Project FY 22-23	June 2023	25 year life
<ul style="list-style-type: none"> • Blue Ravine Road (Prairie City Road to Oak Avenue Parkway) • Willow Creek Estates East • Briggs Ranch • Rancho Diablo 		
On-Call Concrete & Asphalt Repair FY22-23	June 2023	50 year life
Glenn Drive Storm Drain Repair	June 2023	50 year life
Natoma Street Drainage Project (Phase 3)	June 2023	50 year life
Willow Creek Estates Storm Drain Lining (Phase 3)	June 2023	50 year life

POLICY / RULE

The California Transportation Commission (CTC) requires any agency that will receive funding from SB-1 to submit a list of projects that is part of an adopted Resolution.

ANALYSIS

Staff has reviewed the most recent SB-1 reporting and spending requirements and found them to be reasonable and realistic to achieve.

FINANCIAL IMPACT

RMRA revenue for Fiscal Year 2022-23 is estimated to be \$1.8 million. Adopting this list of eligible projects authorizes staff to use the Fiscal Year 2022-23 funding for the projects on the list.

ENVIRONMENTAL REVIEW

This action is exempt from environmental review under the California Environmental Quality Act (CEQA).

ATTACHMENT

Resolution No. 10848- A Resolution Adopting a List of Projects for Fiscal Year 2022-23 to be Funded by Senate Bill 1: The Road Repair and Accountability Act

Submitted,



Mark Rackovan, Public Works Director

RESOLUTION NO. 10848**A RESOLUTION ADOPTING A LIST OF PROJECTS FOR FISCAL YEAR 2022-23
TO BE FUNDED BY SENATE BILL 1: THE ROAD REPAIR AND ACCOUNTABILITY
ACT**

WHEREAS, Senate Bill 1 (SB 1), the Road Repair and Accountability Act of 2017 (Chapter 5, Statutes of 2017) was passed by the Legislature and signed into law by California Governor Brown in April 2017 in order to address the significant multi-modal transportation funding shortfalls statewide; and

WHEREAS, SB 1 includes accountability and transparency provisions that will ensure that Folsom residents are aware of the projects proposed for funding in their community and which projects have been completed each Fiscal Year; and

WHEREAS, the City must pass a resolution adopting a list of eligible projects proposed to receive funding from the Road Maintenance and Rehabilitation Account (RMRA), created by SB 1, which must include a description and the location of each proposed project, a proposed schedule for the project's completion, and the estimated useful life of the improvement; and

WHEREAS, the list of projects will include projects that may not be completed in the fiscal year allotted due to available funding; and

WHEREAS, the City will receive an estimated \$1.8 million in RMRA funding from SB 1 in Fiscal Year 2022-23; and

WHEREAS, this is the sixth year in which the City is receiving SB 1 funding that will enable the City to continue essential pavement resurfacing and drainage projects that would not have otherwise been possible without SB 1; and

WHEREAS, the City used a combination of the Pavement Management System and known priorities to develop the SB 1 project list to ensure revenues are being used on the most high-priority and cost-effective pavement resurfacing projects that also meet Folsom's priorities for transportation investment; and

WHEREAS, the SB 1 project list and overall investment in Folsom's local streets and roads infrastructure — with a focus on basic maintenance and safety, investing in the complete streets infrastructure, and using cutting edge technology, materials and practices — will have significant positive co-benefits citywide:

WHEREAS, the agreement will be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom approves this Resolution adopting the below list of projects to be funded in FY 2022-23 by Senate Bill 1: The Road Repair and Accountability Act.

Project	Project Completion	Estimated Useful Life
Pavement Resurfacing Project FY 22-23	June 2023	25 year life
<ul style="list-style-type: none"> • Blue Ravine Road (Prairie City Road to Oak Avenue Parkway) • Willow Creek Estates East • Briggs Ranch • Rancho Diablo 		
On-Call Concrete & Asphalt Repair FY22-23	June 2023	50 year life
Glenn Drive Storm Drain Repair	June 2023	50 year life
Natoma Street Drainage Project (Phase 3)	June 2023	50 year life
Willow Creek Estates Storm Drain Lining (Phase 3)	June 2023	50 year life

PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

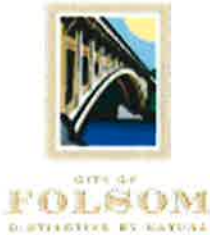
AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

 Kerri M. Howell, MAYOR

ATTEST:

 Christa Freemantle, CITY CLERK

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Folsom City Council Staff Report



MEETING DATE:	5/10/2022
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No.10850– A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 1C South Village No. 4 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 1C Village No. 4 South Subdivision
FROM:	Community Development Department

RECOMMENDATION / CITY COUNCIL ACTION

Staff recommends that the City Council move to adopt:

Resolution No. 10850 – A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 1C South Village No. 4 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 1C South Village No. 4 Subdivision.

BACKGROUND / ISSUE

The Vesting Tentative Subdivision Map for the Mangini Ranch Phase 1C South Village No. 4 subdivision was approved by the City Council on October 26, 2021.

The action for consideration by the City Council is the approval of the Final Map and Subdivision Improvement Agreement for the Mangini Ranch Phase 1C South Village No. 4 subdivision. The Final Map for the Mangini Ranch Phase 1C South Village No. 4 subdivision will create a total of 115 multi-family low density residential lots. With the approval of the Final Map, the subdivision process for this project will be complete.



The Mangini Ranch Phase 1C South Village No. 4 subdivision is located on the south side of Mangini Parkway and Mangini Ranch Phase 1C North Village No. 3, north of White Rock Road and west of Placerville Road in the Folsom Plan Area (FPA) (see Attachment 4).

POLICY / RULE

The Subdivision Map Act of the State of California and the City’s Subdivision Ordinance require that the City Council approve Final Maps and Subdivision Improvement Agreements.

ANALYSIS

The Final Map and conditions of approval for the Mangini Ranch Phase 1C South Village No. 4 subdivision have been reviewed by the Community Development Department and other City departments. The Final Map has been found to be in substantial compliance with the approved Vesting Tentative Subdivision Map, and all conditions pertaining to the map have been satisfied.

Attached is a table which includes the conditions of approval for the Mangini Ranch Phase 1C South Village No. 4 Vesting Tentative Subdivision Map. The tables include information concerning when the condition is required to be satisfied (e.g. at Final Map, building permit, etc.), which City department is responsible to verify that it has been satisfied, and comments or an explanation on how the condition was satisfied.

ENVIRONMENTAL REVIEW

The Mangini Ranch Phase 1C South Village No. 4 Subdivision project has been previously determined to be exempt from review under the California Environmental Quality Act (CEQA) pursuant to Government Code section 65457 and CEQA Guidelines sections 15182 and 15183. No further environmental review is required for this Final Map.

ATTACHMENTS

1. Resolution No. 10850- A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 1C South Village No. 4 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 1C South Village No. 4 Subdivision
2. Mangini Ranch Phase 1C South Village No. 4 Subdivision Improvement Agreement
3. Mangini Ranch Phase 1 C South Village No. 4 Final Map
4. Mangini Ranch Phase 1C South Village No. 4 Vesting Tentative Subdivision Map
5. Table of Conditions of Approval for the Mangini Ranch Phase 1C South Village No. 4 Vesting Tentative Subdivision Map

Submitted,



PAM JOHNS
Community Development Director

ATTACHMENT 1

**RESOLUTION NO. 10850 - A RESOLUTION AUTHORIZING
THE CITY MANAGER TO EXECUTE A SUBDIVISION
IMPROVEMENT AGREEMENT AND ACCEPT OFFERS OF
DEDICATION FOR THE MANGINI RANCH PHASE 1C SOUTH
VILLAGE NO. 4 SUBDIVISION, AND APPROVAL OF THE FINAL
MAP FOR THE MANGINI RANCH PHASE 1C VILLAGE NO. 4
SOUTH SUBDIVISION**

RESOLUTION NO. 10850

A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE A SUBDIVISION IMPROVEMENT AGREEMENT AND ACCEPT OFFERS OF DEDICATION FOR THE MANGINI RANCH PHASE 1C SOUTH VILLAGE NO. 4 SUBDIVISION, AND APPROVAL OF THE FINAL MAP FOR THE MANGINI RANCH PHASE 1C SOUTH VILLAGE NO. 4 SUBDIVISION

WHEREAS, the Final Map for the Mangini Ranch Phase 1C South Village No. 4 subdivision has been reviewed and approved by the City Engineer as complying with the approved or conditionally approved Vesting Tentative Subdivision Map for the subdivision; and

WHEREAS, the City Council has reviewed the Final Map for the Mangini Ranch Phase 1C South Village No. 4 subdivision; and

WHEREAS, the City Council agrees to accept, subject to improvement, any and all offers of dedication as shown on the Final Map for the Mangini Ranch Phase 1C South Village No. 4 subdivision.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Folsom that the Final Map for the Mangini Ranch Phase 1C South Village No. 4 subdivision is hereby approved.

BE IT FURTHER RESOLVED that the City Manager is hereby authorized to execute the Subdivision Improvement Agreement with Arcadian Improvement Company, LLC. in a form acceptable to the City Attorney and accept the offers of dedication for the Mangini Ranch Phase 1C South Village No. 4 subdivision.

PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

AYES: Councilmember(s)

NOES: Councilmember(s)

ABSENT: Councilmember(s)

ABSTAIN: Councilmember(s)

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

ATTACHMENT 2

MANGINI RANCH PHASE 1C SOUTH VILLAGE NO. 4 SUBDIVISION IMPROVEMENT AGREEMENT

No Fee Document Pursuant to Government Code Section 6103.

RECORDING REQUESTED BY:

City of Folsom

WHEN RECORDED MAIL TO:

NAME City of Folsom
City Clerk
MAILING ADDRESS 50 Natoma Street
CITY, STATE, ZIP CODE Folsom, CA 95630

SPACE ABOVE THIS LINE RESERVED FOR RECORDER'S USE

CITY OF FOLSOM

SUBDIVISION IMPROVEMENT AGREEMENT

This Agreement is made and entered into this _____ day of _____, 2022, by and between the **City of Folsom**, hereinafter referred to as "City", and **Arcadian Improvement Company, LLC., a California Limited Liability Company** hereinafter referred to as "Subdivider".

RECITALS

- A. Subdivider has presented to the City a certain Final Map of a proposed subdivision of land located within the corporate limits of the City that has been prepared in accordance with the Subdivision Map Act of the State of California, the subdivision ordinances of the City, and the Tentative Subdivision Map, if any, of the subdivision previously approved by the City Council of the City.
- B. The proposed subdivision of land is commonly known and described as **Mangini Ranch Phase 1C South Village No. 4** and is herein referred to as the "subdivision".
- C. Subdivider has requested approval of the Final Map prior to the construction and completion of the public improvements (as shown on the approved improvement plans and listed in Exhibit A), including, but not limited to streets, highways, public ways, sidewalks, curbs, gutters, bikeways, storm drainage facilities, sanitary sewer facilities, domestic water facilities, public utility facilities, landscaping, public lighting facilities, park or recreational improvements and appurtenances thereto, in or required by the Subdivision Map Act, the subdivision ordinances of the City, the Tentative Subdivision Map and development agreement, if any, approved by the City. The foregoing improvements, more specifically listed on Exhibit A attached hereto, are hereinafter referred to as "the required improvements".

- D. City Council has required as a condition precedent to the approval of the Final Map, the Subdivider first enters into and executes this subdivision improvement agreement with the City.

NOW, THEREFORE, the parties agree as follows:

1. Performance of Work. Subdivider agrees to furnish, construct, and install at his own expense the required improvements as shown on the approved plans and specifications of the subdivision, a copy of which is on file in the Community Development Department, and is incorporated herein by reference, along with any changes or modifications as may be required by the City Engineer due to errors, omissions, changes in conditions, or changes in facilities as required by the City Engineer. The approved plans and specifications of the required improvements may be modified by the Subdivider as the development progresses, provided that any modification is approved in writing by the City Engineer. The total estimated cost of the required improvements, as shown on Exhibit A, is **FOUR MILLION FOUR HUNDRED TWELVE THOUSAND TWO HUNDRED NINETY - TWO AND 00/100 DOLLARS (\$4,412,292.00)**.
2. Work; Satisfaction of City Engineer. All of the work on the required improvements is to be done at the places, of the materials, and in the manner and at the grades, all as shown upon the approved plans and specifications and as required by the City's Improvement Standards and Standard Construction Specifications and any applicable City ordinances or state and federal laws, and to the satisfaction of the City Engineer.
3. Work; Time for Commencement and Performance. Work on the required improvements shall be completed by the Subdivider on or before twelve (12) months from the date of this Agreement. At least fifteen (15) calendar days prior to the commencement of such work, the Subdivider shall notify the City Engineer in writing of the date fixed by Subdivider for commencement of the work.
4. Time of Essence; Extension.
 - a. Time is of the essence of this Agreement. The date for completion of the work of construction may not be extended, except as provided in Section 16.36.110 of the Folsom Municipal Code.
5. Improvement Security. Concurrently with the execution of this Agreement, the Subdivider shall furnish the City:
 - a. Improvement security in the sum of **FOUR MILLION FOUR HUNDRED TWELVE THOUSAND TWO HUNDRED NINETY - TWO AND 00/100 DOLLARS (\$4,412,292.00)**, which sum is equal to one hundred percent of the total estimated cost of constructing the required improvements and the cost of any other obligation to be performed by Subdivider under this Agreement, conditioned upon the faithful performance of this Agreement; and

- b. Separate improvement security in the sum of **FOUR MILLION FOUR HUNDRED TWELVE THOUSAND TWO HUNDRED NINETY - TWO AND 00/100 DOLLARS (\$4,412,292.00)**, which sum is equal to one hundred percent of the estimated cost of constructing the required improvements, securing payment to the contractor, subcontractor and to persons furnishing labor, materials, or equipment to them for the construction of the required improvements.
- c. The Subdivider shall deposit with the City **THREE THOUSAND AND NO/100 DOLLARS (\$3,000.00)** for the Final Map. The deposit may be used at the discretion of the City to correct deficiencies and conditions caused by the Subdivider, contractor, or subcontractors that may arise during or after the construction of the subdivision.
- d. The estimated total cost of required improvements includes a ten percent (10%) construction cost contingency, the cost of the installation of survey monuments in the Subdivision to guarantee and secure the placement of such monuments as provided by Section 66496 of the Government Code of the State of California, and an estimated utility cost in addition to ensure installation of public utilities. In lieu of providing the estimate of total utility costs, the Subdivider may submit, in a form acceptable to the City Engineer, certification from the utility companies that adequate security has been deposited to ensure installation.
6. Plan Checking and Inspection Fees. The Subdivider shall pay to the City fees for the checking, filing, and processing of improvement plans and specifications, and for inspecting the construction of the required improvements in the amounts and at the times established by the City.
7. Indemnification and Hold Harmless. The Subdivider shall indemnify, protect, defend, save and hold the City harmless from any and all claims or causes of action for death or injury to persons, or damage to property resulting from intentional or negligent acts, errors, or omissions of Subdivider or Subdivider's officers, employees, volunteers, and agents during performance of this Agreement, or in connection with Subdivider's work, or from any violation of any federal, state, or municipal law or ordinance, to the extent caused, in whole or in part, by the willful misconduct, negligent acts, or omissions of Subdivider or its employees, subcontractors, or agents, or by the quality or character of Subdivider's work. It is understood that the duty of Subdivider to indemnify and hold harmless includes the duty to defend as set forth in Section 2778 of the California Civil Code. Acceptance by City of insurance certificates and endorsements required under this Agreement does not relieve Subdivider from liability under this indemnification and hold harmless clause. This indemnification and hold harmless clause shall apply to any damages or claims for damages whether or not such insurance policies shall have been determined to apply, and shall further survive the expiration or termination of this Agreement. By execution of this Agreement, Subdivider acknowledges and agrees to the provisions of this Section and that it is a material element of consideration. Subdivider shall, at his own cost and expense, defend any and all actions, suits, or legal

proceedings that may be brought or instituted against the City, its officers and employees, on any such claim or demand, and pay or satisfy any judgement that may be rendered against the City in any such actions, suits or legal proceedings, or result thereof.

8. Insurance. Subdivider and any contractors hired by Subdivider to perform any of the Required Improvements shall, at their expense, maintain in effect for the duration of this Agreement or until the required improvements are accepted by the City, whichever first occurs, not less than the following coverage and limits of insurance, which shall be maintained with insurers and under forms of policy satisfactory to the City. The maintenance by Subdivider and its contractors of the following coverage and limits of insurance is a material element of this Agreement. The failure of Subdivider or any of its contractors to maintain or renew coverage or to provide evidence of renewal may be treated by the City as a material breach of this Agreement.
- a. Minimum Limits of Insurance. Subdivider shall maintain limits not less than:
1. Comprehensive General Liability: \$1,000,000 combined single limit per occurrence for bodily injury, personal injury and property damage.
 2. Automobile Liability: \$1,000,000 combined single limit per accident for bodily injury, personal injury and property damage.
 3. Worker's Compensation and Employers Liability: Worker's Compensation limits as required by the Labor Code of the State of California and Employers Liability limits of \$1,000,000 per accident.
- b. Deductibles and Self-Insured Retentions. Any deductibles or self-insured retentions shall be declared to and approved by the City. At the option of the City, either: the insurer shall reduce or eliminate such deductibles or self-insured retentions as respects to a bond guaranteeing payment of losses and related investigations, claim administration and defense expenses.
- c. Other Insurance Provisions. The policies are to contain, or be endorsed to contain, the following provisions:
1. General Liability and Automobile Liability Coverages
 - A. The City, its officers, officials, employees and volunteers are to be covered as insureds as respects: liability arising out of activities performed by or on behalf of the Subdivider; products and completed operations of the Subdivider; premises owned, leased or used by the Subdivider; or automobiles owned, leased, hired or borrowed by the Subdivider. The coverage shall contain no special limitations on the scope of protection afforded to the City, its officers, officials, employees or volunteers.

- B. The Subdivider's insurance coverage shall be primary insurance as respects the City, its officers, officials, employees and volunteers. Any insurance of self-insurance maintained by the City, its officials, employees or volunteers shall be excess of the Subdivider's insurance and shall not contribute with it.
 - C. Any failure to comply with reporting provisions of the policies shall not affect coverage provided to the City, its officers, officials, employees or volunteers.
 - D. The Subdivider's insurance shall apply separately to each insured against whom claim is made or suit is brought, except with respect to the limits of the insurer's liability.
2. Worker's Compensation and Employers Liability Coverage. The insurer shall agree to waive all rights or subrogation against the City, its officers, officials, employees and volunteers for losses arising from work performed by Subdivider for the City.
 3. All Coverages. Each insurance policy required by this clause shall be endorsed to state that coverage shall not be suspended, voided or cancelled by either party, reduced in coverage or in limits except after thirty (30) days prior written notice by certified mail, return receipt requested, has been given to the City.
- d. Acceptability of Insurers. Insurance is to be placed with insurers with a Best's rating of not less than A: VII.
 - e. Verification of Coverage. Concurrently with the execution of this Agreement, the Subdivider shall furnish the City with original endorsements affecting coverage required by this clause. The endorsements for each insurance policy are to be signed by a person authorized by that insurer to bind coverage on its behalf. The City reserves the right to require complete, certified copies of all required insurance policies at any time.
9. Title to Improvements. Title to and ownership of the required public improvements constructed under this Agreement by Subdivider shall vest absolutely in the City upon completion and written acceptance of such improvements by the City Engineer. The City Engineer shall not accept the required improvements unless Subdivider certifies that such improvements have been constructed in conformity with the approved plans and specifications, approved modifications, if any, the approved Final Map, City Improvement Standards and Standard Construction Specifications, any applicable City Ordinances or State and Federal laws and after 35 days from the date of filing of a Notice of Completion.
 10. Warranty Security. Prior to acceptance of the required improvements by the City Engineer, the Subdivider shall provide security in the amount and in the form as required by the City Engineer to guarantee the improvements against any defective work or labor done or defective materials used in the performance of the required improvements (Warranty Security)

throughout the warranty security period which shall be the period of one year following completion and written acceptance of the improvements (Warranty Security Period). The amount of the Warranty Security shall not be less than 10 percent of the cost of the construction of the improvements, including the cash deposit required in paragraph 5C of this agreement, which shall be retained for the Warranty Security Period.

11. Repair or Reconstruction of Defective Work or Materials. If, within the Warranty Security Period or the applicable statute of limitations, whichever is longer, any improvement or part of any improvement furnished and/or installed or constructed by Subdivider or any of the work done under this Agreement fails to fulfill any of the requirements of the Agreement or the specifications referred to herein as determined by the City, Subdivider shall without delay and without any cost to the City, repair, replace, or reconstruct any defective or otherwise unsatisfactory part or parts of the required improvements. If the Subdivider fails to act promptly or in accordance with this requirement, or if the exigencies of the situation require repairs or replacements to be made before the Subdivider can be notified, then the City may, at its option, make the necessary repairs or replacements or perform the necessary work, and Subdivider shall pay to City the actual cost of such repairs plus fifteen percent (15%) within thirty (30) days of the date of billing for such work by City. The parties further understand and agree that the Warranty Security furnished pursuant to paragraph 10 of this Agreement shall guarantee and secure the faithful performance and payment of the provisions of this paragraph during the Warranty Security Period.
12. Subdivider Not Agent of City. Neither Subdivider nor any of Subdivider's agents or contractors are or shall be considered to be agents of City in connection with the performance of Subdivider's obligations under this Agreement.
13. Notice of Breach and Default. If Subdivider refuses or fails to prosecute the work, or any part thereof, with such diligence as will ensure its completion within the time specified, or any extension thereof, or fails to complete the work within such time, or if Subdivider should be adjudged a bankruptcy, or Subdivider should make a general assignment for the benefit of his creditors, or if a receiver should be appointed in the event of Subdivider's insolvency, or if Subdivider or any of Subdivider's contractors, subcontractors, agents or employees should violate any of the provisions of this Agreement and the City may, but is under no obligation to, serve written notice upon Subdivider and Subdivider's surety, if any, of breach of this Agreement, or of any portion thereof.
14. Breach of Agreement: Performance By Surety or City. In the event of any such notice, Subdivider's surety, if any, shall have the duty to take over and complete the work and the required improvements; provided, however, that if the surety within fifteen (15) days after the serving of such notice of breach upon it does not give the City written notice of its intention to take over the performance thereof within fifteen (15) days after notice to the City of such election, then the City may take over the work and prosecute the same to completion by contract, or by any other method the City may deem advisable, for the account and at the expense of the Subdivider, and the Subdivider's surety shall be liable to City for any excess costs of damages incurred by the City; and in such event, the City, without liability for so doing, may take possession of and utilize in completing the work, such materials, appliances, plant or other property belonging to Subdivider as may be on the site of the work and necessary therefor.

If the form of improvement security is other than a bond, then the City, after giving notice of breach of the Agreement, may proceed to collect against the improvement security in the manner provided by law and by the terms of the security instrument.

- 15. Notices. All notices required under this Agreement shall be in writing, and delivered in person or sent by registered or certified mail, postage prepaid.

Notices required to be given to City shall be addressed as follows:

**City of Folsom
Community Development Department
50 Natoma Street
Folsom, CA 95630
ATTN: City Engineer**

Notices required to be given to Subdivider shall be addressed as follows:

**Arcadian Improvement Company, LLC
4370 Town Center Boulevard, Suite 100
El Dorado Hills, CA 95762
ATTN; William B. Bunce, President**

Notices required to be given surety, if any, of Subdivider shall be addressed as follows:

Any party of the surety may change such address by notice in writing to the other party and thereafter notices shall be addressed and transmitted to the new address.

- 16. Attorney's Fees. In the event any legal action is brought to enforce or interpret this Agreement, the prevailing party shall be entitled to an award of reasonable attorney's fees, in addition to any other relief to which he may be entitled.
- 17. Assignment. This Agreement shall bind and inure to the benefit of the assigns, successors in interest, heirs, executors, and administrators of the parties, and the parties agree that the City may cause a copy of this Agreement to be recorded in the Sacramento County Recorder's Office.

IN WITNESS WHEREOF, the parties have executed this Agreement as follows:

SUBDIVIDER

**Arcadian Improvement Company, LLC
A California Limited Liability Company**

BY: _____

Print Name: _____

DATE _____

CITY OF FOLSOM, a Municipal Corporation

DATE _____

Elaine Andersen
CITY MANAGER

ATTEST:

DATE _____

Christa Freemantle
CITY CLERK

APPROVED AS TO CONTENT:

DATE _____

Pam Johns
COMMUNITY DEVELOPMENT DIRECTOR

APPROVED AS TO FORM:

DATE _____

Steven Wang
CITY ATTORNEY

NOTICE: SIGNATURE(S) ON BEHALF OF "SUBDIVIDER" MUST BE NOTARIZED
Certificate of Acknowledgement pursuant to Civil Code, Section 1189, must be attached.
SUBDIVISION AGREEMENT – **Mangini Ranch Phase 1C South Village No. 4**

Exhibit A

FOLSOM PLAN AREA
Cost Estimate Summary
for
MANGINI RANCH PH1C-SOUTH



	<u>Total Cost</u>	<u>Cost to Complete</u>
MR1C SOUTH	\$ 4,412,292	\$ 4,412,292
<hr/>		
Subtotal MR1C SOUTH	\$ 4,412,292	\$ 4,412,292
TOTAL COSTS	\$ 4,412,292	\$ 4,412,292



FOLSOM PLAN AREA
 Cost Estimate for
 MANGINI RANCH PH1C-SOUTH

Item No	Quantity	Unit	Description	Unit Price	Total	% Complete	Cost to Complete
Site Preparation & Earthwork							
1	30	AC	Clearing & Grubbing	\$ 200.00	\$ 6,000.00	0%	\$ 6,000.00
2	30	AC	Erosion Control	\$ 1,000.00	\$ 30,000.00	0%	\$ 30,000.00
Subtotal Grading & Site Prep					\$ 36,000.00		\$ 36,000.00
Sanitary Sewer System							
1	1,325	LF	6" Sanitary Sewer, PVC SDR 26	\$ 50.00	\$ 66,250.00	0%	\$ 66,250.00
2	2,457	LF	8" Sanitary Sewer, PVC SDR 26	\$ 70.00	\$ 171,990.00	0%	\$ 171,990.00
3	16	EA	48" Standard Sanitary Sewer Manhole	\$ 4,500.00	\$ 72,000.00	0%	\$ 72,000.00
4	2	EA	60" Standard Sanitary Sewer Manhole	\$ 6,500.00	\$ 13,000.00	0%	\$ 13,000.00
5	1	EA	Connect to Existing Sewer Main	\$ 500.00	\$ 500.00	0%	\$ 500.00
6	2	EA	6" Flushing Branch	\$ 1,500.00	\$ 3,000.00	0%	\$ 3,000.00
7	115	EA	4" Sanitary Sewer Service	\$ 1,000.00	\$ 115,000.00	0%	\$ 115,000.00
Subtotal Sewer					\$ 441,740.00		\$ 441,740.00
Storm Drain System							
1	1,271	LF	12" Storm Drain, RCP CL III	\$ 45.00	\$ 57,195.00	0%	\$ 57,195.00
2	590	LF	15" Storm Drain, RCP CL III	\$ 55.00	\$ 32,450.00	0%	\$ 32,450.00
3	347	LF	18" Storm Drain, RCP CL III	\$ 60.00	\$ 20,820.00	0%	\$ 20,820.00
4	602	LF	24" Storm Drain, RCP CL III	\$ 65.00	\$ 39,130.00	0%	\$ 39,130.00
5	17	EA	48" Standard Storm Drain Manhole	\$ 5,000.00	\$ 85,000.00	0%	\$ 85,000.00
6	2	EA	60" Standard Storm Drain Manhole	\$ 7,000.00	\$ 14,000.00	0%	\$ 14,000.00
7	2	EA	72" Standard Storm Drain Manhole	\$ 9,000.00	\$ 18,000.00	0%	\$ 18,000.00
8	1	EA	96" Storm Drain Manhole	\$ 14,500.00	\$ 14,500.00	0%	\$ 14,500.00
9	15	EA	Modified Type 'B' Drainage Inlet	\$ 3,500.00	\$ 52,500.00	0%	\$ 52,500.00
10	11	EA	Type GOL-7 (On-Grade) Drainage Inlet	\$ 4,500.00	\$ 49,500.00	0%	\$ 49,500.00
Subtotal Storm Drain					\$ 383,095.00		\$ 383,095.00
Potable Water Distribution System							
1	175	LF	6" Water Main, PVC C900 CL 235	\$ 50.00	\$ 8,750.00	0%	\$ 8,750.00
2	3,590	LF	8" Water Main, PVC C900 CL 235	\$ 55.00	\$ 197,450.00	0%	\$ 197,450.00
3	291	LF	12" Water Main, PVC C900 CL 235	\$ 70.00	\$ 20,370.00	0%	\$ 20,370.00
4	1	EA	6" Gate Valve	\$ 1,500.00	\$ 1,500.00	0%	\$ 1,500.00
5	23	EA	8" Gate Valve	\$ 2,000.00	\$ 46,000.00	0%	\$ 46,000.00
6	1	EA	12" Butterfly Valve	\$ 2,500.00	\$ 2,500.00	0%	\$ 2,500.00
7	1	EA	Remove 4" Blow-Off Valve & Connect	\$ 500.00	\$ 500.00	0%	\$ 500.00
8	13	EA	Fire Hydrant Assembly (6" Lead & Appurtenances)	\$ 5,000.00	\$ 65,000.00	0%	\$ 65,000.00
9	115	EA	1" Water Service	\$ 1,000.00	\$ 115,000.00	0%	\$ 115,000.00
10	2	EA	4" Blow-Off Valve & Box	\$ 2,500.00	\$ 5,000.00	0%	\$ 5,000.00
Subtotal Water					\$ 462,070.00		\$ 462,070.00
Concrete							
1	7,068	LF	Modified Type 1 Rolled Curb & Gutter	\$ 20.00	\$ 141,360.00	0%	\$ 141,360.00
2	469	LF	Modified Type 2 Vertical Curb & Gutter (w/ 6" AB)	\$ 20.00	\$ 9,380.00	0%	\$ 9,380.00
3	17	LF	Type 5 Median Curb	\$ 15.00	\$ 255.00	0%	\$ 255.00
4	91	LF	Modified Flush Curb	\$ 10.00	\$ 910.00	0%	\$ 910.00
5	25	EA	Sidewalk Curb Ramp	\$ 2,500.00	\$ 62,500.00	0%	\$ 62,500.00
6	123	LF	Mountable Curb & Gutter (w/ 6" AB)	\$ 25.00	\$ 3,075.00	0%	\$ 3,075.00
7	28,987	SF	Sidewalk (6" PCC/ 6" AB)	\$ 6.00	\$ 173,922.00	0%	\$ 173,922.00
Subtotal Concrete					\$ 391,402.00		\$ 391,402.00



FOLSOM PLAN AREA
 Cost Estimate for
 MANGINI RANCH PH1C-SOUTH

Item No	Quantity	Unit	Description	Unit Price	Total	% Complete	Cost to Complete
Streetwork							
1	2,401	Ton	Asphalt Concrete (Type 'B')	\$ 80.00	\$ 192,080.00	0%	\$ 192,080.00
2	8,404	Ton	Aggregate Base (Class 2)	\$ 20.00	\$ 168,080.00	0%	\$ 168,080.00
3	6,313	SF	Bike Path (3"AC Type B/6" AB Class 2)	\$ 2.30	\$ 14,519.90	0%	\$ 14,519.90
4	840	LF	Post & Cable Barrier	\$ 15.00	\$ 12,600.00	0%	\$ 12,600.00
5	402	SF	Pavement Markings	\$ 5.00	\$ 2,010.00	0%	\$ 2,010.00
6	808	LF	4" Centerline Pattern (DTL 1)	\$ 1.00	\$ 808.00	0%	\$ 808.00
7	138	LF	12" Limit Line (DTL LL)	\$ 1.50	\$ 207.00	0%	\$ 207.00
8	6	LF	Removable Bollard	\$ 300.00	\$ 1,800.00	0%	\$ 1,800.00
9	2	EA	Miscellaneous Signs	\$ 300.00	\$ 600.00	0%	\$ 600.00
10	2	EA	Stop Sign (R1-1) on Street Light (Sign Only)	\$ 300.00	\$ 600.00	0%	\$ 600.00
11	3	EA	Street Name Sign on Street Light (Sign Only)	\$ 300.00	\$ 900.00	0%	\$ 900.00
12	8	EA	Stop Sign (R1-1) On Post	\$ 500.00	\$ 4,000.00	0%	\$ 4,000.00
13	6	EA	Street Name Sign On Post	\$ 500.00	\$ 3,000.00	0%	\$ 3,000.00
Subtotal Streetwork					\$ 401,205.00		\$ 401,205.00
Street Lights & Joint Trench							
1	115	LOT	Joint Trench Excavation & Backfill	\$ 8,000.00	\$ 920,000.00	0%	\$ 920,000.00
2	1	EA	Streetlight Service Point	\$ 10,500.00	\$ 10,500.00	0%	\$ 10,500.00
3	22	EA	LED Street Light (including Conduit, Wiring and Appurtenances)	\$ 11,600.00	\$ 255,200.00	0%	\$ 255,200.00
Subtotal Street Lights & Joint Trench					\$ 1,185,700.00		\$ 1,185,700.00
Landscaping & Sound walls							
1	23,412	SF	Landscape & Irrigation	\$ 5.00	\$ 117,060.00	0%	\$ 117,060.00
2	990	LF	6' Sound Walls	\$ 210.00	\$ 207,900.00	0%	\$ 207,900.00
3	1,100	LF	10' Sound Walls	\$ 350.00	\$ 385,000.00	0%	\$ 385,000.00
Subtotal Landscaping & Sound walls					\$ 709,960.00		\$ 709,960.00
Total					\$ 4,011,172.00		\$ 4,011,172.00
Contingency				10%	\$ 401,120.00		\$ 401,120.00
Total Cost Estimate					\$ 4,412,292.00		\$ 4,412,292.00

Summary MR1C SOUTH

	Total Cost	Cost to Complete
Site Preparation & Earthwork	\$ 36,000.00	\$ 36,000.00
Sewer	\$ 441,740.00	\$ 441,740.00
Storm Drain	\$ 383,095.00	\$ 383,095.00
Potable Water	\$ 462,070.00	\$ 462,070.00
Concrete	\$ 391,402.00	\$ 391,402.00
Streetwork	\$ 401,205.00	\$ 401,205.00
Street Lights & Joint Trench	\$ 1,185,700.00	\$ 1,185,700.00
Landscaping & Sound walls	\$ 709,960.00	\$ 709,960.00
Contingency	\$ 401,120.00	\$ 401,120.00
TOTALS	\$ 4,412,292.00	\$ 4,412,292.00

BOND # _____
 PREMIUM: _____

PERFORMANCE BOND
 for
Subdivision Improvement Agreement

WHEREAS, The City Council of the City of Folsom, a Municipal Corporation in the State of California, and **Arcadian Improvement Company, LLC., a California Limited Liability Company**, (hereinafter designated as "Principal") have entered into an agreement where by principal agrees to install and complete certain designated public improvements, which said agreement, dated _____ 2022, and identified as the **Mangini Ranch Phase 1C South Village No. 4 Subdivision Improvement Agreement** is hereby referred to and made a part hereof; and,

WHEREAS, Said Principal is required under the terms of said agreement to furnish a bond for the faithful performance of said agreement;

NOW THEREFORE, We, the principal, and _____, as surety, are held and firmly bound unto the City of Folsom, hereinafter referred to as the City; in the penal sum of **FOUR MILLION FOUR HUNDRED TWELVE THOUSAND TWO HUNDRED NINETY - TWO AND 00/100 DOLLARS (\$4,412,292.00)**, lawful money of the United States, for the payment of which sum well and truly to be made, we bind ourselves, our heirs, successors, executors, and administrators, jointly and severally firmly by these presents.

The condition of this obligation is such that if the above bounded principal, its heirs, executors, administrators, successors or assigns, shall in all things stand to and abide by, and well and truly keep and perform the covenants, conditions, and provisions in the said agreement and any alteration thereof made as therein provided, on its part, to be kept and performed at the time and in the manner therein specified, and in all respects according to their true intent and meaning, and shall indemnify and save harmless the City, its officers, agents, and employees, as therein stipulated, then this obligation shall become null and void; otherwise it shall be and remain in full force and effect.

As a part of the obligation secured hereby and in addition to the face amount specified therefor, there shall be included costs and reasonable expenses and fees, including reasonable attorney's fees, incurred by City in successful enforcing such obligation, all to be taxed as costs and included in any judgment rendered.

The surety hereby stipulates and agrees that no change, extension of time, alteration or addition to the terms of the agreement or to the work to be performed thereunder or the specifications accompanying the same shall in any way affect its obligations on this bond, and it does hereby waive notice of any such change, extension of time, alteration or addition to the terms of the agreement or to the work or to the specifications.

IN WITNESS WHEREOF, this instrument has been duly executed by the principal and surety above named, on _____, 2022.

BY _____
(PRINCIPAL)

BY _____
(PRINCIPAL)

BY _____
(SURETY)

(ADDRESS)

(CITY, STATE, ZIP)

(TELEPHONE)

APPROVED AS TO FORM

CITY ATTORNEY

BOND # _____
 PREMIUM: _____

LABOR & MATERIALS BOND
 for
Subdivision Improvement Agreement

WHEREAS, The City Council of the City of Folsom, a Municipal Corporation of the State of California, and **Arcadian Improvement Company, LLC, a California Limited Liability Company** (hereinafter designated as "Principal"), have entered into an agreement whereby principal agrees to install and complete certain designated public improvements, which said agreement, dated _____ 2022, and identified as the **Mangini Ranch Phase 1C South Village No. 4 Subdivision Improvement Agreement** is hereby referred to and made a part hereof; and,

WHEREAS, under the terms of said agreement, principal is required before entering upon the performance of the work, to file a good and sufficient payment bond with the City of Folsom to secure the claims to which reference is made in Title 15 (commencing with Section 3082) of Part 4 of Division 3 of the Civil Code of the State of California;

NOW THEREFORE, said principal and the undersigned as corporate surety, are held firmly bound unto the City of Folsom and all contractors, subcontractors, laborers, materialmen and other persons employed in the performance of the aforesaid agreement and referred to in the aforesaid Code of Civil Procedure, in the sum of **FOUR MILLION FOUR HUNDRED TWELVE THOUSAND TWO HUNDRED NINETY - TWO AND 00/100 DOLLARS (\$4,412,292.00)** for materials furnished or labor thereon of any kind, or for amounts due under the Unemployment Insurance Act with respect to such work or labor, that said surety will pay the same in an amount not exceeding the amount hereinabove set forth, and also in case suit is brought upon this bond, will pay, in addition to the face amount thereof, cost and reasonable expenses and fees, including reasonable attorney's fees, incurred by City in successfully enforcing such obligation, to be awarded and fixed by the court, and to be taxed as costs and to be included in the judgment therein rendered.

It is hereby expressly stipulated and agreed that this bond shall inure to the benefit of any and all persona, companies and corporations entitled to file claims under Title 15 (commencing with Section 3082) of Part 4 of Division 3 of the Civil Code, so as to give a right of action to them or their assigns in any suit brought upon this bond.

Should the condition of this bond be fully performed, then this obligation shall become null and void, otherwise it shall be and remain in full force and effect.

The surety hereby stipulates and agrees that no change, extension of time, alteration, or addition to the terms of said agreement or the specifications accompanying the same shall in any manner affect its obligations on this bond, and it does hereby waive notice of any such change, extension, alteration, or addition.

IN WITNESS WHEREOF, this instrument has been duly executed by the principal and surety above named, on _____, 2022.

BY _____
(PRINCIPAL)

BY _____
(PRINCIPAL)

BY _____
(SURETY)

(ADDRESS)

(CITY, STATE, ZIP)

(TELEPHONE)

APPROVED AS TO FORM

CITY ATTORNEY

ATTACHMENT 3

MANGINI RANCH PHASE 1C SOUTH VILLAGE NO. 4 FINAL MAP

OWNER'S STATEMENT

THE UNDERSIGNED DOES HEREBY STATE THAT I AM THE ONLY PARTY HAVING ANY RECORD TITLE INTEREST IN THE REAL PROPERTY INCLUDED WITHIN THE BOUNDARIES OF THIS FINAL MAP OF "MANGINI RANCH PHASE 1C SOUTH - VILLAGE 4" AND DO HEREBY DECLARE THE CONSENT FROM NO OTHER PERSON IS NECESSARY, AND I CONSENT TO THE PREPARATION AND RECORDATION OF THIS FINAL MAP AND OFFER FOR DEDICATION AND DO HEREBY DEDICATE AS PUBLIC RIGHT-OF-WAYS AND AS PUBLIC UTILITY EASEMENTS TO THE CITY OF FOLSOM, MANGINI PARKWAY, SAVANNAH PARKWAY, NIGHTBERRY DRIVE, RIATA DRIVE, LUKELLA DRIVE, HAPPY HOLLOW DRIVE, WILLIAMSON DRIVE, HAMPTON DRIVE AND CARAWAY COURT AS SHOWN HEREON.

WE DO HEREBY DEDICATE FOR SPECIFIC PURPOSES THE FOLLOWING:

- 1. A PUBLIC EASEMENT FOR THE INSTALLATION AND MAINTENANCE OF DRAIN, GAS, SEWER AND WATER PIPES, AND FOR UNDERGROUND WIRES AND CONDUITS FOR ELECTRICAL, TELEVISION AND COMMUNICATIONS SERVICES. TOGETHER WITH ANY AND ALL APPURTENANCES PERTAINING THERETO ON, OVER, UNDER AND ACROSS LOTS A, B, C, D, E, F AND L AND THOSE STRIPS OF LAND SHOWN HEREON AND DESIGNATED "PUBLIC UTILITY EASEMENT" (P.U.E.)
2. A PUBLIC EASEMENT AND RIGHT-OF-WAY FOR THE INSTALLATION, REPAIR, REMOVAL OR REPLACEMENT OF LANDSCAPING TOGETHER WITH ANY AND ALL APPURTENANCES PERTAINING THERETO ON, OVER, UNDER AND ACROSS LOTS D, E, AND F AND THOSE STRIPS OF LAND SHOWN HEREON AND DESIGNATED "LANDSCAPE EASEMENT" (L.E.)
3. A PUBLIC EASEMENT FOR THE INSTALLATION AND MAINTENANCE OF SIDEWALK AND PEDESTRIAN ACCESS ON, OVER AND ACROSS LOTS D, E AND F AND THOSE STRIPS OF LAND SHOWN HEREON AND DESIGNATED "PEDESTRIAN ACCESS EASEMENT" (P.A.E.)
4. A PUBLIC EASEMENT FOR CONSTRUCTION AND MAINTAINING CENTRALIZED MAIL DELIVERY BOXES, PEDESTALS AND SLABS TOGETHER WITH ANY AND ALL APPURTENANCES PERTAINING THERETO INCLUDING PEDESTRIAN ACCESS FOR DELIVERY AND RECEIPT OF MAIL ON, OVER, AND ACROSS STRIPS OF LAND FIVE (5) FEET IN WIDTH CONTIGUOUS TO ALL RIGHT-OF-WAYS.

ARCADIAN IMPROVEMENT COMPANY, LLC. A CALIFORNIA LIMITED LIABILITY COMPANY

BY: NAME: TITLE: DATE

NOTARY'S ACKNOWLEDGMENT

A NOTARY PUBLIC OR OTHER OFFICER COMPLETING THIS CERTIFICATE VERIFIES ONLY THE IDENTITY OF THE INDIVIDUAL WHO SIGNED THE DOCUMENT TO WHICH THIS CERTIFICATE IS ATTACHED, AND NOT THE TRUTHFULNESS, ACCURACY OR VALIDITY OF THAT DOCUMENT.

STATE OF CALIFORNIA COUNTY OF ON BEFORE ME, A NOTARY PUBLIC

PERSONALLY APPEARED WHO PROVED TO ME ON THE BASIS OF SATISFACTORY EVIDENCE TO BE THE PERSON(S) WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE WITHIN INSTRUMENT AND ACKNOWLEDGED TO ME THAT HE/SHE/THEY EXECUTED THE SAME IN HIS/HER/THEIR AUTHORIZED CAPACITY(IES), AND THAT BY HIS/HER/THEIR SIGNATURE(S) ON THE INSTRUMENT THE PERSON(S), OR THE ENTITY UPON BEHALF OF WHICH THE PERSON(S) ACTED, EXECUTED THE INSTRUMENT.

I CERTIFY UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE STATE OF CALIFORNIA THAT THE FOREGOING PARAGRAPH IS TRUE AND CORRECT.

WITNESS MY HAND AND OFFICIAL SEAL

SIGNATURE PRINTED NAME MY PRINCIPAL PLACE OF BUSINESS IS COUNTY MY COMMISSION EXPIRES: MY COMMISSION NUMBER:



VICINITY MAP N.T.S.

NAVD88 BENCHMARK-CITY OF FOLSOM

BENCHMARK " 73 " ELEVATION = 500.02' NAVD88

BRASS DISK STAMPED "CITY OF FOLSOM, BM 73" ON THE NORTHWEST CORNER OF A CONCRETE DRAINAGE STRUCTURE. LOCATION OF SITE IS APPROXIMATELY 300 FEET SOUTHEASTERLY OF GRAND PRAIRIE ROAD AND 50 FEET EAST OF THE SPTC/PA RAILROAD TRACKS. APPROXIMATE LATITUDE: N38° 37' 58.89", LONGITUDE: W121° 05' 54.25"

ELEVATION OF 500.02 WAS ESTABLISHED BY A DIFFERENTIAL LEVEL CIRCUIT FROM COUNTY BENCHMARK U01B-009 STAMPED "K-686" IN FEBRUARY 2014 BY MACKAY AND SOMPS CIVIL ENGINEERS, INC.

Preliminary 04/07/2022 7:22:49 AM

SURVEYOR'S STATEMENT

THIS MAP WAS PREPARED BY ME OR UNDER MY DIRECTION AND IS BASED UPON A FIELD SURVEY IN CONFORMANCE WITH THE REQUIREMENTS OF THE SUBDIVISION MAP ACT AND LOCAL ORDINANCE AT THE REQUEST OF ARCADIAN IMPROVEMENT COMPANY, LLC, A CALIFORNIA LIMITED LIABILITY COMPANY IN JULY 2021. I HEREBY STATE THAT THIS FINAL MAP SUBSTANTIALLY CONFORMS TO THE CONDITIONALLY APPROVED TENTATIVE MAP; THAT THE MONUMENTS WILL BE OF THE CHARACTER AND WILL OCCUPY THE POSITIONS AS INDICATED AND WILL BE SET BY DECEMBER 31, 2023; AND THAT SAID MONUMENTS WILL BE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.

MACKAY & SOMPS CIVIL ENGINEERS, INC.



PAUL FERGUSON, JR. P.L.S. 9265 EXP. 03-31-2024 DATE:

CITY ENGINEER'S STATEMENT

I HEREBY STATE THAT I HAVE EXAMINED THIS FINAL MAP OF "MANGINI RANCH PHASE 1C SOUTH - VILLAGE 4" AND FIND IT TO BE SUBSTANTIALLY THE SAME AS THE TENTATIVE MAP APPROVED BY THE CITY COUNCIL OF THE CITY OF FOLSOM, AND THAT ALL PROVISIONS OF THE SUBDIVISION MAP ACT AND ALL APPLICABLE CITY ORDINANCES HAVE BEEN COMPLIED WITH.

STEVEN R. KRAHN, RCE 49291 CITY ENGINEER CITY OF FOLSOM LICENSE EXPIRES: 9/30/2022

DATE:

CITY SURVEYOR'S STATEMENT

I HEREBY STATE THAT I HAVE EXAMINED THIS FINAL MAP OF "MANGINI RANCH PHASE 1C SOUTH - VILLAGE 4" AND I AM SATISFIED THAT SAID MAP IS TECHNICALLY CORRECT.

GERALD A. YOUNG, L.S. 3852 CITY SURVEYOR LICENSE EXPIRES: 6/30/2022

DATE:

CITY CLERK'S STATEMENT

I HEREBY STATE THAT THE CITY COUNCIL OF THE CITY OF FOLSOM HAS APPROVED THIS FINAL MAP OF "MANGINI RANCH PHASE 1C SOUTH - VILLAGE 4", AND HAS ACCEPTED, ON BEHALF OF THE PUBLIC, SUBJECT TO IMPROVEMENTS, ALL RIGHT-OF-WAYS AND EASEMENTS OFFERED HEREON FOR DEDICATION IN ACCORDANCE WITH THE TERMS OF THAT OFFER AND HAS APPROVED THE ABANDONMENT OF THE EASEMENTS LISTED HEREON.

CHRISTA FREEMANTLE CITY CLERK

DATE:

RECORDER'S STATEMENT

FILED THIS DAY OF 2022, AT M. IN BOOK OF MAPS, AT PAGE AT THE REQUEST OF MACKAY & SOMPS CIVIL ENGINEERS, INC. TITLE TO THE LAND INCLUDED IN THIS FINAL MAP BEING VESTED AS PER CERTIFICATE NO. ON FILE IN THIS OFFICE.

DOCUMENT NO.: DONNA ALLRED SACRAMENTO COUNTY RECORDER STATE OF CALIFORNIA

BY: DEPUTY FEE: \$

FINAL MAP (PN 21-086) MANGINI RANCH PHASE 1C SOUTH - VILLAGE 4

BEING A MERGER AND RESUBDIVISION OF LOTS 13, 27 AND 28 AS SHOWN AND SO DESIGNATED ON THAT CERTAIN MAP TITLED "FINAL MAP (PN 14-071) MANGINI RANCH LARGE LOT" FILED FOR RECORD ON APRIL 7, 2017 IN BOOK 395 OF MAPS, AT PAGE 10, SACRAMENTO COUNTY RECORDS AND PARCEL 1 DESCRIBED IN GRANT DEED RECORDED AS DOCUMENT NO. 201904260702, O.R.S.C., SITUATED IN SECTIONS 15, 16, 21 & 22, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

CITY OF FOLSOM • SACRAMENTO COUNTY • CALIFORNIA



MARCH 2022

SHEET 1 OF 7

TRUSTEE'S STATEMENT

PLACER TITLE COMPANY, A CALIFORNIA CORPORATION, AS TRUSTEE UNDER THAT CERTAIN DEED OF TRUST AND ASSIGNMENT OF RENTS, RECORDED DECEMBER 30, 2020, (INSTRUMENT) 202012301820, O.R.S.C., MODIFIED BY DOCUMENT NO. 202107271732 AND DOCUMENT NO. 202109202676, O.R.S.C., HEREBY CONSENTS TO THE RECORDATION OF THIS MAP AND THE SUBDIVISION OF THE LANDS SHOWN HEREON.

NAME: _____ DATE: _____
 TITLE: _____

NOTARY'S ACKNOWLEDGMENT

A NOTARY PUBLIC OR OTHER OFFICER COMPLETING THIS CERTIFICATE VERIFIES ONLY THE IDENTITY OF THE INDIVIDUAL WHO SIGNED THE DOCUMENT TO WHICH THIS CERTIFICATE IS ATTACHED, AND NOT THE TRUTHFULNESS, ACCURACY OR VALIDITY OF THAT DOCUMENT.

STATE OF CALIFORNIA
 COUNTY OF _____
 ON _____ BEFORE ME, _____, A NOTARY PUBLIC

PERSONALLY APPEARED
 WHO PROVED TO ME ON THE BASIS OF SATISFACTORY EVIDENCE TO BE THE PERSON(S) WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE WITHIN INSTRUMENT AND ACKNOWLEDGED TO ME THAT HE/SHE/THEY EXECUTED THE SAME IN HIS/HER/THEIR AUTHORIZED CAPACITIES, AND THAT BY HIS/HER/THEIR SIGNATURE(S) ON THE INSTRUMENT THE PERSON(S), OR THE ENTITY UPON BEHALF OF WHICH THE PERSON(S) ACTED, EXECUTED THE INSTRUMENT.

I CERTIFY UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE STATE OF CALIFORNIA THAT THE FOREGOING PARAGRAPH IS TRUE AND CORRECT.

WITNESS MY HAND AND OFFICIAL SEAL.
 SIGNATURE _____ PRINTED NAME _____
 MY PRINCIPAL PLACE OF BUSINESS IS _____ COUNTY _____
 MY COMMISSION EXPIRES: _____ MY COMMISSION NUMBER: _____

NOTES

- ALL CURVE DIMENSIONS ARE RADIUS, DELTA AND ARC LENGTH; ALL DISTANCES SHOWN ARE GROUND DISTANCES AND ARE IN FEET AND DECIMALS THEREOF. DUE TO ROUNDING THE SUM OF INDIVIDUAL DIMENSIONS MAY NOT EQUAL THE OVERALL DIMENSION.
- THIS FINAL MAP CONTAINS 26.919± ACRES GROSS CONSISTING OF 115 RESIDENTIAL LOTS AND 12 LETTERED LOTS.
- A PRELIMINARY GEOTECHNICAL ENGINEERING REPORT FOR THE MANGINI RANCH (PROJECT NO. E07145 001) WAS PREPARED BY YOUNGDAHL CONSULTING GROUP, INC. IN JULY, 2014 AND IS AVAILABLE FOR PUBLIC INSPECTION AT THE CITY OF FOLSOM COMMUNITY DEVELOPMENT DEPARTMENT.
- FRONT LOT CORNERS WILL BE SET (UNLESS SHOWN OTHERWISE)
 - ALONG STREETS WITH ATTACHED SIDEWALK, CORNERS WILL BE SET WITH A 1" DIAMETER BRASS DISC STAMPED "LS 9286" ON A 1.00 FOOT OFFSET ONTO THE SIDEWALK ON THE SIDE PROPERTY LINE EXTENDED (SEE DETAIL THIS SHEET).
 - ALONG 20' DRIVES WITH A CURB AND GUTTER, CORNERS WILL BE SET WITH A 1" DIAMETER BRASS DISC STAMPED "LS 9265" ON A 1.00 FOOT OFFSET ONTO GUTTER PAN.
 - ALONG 20' DRIVES WITH A FLUSH CURB AND NO GUTTER, CORNERS WILL BE SET WITH A 1" DIAMETER BRASS DISC STAMPED "LS 9265" ON A 0.30 FOOT OFFSET ONTO FLUSH CURB.
- REAR CORNERS WILL BE SET AS FOLLOWS (UNLESS SHOWN OTHERWISE)
 - FOR LOTS 6-23, 46-80, 89/90/91 AND 103/104 WILL BE SET WITH A 5/8" REBAR AND PLASTIC CAP STAMPED "LS 9265".
 - FOR LOTS 1-5, 24-45, 81-89, 94-102, 105-115 WILL BE SET WITH A 5/8" REBAR AND PLASTIC CAP STAMPED "LS 9265" OFFSET 4.00 FEET ON THE SIDE LOT LINE (SEE DETAIL THIS SHEET).
 - FOR THE ANGLE POINTS AT LOTS 92 AND 93 WHICH FALL WITHIN A MASONRY SOUND/RETAINING WALL, WILL BE SET WITH A 3/4" BRASS TAG STAMPED "LS 9265" TO THE FACE OF WALL 2.00 FOOT ABOVE GROUND OR ON TOP OF THE WALL.
- PROPERTY SUBJECT TO CFD 2013-1 (WATER FACILITIES AND SUPPLY) PER 20131230 O.R. 0311.
- PROPERTY SUBJECT TO COMMUNITY FACILITIES DISTRICT NO. 17 (WILLOW HILL PIPELINE) PER 2015025 O.R. 0353.
- PROPERTY SUBJECT TO COMMUNITY FACILITIES DISTRICT NO. 19 (FOLSOM PLAN AREA - AREA WIDE IMPROVEMENTS AND SERVICES) PER 20151209 O.R. 0427 AND 20181116 O.R. 465.
- PROPERTY SUBJECT TO COMMUNITY FACILITIES DISTRICT NO. 19 (MANGINI RANCH) PER 20160113 O.R. 159.
- PROPERTY SUBJECT TO COMMUNITY FACILITIES DISTRICT NO. 23 (FOLSOM RANCH IMPROVEMENT AREA NO. 2) PER DOCUMENT NO. 202006081610, O.R.
- PROPERTY SUBJECT TO "WATER SUPPLY AND FACILITIES FINANCING PLAN AND AGREEMENT" PER 20130124 O.R. 1382 AND DOCUMENTS DECLARING MODIFICATIONS THEREOF PER 20140603 O.R. 0959, 20140603 O.R. 0960, AND 20151211 O.R. 0142.
- PROPERTY SUBJECT TO THE TERMS, CONDITIONS, PROVISIONS AND STIPULATIONS AS CONTAINED IN THE AGREEMENT ENTITLED "FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT AGREEMENT" PER 20140715 O.R. 426, AND AS AMENDED BY 20160129 O.R. 0361, 20160129 O.R. 0362, DN 202010061511, DN 202012301814, AND DN 202012301817.
- PROPERTY SUBJECT TO PERPETUAL, NONEXCLUSIVE EASEMENT FOR AVIGATION AND INCIDENTAL PURPOSES IN FAVOR OF COUNTY OF SACRAMENTO AND CITY OF FOLSOM PER 20140715 O.R. 427 AND 20140715 O.R. 547.
- PROPERTY SUBJECT TO COVENANTS, CONDITIONS AND RESTRICTIONS PER 20020912 O.R. 282 AND 20070509 O.R. 525.
- PROPERTY SUBJECT TO AN "AGREEMENT AFFECTING REAL PROPERTY (INCLUSIONARY HOUSING AGREEMENT)" PER 20150710 O.R. 0641.
- PROPERTY SUBJECT TO TERMS, CONDITIONS AND PROVISIONS AS CONTAINED IN "GRANT OF EASEMENT AND EASEMENT AGREEMENT" PER DOCUMENT NO. 202012301821, O.R.
- PROPERTY SUBJECT TO COMMUNITY FACILITIES DISTRICT SCHOOL FACILITIES IMPROVEMENT DISTRICT NO. 3 PER 20060707 O.R. 9582.
- PROPERTY SUBJECT TO THE TERMS AND PROVISIONS CONTAINED IN "THOSE DOCUMENTS ENTITLED "TIER 1 DEVELOPMENT AGREEMENT" PER 2010803 O.R. 0422, "ASSIGNMENT AND ASSUMPTION AGREEMENT RELATIVE TO THE FOLSOM SOUTH SPECIFIC PLAN TIER 1 DEVELOPMENT AGREEMENT" PER 20121012 O.R. 1351, 1355 & 1357 AND DOCUMENTS DECLARING MODIFICATIONS THEREOF PER 20140715 O.R. 0361, 0410 & 0426 AND PER 20160129 O.R. 0378, 0379, 0381 & 0382.
- PROPERTY SUBJECT TO THE TERMS, CONDITIONS, PROVISIONS, EASEMENTS AND STIPULATIONS AS CONTAINED IN THE AGREEMENT ENTITLED "GRANT OF EASEMENT AND EASEMENT AGREEMENT" BY AND BETWEEN ARCADIAN IMPROVEMENT COMPANY, LLC, A CALIFORNIA LIMITED LIABILITY COMPANY AND TRI POINTE HOMES HOLDINGS, INC. A DELAWARE CORPORATION, RECORDED SEPTEMBER 2, 2021, (INSTRUMENT) 202109020677, OFFICIAL RECORDS.
- PROPERTY SUBJECT TO AN EASEMENT OVER SAID LAND FOR TEMPORARY CONSTRUCTION EASEMENT AND INCIDENTAL PURPOSES, AS IN FAVOR OF TRI POINTE HOMES HOLDINGS, INC., A DELAWARE CORPORATION, IN DEED RECORDED SEPTEMBER 2, 2021, (INSTRUMENT) 202109020677, OFFICIAL RECORDS.
- PROPERTY SUBJECT TO THE CITY OF FOLSOM COMMUNITY FACILITIES DISTRICT NO. 2014-1 (mPOWER PACE PROGRAM), AS DISCLOSED BY THAT CERTAIN ASSESSMENT MAP, RECORDED DECEMBER 23, 2013, AS BOOK 116, PAGE 7 PER 20131223 O.R. 0305.
- PROPERTY SUBJECT TO THE CALIFORNIA HOME FINANCE AUTHORITY COMMUNITIES FACILITIES DISTRICT NO. 2014-1 (CLEAN ENERGY), AS DISCLOSED BY THAT CERTAIN ASSESSMENT MAP, RECORDED OCTOBER 05, 2015, AS (BOOK) 20151005, (PAGE) 0763.
- LOTS A, B, C, D, E, F, G, H, I, J, K AND L AS SHOWN PER THIS MAP ARE TO BE DEEDED TO THE CITY OF FOLSOM EITHER CONCURRENT OR FOLLOWING RECORDATION OF THIS MAP BY SEPARATE DOCUMENT.

NOTES (CONTINUED)

- THE EASEMENTS LISTED BELOW, NOT SHOWN HEREON, ARE HEREBY ABANDONED PER SECTION 65434 (G) OF THE GOVERNMENT CODE.
 - 12.5' P.U.E. OVER LOTS 27 AND 28 PER (1)
 - 30' LE/PAE/PUE ALONG THE SOUTHEASTERLY RIGHT-OF-WAY OF MANGINI PARKWAY AND THE SOUTHWESTERLY RIGHT-OF-WAY OF SAVANNAH PARKWAY PER 395 B.M. 10
 - PORTIONS OF THE DRAINAGE EASEMENT PER 20180630 O.R. 0260 OVER SAVANNAH PARKWAY, LOTS A, B, D, I AND 17

BASIS OF BEARINGS

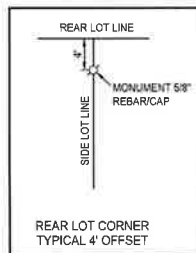
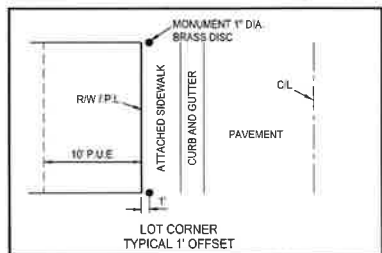
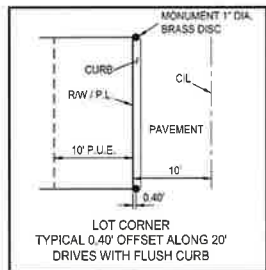
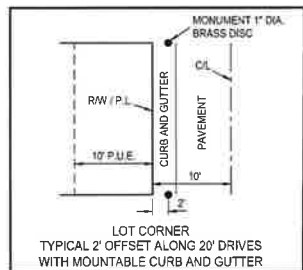
THE BASIS OF BEARINGS FOR THIS MAP IS THE NORTHWESTERLY LINE OF LOTS 12 AND 27 AS SHOWN ON SHEET 5 OF 19 SHEETS ON THAT FINAL MAP TITLED "FINAL MAP (PN 14-071) MANGINI RANCH LARGE LOT" FILED FOR RECORD ON APRIL 7, 2017 IN BOOK 395 OF MAPS, AT PAGE 10, SACRAMENTO COUNTY RECORDS AND HAVING A BEARING OF NORTH 51°09'23" EAST BETWEEN THE FOUND MONUMENTS SHOWN HEREON.

REFERENCES

- | | |
|---|----------------|
| (1) 395 B.M. 10 (MANGINI RANCH LARGE LOT) | (4) 94 R.S. 14 |
| (2) 438 B.M. 4 (PHASE 1C VILLAGE 3) | (5) 87 R.S. 20 |
| (3) 407 B.M. 5 | |

LEGEND

◊	DIMENSION POINT
◆	SECTION CORNER MARKED BY A FOUND 2" ALUMINUM CAP ON A 5/8" REBAR STAMPED "LS 4533" AS SHOWN ON (1)
⊗	STANDARD CITY OF FOLSOM MONUMENT WELL STAMPED "LS 5780" TO BE SET PER 418 B.M. 9
⊗	FOUND 5/8" REBAR WITH PLASTIC CAP STAMPED "LS 4533"(1)
⊗	3/4" IRON PIPE WITH PLASTIC PLUG STAMPED "LS 9265" TO BE SET PER (2)
⊗	STANDARD CITY OF FOLSOM MONUMENT WELL STAMPED "LS 7944" TO BE SET PER (1)
⊗	1-1/4" IRON PIPE WITH PLASTIC PLUG STAMPED "LS 7944" PER (1)
⊗	FOUND 5/8" REBAR WITH METAL CAP STAMPED "LS 5760" PER (4)
⊗	FOUND MONUMENT AS NOTED
⊗	SET STANDARD CITY OF FOLSOM MONUMENT WELL STAMPED "LS 9285"
⊗	SET 3/4" IRON PIPE WITH PLASTIC PLUG STAMPED "LS 9265"
⊗	SET 3/4" BRASS TAG STAMPED "LS 9265"
()	RECORD DATA PER REFERENCE
A.E.	ACCESS EASEMENT
B.M.	BOOK OF MAPS
D.C.R.	DECLARATION OF COVENANTS AND RESTRICTIONS
D.E.	DRAINAGE EASEMENT
C/L	CENTER LINE
DN	DOCUMENT NUMBER, O.R.
L.E.	LANDSCAPE EASEMENT
(OA)	OVERALL
O.R.	OFFICIAL RECORDS OF SACRAMENTO COUNTY
O.S.	OPEN SPACE
P/L	PROPERTY LINE
P.A.E.	PEDESTRIAN ACCESS EASEMENT
P.U.E.	PUBLIC UTILITY EASEMENT
(R)	RADIAL BEARING
R.S.	RECORD OF SURVEY
R/W	RIGHT-OF-WAY
SF	SQUARE FEET
W.E.	WATER LINE EASEMENT
(X)	SHEET INDEX
—	BOUNDARY
—	LOT LINE & RIGHT-OF-WAY
—	LOT LINE
—	ADJACENT PROPERTY
—	RIGHT-OF-WAY (EXISTING)
—	EASEMENT LINE
—	SECTION LINE
—	CENTER LINE



FINAL MAP (PN 21-086)
MANGINI RANCH
PHASE 1C SOUTH - VILLAGE 4
 BEING A MERGER AND RESUBDIVISION OF LOTS 13, 27 AND 28 AS SHOWN AND SO DESIGNATED ON THAT CERTAIN MAP TITLED "FINAL MAP (PN 14-071) MANGINI RANCH LARGE LOT" FILED FOR RECORD ON APRIL 7, 2017 IN BOOK 395 OF MAPS, AT PAGE 10, SACRAMENTO COUNTY RECORDS AND PARCEL 1 DESCRIBED IN GRANT DEED RECORDED AS DOCUMENT NO. 201804260702, O.R.S.C., SITUATED IN SECTIONS 15, 16, 21 & 22, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.
 CITY OF FOLSOM • SACRAMENTO COUNTY • CALIFORNIA
Mackay & Somp
 ENGINEERS PLANNERS SURVEYORS
 122 Creekside Ridge Drive Suite 101 Roseville CA 95678 (916) 772-1189
 MARCH 2022
 SHEET 2 OF 7

FINAL MAP (PN 21-086)
MANGINI RANCH
PHASE 1C SOUTH - VILLAGE 4

BEING A MERGER AND RESUBDIVISION OF LOTS 13, 27 AND 28 AS SHOWN AND SO DESIGNATED ON THAT CERTAIN MAP TITLED "FINAL MAP (PN 14-071) MANGINI RANCH LARGE LOT" FILED FOR RECORD ON APRIL 7, 2017 IN BOOK 395 OF MAPS, AT PAGE 10, SACRAMENTO COUNTY RECORDS AND PARCEL 1 DESCRIBED IN GRANT DEED RECORDED AS DOCUMENT NO. 20190426702, O.R.S.C., SITUATED IN SECTIONS 15, 16, 21 & 22, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

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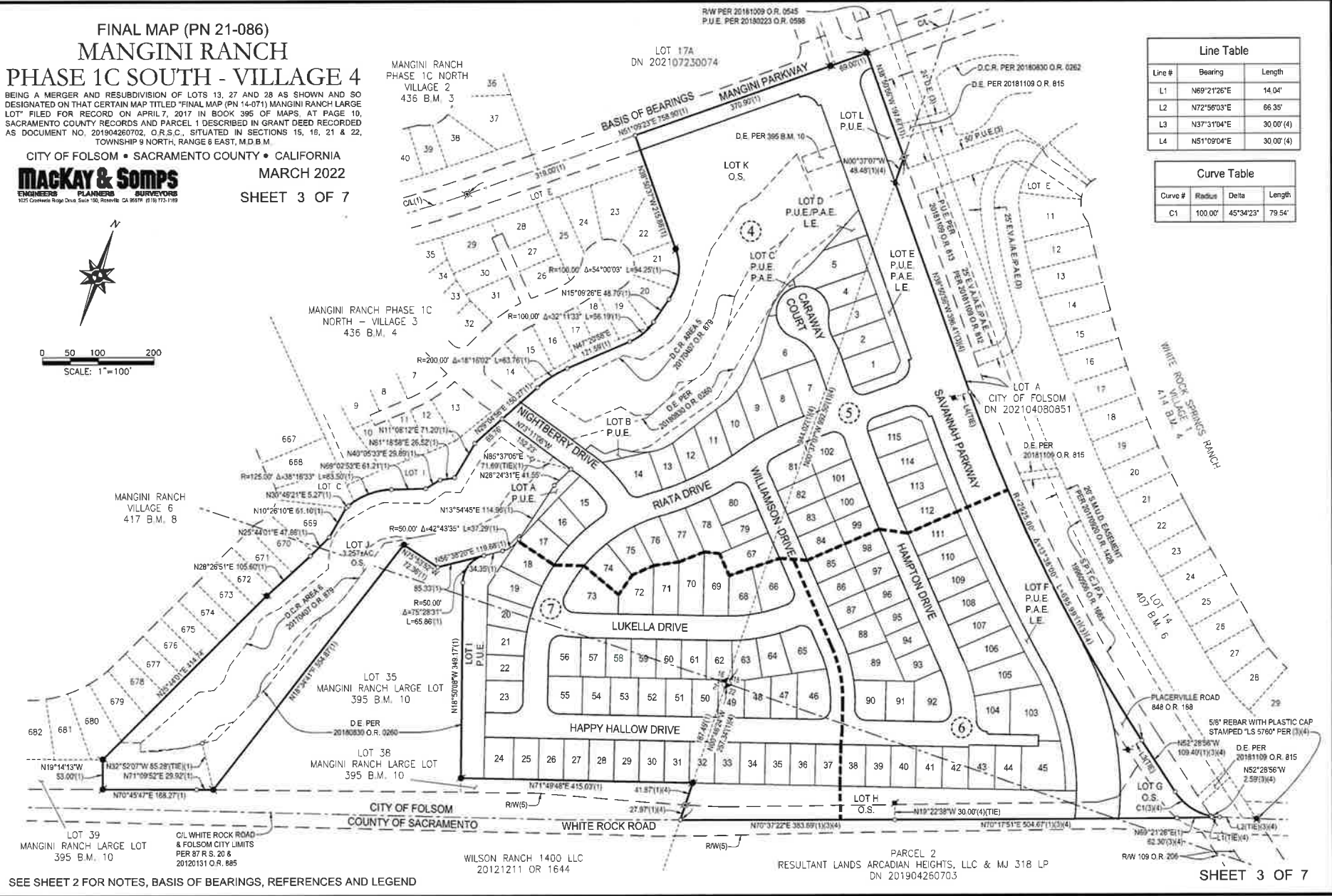
MARCH 2022
 SHEET 3 OF 7



0 50 100 200
 SCALE: 1"=100'

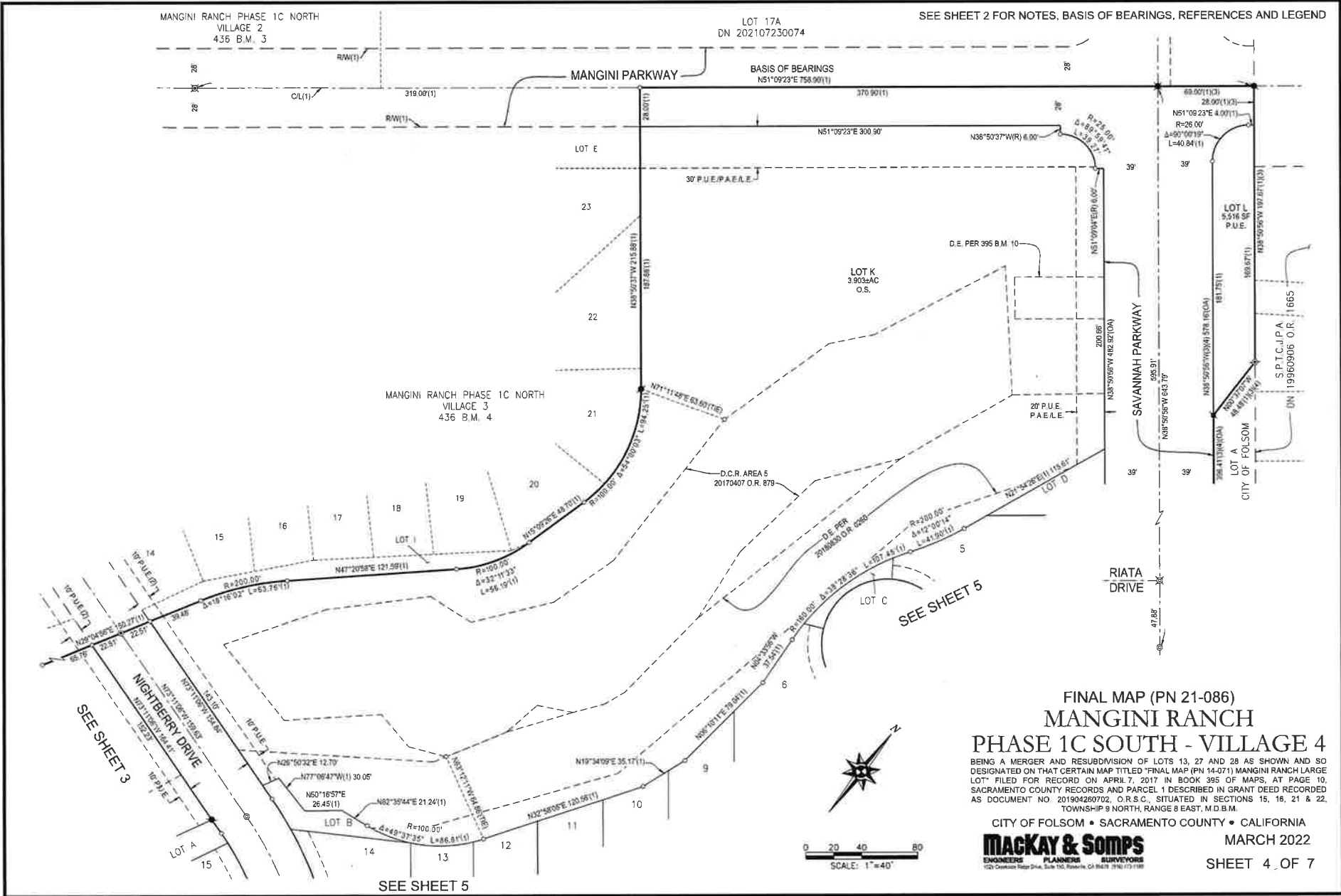
Line Table		
Line #	Bearing	Length
L1	N69°21'26"E	14.04'
L2	N72°56'03"E	66.35'
L3	N37°31'04"E	30.00'(4)
L4	N51°09'04"E	30.00'(4)

Curve Table			
Curve #	Radius	Delta	Length
C1	100.00'	45°34'23"	79.54'



SEE SHEET 2 FOR NOTES, BASIS OF BEARINGS, REFERENCES AND LEGEND

SHEET 3 OF 7



FINAL MAP (PN 21-086)
MANGINI RANCH
PHASE 1C SOUTH - VILLAGE 4

BEING A MERGER AND RESUBDIVISION OF LOTS 13, 27 AND 28 AS SHOWN AND SO DESIGNATED ON THAT CERTAIN MAP TITLED "FINAL MAP (PN 14-071) MANGINI RANCH LARGE LOT" FILED FOR RECORD ON APRIL 7, 2017 IN BOOK 395 OF MAPS, AT PAGE 10, SACRAMENTO COUNTY RECORDS AND PARCEL 1 DESCRIBED IN GRANT DEED RECORDED AS DOCUMENT NO. 201904260702, O.R.S.C., SITUATED IN SECTIONS 15, 16, 21 & 22, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

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Mackay & Somp
 ENGINEERS PLANNERS SURVEYORS
1020 Colusa Road Drive, Suite 100, Roseville, CA 95678 (916) 773-1188

MARCH 2022

SHEET 4 OF 7

FINAL MAP (PN 21-086)
MANGINI RANCH
PHASE 1C SOUTH - VILLAGE 4

BEING A MERGER AND RESUBDIVISION OF LOTS 13, 27 AND 28 AS SHOWN AND SO DESIGNATED ON THAT CERTAIN MAP TITLED "FINAL MAP (PN 14-071) MANGINI RANCH LARGE LOT" FILED FOR RECORD ON APRIL 7, 2017 IN BOOK 395 OF MAPS AT PAGE 10, SACRAMENTO COUNTY RECORDS AND PARCEL 1 DESCRIBED IN GRANT DEED RECORDED AS DOCUMENT NO. 201904260702, O.R.S.C., SITUATED IN SECTIONS 15, 16, 21 & 22, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

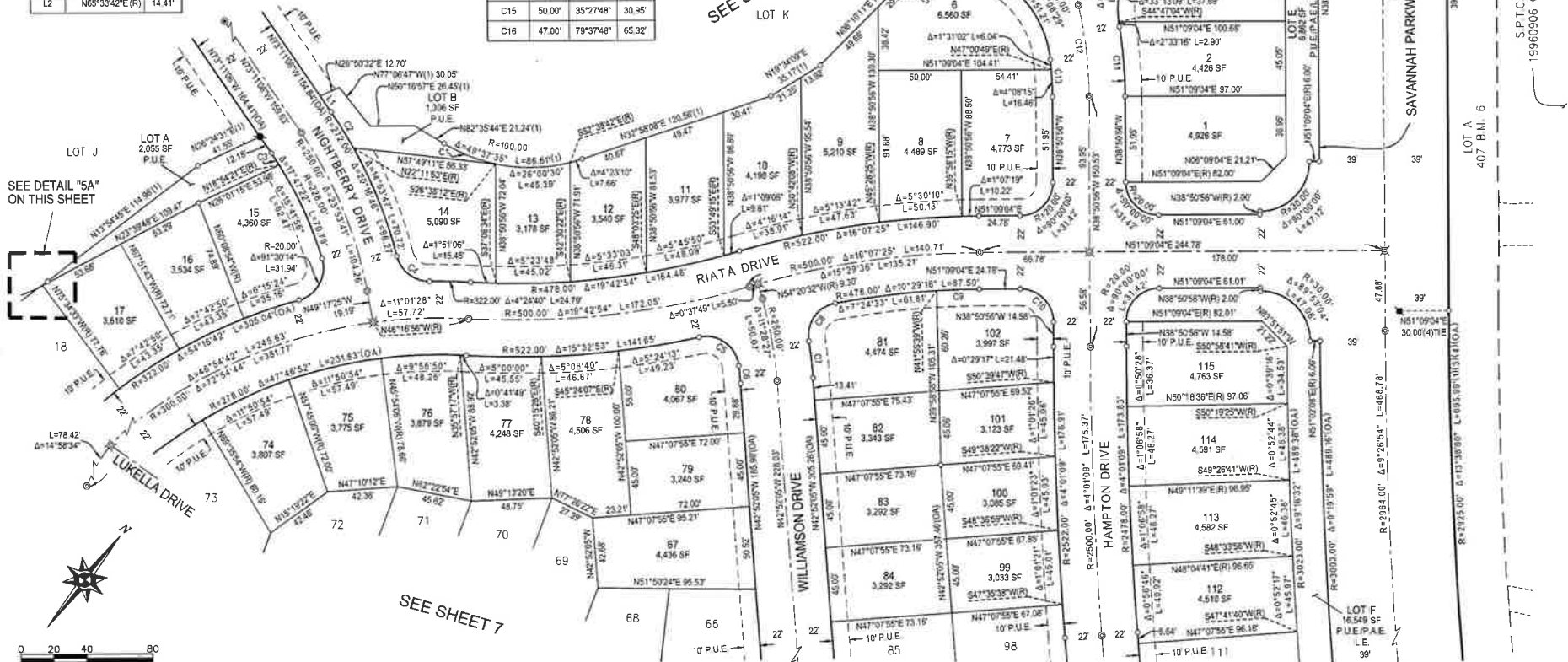
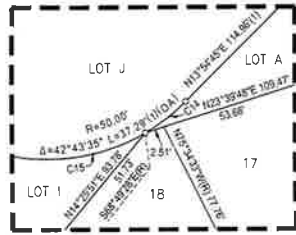
CITY OF FOLSOM • SACRAMENTO COUNTY • CALIFORNIA



MARCH 2022
 SHEET 5 OF 7

Line Table		
Line #	Bearing	Length
L1	N73°11'06"W	11.75'
L2	N65°33'42"E (R)	14.41'

Curve Table			
Curve #	Radius	Delta	Length
C1	100.00'	19°13'55"	33.57'
C2	272.00'	5°22'58"	25.55'
C3	228.00'	2°05'27"	8.32'
C4	20.00'	76°45'47"	26.80'
C5	20.00'	55°14'14"	33.24'
C6	228.00'	2°42'01"	10.75'
C7	272.00'	4°41'58"	22.31'
C8	20.00'	58°13'51"	30.80'
C9	478.00'	3°04'43"	25.68'
C10	20.00'	90°00'00"	31.42'
C11	272.00'	8°55'16"	42.35'
C12	250.00'	12°48'17"	55.67'
C13	228.00'	5°39'17"	22.50'
C14	50.00'	7°15'47"	8.34'
C15	50.00'	35°27'48"	30.35'
C16	47.00'	79°37'48"	65.32'



SEE SHEET 2 FOR NOTES, BASIS OF BEARINGS, REFERENCES AND LEGEND

SEE SHEET 6

SHEET 5 OF 7

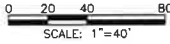
FINAL MAP (PN 21-086)
MANGINI RANCH
PHASE 1C SOUTH - VILLAGE 4

BEING A MERGER AND RESUBDIVISION OF LOTS 13, 27 AND 28 AS SHOWN AND SO DESIGNATED ON THAT CERTAIN MAP TITLED "FINAL MAP (PN 14-071) MANGINI RANCH LARGE LOT" FILED FOR RECORD ON APRIL 7, 2017 IN BOOK 395 OF MAPS, AT PAGE 10, SACRAMENTO COUNTY RECORDS AND PARCEL 1 DESCRIBED IN GRANT DEED RECORDED AS DOCUMENT NO. 201904260702, O.R.S.C., SITUATED IN SECTIONS 15, 16, 21 & 22, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M.

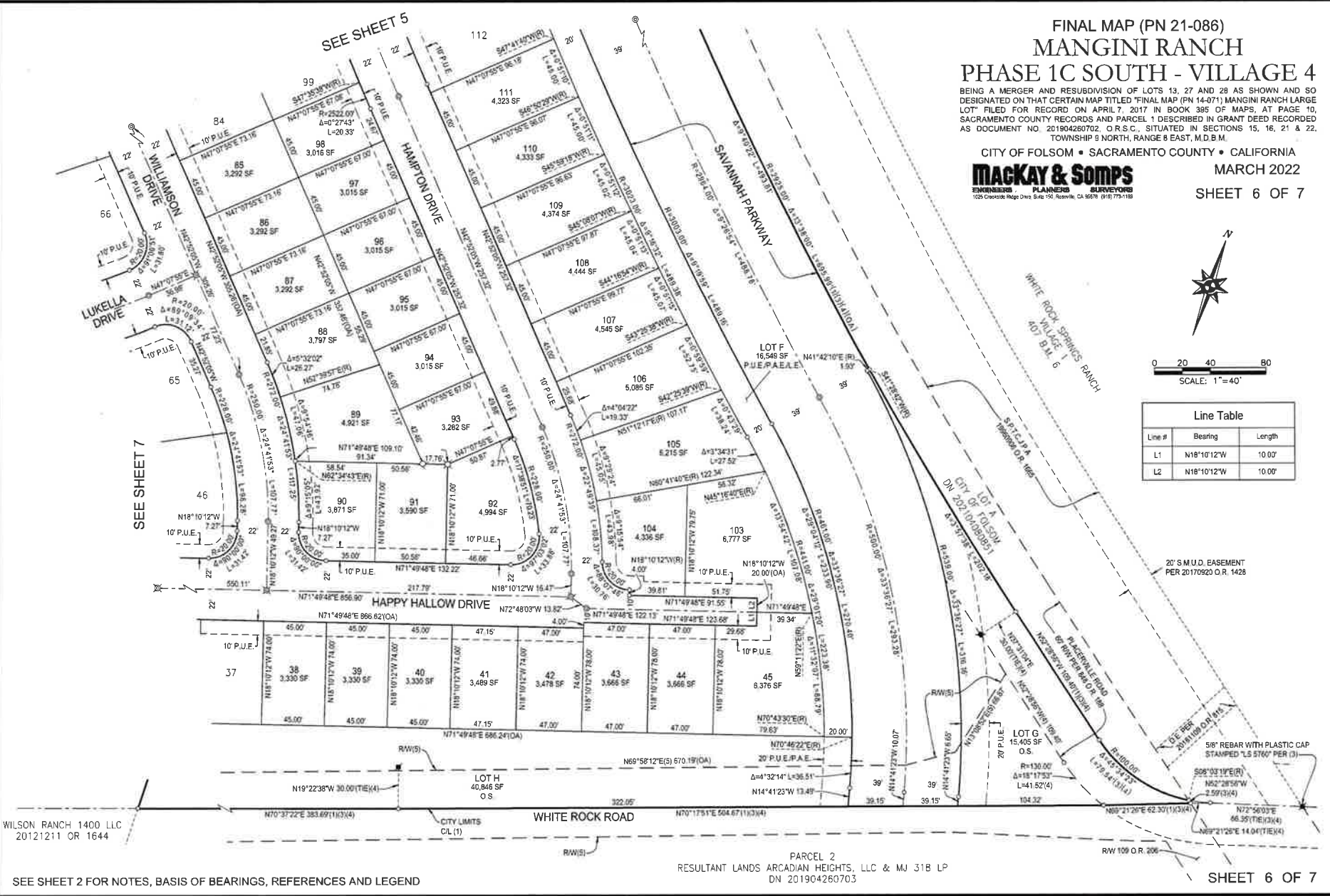
CITY OF FOLSOM • SACRAMENTO COUNTY • CALIFORNIA
 MARCH 2022



SHEET 6 OF 7



Line Table		
Line #	Bearing	Length
L1	N18°10'12"W	10.00'
L2	N18°10'12"W	10.00'



WILSON RANCH 1400 LLC
 20121211 OR 1644

PARCEL 2
 RESULTANT LANDS ARCADIAN HEIGHTS, LLC & MJ 318 LP
 DN 201904260703

SEE SHEET 2 FOR NOTES, BASIS OF BEARINGS, REFERENCES AND LEGEND

SHEET 6 OF 7

SEE SHEET 2 FOR NOTES, BASIS OF BEARINGS, REFERENCES AND LEGEND

FINAL MAP (PN 21-086) MANGINI RANCH PHASE 1C SOUTH - VILLAGE 4

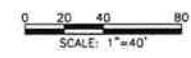
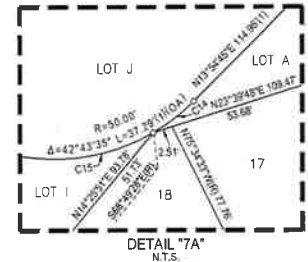
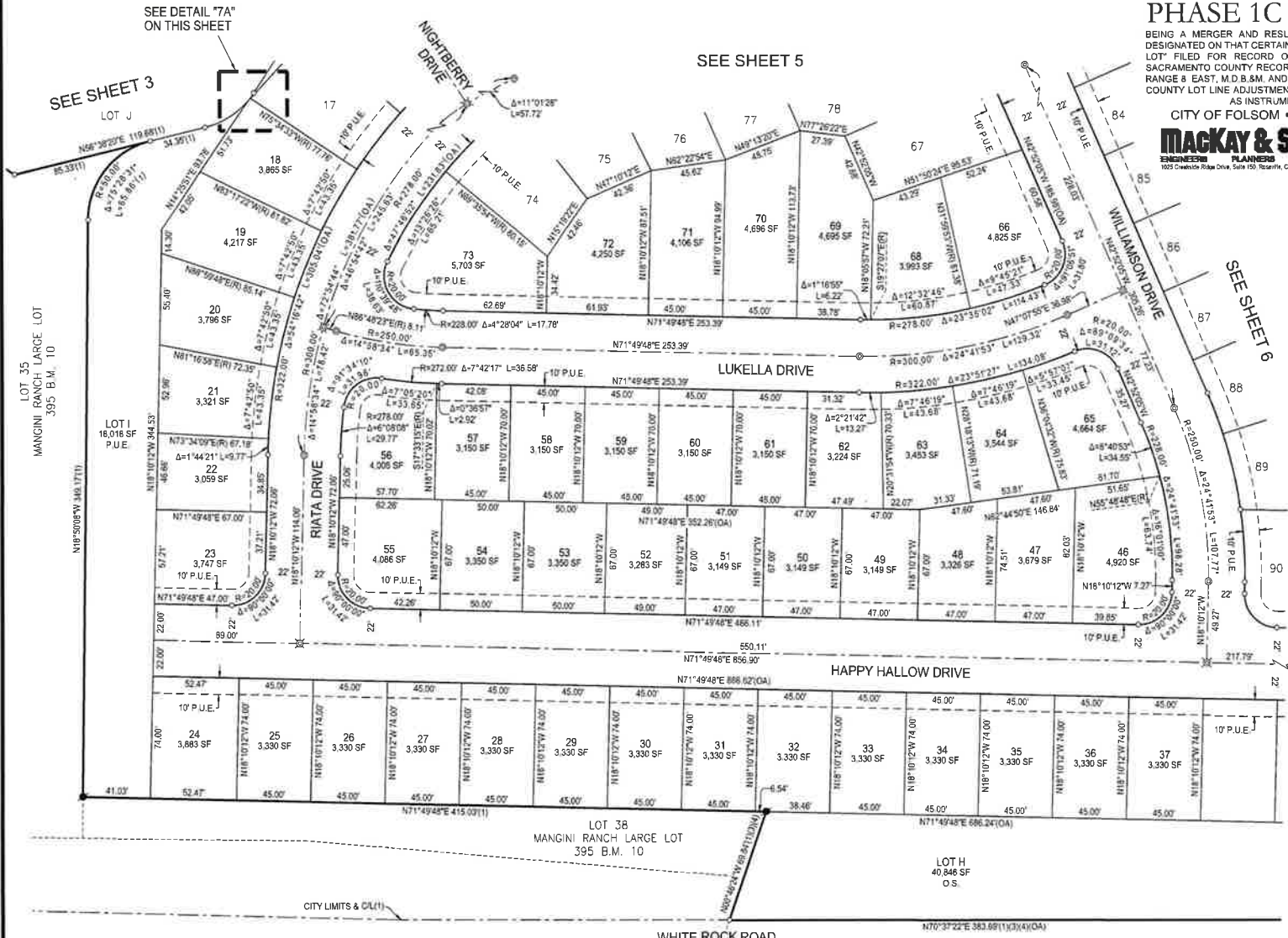
BEING A MERGER AND RESUBDIVISION OF LOTS 13, 27 AND 28 AS SHOWN AND SO DESIGNATED ON THAT CERTAIN MAP TITLED "FINAL MAP (PN 14-071) MANGINI RANCH LARGE LOT" FILED FOR RECORD ON APRIL 7, 2017 IN BOOK 395 OF MAPS, AT PAGE 10, SACRAMENTO COUNTY RECORDS, BEING A PORTION OF SECTION 16, TOWNSHIP 9 NORTH, RANGE 8 EAST, M.D.B.M. AND THE RESULTANT PARCEL 1 OF THAT CERTAIN SACRAMENTO COUNTY LOT LINE ADJUSTMENT RESOLUTION NO. 19-BLS-00015 RECORDED APRIL 26, 2019 AS INSTRUMENT NO. 201904260698, OFFICIAL RECORDS

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Mackay & Somp
ENGINEERS PLANNERS SURVEYORS
1027 Creechville Ridge Drive, Suite 100, Folsom, CA 95630 (916) 771-1100

MARCH 2022

SHEET 7 OF 7



WILSON RANCH 1400 LLC
20121211 OR 1644

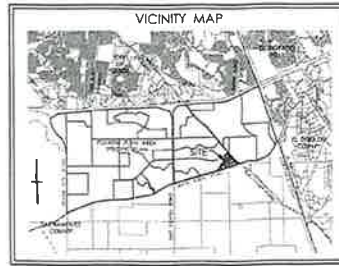
WHITE ROCK ROAD
PARCEL 2
RESULTANT LANDS ARCADIAN HEIGHTS, LLC & MJ 318 LP
DN 201904260703

SHEET 7 OF 7

ATTACHMENT 4

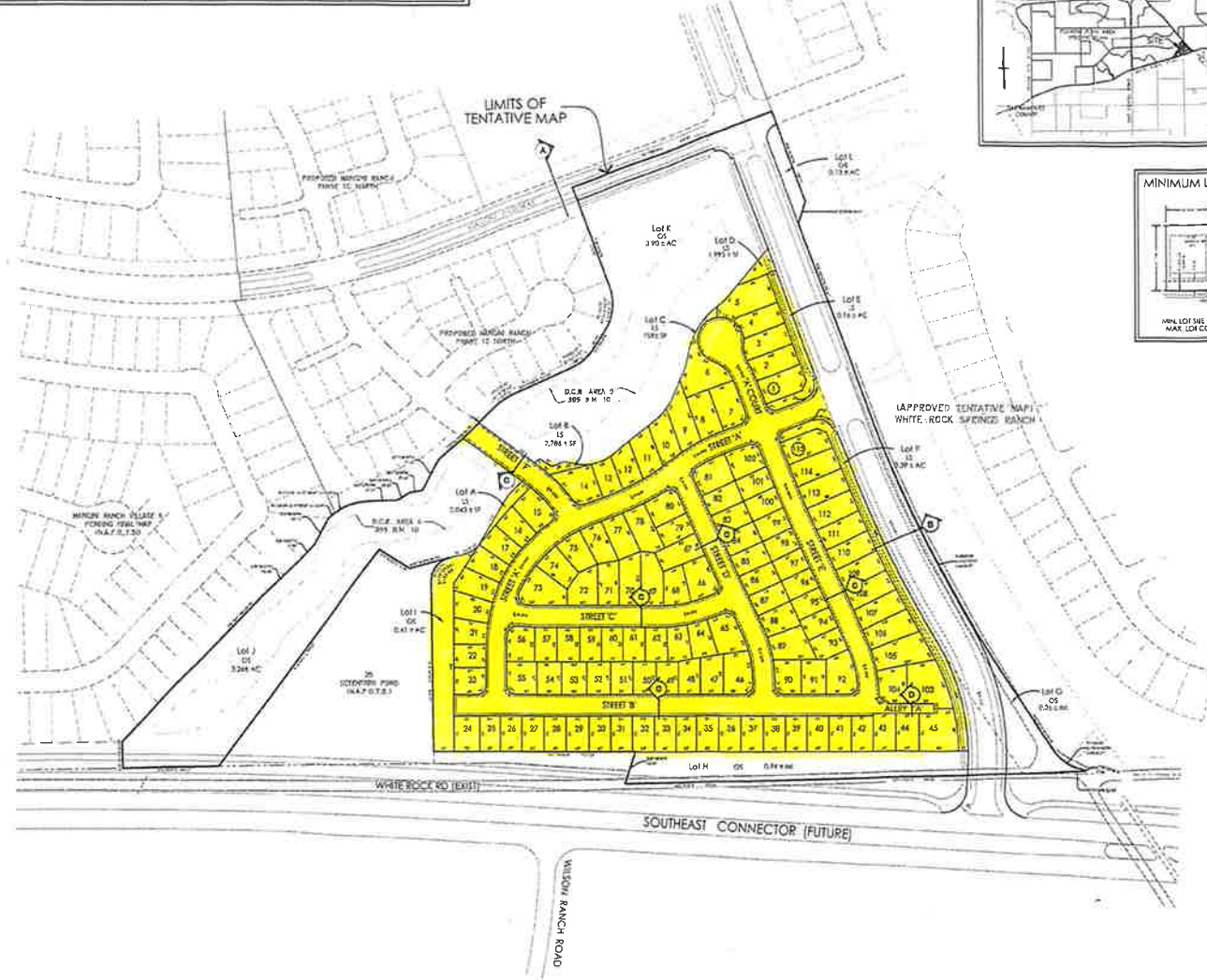
MANGINI RANCH PHASE 1C SOUTH VILLAGE NO. 4 VESTING TENTATIVE SUBDIVISION MAP

LAND USE SUMMARY TABLE						
VILLAGE	CP	SPONSORING DESIGNATION	LAND USE / ZONE DIST.	SPRINK	NET DWELLING	NET DENSITY
ACREAGE	ACRES	UNITS	ACRES	ACRES	SQ. FT.	PER ACRE
Lot A-F	NRD	SPAND	Residential	0.68		
Lot G-L	OK	SP-CO2	Open Space	9.05	9.05	
ICM			Major Roadway	2.83	2.83	
TOTAL				26.56	26.93	113



TENTATIVE MAP	
OWNER/DEVELOPER:	FOCUS 430 14 61 Date
ENGINEER:	MACKAY & SOMPS CIVIL ENGINEERS, INC. 1022 Chestnut Ridge Drive, Suite 100 Palo Alto, CA 94304 (415) 731-1800
ASSESSOR'S PARCEL NO.	073 000 020 & 073 020 003 004 and 073
SITE ACREAGE	24.92 ± A.C.
EXISTING LAND USE ZONE	A portion of the Folsom Plan Area Specific Plan
NUMBER OF LOTS:	172 TOTAL LOTS 112 Single-family residential lots (SFLD) 2 Low-density lots 6 Open Space lots
SERVICE PROVIDERS:	San Jose Community Unified School District
SCHOOL DISTRICTS:	City of Folsom
PAVES & PAVEMENT:	City of Folsom
FIRE & FIRE PROTECTION:	City of Folsom
LANDFILL:	City of Folsom
DOMESTIC WASTE:	City of Folsom
STORM DRAINAGE:	City of Folsom
ELECTRICITY:	SMUD
TELEPHONE:	AT&T
CABLE:	RCLE
	COMCAST

05/10/2022 Item No. 17.



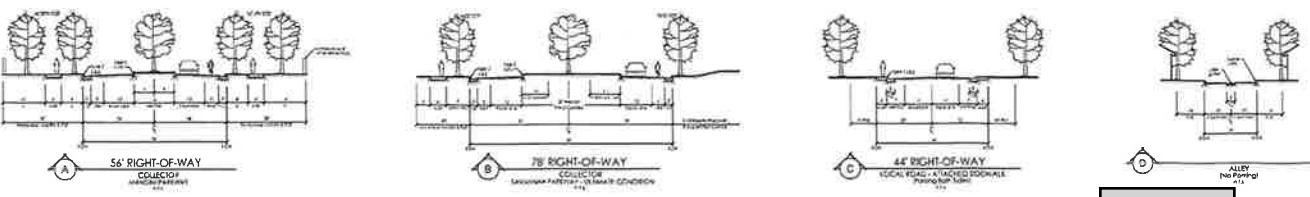
- ### NOTES
- Lot dimensions and bearings are approximate and subject to change.
 - Typical lots are measured from back of apron. On irregular shaped lots, measure lot width at narrowest or vertical.
 - Lot lines and bearings may be adjusted at the time of final map if approved by the City of Folsom. Responsibility for compliance with minimum lot area is assumed by the applicant in a subdivision map application with the approved specific plan. Subject to the approval of the City of Folsom.
 - The final mapping and subdivision development of lots may be affected. (including) to the extent of the Development Agreement.
 - Utility and lot markings to be identified by the applicant and does not include existing utility or development. Utility development zoning that is on-site and will be determined at final map and approved subdivision map.
 - Persons to Government Code Section 65401.1, the applicant may not merge their lots based upon this tentative map. The filing of a final map on a portion of this tentative map may not affect any part of this tentative map.
 - Pursuant to California Government Code Section 64099.20.3, the landowner herein may be merged and subdivided without payment to acreage and may constitute a subdivision for purposes of the existing public trusts, subject to the approval of the City of Folsom, including the following:
 - As a person as set forth in 64099.20.3(a)(2).
 - As a person as set forth in 64099.20.3(b).
 - Street names that are reserved for identification purposes only. A street name will be approved with tentative map that street name is located on lot for identification of the time of final map and development map application.
 - Additional (optional) to accommodate new public utility transmission, access is applied to lot development or other similar requirements needed to accomplish the final map may be located prior to such final map based on lot location.
 - A 10' public utility easement will be located adjacent to an easement right-of-way and is approved by the City of Folsom.
 - The utility program contained within this tentative subdivision map is covered by an Allocation Agreement to the benefit of the City of Folsom and County of Sacramento proposed August 26, 2014 in local order 2014-03-026, Council of Sacramento and Sacramento County.
 - For subdivision purposes that apply to this project see the Development Statement contained within the tentative map.
 - The applicant is to a Development Permit.

BENCHMARK

IDENTIFICATION NO.	City of Folsom Benchmark "19-25"
ELEVATION	414.51 (NAVD 83)
78' TAG AND NAIL IN HEADWALL AT NORTHEAST CORNER OF IRON POINT ROAD AND EAST BIRDELL STREET. BASED ON A LEVEL RUN BY MACKAY & SOMPS IN FEBRUARY 2014.	

ABBREVIATION KEY

OS	OPEN SPACE
LS	LANDSCAPE
DE	DRAINAGE EASEMENT
SE	SEWER EASEMENT
WE	WATERLINE EASEMENT
PUE	PUBLIC UTILITY EASEMENT



SCALE: 1"=100'

COVER SHEET
VESTING SMALL LOT TENTATIVE MAP
PHASE 1C SOUTH
a portion of Window Ranch Phase 1 of the Folsom Plan Area Specific Plan
Folsom, California

MACKAY & SOMPS
CIVIL ENGINEERS

July 1, 2021

ATTACHMENT 5

TABLE OF CONDITIONS OF APPROVAL FOR THE MANGINI RANCH PHASE 1C SOUTH VILLAGE NO. 4 VESTING TENTATIVE SUBDIVISION MAP

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
1.		<p>Final Development Plans The owner/applicant shall submit final site development plans to the Community Development Department that shall substantially conform to the exhibits referenced below:</p> <ol style="list-style-type: none"> 1. Small-Lot Vesting Tentative Subdivision Map, dated July 1, 2021 2. Preliminary Grading, Drainage, and Utility Plan, dated July 1, 2021 3. Residential Schematic Design, dated December 4, 2020 4. Digital Color Board, dated May 4, 2021 5. Access and Circulation Analysis, dated September 23, 2021 6. Environmental Noise Analysis, dated September 15, 2021 7. Applicant's Inclusionary Housing Letter, September 15, 2021 <p>The Small-Lot Vesting Tentative Subdivision Map, Minor Administrative Modifications, Planned Development Permit-Development Standard Deviations and Design Review, Street Names and Inclusionary Housing Plan are approved for the development of a 115-unit single-family residential subdivision (Phase 1 C South Subdivision). Implementation of the project shall be consistent with the above referenced items and these conditions of approval.</p>	G, I, M, B	CD (P)(E)	<p>The Community Development Department has reviewed and approved the improvement plans and the final map for the project. The approved improvement plans are in substantial compliance with the preliminary grading and drainage plans, the preliminary site and utility plans, offsite infrastructure exhibit, the preliminary landscape plans and the community design guidelines.</p> <p>The final map for this subdivision is in substantial compliance with the approved Vesting Tentative Subdivision Map.</p>	Yes
2.		<p>Plan Submittal</p> <p>All civil engineering, improvement, and landscape and irrigation plans, shall be submitted to the Community Development Department for review and approval to ensure conformance with this approval and with relevant codes, policies, standards and other requirements of the City of Folsom.</p>	G, I	CD (P)(E)	<p>Improvement plans for Savannah Parkway, Mangini Parkway, and this subdivision have been approved by the City. Landscape plans for Savannah Parkway and Mangini Parkway have also been approved by the City.</p>	Yes

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
3.		Validity This approval of the Small-Lot Vesting Tentative Subdivision Map shall be valid for a period of twenty-four (24) months pursuant to Section 16.16.110A of the Folsom Municipal Code and the Subdivision Map Act. The term of the Planned Development Permit and approved Inclusionary Housing Agreement shall track the term of the Small-Lot Vesting Tentative Subdivision Map, as may be extended from time to time pursuant to Section 16.16.110.A and 16.16.120 of the Folsom Municipal Code and the Subdivision Map Act.	M	CD (P)	The small-lot vesting tentative subdivision map for the subdivision was approved by the City Council on June 22, 2021.	Yes
4.		FMC Compliance The Small-Lot Final Map shall comply with the Folsom Municipal Code and the Subdivision Map Act.	M	CD (E)	The small-lot vesting tentative subdivision map for the subdivision is in compliance with the Folsom Municipal Code and the State Subdivision Map Act.	Yes
5.		Development Rights The approval of this Small-Lot Vesting Tentative Subdivision Map conveys the right to develop. As noted in these conditions of approval for the Small-Lot Vesting Tentative Subdivision Map, the City has identified improvements necessary to develop the subject parcels. These improvements include on and off-site roadways, water, sewer, storm drainage, landscaping, sound walls, and other improvements.	OG	CD (P)(E)(B) PW, PR, FD, PD	The Community Development Department has reviewed and approved the improvement plans for both the required on-site and off-site improvements and landscape plans.	Yes
6.		Public Right of Way Dedication As provided for in the First Amended and Restated Development Agreement (ARDA) and the Amendments No. 1 and 2 thereto, and any approved amendments thereafter, the Owner/Applicant shall dedicate all public rights-of-way and corresponding public utility easements such that public access is provided to each and every lot within the Phase 1 C South Subdivision project as shown on the Small-Lot Vesting Tentative Subdivision Map (Lots 1-115).	M	CD (E)(P)	The final map for the subdivision includes all required public right-of-way and public utility easements necessary to serve all 115 lots in the subdivision. The public right-of-way and public utility easements are shown on the final map.	Yes

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES																																																												
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied																																																						
7.		<p>Street Names The street names used for the Final Small-Lot Map(s) shall be taken from the Mangini Ranch Street name list approved with this Project or the City's list of approved street names. The following are approved street names:</p> <table border="0"> <tr><td>1. Apollo</td><td>29. Orbit</td></tr> <tr><td>2. Artemis</td><td>30. Nightberry</td></tr> <tr><td>3. Azure</td><td>31. Opal</td></tr> <tr><td>4. Bedrock</td><td>32. Pegasus</td></tr> <tr><td>5. Caprock</td><td>33. Raindrop</td></tr> <tr><td>6. Bellanna</td><td>34. Ranger</td></tr> <tr><td>7. Burns</td><td>35. Rattan</td></tr> <tr><td>8. Cantrell</td><td>36. Riata</td></tr> <tr><td>9. Caraway</td><td>37. Romanini</td></tr> <tr><td>10. Celestial</td><td>38. Selene</td></tr> <tr><td>11. Clementine</td><td>39. Sisal</td></tr> <tr><td>12. Daffodil</td><td>40. Solstice</td></tr> <tr><td>13. Equinox</td><td>41. Starlight</td></tr> <tr><td>14. Firefly</td><td>42. Southpointe</td></tr> <tr><td>15. Freesia</td><td>43. Sunflower</td></tr> <tr><td>16. Frontier</td><td>44. Tily</td></tr> <tr><td>17. Gibbous</td><td>45. Twilight</td></tr> <tr><td>18. Hampton</td><td>46. Violet</td></tr> <tr><td>19. Happy Hallow</td><td>47. Voyager</td></tr> <tr><td>20. Haven</td><td>48. Williamson</td></tr> <tr><td>21. Indigo</td><td></td></tr> <tr><td>22. Lukella</td><td></td></tr> <tr><td>23. Magnolia</td><td></td></tr> <tr><td>24. Margauex</td><td></td></tr> <tr><td>25. Mascon</td><td></td></tr> <tr><td>26. Monarch</td><td></td></tr> <tr><td>27. Moonbeam</td><td></td></tr> </table>	1. Apollo	29. Orbit	2. Artemis	30. Nightberry	3. Azure	31. Opal	4. Bedrock	32. Pegasus	5. Caprock	33. Raindrop	6. Bellanna	34. Ranger	7. Burns	35. Rattan	8. Cantrell	36. Riata	9. Caraway	37. Romanini	10. Celestial	38. Selene	11. Clementine	39. Sisal	12. Daffodil	40. Solstice	13. Equinox	41. Starlight	14. Firefly	42. Southpointe	15. Freesia	43. Sunflower	16. Frontier	44. Tily	17. Gibbous	45. Twilight	18. Hampton	46. Violet	19. Happy Hallow	47. Voyager	20. Haven	48. Williamson	21. Indigo		22. Lukella		23. Magnolia		24. Margauex		25. Mascon		26. Monarch		27. Moonbeam		M	CD (E)(P)	The required street names are shown on the final map for the subdivision and have been approved by the Planning Commission.	Yes
1. Apollo	29. Orbit																																																											
2. Artemis	30. Nightberry																																																											
3. Azure	31. Opal																																																											
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CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
8.		<p>Indemnity for City The Owner/Applicant shall protect, defend, indemnify, and hold harmless the City and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul any approval by the City or any of its agencies, departments, commissions, agents, officers, employees, or legislative body concerning the project, which claim, action or proceeding is brought within the time period provided therefore in Government Code Section 66499.37 or other applicable statutes of limitation. The City will promptly notify the owner/applicant of any such claim, action or proceeding, and will cooperate fully in the defense. If the City should fail to cooperate fully in the defense, the owner owner/applicant shall not thereafter be responsible to defend, indemnify and hold harmless the City or its agents, officers, and employees, pursuant to this condition. The City may, within its unlimited discretion, participate in the defense of any such claim, action or proceeding if both of the following occur:</p> <ul style="list-style-type: none"> • The City bears its own attorney’s fees and costs, and • The City defends the claim, action or proceeding in good faith <p>The owner/applicant shall not be required to pay or perform any settlement of such claim, action or proceeding unless the settlement is approved by the owner/applicant. The owner/applicant’s obligations under this condition shall apply regardless of whether a Final Map is ultimately recorded with respect to this project.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD	The City standard subdivision improvement agreement includes language that satisfies this condition. The subdivision improvement agreement will be executed by the City Manager upon approval by the City Council.	Yes
9.		<p>Small-Lot Vesting Tentative Subdivision Map The Small-Lot Vesting Tentative Subdivision map is expressly conditioned upon compliance with all environmental mitigation measures identified in the Folsom</p>	OG	CD	The owner/applicant has complied with all applicable mitigation measures from the FPASP FEIR/EIS and other noted approved environmental documents prior to the issuance of a grading permit. Additionally,	Yes

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Plan Area Specific Plan EIR/EIS as amended by the Revised Proposed Water Supply Facility Alternative (November 2012), the Folsom South of U.S. Highway 50 Backbone Infrastructure Mitigated Negative Declaration (December 2014).			construction inspection and monitoring was conducted throughout construction by the City and/or its Consultants.	
10.		ARDA and Amendments The Owner/Applicant shall comply with all provisions of Amendments No. 1 and 2 to the First Amended and Restated Tier 1 Development Agreement and any approved amendments thereafter by and between the City and the owner/applicant of the project.	M	CD (E)	The Community Development Department has reviewed the subdivision and verified that it is in compliance with Amendment No. 1 and No. 2 to the First Amended and Restated Tier 1 development Agreement.	Yes
11.		Mitigation Monitoring The owner/applicant shall participate in a mitigation monitoring and reporting program pursuant to City Council Resolution No. 2634 and Public Resources Code 21081.6. The mitigation monitoring and reporting measures identified in the Folsom Plan Area Specific Plan FEIR/EIS have been incorporated into these conditions of approval in order to mitigate or avoid significant effects on the environment. These mitigation monitoring and reporting measures are identified in the mitigation measure column. Applicant shall fund on a Time and Materials basis all mitigation monitoring (e.g., staff and consultant time).	OG	CD (P)	The owner/applicant has funded and participated in a MMRP reporting program performed by the City's consultant (Helix) and/or City staff.	Yes
12.		The owner/applicant acknowledges that the State adopted amendments to Section 65850 of the California Government Code (specifically Section 65850(9)), effective January 1, 2018, to allow for the implementation of inclusionary housing requirements in residential rental units, upon adoption of an ordinance by the City. The Landowner is not currently contemplating any residential rental projects within the Subject Property; however, in the event the City amends its Inclusionary Housing Ordinance with respect to rental housing pursuant to	OG	CD (P)	The Landowner has acknowledged this requirement and has confirmed that there is currently no plan to include any residential rental property within the subdivision.	Yes

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		Section 65850(9), Landowner (or successor in interest) agrees that the Subject Property shall be subject to said City Ordinance, as amended, should any residential rental project be proposed within the Subject Property.				
POLICE/SECURITY REQUIREMENT						
13.		<p>The Owner/Applicant shall consult with the Police Department in order to incorporate all reasonable crime prevention measures. The following security/safety measures shall be considered:</p> <ul style="list-style-type: none"> • A security guard on-duty at all times at the site or a six-foot security fence shall be constructed around the perimeter of construction areas. • Security measures for the safety of all construction equipment and unit appliances. • Landscaping shall not cover exterior doors or windows, block line-of-sight at intersections or screen overhead lighting. 	G, I, B	PD	The owner/applicant provided onsite security during construction and has incorporated line of sight guidelines into landscaping plans at intersections which have been reviewed and approved by the City.	Yes
DEVELOPMENT COSTS AND FEE REQUIREMENTS						
14.		<p>Taxes and Fees The owner/applicant shall pay all applicable taxes, fees and charges for the project at the rate and amount required by the Public Facilities Financing Plan and Amendments No. 1 and No. 2 to the Amended and Restated Tier 1 Development Agreement.</p>	M	CD (P)(E)	The owner/applicant has paid all current taxes and fees associated with this subdivision.	Yes
15.		<p>Assessments If applicable, the owner/applicant shall pay off any existing assessments against the property, or file necessary segregation request and pay applicable fees.</p>	M	CD (E)	The owner/applicant has paid all taxes and fees associated with this subdivision and filed a tax segregation request for applicable taxes.	Yes

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16.		<p>FPASP Development Impact Fees The Owner/Applicant shall be subject to all Folsom Plan Area Specific Plan Area development impact fees in place at the time of approval or subsequently adopted consistent with the Public Facilities Financing Plan (PFFP), Development Agreement and amendments thereto, unless exempt by previous agreement. The owner/applicant shall be subject to all applicable Folsom Plan Area plan-wide development impact fees in effect at such time that a building permit is issued. These fees may include, but are not limited to, the Folsom Plan Area Specific Plan Fee, Specific Plan Infrastructure Fee (SPIF), Solid Waste Fee, Corporation Yard Fee, Transportation Management Fee, Transit Fee, Highway 50 Interchange Fee, General Park Equipment Fee, Housing Trust Fee, etc.</p> <p>Any protest to such for all fees, dedications, reservations or other exactions imposed on this project will begin on the date of final approval (November 2021), or otherwise shall be governed by the terms of Amendments No. 1 and 2 to ARDA. The fees shall be calculated at the fee rate set forth in the PFFP and the ARDA.</p>	B	CD (P), PW, PK	The Owner/Applicant shall pay all required City fees and Plan Area wide fees prior to issuance of building permits.	Condition will be satisfied prior to issuance of a building permit.
17.		<p>Legal Counsel The City, at its sole discretion, may utilize the services of outside legal counsel to assist in the implementation of this project, including, but not limited to, drafting, reviewing and/or revising agreements and/or other documentation for the project. If the City utilizes the services of such outside legal counsel, the City shall provide notice to the owner/applicant of the outside counsel selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for all outside legal fees and costs incurred and documented by the City for such services. The owner/applicant may be required, at the sole discretion of the City Attorney, to submit a deposit to the City for these</p>	OG	CD (P)(E)	The City has not yet utilized any outside services for any type of legal issues for this subdivision. If at any time during the development of this subdivision, any outside legal services were necessary, the owner/applicant would be required to comply with this condition	Yes

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		services prior to initiation of the services. The owner/applicant shall be responsible for reimbursement to the City for the services regardless of whether a deposit is required.				
18.		Consultant Services If the City utilizes the services of consultants to prepare special studies or provide specialized design review or inspection services for the project, the City shall provide notice to the owner/applicant of the outside consultant selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for actual costs incurred and documented in utilizing these services, including administrative costs for City personnel. A deposit for these services shall be provided prior to initiating review of the Grading Plan, Final Map, improvement plans, or beginning inspection, whichever is applicable.	G, I, M, B	CD (P)(E)	The City has provided notice to the owner/applicant for various Consultants performing services for the development of this subdivision. The City has collected deposits in advance of such work for these services.	Yes
GRADING PERMIT REQUIREMENTS						
19.		Walls/Fences The final location, design, height, materials, and colors of the walls and fences shall be consistent with the submitted tentative map exhibits and noise barrier exhibit, subject to review and approval by the Community Development Department to ensure consistency with the Folsom Ranch Central District Design Guidelines. The location of the fencing shall remain in perpetuity as shown and installed originally by the Applicant (i.e., fence may not be moved into the PUE on side/corner lots).	G, I, B	CD (P)(E), FD	The Community Development Department has verified that the proposed walls and fences for the subdivision are in compliance with the Folsom Ranch Central Design Guidelines. The Community Development Department will review all proposed site plans to verify compliance with the Folsom Fire Code.	Yes Condition will be satisfied prior to issuance of a building permit.
20.		Mine Shaft Remediation The owner/applicant shall locate and remediate all antiquated mine shafts, drifts, open cuts, tunnels, and water conveyance or impoundment structures existing on the project site, with specific recommendations for the sealing, filling, or removal of each that meet all applicable health, safety and engineering standards. Recommendations shall be prepared by an appropriately licensed engineer or geologist. All remedial	G	CD (E)	During the course of grading and construction for this subdivision no mine shafts or tunnels were located or discovered.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		plans shall be reviewed and approved by the City prior to approval of grading plans.				
21		<p>Prepare Traffic Control Plan. Prior to construction, a Traffic Control Plan for roadways and intersections affected by construction shall be prepared by the owner/applicant. The Traffic Control Plan prepared by the owner/applicant shall, at minimum, include the following measures:</p> <ul style="list-style-type: none"> • Maintaining the maximum amount of travel lane capacity during non-construction periods, possible, and advanced notice to drivers through the provision of construction signage. • Maintaining alternate one-way traffic flow past the lay down area and site access when feasible. • Heavy trucks and other construction transport vehicles shall avoid the busiest commute hours (7 a.m. to 8 a.m. and 5 p.m. to 6 p.m. on weekdays). • A minimum 72-hour advance notice of access restrictions for residents, businesses, and local emergency response agencies. This shall include the identification of alternative routes and detours to enable for the avoidance of the immediate construction zone. • A phone number and City contact for inquiries about the schedule of the construction throughout the construction period. This information will be posted in a local newspaper, via the City's web site, or at City Hall and will be updated on a monthly basis. 	G	CD (E)	The required Traffic Control Plans in this condition are no longer applicable. This condition was added to the project since the original concept for water supply contemplated extending a large diameter water main from the Sacramento River to the FPA. In lieu of the water supply being extended through the various jurisdictions noted in this condition, the water supply provided to the FPA comes from conservation efforts by the City to provide the needed water supply for the FPA. The new water supply proposal was validated in 2014.	Yes
22.		<p>State and Federal Permits The owner/applicant shall obtain all required State and Federal permits and provide evidence that said permits have been obtained, or that the permit is not required, subject to staff review prior to approval of any grading or improvement plan.</p>	G, I	CD (P)(E)	The owner/applicant has obtained all required State and Federal permits and copies are available from the Community Development Department.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
23.		<i>Landslide /Slope Failure</i> The owner/applicant shall retain an appropriately licensed engineer during grading activities to identify existing landslides and potential slope failure hazards. The said engineer shall be notified a minimum of two days prior to any site clearing or grading to facilitate meetings with the grading contractor in the field.	G	CD (E) PW	The owner/applicant retained a geotechnical engineer and implemented recommendations for this mitigation measure. A geotechnical report outlining these recommendations is available from Community Development Department.	Yes
IMPROVEMENT PLAN REQUIREMENTS						
24.		<i>Improvement Plans</i> The improvement plans for the required public and private subdivision improvements necessary to serve any and all phases of development shall be reviewed and approved by the Community Development Department prior to approval of a Final Map.	M	CD (E)	The owner/applicant submitted improvement plans for all infrastructure improvements required to serve this subdivision. The City Engineer has reviewed and approved all required improvement plans to serve this subdivision. Copies of the improvement plans are available from the Community Development Department.	Yes
25.		<i>Standard Construction Specifications and Details</i> Public and private improvements, including roadways, curbs, gutters, sidewalks, bicycle lanes and trails, streetlights, underground infrastructure and all other improvements shall be provided in accordance with the latest edition of the City of Folsom <u><i>Standard Construction Specifications and Details</i></u> and the <u><i>Design and Procedures Manual and Improvement Standards</i></u> .	I	CD (P)(E)	The owner/applicant submitted improvement plans in accordance with the City's Standard Specifications.	Yes
26.		<i>Water and Sewer Infrastructure</i> All City-owned water and sewer infrastructure shall be placed within the street right of way. In the event that a City-maintained public water or sewer main needs to be placed in an area other than the public right of way, such as through an open space corridor, landscaped area, etc., the following criteria shall be met; <ul style="list-style-type: none"> • The Owner/Applicant shall provide public sewer and water main easements. • An access road shall be designed and constructed to allow for the operations, maintenance and replacement of the public water or sewer line by the 	I	CD (E)	The owner/applicant has installed all sewer and water infrastructure within the street right of way. The outfall sewer line has been installed within easements or planned right of way within future streets.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		City along the entire water and/or sewer line alignment. <ul style="list-style-type: none"> In no case shall a City-maintained public water or public sewer line be placed on private residential property. 				
27.		Lighting Plan The owner/applicant of all project phases shall submit a lighting plan for the project to the Community Development Department. The lighting plan shall be consistent with the Folsom Ranch Central District Design Guidelines: <ul style="list-style-type: none"> Shield or screen lighting fixtures to direct the light downward and prevent light spill on adjacent properties. Place and shield or screen flood and area lighting needed for construction activities, nighttime sporting activities, and/or security so as not to disturb adjacent residential areas and passing motorists; For public lighting in residential neighborhoods, prohibit the use of light fixtures that are of unusually high intensity or that blink or flash; Use appropriate building materials (such as low-glare glass, low-glare building glaze or finish, neutral, earth toned colored paint and roofing materials), shielded or screened lighting, and appropriate signage in the office/commercial areas to prevent light and glare from adversely affecting motorists on nearby roadways; and Design exterior on-site lighting as an integral part of the building and landscaping design in the Specific Plan Area. Lighting fixtures shall be architecturally consistent with the overall site design. Lights used on signage should be directed to light only the sign face with no off-site glare. 	I, B	CD (P)	The owner/applicant submitted a Lighting Plan for all backbone roadways and subdivisions in accordance with the Design Guidelines and City Standards for Street Lighting. A copy of the lighting plans are available from the Community Development Department.	Yes
28.		Utility Coordination The owner/applicant shall coordinate the planning, development and completion of this project with the various utility agencies (i.e., SMUD, PG&E, etc.). The owner/applicant shall provide the	M	CD (P)(E)	The owner/applicant has coordinated with the various utility agencies.	Yes

<p align="center">CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES</p>						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		City with written confirmation of public utility service prior to approval of the final map.				
29.		<p>Replacing Hazardous Facilities The owner/applicant shall be responsible for replacing any and all damaged or hazardous public sidewalk, curb and gutter, and/or bicycle trail facilities along the site frontage and/or boundaries, including pre-existing conditions and construction damage, to the satisfaction of the Community Development Department.</p>	I, OG	CD (E)	The owner/applicant has reconstructed a portion of Mangini Parkway to the satisfaction of the Community Development Department. There is a general note on the approved improvements plans requiring compliance with this condition.	Yes
30.		<p>Future Utility Lines All future utility lines lower than 69 KV that are to be built within the project shall be placed underground within and along the perimeter of the project at the developer's cost. The owner/applicant shall dedicate to SMUD all necessary underground easements for the electrical facilities that will be necessary to service development of the project.</p>	M	CD (E)	All proposed utility service to the lots in this subdivision have been placed underground	Yes
31.		<p>Water Meter Fixed Network System The owner owner/applicant shall pay for, furnish and install all infrastructure associated with the water meter fixed network system for any City-owned and maintained water meter within the project.</p>	I	CD (E), EWR	The Owner/applicant has completed the infrastructure allowing for the water meter fixed network system. Meters will be furnished and installed during home construction for each individual metered connection.	Yes
32.		<p>Class II Bike Lanes All Class II bike lanes (Savannah Parkway) shall be striped, and the legends painted to the satisfaction of the Community Development Department. No parking shall be permitted within the Class II bike lanes.</p>	I	CD (E)(P)	All Class II bike lanes have been constructed in accordance with the Specific Plan, Design Guidelines and City standards.	Yes
33.		<p>Noise Barriers and Window Assemblies Based on the Environmental Noise Assessment (the "Noise Assessment") prepared by Bollard Acoustical Consultants on September 15, 2021, the following measures shall be implemented to the satisfaction of the Community Development Department:</p> <ul style="list-style-type: none"> • Six to ten-foot noise barriers shall be constructed along the Project boundaries adjacent to Savannah Parkway and White 	I, O	CD (E)(P)	The approved landscape plans for Savannah Parkway, White Rock Road and Mangini Parkway include the required sound walls along Savannah Parkway, White Rock Road and Mangini Parkway in accordance with the recommendations of the acoustical study.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<p>Rock Road/Future Southeast Connector. Lots 36-45 require a 10-ft wall, Lots 27-35 require a 9-foot wall, Lots 24-36 an 8-foot wall, Lots 105-115 require a 7-foot wall, and Lots 1-5 a 6-foot wall. The noise barriers could take the form a masonry wall, earthen berm, or combination of the two. For the 9-foot and 10-foot barriers segments adjacent to White Rock Road, the Applicant shall limit the barrier height to 8 feet, as feasible, and implement construction measures (e.g., berming, adjustments to finished grades, etc.) to meet exterior noise standards.</p> <ul style="list-style-type: none"> • Suitable materials for the traffic noise barriers include masonry and precast concrete panels. Other materials may be acceptable but shall be reviewed by an acoustical consultant and approved by the Community Development Department prior to use. • To ensure compliance with the General Plan 45 dB DNL exterior noise level standard including a factor of safety, all upper-floor bedroom windows of residences constructed adjacent to Savannah Parkway and White Rock Road from which the roadway would be visible shall be upgraded to a minimum STC rating of 32, 34 and 35. Lots 1-5 and Lots 103 – 115 require an STC rating of 32. Lot's 24-35 require an STC rating of 34 and Lot's 36-45 require an STC rating of 35. 				

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<ul style="list-style-type: none"> Mechanical ventilation (air conditioning) shall be provided for all residences in this development to allow the occupants to close doors and windows as desired to achieve compliance with the applicable General Plan 45 dB DNL interior noise level standard. 				
34.		<p>Master Plan Updates</p> <p>The Owner/Applicant shall provide sanitary sewer, water and storm drainage improvements with corresponding easements, as necessary, in accordance with these studies and the latest edition of the City of Folsom <u>Standard Construction Specifications and Details</u>, and the <u>Design and Procedures Manual and Improvement Standards</u>.</p> <p>The storm drainage design shall provide for no net increase in run-off under post- development conditions.</p>	G, I	CD(E), EWR, PW	The Owner/applicant has provided updated Master Plans for approval prior to the issuance of a grading permit. Copies of the Master Plans are available from the Community Development Department. The storm drainage system for the subdivision provides for no net increase in run-off under post development conditions.	Yes
35.		<p>Best Management Practices</p> <p>The storm drain improvement plans shall provide for "Best Management Practices" that meet the requirements of the water quality standards of the City's National Pollutant Discharge Elimination System Permit issued by the State Regional Water Quality Control Board.</p> <p>In addition to compliance with City ordinances, the owner/applicant shall prepare a Stormwater Pollution Prevention Plan (SWPPP) and implement Best Management Practices (BMPs) that comply with the General Construction Stormwater Permit from the Central Valley RWQCB, to reduce water quality effects during construction. Detailed information about the SWPPP and BMPs are provided in Chapter 3A.9, "Hydrology and Water Quality."</p>	G, I	CD (E)	The Owner/applicant has received a NPDES permit from the State Regional Water Quality Control Board (SRWQCB). The NDES Permit requires the implementation of BMP's, monitoring and reporting for stormwater runoff. The NPDES Permit includes a Storm Water Pollution Prevention Plan (SWPPP), which outlines monitoring standards, frequency and baseline modeling. The Owner/applicant has submitted monthly reports to the City and SRWQCB.	Yes
36.		<p>Litter Control</p> <p>During Construction, the owner/applicant shall be responsible for litter control and sweeping of all paved surfaces in accordance with City standards. All on-site storm drains shall</p>	OG	CD (E)	The owner/applicant has complied with this provision and completed periodic on-site cleaning and sweeping of the project site.	Yes

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		be cleaned immediately before the official start of the rainy season (October 15).				
FIRE DEPT REQUIREMENTS						
37.		<p>All-Weather Access and Fire Hydrants The Owner/Applicant shall provide all-weather access and fire hydrants before combustible materials are allowed on any project site or other approved alternative method as approved by the Fire Department. All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material or vertical construction is allowed on any project site or other approved alternative method as approved by the Fire Department. (All-weather access is defined as six inches of compacted aggregate base from May 1 to September 30 and two inches asphalt concrete over six inches aggregate base from October 1 to April 30). The buildings shall have illuminated addresses visible from the street or drive fronting the property. Size and location of address identification shall be reviewed and approved by the Fire Department.</p> <ul style="list-style-type: none"> Residential Fire-Flow with Automatic Fire Sprinkler System: The required fire-flow for the proposed subdivision is determined to be 500 gpm per minute for 30 minutes. All public streets shall meet City of Folsom Street Standards. The maximum length of any dead-end street shall not exceed 500 feet in accordance with the Folsom Fire Code (unless approved by the Fire Department). All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material storage or vertical construction is allowed. All-weather access is defined as 6" of compacted AB from May 1 to September 30 and 2"AC over 6" AB from October 1 to April 30 	G, I, M, B	CD (P), FD	The owner/applicant has designed and received approval for all weather access improvements and fire hydrants for this subdivision. Permits for vertical construction will not be issued prior to these improvements being completed.	Yes
LANDSCAPE/TREE PRESERVATION REQUIREMENTS						

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
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38.		<p>Landscaping Plans Final landscape plans and specifications shall be prepared by a registered landscape architect and approved by the City prior to the approval of the first building permit. Said plans shall include all on-site landscape specifications and details including a tree planting exhibit demonstrating sufficient diversity and appropriate species selection to the satisfaction of the Community Development Department. The tree exhibit shall include all street trees, accent trees, parking lot shading trees, and mitigation trees proposed within the development. Said plans shall comply with all State and local rules, regulations, Governor's declarations and restrictions pertaining to water conservation and outdoor landscaping.</p> <p>Landscaping shall meet shade requirements as outlined in the Folsom Plan Area Specific Plan where applicable. The landscape plans shall comply and implement water efficient requirements as adopted by the State of California (Assembly Bill 1881) (State Model Water Efficient Landscape Ordinance) until such time the City of Folsom adopts its own Water Efficient Landscape Ordinance at which time the owner/applicant shall comply with any new ordinance. Shade and ornamental trees shall be maintained according to the most current American National Standards for Tree Care Operations (ANSI A-300) by qualified tree care professionals. Tree topping for height reduction, view protection, light clearance or any other purpose shall not be allowed. Specialty-style pruning, such as pollarding, shall be specified within the approved landscape plans and shall be implemented during a 5-year establishment and training period. The owner/applicant shall comply with city-wide landscape rules or regulations on water usage. The Owner/Applicant shall comply with any state or local rules</p>	B	CD (P)(E)	The Community Development Department has reviewed and approved the landscape plans for subdivision. The approved plans are in accordance with all City requirements and Design Guidelines for the Folsom Ranch Central Design Area. The final map includes Lots A through L. These lots will be dedicated to the City as the various improvements in the subdivision and within these lots are completed and accepted by the City.	Yes

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		and regulations relating to landscape water usage and landscaping requirements necessitated to mitigate for drought conditions on all landscaping in the Phase 1 C South Subdivision project. <ul style="list-style-type: none"> a. Open fencing shall be provided in Lots 5, 6 and 9-24 for any property lines that abut open space. b. A six-foot concrete pedestrian path shall be provided at the end of Court "A" to provide access to the Class 1 trail located in the open space to the north (Lot K). A six-foot-wide concrete pedestrian path shall be provided on the west end of Street B to provide access to the Class 1 trail located in the open space to the west (Lot I) c. Open space and landscape Lots A-L shall be dedicated to the City. 				
MAP REQUIREMENTS						
39.		Subdivision Improvement Agreement Prior to the approval of any Final Map, the owner/applicant shall enter into a subdivision improvement agreement with the City, identifying all required improvements, if any, to be constructed with each proposed phase of development. The owner/applicant shall provide security acceptable to the City, guaranteeing construction of the improvements.	M	CD (E)	The required subdivision improvement agreement is included as part of the City staff report accompanying the final map for City Council approval. The resolution approving the final map for this subdivision includes a statement authorizing the City Manager to execute the subdivision improvement agreement for the subdivision along with approval of the final map.	Yes
40.		The Final Inclusionary Housing Plan The Final Inclusionary Housing Plan shall be approved by the City Council. The Inclusionary Housing Agreement, which will be approved by the City Attorney, shall be executed prior to recordation of the Final Map for the Phase 1 C South Subdivision project.	M	CD (P)(E)	The owner/applicant has executed an Inclusionary Housing Agreement with the City. The agreement allows the owner/applicant to provide an in-lieu fee assigned to each building permit in the subdivision. The in-lieu housing fee will be paid at the time of building permit issuance.	Yes
41.		Department of Real Estate Public Report The owner/applicant shall disclose to the homebuyers in the Department of Real Estate Public Report and/or the CC&R's the following items:			The Community Development Department has reviewed the subdivisions CC & R's and verified that all required disclosures in this condition of approval are included.	Yes

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		1) The soil in the subdivision may contain naturally occurring asbestos and naturally occurring arsenic. 2) The collecting, digging, or removal of any stone, artifact, or other prehistoric or historic object located in public or open space areas, and the disturbance of any archaeological site or historic property, is prohibited. 3) The project site is located close to the Mather Airport flight path and overflight noise may be present at various times. 4) Owner/Applicant acknowledges the final design, location, grade and configuration of the Southeast Connector Project along the southern boundary is not known. As such, Owner/Applicant will include a recorded disclosure to be provided to all potential buyers of homes within Mangini Ranch Phase 1C South Project advising of the future Connector Project and associated noise, grade changes, height, location, design, traffic and construction as eventually approved. 5) That all properties located within one mile of an on- or off-site area zoned or used for agricultural use (including livestock grazing) shall be accompanied by written disclosure from the transferor, in a form approved by the City of Folsom, advising any transferee of the potential adverse odor impacts from surrounding agricultural operations, which disclosure shall direct the transferee to contact the County of Sacramento concerning any such property within the County zoned for agricultural uses within one mile of the subject property being transferred.	M	CD (P, PK)		

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		6) Applicant shall ensure that the CC&Rs contain a notice that the side yard fencing cannot be relocated and must remain as installed by Applicant.				
42.		Public Utility Easements The Owner/Applicant shall dedicate public utility easements for underground facilities on properties adjacent to the public and private streets. A minimum of twelve and one-half-foot (12.5') wide Public Utility Easements for underground facilities (i.e., SMUD, Pacific Gas and Electric, cable television, telephone) shall be dedicated adjacent to all public and private street rights-of-way. The Owner/Applicant shall dedicate additional width to accommodate extraordinary facilities as determined by the City. The width of the public utility easements adjacent to public and private right of way may be reduced with prior approval from public utility companies.	M	CD (E)	The owner/applicant has dedicated a 12.5' PUE along all roadway utility corridors as well as internal streets within the subdivision. The public utility companies have approved the reduction of the width of the PUE along various roadways and alleys within this subdivision. The public utility easements are shown on the final map.	Yes
43.		Backbone Infrastructure As provided for in the ARDA and the Amendment No. 1 thereto, the Owner/Applicant shall provide fully executed grant deeds, legal descriptions, and plats for all necessary Infrastructure to serve the project, including but not limited to lands, public rights of way, public utility easements, public water main easements, public sewer easements, irrevocable offers of dedication and temporary construction easements. All required easements as listed necessary for the infrastructure shall be reviewed and approved by the City and recorded with the Sacramento County Recorder pursuant to the timing requirements set forth in Section 3.8 of the ARDA, and any amendments thereto.	M	CD (E)	The owner/applicant provided all necessary public utility easements, grant deeds, offers of dedication or temporary construction easements required to build all of the required Backbone Infrastructure needed to serve the subdivision. These were recorded with Sacramento County Recorder by separate instrument.	Yes
44.		New Permanent Benchmarks The owner/applicant shall provide and establish new permanent benchmarks on the (NAVD 88) datum in various locations within the subdivision or at any other locations in the vicinity of the project/subdivision as directed by the City Engineer. The type and specifications for the permanent benchmarks shall be provided by the City. The new benchmarks shall be placed by the owner/applicant within 6	M	CD (E)	The owner/applicant has installed new benchmarks per the direction of the City Engineer and the new benchmarks have been placed in compliance with this condition of approval.	Yes

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		months from the date of approval of the vesting tentative subdivision map.				
45.		Centralized Mail Delivery Units All Final Maps shall show easements or other mapped provisions for the placement of centralized mail delivery units. The owner/applicant shall provide a concrete base for the placement of any centralized mail delivery unit. Specifications and location of such base shall be determined pursuant to the applicable requirements of the U. S. Postal Service and the City of Folsom Community Development Department, with due consideration for street light location, traffic safety, security, and consumer convenience.	M	CD (E)	The final map this subdivision includes the required easement for the placement of centralized mail delivery units. The USPS will provide the owner/applicant with the location of the mail delivery unit and will provide a concrete pad for the placement is the specified location in the subdivision.	Yes
46.		Recorded Final Map Prior to the issuance of building permits, the owner/applicant shall provide a digital copy of the recorded Final Map (in AutoCAD format) to the Community Development Department. The exception to this requirement is model homes. Building permits for model homes only may be issued prior to recording of the Final Map, subject to approval by the Community Development Department.	B	CD (E)	The Community Development Department shall verify that the owner/applicant has provided the required recorded copy of the final map to the CDD prior to building permit issuance in the subdivision.	Condition will be satisfied prior to issuance of a building permit.
47.		Recorded Final Map Prior to issuance of building permits, the owner/applicant shall provide the Folsom- Cordova Unified School District with a copy of the recorded Final Map.	B	CD (P), FCUSD	The Community Development Department shall verify that the owner/applicant has provided the required recorded copy of the final map to the FCUSD prior to building permit issuance in the subdivision.	Condition will be satisfied prior to issuance of a building permit.
48.		Credit Reimbursement Agreement Prior to the recordation of the first Small-Lot Final Map, the Owner/Applicant and City shall enter into a credit and reimbursement agreement for constructed improvements that are included in the Folsom Plan Area's Public Facilities Financing Plan.	M	CD (E)	The owner/applicant has executed all of the required Specific Plan Infrastructure Fee Credit/Reimbursement Agreements for the Mangini Ranch Phase 1C North & South subdivision. The executed agreements are on file with the Community Development Department.	Yes

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TRAFFIC/ACCESS/CIRCULATION/PARKING REQUIREMENTS						
49.		The following conditions of approval are related to roadway and traffic related improvements for the Phase 1 C South Subdivision project: <ol style="list-style-type: none"> a. Vehicle Access shall be granted on Street F to provide and maintain secondary access to the north (via the Mangini Ranch Phase 1C North project) for a connection to Mangini Parkway. b. Required public and private subdivision improvements, including but not limited to street and frontage improvements on Savannah Parkway, and Mangini Parkway shall be completed prior to issuance of the first Certificate of Occupancy for the subdivision. c. The northbound left-turn from Savannah Parkway into Street "A" shall be constructed to include a minimum of 125-feet of storage/ deceleration plus a 60-foot bay taper. d. The Project shall install the traffic signal control on Savannah Parkway at Mangini Parkway to the satisfaction of the City prior to the issuance of the first certificate of occupancy. The northbound left-turn lane on Savannah Parkway to Mangini Parkway shall be constructed to include a minimum of 180-feet of storage/deceleration plus a 60-foot bay taper. e. The Project shall construct shoulder improvements along the Project's entire frontage of westbound White Rock Road to the satisfaction of the City prior to approval of the final map. In lieu of constructing the interim shoulder improvements, the Project may enter into a 	B	CD (E), PW, FD	The Community Development Department has reviewed and approved the improvement plans for the construction of the subdivision. The approved plans include the required off-site improvements including, the emergency vehicle access and the street and frontage improvements on Savannah Parkway. The Community Development Department has also approved the plans for the adjoining Mangini Ranch Phase 1C North Village No. 3 subdivision (PN21-003) including a new traffic signal at the intersection of Mangini Parkway and Savannah Parkway and these improvements are being constructed concurrently with this subdivision.	Condition will be satisfied prior to issuance of a building permit.

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		Subdivision Improvement Agreement with the City and post adequate security to the City's satisfaction to ensure construction of said improvements; the security shall be in place for a minimum period of 10 years. If shoulder improvements are constructed and/or funded by the Project, then said costs may be included in an applicable fee program established and approved for the Folsom Plan Area subject to approval by the City and the actual costs expended by the owner/applicant may therefore be eligible for a credit and/or reimbursement. f. With the project, the Savannah Parkway frontage will be constructed and the right-of-way necessary for the ultimate intersection with the Capital Southeast Connector will be dedicated. All right-of-way within the City of Folsom required to construct the interim and ultimate improvements (as per Exhibit 1 of the traffic report shall be provided as part of this Project. g. If construction of the Capital Southeast Connector Project between East Bidwell Street and the El Dorado County line has commenced during the term of the required Subdivision Improvement Agreement, then the shoulder improvement condition will be deemed satisfied, and the security shall be released to the Owner/Applicant.				
50.		The Phase 1C South Subdivision project shall comply with the following architecture and design requirements: 1. This approval is for two-story master plans in three architectural styles with 9 color and material options. The Applicant shall submit building plans that comply	B,	CD (P) (B)	The Community Development Department will review and approve all site and building plans in the subdivision to verify compliance with this condition prior to building permit issuance on all lots in the subdivision.	Condition will be satisfied prior to issuance of a building permit.

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		with this approval and the attached building elevations dated December 4, 2020. 2. The design, materials, and colors of the single-family residential units shall be consistent with the approved building elevations, materials samples, and color schemes to the satisfaction of the Community Development Department. 3. Lots 13, 21-22, 49-55, 81, 90 and 93-102 shall have garages setback 18-feet and front yard setback of 12.5-feet. 4. Lots 14, 48, 56-64, 79-80, 91 and 104 shall have garages setback 18-feet and have the option of a front yard setback of 12.5-feet to accommodate a rear yard covered patio. 5. Lots 24-42 shall have garages setback 18-feet and a front yard setback of 12.5 to provide additional rear yard separation from the soundwall. 6. Side of all corner lots. All front yard irrigation and landscaping shall be installed prior to a Building Permit Final. 7. The Community Development Department shall approve the individual lot permits to assure no duplication or repetition of the same house, same roof-line, same elevation style, side-by-side, or across the street from each other. 8. All mechanical equipment shall be ground-mounted and concealed from view of public streets, neighboring properties and nearby higher buildings. 9. Decorative light fixtures, consistent with the Folsom Ranch Central District Design Guidelines and unique to each architectural design theme, shall be added to the front elevation of each Master Plan to the satisfaction of the Community Development				

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		Department. A minimum of one street tree shall be planted in the front yard of each residential lot within the subdivision. A minimum of two trees are required along the street-				
51.		Trash/Recycling Containers and Air Conditioner Screening Trash, recycling, and yard waste containers shall be placed behind the side yard fence so that they are not visible from the public right-of-way to the satisfaction of the Community Development Department. In addition, air conditioning units shall also be placed behind the side yard fence or located in the rear yard so that they are not visible from the public right-of-way to the satisfaction of the Community Development Department.	OG	CD (P) (E)	The Community Development Department will review and approve all site and building plans in the subdivision to verify compliance with this condition prior to building permit issuance on all lots in the subdivision.	Condition will be satisfied prior to issuance of a building permit.
MITIGATION MEASURES						
52.		Phase 1 C South Subdivision Mitigation Monitoring Reporting Program (MMRP). The conditions of approval below (numbered 53-1 to 53-89) implement the applicable mitigation measures from the FPASP (May 2011) MMRP, as amended by the Revised Proposed Water Supply Facility Alternative (November 2012), the Folsom South of U.S. Highway 50 Backbone Infrastructure Mitigated Negative Declaration (December 2014), and the Westland Eagle Specific Plan Amendment (September 2015)				
AESTHETIC						
53-1	3A.1-4 (FPASP EIR/EIS)	Screen Construction Staging Areas. The project applicant(s) for any particular discretionary development application shall locate staging and material storage areas as far away from sensitive biological resources and sensitive land uses (e.g., residential areas, schools, parks) as feasible. Staging and material storage areas shall be approved by the appropriate agency (identified below) before the approval of grading plans for all project phases	Before approval of grading plans and during construction for all project phases.	City of Folsom Community Dev. Department.	The construction staging area does not require screening due to distance from existing residences (>1000 feet).	Yes

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		<p>and shall be screened from adjacent occupied land uses in earlier development phases to the maximum extent practicable. Screens may include, but are not limited to, the use of such visual barriers such as berms or fences. The screen design shall be approved by the appropriate agency to further reduce visual effects to the extent possible.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries shall be developed by the project applicant(s) of each applicable project phase in consultation with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, and Caltrans) to reduce to the extent feasible the visual effects of construction activities on adjacent project land uses that have already been developed.</p>				
53-2	3A.1-5 (FPASP EIR/EIS)	<p>Establish and Require Conformance to Lighting Standards and Prepare and Implement a Lighting Plan. To reduce impacts associated with light and glare, the City shall:</p> <p>Establish standards for on-site outdoor lighting to reduce high-intensity nighttime lighting and glare as part of the Folsom Specific Plan design guidelines/standards. Consideration shall be given to design features, namely directional shielding for street lighting, parking lot lighting, and other substantial light sources, that would reduce effects of nighttime lighting. In addition, consideration shall be given to the use of automatic shutoffs or motion sensors for lighting features to further reduce excess nighttime light.</p> <p>Use shielded or screened public lighting fixtures to prevent the light from shining off of the surface intended to be illuminated. To reduce impacts associated with light and glare, the project applicant(s) of all project phases shall: Shield or screen lighting fixtures to direct the light downward</p>	Before approval of building permits.	City of Folsom Community Development Department	The Community Development Department (CDD) has reviewed and approved lighting plan for subdivision	Yes

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		and prevent light spill on adjacent properties. Flood and area lighting needed for construction activities, nighttime sporting activities, and/or security shall be screened or aimed no higher than 45 degrees above straight down (half-way between straight down and straight to the side) when the source is visible from any off-site residential property or public roadway. For public lighting in residential neighborhoods, prohibit the use of light fixtures that are of unusually high intensity or brightness (e.g., harsh mercury vapor, low-pressure sodium, or fluorescent bulbs) or that blink or flash. Use appropriate building materials (such as low-glare glass, low-glare building glaze or finish, neutral, earth-toned colored paint and roofing materials), shielded or screened lighting, and appropriate signage in the office/commercial areas to prevent light and glare from adversely affecting motorists on nearby roadways. Design exterior on-site lighting as an integral part of the building and landscape design in the Folsom Specific Plan area. Lighting fixtures shall be architecturally consistent with the overall site design.				
AIR QUALITY						
53-3	3A.2-1a (FPASP EIR/EIS)	<i>Implement Measures to Control Air Pollutant Emissions Generated by Construction of On-Site Elements.</i> To reduce short-term construction emissions, the project applicant(s) for any particular discretionary development application shall require their contractors to implement SMAQMD's list of Basic Construction Emission Control Practices, Enhanced Fugitive PM Dust Control Practices, and Enhanced Exhaust Control Practices (list below) in effect at the time individual portions of the site undergo construction. In addition to SMAQMD-recommended measures, construction operations shall comply with all applicable SMAQMD rules and regulations.	Before the approval of all grading plans by the City and throughout project construction, where applicable, for all project phases.	City of Folsom Community Development Department	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes

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		<p>Basic Construction Emission Control Practices Water all exposed surfaces two times daily. Exposed surfaces include, but are not limited to soil piles, graded areas, unpaved parking areas, staging areas, and access roads. Cover or maintain at least two feet of free board space on haul trucks transporting soil, sand, or other loose material on the site. Any haul trucks that would be traveling along freeways or major roadways should be covered. Use wet power vacuum street sweepers to remove any visible trackout mud or dirt onto adjacent public roads at least once a day. Use of dry power sweeping is prohibited. Limit vehicle speeds on unpaved roads to 15 miles per hour (mph). All roadways, driveways, sidewalks, parking lots to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used. Minimize idling time either by shutting equipment off when not in use or reducing the time of idling to 5 minutes (as required by the state airborne toxics control measure [Title 13, Section 2485 of the California Code of Regulations]). Provide clear signage that posts this requirement for workers at the entrances to the site. Maintain all construction equipment in proper working condition according to manufacturer's specifications. The equipment must be checked by a certified mechanic and determine to be running in proper condition before it is operated.</p> <p>Enhanced Fugitive PM Dust Control Practices – Soil Disturbance Areas Water exposed soil with adequate frequency for continued moist soil. However, do not overwater to the extent that sediment flows off the site. Suspend excavation, grading, and/or demolition activity when</p>				

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		<p>wind speeds exceed 20 mph. Plant vegetative ground cover (fast-germinating native grass seed) in disturbed areas as soon as possible. Water appropriately until vegetation is established.</p> <p>Enhanced Fugitive PM Dust Control Practices – Unpaved Roads Install wheel washers for all exiting trucks, or wash off all trucks and equipment leaving the site. Treat site accesses to a distance of 100 feet from the paved road with a 6 to 12-inch layer of wood chips, mulch, or gravel to reduce generation of road dust and road dust carryout onto public roads. Post a publicly visible sign with the telephone number and person to contact at the construction site regarding dust complaints. This person shall respond and take corrective action within 48 hours. The phone number of SMAQMD and the City contact person shall also be posted to ensure compliance.</p> <p>Enhanced Exhaust Control Practices The project shall provide a plan, for approval by the City of Folsom Community Development Department and SMAQMD, demonstrating that the heavy-duty (50 horsepower [hp] or more) off-road vehicles to be used in the construction project, including owned, leased, and subcontractor vehicles, will achieve a project wide fleet-average 20% NOX reduction and 45% particulate reduction compared to the most current California Air Resources Board (ARB) fleet average that exists at the time of construction. Acceptable options for reducing emissions may include use of late-model engines, low-emission diesel products, alternative fuels, engine retrofit technology, after-treatment products, and/or other options as they become available. The project applicant(s) of each</p>				

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<p>project phase or its representative shall submit to the City of Folsom Community Development Department and SMAQMD a comprehensive inventory of all off-road construction equipment, equal to or greater than 50 hp, that would be used an aggregate of 40 or more hours during any portion of the construction project. The inventory shall include the horsepower rating, engine production year, and projected hours of use for each piece of equipment. The inventory shall be updated and submitted monthly throughout the duration of the project, except that an inventory shall not be required for any 30-day period in which no construction activity occurs. At least 48 hours prior to the use of heavy-duty off-road equipment, the project representative shall provide SMAQMD with the anticipated construction timeline including start date, and name and phone number of the project manager and on-site foreman. SMAQMD's Construction Mitigation Calculator can be used to identify an equipment fleet that achieves this reduction (SMAQMD 2007a). The project shall ensure that emissions from all off-road diesel powered equipment used on the SPA do not exceed 40% opacity for more than three minutes in any one hour. Any equipment found to exceed 40 percent opacity (or Ringelmann 2.0) shall be repaired immediately, and the City and SMAQMD shall be notified within 48 hours of identification of noncompliant equipment. A visual survey of all in-operation equipment shall be made at least weekly, and a monthly summary of the visual survey results shall be submitted throughout the duration of the project, except that the monthly summary shall not be required for any 30-day period in which no construction activity occurs. The monthly summary shall include the quantity and type of vehicles surveyed as well as the dates of each survey. SMAQMD</p>				

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		staff and/or other officials may conduct periodic site inspections to determine compliance. Nothing in this mitigation measure shall supersede other SMAQMD or state rules or regulations. If at the time of construction, SMAQMD has adopted a regulation or new guidance applicable to construction emissions, compliance with the regulation or new guidance may completely or partially replace this mitigation if it is equal to or more effective than the mitigation contained herein, and if SMAQMD so permits.				
53-4	3A.2-1b (FPASP EIR/EIS)	<p><i>Pay Off-site Mitigation Fee to SMAQMD to Off-Set NOX Emissions Generated by Construction of On-Site Elements.</i></p> <p>Implementation of the project or the other four other action alternatives would result in construction-generated NOX emissions that exceed the SMAQMD threshold of significance, even after implementation of the SMAQMD Enhanced Exhaust Control Practices (listed in Mitigation Measure 3A.2-1a). Additionally, Mitigation Measure 3A.4-1 (Implement Additional Measures to Control Construction-Generated GHG Emissions, pages 3A.4-14 to 15) has the potential to both reduce and increase NOX emissions, depending on the types of alternative fuels and engine types employed. Therefore, the project applicant(s) shall pay SMAQMD an off-site mitigation fee for implementation of any of the five action alternatives for the purpose of reducing NOX emissions to a less-than- significant level (i.e., less than 85 lb/day). All NOX emission reductions and increases associated with GHG mitigation shall be added to or subtracted from the amount above the construction threshold to determine off-site mitigation fees, when possible. The specific fee amounts shall be calculated when the daily construction emissions can be more accurately determined: that is, if the City/USACE select</p>	Before the approval of all grading plans by the City and throughout project construction for all project phases.	The City of Folsom Community Development Department shall not grant any grading permits to the respective project applicant(s) until the respective project applicant(s) have paid the appropriate off-site mitigation fee to SMAQMD.	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
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		and certify the EIR/EIS and approves the Proposed Project or one of the other four other action alternatives, the City and the applicants must establish the phasing by which development would occur, and the applicants must develop a detailed construction schedule. Calculation of fees associated with each project development phase shall be conducted by the project applicant(s) in consultation with SMAQMD staff before the approval of grading plans by the City. The project applicant(s) for any particular discretionary development application shall pay into SMAQMD's off-site construction mitigation fund to further mitigate construction generated emissions of NOX that exceed SMAQMD's daily emission threshold of 85 lb/day. The calculation of daily NOX emissions shall be based on the cost rate established by SMAQMD at the time the calculation and payment are made. At the time of writing this EIR/EIS the cost rate is \$16,000 to reduce 1 ton of NOX plus a 5% administrative fee (SMAQMD 2008c). The determination of the final mitigation fee shall be conducted in coordination with SMAQMD before any ground disturbance occurs for any project phase.				
53-5	3A.2-1c (FPASP EIR/EIS)	Analyze and Disclose Projected PM10 Emission Concentrations at Nearby Sensitive Receptors Resulting from Construction of On-Site Elements. Prior to construction of each discretionary development entitlement of on-site land uses, the project applicant shall perform a project-level CEQA analysis (e.g., supporting documentation for an exemption, negative declaration, or project-specific EIR) that includes detailed dispersion modeling of construction-generated PM10 to disclose what PM10 concentrations would be at nearby sensitive receptors. The dispersion modeling shall be performed in accordance with applicable SMAQMD guidance that is in place at the time the analysis is performed. At the time of writing this EIR/EIS, SMAQMD's most current and most detailed guidance for addressing construction-generated PM10 emissions is found in its Guide to Air Quality Assessment in Sacramento County (SMAQMD 2009a). The project-level analysis shall incorporate detailed parameters of	Before the approval of all grading plans by the City.	City of Folsom Community Development Department	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Spring of 2021. Compliance table is on file with the City.	Yes

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		the construction equipment and activities, including the year during which construction would be performed, as well as the proximity of potentially affected receptors, including receptors proposed by the project that exist at the time the construction activity would occur.				
53-6	3A.2-2 (FPASP EIR/EIS)	<p><i>Implement All Measures Prescribed by the Air Quality Mitigation Plan to Reduce Operational Air Pollutant Emissions.</i></p> <p>To reduce operational emissions, the project applicant(s) for any particular discretionary development application shall implement all measures prescribed in the SMAQMD-approved Folsom Plan Area Specific Plan Air Quality Mitigation Plan (AQMP) (Torrence Planning 2008), a copy of which is included in Appendix C2. The AQMP is intended to improve mobility, reduce vehicle miles traveled, and improve air quality as required by AB 32 and SB 375. The AQMP includes, among others, measures designed to provide bicycle parking at commercial land uses, an integrated pedestrian/bicycle path network, transit stops with shelters, a prohibition against the use the wood-burning fireplaces, energy star roofing materials, electric lawnmowers provided to homeowners at no charge, and on-site transportation alternatives to passenger vehicles (including light rail) that provide connectivity with other local and regional alternative transportation networks.</p>	Before issuance of subdivision maps or improvement plans.	City of Folsom Community Development Department	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes
53-7	3A.2-4a (FPASP EIR/EIS)	<p><i>Develop and Implement a Plan to Reduce Exposure of Sensitive Receptors to Construction-Generated Toxic Air Contaminant Emissions.</i></p> <p>The project applicant(s) for any particular discretionary development application shall develop a plan to reduce the exposure of sensitive receptors to TACs generated by project construction activity associated with buildout of the selected alternative. Each plan shall be developed by the project applicant(s) in consultation with SMAQMD. The plan shall be submitted to the City for review and approval before the approval of any grading plans.</p>	Before the approval of all grading plans by the City and throughout project construction, where applicable, for	City of Folsom Community Development Department	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes

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		The plan may include such measures as scheduling activities when the residences are the least likely to be occupied, requiring equipment to be shut off when not in use, and prohibiting heavy trucks from idling. Applicable measures shall be included in all project plans and specifications for all project phases. The implementation and enforcement of all measures identified in each plan shall be funded by the project applicant(s) for the respective phase of development.	all project phases.			
53-8	3A.2-6 (FPASP EIR/EIS)	<p><i>Implement Measures to Control Exposure of Sensitive Receptors to Operational Odorous Emissions.</i></p> The project applicant(s) for any particular discretionary development application shall implement the following measure: The deeds to all properties located within the plan area that are within one mile of an on- or off-site area zoned or used for agricultural use (including livestock grazing) shall be accompanied by a written disclosure from the transferor, in a form approved by the City of Folsom, advising any transferee of the potential adverse odor impacts from surrounding agricultural operations, which disclosure shall direct the transferee to contact the County of Sacramento concerning any such property within the County zoned for agricultural uses within one mile of the subject property being transferred.	Before the approval of building permits by the City and throughout project construction, where applicable, for all project phases.	City of Folsom Community Development Department	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes
BIOLOGICAL RESOURCES						
53-9	3A.3-1a (FPASP EIR/EIS)	<p><i>Design Stormwater Drainage Plans and Erosion and Sediment Control Plans to Avoid and Minimize Erosion and Runoff to All Wetlands and Other Waters That Are to Remain on the SPA and Use Low Impact Development Features.</i></p> To minimize indirect effects on water quality and wetland hydrology, the project applicant(s) for any particular discretionary development application shall include	Before the approval of all grading plans by the City and throughout project construction,	City of Folsom Community Development Department	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City. There are no off-site elements outside the City limits for this subdivision.	Yes

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		<p>stormwater drainage plans and erosion and sediment control plans in their improvement plans and shall submit these plans to the City Public Works Department for review and approval. For off-site elements within Sacramento County or El Dorado County jurisdiction (e.g., off-site detention basin and off-site roadway connections to El Dorado Hills), plans shall be submitted to the appropriate county planning department. Before approval of these improvement plans, the project applicant(s) for any particular discretionary development application shall obtain a NPDES MS4 Municipal Stormwater Permit and Grading Permit, comply with the City's Grading Ordinance and County drainage and stormwater quality standards, and commit to implementing all measures in their drainage plans and erosion and sediment control plans to avoid and minimize erosion and runoff into Alder Creek and all wetlands and other waters that would remain on-site. Detailed information about stormwater runoff standards and relevant City and County regulation is provided in Chapter 3A.9, "Hydrology and Water Quality."</p> <p>The project applicant(s) for any particular discretionary development entitlement shall implement stormwater quality treatment controls consistent with the Stormwater Quality Design Manual for Sacramento and South Placer Regions in effect at the time the application is submitted. Appropriate runoff controls such as berms, storm gates, off-stream detention basins, overflow collection areas, filtration systems, and sediment traps shall be implemented to control siltation and the potential discharge of pollutants. Development plans shall incorporate Low Impact Development (LID) features, such as pervious strips, permeable pavements, bioretention ponds, vegetated swales, disconnected rain gutter downspouts, and rain gardens, where appropriate. Use of LID features is recommended by the EPA to minimize impacts on water quality, hydrology, and stream geomorphology and is specified as a method for protecting water quality in the proposed specific plan. In addition, free spanning bridge systems shall be used for all</p>	where applicable, for all project phases.			

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		<p>roadway crossings over wetlands and other waters that are retained in the on-site open space. These bridge systems would maintain the natural and restored channels of creeks, including the associated wetlands, and would be designed with sufficient span width and depth to provide for wildlife movement along the creek corridors even during high-flow or flood events, as specified in the 404 permit. In addition to compliance with City ordinances, the project applicant(s) for any particular discretionary development application shall prepare a Stormwater Pollution Prevention Plan (SWPPP), and implement Best Management Practices (BMPs) that comply with the General Construction Stormwater Permit from the Central Valley RWQCB, to reduce water quality effects during construction. Detailed information about the SWPPP and BMPs are provided in Chapter 3A.9, "Hydrology and Water Quality."</p> <p>Each project development shall result in no net change to peak flows into Alder Creek and associated tributaries, or to Buffalo Creek, Carson Creek, and Coyote Creek. The project applicant(s) shall establish a baseline of conditions for drainage on-site. The baseline-flow conditions shall be established for 2-, 5-, and 100-year storm events. These baseline conditions shall be used to develop monitoring standards for the stormwater system on the SPA. The baseline conditions, monitoring standards, and a monitoring program shall be submitted to USACE and the City for their approval. Water quality and detention basins shall be designed and constructed to ensure that the performance standards, which are described in Chapter 3A.9, "Hydrology and Water Quality," are met and shall be designed as off-stream detention basins. Discharge sites into Alder Creek and associated tributaries, as well as tributaries to Carson Creek, Coyote Creek, and Buffalo Creek, shall be monitored to ensure that pre-project conditions are being met. Corrective measures shall be implemented as necessary. The mitigation measures will be satisfied when the monitoring standards are met for 5 consecutive years without</p>				

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		undertaking corrective measures to meet the performance standard. See FEIR/FEIS Appendix S showing that the detention basin in the northeast corner of the SPA has been moved off stream. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase in consultation with the affected oversight agency(ies) (i.e., El Dorado County for the roadway connections, Sacramento County for the detention basin west of Prairie City Road, and Caltrans for the U.S. 50 interchange improvements) such				
53-10	3A.3-2a (FPASP EIR/EIS)	<p>Avoid Direct Loss of Swainson's Hawk and Other Raptor Nests.</p> <p>To mitigate impacts on Swainson's hawk and other raptors (including burrowing owl), the project applicant(s) of all project phases shall retain a qualified biologist to conduct preconstruction surveys and to identify active nests on and within 0.5 mile of the project and active burrows on the project site. The surveys shall be conducted before the approval of grading and/or improvement plans (as applicable) and no less than 14 days and no more than 30 days before the beginning of construction for all project phases. To the extent feasible, guidelines provided in Recommended Timing and Methodology for Swainson's Hawk Nesting Surveys in the Central Valley (Swainson's Hawk Technical Advisory Committee 2000) shall be followed for surveys for Swainson's hawk. If no nests are found, no further mitigation is required.</p> <p>If active nests are found, impacts on nesting Swainson's hawks and other raptors shall be avoided by establishing appropriate buffers around the nests. No project activity shall commence within the buffer area until the young have fledged, the nest is no longer active, or until a qualified biologist has determined in consultation with DFG that</p>	Before the approval of grading and improvement plans, before any ground disturbing activities, and during project construction as applicable for all project phases.	California Department of Fish and Game and City of Folsom Community Development Department.	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		reducing the buffer would not result in nest abandonment. DFG guidelines recommend implementation of 0.25- or 0.5-mile-wide buffers, but the size of the buffer may be adjusted if a qualified biologist and the City, in consultation with DFG, determine that such an adjustment would not be likely to adversely affect the nest. Monitoring of the nest by a qualified biologist during and after construction activities will be required if the activity has potential to adversely affect the nest. If active burrows are found, a mitigation plan shall be submitted to the City for review and approval before any ground-disturbing activities. The City shall consult with DFG. The mitigation plan may consist of installation of one-way doors on all burrows to allow owls to exit, but not reenter, and construction of artificial burrows within the project vicinity, as needed; however, burrow owl exclusions may only be used if a qualified biologist verifies that the burrow does not contain eggs or dependent young. If active burrows contain eggs and/or young, no				
GEOLOGY AND SOILS						
53-11	3A.7-1a (FPASP EIR/EIS)	Prepare Site-Specific Geotechnical Report per CBC Requirements and Implement Appropriate Recommendations. Before building permits are issued and construction activities begin any project development phase, the project applicant(s) of each project phase shall hire a licensed geotechnical engineer to prepare a final geotechnical subsurface investigation report for the on- and off-site facilities, which shall be submitted for review and approval to the appropriate City or county department (identified below). The final geotechnical engineering report shall address and make recommendations on the following: Site preparation; Soil bearing capacity;	Before issuance of building permits and ground-disturbing activities.	City of Folsom Community Development Department	Owner/applicant has provided Geotechnical Report to the City. The Geotechnical report for the subdivision is on file with the City.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Appropriate sources and types of fill; Potential need for soil amendments; Road, pavement, and parking areas; Structural foundations, including retaining-wall design; Grading practices; Soil corrosion of concrete and steel; Erosion/winterization; Seismic ground shaking; Liquefaction; and Expansive/unstable soils. In addition to the recommendations for the conditions listed above, the geotechnical investigation shall include subsurface testing of soil and groundwater conditions, and shall determine appropriate foundation designs that are consistent with the version of the CBC that is applicable at the time building and grading permits are applied for. All recommendations contained in the final geotechnical engineering report shall be implemented by the project applicant(s) of each project phase. Special recommendations contained in the geotechnical engineering report shall be noted on the grading plans and implemented as appropriate before construction begins. Design and construction of all new project development shall be in accordance with the CBC. The project applicant(s) shall provide for engineering inspection and certification that earthwork has been performed in conformity with recommendations contained in the geotechnical report.				
53-12	3A.7-1b (FPASP EIR/EIS)	<i>Monitor Earthwork during Earthmoving Activities.</i> All earthwork shall be monitored by a qualified geotechnical or soils engineer retained by the project applicant(s) of each project phase. The geotechnical or soils engineer shall	Before issuance of building	City of Folsom Community	Compliance with this condition has been monitored through construction inspection by the City.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		provide oversight during all excavation, placement of fill, and disposal of materials removed from and deposited on both on- and off-site construction areas. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, or Caltrans).	permits and ground-disturbing activities.	Development Department		
53-13	3A.7-3 (FPASP EIR/EIS)	<p><i>Prepare and Implement the Appropriate Grading and Erosion Control Plan.</i></p> <p>Before grading permits are issued, the project applicant(s) of each project phase that would be located within the City of Folsom shall retain a California Registered Civil Engineer to prepare a grading and erosion control plan. The grading and erosion control plan shall be submitted to the City Public Works Department before issuance of grading permits for all new development. The plan shall be consistent with the City's Grading Ordinance, the City's Hillside Development Guidelines, and the state's NPDES permit, and shall include the site-specific grading associated with development for all project phases.</p> <p>The plans referenced above shall include the location, implementation schedule, and maintenance schedule of all erosion and sediment control measures, a description of measures designed to control dust and stabilize the construction-site road and entrance, and a description of the location and methods of storage and disposal of construction materials. Erosion and sediment control measures could include the use of detention basins, berms, swales, wattles, and silt fencing, and covering or watering of stockpiled soils to reduce wind erosion. Stabilization on steep slopes could include construction of retaining walls and reseeded with vegetation after construction. Stabilization of construction entrances to minimize trackout (control dust) is commonly achieved by installing filter fabric and crushed rock to a depth of approximately 1 foot. The project</p>	Before the start of construction activities.	City of Folsom Community Development Department	Compliance with this condition has been monitored through construction inspection by the City.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<p>applicant(s) shall ensure that the construction contractor is responsible for securing a source of transportation and deposition of excavated materials.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties).</p> <p>Implementation of Mitigation Measure 3A.9-1 (discussed in Section 3A.9, "Hydrology and Water Quality – Land") would also help reduce erosion-related impacts.</p>				
53-14	3A.7-5 (FPASP EIR/EIS)	<p><i>Divert Seasonal Water Flows Away from Building Foundations.</i></p> <p>The project applicant(s) of all project phases shall either install subdrains (which typically consist of perforated pipe and gravel, surrounded by nonwoven geotextile fabric), or take such other actions as recommended by the geotechnical or civil engineer for the project that would serve to divert seasonal flows caused by surface infiltration, water seepage, and perched water during the winter months away from building foundations.</p>	Before and during earthmoving activities.	City of Folsom CDD	The Community Development Department has reviewed and approved all improvement plans in the subdivision to verify compliance with mitigation measure.	Yes
53-15	3A.7-10 (FPASP EIR/EIS)	<p><i>Conduct Construction Personnel Education, Stop Work if Paleontological Resources are Discovered, Assess the Significance of the Find, and Prepare and Implement a Recovery Plan as Required.</i> To minimize potential adverse impacts on previously unknown potentially unique, scientifically important paleontological resources, the project applicant(s) of all project phases where construction would occur in the lone and Mehrten Formations shall do the following:</p> <p>Before the start of any earthmoving activities for any project phase in the lone or Mehrten Formations, the project applicant(s) shall retain a qualified paleontologist or archaeologist to train all construction personnel involved with earthmoving activities, including the</p>	During earthmoving activities in the lone and Mehrten Formations.	City of Folsom CDD	<p>The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.</p> <p>No human remains or paleontological resources have been encountered in the subdivision during grading and construction.</p>	Yes

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		<p>site superintendent, regarding the possibility of encountering fossils, the appearance and types of fossils likely to be seen during construction, and proper notification procedures should fossils be encountered.</p> <p>If paleontological resources are discovered during earthmoving activities, the construction crew shall immediately cease work in the vicinity of the find and notify the appropriate lead agency (identified below). The project applicant(s) shall retain a qualified paleontologist to evaluate the resource and prepare a recovery plan in accordance with Society of Vertebrate Paleontology guidelines (1996). The recovery plan may include, but is not limited to, a field survey, construction monitoring, sampling and data recovery procedures, museum storage coordination for any specimen recovered, and a report of findings. Recommendations in the recovery plan that are determined by the lead agency to be necessary and feasible shall be implemented before construction activities can resume at the site where the paleontological resources were discovered.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Sacramento County).</p>				
GREENHOUSE GAS EMISSIONS AND CLIMATE CHANGE						
53-16	3A.4-1 (FPASP EIR/EIS)	<p>Implement Additional Measures to Control Construction-Generated GHG Emissions.</p> <p>To further reduce construction-generated GHG emissions, the project applicant(s) shall implement all feasible measures for reducing GHG emissions associated with construction that are recommended by SMAQMD at the time individual portions of the site undergo construction. Such measures may reduce GHG exhaust emissions from the use of on-site</p>	Before approval of small-lot final maps and building permits for all discretionary development project, including	City of Folsom Community Development Department	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<p>equipment, worker commute trips, and truck trips carrying materials and equipment to and from the SPA, as well as GHG emissions embodied in the materials selected for construction (e.g., concrete). Other measures may pertain to the materials used in construction. Prior to releasing each request for bid to contractors for the construction of each discretionary development entitlement, the project applicant(s) shall obtain the most current list of GHG reduction measures that are recommended by SMAQMD and stipulate that these measures be implemented in the respective request for bid as well as the subsequent construction contract with the selected primary contractor. The project applicant(s) for any particular discretionary development application may submit to the City and SMAQMD a report that substantiates why specific measures are considered infeasible for construction of that particular development phase and/or at that point in time. The report, including the substantiation for not implementing particular GHG reduction measures, shall be approved by the City, in consultation with SMAQMD prior to the release of a request for bid by the project applicant(s) for seeking a primary contractor to manage the construction of each development project. By requiring that the list of feasible measures be established prior to the selection of a primary contractor, this measure requires that the ability of a contractor to effectively implement the selected GHG reduction measures be inherent to the selection process.</p> <p>SMAQMD's recommended measures for reducing construction-related GHG emissions at the time of writing this EIR/EIS are listed below and the project applicant(s) shall, at a minimum, be required to implement the following: Improve fuel efficiency from construction equipment; reduce unnecessary idling (modify work practices, install auxiliary power for driver comfort); perform equipment maintenance (inspections, detect failures early, corrections); train equipment operators in proper use of equipment;</p>	<p>all on- and off-site elements and implementation throughout project construction.</p>			

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
53-17	3A.8-2 (FPASP EIR/EIS)	<p><i>Complete Investigations Related to the Extent to Which Soil and/or Groundwater May Have Been Contaminated in Areas Not Covered by the Phase I and II Environmental Site Assessments and Implement Required Measures.</i></p> <p>The project applicant(s) for any discretionary development application shall conduct Phase I Environmental Site Assessments (where an Phase I has not been conducted), and if necessary, Phase II Environmental Site Assessments, and/or other appropriate testing for all areas of the SPA and include, as necessary, analysis of soil and/or groundwater samples for the potential contamination sites that have not yet been covered by previous investigations (as shown in Exhibit 3A.8-1) before construction activities begin in those areas. Recommendations in the Phase I and II Environmental Site Assessments to address any contamination that is found shall be implemented before initiating ground-disturbing activities in these areas.</p> <p>The project applicant(s) shall implement the following measures before ground-disturbing activities to reduce health hazards associated with potential exposure to hazardous substances: Prepare a plan that identifies any necessary remediation activities appropriate for proposed on- and off-site uses, including excavation and removal of on-site contaminated soils, redistribution of clean fill material in the SPA, and closure of any abandoned mine shafts. The plan shall include measures that ensure the safe transport, use, and disposal of contaminated soil and building debris removed from the site. In the event that contaminated groundwater is encountered during site excavation activities, the contractor shall report the contamination to the appropriate regulatory agencies.</p>	Before and during earth moving activities	City of Folsom Community Development Department	<p>The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.</p> <p>No hazardous and/or contaminated soil or groundwater has been discovered in the subdivision.</p>	Yes

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		<p>dewater the excavated area, and treat the contaminated groundwater to remove contaminants before discharge into the sanitary sewer system. The project applicant(s) shall be required to comply with the plan and applicable Federal, state, and local laws. The plan shall outline measures for specific handling and reporting procedures for hazardous materials and disposal of hazardous materials removed from the site at an appropriate off-site disposal facility.</p> <p>Notify the appropriate Federal, state, and local agencies if evidence of previously undiscovered soil or groundwater contamination (e.g., stained soil, odorous groundwater) is encountered during construction activities. Any contaminated areas shall be remediated in accordance with recommendations made by the Sacramento County Environmental Management Department, Central Valley RWQCB, DTSC, and/or other appropriate Federal, state, or local regulatory agencies.</p> <p>Obtain an assessment conducted by PG&E and SMUD pertaining to the contents of any existing pole-mounted transformers located in the SPA. The assessment shall determine whether existing on-site electrical transformers contain PCBs and whether there are any records of spills from such equipment. If equipment containing PCB is identified, the maintenance and/or disposal of the transformer shall be subject to the regulations of the Toxic Substances Control Act under the authority of the Sacramento County Environmental Health Department.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., Sacramento County).</p>				
HYDROLOGY AND WATER QUALITY						

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
53-18	3A.9-1 (FPASP EIR/EIS)	<p>Acquire Appropriate Regulatory Permits and Prepare and Implement SWPPP and BMPs.</p> <p>Prior to the issuance of grading permits, the project applicant(s) of all projects disturbing one or more acres (including phased construction of smaller areas which are part of a larger project) shall obtain coverage under the SWRCB's NPDES stormwater permit for general construction activity (Order 2009-0009-DWQ), including preparation and submittal of a project-specific SWPPP at the time the NOI is filed. The project applicant(s) shall also prepare and submit any other necessary erosion and sediment control and engineering plans and specifications for pollution prevention and control to Sacramento County, City of Folsom, El Dorado County (for the off-site roadways into El Dorado Hills under the Proposed Project Alternative). The SWPPP and other appropriate plans shall identify and specify:</p> <p>The use of an effective combination of robust erosion and sediment control BMPs and construction techniques accepted by the local jurisdictions for use in the project area at the time of construction, that shall reduce the potential for runoff and the release, mobilization, and exposure of pollutants, including legacy sources of mercury from project-related construction sites. These may include but would not be limited to temporary erosion control and soil stabilization measures, sedimentation ponds, inlet protection, perforated riser pipes, check dams, and silt fences</p> <p>The implementation of approved local plans, non-stormwater management controls, permanent post-construction BMPs, and inspection and maintenance responsibilities;</p> <p>The pollutants that are likely to be used during construction</p>	<p>Submittal of the State Construction General Permit NOI and SWPPP (where applicable) and development and submittal of any other locally required plans and specifications before the issuance of grading permits for all on-site project phases and off-site elements and implementation throughout project construction.</p>	<p>City of Folsom Community Development Department</p>	<p>The owner/applicant has been issued a WDID # and has submitted a SWPPP approved by the RWQCB. SWPPP is on file at the City.</p>	<p>Yes</p>

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		<p>that could be present in stormwater drainage and non-stormwater discharges, including fuels, lubricants, and other types of materials used for equipment operation; Spill prevention and contingency measures, including measures to prevent or clean up spills of hazardous waste and of hazardous materials used for equipment operation, and emergency procedures for responding to spills;</p> <p>Personnel training requirements and procedures that shall be used to ensure that workers are aware of permit requirements and proper installation methods for BMPs specified in the SWPPP; and</p> <p>The appropriate personnel responsible for supervisory duties related to implementation of the SWPPP.</p> <p>Where applicable, BMPs identified in the SWPPP shall be in place throughout all site work and construction/demolition activities and shall be used in all subsequent site development activities. BMPs may include, but are not limited to, such measures as those listed below.</p> <p>Implementing temporary erosion and sediment control measures in disturbed areas to minimize discharge of sediment into nearby drainage conveyances, in compliance with state and local standards in effect at the time of construction. These measures may include silt fences, staked straw bales or wattles, sediment/silt basins and traps, geofabric, sandbag dikes, and temporary vegetation.</p> <p>Establishing permanent vegetative cover to reduce erosion in areas disturbed by construction by slowing runoff velocities, trapping sediment, and enhancing filtration and transpiration.</p> <p>Using drainage swales, ditches, and earth dikes to control erosion and runoff by conveying surface runoff down sloping land, intercepting and diverting runoff to a watercourse or channel, preventing sheet flow over sloped surfaces,</p>				

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		preventing runoff accumulation at the base of a grade, and avoiding flood damage along roadways and facility infrastructure. A copy of the approved SWPPP shall be maintained and available at all times on the construction site. For those areas that would be disturbed as part of the U.S. 50 interchange improvements, Caltrans shall coordinate with the development and implementation of the overall project SWPPP, or develop and implement its own SWPPP specific to the interchange improvements, to ensure that water quality degradation would be avoided or minimized to the maximum extent practicable. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, or Caltrans).				
53-19	3A.9-2 (FPASP EIR/EIS)	<p><i>Prepare and Submit Final Drainage Plans and Implement Requirements Contained in Those Plans.</i></p> Before the approval of grading plans and building permits, the project applicant(s) of all project phases shall submit final drainage plans to the City, and to El Dorado County for the off-site roadway connections into El Dorado Hills, demonstrating that off-site upstream runoff would be appropriately conveyed through the SPA, and that project-related on-site runoff would be appropriately contained in detention basins or managed with through other improvements (e.g., source controls, biotechnical stream stabilization) to reduce flooding and hydromodification impacts. The plans shall include, but not be limited to, the following items: An accurate calculation of pre-project and post-project runoff scenarios, obtained using appropriate engineering methods, that accurately evaluates potential changes to runoff, including increased surface runoff;	Before approval of grading plans and building permits of all project phases.	City of Folsom Community Development Department.	The City has reviewed and approved the storm drain plans for this subdivision. The storm drain improvements are in compliance with the approved Folsom Plan Storm Drain Master Plan approved by the City.	Yes

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		<p>Runoff calculations for the 10-year and 100-year (0.01 AEP) storm events (and other, smaller storm events as required) shall be performed and the trunk drainage pipeline sizes confirmed based on alignments and detention facility locations finalized in the design phase;</p> <p>A description of the proposed maintenance program for the on-site drainage system;</p> <p>Project-specific standards for installing drainage systems;</p> <p>City and El Dorado County flood control design requirements and measures designed to comply with them;</p> <p>Implementation of stormwater management BMPs that avoid increases in the erosive force of flows beyond a specific range of conditions needed to limit hydromodification and maintain current stream geomorphology. These BMPs will be designed and constructed in accordance with the forthcoming SSQP Hydromodification Management Plan (to be adopted by the RWQCB) and may include, but are not limited to, the following:</p> <ul style="list-style-type: none"> • Use of Low Impact Development (LID) techniques to limit increases in stormwater runoff at the point of origination (these may include, but are not limited to: surface swales; replacement of conventional impervious surfaces with pervious surfaces [e.g., porous pavement]; impervious surfaces disconnection; and trees planted to intercept stormwater); • Enlarged detention basins to minimize flow changes and changes to flow duration characteristics; • Bioengineered stream stabilization to minimize bank erosion, utilizing vegetative and rock stabilization, and inset floodplain restoration features that provide for enhancement of riparian habitat and maintenance of natural hydrologic and channel to floodplain interactions; 				

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		<ul style="list-style-type: none"> Minimize slope differences between any stormwater or detention facility outfall channel with the existing receiving channel gradient to reduce flow velocity; and Minimize to the extent possible detention basin, bridge embankment, and other encroachments into the channel and floodplain corridor, and utilize open bottom box culverts to allow sediment passage on smaller drainage courses. <p>The final drainage plan shall demonstrate to the satisfaction of the City of Folsom Community Development and Public Works Departments and El Dorado County Department of Transportation that 100-year (0.01 AEP) flood flows would be appropriately channeled and contained, such that the risk to people or damage to structures within or down gradient of the SPA would not occur, and that hydromodification would not be increased from pre-development levels such that existing stream geomorphology would be changed (the range of conditions should be calculated for each receiving water if feasible, or a conservative estimate should be used, e.g., an Ep of 1 ±10% or other as approved by the Sacramento Stormwater Quality Partnership and/or City of Folsom Public Works Department).</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with El Dorado County.</p>				
53-20	3A.9-3 (FPASP EIR/EIS)	<p>Develop and Implement a BMP and Water Quality Maintenance Plan. Before approval of the grading permits for any development project requiring a subdivision map, a detailed BMP and water quality maintenance plan shall be prepared by a qualified engineer retained by the project applicant(s) the development project. Drafts of the plan shall be submitted to the City of Folsom and El Dorado County for the off-site</p>	Prepare plans before the issuance of grading permits for all project phases and off-site	City of Folsom Community Development Department	The owner/applicant has been issued a WDID # and has submitted a SWPPP approved by the RWQCB. SWPPP is on file at the City.	Yes

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		<p>roadway connections into El Dorado Hills, for review and approval concurrently with development of tentative subdivision maps for all project phases. The plan shall finalize the water quality improvements and further detail the structural and nonstructural BMPs proposed for the project. The plan shall include the elements described below. A quantitative hydrologic and water quality analysis of proposed conditions incorporating the proposed drainage design features.</p> <p>Predevelopment and post development calculations demonstrating that the proposed water quality BMPs meet or exceed requirements established by the City of Folsom and including details regarding the size, geometry, and functional timing of storage and release pursuant to the "Stormwater Quality Design Manual for Sacramento and South Placer Regions" (ISSQP 2007b) per NPDES Permit No. CAS082597 WDR Order No. R5-2008-0142, page 46) and El Dorado County's NPDES SWMP (County of El Dorado 2004).</p> <p>Source control programs to control water quality pollutants on the SPA, which may include but are limited to recycling, street sweeping, storm drain cleaning, household hazardous waste collection, waste minimization, prevention of spills and illegal dumping, and effective management of public trash collection areas.</p> <p>A pond management component for the proposed basins that shall include management and maintenance requirements for the design features and BMPs, and responsible parties for maintenance and funding.</p> <p>LID control measures shall be integrated into the BMP and water quality maintenance plan. These may include, but are not limited to:</p> <ul style="list-style-type: none"> • Surface swales; • Replacement of conventional impervious surfaces 	elements and implementation throughout project construction.			

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		with pervious surfaces (e.g., porous pavement); <ul style="list-style-type: none"> • Impervious surfaces disconnection; and • Trees planted to intercept stormwater. New stormwater facilities shall be placed along the natural drainage courses within the SPA to the extent practicable so as to mimic the natural drainage patterns. The reduction in runoff as a result of the LID configurations shall be quantified based on the runoff reduction credit system methodology described in "Stormwater Quality Design Manual for the Sacramento and South Placer Regions, Chapter 5 and Appendix D4" (SSQP 2007b) and proposed detention basins and other water quality BMPs shall be sized to handle these runoff volumes. For those areas that would be disturbed as part of the U.S. 50 interchange improvements, it is anticipated that Caltrans would coordinate with the development and implementation of the overall project SWPPP, or develop and implement its own SWPPP specific to the interchange improvements, to ensure that water quality degradation would be avoided or minimized to the maximum extent practicable. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with El Dorado County and Caltrans.				
NOISE AND VIBRATION						
53-21	3A.11-1 (FPASP EIR/EIS)	<i>Implement Noise-Reducing Construction Practices, Prepare and Implement a Noise Control Plan, and Monitor and Record Construction Noise near Sensitive Receptors.</i> To reduce impacts associated with noise generated during project related construction activities, the project applicant(s) and their primary contractors for engineering design and construction of all project phases shall ensure that the following requirements are implemented at each work site in	Before and during construction activities on the SPA and within El Dorado Hills.	City of Folsom Community Development Department.	The owner/applicant has implemented noise reducing construction practices included as part of the required Noise Control Plan. Compliance with these requirements has been monitored through construction inspection.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<p>any year of project construction to avoid and minimize construction noise effects on sensitive receptors. The project applicant(s) and primary construction contractor(s) shall employ noise-reducing construction practices. Measures that shall be used to limit noise shall include the measures listed below:</p> <p>Noise-generating construction operations shall be limited to the hours between 7 a.m. and 7 p.m. Monday through Friday, and between 8 a.m. and 6 p.m. on Saturdays and Sundays.</p> <p>All construction equipment and equipment staging areas shall be located as far as possible from nearby noise-sensitive land uses.</p> <p>All construction equipment shall be properly maintained and equipped with noise-reduction intake and exhaust mufflers and engineshrouds, in accordance with manufacturers' recommendations.</p> <p>Equipment engine shrouds shall be closed during equipment operation.</p> <p>All motorized construction equipment shall be shut down when not in use to prevent idling.</p> <p>Individual operations and techniques shall be replaced with quieter procedures (e.g., using welding instead of riveting, mixing concrete offsite instead of on-site).</p> <p>Noise-reducing enclosures shall be used around stationary noise-generating equipment (e.g., compressors and generators) as planned phases are built out and future noise sensitive receptors are located within close proximity to future construction activities.</p> <p>Written notification of construction activities shall be</p>				

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		<p>provided to all noise-sensitive receptors located within 850 feet of construction activities. Notification shall include anticipated dates and hours during which construction activities are anticipated to occur and contact information, including a daytime telephone number, for the project representative to be contacted in the event that noise levels are deemed excessive. Recommendations to assist noise-sensitive land uses in reducing interior noise levels (e.g., closing windows and doors) shall also be included in the notification.</p> <p>To the extent feasible, acoustic barriers (e.g., lead curtains, sound barriers) shall be constructed to reduce construction-generated noise levels at affected noise-sensitive land uses. The barriers shall be designed to obstruct the line of sight between the noise-sensitive land use and on-site construction equipment. When installed properly, acoustic barriers can reduce construction noise levels by approximately 8–10 dB (EPA 1971).</p> <p>When future noise sensitive uses are within close proximity to prolonged construction noise, noise-attenuating buffers such as structures, truck trailers, or soil piles shall be located between noise sources and future residences to shield sensitive receptors from construction noise.</p> <p>The primary contractor shall prepare and implement a construction noise management plan. This plan shall identify specific measures to ensure compliance with the noise control measures specified above. The noise control plan shall be submitted to the City of Folsom before any noise-generating construction activity begins. Construction shall not commence until the construction noise management plan is</p>				

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		approved by the City of Folsom. Mitigation for the two off-site roadway connections into El Dorado County must be coordinated by the project applicant(s) of the applicable project phase with El Dorado County, since the roadway extensions are outside of the City of Folsom's jurisdictional boundaries.				
PUBLIC SERVICES						
53-22	3A.14-1 (FPASP EIR/EIS)	<i>Prepare and Implement a Construction Traffic Control Plan.</i> The project applicant(s) of all project phases shall prepare and implement traffic control plans for construction activities that may affect road rights- of-way. The traffic control plans must follow any applicable standards of the agency responsible for the affected roadway and must be approved and signed by a professional engineer. Measures typically used in traffic control plans include advertising of planned lane closures, warning signage, a flag person to direct traffic flows when needed, and methods to ensure continued access by emergency vehicles. During project construction, access to existing land uses shall be maintained at all times, with detours used as necessary during road closures. Traffic control plans shall be submitted to the appropriate City or County department or the California Department of Transportation (Caltrans) for review and approval before the approval of all project plans or permits, for all project phases where implementation may cause impacts on traffic. Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties and Caltrans).	Before the approval of all relevant plans and/or permits and during construction of all project phases.	City of Folsom Community Development Department	The Community Development Department has reviewed and approved all traffic control plans required for the construction of both off-site and on-site improvements for this subdivision to verify compliance with City ordinances and to minimize delays to the travelling public. There are no off-site elements outside the City limits for this subdivision.	Yes
53-23	3A.14-2 (FPASP EIR/EIS)	<i>Incorporate California Fire Code; City of Folsom Fire Code Requirements; and EDHFD Requirements, if Necessary, into Project Design and Submit Project Design to the City of Folsom Fire Department for</i>	Before issuance of building permits and	City of Folsom Fire Department, City of Folsom Community	The City Fire Department has reviewed and approved all proposed improvements for the subdivision. The City FD will verify adequate fire flow prior to building permit issuance in the subdivision.	Yes

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<p>Review and Approval. To reduce impacts related to the provision of new fire services, the project applicant(s) of all project phases shall do the following, as described below.</p> <p>1. Incorporate into project designs fire flow requirements based on the California Fire Code, Folsom Fire Code (City of Folsom Municipal Code Title 8, Chapter 8.36), and other applicable requirements based on the City of Folsom Fire Department fire prevention standards.</p> <p>Improvement plans showing the incorporation automatic sprinkler systems, the availability of adequate fire flow, and the locations of hydrants shall be submitted to the City of Folsom Fire Department for review and approval. In addition, approved plans showing access design shall be provided to the City of Folsom Fire Department as described by Zoning Code Section 17.57.080 ("Vehicular Access Requirements"). These plans shall describe access-road length, dimensions, and finished surfaces for firefighting equipment. The installation of security gates across a fire apparatus access road shall be approved by the City of Folsom Fire Department. The design and operation of gates and barricades shall be in accordance with the Sacramento County Emergency Access Gates and Barriers Standard, as required by the City of Folsom Fire Code.</p> <p>2. Submit a Fire Systems New Buildings, Additions, and Alterations Document Submittal List to the City of Folsom Community Development Department Building Division for review and approval before the issuance of building permits.</p> <p>In addition to the above measures, the project applicant(s) of all project phases shall incorporate the provisions described below for the portion of the SPA within the EDHFD service area, if it is determined through City/EI Dorado County negotiations that EDHFD would serve the 178- acre portion of the SPA.</p> <p>3. Incorporate into project designs applicable requirements</p>	issuance of occupancy permits or final inspections for all project phases.	Development Department		

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<p>based on the EDHFD fire prevention standards. For commercial development, improvement plans showing roadways, land splits, buildings, fire sprinkler systems, fire alarm systems, and other commercial building improvements shall be submitted to the EDHFD for review and approval. For residential development, improvement plans showing property lines and adjacent streets or roads; total acreage or square footage of the parcel; the footprint of all structures; driveway plan views describing width, length, turnouts, turnarounds, radiuses, and surfaces; and driveway profile views showing the percent grade from the access road to the structure and vertical clearance shall be submitted to the EDHFD for review and approval.</p> <p>4. Submit a Fire Prevention Plan Checklist to the EDHFD for review and approval before the issuance of building permits. In addition, residential development requiring automation fire sprinklers shall submit sprinkler design sheet(s) and hydraulic calculations from a California State Licensed C-16 Contractor.</p> <p>The City shall not authorize the occupancy of any structures until the project applicant(s) have obtained a Certificate of Occupancy from the City of Folsom Community Development Department verifying that all fire prevention items have been addressed on-site to the satisfaction of the City of Folsom Fire Department and/or the EDHFD for the 178-acre area of the SPA within the EDHFD service area.</p>				
53-24	3A.14-3 (FPASP EIR/EIS)	<p><i>Incorporate Fire Flow Requirements into Project Designs.</i></p> <p>The project applicant(s) of all project phases shall incorporate into their project designs fire flow requirements based on the California Fire Code, Folsom Fire Code, and/or EDHFD for those areas of the SPA within the EDHFD service area and shall verify to City of Folsom Fire Department that adequate water flow is</p>	Before issuance of building permits and issuance of occupancy permits or final	City of Folsom Fire Department, City of Folsom CDD	The City Fire Department has reviewed and approved all proposed improvements for the subdivision. The City FD will verify adequate fire flow prior to building permit issuance in the subdivision.	Yes

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		available, prior to approval of improvement plans and issuance of occupancy permits or final inspections for all project phases.	inspections for all project phases.			
TRAFFIC AND TRANSPORTATION						
53-25	3A.15-1a (FPASP EIR/EIS)	<p><i>The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Folsom Boulevard/Blue Ravine Road Intersection (Intersection 1).</i></p> <p>To ensure that the Folsom Boulevard/Blue Ravine Road intersection operates at an acceptable LOS, the eastbound approach must be reconfigured to consist of two left-turn lanes, one through lane, and one right-turn lane. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Folsom Boulevard/Blue Ravine Road intersection (Intersection 1).</p>	A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented and when fair share funding should be paid.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-26	3A.15-1b (FPASP EIR/EIS)	<p><i>The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements at the Sibley Street/Blue Ravine Road Intersection (Intersection 2).</i></p> <p>To ensure that the Sibley Street/Blue Ravine Road intersection operates at an acceptable LOS, the northbound approach must be reconfigured to consist of two left-turn lanes, two through lanes, and one right-turn lane. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Sibley Street/Blue Ravine Road intersection (Intersection 2).</p>	A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented and when fair share funding	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			should be paid.			
53-27	3A.15-1c (FPASP EIR/EIS)	<p><i>The Applicant Shall Fund and Construct Improvements to the Scott Road (West)/White Rock Road Intersection (Intersection 28).</i> To ensure that the Scott Road (West)/White Rock Road intersection operates at an acceptable LOS, a traffic signal must be installed.</p>	A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-28	3A.15-1e (FPASP EIR/EIS)	<p><i>Fund and Construct Improvements to the Hillside Drive/Easton Valley Parkway Intersection (Intersection 41).</i> To ensure that the Hillside Drive/Easton Valley Parkway intersection operates at an acceptable LOS, the eastbound approach must be reconfigured to consist of one dedicated left turn lane and two through lanes, and the westbound approach must be reconfigured to consist of two through lanes and one dedicated right-turn lane. The applicant shall fund and construct these improvements.</p>	A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-29	3A.15-1f (FPASP EIR/EIS)	<p><i>Fund and Construct Improvements to the Oak Avenue Parkway/Middle Road Intersection (Intersection 44).</i> To ensure that the Oak Avenue Parkway/Middle Road intersection operates at an acceptable LOS, control all movements with a stop sign. The applicant shall fund and construct these improvements.</p>	A phasing analysis shall be performed prior to approval of the first subdivision map to	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			determine when the improvement should be implemented.			
53-30	3A.15-1h (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts to the Hazel Avenue/Folsom Boulevard Intersection (Sacramento County Intersection 2).</p> <p>To ensure that the Hazel Avenue/Folsom Boulevard intersection operates at an acceptable LOS, this intersection must be grade separated including "jug handle" ramps. No at grade improvement is feasible. Grade separating and extended (south) Hazel Avenue with improvements to the U.S. 50/Hazel Avenue interchange is a mitigation measure for the approved Easton-Glenbrough Specific Plan development project. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Hazel Avenue/Folsom Boulevard intersection (Sacramento County Intersection 2).</p>	A phasing analysis shall be performed prior to approval of the first subdivision map to determine when the improvement should be implemented.	Sacramento County Public Works Department and Caltrans	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-31	3A.15-1i (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/White Rock Road Intersection and to White Rock Road widening between the Rancho Cordova City limit to Prairie City Road (Sacramento County Intersection 3).</p> <p>Improvements must be made to ensure that the Grant Line Road/White Rock Road intersection operates at an acceptable LOS. The currently County proposed White Rock Road widening project will widen and realign White Rock Road from the Rancho Cordova City limit to the El Dorado County line (this analysis assumes that the Proposed Project and build alternatives will widen White Rock Road to five lanes from Prairie City road to the El Dorado County Line). This widening includes improvements to the Grant Line Road intersection and realigning White Rock Road to be the through movement. The</p>	Before project build out. Design of the White Rock Road widening to four lanes, from Grant Line Road to Prairie City Road, with Intersection improvements has begun, and because this widening	Sacramento County Public Works Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		improvements include twoeastbound through lanes, one eastbound right turn lane, two northbound left turn lanes, two northbound right turn lanes, two westbound left turn lanes and two westbound through lanes. This improvement also includes the signalization of the White Rock Road and Grant Line Road intersection. With implementation of this improvement, the intersection would operate at an acceptable LOS A. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Grant Line Road/White Rock Road intersection (Sacramento County Intersection 3).	project is environmental ly cleared and fully funded, it's construction is expected to be complete before the first phase of the Proposed Project or alternative is built.			
53-32	3A.15-1j (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on Hazel Avenue between Madison Avenue and Curragh Downs Drive (Roadway Segment 10).</i></p> <p>To ensure that Hazel Avenue operates at an acceptable LOS between Curragh Downs Drive and Gold Country Boulevard, Hazel Avenue must be widened to six lanes. This improvement is part of the County adopted Hazel Avenue widening project.</p>	Before project build out. Construction of phase two of the Hazel Avenue widening, from Madison Avenue to Curragh Downs Drive, is expected to be completed by year 2013, before the first phase of the Proposed Project or alternative is complete. The applicant shall pay its	Sacramento County Public Works Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			proportionate share of funding of improvements to the agency responsible for improvements , based on a program established by that agency to reduce the impacts to Hazel Avenue between Madison Avenue and Curragh Downs Drive (Sacramento County Roadway Segment 10).			
53-33	3A.15-11 (FPASP EIR/EIS)	<i>Participate in Fair Share Funding of Improvements to Reduce Impacts on the White Rock Road/Windfield Way Intersection (El Dorado County Intersection 3).</i> To ensure that the White Rock Road/Windfield Way intersection operates at an acceptable LOS, the intersection must be signalized and separate northbound left and right turn lanes must be striped. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the White	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to	El Dorado County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Rock Road/Windfield Way intersection (El Dorado County Intersection 3).	determine during which project phase the improvement should be built.			
53-34	3A.15-1o (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 as an alternative to improvements at the Folsom Boulevard/U.S. 50</p> <p>Eastbound Ramps Intersection (Caltrans Intersection 4). Congestion on eastbound U.S. 50 is causing vehicles to use Folsom Boulevard as an alternate parallel route until they reach U.S. 50, where they must get back on the freeway due to the lack of a parallel route. It is preferred to alleviate the congestion on U.S. 50 than to upgrade the intersection at the end of this reliever route. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Folsom Boulevard/U.S. 50 Eastbound Ramps intersection (Caltrans Intersection 4). To ensure that the Folsom Boulevard/U.S. 50 eastbound ramps intersection operates at an acceptable LOS, auxiliary lanes should be added to eastbound U.S. 50 from Hazel Avenue to east of Folsom Boulevard. This was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project.</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom CDD and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-35	3A.15-1p (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/ State Route 16 Intersection (Caltrans Intersection 12).</p> <p>To ensure that the Grant Line Road/State Route 16 intersection operates at an acceptable LOS, the northbound and southbound approaches must be reconfigured to consist of one left-turn lane and one shared through/right-turn lane. Protected left-turn signal phasing must be</p>	Before project build out. A phasing analysis should be performed prior to approval of	Sacramento County Department of Transportation and the City of Rancho Cordova Department of Public Works	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		provided on the northbound and southbound approaches. Improvements to the Grant Line Road/State Route 16 intersection are contained within the County Development Fee Program and are scheduled for Measure A funding. Improvements to this intersection must be implemented by Caltrans, Sacramento County, and the City of Rancho Cordova. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Grant Line Road/State Route 16 intersection (Caltrans Intersection 12).	the first subdivision map to determine during which project phase the improvement should be built.			
53-36	3A.15-1q (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).</i></p> <p>To ensure that Eastbound U.S. 50 operates at an acceptable LOS between Zinfandel Drive and Sunrise Boulevard, a bus-carpool (HOV) lane must be constructed. This improvement is currently planned as part of the Sacramento 50 Bus-Carpool Lane and Community Enhancements Project. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Eastbound U.S. 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).</p>	Before project build out. Construction of the Sacramento 50 Bus-Carpool Lane and Community Enhancements Project is expected to be completed by year 2013, before the first phase of the Proposed Project or alternative is complete. Construction of the Sacramento 50 Bus-Carpool Lane	Caltrans	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			and Community Enhancement s Project has started since the writing of the Draft EIS/EIR.			
53-37	3A.15-1r (FPASP EIR/EIS)	<i>Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Hazel Avenue and Folsom Boulevard (Freeway Segment 3).</i> To ensure that Eastbound U.S. 50 operates at an acceptable LOS between Hazel Avenue and Folsom Boulevard, an auxiliary lane must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Eastbound U.S. 50 between Hazel Avenue and Folsom Boulevard (Freeway Segment 3).	Before project build out. A phasing analysis should be performed to determine during which project phase the improvement should be built.	City of Folsom Community Development Department and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-38	3A.15-1s (FPASP EIR/EIS)	<i>Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound U.S. 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 4).</i> To ensure that Eastbound U.S. 50 operates at an acceptable LOS between Folsom Boulevard and Prairie City Road, an auxiliary lane must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to Eastbound U.S. 50	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the	City of Folsom Community Development Department and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		between Folsom Boulevard and PrairieCity Road (Freeway Segment 4).	improvement should be built.			
53-39	3A.15-1u (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on Westbound U.S. 50 between Prairie City Road and Folsom Boulevard (Freeway Segment 16).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS between Prairie City Road and Folsom Boulevard, an auxiliary lane must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to Westbound U.S. 50 between Prairie City Road and Folsom Boulevard (Freeway Segment 16).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-40	3A.15-1v (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on Westbound U.S. 50 between Hazel Avenue and Sunrise Boulevard (Freeway Segment 18).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS between Hazel Avenue and Sunrise Boulevard, an auxiliary lane must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project and included in the proposed Rancho Cordova Parkway interchange project.</p> <p>Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Westbound U.S. 50 between Hazel Avenue and Sunrise Boulevard (Freeway</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	City of Rancho Cordova Department of Public Works and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

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No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Segment 18).	should be built.			
53-41	3A.15-1w (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Folsom Boulevard Ramp Merge (Freeway Merge 4).</p> <p>To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Folsom Boulevard merge, an auxiliary lane from the Folsom Boulevard merge to the Prairie City Road diverge must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the U.S. 50 Eastbound/Folsom Boulevard Ramp Merge (Freeway Merge 4).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-42	3A.15-1x (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Diverge (Freeway Diverge 5).</p> <p>To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Prairie City Road off-ramp diverge, an auxiliary lane from the Folsom Boulevard merge must be constructed. This improvement was recommended in the Traffic Operations Analysis Report for the U.S. 50 Auxiliary Lane Project. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound/Prairie City Road diverge (Freeway Diverge 5).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	City of Folsom Community Development Department and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			should be built.			
53-43	3A.15-1y (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Direct Merge (Freeway Merge 6).</i></p> <p>To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Prairie City Road onramp direct merge, an auxiliary lane to the East Bidwell Street – Scott Road diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound/Prairie City Road direct merge (Freeway Merge 6).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-44	3A.15-1z (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Prairie City Road Flyover On-Ramp to Oak Avenue Parkway Off-Ramp Weave (Freeway Weave 8).</i></p> <p>To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Prairie City Road flyover on-ramp to Oak Avenue Parkway off-ramp weave, an improvement acceptable to Caltrans should be implemented to eliminate the unacceptable weaving conditions. Such an improvement may involve a “braided ramp”.</p> <p>The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound / Prairie City Road flyover on-ramp to Oak Avenue Parkway off-ramp weave (Freeway Weave 8).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			should be built.			
53-45	3A.15-1aa (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Oak Avenue Parkway Loop Merge (Freeway Merge 9).</i></p> <p>To ensure that Eastbound U.S. 50 operates at an acceptable LOS at the Oak Avenue Parkway loop merge, an auxiliary lane to the East Bidwell Street – Scott Road diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound/ Oak Avenue Parkway loop merge (Freeway Merge 9).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-46	3A.15-1dd (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Empire Ranch Road Loop Ramp Merge (Freeway Merge 23).</i></p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS, the northbound Empire Ranch Road loop on ramp should start the westbound auxiliary lane that ends at the East Bidwell Street – Scott Road off ramp. The slip on ramp from southbound Empire Ranch Road would merge into this extended auxiliary lane. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound/Empire Ranch Road loop ramp merge (Freeway Merge 23).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			should be built.			
53-47	3A.15-1ee (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Oak Avenue Parkway Loop Ramp Merge (Freeway Merge 29).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS, the northbound Oak Avenue Parkway loop on ramp should start the westbound auxiliary lane that ends at the Prairie City Road off ramp. The slip on ramp from southbound Oak Avenue Parkway would merge into this extended auxiliary lane. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound/Oak Avenue Parkway loop ramp merge (Freeway Merge 29).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-48	3A.15-1ff (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Prairie City Road Loop Ramp Merge (Freeway Merge 32).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS at the Prairie City Road loop ramp merge, an auxiliary lane to the Folsom Boulevard off ramp diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound/Prairie City Road Loop Ramp Merge (Freeway Merge 32).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	City of Folsom Community Development Department and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			should be built.			
53-49	3A.15-1gg (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Prairie City Road Direct Ramp Merge (Freeway Merge 33).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS at the Prairie City Road direct ramp merge, an auxiliary lane to the Folsom Boulevard off ramp diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound/Prairie City Road direct ramp merge (Freeway Merge 33).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-50	3A.15-1hh (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound/Folsom Boulevard Diverge (Freeway Diverge 34).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS at the Folsom Boulevard Diverge, an auxiliary lane from the Prairie City Road loop ramp merge must be constructed. Improvements to this freeway segment must be implemented by Caltrans. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound / Folsom Boulevard diverge (Freeway Diverge 34).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	City of Folsom Community Development Department and Sacramento County Department of Transportation	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			should be built.			
53-51	3A.15-1ii (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound/Hazel Avenue Direct Ramp Merge (Freeway Merge 38).</p> <p>To ensure that Westbound U.S. 50 operates at an acceptable LOS at the Hazel Avenue direct ramp merge, an auxiliary lane to the Sunrise Boulevard off ramp diverge must be constructed. This auxiliary lane improvement is included in the proposed 50 Corridor Mobility Fee Program. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the U.S. 50 Westbound/Hazel Avenue direct ramp merge (Freeway Merge 38).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	Sacramento County Department of Transportation and City of Rancho Cordova Department of Public Works	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-52	3A.15-2a (FPASP EIR/EIS)	<p>Develop Commercial Support Services and Mixed-use Development Concurrent with Housing Development and Develop and Provide Options for Alternative Transportation Modes.</p> <p>The project applicant(s) for any particular discretionary development application including commercial or mixed-use development along with residential uses shall develop commercial and mixed-use development concurrent with housing development, to the extent feasible in light of market realities and other considerations, to internalize vehicle trips. Pedestrian and bicycle facilities shall be implemented to the satisfaction of the City Public Works Department. To further minimize impacts from the increased demand on area roadways and intersections, the project applicant(s) for any particular discretionary development application involving schools or</p>	Before approval of improvement plans for all project phases any particular discretionary development application that includes residential and commercial or mixed-use development. As a condition	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		commercial centers shall develop and implement safe and secure bicycle parking to promote alternative transportation uses and reduce the volume of single-occupancy vehicles using area roadways and intersections. The project applicant(s) for any particular discretionary development application shall participate in capital improvements and operating funds for transit service to increase the percent of travel by transit. The project's fair-share participation and the associated timing of the improvements and service shall be identified in the project conditions of approval and/or the project's development agreement. Improvements and service shall be coordinated, as necessary, with Folsom Stage Lines and Sacramento RT.	of project approval and/or as a condition of the development agreement for all project phases.			
53-53	3A.15-2b (FPASP EIR/EIS)	<i>Participate in the City's Transportation System Management Fee Program.</i> The project applicant(s) for any particular discretionary development application shall pay an appropriate amount into the City's existing Transportation System Management Fee Program to reduce the number of single-occupant automobile travel on area roadways and intersections.	Concurrent with construction for all project phases.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-54	3A.15-2c (FPASP EIR/EIS)	<i>Participate with the 50 Corridor Transportation Management Association.</i> The project applicant(s) for any particular discretionary development application shall join and participate with the 50 Corridor Transportation Management Association to reduce the number of single-occupant automobile travel on area roadways and intersections.	Concurrent with construction for all project phases.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-55	3A.15-3 (FPASP EIR/EIS)	<i>Pay Full Cost of Identified Improvements that Are Not Funded by the City's Fee Program.</i> In accordance with Measure W, the project applicant(s) for any particular discretionary development application shall provide fair-share contributions to the City's transportation impact fee program to fully fund improvements only required because of the Specific Plan.	As a condition of project approval and/or as a condition of the development agreement for	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			all project phases.			
53-56	3A.15-4a (FPASP EIR/EIS)	<p><i>The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Sibley Street/Blue Ravine Road Intersection (Folsom Intersection 2).</i></p> <p>To ensure that the Sibley Street/Blue Ravine Road intersection operates at a LOS D with less than the Cumulative No Project delay, the northbound approach must be reconfigured to consist of two left-turn lane, two through lanes, and one dedicated right-turn lane. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Sibley Street/Blue Ravine Road intersection (Folsom Intersection 2).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-57	3A.15-4b (FPASP EIR/EIS)	<p><i>The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Oak Avenue Parkway/East Bidwell Street Intersection (Folsom Intersection 6).</i></p> <p>To ensure that the Oak Avenue Parkway/East Bidwell Street intersection operates at an acceptable LOS, the eastbound (East Bidwell Street) approach must be reconfigured to consist of two left-turn lanes, four through lanes and a right-turn lane, and the westbound (East Bidwell Street) approach must be reconfigured to consist of two left turn lanes, four through lanes, and a right-turn lane. It is against the City of Folsom policy to have eight lane roads because of the impacts to non-motorized traffic and adjacent development; therefore, this improvement is infeasible.</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			should be built.			
53-58	3A.15-4c (FPASP EIR/EIS)	<p><i>The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the East Bidwell Street/College Street Intersection (Folsom Intersection 7).</i></p> <p>To ensure that the East Bidwell Street/College Street intersection operates at acceptable LOS C or better, the westbound approach must be reconfigured to consist of one left-turn lane, one left-through lane, and two dedicated right-turn lanes. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the East Bidwell Street/Nesmith Court intersection (Folsom Intersection 7).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-59	3A.15-4d (FPASP EIR/EIS)	<p><i>The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the East Bidwell Street/Iron Point Road Intersection (Folsom Intersection 21).</i></p> <p>To ensure that the East Bidwell Street /Iron Point Road intersection operates at an acceptable LOS, the northbound approach must be reconfigured to consist of two left-turn lanes, four through lanes and a right-turn lane, and the southbound approach must be reconfigured to consist of two left-turn lanes, four through lanes and a right-turn lane. It is against the City of Folsom policy to have eight lane roads because of the impacts to non-motorized traffic and adjacent development; therefore, this improvement is infeasible.</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
			should be built.			
53-60	3A.15-4e (FPASP EIR/EIS)	<p><i>The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Serpa Way/ Iron Point Road Intersection (Folsom Intersection 23).</i></p> <p>To improve LOS at the Serpa Way/ Iron Point Road intersection, the northbound approaches must be restriped to consist of one left-turn lane, one shared left-through lanes, and one right-turn lane. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Serpa Way/Iron Point Road Intersection (Folsom Intersection 23).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-61	3A.15-4f (FPASP EIR/EIS)	<p><i>The Applicant Shall Pay a Fair Share to Fund the Construction of Improvements to the Empire Ranch Road/Iron Point Road Intersection (Folsom Intersection 24).</i></p> <p>To ensure that the Empire Ranch Road / Iron Point Road intersection operates at a LOS D or better, all of the following improvements are required: The eastbound approach must be reconfigured to consist of one left-turn lane, two through lanes, and a right-turn lane. The westbound approach must be reconfigured to consist of two left-turn lanes, one through lane, and a through-right lane. The northbound approach must be reconfigured to consist of two left-turn lanes, three through lanes, and a right-turn lane. The southbound approach must be reconfigured to consist of two left-turn lanes, three through lanes, and a right-turn lane. The applicant shall pay its</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the Empire Ranch Road / Iron Point Road Intersection Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built. (Folsom Intersection 24).	should be built.			
53-62	3A.15-4g (FPASP EIR/EIS)	<i>The Applicant Shall Fund and Construct Improvements to the Oak Avenue Parkway/Easton Valley Parkway Intersection (Folsom Intersection 33).</i> To ensure that the Oak Avenue Parkway/Easton Valley Parkway intersection operates at an acceptable LOS the southbound approach must be reconfigured to consist of two left-turn lanes, two through lanes, and two right-turn lanes. The applicant shall fund and construct these improvements.	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	City of Folsom Community Development Department	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-63	3A.15-4i (FPASP EIR/EIS)	<i>Participate in Fair Share Funding of Improvements to Reduce Impacts on the Grant Line Road/White Rock Road Intersection (Sacramento County Intersection 3).</i> To ensure that the Grant Line Road/White Rock Road intersection operates at an acceptable LOS E or better this intersection should be replaced by some type of grade separated intersection or interchange. Improvements to this intersection are identified in the Sacramento County's	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Proposed General Plan. Implementation of these improvements would assist in reducing traffic impacts on this intersection by providing acceptable operation. Intersection improvements must be implemented by Sacramento County. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Grant Line Road/White Rock Road Intersection (Sacramento County Intersection 3).	map to determine during which project phase the improvement should be built.			
53-64	3A.15-4j (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on Grant Line Road between White Rock Road and Kiefer Boulevard (Sacramento County Roadway Segments 5-7).</i></p> <p>To improve operation on Grant Line Road between White Rock Road and Kiefer Boulevard, this roadway segment must be widened to six lanes. This improvement is proposed in the Sacramento County and the City of Rancho Cordova General Plans; however, it is not in the 2035 MTP. Improvements to this roadway segment must be implemented by Sacramento County and the City of Rancho Cordova. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Grant Line Road between White Rock Road and Kiefer Boulevard (Sacramento County Roadway Segments 5-7). The identified improvement would more than offset the impacts specifically related to the Folsom South of U.S. 50 project on this roadway segment.</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-65	3A.15-4k (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on Grant Line Road between Kiefer Boulevard and Jackson Highway (Sacramento County Roadway Segment 8).</i></p> <p>To improve operation on Grant Line Road between Kiefer</p>	Before project build out. A phasing analysis should be	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<p>Boulevard Jackson Highway, this roadway segment could be widened to six lanes. This improvement is proposed in the Sacramento County and the City of Rancho Cordova General Plans; however, it is not in the 2035 MTP. Improvements to this roadway segment must be implemented by Sacramento County and the City of Rancho Cordova. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Grant Line Road between Kiefer Boulevard and Jackson Highway (Sacramento County Roadway Segment</p> <p>8). The identified improvement would more than offset the impacts specifically related to the Folsom South of U.S. 50 project on this roadway segment.</p>	<p>performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>			
53-66	3A.15-4l (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on Hazel Avenue between Curragh Downs Drive and U.S. 50 Westbound Ramps (Sacramento County Roadway Segments 12-13).</i></p> <p>To improve operation on Hazel Avenue between Curragh Downs Drive and the U.S. 50 westbound ramps, this roadway segment could be widened to eight lanes. This improvement is inconsistent with Sacramento County's general plan because the county's policy requires a maximum roadway cross section of six lanes. Analysis shown later indicates that improvements at the impacted intersection in this segment can be mitigated (see Mitigation Measure 3A.15-4q). Improvements to impacted intersections on this segment will improve operations on this roadway segment and, therefore; mitigate this segment impact. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Hazel Avenue between Curragh Downs Drive and U.S. 50</p>	<p>Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.</p>	<p>Sacramento County Department of Transportation.</p>	<p>This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.</p>	<p>Condition will be satisfied prior to issuance of a building permit.</p>

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Westbound Ramps (Sacramento County Roadway Segments 12-13).				
53-67	3A.15-4m (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on White Rock Road between Grant Line Road and Prairie City Road (Sacramento County Roadway Segment 22).</p> <p>To improve operation on White Rock Road between Grant Line Road and Prairie City Road, this roadway segment must be widened to six lanes. This improvement is included in the 2035 MTP but is not included in the Sacramento County General Plan. Improvements to this roadway segment must be implemented by Sacramento County. The identified improvement would more than offset the impacts specifically related to the Folsom South of U.S. 50 project on this roadway segment. However, because of other development in the region that would substantially increase traffic levels, this roadway segment would continue to operate at an unacceptable LOS F even with the capacity improvements identified to mitigate Folsom South of U.S. 50 impacts. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to White Rock Road between Grant Line Road and Prairie City Road (Sacramento County Roadway Segment 22).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-68	3A.15-4n (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on White Rock Road between Empire Ranch Road and Carson Crossing Road (Sacramento County Roadway Segment 28).</p> <p>To improve operation on White Rock Road between Empire Ranch Road and Carson Crossing Road, this roadway segment must be widened to six lanes. Improvements to this roadway segment must be implemented by Sacramento County. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		agency to reduce the impacts to White Rock Road between Empire Ranch Road and Carson Crossing Road (Sacramento County Roadway Segment 28).	during which project phase the improvement should be built.			
53-69	3A.15-4o (FPASP EIR/EIS)	<i>Participate in Fair Share Funding of Improvements to Reduce Impacts on the White Rock Road/Carson Crossing Road Intersection (El Dorado County 1).</i> To ensure that the White Rock Road/Carson Crossing Road intersection operates at an acceptable LOS, the eastbound right turn lane must be converted into a separate free right turn lane, or double right. Improvements to this intersection must be implemented by El Dorado County. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to the White Rock Road/Carson Crossing Road Intersection (El Dorado County 1).	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-70	3A.15-4p (FPASP EIR/EIS)	<i>Participate in Fair Share Funding of Improvements to Reduce Impacts on the Hazel Avenue/U.S. 50 Westbound Ramps Intersection (Caltrans Intersection 1).</i> To ensure that the Hazel Avenue/U.S. 50 westbound ramps intersection operates at an acceptable LOS, the westbound approach must be reconfigured to consist of one dedicated left turn lane, one shared left through lane and three dedicated right-turn lanes. Improvements to this intersection must be implemented by Caltrans and Sacramento County. The applicant shall pay its proportionate share of funding of improvements to the	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		agency responsible for improvements, based on a program established by that agency to reduce the impacts to the Hazel Avenue/U.S. 50 Westbound Ramps Intersection (Caltrans Intersection 1).	project phase the improvement should be built.			
53-71	3A.15-4q (FPASP EIR/EIS)	Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1). To ensure that Eastbound US 50 operates at an acceptable LOS between Zinfandel Drive and Sunrise Boulevard, an additional eastbound lane could be constructed. This improvement is not consistent with the Concept Facility in Caltrans State Route 50 Corridor System Management Plan; therefore, it is not likely to be implemented by Caltrans by 2030. Construction of the Capitol South East Connector, including widening White Rock Road and Grant Line Road to six lanes with limited access, could divert some traffic from U.S. 50 and partially mitigate the project's impact. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Eastbound U.S. 50 between Zinfandel Drive and Sunrise Boulevard (Freeway Segment 1).	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-72	3A.15-4r (FPASP EIR/EIS)	Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Rancho Cordova Parkway and Hazel Avenue (Freeway Segment 3). To ensure that Eastbound US 50 operates at an acceptable LOS between Rancho Cordova Parkway and Hazel Avenue, an additional eastbound lane could be constructed. This improvement is not consistent with the Concept Facility in Caltrans State Route 50 Corridor System Management Plan; therefore, it is not likely to be implemented by Caltrans by 2030. Construction of the Capitol South East Connector, including widening White	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Rock Road and Grant Line Road to six lanes with limited access, could divert some traffic off of U.S. 50 and partially mitigate the project's impact. The applicant shall pay its proportionate share of funding of improvements to the agency responsible for improvements, based on a program established by that agency to reduce the impacts to Eastbound U.S. 50 between Rancho Cordova Parkway and Hazel Avenue (Freeway Segment 3).	project phase the improvement should be built.			
53-73	3A.15-4s (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 5).</i></p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS between Folsom Boulevard and Prairie City Road, the eastbound auxiliary lane should be converted to a mixed flow lane that extends to and drops at the Oak Avenue Parkway off ramp (see mitigation measure 3A.15-4t). Improvements to this freeway segment must be implemented by Caltrans. This improvement is not consistent with the Concept Facility in Caltrans State Route 50 Corridor System Management Plan; therefore, it is not likely to be implemented by Caltrans by 2030. Construction of the Capitol South East Connector, including widening White Rock Road and Grant Line Road to six lanes with limited access, could divert some traffic off of U.S. 50 and partially mitigate the project's impact. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to Eastbound U.S. 50 between Folsom Boulevard and Prairie City Road (Freeway Segment 5).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
53-74	3A.15-4t (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on Eastbound US 50 between Prairie City Road and Oak Avenue Parkway (Freeway Segment 6).</i></p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS between Prairie City Road and Oak Avenue Parkway, the northbound Prairie City Road slip on ramp should merge with the eastbound auxiliary lane that extends to and drops at the Oak Avenue Parkway off ramp (see Mitigation Measures 3A.15-4u, v and w), and the southbound Prairie City Road flyover on ramp should be braided over the Oak Avenue Parkway off ramp and start an extended full auxiliary lane to the East Bidwell Street – Scott Road off ramp. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to Eastbound U.S. 50 between Prairie City Road and Oak Avenue Parkway (Freeway Segment 6).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-75	3A.15-4u (FPASP EIR/EIS)	<p><i>Participate in Fair Share Funding of Improvements to Reduce Impacts on the U.S. 50 Eastbound / Prairie City Road Slip Ramp Merge (Freeway Merge 6).</i></p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS, the northbound Prairie City Road slip on ramp should start the eastbound auxiliary lane that extends to and drops at the Oak Avenue Parkway off ramp (see mitigation measure 3A.15-4u, w and x), and the southbound Prairie City Road flyover on ramp should be braided over the Oak Avenue Parkway off ramp and start an extended full auxiliary lane to the East Bidwell Street – Scott Road off ramp. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Eastbound / Prairie City Road slip ramp merge (Freeway Merge 6).	should be built.			
53-76	3A.15-4v (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on the U.S. 50 Eastbound / Prairie City Road Flyover On Ramp to Oak Avenue Parkway Off Ramp Weave (Freeway Weave 7).</p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS, the northbound Prairie City Road slip on ramp should start the eastbound auxiliary lane that extends to and drops at the Oak Avenue Parkway off ramp (see mitigation measure 3A.15-4u, v and x), and the southbound Prairie City Road flyover on ramp should be braided over the Oak Avenue Parkway off ramp and start an extended full auxiliary lane to the East Bidwell Street – Scott Road off ramp. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Eastbound / Prairie City Road Flyover On Ramp to Oak Avenue Parkway Off Ramp Weave (Freeway Weave 7).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.
53-77	3A.15-4w (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Eastbound / Oak Avenue Parkway Loop Ramp Merge (Freeway Merge 8).</p> <p>To ensure that Eastbound US 50 operates at an acceptable LOS, the southbound Oak Avenue Parkway loop on ramp should merge with the eastbound auxiliary lane that starts at the southbound Prairie City Road braided flyover on ramp and ends at the East Bidwell Street – Scott Road off ramp (see mitigation measure 3A.15-4u, v and w). Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to U.S. 50 Eastbound / Oak</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Avenue Parkway Loop Ramp Merge (Freeway Merge 8).	should be built.			
53-78	3A.15-4x (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound / Empire Ranch Road Loop Ramp Merge (Freeway Merge 27).</p> <p>To ensure that Westbound US 50 operates at an acceptable LOS, the northbound Empire Ranch Road loop on ramp should start the westbound auxiliary lane that ends at the East Bidwell Street – Scott Road off ramp. The slip-on ramp from southbound Empire Ranch Road slip ramp would merge into this extended auxiliary lane. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound / Empire Ranch Road loop ramp merge (Freeway Merge 27).</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement should be built.	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision	Condition will be satisfied prior to issuance of a building permit.
53-79	3A.15-4y (FPASP EIR/EIS)	<p>Participate in Fair Share Funding of Improvements to Reduce Impacts on U.S. 50 Westbound / Prairie City Road Loop Ramp Merge (Freeway Merge 35).</p> <p>To ensure that Westbound US 50 operates at an acceptable LOS, the northbound Prairie City Road loop on ramp should start the westbound auxiliary lane that continues beyond the Folsom Boulevard off ramp. The slip-on ramp from southbound Prairie City Road slip ramp would merge into this extended auxiliary lane. Improvements to this freeway segment must be implemented by Caltrans. The applicant shall pay its proportionate share of funding of improvements, as may be determined by a nexus study or other appropriate and reliable mechanism paid for by applicant, to reduce the impacts to the U.S. 50 Westbound /</p>	Before project build out. A phasing analysis should be performed prior to approval of the first subdivision map to determine during which project phase the improvement	Sacramento County Department of Transportation.	This subdivision will pay its fair share of the following improvements with the payment of Specific Plan Infrastructure Fees (SPIF), City Traffic Impact Fees and Sacramento County Wide Transportation fees collected prior to building permit issuance in this subdivision.	Condition will be satisfied prior to issuance of a building permit.

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		Prairie City Road Loop Ramp Merge (Freeway Merge 35).	should be built.			
UTILITIES AND SERVICE SYSTEMS						
53-80	3A.16-1 (FPASP EIR/EIS)	<p>Submit Proof of Adequate On- and Off-Site Wastewater Conveyance Facilities and Implement On- and Off-Site Infrastructure Service Systems or Ensure That Adequate Financing Is Secured.</p> <p>Before the approval of the final map and issuance of building permits for all project phases, the project applicant(s) of all project phases shall submit proof to the City of Folsom that an adequate wastewater conveyance system either has been constructed or is ensured through payment of the City's facilities augmentation fee as described under the Folsom Municipal Code Title 3, Chapter 3.40, "Facilities Augmentation Fee – Folsom South Area Facilities Plan," or other sureties to the City's satisfaction. Both on-site wastewater conveyance infrastructure and off- site force main sufficient to provide adequate service to the project shall be in place for the amount of development identified in the tentative map before approval of the final map and issuance of building permits for all project phases, or their financing shall be ensured to the satisfaction of the City.</p>	Before approval of final maps and issuance of building permits for any project phases.	City of Folsom Community Development Department	The Phase 1 Sanitary Sewer infrastructure including the off-site sewer trunk main, the Alder Creek Parkway sewer lift station and forced main to serve this subdivision have been constructed by the FPA landowners and have been completed and accepted by the City and are currently in operation.	Yes
53-81	3A.16-3 (FPASP EIR/EIS)	<p>Demonstrate Adequate SRWTP Wastewater Treatment Capacity.</p> <p>The project applicant(s) of all project phases shall demonstrate adequate capacity at the SRWTP for new wastewater flows generated by the project. This shall involve preparing a tentative map-level study and paying connection and capacity fees as identified by SRCSD. Approval of the final map and issuance of building permits for all project phases shall not be granted until the City verifies adequate SRWTP capacity is available for the amount of development identified in the tentative map.</p>	Before approval of final maps and issuance of building permits for any project phases.	City of Folsom Community Development Department	The City obtained a letter from Regional San which provides verification that there is adequate capacity in the existing Regional San conveyance and treatment system to accommodate the entire Folsom Plan Area at buildout. Confirmation from Regional San was required because the Folsom Plan Area is served by the existing Regional San Lift Station on Iron Point Road. The City Sewer Lift Station and Forced Main which connects to the Regional San Lift Station has been accepted by the City and is currently in operation.	Yes
53-82	3A.18-1 (FPASP EIR/EIS)	<p>Submit Proof of Surface Water Supply Availability.</p> <p>a. Prior to approval of any small-lot tentative subdivision map subject to Government Code Section 66473.7 (SB 221), the</p>	Before approval of final maps	City of Folsom Community	The owner/applicant has constructed the necessary infrastructure to provide potable water to the subdivision. The potable Phase 1 water infrastructure	Yes

CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
No.	Mitigation Measure	Condition of Approvals	When Required	Responsible Department	Comments	Conditioned Satisfied
		<p>City shall comply with that statute. Prior to approval of any small-lot tentative subdivision map for a proposed residential project not subject to that statute, the City need not comply with Section 66473.7, or formally consult with any public water system that would provide water to the affected area; nevertheless, the City shall make a factual showing or impose conditions similar to those required by Section 66473.7 to ensure an adequate water supply for development authorized by the map.</p> <p>b. Prior to recordation of each final subdivision map, or prior to City approval of any similar project-specific discretionary approval or entitlement required for nonresidential uses, the project applicant(s) of that project phase or activity shall demonstrate the availability of a reliable and sufficient water supply from a public water system for the amount of development that would be authorized by the final subdivision map or project-specific discretionary nonresidential approval or entitlement. Such a demonstration shall consist of information showing that both existing sources are available or needed supplies and improvements will be in place prior to occupancy.</p>	and issuance of building permits for any project phases.	Development Department	for the Folsom Plan Area has been reviewed, approved and accepted by the City and is currently in operation.	
53-83	3A.18-2a (FPASP EIR/EIS)	<p><i>Submit Proof of Adequate Off-Site Water Conveyance Facilities and Implement Off-Site Infrastructure Service System or Ensure That Adequate Financing Is Secured.</i></p> <p>Before the approval of the final subdivision map and issuance of building permits for all project phases, the project applicant(s) of any particular discretionary development application shall submit proof to the City of Folsom that an adequate off-site water conveyance system either has been constructed or is ensured or other sureties to the City's satisfaction. The off-site water conveyance infrastructure sufficient to provide adequate service to the project shall be in place for the amount of development identified in the tentative map before approval of the final subdivision map and issuance of building permits for all</p>	Before approval of final maps and issuance of building permits for any project phases.	City of Folsom Community Development Department	The off-site potable water infrastructure to serve the subdivision has been reviewed, approved and accepted by the City and is currently in operation. In addition, the City has verified that the off-site potable water infrastructure is adequate to serve the subdivision.	Yes

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		project phases, or their financing shall be ensured to the satisfaction of the City. A certificate of occupancy shall not be issued for any building within the SPA until the water conveyance infrastructure sufficient to serve such building has been constructed and is in place.				
53-84	3A.18-2b (FPASP EIR/EIS)	<p>Demonstrate Adequate Off-Site Water Treatment Capacity (if the Off-Site Water Treatment Plant Option is Selected).</p> <p>If an off-site water treatment plant (WTP) alternative is selected (as opposed to the on-site WTP alternative), the project applicant(s) for any particular discretionary development application shall demonstrate adequate capacity at the off-site WTP. This shall involve preparing a tentative map-level study and paying connection and capacity fees as determined by the City. Approval of the final project map shall not be granted until the City verifies adequate water treatment capacity either is available or is certain to be available when needed for the amount of development identified in the tentative map before approval of the final map and issuance of building permits for all project phases. A certificate of occupancy shall not be issued for any building within the SPA until the water treatment capacity sufficient to serve such building has been constructed and is in place.</p>	Before approval of final maps and issuance of building permits for any project phases.	City of Folsom Community Development Department	This condition is not applicable to this subdivision. The water supply for this subdivision is provided by the City of Folsom Water Treatment Plant rather than an Off-Site Water Treatment Plant	Yes
53-85	4.4-1 (Westland / Eagle SPA)	<p>Conduct Environmental Awareness Training for Construction Employees.</p> <p>Prior to beginning construction activities, the Project Applicant shall employ a qualified biologist to develop and conduct environmental awareness training for construction employees. The training shall describe the importance of onsite biological resources, including special-status wildlife habitats; potential nests of special-status birds; and roosting habitat for special-status bats. The biologist shall also explain the importance of other responsibilities related to the protection of wildlife during construction such as inspecting open trenches and</p>	Before approval of grading or improvement plans or any ground disturbing activities, including grubbing or clearing, for	City of Folsom Community Development Department	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes

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		looking under vehicles and machinery prior to moving them to ensure there are no lizards, snakes, small mammals, or other wildlife that could become trapped, injured, or killed in construction areas or under equipment. The environmental awareness program shall be provided to all construction personnel to brief them on the life history of special-status species in or adjacent to the project area, the need to avoid impacts on sensitive biological resources, any terms and conditions required by State and federal agencies, and the penalties for not complying with biological mitigation requirements. If new construction personnel are added to the project, the contractor's superintendent shall ensure that the personnel receive the mandatory training before starting work. An environmental awareness handout that describes and illustrates sensitive resources to be avoided during project construction and identifies all relevant permit conditions shall be provided to each person.	any project phase.			
53-86	4.4-7 (Westland / Eagle SPA)	<i>Preconstruction Nesting Bird Survey.</i> The Project Applicant shall conduct a preconstruction nesting bird survey of all areas associated with construction activities on the project site within 14 days prior to commencement of construction during the nesting season (1 February through 31 August). If active nests are found, a no-disturbance buffer around the nest shall be established. The buffer distance shall be established by a qualified biologist in consultation with CDFW. The buffer shall be maintained until the fledglings are capable of flight and become independent of the nest, to be determined by a qualified biologist. Once the young are independent of the nest, no further measures are necessary. Pre- construction nesting surveys are not required for construction activity outside of the nesting season.	Before approval of grading or improvement plans or any ground disturbing activities, including grubbing or clearing, for any project phase.	California Department of Fish and Game, and City of Folsom CDD	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes
53-87	3A.5-1a (Westland /	<i>Comply with the Programmatic Agreement.</i> The PA for the project is incorporated by reference. The PA provides a management framework for identifying historic	During all construction phases	City of Folsom CDD; U.S. Army	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance	Yes

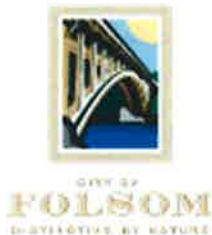
CONDITIONS OF APPROVAL FOR THE PHASE 1 C SOUTH SUBDIVISION (PN 21-086) WEST OF SAVANNAH PARKWAY, SOUTH OF MANGINI PARKWAY AND NORTH OF WHITE ROCK SMALL-LOT VESTING TENTATIVE SUBDIVISION MAP, PLANNED DEVELOPMENT PERMIT- DEVELOPMENT STANDARD DEVIATIONS AND DESIGN REVIEW, AND MINOR ADMINISTRATIVE MODIFICATIONS AND STREET NAMES						
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	Eagle SPA)	properties, determining adverse effects, and resolving those adverse effects as required under Section 106 of the National Historic Preservation Act. This document is incorporated by reference. The PA is available for public inspection and review at the California Office of Historic Preservation 1725 23rd Street Sacramento, CA 95816.		Corp of Engineers	was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	
53-88	3A.5-2 (Westland / Eagle SPA)	<p><i>Conduct Construction Personnel Education, Conduct On-Site Monitoring If Required, Stop Work if Cultural Resources are Discovered, Assess the Significance of the Find, and Perform Treatment or Avoidance as Required.</i></p> <p>To reduce potential impacts to previously undiscovered cultural resources, the project applicant(s) of all project phases shall do the following:</p> <p>Before the start of ground-disturbing activities, the project applicant(s) of all project phases shall retain a qualified archaeologist to conduct training for construction workers as necessary based upon the sensitivity of the project APE, to educate them about the possibility of encountering buried cultural resources and inform them of the proper procedures should cultural resources be encountered.</p> <p>As a result of the work conducted for Mitigation Measures 3A.5-1a and 3A.5-1b, if the archaeologist determines that any portion of the SPA or the off-site elements should be monitored for potential discovery of as-yet- unknown cultural resources, the project applicant(s) of all project phases shall implement such monitoring in the locations specified by the archaeologist. USACE should review and approve any recommendations by archaeologists with respect to monitoring.</p> <p>Should any cultural resources, such as structural features, unusual amounts of bone or shell, artifacts, or architectural remains be encountered during any construction activities, work shall be suspended in the vicinity of the find and the appropriate oversight agency(ies) (identified below) shall be</p>	Before approval of grading or improvement plans or any ground disturbing activities, including grubbing or clearing, for any project phase.	City of Folsom CDD; U.S. Army Corp of Engineers	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City.	Yes

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		<p>notified immediately. The appropriate oversight agency(ies) shall retain a qualified archaeologist who shall conduct a field investigation of the specific site and shall assess the significance of the find by evaluating the resource for eligibility for listing on the CRHR and the NRHP. If the resource is eligible for listing on the CRHR or NRHP and it would be subject to disturbance or destruction, the actions required in Mitigation Measures 3A.5-1a and 3A.5-1b shall be implemented. The oversight agency shall be responsible for approval of recommended mitigation if it is determined to be feasible in light of the approved land uses and shall implement the approved mitigation before resuming construction activities at the archaeological site.</p> <p>Mitigation for the off-site elements outside of the City of Folsom's jurisdictional boundaries must be coordinated by the project applicant(s) of each applicable project phase with the affected oversight agency(ies) (i.e., El Dorado and/or Sacramento Counties, or Caltrans).</p> <p>The project applicant, in coordination with USACE, shall ensure that an archaeological sensitivity training program is developed and implemented during a pre-construction meeting for construction supervisors. The sensitivity training program shall provide information about notification procedures when potential archaeological material is discovered, procedures for coordination between construction personnel and monitoring personnel, and information about other treatment or issues that may arise if cultural resources (including human remains) are discovered during project construction. This protocol shall be communicated to all new construction personnel during orientation and on a poster that is placed in a visible location inside the construction job trailer. The phone number of the USACE cultural resources staff member shall also be included.</p> <p>The on-site sensitivity training shall be carried out each time a new contractor will begin work in the APE and at the beginning of</p>				

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		<p>each construction season by each contractor. If unanticipated discoveries of additional historic properties, defined in 36 CFR 800.16 (l), are made during the construction of the project, the USACE shall ensure that they will be protected by implementing the following measures:</p> <p>The Construction Manager, or archaeological monitor, if given the authority to halt construction activities, shall ensure that work in that area is immediately halted within a 100-foot radius of the unanticipated discovery until the find is examined by a person meeting the professional qualifications standards specified in Section 2.2 of Attachment G of the HPMP. The Construction Manager, or archaeological monitor, if present, shall notify the USACE within 24 hours of the discovery.</p> <p>The USACE shall notify the State Historic Preservation Officer (SHPO) within one working day of an unanticipated discovery and may initiate interim treatment measures in accordance with this HPTP. Once the USACE makes a formal determination of eligibility for the resource, the USACE will notify the SHPO within 48 hours of the determination and afford the SHPO an opportunity to comment on appropriate treatment. The SHPO shall respond within 72 hours of the request to consult. Failure of the SHPO to respond within 72 hours shall not prohibit the USACE from implementing the treatment measures.</p> <p>The project applicants shall be required to submit to the City proof of compliance in the form of a completed training roster and copy of training materials.</p>				
53-89	3A.5-3 (Westland / Eagle SPA)	<p><i>Suspend Ground-Disturbing Activities if Human Remains are Encountered and Comply with California Health and Safety Code Procedures.</i></p> <p>In accordance with the California Health and Safety Code, if human remains are uncovered during ground-disturbing activities, including those associated with off-site elements, the project applicant(s) of all project phases shall immediately halt all ground-disturbing activities in the area of the find and notify the</p>	During all ground disturbing activities, for any project phase.	Sacramento County Coroner; Native American Heritage Commission; City of Folsom CDD	The City and its Environmental Compliance Consultant (Helix) have reviewed all MMRP conditions and verified compliance with this Mitigation Measure. Compliance was verified prior to commencement of grading and construction in Fall of 2021. Compliance table is on file with the City. No human remains have been encountered in the subdivision during grading and construction	Yes

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		<p>Sacramento County Coroner and a professional archaeologist skilled in osteological analysis to determine the nature of the remains. The coroner is required to examine all discoveries of human remains within 48 hours of receiving notice of a discovery on private or public lands (California Health and Safety Code Section 7050.5[b]). If the coroner determines that the remains are those of a Native American, he or she must contact the NAHC by phone within 24 hours of making that determination (California Health and Safety Code Section 7050[c]).</p> <p>After the coroner's findings are complete, the project applicant(s), an archaeologist, and the NAHC-designated Most Likely Descendant shall determine the ultimate treatment and disposition of the remains and take appropriate steps to ensure that additional human interments are not disturbed. The responsibilities for acting on notification of a discovery of Native American human remains are identified in Section 5097.9 of the California Public Resources Code.</p> <p>Upon the discovery of Native American remains, the procedures above regarding involvement of the applicable county coroner, notification of the NAHC, and identification of an Most Likely Descendant shall be followed. The project applicant(s) of all project phases shall ensure that the immediate vicinity (according to generally accepted cultural or archaeological standards and practices) is not damaged or disturbed by further development activity until consultation with the Most Likely Descendant has taken place. The Most Likely Descendant shall have 48 hours after being granted access to the site to inspect the site and make recommendations. A range of possible treatments for the remains may be discussed: nondestructive removal and analysis, preservation in place, relinquishment of the remains and associated items to the descendants, or other culturally appropriate treatment. As suggested by AB 2641 (Chapter 863, Statutes of 2006), the concerned parties may extend discussions beyond the initial 48 hours to allow for the discovery of additional remains. AB 2641(e) includes a list of site protection measures and states that the project applicant(s) shall comply with one or more of the</p>				

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		following requirements: record the site with the NAHC or the appropriate Information Center, use an open-space or conservation zoning designation or easement, or record a reinternment document with the county. The project applicant(s) or its authorized representative of all project phases shall rebury the Native American human remains and associated grave goods with appropriate dignity on the property in a location not subject to further subsurface disturbance if the NAHC is unable to identify an Most Likely Descendant or if the Most Likely Descendant fails to make a recommendation within 48 hours after being granted access to the site. The project applicant(s) or its authorized representative may also reinter the remains in a location not subject to further disturbance if it rejects the recommendation of the Most Likely Descendant and				



Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	Public Hearing
SUBJECT:	Appeal by Igor Semenyuk of a Decision by the Historic District Commission Denying a Conditional Use Permit for the Lakeside Memorial Lawn Crematorium project (PN 19-182) located at 1201 Forrest Street
FROM:	Community Development Department

RECOMMENDATION/CITY COUNCIL ACTION

Move to deny the appeal by Igor Semenyuk of a Decision by the Historic District Commission Denying a Conditional Use Permit for the Lakeside Memorial Lawn Crematorium project (PN 19-182) located at 1201 Forrest Street.

BACKGROUND

The Lakeside Memorial Lawn located at 1201 Forrest Street has been in operation since the 19th century and represents a combination of several old Folsom cemeteries, including the Masonic, Odd Fellows, Jewish, Citizen's, and Cook's cemeteries. It is adjacent to the Chung Wah cemetery to the south, dredge mining tailings to the east, the Folsom Lake State Recreation Area and American River to the west and the Preserve/Lake Natoma Shores subdivision to the north.

The Lakeside Cemetery (including the Odd Fellows and Mason's Cemeteries) and the adjacent Chung Wah Cemetery and mining tailings all appear on the City of Folsom's Cultural Resources Inventory list and the Lakeside Memorial Lawn and Chung Wah cemeteries were both designated as "historic" by the Sacramento County Board of Supervisors and Cemetery Advisory Commission. The Chung Wah Cemetery is also listed on the National Register of Historic Places.

The Planning Commission approved a Use Permit and Variance for Mausoleums at Lakeside Memorial Lawn in 1991 (PC91-042). An amendment to the approval was granted in 1995 (PC95-033). That approval allowed for the construction of twelve mausoleums. To date, two mausoleums have been built.

In 2002, Lorin Claney applied for a Conditional Use Permit (CUP) for a crematorium at the Lakeside Memorial Lawn (PN 02-258). Staff prepared a Draft Initial Study/Mitigated Negative Declaration (IS/MND) for the project and routed it for public review. Staff received several comments from the public and from the Sacramento County Cemetery Advisory Commission recommending denial of the project.

While no formal air quality or cultural resources studies were done as part of the 2002 IS/MND, staff prepared a staff report for the Historic District Commission recommending denial of the project in 2003 on the grounds that the letter from the Cemetery Advisory Commission stated that the crematorium would have a negative impact on the historical character of the existing cemetery and historical use of the area. The staff report stated that, based on the Cemetery Advisory Commission letter, the conglomeration of historic cemeteries, combined with the tailings, create a rare combination of unique cultural resources that will be impacted by the proposal, that a crematorium is not a use consistent with a historic cemetery, and that approval of a CUP for a contemporary use such as a crematorium would jeopardize the eligibility status of the site as an historic cemetery by the County. Because the staff report recommended denial, the CEQA analysis was not included in the staff report since CEQA does not apply to projects which a public agency rejects or disapproves.

Prior to the scheduled public hearing, the applicant withdrew the application based on costs associated with preparing additional environmental analysis for air quality and cultural resources studies. The Historic District Commission did not review or consider the project proposed in 2003, including the CUP application or the associated IS/MND.

An existing maintenance building, approximately 1,071 square feet in area (where the crematorium is proposed), is located along the south border of the cemetery. The design review application for a second 1,600-square-foot maintenance shed was approved by the Historic District Commission in 2020 (PN 20-160) and the shed was subsequently constructed.

On May 16, 2019, the applicant (Igor Semenyuk) submitted a development application for approval of a Conditional Use Permit for operation of a crematorium inside the Miller Funeral Home located at 507 Scott Street. In coordinating with the Sacramento Metropolitan Air Quality Management District (SMAQMD), the applicant determined that a crematorium at this location would not be feasible. The applicant then proposed to perform alkaline hydrolysis (liquid cremations) at this location that would not employ any burning or produce emissions but instead would produce a liquid effluent that would be flushed down sewage pipes. While this method of cremation was approved in California in 2017, City Environmental and Water

Resources staff, in coordination with the Sacramento Regional County Sanitation District, did not have enough local examples of the impacts of these effluents to support this proposal. The applicant subsequently proposed to locate the crematorium in the Lakeside Memorial Lawn in the same metal structure in which it was proposed in 2003.

The proposed project includes the installation of one HCT Apex-250 crematory (manufactured by Hartwick Combustion) within an existing metal structure. The roof of the structure would be cut open to accommodate this device, but the structure's footprint would not be expanded. In addition, a 10-foot-wide by 15-foot-wide walk-in cooler would be installed inside the structure to provide temporary, short-term storage of human remains prior to cremation. Two 250-gallon propane tanks would be installed on a concrete pad along the northern side of the structure to provide power for the crematorium, as no gas lines currently exist on the property. The concrete pad covers approximately 38.3 square feet of ground. An exhaust stack would be installed on the roof of the metal structure approximately 19.5 feet from finished grade. The applicant proposes up to four cremations per day and expects that the total will not exceed 500 cremations per year. Average cremation time is approximately 90 minutes. One to two employees would be present to operate the crematorium during the cremation process. Existing parking spaces would be utilized to accommodate employees and visitors.

If the CUP is approved, the project is also required to obtain an authority to construct permit to operate (A/C, PTO) from SMAQMD before the crematory can be installed and operated. As part of that process, the applicant would be required to provide analysis to ensure the crematory will meet all air district rules/regulations and significance thresholds during the air district's A/C, PTO application and review process. In the event the air district's refined health risk assessment analysis shows that cancer risk or hazard index numbers exceed the significance thresholds noted, further restrictions to the crematorium would be required by SMAQMD before they would issue the A/C, PTO. All crematoriums in Sacramento County are also subject to regular inspections by SMAQMD, which investigates all public complaints about the facilities and fines all facilities for any violations.

Based on the potential for the project to impact nearby residences and cultural resources, staff had an environmental study, called an Initial Study (IS), prepared. Staff retained HELIX Environmental Planning, Inc. to prepare the study, including a full Air Quality and Greenhouse Gas Emissions Assessment. HELIX also hired ECORP Consulting to prepare a Cultural Resources Inventory Report that informed the environmental conclusions regarding cultural and tribal resources. As a result of the Initial Study, a Mitigated Negative Declaration (MND) was prepared by HELIX and that environmental document found that all environmental impacts related to the project would be less than significant with mitigation. The IS/MND is included as an appendix within Attachment 2 of this staff report.

After the IS/MND had been made public, staff prepared a staff report recommending approval of the proposed CUP and IS/MND to the Historic District Commission (provided in Attachment 2). In the staff report, staff noted that with the rigorous analysis of air quality and

cultural resources provided in the new IS/MND, staff determined that the project would not be detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing or working in the neighborhood or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city, as the proposed project would not have negative impacts to nearby commercial or residential uses that have not been mitigated. Notice of the IS/MND publication and the public hearing in front of the Historic District Commission were placed in the Sacramento Bee and mailed to property owners within 300 feet of the subject property.

During the life of the project application, staff received directly or was forwarded over 120 unique public comments regarding the project and/or the environmental document, the vast majority of which expressed concerns about the project and the environmental document. Also submitted to staff were an online petition with over 160 signatures, a hand-signed petition with almost 500 signatures, and over 300 signed form letters all expressing opposition to the proposed project. Staff included these letters and petitions as part of the staff report and made them available to the public, the Historic District Commissioners, and the applicant. Staff also summarized each of the unique points made in the public comments in the staff report and HELIX provided a response letter to the general points that were made in several of the letters regarding environmental issues addressed in the IS/MND. All of the comment letters were provided to the Historic District Commission for their consideration (included as part of Attachment 2 to this staff report) at the February 16, 2022 meeting. Comments received after the publication of the staff report are included in Attachment 3 of this staff report.

The individuals who were opposed to the proposed project voiced concerns regarding a number of topics including the following:

- Concerns regarding air quality emissions and the data used in the Air Quality and Greenhouse Gas Emissions Assessment;
- Concerns regarding traffic, circulation and parking;
- Concerns regarding wildfire and life safety in case of fire caused by the crematorium;
- Cultural and archaeological concerns regarding the impact of the crematory machine on nearby cultural resources and burial sites
- Concerns regarding the appropriateness of the land use (a crematorium) in the Historic District;
- Aesthetic concerns regarding the height and design of the stack and impacts to views from nearby cultural resources and residences;
- Whether members of the public could be present at cremations;
- Differences in the conclusions of the 2003 and 2022 staff reports for the crematorium;
- Concerns regarding the potential decrease in property values of nearby residences due to the presence of the crematorium; and
- Psychological effects of nearby residences being located near a crematorium.

After publication of the staff report, staff received six additional public comment letters that were added to the record. Included in those comments were two letters from individuals belonging to the Sacramento County Cemetery Advisory Commission. The Chair of the Cemetery Advisory Commission reiterated the points that were made by the group in 2003, stating that construction of a crematorium would be inappropriate near the surrounding cultural resources of historic importance. The Chair also provided evidence that the Lakeside Cemetery and Chung Wah Cemetery were listed for historical designation by the County. Another member of the Cemetery Advisory Commission wrote a letter stating that the proposed crematorium could threaten the nomination of the Young Wo Cemetery and that construction of the crematorium could adversely affect the historic features and burials of those buried nearby. HELIX provided a topical response to these comments prior to the hearing. This memorandum has been included in Attachment 4 of this staff report.

The Historic District Commission reviewed the Lakeside Memorial Lawn Crematorium project at its February 16, 2022 meeting. At this meeting, 36 individuals (mostly residents of the Preserve/Lake Natoma Shores subdivision and the surrounding neighborhoods of the Historic District, as well as a representative from the Sacramento County Funeral Commission) voiced concerns about and asked for denial of the proposed project, citing many of the same reasons expressed in the previously submitted letters. Following extensive public comment, the Historic District Commission engaged in a lengthy discussion regarding the proposed project on primary topics related to air quality, fire hazards, aesthetics, cultural resources, property values and required CUP findings.

With respect to air quality, the Historic District Commission discussed the preliminary Health Risk Assessment performed for the IS/MND and spoke with a representative from SMAQMD about the air district's permitting and enforcement process. It was reiterated that Best Available Control Technologies (BACTs) would be required for the cremation machine which would limit air pollution. Regarding complaints, the SMAQMD explained that they typically respond within 24 hours and visit both the complainant and the potential sources of the complaint. Notices of violation and fines may be levied under the air district's authority to enforce public nuisance provisions and facilities with persistent violations or nuisances are subject to permit revocation. SMAQMD Staff also noted that persistent complaints that result in such actions may result in revocation of the CUP.

With regards to an increase in potential cancer rates due to the crematorium, HELIX staff began the discussion by noting that there is a risk of potentially developing cancer inherent in breathing the air around us. In the state of California, that baseline risk is approximately 800 cases per 1 million people. When analyzing whether a proposed project has the potential to increase that baseline risk due to the project's impact on air quality, an increase in 10 cancer cases per million people would be considered significant. Based on modeling and its analysis, HELIX determined that this project has the potential to increase the baseline risk of potentially

developing cancer by 0.5 cases per million people. This analysis assumes that someone would remain at the maximum point of exposure consecutively for 30 years. HELIX staff also explained that this analysis does not mean that anyone will necessarily develop cancer; the focus is on the potential risk that it will happen. For CEQA purposes, a potential increase of 0.5 cases per million people is not considered significant.

Ultimately, a majority of the Historic District Commissioners expressed confidence in the air quality analysis in the IS/MND and SMAQMD process.

With respect to fire hazards, one or more Historic District Commissioners asked about the potential fire hazards of the proposed propane tanks. Folsom Fire Chief Ken Cusano clarified that the commercial-grade tanks require an operation permit from the Fire Department and are designed to vent. Chief Cusano was also asked about public concern regarding the adequacy of existing evacuation routes in case of fire. Chief Cusano stated that since the crematorium posed no significant fire danger to the area as conditioned, there would be no significant impact to evacuation routes. Chief Cusano also stated that the Fire Department can open the opposite end of Forrest Street if needed and that potential issues with Light Rail gates being in the down position on the evacuation route during an emergency event can be worked out by contacting Light Rail during an emergency to stop trains from coming to the area. During deliberation, individual Historic District Commissioners expressed confidence with regards to staff's analysis of fire hazard impacts.

With respect to aesthetics, individual Historic District Commissioners asked for clarification on the ultimate height of the stack and what kind of control SMAQMD has over the height. Commissioners also questioned whether design review would be warranted for the proposed stack or whether they could provide design parameters as part of the CUP so that a separate design review would not be necessary. HELIX commented that the analysis done for the IS/MND assumed a height of 19.5 feet from finished grade based on specifications provided to them by the applicant. The SMAQMD representative noted that the Air District does not advocate for a specific height unless the height proposed by the applicant does not meet the district's air quality standards for nearby sensitive receptors.

With respect to cultural resources, individual Historic District Commissioners expressed concern that the project was not consistent with Historic District Design and Development Guidelines (DDGs) Policy 2.2, which states that "the City shall encourage National Register nomination of historic buildings as well as other historical designations by state or local agencies" given the Sacramento County Cemetery Advisory Commission's letters. Individual Commissioners also expressed concern that the proposed crematorium is not an historic use. ECORP staff, who prepared the Cultural Resources Inventory Report, stated that many different cultures' funerary practices have occurred at Lakeside Memorial Lawn and that introducing a new practice (cremation) would not be the first time that a new funerary practice has been introduced on the project site. They also stated that while cremation is not a typical

part of Chinese funerary practices, burning of possessions was common in such practices, and occurred in the Chung Wah cemetery until the 1960's.

ECORP stated that whether a crematory is an appropriate use on the project site is not an archeological question but if a crematory were to be located on the project site, the proposed location would be the most appropriate place for it since it is in a modern, previously disturbed maintenance area of the cemetery. Historic District Commissioners agreed that the appropriateness of the use is for the HDC to decide, but several commissioners stated that their cultural resources concerns had not been adequately addressed, especially since the Sacramento County Cemetery Advisory Commission stated that the crematorium would have a negative impact on the historical significance of the cemetery. Individual Historic District Commissioners stated that they wished that the Cemetery Advisory Commission would have provided more information regarding how specifically the crematory would have a negative impact but stated that the Cemetery Advisory Commission would have time to provide a greater level of detail before a City Council meeting, as it was assumed that their decision would be appealed by either the public or the applicant based on the level of project controversy. Staff has since reached out to the Cemetery Advisory Commission directly after the hearing for more information. The Cemetery Advisory Commission communicated to staff that they were able to meet regarding the proposed crematory prior to publication of the City Council staff report and that the comments they made in 2002 still stand.

With respect to property values, individual Historic District Commissioners asked whether property owners would be required to provide disclosures that they live near a crematorium. The Assistant City Attorney stated that they did not see anything specifically in their research of real estate laws that would necessarily require that the presence of a crematory would have to be disclosed but recommended that anyone with concerns about real estate disclosures should consult a realtor or a real estate attorney. However, multiple Historic District Commissioners stated that they believe that the presence of a crematory would be detrimental to nearby residential property values and that realtors would take that into account when determining home listing prices.

Finally, with respect to the required CUP findings, individual Historic District Commissioners asked for clarification as to why the findings in the 2003 staff report differed from those in the 2022 staff report. One commissioner referenced that the 2003 staff report showed that the originally proposed crematorium was not consistent with the DDGs and questioned how the newly proposed project is different. Multiple commissioners brought up the high volume of public comments received from members of the surrounding community and wondered if the finding that the project would not be detrimental to the peace, morals, comfort and general welfare of persons residing or working in the neighborhood or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city could be met given the amount of opposition from the neighborhood and chances of property values going down due to the presence of a crematorium in the area. Staff clarified that the 2003 staff

report relied on an internally produced IS/MND with no technical studies performed by cultural resources experts, and without further information beyond the letter from the Cemetery Advisory Commission, denial was recommended. Staff recommended approval in the 2022 staff report in part because of the conclusions of the Ecorp Cultural Resources Inventory Report.

At the conclusion of the Historic District Commission meeting on February 16, 2022, a motion was made to approve the proposed CUP and IS/MND with additional conditions added to install a weather station on the structure on which the crematory would be located to collect wind data for potential smoke and odor complaints and for the stack to be subject to design review by the Historic District Commission subsequent to obtaining a permit from the SMAQMD. That motion was seconded but failed by a vote of 1-6. A second motion was then made to deny the CUP using the same finding that was made in the 2003 staff report that stated that the use is detrimental to the health, safety, or general welfare of persons residing or working in the neighborhood, and detrimental or injurious to property and improvements in the neighborhood and the general welfare of the city because introduction of this use will impact the historical character of the existing cemetery and historical use of the area. This motion was seconded and passed with a vote of 6-1.

Since the Historic District Commission hearing, and in response to the timely appeal, staff received additional comments from the public and the Cemetery Advisory Commission for consideration during the appeal. All such comments oppose the project and recommend denial of the appeal. These letters are included in Attachment 6 of this staff report.

The City Council considered the appeal at the April 26, 2022 meeting. At that meeting, staff presented the project and appeal, the applicant presented his appeal, and the City Council opened the public hearing and heard public testimony from members of the public who were physically present at the meeting. The public hearing was then continued to this meeting in response to technical challenges to ensure that any interested persons intending to participate remotely would have an opportunity to provide comments prior to any action/decision by the City Council. Public comment letters that were received after publication of the April 26, 2022 staff report are included in Attachment 7 and public comment letters received since the April 26, 2022 hearing are included in Attachment 8.

POLICY / RULE

As set forth in Section 17.52.700 of the Folsom Municipal Code, actions of the Historic District Commission may be appealed to the City Council. The appeal shall be in writing, shall state the specific reason for the appeal and grounds asserted for relief, and shall be filed no later than 10 calendar days after the date of the action being appealed.

APPEAL/ANALYSIS

On February 22, 2022, project applicant Igor Semenyuk submitted a timely appeal of the decision of the Historic District Commission denying the proposed project (Attachment 1). The applicant/appellant later submitted a supplemental letter providing additional reasoning for the appeal, also included in Attachment 1.

In his appeal and supplemental letter, Mr. Semenyuk makes the following claims:

1. The findings for denial were that the project was detrimental to health and safety/general welfare of the neighborhood, and to property values in the neighborhood and city and that there would be impacts to the historical character of the cemetery and area. However, no data was provided by the Commission to support their findings for denial. Their denial was based on the residents' concerns rather than the data from the staff report and IS/MND.
2. Staff recommended approval of the crematorium and an IS/MND was prepared by multiple parties, including experts who were present at the hearing to defend their findings. The IS/MND showed that the project was well below CEQA thresholds of significance.
3. The crematorium would not impact the historical character of the cemetery since it is being placed inside a modern metal shed within a modern area of the cemetery.
4. There are existing regional cemeteries included on historical resources lists in which modern crematoriums currently operate.
5. The crematorium would not significantly impact property values of the neighborhood as property values are dictated by current market values in real estate.
6. There are no industrially zoned areas in Folsom that have a neighborhood more than 500 feet from the property, so alternative industrially-zoned locations would not result in fewer impacts to residences.

City Staff Response

Reasoning Behind Commission Findings for Denial

In the finding for denial, the Historic District Commission stated that the introduction of the crematory use will impact the historical character of the existing cemetery and historical use of the area and would therefore be detrimental or injurious to property and improvements in the neighborhood and the general welfare of the city. In their deliberations, the Commission cited the letters received from the Cemetery Advisory Commission which stated that the crematorium would be inappropriate near the surrounding cultural resources of historic

importance and could threaten the nomination of the Young Wo Cemetery and adversely affect the historic features and burials of those buried nearby. While the Historic District Commission discussed factors related to air quality, property values, and fire hazards, the finding they made was related specifically to the historical character of the cemetery and area, including the historical nature of the Young Wo Cemetery and the people who are buried there.

Staff's Recommendation for HDC Approval

As explained in the February 2022 HDC staff report, the analysis in the IS/MND did result in a staff recommendation for HDC approval of the project. To approve the project, the Historic District Commission had to find that the project WOULD NOT be detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing or working in the neighborhood or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city. However, after considering the staff report, IS/MND, public comments and comments from the Cemetery Advisory Commission, the Historic District Commission determined that the project WOULD (as noted in the analysis above), be detrimental or injurious to property and improvements in the neighborhood and the general welfare of the city due to the introduction of the crematory use impacting the historical character of the existing cemetery and historical use of the area.

Impacts to the Historical Character of the Cemetery

During the Historic District Commission's questions to staff and consultants, Lisa Westwood with Ecorp Consulting stated that if a crematorium were to be placed on the cemetery grounds, the existing metal building within the modern maintenance area would be the best location for a crematorium on the project site. Furthermore, the Cultural and Tribal Resources section of the IS/MND found that there would be a less than significant impact with standard mitigation related to the unanticipated discovery of archeological or paleontological resources during construction based on the analysis provided in Ecorp's Cultural Resources Inventory Report. However, the letters sent by the Cemetery Advisory Commission stated that the crematorium would be inappropriate near the surrounding cultural resources of historic importance and could threaten the nomination of the Young Wo Cemetery and adversely affect the historic features and burials of those buried nearby. Ultimately, the Historic District Commission voted to deny the Conditional Use Permit because the Commission found that introduction of a crematorium would impact the historical character of the existing cemetery and historical use of the area.

Other Historic Cemeteries with Crematoriums

Staff coordinated with SMAQMD to identify all crematoriums operating in Sacramento County. While there are five cemeteries in Sacramento County with operational crematoriums, staff did not identify any of these cemeteries as being on a cultural or historical resources list.

Impacts to Property Values

Several members of the Preserve/Lake Natoma Shores community expressed concern that the crematorium would negatively impact the property values of their homes. While the Historic District Commission discussed potential impacts to property values, the finding for denial was based on the introduction of the crematory use negatively impacting the historical character of the existing cemetery and historical use of the area.

Industrial Properties Adjacent to Residential Uses

Staff researched industrial properties located within the city in relation to their proximity to residential uses. Staff found that, of the properties within the city zoned M-1 and M-2, four areas (Lake Forest Technical Center, Kikkoman property, 250 Outcropping Way, and 600 Coolidge Drive) have either buildings or vacant property located over 600 feet from residential uses, though none of the properties currently on the real estate market consist of a building in scale with the small shed in which the applicant is proposing to operate the crematorium.

FINDING FOR DENIAL

Based on the foregoing, staff recommends that the appeal be denied based on the following finding:

CONDITIONAL USE PERMIT FINDING

- A. THE USE APPLIED FOR IS DETRIMENTAL TO THE HEALTH, SAFETY, PEACE, COMFORT, MORALS, OR GENERAL WELFARE OF PERSONS RESIDING OR WORKING IN THE NEIGHBORHOOD, AND DETRIMENTAL OR INJURIOUS TO PROPERTY AND IMPROVEMENTS IN THE NEIGHBORHOOD AND THE GENERAL WELFARE OF THE CITY BECAUSE INTRODUCTION OF THIS USE WILL IMPACT THE HISTORICAL CHARACTER OF THE EXISTING CEMETERY AND HISTORICAL USE OF THE AREA.

CONCLUSION

Based on the information contained in this report and the Historic District Commission's denial of the Conditional Use Permit for the Lakeside Memorial Lawn Crematorium project (PN 19-182) located at 1201 Forrest Street, staff recommends denial of the appeal by Igor Semenyuk to overturn the Historic District Commission decision.

ATTACHMENTS

1. Letter of Appeal from Igor Semenyuk, dated February 22, 2022, and Supplemental Appeal Reasoning, received March 11, 2022
2. Historic District Commission Staff Report, dated February 16, 2022

3. Public Comment Letters received after the February 16, 2022 Historic District Commission Meeting
4. HELIX Topical Responses to IS/MND Comments
5. Draft Minutes from February 16, 2022 Historic District Commission Meeting
6. Public Comment Letters Regarding the Applicant's Appeal
7. Public Comment Letters Received After Publication of the April 26, 2022 Staff Report
8. Public Comment Letters Received After the April 26, 2022 Hearing

Submitted,

A handwritten signature in blue ink, appearing to read "Pam Johns", with a long horizontal flourish extending to the right.

PAM JOHNS
Community Development Director

Attachment 1

Letter of Appeal from Igor Semenyuk, dated
February 22, 2022, and Supplemental Appeal Reasoning,
received March 11, 2022

CITY OF FOLSOM

FOLSOM CITY CLERK'S DEPT
22 FEB '22 AM 11:10

APPEAL FORM

NAME OF APPELLANT: Igor Semenyuk

MAILING ADDRESS: 507 SCOTT ST
Folsom CA 95670

Interest in Matter: Lakeride Memorial Lawn Crematorium

Daytime Phone: 916-548-6808

Action Being Appealed: Motion was not passed.

Date of Decision or Date Project was Heard: 2/16/2022

Reason for Appeal: Project denied for not having met a
standard. Standard was not provided when asked.
Environmental report declared a negative declaration,
all data and science showed it to be
way below threshold of CA regulations. Resident near
area very bias. ~~and~~ ~~pro~~

[Signature] 2/22/2022
Appellant's Signature Date

STAFF USE ONLY:

Date Received: February 22, 2022 Fee Paid: \$479

Planning Comm. or Historic District Comm.
Decision Appeal

Admin. (staff decision) Appeal

Type of Project/fee:
Owner Occupied/Single Family Dwelling \$238
All Others \$479

Type of Project/fee:
Owner Occupied \$233
Other \$468

Tentative Hearing Date: _____ Time Limit Waived: _____

Copies to: Community Development Director
City Manager
City Attorney
City Clerk

Received by: [Signature]

Appeal fees set by City Council Resolution No. 10297 approved June 11, 2019.

To: Folsom City Council

Reason for Appeal of Historic District Commissioners Meeting held 2/16/2022

In the interest of the project called Lakeside Memorial Lawn Crematorium the motion of the commissioners was denial, all but one- Commissioner Justin Raithel.

The Findings claimed for denial was as follows:

- Detrimental to health and safety/ general welfare of neighborhood.
- Detrimental to property value in neighborhood and city.
- Impacts historical character of cemetery and area.

The recommendation of the planning committee was approval of the crematorium. A full scope Environmental Report was done with multiple experts. Experts were present in the meeting to defend their findings. The findings and data showed that the project was well below any thresholds of CA standards for health and safety. The crematorium would not impact any historical character of the cemetery as the project was being placed inside the modern metal shed, in the modern area of the cemetery. It does not significantly impact the property values of the neighborhood. That is dictated by current market values in real estate.

In the meeting, a presentation was shown with funeral homes and cemeteries that are listed as historical cemeteries having and operating modern crematoriums in their facilities. All of the crematoriums shown are closer to residents, business and restaurants than the proposed crematorium at Lakeside Cemetery.

The data was not considered at all. Everything was drowned out by residents who just don't want it near their neighborhood. They want cremation services; they just want it done in other cities. There is no area in Folsom that is zoned industrial that does not have a neighborhood within 500 feet of it. Folsom is growing. Cremation demand is growing. A crematory is essential to the growing demand. If you can't place a crematory in a cemetery, then where can you?

The denial was based on findings, yet when asked what those were, none could be provided. No data was provided to prove that it is detrimental to the health and safety, that it is detrimental to the property values and that it impacts any historical character. No matter what the experts data was, it was just dismissed with prejudice.

We implore you to reconsider the historic commission decision on the crematory. A city is larger than just one neighborhood.

Thank you,

Igor Semenyuk



Attachment 2

Historic District Commission Staff Report Dated February 16, 2022



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

AGENDA ITEM NO. 1
Type: Public Hearing
Date: February 16, 2022

Historic District Commission Staff Report

50 Natoma Street, Council Chambers
Folsom, CA 95630

Project: Lakeside Memorial Lawn Crematorium Conditional Use Permit
File #: PN-19-182
Request: Conditional Use Permit
Location: 1201 Forrest Street
Parcel(s): 070-0260-001-0000
Staff Contact: Josh Kinkade, Associate Planner, 916-461-6209
jkinkade@folsom.ca.us

Property Owner

Name: Lakeside Memorial Lawn
Address: 1201 Forrest Street
Folsom CA 95630

Applicant

Name: Igor Semenyuk
Address: 507 Scott Street
Folsom, CA 95630

Recommendation: Conduct a public hearing and upon conclusion recommend approval of a Conditional Use Permit to allow for a crematory to operate in an existing metal structure situated within the Lakeside Memorial Lawn cemetery located at 1201 Forrest Street (PN 19-182) subject to the findings included in this report (Findings A-K) and attached conditions of approval (Conditions 1-30).

Project Summary: The proposed project includes operation of a crematorium in an existing 1,071-square foot metal structure within the Lakeside Memorial Lawn cemetery located at 1201 Forrest Street.

Table of Contents:

1. Description/Analysis
2. Background
3. Proposed Conditions of Approval
4. Vicinity Map
5. Overall Site Plan, Detailed Site Plan and Floor Plan Dated 2-19-20
6. Hartwick Combustion Technologies Floor Plan, Outer Dimensions and Specs
7. Updated Site Plan with Current Layout of Maintenance Area
8. Applicant's Rendering of Proposed Crematorium Stack
9. Applicant's Narrative
10. Photographs of the Project Site
11. Draft Initial Study/Mitigated Negative Declaration, Dated January 2022



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

AGENDA ITEM NO. 1
Type: Public Hearing
Date: February 16, 2022

- 12. Images of Crematoriums Adjacent to Residential Uses in the Region
- 13. Public Comments Received Prior to Staff Report Publication

Submitted,

A handwritten signature in blue ink, appearing to read "Pam Johns", with a long horizontal flourish extending to the right.

PAM JOHNS
Community Development Director

ATTACHMENT 1 DESCRIPTION/ANALYSIS

APPLICANT'S PROPOSAL

The applicant, Igor Semenyuk, is requesting approval of a Conditional Use Permit (PN 19-182) to operate a crematorium in an existing 1,071-square foot metal structure within the Lakeside Memorial Lawn cemetery located at 1201 Forrest Street. The proposed project includes the installation of one HCT Apex-250 crematory (manufactured by Hartwick Combustion) within an existing metal structure. The roof of the structure would be cut open to accommodate this device, but the structure's footprint would not be expanded. In addition, a 10-foot-wide by 15-foot-wide walk-in cooler would be installed inside the structure to provide temporary, short-term storage of human remains prior to cremation. Two 250-gallon propane tanks will be installed on a proposed concrete pad along the northern side of the structure to provide power for the crematorium, as no gas lines currently exist on the property. The concrete pad will cover approximately 38.3 square feet of ground. An exhaust stack will be installed on the roof of the metal structure (as shown in Attachment 6). The applicant proposes up to four cremations per day and expects that the total will not exceed 500 cremations per year. Average cremation time is approximately 90 minutes. One to two employees would be present to operate the crematorium during the cremation process. Existing parking spaces would be utilized to accommodate employees and visitors. The Applicant's project description, which includes additional information related to the cremation process and other project-related details, is included as Attachment 9. Please refer to Attachment 5 for the proposed site plans and floor plan and Attachment 6 for the Hartwick Combustion Technologies Floor Plan, Outer Dimensions and Specifications.

POLICY/RULE

Section 17.52.550 of the Folsom Municipal Code (FMC) states that cemeteries are permitted in the Open Space/Public Primary Area of the Historic District with a conditional use permit ("CUP"). The Folsom Municipal Code does not address crematoriums as a land use.

Section 17.52.360 of the FMC states that the Historic District Commission (HDC) shall have final authority relating to the issuance of Conditional Use Permits for any of the uses or purposes for which such permits are required or permitted by the terms of this title, within the boundaries of the Historic District.

Use Permits are governed by Chapter 17.60 of the FMC. Section 17.60.040 states that the findings of the Commission shall be that the establishment, maintenance or operation of the use applied for will or will not, under the circumstances of the particular case, be detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing or working in the neighborhood of such proposed use or be detrimental or injurious to property and improvements in the neighborhood, or to the general welfare of

the city.

ANALYSIS

General Plan and Zoning Consistency

The General Plan land use designation for the project site is OS (Open Space) and the zoning designation for the project site is OS/P (Open Space/Public Primary Area of the Historic District) with an underlying zoning of OSC (Open Space Conservation District). The zoning designation is consistent with the General Plan designation.

Cemeteries are a permitted use within the OS/P Primary Area upon approval of a Conditional Use Permit (CUP) per Section 17.52.550 of the FMC. The subject cemetery has been in operation since the 1800's and pre-dates the requirement for a CUP. The cemetery did receive a CUP for operation of a mausoleum in 1995. The proposed crematory would be operating as an accessory use to the existing cemetery, not as a stand-alone business.

Crematories are not specifically addressed in the Folsom Municipal Code. The Commission would then determine whether to approve the request for a CUP and impose appropriate conditions. If not, then the proposed crematory would not be considered a permitted use, even with a CUP.

A primary or principal use is the main use to which premises are devoted and the primary purpose for which the premises exist. Primary uses may be permitted by right or may be conditional uses subject to a CUP. Accessory uses are structures or activities that are subordinate in area, extent, and purpose to the primary use; contribute to the comfort, convenience, or necessity of the principal use; and are located on the same lot and the same zoning district as the principal use. Home occupations are a typical example of an accessory use. By definition, an accessory use must be associated with a principal use and cannot be established on a property without a principal use. (Adam U. Lindgren & Steven T. Mattas, California Land Use Practice (2021) § 4.46, Primary and Accessory Uses.)

In this case, the cemetery is the primary or principal use. The applicant proposes a crematorium as an accessory use to the existing cemetery. As proposed, the crematorium would be subordinate in area, extent, and purpose to that of the existing cemetery. It would provide a service related to and supportive of the service already provided by the cemetery and mausoleum. It would be located on the same lot and in the same zoning district as the principal use. It would be owned and operated by the same people who own and operate the existing cemetery and mausoleum.

As a part of the work to analyze whether a crematory is appropriate as an accessory use

to an existing cemetery, staff researched data available from the Sacramento Air Quality Management District regarding the locations of existing crematoriums within the county. As described in more detail in the Land Use Compatibility/Site Considerations section of the report, staff found that 16 crematoriums are currently operating within Sacramento County, five of which are located within cemeteries (East Lawn Memorial Park, Sacramento Memorial Lawn, St. Mary Cemetery and Funeral Center, and Sunset Lawn Chapel of the Chimes in Sacramento, and Mount Vernon Memorial Park & Mortuary in Fair Oaks).

Public comment letters have stated that a crematory is not appropriate as an accessory use in this case for a variety of reasons. Upon review, staff reads the majority of those comments to raise concerns about the impacts of the proposed use. Many, if not most, of the impacts discussed in the public comment letters can be mitigated by imposing appropriate conditions on the project. For that reason, as well as the fact that the crematory is not proposed as a stand-alone business but as an accessory use to the existing cemetery, staff analyzed those comments through the lens of the CUP.

Based on the analysis described above, staff concluded that the crematorium, as proposed, is an appropriate accessory use to the existing cemetery.

Conditional Use Permit

In order to approve a request for a Conditional Use Permit, the Commission must find that the "establishment, maintenance, or operation of the use or building applied for will or will not, under the circumstances of the particular case, be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood of such proposed use, or be detrimental or injurious to property and improvements in the neighborhood, or to the general welfare of the City" (FMC section 17.60.040).

In evaluating the Conditional Use Permit, staff considered the proposed use and its compatibility with existing land uses in the area, as well as air quality impacts, cultural resources impacts, fire/life safety impacts, traffic/access/circulation/parking impacts, noise impacts, and visual impacts. Each of these subject matters are discussed in detail below and are further discussed in the Lakeside Memorial Lawn Crematorium Initial Study/Mitigated Negative Declaration (IS/MND) provided in Attachment 11.

Should the Commission grant the proposed Lakeside Memorial Lawn Crematorium Conditional Use Permit, the CUP would be subject to ongoing review by the Community Development Department to ensure that it does not result in any adverse impacts to the community. Pursuant to FMC Section 17.60.050, in any case where the conditions to the granting of a use permit have not been, or are not, complied with, the Historic District Commission may revoke the permit after a public hearing on the matter. In addition, if the Community Development Director finds evidence that conditions of approval for the

Lakeside Memorial Lawn Crematory business have not been fulfilled or that the use has resulted in a substantial adverse effect on the health, and/or general welfare of users of adjacent or proximate property, or have a substantial adverse impact on public facilities or services or the general welfare of the City, the Director will refer the use permit to the Historic District Commission for review. If, upon such review, the Historic District Commission finds that any of the above-stated results have occurred, the Commission may modify or revoke the Conditional Use Permit following a hearing on the matter. Condition No. 4 is included to reflect this requirement.

Air Quality

As part of the CEQA documentation for the proposed project, an Air Quality and Greenhouse Gas Assessment was prepared by HELIX Environmental Planning, Inc., and is included in Attachment 11. Two daily cremations were analyzed under the original environmental document, an Initial Study/Mitigated Negative Declaration (IS/MND) and the applicant later asked that the analysis show impacts for four cremations per day. As a result, HELIX also prepared an Addendum to the Folsom Lakeside Crematorium Project Air Quality and Greenhouse Gas Emissions Assessment, included in Appendix B of the IS/MND, that provides additional analysis regarding the air quality and greenhouse gas emissions assessment based on additional daily cremations. This analysis was incorporated into the IS/MND as well.

As a part of its environmental review, HELIX analyzed whether the proposed project will have or will potentially have a significant adverse impact on the environment, either individually or cumulatively with other projects. With respect to air quality, HELIX used the standard Environmental Initial Study Checklist to consider four main questions:

- a. Would the project conflict with or obstruct implementation of the applicable air quality plan?
- b. Would the project result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?
- c. Would the project expose sensitive receptors to substantial pollutant concentrations?
- d. Would the project result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

Ultimately, Helix determined that the proposed project would have no impact with respect to question b and would have a less than significant impact with respect to questions a, c, and d. The following sections provide an overview of the analysis related to sensitive receptors, criteria pollutants, toxic air contaminants and odors as they relate to both construction and operation of the proposed project.

Sensitive Receptors

In general, some land uses are considered more sensitive to air pollution than others due to the types of population groups or activities involved and are referred to as sensitive receptors. Examples of these sensitive receptors are residences, schools, hospitals, and daycare centers.

The California Air Resources Board (CARB) and the Office of Environmental Health Hazard Assessment (OEHHA) have identified the following groups of individuals as the most likely to be affected by air pollution: people over 65 years old, children under 14 years old, infants (including in utero in the third trimester of pregnancy), and persons with cardiovascular and chronic respiratory diseases such as asthma, emphysema, and bronchitis.

Residential areas are considered sensitive receptors to air pollution because residents (including children and people over 65) tend to be at home for extended periods of time, resulting in sustained exposure to any pollutants present. Children and infants are considered more susceptible to health effects of air pollution due to their immature immune systems, developing organs, and higher breathing rates. As such, schools are also considered sensitive receptors, as children are present for extended durations and engage in regular outdoor activities.

The closest existing sensitive receptors to the project site are multiple single-family residences adjacent to the cemetery to the north, between 450 and 750 feet from the proposed crematory location, and mobile homes across Folsom Boulevard to the east, approximately 700 feet from the proposed crematory location. The closest schools to the project site are the Folsom Montessori School approximately 3,200 feet (0.6 miles) to the northeast and the Golden Valley Charter River School. That school is located across Lake Natoma from the project site and, though it is approximately 2.7 miles away by car, its physical location is about 3,000 feet (just over one-half mile) northwest of the project site. The nearest daycare is located 1,800 feet (0.3 miles) from the project site and the nearest hospital is located over 9,000 feet (1.8 miles) from the project site

Criteria Pollutants

Ambient air quality is described in terms of compliance with state and national standards, and the levels of air pollutant concentrations considered safe, to protect the public health and welfare. These standards are designed to protect people most sensitive to respiratory distress, such as asthmatics, the elderly, very young children, people already weakened by other disease or illness, and persons engaged in strenuous work or exercise. The U.S. Environmental Protection Agency (USEPA), the federal agency that administrates the Federal Clean Air Act of 1970, as amended in 1990, has established national ambient air quality standards (NAAQS) for several air pollution constituents known as criteria pollutants, including: ozone (O₃); carbon monoxide (CO); coarse particulate matter (PM₁₀; particles 10 microns or less) and fine particulate matter (PM_{2.5}; particles 2.5 microns or less); sulfur dioxide (SO₂); and lead (Pb).

As permitted by the Clean Air Act, California has adopted the more stringent California ambient air quality standards (CAAQS) and expanded the number of regulated air constituents. Ground-level ozone is not emitted directly into the environment but is generated from complex chemical and photochemical reactions between precursor pollutants, primarily reactive organic gases (ROGs; also known as volatile organic compounds [VOC]), and oxides of nitrogen (NO_x). PM₁₀ and PM_{2.5} are generated from a variety of sources, including road dust, diesel exhaust, fuel combustion, tire and brake wear, construction operations and windblown dust. In addition, PM₁₀ and PM_{2.5} can also be formed through chemical and photochemical reactions of precursor pollutants in the atmosphere.

CARB is required to designate areas of the state as attainment, nonattainment, or unclassified for the ambient air quality standards. An "attainment" designation for an area signifies that pollutant concentrations do not violate the standard for that pollutant in that area. A "nonattainment" designation indicates that a pollutant concentration violated the standard at least once. An "unclassified" designation indicates that insufficient data was available to determine the status. Sacramento County is designated as being in nonattainment for the state and federal ozone standards, the state PM₁₀ standards, and the federal PM_{2.5} standards.

The Sacramento Metropolitan Air Quality Management District (SMAQMD) is responsible for implementing emissions standards and other requirements of federal and state laws in Sacramento County. Attainment plans for meeting the federal air quality standards are incorporated into the State Implementation Plan (SIP), which is subsequently submitted to the USEPA.

The current air quality plan applicable to the project, the *Sacramento Regional 2008 NAAQS 8-Hour Ozone Attainment and Reasonable Further Progress Plan* (Regional Ozone Plan), was developed by the SMAQMD and adjacent air districts to describe how the air districts in and near the Sacramento metropolitan area will continue the progress toward attaining state and national ozone air quality standards

With respect to criteria pollutants, HELIX looked at whether the proposed project would conflict with or obstruct implementation of the Regional Ozone Plan or the SIP, either during construction or operation of the proposed project. HELIX also analyzed whether the proposed project would result in a cumulatively considerable net increase of any criteria pollutant for which the region is in non-attainment under applicable California Ambient Air Quality Standards. Highlights from that analysis are included in the following paragraphs. The full analysis is contained in Attachment 11. Furthermore, Condition No. 16, requires compliance with the Regional Ozone Plan.

Construction (Short-Term) Emissions

Construction of the project would involve the use of a crane for several hours to unload the chiller and crematory from the truck, and the use of a mini excavator or skid steer loader for one day and one truck load of concrete to install a small pad for the two propane

tanks.

Typically, projects that are 35 acres or less in size will not exceed the SMAQMD's construction NOx or PM thresholds of significance. Even so, all construction projects are required to implement the SMAQMD's Basic Construction Emission Control Practices (also known as Best Management Practices [BMPs]). The BMPs satisfy the requirements of SMAQMD's Rule 403, *Fugitive Dust*, which requires every reasonable precaution not to cause or allow the emissions of fugitive dust from being airborne beyond the property line from which the emission originates.

Reactive Organic Gas (ROG) emissions during construction are generally associated with the application of architectural coatings. The project does not propose any new structures, would not require substantial amounts of painting, and would not result in significant emissions of ROGs.

If approved, the proposed project will be required to implement SMAQMD's BMPs and it will not result in significant emissions of ROGs. Therefore, construction of the project would not result in a cumulatively considerable net increase of any criteria pollutant and the proposed project's impact on implementation of the Regional Ozone Plan would be less than significant with implementation of Condition No. 16, which requires implementation of the SMAQMD's BMPs.

Operation (Long-Term) Emissions

The project would result in long-term operational emissions from two sources: (1) vehicles that drive to and from the project and (2) from operation of the crematory.

With respect to emissions from vehicles, because there are no crematories currently operating in Folsom, demand for cremation services is filled by transporting the deceased to facilities outside of the City. Therefore, operations of the project would not result in new vehicle trips (nor the associated emissions in the region). Instead, the project would replace existing regional vehicle trips with shorter trips (and reduced associated emissions).

Operation of a propane-fired crematory would be considered a new stationary source of emissions. The project is subject to SMAQMD's Rule 201, *General Permit Requirements*, and Rule 202, *New Source Review*. Pursuant to these Rules, the project would be required to implement best available control technology (BACT) for the minimization of emissions.

BACT for crematories is incorporated into the product design in the form of controls which ensure maintenance of the correct temperatures and cycle times, and a secondary combustion chamber which ensures oxygenation and complete combustions of all fuels.

As shown in Table 4 of the IS/MND included in Attachment 11 and Table 1 of the Air Quality Addendum memo included in Appendix B of the IS/MND, the project's operational

emissions of criteria pollutants and precursors would not exceed the SMAQMD daily or annual thresholds. Therefore, the project's operational emissions would not result in a cumulatively considerable net increase of any criteria pollutant and impacts from operation of the crematory on implementation of the Regional Ozone Plan would be less than significant.

Toxic Air Contaminants

Toxic air contaminants (TACs) are a diverse group of air pollutants that may cause or contribute to an increase in deaths or in serious illness or that may pose a present or potential hazard to human health. TACs can cause long-term chronic health effects such as cancer, birth defects, neurological damage, asthma, bronchitis, or genetic damage, or short-term acute effects such as eye watering, respiratory irritation (a cough), runny nose, throat pain, and headaches. TACs are considered either carcinogenic or noncarcinogenic based on the nature of the health effects associated with exposure to the pollutant. For carcinogenic TACs, there is no level of exposure that is considered safe and impacts are evaluated in terms of overall relative risk expressed as excess cancer cases per one million exposed individuals. Noncarcinogenic TACs differ in that there is generally assumed to be a safe level of exposure below which no negative health impact is believed to occur. These levels are determined on a pollutant-by-pollutant basis.

Crematories are a potential source of TACs as a result of trace metals and organic compounds that accumulate in the body throughout a person's life and are released during combustion of human remains, and as a result of trace organic compounds that are formed in the combustion process. These TACs include: metals and inorganics (i.e., arsenic, beryllium, cadmium, chromium, copper, hydrogen fluoride, lead, mercury, nickel, selenium, zinc); VOCs (i.e., benzene, toluene, xylenes, vinyl chloride); aldehydes (i.e., acetaldehyde, formaldehyde); polyaromatic hydrocarbons (PAHs); polychlorinated dibenzodioxins (dioxins; PCDD); and polychlorinated dibenzofurans (furans; PCD). Prolonged exposure to significant concentrations of these TACS can result in a variety of adverse health effects including cancers, chronic conditions, and/or acute conditions, depending on the substance and level of exposure. Based on the results of the Health Risk Assessment (HRA) included in Attachment 11, hexavalent chromium and mercury are the primary drivers of the health risks from crematory emissions because the health risks from crematory emissions of these substances are one or more orders of magnitude greater than the health risks from other TACs in crematory emissions.

Health risks associated with cancer from development projects are estimated using the incremental excess cancer risk expressed as cancer cases per one million exposed individuals. The incremental excess cancer risk is an estimate of the chance a person exposed to specific sources of a TACs may have of developing cancer from that exposure beyond the individual's risk of developing cancer from existing background levels of pollutants in the ambient air. For context, the average cancer risk from TACs in the ambient air for an individual living in an urban area of California is 830 in 1 million. Cancer risk estimates do not mean, and should not be interpreted to mean, that a person will develop cancer from estimated exposures to toxic air pollutants.

The maximum estimated community incremental excess cancer, chronic and acute health risks due to exposure to the project TAC emissions from long term operation of the proposed crematory are presented in Table 5 of the attached IS/MND. These estimates are conservative (health protective) and assume that the resident or worker is outdoors for the entire exposure period. This table shows that the maximum incremental increased cancer risks and maximum non-cancer chronic and acute hazard index due to exposure to TACs from long-term operation of the proposed crematory would not exceed the SMAQMD thresholds. Therefore, operation of the project would not result in the exposure of sensitive receptors to substantial TAC concentrations and the impact would be less than significant.

The Addendum to the IS/MND clarifies that regardless of whether the applicant performs two or four cremations daily, the Health Risk Assessment remains accurate, as it was based on the maximum hourly emissions rate and the average annual emissions generated by project operations, not the number of cremations performed daily. The hourly cremation rate would not change because there is only one cremation machine proposed as a part of this project. The annual emissions generated by the project also would not change with an increase from two to four cremations per day because the applicant has not requested modification to their request for up to 500 cremations per year. To ensure that the numbers in the HRA remain accurate, staff has added Condition No. 13 stating that no more than four cremations may be performed each and that no more than 500 cremations may be performed each year.

In addition, the proposed crematory is considered a combustion source requiring an authority to construct permit to operate (A/C, PTO) from SMAQMD before it can be installed and operated. Staff has provided Condition No. 16, which states that a building permit will not be issued until an A/C, PTO has been obtained from SMAQMD. SMAQMD has stated that the applicant will be required to provide analysis to ensure the crematory will meet all air district rules/regulations and significance thresholds during the air district's A/C, PTO application and review process. In the event the air district's refined health risk assessment analysis shows that cancer risk or hazard index numbers exceed the significance thresholds noted, further restrictions to the crematorium would be required by SMAQMD before they would issue the A/C, PTO. As noted above, Condition No. 16 requires the proposed project to obtain an A/C, PTO from the air district before the City will issue a building permit, so neither construction nor operation of the proposed project can occur without the A/C, PTO.

Odors

The IS/MND prepared for the proposed project found that diesel equipment could generate diesel exhaust odors during construction activities. The generation of odors during the construction period would be temporary, intermittent, and dispersed within a short distance from the active work area. Once operational, potential odors from human remains prior to cremation would be minimized either by immediately processing remains

or by temporarily storing remains in the proposed refrigeration chiller. Operation of the crematory would not be a significant source odors or other emissions due to the BACT features of the crematory, including process temperature and cycle time controls, as well as secondary combustion chambers which ensure the complete combustion of all solids, liquids, and gaseous fuels. Therefore, the project would not result in other emissions (such as those leading to odors).

Cultural, Archaeological, Paleontological and Historical/Tribal Resources

Lakeside Memorial Lawn cemetery is Folsom's only active historic cemetery. It represents a combination of several old Folsom cemeteries, including the Masonic, Odd Fellows, Jewish, Citizen's, and Cook's cemeteries,

The existing metal structure was built in the 1990s and is located in a previously disturbed maintenance area. The project area itself is surrounded on the west and south by dredge mining tailings and the parcel is situated along a perennial waterway. All mining operations in the immediate vicinity of the project site had ceased by 1962. Based on a Cultural Resources Investigation performed by PAR Environmental for the mausoleum project for Lakeside Memorial Lawn in 1995, these tailings were determined not eligible for inclusion in the National Register of Historic Places by the Office of Historic Preservation. The report also states that the tailings do not meet California Environmental Quality Act guidelines as an important resource and do not qualify for inclusion in the California Register of Historic Places.

ECORP Consulting prepared a Cultural Resources Inventory Report of the proposed project that included a record search, Native American tribal consultation and a pedestrian survey. The results of the ECORP record search for cultural resources and Native American Heritage Commission Sacred Lands File Coordination search failed to reveal the presence of Native American cultural resources in the project area.

ECORP's pedestrian survey revealed no precontact or historic resources. The project site itself is not in an area otherwise suspected to contain unknown archaeological resources. The site survey and surveys of written records, historical maps and photographs, and outreach to groups with knowledge of the area's history all suggest that no known or previously unknown archaeological resources would be encountered or disturbed during construction. Ground disturbing activity would be limited to shallow ground clearing and site prep for the installation of a concrete pad to support two propane tanks. Still, the potential exists for inadvertent discovery of archaeological resources during project construction. The implementation of standard archaeological resource construction conditions (Condition 18-21) would ensure that potential impacts would be less than significant.

Though the project site is located on the property of the Lakeside Memorial Lawn Cemetery, no human remains are known to exist in the immediate vicinity of the project site. No evidence of potential human remains outside of marked graves was found in the project area during the cultural resources site survey by ECORP's archaeologist. Ground

disturbing activity would be limited to shallow ground clearing and site prep for the installation of a concrete pad to support two propane tanks. However, there is always the possibility that subsurface construction activities associated with the proposed project, specifically the preparation of the site for the small concrete pad, could potentially damage or destroy previously undiscovered human remains. However, if human remains were discovered, implementation of Condition No. 20 would reduce this potential impact to a less than significant level.

The ethnographic information reviewed for the project, including ethnographic maps, does not identify any villages, occupational areas, or resource procurement locations in or around the current project area. In addition, the Sacred Lands File failed to identify any sacred lands or tribal resources in or near the project area. The cultural resources survey did not reveal any Native American archaeological sites within or adjacent to the proposed project area. As stated in the attached IS/MND document, the City performed AB-52 consultation with three local Native American tribes that have previously submitted general request letters: Wilton Rancheria, the Lone Board of Miwok Indians and the United Auburn Indian Community. The purpose of consultation is to identify Tribal Cultural Resources (TCR) that may be significantly impacted by the proposed project and to allow the City to avoid or mitigate significant impacts prior to project approval and implementation. No specific TCRs were identified on the project site as a result of this consultation. In case of unanticipated discovery of tribal cultural resources, staff has incorporated Condition No. 21 (as described above). Implementation of this condition would ensure that there would not be a significant impact on tribal cultural resources.

Fire/Life Safety

The proposed project proposes to locate two 250-gallon propane tanks immediately adjacent to the existing metal structure in which the crematorium is proposed. Propane is considered a hazardous material in that is extremely flammable and may cause burns, irritations, and/or asphyxiation if humans come into direct contact with significant quantities of it. Federal and state laws include provisions for the safe handling of hazardous substances. The federal Occupational Safety and Health Administration (OSHA) administers requirements to ensure worker safety. Construction activity must also be in compliance with California OSHA regulations. Further, significant damage to the tanks, failure of safety mechanisms, and/or the presence of an ignition source may make the tanks an explosion hazard. However, the tanks would be secured in place on a concrete pad, marked conspicuously, and placed in an area at low risk of impact from any vehicle or piece of equipment. They would not be located in an area of the cemetery frequented by the public. All installation, maintenance, and operations would be done by trained individuals in accordance with the manufacturer's recommendations and state regulations. The tanks would be regularly inspected to ensure soundness and proper function. Delivery of propane and filling of the tanks would be done only by licensed professionals following all applicable regulations and best practices. Furthermore, the Folsom Fire Department has reviewed the proposed site plan and has provided Condition No. 26 which requires the propane tanks to be a minimum of 10 feet from the metal structure in which the cremations would occur.

Fire Station 35 is the nearest station to the project site and is located approximately 1.5 miles east of the project site. The project site is easily accessible to fire service personnel. Consistent with the City's Multi-Hazard Emergency Management Plan, the City of Folsom maintains pre-designated emergency evacuation routes along major streets and thoroughfares. The proposed project would not modify any pre-designated emergency evacuation route or preclude their continued use as an emergency evacuation route. Emergency vehicle access would be maintained throughout the project site to meet the Fire Department standards for fire engine maneuvering, location of fire engine to fight a fire, rescue access to the units, and fire hose access to all sides of the building.

The project is not located in or near a State Responsibility Area or in a Very High Fire Hazard Severity Zone. Vegetation on the property is irrigated and includes maintained lawns and well-spaced trees with a generally open canopy and limbs pruned near ground level. The project site is located in an urbanized area in the City of Folsom and is provided urban levels of fire protection by the City. Landscaping on the property is well-irrigated, well-spaced, trimmed, pruned, and generally maintained. To the north of the site is a residential neighborhood, to the east is gravel/rock cover and Folsom Boulevard, and to the south and west is a greenbelt that runs parallel to Lake Natoma on a north/south axis. The natural spaces are small, with a relatively open and discontinuous canopy. The project is not likely to cause any ignition, given that the crematory will not emit sparks, and any ignition caused by other factors could be quickly controlled by the City of Folsom Fire Department and would not spread great distances given the land use and vegetation surrounding and occupying the site.

As an existing facility, Lakeside Memorial Lawn maintains adequate fire response infrastructure for both current operations and the proposed project. The City of Folsom Fire Department reviewed the project application and did not raise any concerns regarding the adequacy of water supply or site access. The Fire Department also provided Condition No. 24, which requires a fire alarm/detection system if sprinklers are not provided, Condition No. 25, which requires on-site fire hydrants and mains capable of supplying the water flow for fires and Condition No. 26, which requires that combustible materials, trash, weeds and brush are not stored or located within 10 feet of the propane tanks. Furthermore, Condition No. 27 states that a trained operator must be present at all times that the crematorium machine is in operation. Therefore, with these conditions, the proposed project would not expose people or structures to a significant risk of loss due to wildland fires.

Traffic, Access, Circulation and Parking

The project site can be accessed from either northbound or southbound on Folsom Boulevard or from westbound Natoma Street approaching this intersection with Folsom Boulevard. The site would be accessed by proceeding a very short distance west on Forrest Street to reach the entry gate to the cemetery and turning left onto Mormon Street.

Diagonal parking spaces can be found along both sides of Mormon Street. At the terminus

of Mormon Street, members of the public may continue straight onto a main cemetery access road to find an additional parking lot. All existing parking spaces would be maintained. Access to the project site directly would continue to be provided by a smaller existing access road located at the southwestern terminus of Mormon Street. Both the smaller access road and the main access road can be reached at the terminus of Mormon Street, but the two roads do not form a continuous loop because of a fence line separating them. No new parking spaces or parking facilities would be constructed.

As the crematorium would not be located in or near a funeral home and would be separate from any funeral services or public gatherings provided by the project applicant, access would only need to accommodate a small number of people at the site. There are a total of 92 parking spaces in the cemetery with room for 23 additional parking spaces during large events. The applicant has seen an average of 12 to 15 visitors to the cemetery on an average day. The maximum number of visitors under the proposed project conditions would be during large funeral services at the site. However, the 921 parking spaces and overflow parking available on-site are designed accommodate such services.

The City of Folsom identifies most major streets in the City as emergency evacuation routes. The proposed project would not modify any major street and/or preclude their continued use as an emergency evacuation route. The project does not propose any alterations to any access roads, rights-of-way or other routes, and would not increase traffic to a point that additional risk on existing routes would be incurred. Emergency vehicle access would be maintained throughout the project site to meet the Fire Department standards for fire engine maneuvering, location of fire engine to fight a fire, rescue access, and fire hose access to all sides of the building.

Noise

Development of the crematory facility will temporarily increase noise levels in the project vicinity during the construction period. Construction activities, including site work and construction, would be considered an intermittent noise impact throughout the construction period of the project. The City's Noise Ordinance excludes construction activities from meeting the General Plan Noise Element standards, provided that all phases of construction are limited to the hours between 7:00 a.m. and 6:00 p.m. on weekdays, and between 8:00 a.m. and 5:00 p.m. on Saturdays. To ensure compliance with the City's Noise Control Ordinance and General Plan Noise Element, staff recommends that hours of construction operation be limited from 7:00 a.m. to 6:00 p.m. on weekdays and 8:00 a.m. to 5:00 p.m. on Saturdays with no construction permitted on Sundays or holidays. Condition No. 12 is included to reflect these requirements.

Visual Impacts

The project site is currently a small cemetery, with associated landscaping, outbuildings, and access roads. Lands to the south and west contain woodland habitat typical of riparian communities in the Sierra Nevada foothills. Soils at the project site are comprised of dredge tailings and other fill material. Tailing piles between the site and Folsom Boulevard prevent the site from being visible from that street. To the west is also the

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February 16, 2022

Jedidiah Smith Memorial Trail that runs along the eastern shore of Lake Natoma. To the north is a small residential neighborhood with single family dwellings (The Preserve/Lake Natoma Shores). Folsom Boulevard runs in a north/south line just east of the property. East of Folsom Boulevard is a large, developed area containing single family homes, apartment complexes, a mobile home park, and some small businesses. The more regional setting is primarily characterized by residential development with a commercial shopping center to the east.

The project would be located in an existing metal structure on the grounds of the Lakeside Memorial Lawn Cemetery. The structure would be modified with an exhaust stack added to the roof to accommodate the crematorium, but its footprint would not be expanded. The height of the stack is determined during the SMAQMD permitting process in order to meet their air quality standards. Based on initial consultation with SMAQMD, the stack would most likely be located approximately 19.5 feet above grade and would project up to 10 feet above the existing roof of the structure. The crematory would be placed in the northwest corner of the structure. Two 250-gallon propane tanks would be installed on a proposed concrete pad along the northern side of the structure to provide power for the crematorium, as no gas lines currently exist. An existing wooden fence would shield these tanks from view from the publicly used areas of the cemetery.

Existing landscaping at the cemetery and around the metal structure consists of mature broad-leaved, coniferous, and palm trees. These trees also provide shade for much of the cemetery and many of the parking spaces. An irrigated lawn surrounds the existing cemetery plots and a smaller lawn surrounds the rear of the structure (i.e., the non-service entrance side). Native oak/gray pine woodland habitat exists surrounding the cemetery. No new landscaping installation or modification is proposed. Native habitat in the vicinity of the project would not be disturbed.

An existing brick and wrought iron fence marks the edge of the cemetery from the Forrest Street side, but does not extend the length of Mormon Street. A wooden fence currently separates the front side of the metal structure (facing the access road) from the back side and extends both north and south of the structure. The fencing south of the structure further extends to block the access road and restrict access between the lawn to the west of the structure and the access road to the east of the structure. No changes to fencing are proposed as part of this action. The access side of the structure that would be used for crematory operations is shielded from view from the rest of the cemetery by wooden fencing. A gravel berm shields views of the structure from the east, including from Folsom Boulevard.

Neither the project site nor the surrounding areas are scenic vistas due to the presence of existing nearby commercial and residential developments. Further, neither the project site, nor views to or from the project site, have been designated as important scenic resources by the City of Folsom or any other public agency. Additionally, the site of proposed modifications is already largely shielded from public view and would remain so. Images of the metal structure taken from the edge of the cemetery adjacent to the Lake

Natoma Shores/Preserve subdivision are provided in Attachment 10. All human remains are transported to or from the crematorium fully shielding the deceased from public view with fully shielded gurneys. The vehicles back up inside the structures' doors to unload the gurney, similar to what is done at the applicant's Miller Funeral Home on the corner of Scott Street and Natoma Street. Given that external modifications would be very minor and that the building is already mostly shielded from view, staff does not anticipate a significant visual impact.

Land Use Compatibility/Site Considerations

The project site consists of a small cemetery, with associated landscaping, outbuildings, and access roads. Lands to the south and west contain woodland habitat. Tailing piles between the site and Folsom Boulevard prevent the site from being visible from that street. To the west is also the Jedidiah Smith Memorial Trail, which is part of the American River Parkway. To the north of the project site is a residential neighborhood with single-family dwellings (Lake Natoma Shores/The Preserve). Folsom Boulevard runs in a north/south line just east of the property. East of Folsom Boulevard is a large, developed area containing single-family homes, apartment complexes, a mobile home park, and some small businesses. The more regional setting is primarily characterized by residential development with a commercial shopping center to the east.

Staff researched data available from the Sacramento Air Quality Management District regarding the locations of existing crematoriums within the county, their proximity to residential uses, complaints received by SMAQMD for these facilities, and actions taken against these facilities by SMAQMD. Staff found that 16 crematoriums are currently operating within Sacramento County, of which five were located within cemeteries (East Lawn Memorial Park, Sacramento Memorial Lawn, St. Mary Cemetery and Funeral Center, and Sunset Lawn Chapel of the Chimes in Sacramento and Mount Vernon Memorial Park & Mortuary in Fair Oaks). Nine of the crematoriums were within 1,100 feet of residential uses, as shown in the table on the following page. Images of these facilities are provided in Attachment 12. Note that the closest residence to the proposed crematorium is 450 feet from the building.

TABLE 1: CREMATORIUMS IN SACRAMENTO COUNTY LOCATED WITHIN 1,100 FEET OF RESIDENTIAL LAND USES

Name of Facility	Address	Distance to Residential Uses
East Lawn Memorial Park	4300 Folsom Blvd. Sacramento	525 ft.
Sacramento Memorial Lawn	6100 Stockton Blvd. Sacramento	1,100 ft.
St. Mary Cemetery and Funeral Center	6509 Fruitridge Rd. Sacramento	275 ft.
Sunset Lawn Chapel of the Chimes	4701 Marysville Rd. Sacramento	600 ft.
Mount Vernon Memorial Park & Mortuary	8201 Greenback Ln. Fair Oaks	175 ft.
Evergreen Memorial	3030 Fruitridge Rd. Sacramento	90 ft.
North Sacramento Funeral Home	725 El Camino Ave. Sacramento	100 ft.
Pet Loss Services of North America	4601 Pell Dr. Sacramento	650 ft.
Sierra View Funeral Chapel & Crematory	6201 Fair Oaks Blvd. Carmichael	150 ft.

All crematoriums in Sacramento County are subject to additional permitting and regular inspections by SMAQMD, who investigates all public complaints about the facilities and fines all facilities for any violations. Three of the facilities listed in the above table have been issued violations from SMAQMD, for a total of 17 violations. Of these violations, four were discharge related. The last discharge-related violation issued by SMAQMD at one of these facilities was in 2010. Five of the facilities listed above have been the subject of public complaints. SMAQMD has received a total of 26 public complaints regarding five of these facilities since 1992. Of these complaints, eight have been received since 2010, predominantly regarding odor and smoke. SMAQMD requires recordkeeping and reporting of all cremations and has the right to inspect cremation facilities at any time.

The applicant has also stated that they currently operate two crematoriums in the region, the Lakeside Colonial Chapel in Marysville and the Chapel of the Twin Cities in Yuba City, both directly adjacent to residential uses. Given that the proposed project is located 450 to 750 feet away from the nearest sensitive receptors (residences), this is consistent with the location of other crematoriums located in residential areas of Sacramento County as well as the regional crematoriums operated by the applicant.

Based on the analysis presented in the attached staff report and attached IS/MND, staff concluded that there would be a less than significant impact to surrounding land uses with regards to air quality, fire/life safety, traffic, noise and aesthetics. As conditioned, the

crematorium would be subject to stringent air quality permitting and monitoring from SMAQMD. The crematorium is also subject to an annual unannounced inspection from the California Cemetery and Funeral Board. Furthermore, staff has provided Condition No. 10 which states that any intensification or expansion of the crematorium will require a subsequent Conditional Use Permit by the Historic District Commission and Condition No. 13 which states that no expansion in number of cremations per day or per year beyond what has been conditioned shall be permitted without prior approval being obtained from the Historic District Commission through a Conditional Use Permit. Any increase in air quality or noise impacts beyond what is described in this report is subject to either permit revocation or modification by the commission.

With these conditions in place, staff has concluded that the establishment, maintenance, and operation of the proposed use will be compatible with surrounding land uses and will not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood or be detrimental or injurious to property or improvements in the neighborhood, or to the general welfare of the city.

Public Outreach, Noticing and Comments

Staff routed the initial development application to relevant local and state agencies. Based on comments received, staff has coordinated with SMAQMD to appropriately condition the project and ensure that the applicant goes through the required SMAQMD permitting process. No other public agencies provided comments. The application was also routed to the Sacramento County Cemetery Advisory Commission, the California Cemetery and Funeral Bureau and the Cremation Association of Northern California. Staff received no comments from these organizations.

A public notice was placed in the Folsom Telegraph on January 6, 2022, and notices were mailed to all property owners located within 300 feet of the project site and also to residents who filed written requests for notices, consistent with Government Code sections 65905, 65091, and 65094, as well as CEQA Guidelines sections 15072 and 15105. A public notice was also placed on the project site 30 days prior to the public hearing.

Staff received numerous public comments regarding the project in general and also the IS/MND prior to publication of this staff report. Most comments received were from residents of the Historic District (especially residents of the Lake Natoma Shores/Preserve subdivision) and the Chinese Historical Society of America in opposition to the project, with one letter from a resident expressing support for the project. Written comments received prior to publication of the staff report are included in Attachment 13. Furthermore, the applicant held a phone meeting for nearby residents to listen to their concerns about the proposed project. Issues brought up in these comment letters and during the applicant meeting include:

Air Quality Concerns

- Proximity to residential uses, places where children recreate and impacts to those with compromised lungs and other at-risk populations
- Air quality concerns related to particulate matter, CO₂, TACs, NO_x, and cremated bodies containing mercury from fillings, pacemakers, chemotherapy chemicals and radiation and SMAQMD/Sacramento County's oversight of these pollutants
- Lack of analysis regarding cumulative air quality effects of the crematory with car fumes on Folsom Blvd.
- Whether the crematory would be allowed to operate on days with poor air quality
- Whether wind would blow smoke into the adjacent neighborhoods
- Consistency with General Plan air quality goals, especially given that the property is in an existing non-attainment area
- Accuracy of meteorological weather data used
- Lack of methodology and citations used to justify statements in odor analysis
- Lack of analysis regarding the Schultz Eddy Phenomenon which causes air to be trapped in the Preserve neighborhood
- Need for additional air quality modelling due to varying air pollution patterns
- Optimal operation of crematory facilities vs. reality
- How filters inside smokestack are cleaned
- Lack of clarity regarding which agencies regulate and inspect crematories for air quality effects and how those agencies regulate emissions on days they are not testing them
- Potential odors during cremation activities, especially on evenings when Code Enforcement staff cannot be present
- Lack of maintenance cleanup procedures with regards to runoff, residue and dust
- Conclusions of the air quality analysis being related to an average healthy human rather than sensitive receptors such as children and the elderly
- Whether a rain cover is proposed on the exhaust stack

Traffic, Circulation, Parking, Wildfire and Life Safety Concerns

- Additional traffic in a busy neighborhood due to additional trips for witnessed cremations, propane delivery, and morgue delivery, both in the neighborhood and in the larger Historic District
- Amount of people allowed to attend a witnessed cremation
- Emergency access to the structure in which the cremations are proposed and propane tanks in case of fire
- Increased air quality concerns when cremating bodies with excess weight
- Evacuation of residents in case of fire on narrow streets accessing many existing uses and frequent light rail crossing
- Parking concerns during on-site funeral and witnessed cremation services
- Lack of analysis regarding cumulative traffic, circulation, and wildfire effects
- Whether a trained operator would always be on-site in case of malfunction
- Chances of a propane explosion near historic cemeteries, State parks, and residences
- Whether there is room in the metal structure in which the crematorium is proposed for both maintenance equipment and crematory equipment
- Insufficient fencing and security measures at the site
- Absence of design measures to create defensible space
- Consistency with the Folsom Community Wildfire Protection Plan
- Whether the recently constructed storage shed was taken into account in the emergency access analysis
- Greater evidence that the crematory machine will not produce particles that could ignite a fire
- Documented feedback on the project from the Fire Department and California State Parks

Quality of Life Concerns

- Whether property values would go down as a result of having to disclose that there is a crematory nearby
- Psychological effects of knowing that cremations are occurring close to residences
- Concerns over how much cremation noise would be discernable from the nearest residences and to visitors of the cemetery
- Whether the smoke stack would be visible from nearby streets and residential yards (and lack of clarity on the height of the stack)
- Whether bodies to be cremated would be transferred to the crematory facility in public view
- Impacts to those using nearby trails and recreational areas
- Whether there would be noise impacts when the crematory machine is running

Cultural, Archaeological, Paleontological and Historical/Tribal Resources Concerns

- Potential presence of archeological and paleontological resources on-site
- Potential smoke impacts to nearby Chinese cemetery, Veteran's Hall, Murer House, and State Recreation Area
- Consideration of the Chung Wah Cemetery being placed on the National Register and California Registry of Historic Places and Young Wo Chinese Cemetery being placed on the California Registry of Historic Points of Interest and both cemeteries being placed on the Sacramento County Cemetery Commission's Registry of Pioneer Cemeteries
- Cremation not being consistent with Chinese funerary customs and practice and not an integral part of Gold Rush pioneer cemetery customs
- The presence of the crematorium changing the character and historic significance of a pioneer cemetery
- Historical accuracy of a crematory chimney

Land Use Concerns

- How a crematory could be allowed in an Open Space zone
- Compatibility with the Open Space element of the General Plan
- The use being more appropriate for an industrial or commercial zone
- The M-2 zone being the only zone that calls out specific uses close to the crematory being allowed, such as pottery kilns and ceramic works
- Lack of compatibility with an historic use in the historic district
- Impact of people who own cemetery plots and niches in Lakeside Memorial Lawn near the proposed crematory

General and Miscellaneous Concerns

- Public noticing and agency noticing irregularities
- Whether the City Council should be hearing the project rather than the HDC
- Whether a design review application is warranted
- Public outreach from the applicant
- City delays in publishing documentation based on when application was submitted
- Legal standing of the IS/MND
- The Commission's standards of guidance to issue a CUP
- Number of cremations per day being proposed and allowed
- Issues with accuracy of applicant's provided scope of work
- Conflicting information about whether the crematorium will be accessible to members of the public or just employees
- Whether sewer or bathroom facilities are available at the cremation site

- The previous proposal for a crematory on this site being rejected/withdrawn
- Historical easements to the Chung Wah cemetery
- Location of property lines
- Lack of discussion of recommendations in the 2003 Crematory CUP
- Time limits and conditions on the 2003 Crematory CUP
- Operation of the crematorium changing people's perception of spiritual worship in the Chung Wah and Young Wo cemeteries due to ash and smoke
- Necessity of the project given other crematories within the general vicinity
- Applicant starting fencing work and flatwork prior to a decision being made on the CUP
- Future growth of the Corporation Yard land
- Whether there is a need for witnessed cremations
- Impacts related to potential increases in neighborhood resources
- If heat from the exhaust stack would impact special status bird and bat species nesting nearby
- The consideration of performing aquamation (liquid cremation) instead of standard cremations

Staff notes that the air quality, odor, traffic/circulation, parking, wildfire safety, cultural/archaeological, paleontological and historical/tribal resources, noise and land use concerns have been addressed in the analysis sections of this staff report and the attached IS/MND. Noticing has been completed pursuant to Government Code sections 65905, 65091, and 65094 as well as CEQA Guidelines sections 15072 and 15105, and the applicant has held a public outreach meeting as described above.

With regards to the previous proposal for a crematorium in 2003 that recommended denial, staff notes that the technology for crematorium machines has improved significantly since 2003 and based on the air quality assessment performed for the

project, staff concluded that the air quality impacts of the newly proposed crematory would not result in the same impacts as the crematorium proposed in 2003. Furthermore, the Sacramento County Cemetery Commission that expressed concern over the 2003 project did not comment on the current project as proposed.

Finally, staff has concluded that concerns regarding consistency with the General Plan Air Quality and Open Space Goals and Policies and the Historic District Design and Development Guidelines, concerns regarding Chinese and Gold Rush customs, and concerns over a drop in property values will be adequately alleviated by the filtering and burning technology of the proposed crematory machinery as well as the required SMAQMD permitting and monitoring, the shielding of the facility from public view, and the conditions imposed on the proposed project as a part of the Conditional Use Permit, as described in this staff report. Particularly significant to staff's analysis, as detailed above, are the following facts: (1) the crematorium would be subject to stringent air quality permitting and monitoring from SMAQMD; and (2) any impacts beyond those described in this report are not permitted and, if they occur, will subject the applicant to either permit revocation or modification by the Commission.

ENVIRONMENTAL REVIEW

An Initial Study and Mitigated Negative Declaration, and an associated Addendum, have been prepared for this project (Attachment 11). A detailed summary of the environmental analysis for this project is contained in other sections of this report, with a particular focus on Air Quality. A summary of the environmental analyses on the issues of cultural, archeological, paleontological, and historical/tribal resources; fire/life safety; traffic, access, circulation, and parking; noise; and visual impacts are also included above. Note that an addendum to the initial air quality analysis was also prepared, and has been included both in the final Air Quality section and Appendix B of the IS/MND.

A detailed description of the potential impacts is provided within the Initial Study for this project, which is included as Attachment 11 to this report. As described above, the Mitigated Negative Declaration includes mitigation measures which, when implemented, will reduce the identified impacts to a less than significant level. These mitigation measures have been included as conditions of approval for this project. Staff points to Conditions 18 through 21 as particularly significant in this regard.

To date, several public comments were received during the Mitigated Negative Declaration public review period which started on January 7, 2022 and ended on February 7, 2022. Those public comments have been included in Attachment 13. HELIX Environmental will provide responses to those comments for the Public Hearing and on the City's website at the following link: <https://www.folsom.ca.us/government/community-development/planning-services/current-project-information>

Pursuant to AB 52, before the release of the mitigated negative declaration for this project, the City began the process of consultation with any California Native American tribes traditionally and culturally affiliated with the geographic area of the proposed project. The consultation was concluded and no changes to the project were required as a result of the consultation process.

RECOMMENDATION

Based on the information included in this report, staff recommends finding that the proposed crematory is appropriate as an accessory use to the existing cemetery. Staff also recommends approval of a Conditional Use Permit for the proposed project, based on the information included in this report and the environmental documents in Attachment 11.

HISTORIC DISTRICT COMMISSION ACTION

MOVE TO ADOPT THE MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING AND REPORTING PROGRAM FOR THE LAKESIDE MEMORIAL LAWN CREMATORIUM, PER ATTACHMENT 11;

AND

MOVE TO FIND THAT THE PROPOSED CREMATORIUM IS APPROPRIATE AS AN ACCESSORY USE TO THE EXISTING CEMETERY;

AND

MOVE TO APPROVE THE CONDITIONAL USE PERMIT (PN 19-182) FOR OPERATION OF A CREMATORIUM WITHIN AN EXISTING 1,071-SQUARE FOOT METAL STRUCTURE LOCATED AT 1201 FORREST STREET, WITHIN THE LAKESIDE MEMORIAL LAWN CEMETERY AS ILLUSTRATED IN ATTACHMENTS 5 and 6, WITH THE FOLLOWING FINDINGS (A-K) AND CONDITIONS (NOS. 1-30):

GENERAL FINDINGS

- A. NOTICE OF HEARING HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE PROJECT IS CONSISTENT WITH THE GENERAL PLAN AND THE ZONING CODE OF THE CITY.

CEQA FINDINGS

- C. A MITIGATED NEGATIVE DECLARATION HAS BEEN PREPARED FOR THE PROJECT IN ACCORDANCE WITH CEQA.
- D. PURSUANT TO AB 52, BEFORE RELEASE OF THE MITIGATED NEGATIVE DECLARATION FOR THIS PROJECT, THE CITY CONTACTED ALL CALIFORNIA NATIVE AMERICAN TRIBES ON THE CITY'S AB 52 CONTACT LIST IN ASSOCIATION WITH THIS PROJECT.
- E. THE CITY RECEIVED ONE REQUEST FOR CONSULTATION FROM CALIFORNIA NATIVE AMERICAN TRIBES AND CONSULTATION WAS SUBSEQUENTLY CONCLUDED. NO CHANGES TO THE PROJECT WERE REQUIRED AS A RESULT OF THE CONSULTATION.
- F. THE HISTORIC DISTRICT COMMISSION HAS CONSIDERED THE INITIAL STUDY, THE PROPOSED MITIGATED NEGATIVE DECLARATION AND ALL COMMENTS RECEIVED DURING THE PUBLIC REVIEW PROCESS BEFORE MAKING A DECISION REGARDING THE PROJECT.
- G. THE MITIGATED NEGATIVE DECLARATION REFLECTS THE INDEPENDENT JUDGMENT AND ANALYSIS OF THE CITY OF FOLSOM.
- H. THE MITIGATED NEGATIVE DECLARATION HAS DETERMINED THAT THE PROPOSED PROJECT WOULD NOT HAVE A SIGNIFICANT EFFECT ON THE ENVIRONMENT WITH THE REQUIRED MITIGATION MEASURES AND THE CONDITIONS OF APPROVAL ON THE PROJECT.
- I. ON THE BASIS OF THE WHOLE RECORD, THERE IS NO SUBSTANTIAL EVIDENCE THAT THE PROJECT WILL HAVE A SIGNIFICANT EFFECT ON THE ENVIRONMENT WITH THE REQUIRED MITIGATION MEASURES AND THE CONDITIONS OF APPROVAL ON THE PROJECT.

LAND USE COMPATIBILITY FINDING

- J. AS CONDITIONED, THE PROPOSED CREMATORY IS APPROPRIATE AS AN ACCESSORY USE TO THE EXISTING CEMETERY.

CONDITIONAL USE PERMIT FINDING

- K. AS CONDITIONED, THE ESTABLISHMENT, MAINTENANCE AND OPERATION OF THE USE APPLIED FOR WILL NOT, UNDER THE CIRCUMSTANCES OF THIS PARTICULAR CASE, BE DETRIMENTAL TO THE HEALTH, SAFETY, PEACE, MORALS, COMFORT, AND GENERAL WELFARE OF PERSONS RESIDING OR WORKING IN THE NEIGHBORHOOD, OR BE DETRIMENTAL OR INJURIOUS TO PROPERTY AND IMPROVEMENTS IN THE

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NEIGHBORHOOD OR TO THE GENERAL WELFARE OF THE CITY, AS THE PROPOSED PROJECT WILL NOT HAVE NEGATIVE IMPACTS TO NEARBY COMMERCIAL OR RESIDENTIAL USES THAT HAVE NOT BEEN MITIGATED.

ATTACHMENT 2 BACKGROUND

BACKGROUND

The Planning Commission approved a Use Permit and Variance for Mausoleums at Lakeside Memorial Lawn in 1991 (PC91-042). An amendment to the approval was granted in 1995 (PC95-033). That approval allowed for the construction of twelve mausoleums. To date, two mausoleums have been built. In 2002, Lorin Claney applied for a Conditional Use Permit for a crematorium at the Lakeside Memorial Lawn (PN 02-258). Staff routed the project for public review and prepared a staff report recommending denial of the project in 2003, but the applicant withdrew the application before Historic District Commission review. An existing maintenance building (where the crematorium is proposed), approximately 1,071 square feet in area, is located along the south border of the cemetery. The design review for a second 1,600-square-foot maintenance shed was approved by the Historic District Commission in 2020 (PN 20-160) and was subsequently constructed. The site plan associated with this approval showing both structures is provided in Attachment 7.

The applicant for the crematorium proposed in this staff report initially proposed that it be located in the Miller Funeral Home located at 507 Scott Street. Staff expressed initial concern with the crematorium being located directly adjacent to residences. The applicant also coordinated with SMAQMD to determine what measures would need to be taken to locate the crematory adjacent to residences and ultimately decided not to move forward with that location. The applicant then proposed to modify the application for cremations at the Miller Funeral Home to instead perform alkaline hydrolysis (liquid cremations) that would not employ any burning or produce emissions but instead would produce a liquid effluent that would be flushed down sewage pipes. While this method of cremation was approved in California in 2017, City Environmental and Water Resources staff, in coordination with the Sacramento Regional County Sanitation District, did not have enough local examples of the impacts of these effluents to support this proposal. The applicant subsequently decided to move forward with a standard crematorium at the Lakeside Memorial Lawn. The proposed location in the existing metal structure is further away from sensitive receptors than the previous location at the Miller Funeral Home.

The front of the project site is mostly level with a slight to moderate downward slope towards the rear of the site. Lakeside Cemetery has a variety of mature deciduous and evergreen trees. The front of the cemetery, along Forrest Street, is bounded by a brick wall capped with wrought iron fencing. Photographs of the Lakeside Memorial Lawn are included here as Attachment 10. Both the Lakeside Cemetery (including the Odd Fellows and Mason's Cemeteries) and the adjacent Chung Wah Cemetery appear on the City of Folsom's Cultural Resources Inventory. The subject property is located in the Open Space/Public Primary Area of the Historic District, with an underlying zoning of OSC (Open Space and Conservation District).

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GENERAL PLAN DESIGNATION	OS, Open Space
ZONING	OS/P (Open Space/Public Primary Area), underlying zoning of OSC (Open Space and Conservation)
ADJACENT LAND USES/ZONING	<p>North: Lakeside Memorial Lawn Cemetery (OS/P) with single-family residential subdivision (Lake Natoma Shores/The Preserve) beyond (PRE)</p> <p>South: Vacant land with mine tailings (OSC)</p> <p>East: Vacant land with mine tailings (OSC) with Folsom Boulevard and Folsom Village Mobile Homes beyond (NRB)</p> <p>West: Lakeside Memorial Lawn Cemetery (OS/P) with Folsom Lake State Recreation Area (OSC) beyond</p>
SITE CHARACTERISTICS	The 12.02-acre project site contains mausoleums, gravesites, landscaping, a utility building, mine tailings and a variety of mature deciduous and evergreen trees. The project site is generally flat, ranging from about 175 to 185 feet above mean sea level. There are no wetlands, streams, or jurisdictional features located on the project site.
APPLICABLE CODES	<p><u>FMC</u> Chapter 15.52; HD, Historic District</p> <p><u>FMC</u> Section 17.52.550, Open Space/Public Primary Area Special Use and Design Standards</p> <p><u>FMC</u> Section 17.52.360, Conditional Use Permit Review</p> <p><u>FMC</u> Chapter 17.60, Use Permits</p>

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ATTACHMENT 3

Proposed Conditions of Approval

CONDITIONS OF APPROVAL FOR LAKESIDE MEMORIAL LAWN CREMATORIUM CONDITIONAL USE PERMIT (PN 19-182)				
Cond. No.	Mitigation Measure	GENERAL REQUIREMENTS	When Required	Responsible Department
1.		Issuance of a Building Permit is required. The applicant shall submit final site and building plans to the Community Development Department that substantially conform to the overall site plan, detailed site plan and floor plans dated February 19, 2020, included in Attachment 5 and the Outer Dimensions dated August 27, 2020, contained in Attachment 6. Implementation of this project shall be consistent with the above referenced items as modified by these conditions of approval.	B	CD (B)
2.		Compliance with all local, state and federal regulations pertaining to building construction and demolition is required.	OG	CD (B)
3.		This approval is for operation of a crematorium within an existing 1,071-square foot metal structure located at 1201 Forrest Street, within the Lakeside Memorial Lawn cemetery. The applicant shall submit building plans that substantially comply with this approval and the site plan and floor plan dated February 19, 2020, included in Attachment 5 and the Outer Dimensions dated August 27, 2020, contained in Attachment 6.	B	CD (P)
4.		If the Community Development Director finds evidence that conditions of approval for the Lakeside Memorial Lawn Crematorium Conditional Use Permit have not been fulfilled or complied with, that the use has resulted in a substantial adverse effect on the health, and/or general welfare of users of adjacent or proximate property, or has a substantial adverse impact on public facilities or services, the Director will refer the use permit to the Historic District Commission for review. If, upon such review, the Historic District Commission finds that any of the above-stated results have occurred, the Commission may modify or revoke the Conditional Use Permit.	OG	CD (P)

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5.		<p>This project approval shall remain in effect for one year until February 16, 2023. Failure to submit a complete application for the relevant building permits within this time period, without the subsequent extension of this approval, or failure to vest the Conditional Use Permit within the identified time frame prior to the expiration date and the applicant has not demonstrated substantial progress towards the development of the project, shall render this approval null and void. The owner/applicant may file an application with the Community Development Department for a permit extension not less than 30 days prior to the expiration date of the permit, along with appropriate fees and necessary submittal materials pursuant to Chapter 17.60 of the <u>Folsom Municipal Code</u>.</p>	B	CD (P, B)
6.		<p>The owner/applicant shall defend, indemnify, and hold harmless the City and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul any approval by the City or any of its agencies, departments, commissions, agents, officers, employees, or legislative body concerning the project. The City will promptly notify the owner/applicant of any such claim, action or proceeding, and will cooperate fully in the defense. The City may, within its unlimited discretion, participate in the defense of any such claim, action or proceeding if both of the following occur:</p> <ul style="list-style-type: none"> • The City bears its own attorney's fees and costs; and • The City defends the claim, action or proceeding in good faith <p>The owner/applicant shall not be required to pay or perform any settlement of such claim, action or proceeding unless the settlement is approved by the owner/applicant.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD
DEVELOPMENT COSTS AND FEE REQUIREMENTS				
7.		<p>The owner/applicant shall pay all applicable taxes, fees and charges at the rate and amount in effect at the time such taxes, fees and charges become due and payable.</p>	B	CD (P)(E)

8.		The City, at its sole discretion, may utilize the services of outside legal counsel to assist in the implementation of this project, including, but not limited to, drafting, reviewing and/or revising agreements and/or other documentation for the project. If the City utilizes the services of such outside legal counsel, the applicant shall reimburse the City for all outside legal fees and costs incurred by the City for such services. The applicant may be required, at the sole discretion of the City Attorney, to submit a deposit to the City for these services prior to initiation of the services. The applicant shall be responsible for reimbursement to the City for the services regardless of whether a deposit is required.	B	CD (P)(E)
9.		This project shall be subject to all City-wide development impact fees, unless exempt by previous agreement. This project shall be subject to all Citywide development impact fees in effect at such time that a building permit is issued. These fees may include, but are not limited to, fees for fire protection, park facilities, park equipment, Quimby, Humbug-Willow Creek Parkway, Light Rail, TSM, capital facilities and traffic impacts. The 90-day protest period for all fees, dedications, reservations or other exactions imposed on this project has begun. The fees shall be calculated at the fee rate in effect at the time of building permit issuance.	B	CD (P)(E), PW, PK
CONDITIONAL USE PERMIT REQUIREMENTS				
10.		Any intensification or expansion of the use approved and conditioned herein will require a subsequent Conditional Use Permit Modification by the Historic District Commission. No approvals are granted in this Conditional Use Permit except as provided.	B, OG	CD (P)
11.		All conditions of the original Conditional Use Permit (PC91-042 as modified by PC 95-033) are incorporated by reference.	OG	CD (P)
12.		Compliance with the City of Folsom's Noise Control Ordinance (<u>Folsom Municipal Code Chapter 8.42</u>) and General Plan Noise Element shall be required.	OG	CD (P)
13.		No more than four cremations shall occur per day and no more than 500 cremations shall be performed each year. No expansion in number of cremations per day or per year beyond what is stated above shall be permitted without prior approval being obtained from the Historic District Commission through a Conditional Use Permit Modification.	OG	CD (P)
14.		The building in which the crematorium is located shall remained locked at all times when a cremation is not occurring.	OG	CD (P)
15.		This Conditional Use Permit shall be deemed revoked without further action by the Historic District Commission if the operation of the facility in the manner described in the Conditional Use Permit ceases for any consecutive period of six (6) months.	OG	CD
AIR QUALITY REQUIREMENTS				

16.		<p>An authority to construct and permit to operate (A/C, PTO) shall be obtained from the Sacramento Metropolitan Air Quality Management District (SMAQMD) prior to the commencement of equipment installation. As part of this process, SMAQMD will determine if the emissions from the propane combustion and toxic emissions meet applicable rules and Best Available Control Technology (BACT) requirements, as well as the NAAQS 8-Hour Ozone Attainment and Reasonable Further Progress Plan (Regional Ozone Plan). A building permit will not be issued until an A/C, PTO has been obtained.</p> <p>Compliance with the SMAQMD A/C, PTO and the maintenance of such permits in good standing are requirements for validity of this Conditional Use Permit and the failure to comply with such permits and/or to maintain such permits in good standing shall trigger the process described in Condition of Approval No. 4.</p>	B	CD (P, B)
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17.		<p>The following SMAQMD Basic Construction Emission Control Practices shall be implemented throughout construction activities:</p> <ul style="list-style-type: none">• All exposed surfaces shall be watered two times daily. Exposed surfaces include, but are not limited to soil piles, graded areas, unpaved parking areas, staging areas, and access roads.• Cover or maintain at least two feet of free board space on haul trucks transporting soil, sand, or other loose material on the site. Any haul trucks that would be traveling along freeways or major roadways shall be covered.• Use wet power vacuum street sweepers to remove any visible trackout mud or dirt onto adjacent public roads at least once a day. Use of dry power sweeping is prohibited.• Limit vehicle speeds on unpaved roads to 15 miles per hour.• All roadways, driveways, sidewalks, parking lots shall be paved as soon as possible. In addition, building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.• Minimize idling time either by shutting equipment off when not in use or reducing the time of idling to 5 minutes (as required by the state airborne toxics control measure [Title 13, Section 2485 of the California Code of Regulations]). Provide clear signage that posts this requirement for workers at the entrances to the site.• Maintain all construction equipment in proper working condition according to manufacturer's specifications. The equipment shall be checked by a certified mechanic and determine to be running in proper condition before it is operated.	DC	CD (E)
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CULTURAL, ARCHAEOLOGICAL, PALEONTOLOGICAL AND HISTORICAL/TRIBAL RESOURCES REQUIREMENTS				
18.	✓	<p>Prior to the initiation of ground disturbing activity, a qualified professional archaeologist shall be retained to develop and deliver a contractor awareness training program to construction supervisors. The purpose of the training is to ensure that contractors are aware of the need to limit their activity, including equipment storage, staging, parking, and ground disturbance to only those locations identified as work areas on the official site plans.</p> <p>Prior to the initiation of ground disturbing activity, a qualified professional archaeologist shall be retained to monitor the installation of temporary high-visibility exclusionary fencing along the toe of existing mine tailings features adjacent to the shed. The fencing shall remain in place until all project activities are completed. City inspectors shall include a verification of the fencing during all required inspections. In the event that exclusionary fencing has failed, the construction supervisor must re-install or repair the fence within 24 hours.</p>	G, I, B	CD (P)(E)(B)

19.	✓	<p>If subsurface deposits believed to be cultural in origin are discovered during construction, all work must halt within a 50-foot radius of the discovery. A qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for pre-contact and historic archaeologist, shall be retained to evaluate the significance of the find, and shall have the authority to modify the no-work radius as appropriate, using professional judgment. The following notifications shall apply, depending on the nature of the find:</p> <ul style="list-style-type: none">• If the professional archaeologist determines that the find does not represent a cultural resource, work may resume immediately, and no agency notifications are required.• If the professional archaeologist determines that the find does represent a cultural resource from any time period or cultural affiliation, he or she shall immediately notify the City to consult on a finding of eligibility and implement appropriate treatment measures, if the find is determined to be a Historical Resource under CEQA, as defined in Section 15064.5(a) of the CEQA Guidelines or a historic property under Section 106 NHPA, if applicable. Work may not resume within the no-work radius until the City, through consultation as appropriate, determines that the site either: 1) is not an Historical Resource under CEQA, as defined in Section 15064.5(a) of the CEQA Guidelines; or 2) that the treatment measures have been completed to its satisfaction.	G, I, B	CD (P)(E)(B)
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20.	✓	<p>If subsurface deposits believed to be cultural or human in origin are discovered during construction, all work must halt within a 50-foot radius of the discovery. A qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for pre-contact and historic archaeologist, shall be retained to evaluate the significance of the find, and shall have the authority to modify the no-work radius as appropriate, using professional judgment. The following notifications shall apply, depending on the nature of the find:</p> <ul style="list-style-type: none"> • If the find includes human remains, or remains that are potentially human, he or she shall ensure reasonable protection measures are taken to protect the discovery from disturbance (AB 2641). The archaeologist shall notify the Sacramento County Coroner (per §7050.5 of the Health and Safety Code). The provisions of §7050.5 of the California Health and Safety Code, §5097.98 of the California PRC, and Assembly Bill 2641 will be implemented. If the Coroner determines the remains are Native American and not the result of a crime scene, the Coroner will notify the NAHC, which then will designate a Native American Most Likely Descendant (MLD) for the project (§5097.98 of the PRC). The designated MLD will have 48 hours from the time access to the property is granted to make recommendations concerning treatment of the remains. If the landowner does not agree with the recommendations of the MLD, the NAHC can mediate (§5097.94 of the PRC). If no agreement is reached, the landowner must rebury the remains where they will not be further disturbed (§5097.98 of the PRC). This will also include either recording the site with the NAHC or the appropriate Information Center; using an open space or conservation zoning designation or easement; or recording a reinternment document with the county in which the property is located (AB 2641). If the Coroner determines that the remains are human but are not Native American, then the Coroner will direct subsequent steps to address the discovery. Work may not resume within the no-work radius until the City, through consultation as appropriate, determines that the treatment measures have been completed to its satisfaction. 	G, I, B	CD (P)(E)(B)
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21.	✓	If potentially significant Tribal Cultural Resources (TCRs) are discovered during ground disturbing construction activities, all work shall cease within 50 feet of the find. A Native American Representative from traditionally and culturally affiliated Native American Tribes that requested consultation on the project shall be immediately contacted and invited to assess the significance of the find and make recommendations for further evaluation and treatment, as necessary. If deemed necessary by the City, a qualified cultural resources specialist meeting the Secretary of Interior's Standards and Qualifications for Archaeology, may also assess the significance of the find in joint consultation with Native American Representatives to ensure that Tribal values are considered. Work at the discovery location cannot resume until the City, in consultation as appropriate and in good faith, determines that the discovery is either not a TCR, or has been subjected to culturally appropriate treatment, if avoidance and preservation cannot be accommodated.	G, I, B	CD (P)(E)(B)
22.		If any archaeological, cultural, or historical resources or artifacts, or other features are discovered during the course of construction anywhere on the project site, work shall be suspended in that location until a qualified professional archaeologist assesses the significance of the discovery and provides consultation with the Folsom Historical Society, City staff, and the Heritage Preservation League. Appropriate mitigation as recommended by the archaeologist and the Historical Society representative shall be implemented. If agreement cannot be met, the Historic District Commission shall determine the appropriate implementation method.	G, I, B	CD (P)(E)(B)
FIRE AND LIFE SAFETY REQUIREMENTS				
23.		Current occupancy loads shall be posted at all times, and the owner/applicant shall have an effective system to keep count of the number of occupants present at any given time. This information shall be provided to public safety personnel upon request.	OG	CD (F)
24.		The building in which the crematorium is located shall be provided with an approved monitored fire alarm/detection system when the building is not protected by an automatic sprinkler system in accordance with the Folsom Municipal Code Section 907.9.1.	O	CD (F)

25.		On-site fire hydrants and mains capable of supplying the fire flow shall be provided in accordance with the 2019 <u>California Fire Code (CFC)</u> Section 507 Fire Protection Water Supplies: An approved water supply capable of supplying the required fire flow for fire protection shall be provided to all premises upon which facilities, buildings or portions of buildings are hereby constructed or moved into or within the jurisdiction, including the structure where cremations are proposed as a part of this project. Where a portion of a facility or building constructed is more than 400 feet from a hydrant on a fire apparatus access road, as measured by an approved route around the exterior of the facility or building, on-site fire hydrants and mains capable of supplying the fire flow shall be provided where required by the fire code official. <u>CFC 507</u>	O	CD(F)
26.		Installation of the propane tanks shall comply with the following: a. There shall be no more than two propane tanks with capacities of no more than 250 gallons each. b. The installation shall conform to 2019 CFC, Chapter 61, and 2014 <u>National Fire Protection Association (NFPA)</u> Standard 58. c. The container installation shall conform to Sec. 6.6.3, <u>NFPA 58</u> , which requires a minimum 10-foot separation distance between the 250-gallon propane tanks and the building in which the crematorium is located. d. Piping shall be installed per Section 6.9, <u>NFPA 58</u> , and tested per Section 6.14. e. Combustible materials, trash, weeds and brush shall not be stored or located within 10 feet of the propane tanks. <u>CFC 6107.3 & NFPA 58: 6.4.4.3</u> f. If the point of transfers for the propane tanks are not located at the tanks, they shall be located in accordance with <u>NFPA 58: Table 6.5.2.1</u> . g. Support of the propane tanks shall comply with <u>NFPA 58: 6.3.3(A) through 6.3.3(D)</u> . h. Steel supports shall be protected against fire exposure with materials having a fire resistance rating of at least 2 hours if the height limits specified in <u>NFPA 58: Table 6.3.3(A)</u> are exceeded.	O	CD (F)
27.		A trained operator shall remain on-site at all times that the crematorium machine is in operation.	O	CD (P)
OTHER AGENCY REQUIREMENTS				

Historic District Commission
 Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
 February 16, 2022

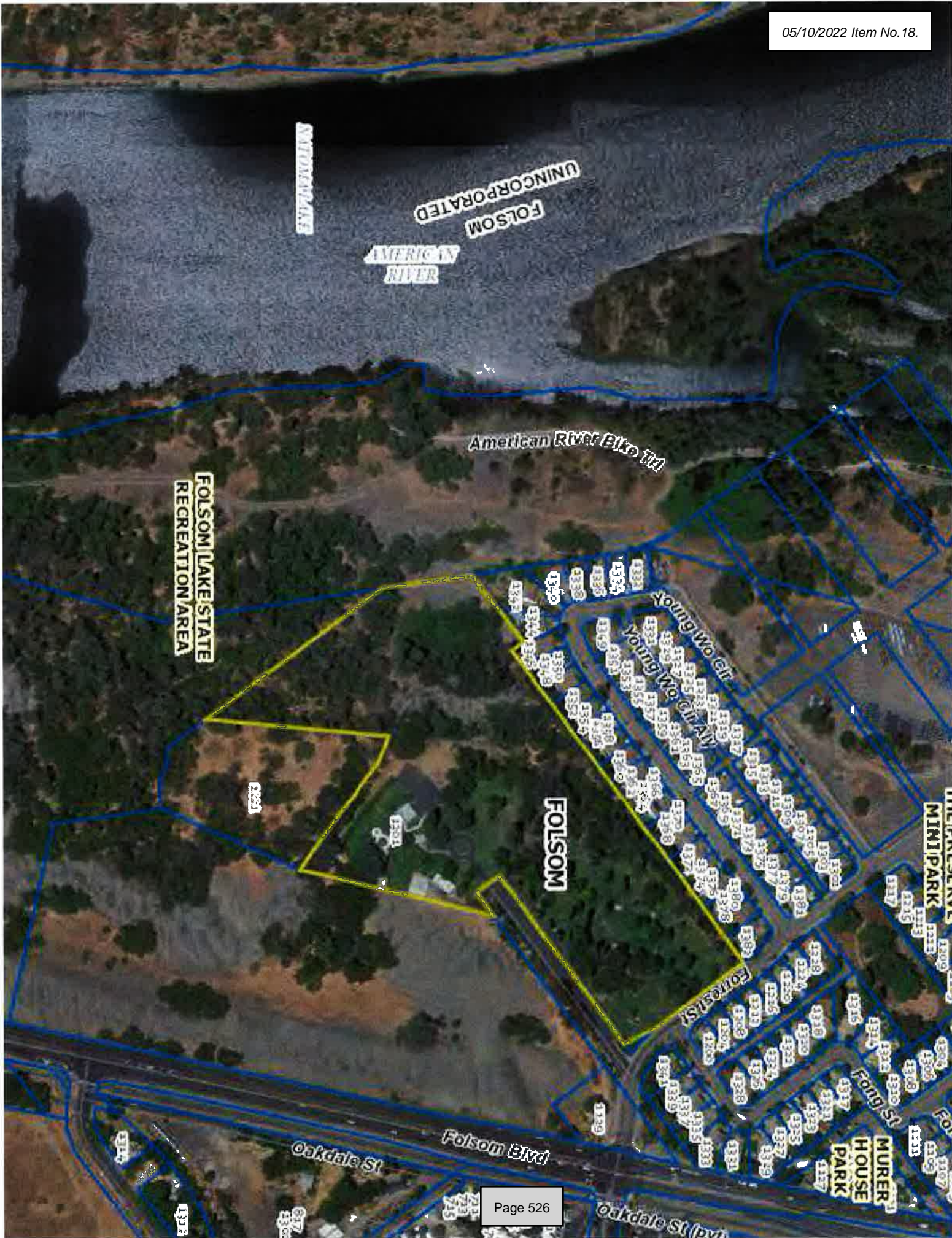
28.		The owner/applicant shall obtain and continually maintain in good standing all required State and Federal permits and provide evidence that said permits have been obtained, or that the permit is not required, subject to staff review and approval. Compliance with all required State and Federal permits and the maintenance of such permits in good standing are requirements for validity of this Conditional Use Permit and the failure to comply with such permits and/or to maintain such permits in good standing shall trigger the process described in Condition of Approval No. 4.	I, G	CD (P)(E)
29.		The owner/applicant shall obtain permission (permit, letter, agreement, etc.) from all applicable public utility companies (SMUD, PG&E, WAPA, etc.) in a form acceptable to the Community Development Department for construction-related activities proposed within the existing public utility easements.	I	CD (P)
30.		The proposed project shall comply with all State and local rules, regulations, Governor's Declarations, and restrictions including but not limited to requirements relative to water usage and conservation established by the State Water Resources Control Board, and water usage and conservation requirements established within the <u>Folsom Municipal Code</u> , (Chapter 13.26 Water Conservation), as amended from time to time.	I, B, OG	CD (P)(E)

RESPONSIBLE DEPARTMENT		WHEN REQUIRED	
CD	Community Development Department	I	Prior to approval of Improvement Plans
(P)	Planning Division	M	Prior to approval of Final Map
(E)	Engineering Division	B	Prior to issuance of first Building Permit
(B)	Building Division	O	Prior to approval of Occupancy Permit
(F)	Fire Division	G	Prior to issuance of Grading Permit
PW	Public Works Department	DC	During construction
PR	Park and Recreation Department	OG	On-going requirement
PD	Police Department		

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February 16, 2022

ATTACHMENT 4

Vicinity Map



AMERICAN RIVER
 FOLSOM
 UNINCORPORATED

FOLSOM LAKE STATE
 RECREATION AREA

American River Blk 171

FOLSOM

MINI PARK

MURER HOUSE
 PARK

Oakdale St

Folsom Blvd

Oakdale St (pvt)

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Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
February 16, 2022

ATTACHMENT 5
Overall Site Plan, Detailed Site Plan and Floor
Plan, Dated 2-19-20

1201 FORREST ST IN FOLSOM, CA 95630

PROJECT DESCRIPTION & SCOPE OF WORK
SCOPE OF WORK
 USE PERMIT FOR A NEW CREAM/PEACH COOLER IN AN EXISTING SHED.

APN	07002600010000
ZONING	OSC
OCCUPANCY GROUP	
CONSTRUCTION TYPE	VB
NO. OF FLOORS	1
LOT AREA	523591 SQ. FT. / 12.02 ACRES

**LEV
DESIGNS**

VADIM ZANKO
 7844 MADISON AVE
 STE 105 FAIR OAKS, CA
 95628
 916-312-2131
 support@levdesigns.com



LAKESIDE MEMORIAL
LAWN CEMETERY

1201 FORREST ST,
FOLSOM
CA 95630

07002600010000

PROJECT DIRECTORY

DESIGNER
 VADIM ZANKO
 LEV DESIGNS
 (916)-312-2131
 vzanko@levdesigns.com

OWNER

SHEET INDEX

SHEET	DESCRIPTION
A1.0	COVER PAGE
A2.0	OVERALL SITE PLAN
A2.1	DETAILED SITE PLAN
A3.0	EXISTING FLOOR PLAN

BUILDING INFORMATION

ALL WORK SHALL FULLY COMPLY WITH BUT NOT BE LIMITED TO:

A. 2019 CALIFORNIA BUILDING CODE (CBC)
 B. 2019 CALIFORNIA PLUMBING CODE (CPC)
 C. 2019 CALIFORNIA MECHANICAL CODE (CMC)
 D. 2019 CALIFORNIA ELECTRICAL CODE (CEC)
 E. 2019 CALIFORNIA FIRE CODE (FC)
 F. 2019 CALIFORNIA REFERENCED STANDARDS CODE
 G. 2019 GREEN BUILDING STANDARDS CODE (GBS)
 H. 2019 CALIFORNIA ENERGY
 I. 2019 ENERGY STANDARDS
 J. ALL OTHER APPLICABLE CODES AND ORDINANCES (CITY AND COUNTY).

GOVERNMENT AUTHORITIES AND CODES TAKE PRECEDENCE OVER DRAWINGS AND SPECIFICATIONS. REPORT DISCREPANCIES TO THE ARCHITECT IMMEDIATELY.

ABBREVIATIONS

1P1S	1 POLE / 1 SHELF
SS	S SHELVES
ABV	ABOVE
A.F.F.	ABOVE FINISHED FLOOR
BDRM	BEDROOM
BLW	BELOW
B.O.	BOTTOM OF
C.L.O.	CEILING
CMNT	CASSEMENT WINDOW
CONC.	CONCRETE
DET.	DETAIL
DS	DOWNSPOUT
DS BLW	DOWNSPOUT BELOW
F.F.	FACE OF FRAMING
FLR	FLOOR
FOUND	FOUNDATION
FR DR	FRENCH DOOR
FW	FIXED WINDOW
HDR	HEADER
HL HT	HEEL HEIGHT
HT	HEIGHT
LNDY	LAUNDRY
LNB	LAUNDRY
LIN	LINEN
MIN.	MINIMUM
N.I.C.	NOT IN CONTRACT
O.H.	OPPOSITE HAND
OPT.	OPTIONAL
P.T. LUMBER	PRESSURE TREATED LUMBER
RF	ROOF
RF BLW	ROOF BELOW
S.D.	SEE STRUCTURAL DRAWINGS
S.H	SINGLE HUNG
SL	SLIDER WINDOW
T	TEMPERED GLASS
T&G	TONGUE AND GROOVE
TEMP	TEMPERED GLASS
T.G.PAD	TOP OF GRADED PAD
T.O.P.	TOP OF PLATE
T.O.C.	TOP OF CURB
T.O.SLAB	TOP OF SLAB
T.O.S.F.	TOP OF SUBFLOOR
T.O.S.	TOP OF BEAM
TYP.	TYPICAL
U.O.N.	UNLESS OTHERWISE NOTED
VERT.	VERTICAL
WD	WOOD
W.D.W.	WINDOW
WP	WEATHER PROOF
V.I.F.	VERIFY IN FIELD

SQUARE FOOTAGE BREAKDOWN

SQUARE FOOTAGE BREAKDOWN			DATE: 05/10/22
EXISTING SHED AREA			1431
REMODELLED SHED AREA			2411 (971) = 22%


SPECIAL INSPECTIONS

DEFERRED SUBMITTALS:

SEPARATE PERMIT:

VICINITY MAP
N.T.S.

PARCEL LOCATION



NO.	DATE	ISSUES FOR REVIEW

PROJECT NO: CAD SWG FILE

DRAWN BY: SERGIO VIZCAINO
 CHECKED BY: VADIM ZANKO
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REVISIONS
 COVER PAGE

A1.0



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LAKESIDE MEMORIAL
LAWN CEMETERY

1201 FORREST ST,
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CA 95630

07002600010000

NO.	REVISIONS ISSUED FOR REVISION

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SHEET TITLE:
OVERALL SITE PLAN

A2.0

PATCH
EXISTING
WALKWAY/DRIVEWAY
STOOPLANDING

EXISTING TREE

LEGEND:
PROPERTY LINE
EXISTING AREA
AREA OF WORK

OVERALL SITE PLAN 1" = 50'





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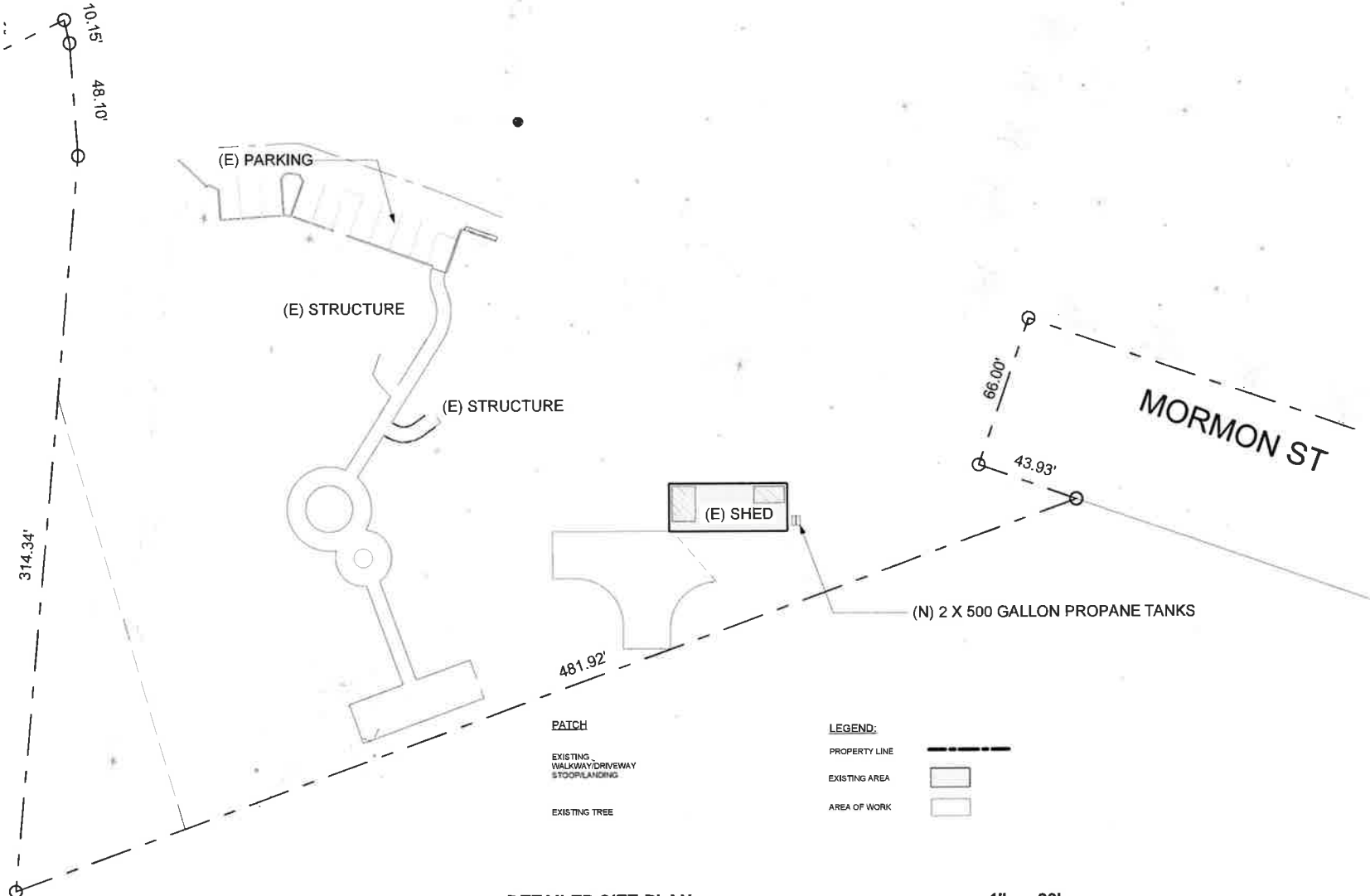
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LAWN CEMETERY

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FOLSOM
CA 95630

07002600010000

DATE	ISSUED FOR	DESCRIPTION

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 SHEET TITLE:
 DETAILED SITE PLAN



DETAILED SITE PLAN

1" = 20'

A2.1

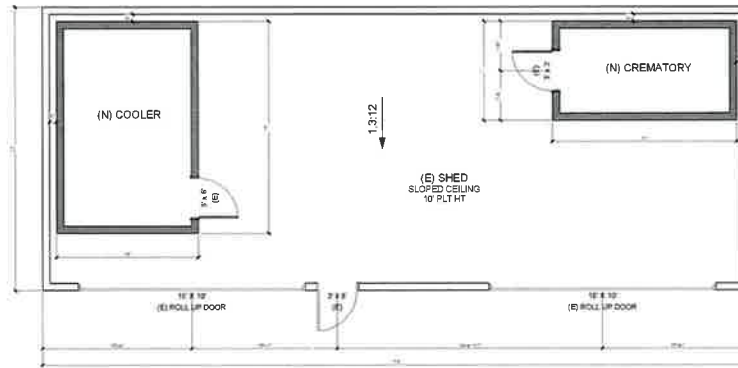


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CA 95630

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EXISTING FLOOR PLAN

1/4" = 1'-0"

FLOOR PLAN NOTES:

- 1. WINDOWS AND DOORS NOTED AS FEET (E.G. 3'-0" X 4'-0")
- 2. DOORS ARE NOTED WITHOUT FRAME CONTRACTOR TO VERIFY WITH MANUFACTURER FOR DOOR FRAME SIZE PAIR TO FRAMING DOOR COORDINATE
- 3. DO NOT SCALE OFF PLANS
- 4. ALL DIMENSIONS ARE TO FACE OF STUD I.P.S.D.
- 5. WALL INSULATION TO BE A MINIMUM OF R-11 BATTING AND ATTIC INSULATION TO BE R-38 SHOWN IN BATT OR RIGID
- 6. ON SITE VERIFICATION OF ALL DIMENSIONS AND CONDITIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND THE SUB-CONTRACTORS. NOTED DIMENSIONS TAKE PRECEDENT OVER SCALE. EACH CONTRACTOR OR SUB-CONTRACTOR SHALL REPORT TO ARCHITECTS ON ALL CONDITIONS WHICH PREVENT THE PROPER EXECUTION OF THEIR WORK
- 7. CONTRACTOR TO BE NOTICED IMMEDIATELY BY CONTRACTOR OR SUB-CONTRACTOR SHOULD ANY DISCREPANCY OR OTHER QUESTION ARISE PERTAINING TO THE DRAWING DIMENSIONS AND/OR SPECIFICATIONS. THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR THE RESULTS OF ANY ERRORS, DISCREPANCIES OR DIMENSIONS WHICH THE CONTRACTOR FAILS TO NOTIFY THE ARCHITECTS PRIOR TO COMMENCEMENT OF CONSTRUCTION AND/OR FABRICATION OF THE WORK.

LEGEND

- (E) EXISTING
- (N) NEW

WALL LEGEND

- (E) 1/2" CMU
- (N) 1/2" CMU

PLAN AREA SQUARE FOOTAGE BREAKDOWN:

EXISTING SHED AREA 1,071 SQ. FT.

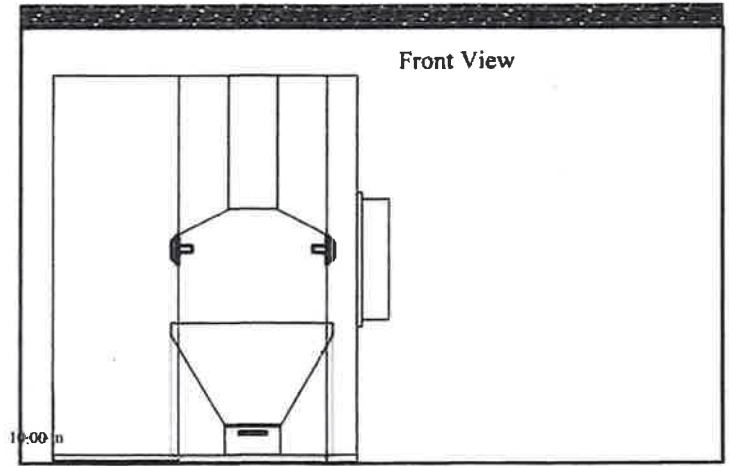
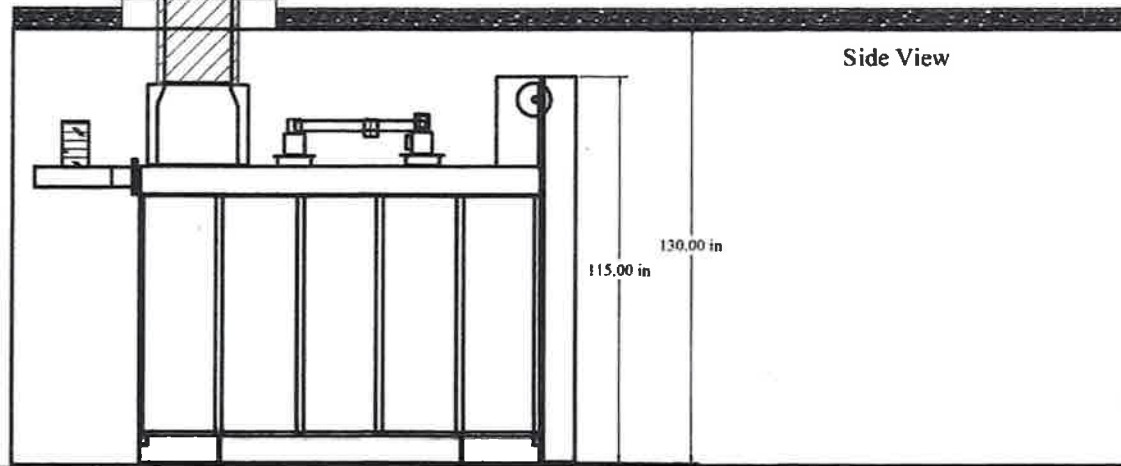
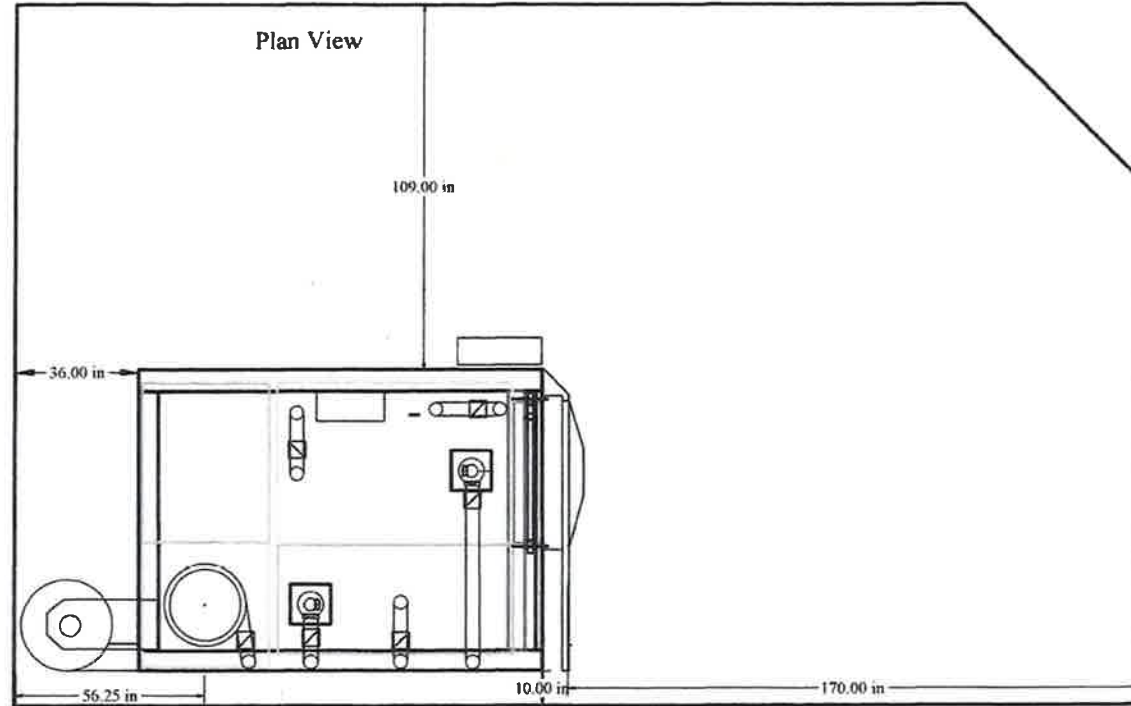
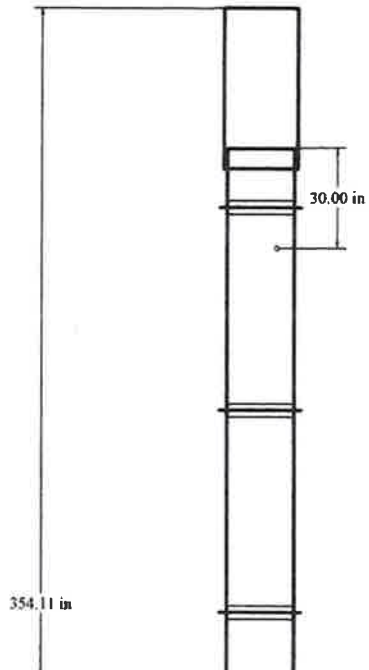
NO.	DATE	DESCRIPTION

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SHEET TITLE:
EXISTING FLOOR PLAN

A3.0

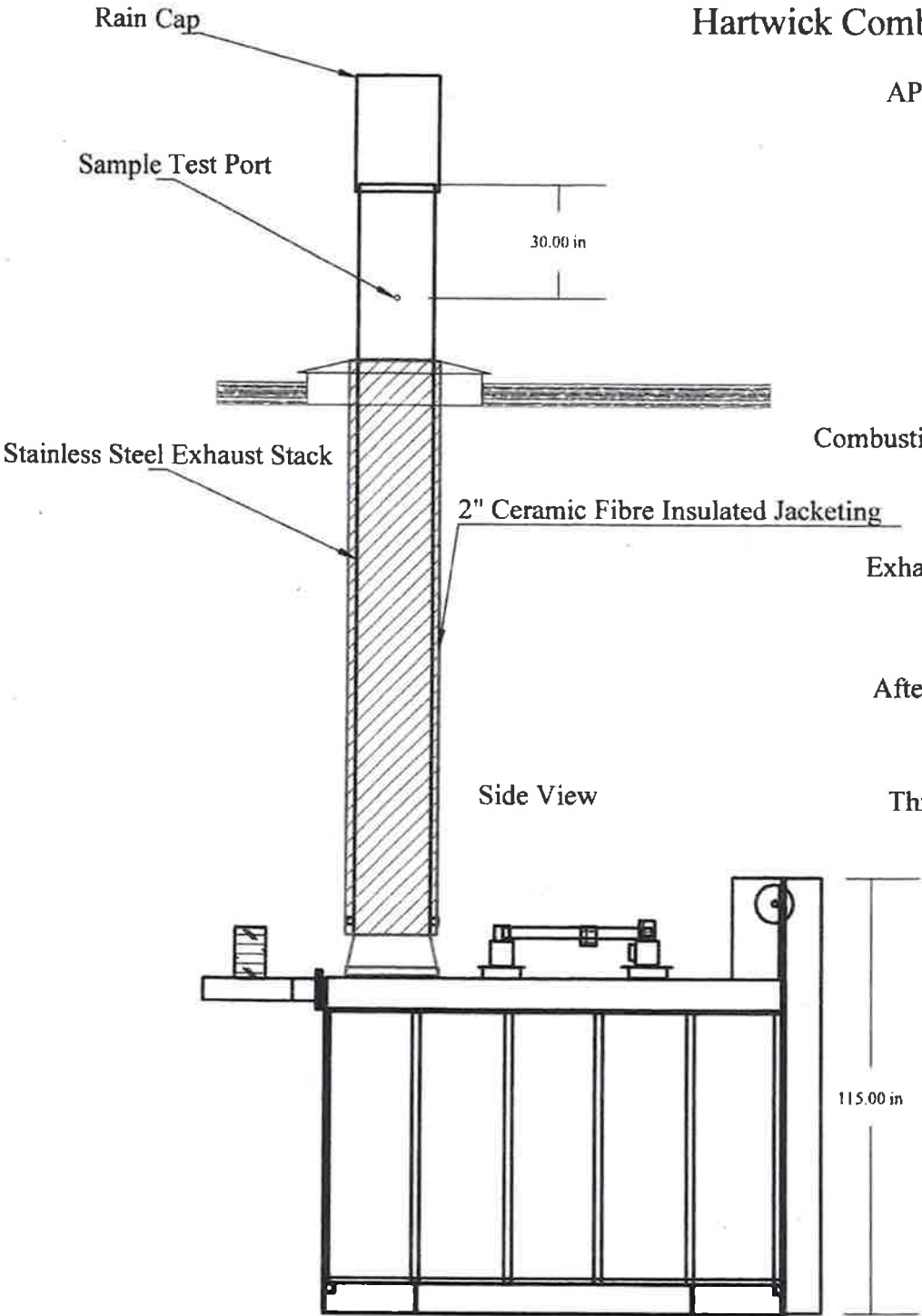
Historic District Commission
Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
February 16, 2022

ATTACHMENT 6
Hartwick Combustion Technologies Floor Plan,
Outer Dimensions and Specs

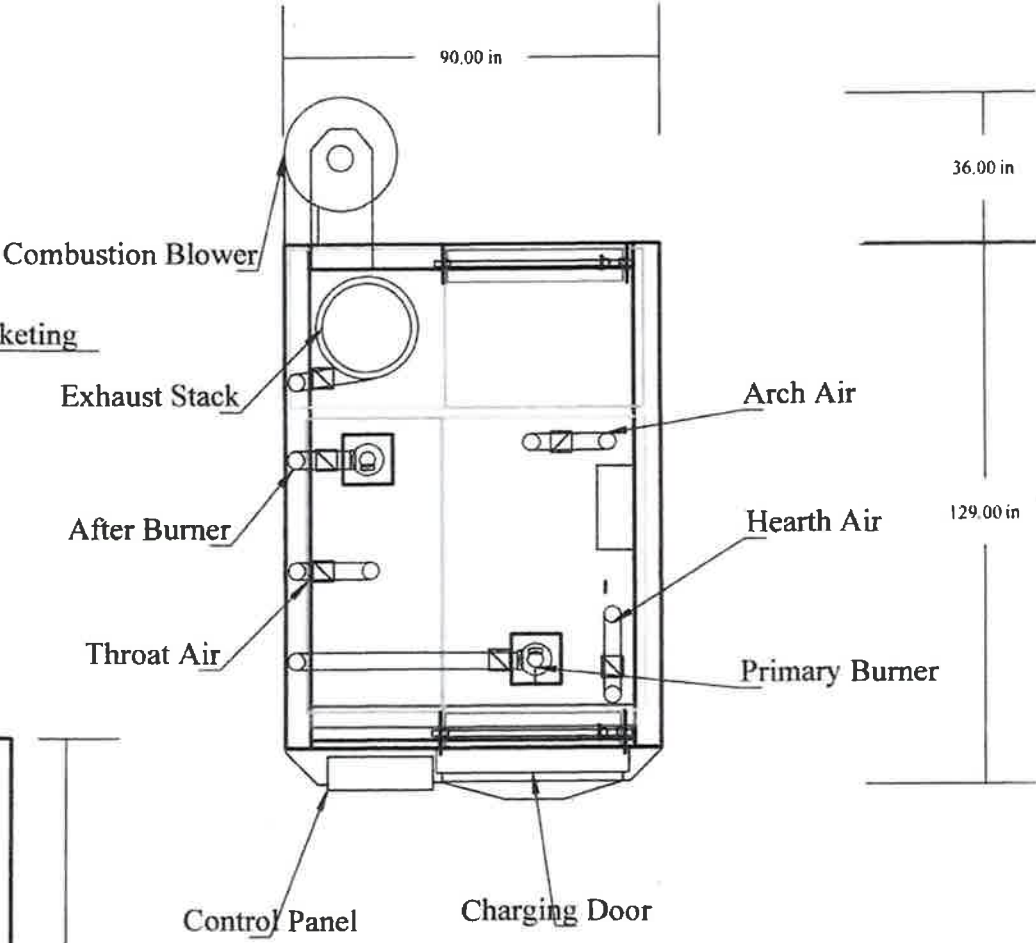


Hartwick Combustion Technologies, Inc.

APEX 250



Top View of Mechanical features





Hartwick Combustion Technologies, Inc.

Apex-250 XP

High Efficiency

Average cycle time 60 minutes
 Secondary chamber 120 cu/ft
 Natural gas / propane
 Length 175"
 Width 90"
 Height 115"
 Weight 36,000 lbs
 2,500,000 BTU
 1 Charging door
 1 Rear door/ Retrieval
 2 Primary burners
 1 Secondary burner
 Self cooling exterior walls
 Continuous operation
 230 Volt 1-phase / 3 phase
 Oversized chamber
 1000 lbs maximum load
 Quiet operation 60 DB range
 5-8 cases in 8 hours

Apex-250

High Production

Average cycle time 90 minutes
 Secondary chamber 90 cu/ft
 Natural gas / propane
 Length 140"
 Width 90"
 Height 115"
 Weight 32,000 lbs
 2,000,000 Uncontrolled BTU
 1,800,000 Controlled BTU
 1 Charging door
 1 Primary burner
 1 Secondary burner
 Self cooling exterior walls
 Continuous operation
 230 volt 1-phase / 3 phase
 1000 lbs maximum load
 Quiet operation 60 DB range
 4-6 cases in 8 hours



Business Office; 3533 San Gabriel River Parkway, Pico Rivera, Ca 90660
 (800)816-9125 Fax (562)922-8305
 E-mail; info@hartwickcombustion.com

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Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
February 16, 2022

ATTACHMENT 7
Updated Site Plan with Current Layout of
Maintenance Area

SHEET INDEX:
 SHEET A2.1 - EXISTING SITE PLAN
 CALIFORNIA ALL-STEEL ENGINEERING PACKAGE (DRAWING NO: MBD3E6CEE66)

CODE GOVERNED:
 ALL NEW WORK SHALL COMPLY WITH 2019 CBC AND ANY LOCAL AMENDMENTS

SCOPE OF WORK:
 CONSTRUCT A NEW STORAGE SHED FOR STORAGE. NO CHEMICAL STORED.



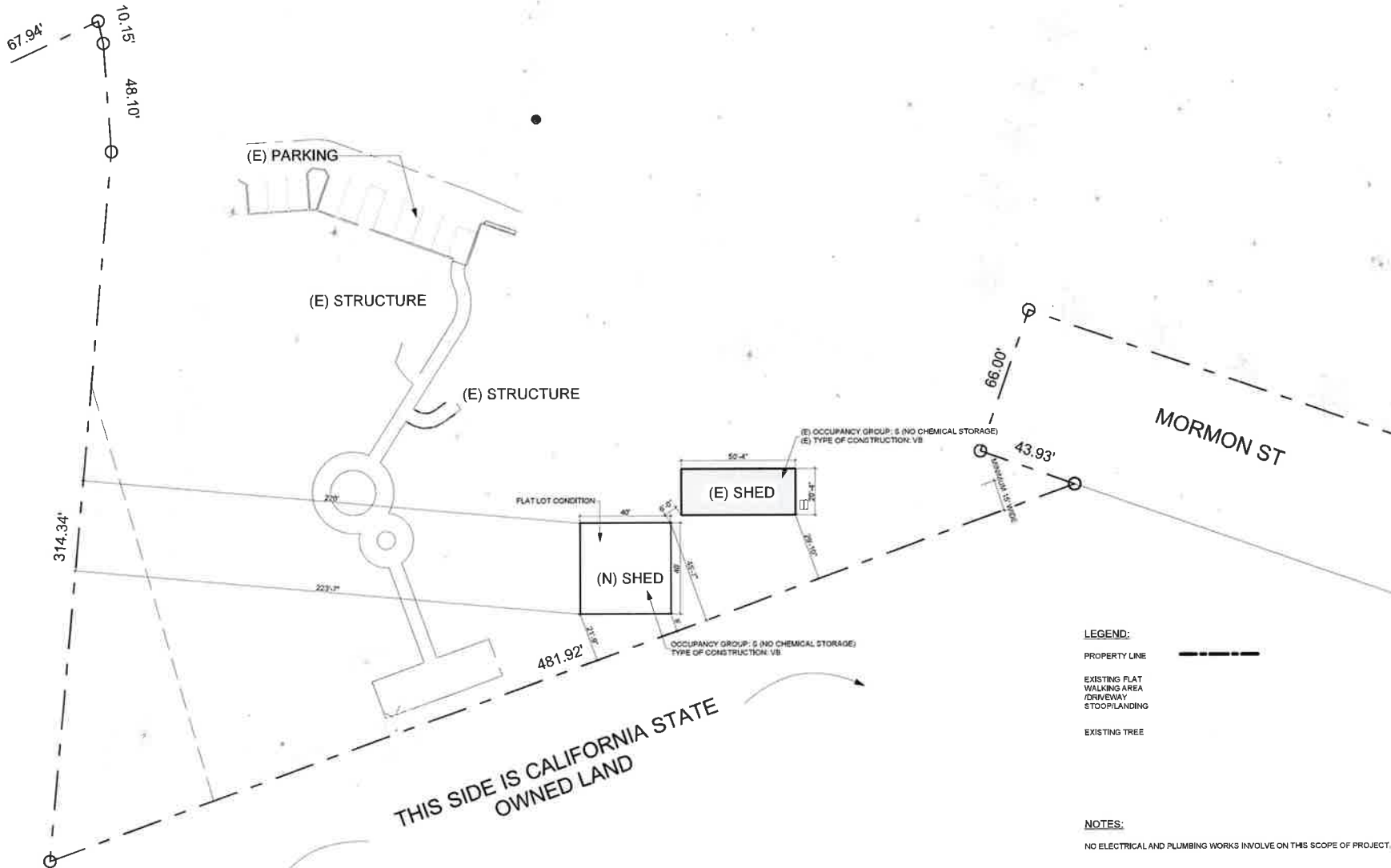
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SIGNATURE

LAKESIDE MEMORIAL
 LAWN CEMETERY
 NEW STORAGE SHED

1201 FORREST ST,
 FOLSOM
 CA 95630

07002600010000



- LEGEND:**
- PROPERTY LINE
 - EXISTING FLAT WALKING AREA /DRIVEWAY /STOOD/PLANDING
 - EXISTING TREE

NOTES:
 NO ELECTRICAL AND PLUMBING WORKS INVOLVE ON THIS SCOPE OF PROJECT.

EXISTING SITE PLAN 1" = 20'

NO.	REVISION	ISSUED FOR REVIEW

DATE	DESCRIPTION

PROJECT NO:
 SHEET NO:
 DRAWN BY: SERGIO KOVALOV
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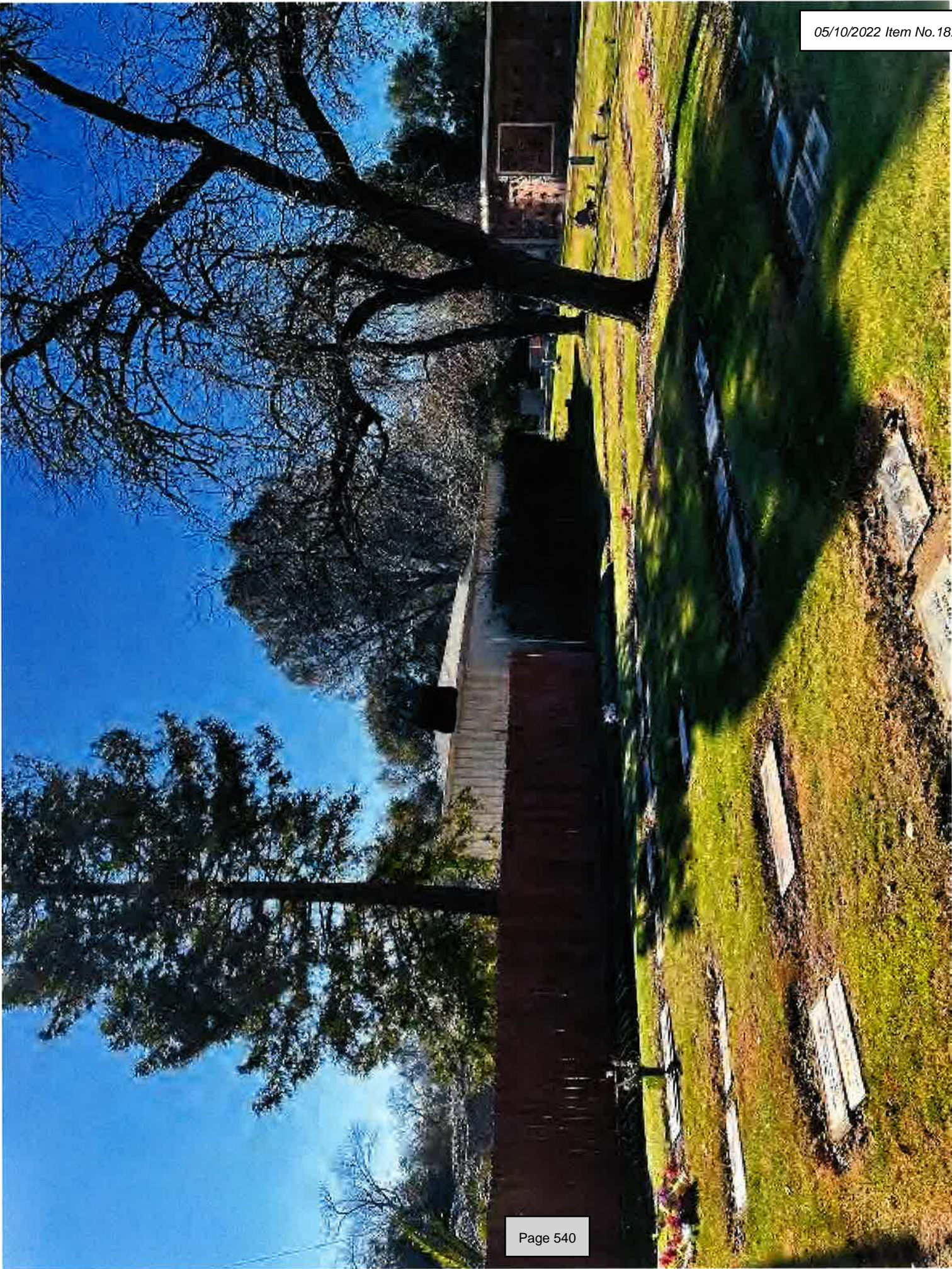
SHEET TITLE:
 EXISTING SITE PLAN

A2.1

Historic District Commission
Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
February 16, 2022

ATTACHMENT 8
Applicant's Rendering of Proposed
Crematorium Stack





Historic District Commission
Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
February 16, 2022

ATTACHMENT 9

Applicant's Narrative



Scope of Work- Installing a Crematory

We are proposing to install a Hartwick Apex 250 Human Crematory at our existing shed in our cemetery Lakeside Memorial Lawn 1201 Forrest St Folsom CA 95630.

Introduction

Lakeside Memorial Lawn has existed since 1850. It has been a large part of the history of the city of Folsom and remains to be a part of the future of the city of Folsom. It is privately held and operated by family, who currently reside in Folsom and have a long heritage in Folsom. Lakeside Memorial Lawn alongside Miller Funeral Home have over 150 years in service to the community.

The Claney and Semenyuk family has a combined experience of 100 years in the funeral industry. Residents of Folsom and active partners in the community, local ordinances and the Folsom Police Department in Every 15 Minutes Program. We also own Caring Service Group, which has 8 funeral homes from Clear Lake to Arroyo Grande. With four crematories in four separate cities. Coordinating with their respected cities and local air ordinances, Caring Service Group has been successful in operating and installing these crematories. They have been in operation for the last 10 years and have not received one complaint against their licensing as they continue to operate at full capacity. All four crematories are inside the funeral home buildings, which is an impressive feat on its own. These crematories have facilitated thousands of cremations and will continue to do so. This is to demonstrate the knowledge, professionalism and ethical practices we uphold when we install and operate crematories.

Point at Issue

There are several large cultural communities residing and moving into Folsom. Regretfully, we do not have the ability nor the capability to serve the Sikh, Hindu, Buddhist or other cremation-based cultures as they require an on-site crematory so that they may be able to exercise their funeral rights and customs. Currently, there is no crematory in the city of Folsom, so they must find these services outside their city.

The increase in homes being built in the city of Folsom, including Folsom Ranch, we need to have an on-site crematory in order to facilitate its increase in cremation service. Folsom tax paying residents are unable to choose cremation disposition to be performed locally.

California currently has a cremation rate of 66.7 percent and will increase to 80 percent by 2030. Since 2019 the city of Folsom has an estimated 450 deaths a year and rising as the population increases. That is an estimated 300 cremations needed to be performed every year.

Our Proposition

The crematory placement at Lakeside Memorial Lawn is a strategic placement. It is removed from any residences, parks, or schools. It is not in eyesight of any homes, business's or restaurants. It will be inside the existing shed of Lakeside Memorial Lawn, surrounded by the rock piles of Folsom. As the only operational cemetery in Folsom, it is governed by the California Funeral and Cemetery Bureau and crematories fall under its' jurisdiction in licensing and on-site inspections.

As the cemetery does not have a gas or sewer line, we will be installing two propane tanks to facilitate the energy needed for the machine. It is a simple and cost-efficient option.

Cremation Process

A small explanation of cremation and its process.

Once paperwork - death certificate and local disposition permit is filed, with the family's permission and consent, we can then proceed with cremation.

Through a very strict identification process, we take the remains of John Doe and place them in a cremation container. It is identified again, weighed and recorded into the crematory log.

The machine or Cremator is started and heated up to a degree that the local air quality board deems necessary for the area, prior to remains being placed into the main chamber. Usually ranges from 1100-1600 degrees Fahrenheit. A Cremator has two burners, a main burner which is directly over the middle of the main chamber and a back burner, which is in the back in the second chamber.

The function of the second chamber is to initially heat up the machine and to incinerate any particles that escape the main chamber during the cremation process. That way there is no smoke or residue that leaves the chamber.

The main burner is there to facilitate the cremation process. It continues to heat up the main chamber and maintains a consistent temperature that allows the water in the subject to evaporate.

Once the Cremator reaches the set temperature, the cremation container is then rolled inside the main chamber. When the door opens, the main burner shuts off for safety reasons. That prevents any harm to the crematory operator and facility. The door is then shut and after a few minutes the main burner starts again and thus begins the cremation process.

This process is self-operating and does not require anything else apart from time. The usual time cremation takes is an average of 2 hours for the combustion process to be complete. It is a self-contained and safe process from start to finish.

The side door is opened, and the remains are collected and placed to cool down. Once cool, they are processed through a processor and placed into a plastic bag that is identified and sealed. The bag is placed into an urn and sealed. The urn is identified with name, date of cremation and the crematories license number. All this information is logged into the crematory book that is overseen by the CA State Funeral and Cemetery Bureau.

Usual Safety Concerns

In the process of any errors or malfunction, there are safety features installed. In the event of an operational failure, the gas is immediately and automatically shut off. There is a manual switch as well. There has never been a crematory blowing up in the history of cremation.

We buy our machines from a very reputable, if not the best in the cremation industry manufacturer called Hartwick Combustion Technologies. They are manufactured in California, are designed to use less fuel, have lower NOx emissions, is NFPA compliant, meets Air Quality standards and the components used are UL listed.

We have one of their machines located within our Lakeport funeral home and we have not had one issue or complaint. We trust in the safety and manufacturing of these machines and ourselves hold years of experience in the cremation industry with our own high safety standards.

Scope of Work

We are hoping to get a use permit from the city of Folsom to operate our very own Hartwick Apex 250 Human Crematory. To accomplish this, we will be working side by side with the city of Folsom, Sacramento Air Quality Board and the CA Funeral and Cemetery Bureau to meet all necessary standards, laws and regulations in order to complete said application and Use permit.

Once we are issued the Use Permit from the city of Folsom, we will go ahead and install the Cremator inside the existing shed at Lakeside Memorial Lawn.

We will have it lifted with a crane off the truck and rolled inside the shed. Once in the proper place, the stack will be installed, and the electrical/propane lines connected by its respected professionals. The CA Cemetery and Funeral Bureau will do a final inspection and they will issue a crematory license. At that point we will be operational, and the city of Folsom will have its first crematory.

We have done this process twice in two other cities and we look forward to doing this in the city we live in.

If you have any questions or require more information, please contact me.

Igor Semenyuk

Chief Operating Officer/Partner

igor@caringservicegroup.com

O. 916-985-2295

M. 916-548-6808

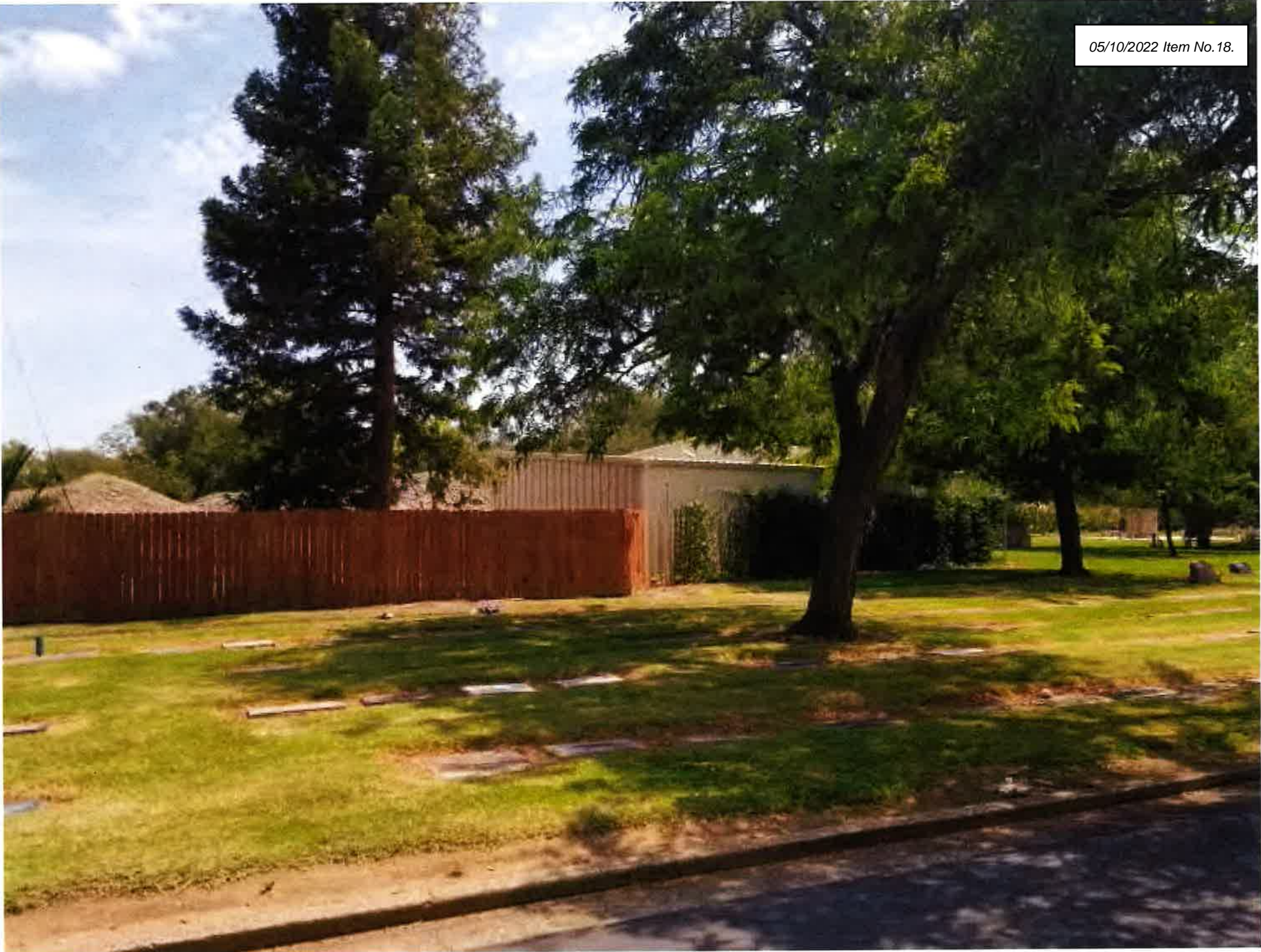
Historic District Commission
Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
February 16, 2022

ATTACHMENT 10

Photographs of the Project Site



VIEW OF MAINTENANCE AREA FROM INSIDE CEMETERY



VIEW OF MAINTENANCE AREA FROM INSIDE CEM



VIEW OF MAINTENANCE AREA FROM CEMETERY PARKING LOT



VIEW OF MAINTENANCE AREA FROM OUTER EDGE OF CEMETERY NEAR PRESERVE



INTERIOR VIEW OF MAINTENANCE AREA



VIEW INSIDE STRUCTURE FOR PROPOSED CREMATORIUM



VIEW OF MAINTENANCE AREA FROM THE PRESERVE SUBDIVISION



VIEW OF MINING TAILINGS FROM MAINTENANCE AREA



VIEW OF MINING TAILINGS FROM FOLSOM BOULEVARD AND FORREST STREET

Historic District Commission
Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
February 16, 2022

ATTACHMENT 11
Draft Initial Study/Mitigated Negative Declaration,
Dated January 2022

Lakeside Memorial Lawn Crematorium

Initial Study/Mitigated Negative Declaration

Prepared by:
City of Folsom
Community Development Department
50 Natoma Street
Folsom, CA 95630

With Technical Support by:
HELIX Environmental Planning, Inc.
11 Natoma Street, Suite 155
Folsom, CA 95630

January 2022

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1.0 INTRODUCTION

This Initial Study (IS) addresses the proposed Lakeside Memorial Lawn Crematorium project (proposed project) and whether it may cause significant effects on the environment. The IS also assesses whether any environmental impacts of the project are susceptible to substantial reduction or avoidance by project revision, imposition of conditions, or any other means [§15152(b)(2)] of the California Environmental Quality Act (CEQA) Guidelines. If such revisions, conditions, or other means are identified, they will be included as mitigation measures.

This Initial Study relies on CEQA Guidelines Sections §15064 and 15064.4 in its determination of the significance of the environmental impacts. Per §15064, the finding as to whether a project may have one or more significant impacts shall be based on substantial evidence in the record, and that controversy alone, without substantial evidence of a significant impact, does not trigger the need for an Environmental Impact Report (EIR).

2.0 PROJECT BACKGROUND

The following project specific technical reports quantified analysis and or surveys were used in preparation of this Initial Study and are incorporated by reference:

- Air Quality and Greenhouse Gas Analysis, prepared by HELIX Environmental Planning, Inc. (December 2020).
- Addendum to the Folsom Lakeside Crematorium Project Air Quality and Greenhouse Gas Emissions Assessment, prepared by HELIX Environmental Planning, Inc. (November 2021).
- Cultural Resources Inventory Report for the Lakeside Memorial Lawn Storage Shed Project, prepared by ECORP Consulting, Inc. (November 2020).
- Tribal Consultation Record for Compliance with Assembly Bill 52 and CEQA for the Lakeside Memorial Lawn Storage Shed Project, prepared by ECORP Consulting, Inc. (January 2021).

3.0 PROJECT DESCRIPTION

3.1 Project Location

The proposed project would be constructed on an approximately 12-acre parcel situated near the western boundary of the City of Folsom in Sacramento County, California. The project site is located west of the intersection of Forrest Street/Natoma Street along Folsom Boulevard within the existing Lakeside Memorial Lawn Cemetery. It lies along the eastern shore of Lake Natoma. The crematorium would be constructed within an existing shed along the eastern boundary of the property, just west of the end of Mormon Street. The project site is identified as Assessor's Parcel Number (APN) 070-0260-001. Refer to **Figure 1** for the regional location and **Figure 2** for an aerial view of the project site. All figures are included in **Appendix A**.

3.2 Project Setting and Surrounding Land Uses

The project site is currently a small cemetery, with associated landscaping, outbuildings, and access roads. Lands to the south and west contain woodland habitat typical of riparian communities in the Sierra Nevada foothills. Soils at the project site are comprised of dredge tailings and other fill material.

Tailing piles between the site and Folsom Boulevard prevent the site from being visible from that street. To the west is also the Jedidiah Smith Memorial Trail that runs along the eastern shore of Lake Natoma. The trail, also known as the American River Bike Trail, connects Folsom Lake (north of the project site) to the confluence of the American and Sacramento Rivers in Downtown Sacramento. It is a part of the American River Parkway that is operated by the California Department of Parks and Recreation. To the north of the project site is a small residential neighborhood with single-family dwellings. Folsom Boulevard runs in a north/south line just east of the property. East of Folsom Boulevard is a large, developed area containing single-family homes, apartment complexes, a mobile home park, and some small businesses. The more regional setting is primarily characterized by residential development with a commercial shopping center to the east.

The project site is generally flat, ranging from about 175 to 185 feet above mean sea level. There are no wetlands, streams, or jurisdictional features located on the project site.

3.3 Project Characteristics

The project would be located in an existing metal shed on the grounds of the existing Lakeside Memorial Lawn Cemetery. The shed can be reached by following Mormon Street to its terminus, making a slight left turn, and continuing for approximately 100 feet down an access road. The proposed project includes the installation of one HCT Apex-250 crematory manufactured by Hartwick Combustion. The shed would be modified to accommodate this device, but the shed's footprint would not be expanded. The shed currently covers 1,071 square feet. Further, a 10 foot by 15-foot walk-in cooler would be installed inside the shed to provide temporary, short-term storage of human remains prior to cremation. Two 250-gallon propane tanks would be installed on a proposed concrete pad along the northern side of the shed to provide power for the crematorium, as no gas lines currently exist on the property. The pad would cover approximately 38.3 square feet of ground. A small exhaust stack would be installed on the roof of the shed.

The applicant anticipates 1-4 cremations on business days (Monday through Friday) with the total number of cremations not exceeding 500 per year. Average cremation time is approximately 90 minutes. Refer to **Figure 3** for the site design plan in **Appendix A**.

Parking and Circulation

Diagonal parking spaces can be found along both sides of Mormon Street. At the terminus of Mormon Street, members of the public may continue straight onto a main cemetery access road to find an additional parking lot. All existing parking spaces would be maintained. Access to the project site directly would continue to be provided by a smaller existing access road located at the southwestern terminus of Mormon Street. Both the smaller access road and the main access road can be reached at the terminus of Mormon Street, but the two roads do not form a continuous loop due to a fence line dividing them. No new parking spaces or facilities would be constructed.

As the crematorium would not be located in or near a funeral home and would be separate from any funeral services or public gatherings provided by the project applicant, access would only need to accommodate a small number of staff members with business at the site.

Emergency Vehicle Access

Emergency vehicle access would be maintained throughout the project site to meet the Fire Department standards for fire engine maneuvering, location of fire engine to fight a fire, rescue access to the units, and fire hose access to all sides of the building.

Utilities

The cemetery is currently serviced with potable water and irrigation water from the City of Folsom. There is no need to seek a "will serve" letter as the City currently provides water and the crematory would not substantially increase flow demand. The City also provides solid waste collection and disposal services; the project is not expected to result in a significantly increased demand for solid waste removal.

As an existing facility, Lakeside Memorial Lawn maintains adequate fire response infrastructure for both current operations and the proposed project. The City Fire Department reviewed the project application and did not raise any concerns regarding the adequacy of water supply or site access.

The cemetery is currently served with an electricity supply from the Sacramento Municipal Utilities District (SMUD). Electrical connections already exist for the shed, and may be upgraded as needed as part of the proposed project. Installation and operation of the crematory would not result in a significant increase in demand for electricity on the project site.

The cemetery, including the shed, does not have an existing sewer line. This project would not require access to, or construction of, a sewer line. The two 250-gallon propane tanks and a concrete pad for securing them would be constructed along the northern edge of the shed to provide power for the crematorium.

Stormwater flows on the site are retained and drained to Lake Natoma. There would be no change in the hydrologic regime of the project site due to the installation or operation of the proposed project.

Landscaping

Existing landscaping at the cemetery and around the shed consists of mature broad-leaved, coniferous, trees and palms. These trees and landscaping also provide shade for much of the cemetery and many of the parking spaces. An irrigated lawn surrounds the existing cemetery plots, and a smaller lawn surrounds the rear of the shed (i.e., the non-service entrance side). Native oak/gray pine woodland habitat surrounds the cemetery.

No new landscaping installation or modification is proposed. Native habitat in the vicinity of the project would not be disturbed. No built footprint would be expanded.

Fencing

An existing brick and wrought iron fence demarcate the boundary to the cemetery from the Forrest Street side, but does not extend the length of Mormon Street. A wooden fence currently separates the front side of the shed (facing the access road) from the back side and extends both north and south of the shed. The fencing south of the shed further extends to block the access road and restrict access between the lawn to the west of the shed and the access road to the east of the shed.

Grading

No grading of the site would be required.

3.4 City Regulation of Urban Development

General Plan

The City of Folsom updated and adopted its current comprehensive General Plan in August 2018. The General Plan is a long-term planning document that guides growth and land development in the City. It provides the foundation for establishing community goals and supporting policies, and directs appropriate land uses for all land parcels within the City. The project site is designated as Open Space (OS) in the City of Folsom General Plan. It is also within the Historic District and within a Sacramento Area Council of Governments (SACOG) Transit Priority Area.

Zoning Ordinance

Developed land uses in the City of Folsom are regulated specifically by the City's Zoning Code (Title 17 of the City's Municipal Code), in addition to the other adopted regulations and programs that apply to all proposed development within the City. In more detail than the General Plan, the Zoning Code regulates land uses on a parcel-by-parcel basis throughout the City. In order to achieve this regulation, the City assigns each parcel within the City to a zoning district, such as a district for single-family homes. Regulations for each district apply equally to all properties within the district.

The project site is currently within the Open Space/Public Primary Area of the Historic District (OS/P), with an underlying zoning of Open Space and Conservation (OSC). The applicant is seeking a Conditional Use Permit from the City to authorize their installation of a crematory.

3.5 Other City Regulation of Urban Development

The City of Folsom further regulates urban development through standard construction conditions and through mitigation, building, and construction requirements set forth in the Folsom Municipal Code. Required of all projects constructed throughout the City, compliance with the requirements of the City's standard conditions and the provisions of the Municipal Code avoids or reduces many potential environmental effects. City procedures to minimize negative environmental effects and disruptions include an analysis of existing features, responsible agency and public input to the design process, engineering and design standards, and construction controls. The activities that mitigate typical environmental impacts to be implemented by the City during the project review, design, and construction phases are described in greater detail below.

Community Development Department Standard Construction Conditions

The City's standard construction requirements are set forth in the City of Folsom, Community Development Standard Construction Specifications updated in February of 2020. A summary of these requirements is set forth below and incorporated by reference into the project description. Copies of these documents may be reviewed at the City of Folsom, Community Development Department, 50 East Natoma Street, Folsom, California 95630.

The Department's standard construction specifications are required to be adhered to by any contractor constructing a public or private project within the City.

Use of Pesticides – Requires contractors to store, use, and apply a wide range of chemicals consistent with all local, state, and federal rules and regulations.

Air Pollution Control – Requires compliance with all Sacramento Metropolitan Air Quality Management District (SMAQMD) and City air pollution regulations.

Water Pollution – Requires compliance with City water pollution regulations, including National Pollutant Discharge Elimination System (NPDES) provisions.

Noise Control – Requires that all construction work comply with the Folsom Noise Ordinance (discussed further below), and that all construction vehicles be equipped with a muffler to control sound levels.

Naturally Occurring Asbestos – Requires compliance with all SMAQMD and City air pollution regulations, including preparation and implementation of an Asbestos Dust Mitigation Plan consistent with the requirements of Section 93105 of the State Government Code.

Weekend, Holiday, and Night Work – Prohibits construction work during evening hours, or on Sunday or holidays, to reduce noise and other construction nuisance effects.

Public Convenience – Regulates traffic through the work area, operations of existing traffic signals, roadway cuts for pipelines and cable installation, effects to adjacent property owners, and notification of adjacent property owners and businesses.

Public Safety and Traffic Control – Regulates signage and other traffic safety devices through work zones.

Existing Utilities – Regulates the relocation and protection of utilities.

Preservation of Property – Requires preservation of trees and shrubbery and prohibits adverse effects to adjacent property and fixtures.

Cultural Resources – Requires that contractors stop work upon the discovery of unknown cultural or historic resources, and that an archaeologist be retained to evaluate the significance of the resource and to establish mitigation requirements, if necessary.

Protection of Existing Trees – Specifies measures necessary to protect both ornamental and native oak trees.

Clearing and Grubbing – Specifies protection standards for signs, mailboxes, underground structures, drainage facilities, sprinklers and lights, trees and shrubbery, and fencing. Also requires the preparation of a Stormwater Pollution Prevention Plan (SWPPP) to control erosion and siltation of receiving waters.

Reseeding – Specifies seed mixes and methods for reseeded of graded areas.

City of Folsom Municipal Code

The City regulates many aspects of construction and development through requirements and ordinances established in the Folsom Municipal Code. These requirements are summarized in **Table 1** and

incorporated by reference into the project description. Copies of these documents may be reviewed at the City of Folsom, Office of the City Clerk, 50 East Natoma Street, Folsom, California 95630.

Table 1. City of Folsom Municipal Code Regulating Construction and Development

CODE SECTION	CODE NAME	EFFECT OF CODE
8.42	Noise Control	Establishes interior and exterior noise standards that may not be exceeded within structures, including residences; establishes time periods for construction operations.
8.70	Stormwater Management and Discharge Control	Establishes conditions and requirements for the discharge of urban pollutants and sediments to the storm-drainage system; requires preparation and implementation of Stormwater Pollution Prevention Plans.
9.34	Hazardous Materials Disclosure	Defines hazardous materials; requires filing of a Hazardous Material Disclosure Form by businesses that manufacture, use, or store such materials.
9.35	Underground Storage of Hazardous Substances	Establishes standards for the construction and monitoring of facilities used for the underground storage of hazardous substances and establishes a procedure for issuance of permits for the use of these facilities.
12.16	Tree Preservation	Regulates the cutting or modification of trees, including oaks and specified other trees; requires a Tree Permit prior to cutting or modification; establishes mitigation requirements for cut or damaged trees.
13.26	Water Conservation	Prohibits the wasteful use of water; establishes sustainable landscape requirements; defines water use restrictions.
14.19	Energy Code	Adopts the California Energy Code, 2010 Edition, published as Part 6, Title 24, C.C.R. to require energy efficiency standards for structures.
14.20	Green Building Standards Code	Adopts the California Green Building Standards Code (CALGreen Code), 2010 Edition, excluding Appendix Chapters A4 and A5, published as Part 11, Title 24, C.C.R. to promote and require the use of building concepts having a reduced negative impact or positive environmental impact and encouraging sustainable construction practices.
14.29	Grading Code	Requires a grading permit prior to the initiation of any grading, excavation, fill or dredging; establishes standards, conditions, and requirements for grading, erosion control, stormwater drainage, and revegetation.
14.32	Flood Damage Prevention	Restricts or prohibits uses that cause water or erosion hazards, or that result in damaging increases in erosion or in flood heights; requires that uses vulnerable to floods be protected against flood damage; controls the modification of floodways; regulates activities that may increase flood damage or that could divert floodwaters.

4.0 PROJECT OBJECTIVES

The project objectives, as expressed by the applicant, are to:

- Provide cremation services for those who currently live in and around Folsom, as no such services currently exist in the City;
- Provide cremation services for members of the population whose customs or religions require such practices;
- Prepare for an increase in the demand for cremation services as cremations become more popular in California and as Folsom's population grows;
- Upgrade existing facilities to capitalize on a business opportunity that has proven successful for the applicant elsewhere in California.

5.0 REQUIRED APPROVALS

A listing and brief description of the project approvals required to implement the proposed project is provided below. This environmental document is intended to address the environmental impacts associated with all the following decision actions and approvals:

- Conditional Use Permit

The City of Folsom has the following discretionary powers related to the proposed project:

- **Certification of the environmental document:** The Folsom City Council will act as the lead agency as defined by the California Environmental Quality Act (CEQA) and will have authority to determine if the environmental document is adequate under CEQA.
- **Approval of project:** The Folsom City Council will consider approval of the project and all entitlements as described above.

6.0 PREVIOUS RELEVANT ENVIRONMENTAL ANALYSIS

6.1 City of Folsom General Plan

The Program EIR for the City of Folsom General Plan (2018) provides relevant policy guidance for this environmental analysis. The EIR evaluated the environmental impacts that could result from implementation of the City of Folsom 2035 General Plan (2035 General Plan) (City of Folsom 2018a). The Program EIR is intended to provide information to the public and to decision makers regarding the potential effects of adoption and implementation of the 2035 General Plan, which consists of a comprehensive update of Folsom's current General Plan. The 2035 General Plan consists of a policy document, including Land Use and Circulation Diagrams.

6.2 Tiering

“Tiering” refers to the relationship between a Program EIR (where long-range programmatic cumulative impacts are the focus of the environmental analysis) and subsequent environmental analyses such as the subject document, which focus primarily on issues unique to a smaller project within the larger program or plan. Through tiering a subsequent environmental analysis can incorporate, by reference, discussion that summarizes general environmental data found in the Program EIR that establishes cumulative impacts and mitigation measures, the planning context, and/or the regulatory background. These broad-based issues need not be reevaluated subsequently, having been previously identified and evaluated at the program stage.

Tiering focuses the environmental review on the project-specific significant effects that were not examined in the prior environmental review, or that are susceptible to substantial reduction or avoidance by specific revisions in the project, by the imposition of conditions or by other means. Section 21093(b) of the Public Resources Code requires the tiering of environmental review whenever feasible, as determined by the Lead Agency.

In the case of the proposed project, this Initial Study tiers from the Program EIR for the City of Folsom 2035 General Plan. The Folsom 2035 General Plan is a project that is related to the proposed project and, pursuant to §15152(a) of the State CEQA Guidelines, tiering of environmental documents is appropriate. State CEQA Guidelines §15152(e) specifically provides that:

“[w]hen tiering is used, the later EIRs or Negative Declarations shall refer to the prior EIR and state where a copy of the prior EIR may be examined. The later [environmental document] should state that the Lead Agency is using the tiering concept and that the [environmental document] is being tiered with the earlier EIR.”

The above-mentioned Program EIR and this Initial Study can be reviewed at the following location:

City of Folsom
Community Development Department
50 East Natoma Street
Folsom, CA 95630
Contact: Mr. Josh Kinkade, Associate Planner
(916) 461-6209

6.3 Incorporation of the Folsom General Plan and East Area Facilities Plan EIRs by Reference

The Program EIR for the Folsom 2035 General Plan is a comprehensive document. Due to various references to the Folsom 2035 General Plan Program EIR in this proposed project, and to its importance relative to understanding the environmental analysis that has occurred to date with respect to development in the Folsom area, the program EIR document is hereby incorporated by reference pursuant to CEQA Guidelines Section 15150.

6.4 Summary of Folsom 2035 General Plan EIR

The 2035 General Plan Program EIR focused on the secondary or indirect effects of implementing the 2035 General Plan. Indirect physical changes to the environment (impacts) that could result from

implementation of 2035 General Plan are addressed in the appropriate technical chapters of the Program EIR. Likewise, inconsistency with an adopted plan, in general, is not considered a direct physical impact to the environment, but may result in impacts, which are discussed in the appropriate technical chapters. According to this definition, potential secondary or indirect environmental effects may be divided into two broad classes:

- Coverage Impacts - Those that result from development or other activities covering land or otherwise physically interfering with a resource (e.g., constructing a paved parking lot over a sensitive biological resource); and,
- Intensity Impacts - Those that result from increased levels of human activity (e.g., increases in traffic levels leading to increased emissions of criteria air pollutants).

The 2035 General Plan does not identify any additional areas designated for urban uses beyond those set forth in the 1988 General Plan as amended through fall 2017. Therefore, the environmental analysis concentrates its evaluation on those undeveloped areas designated for urban uses and the resources still present within them, including within the Folsom Plan Area Specific Plan (FPASP) area, south of Highway 50.

Coverage Impacts

These impacts are based on the conversion of existing vacant parcels to a developed land use. Conversion can result in the eradication of, or damage to, a resource, revealing of environmental conditions detrimental to a developed land use, or exposure of the developed use to an existing environmental hazard. For the purposes of evaluating these effects, the Program EIR assumed that all land identified for urban uses in the 2035 General Plan would be developed with such uses within the 20-year planning horizon.

For areas designated for urban or infrastructure uses by the 2035 General Plan, potential coverage effects for certain environmental topics were assessed in a multi-step process. Quantitative evaluations began with a review of resources potentially affected by the implementation of the 2035 General Plan project, and the areal extent of identified resources.

To determine the locations where a resource could be converted to developed uses under the proposed 2035 General Plan, an inventory of each environmental resource within each urban area project boundary was completed. Using geospatial data, or geographic information systems (GIS), all parcels or lots within the 2035 Plan Evaluation Area were identified as developed or vacant. Vacant parcels were further identified as being located north of Highway 50, or south of Highway 50 within the FPASP area.

For vacant parcels north of Highway 50, the analysis identified 453 total vacant parcels encompassing 441 acres. Of these 453 parcels, 377 are lots within existing single-family residential subdivisions totaling 163 acres, with a gross median lot size of 16,125 square feet. Of the remaining 76 parcels, the majority are designated for commercial or multi-family uses. For these uses, the total acreage is 278 acres with a gross median parcel size of 37,150 square feet. Once the 453 parcels were identified, each was evaluated using aerial photographs to determine its condition. As evidenced on the aerial photographs, the overwhelming majority of both the single-family residential and commercial/multi-family residential parcels are remnant areas within subdivisions or larger development projects, and most have been disturbed by prior rough grading and/or the construction of roads and utilities.

There are a total of 3,336 acres in the FPASP area south of Highway 50, of which 1,118 acres would remain in open space. The remaining 2,218 acres would be developed with a variety of urban land uses and supporting infrastructure. Although potential environmental impacts could occur throughout the 2035 Plan Evaluation Area, the majority of the land available for new development of urban uses (77 percent of the citywide total or 2,218 acres) would be located within the FPASP area.

The possibility of potential coverage impacts was determined by layering maps of sensitive resources (e.g., sensitive species, areas of naturally occurring asbestos, flood hazards) over the map of vacant parcels. The results of this type of analysis are reported in the following chapters of the PEIR: 6. Aesthetics and Visual Resources, 7. Agricultural and Forestry Resources, 9. Biological Resources, 10. Cultural Resources, 11. Geology, Soils, and Mineral Resources, 13. Hazards and Hazardous Materials, 14. Hydrology and Water Quality, and 18. Tribal Cultural Resources.

Intensity Impacts

Intensity impacts, such as those for traffic, air quality, greenhouse gas emissions, and noise, depend upon both the location and level of human activity. Other impacts, such as those to public services and utilities depend upon the size of the served population.

The 2035 General Plan proposed no increases in the amount of land identified for urban uses beyond that currently identified in the 1988 General Plan as amended. However, the development of urban uses on vacant land designated and available for residential and employment uses would result in an increase in the number of people and jobs in the City over existing (2015/2017) conditions. For intensity impacts, the PEIR evaluated a forecast of 2035 conditions consistent with the land uses identified in the 2035 General Plan.

The 2035 development forecast is based on a buildout model for use in the analysis of future traffic conditions. Summarily, the buildout model forecasts full development of all planned land uses within the existing city limits, full buildout of the Easton and Glenborough projects as approved by Sacramento County, and background land use assumptions outside of the City, Glenborough, and Easton consistent with the land use assumptions of Sacramento Area Council of Governments (SACOG) Metropolitan Transportation Plan/Sustainable Communities Strategy (MTP/SCS). Because the MTP/SCS forecasts conditions for the year 2036, the buildout model used in the Program EIR interpolates 2035 conditions, the horizon year for the proposed Folsom General Plan.

As with the Coverage Impact analysis, the Intensity Impact Analysis focused on the difference between the location and level of human activity currently existing (2015/2017), and the level of activity that would exist with implementation of the 2035 General Plan. The results of this type of analysis are reported in the following chapters of the Program EIR: 8. Air Resources, 12. Global Climate Change, 15. Noise and Vibration, 16. Public Services and Recreation, 17. Transportation, and 19. Utilities and Service Systems.

7.0 ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that may require mitigation to reduce the impact from "Potential Impact" to "Less than Significant" as indicated by the checklist on the following pages.

An Initial Study is conducted by a Lead Agency to determine if a project may have a potentially significant effect on the environment (CEQA Guidelines Section 15063). An Environmental Impact Report (EIR) must be prepared if an Initial Study indicates that further analysis is needed to determine whether a significant impact will occur or if there is substantial evidence in the record that a project may have a significant effect on the environment (CEQA Guidelines Section 15064(f)).

<input type="checkbox"/> Aesthetics	<input type="checkbox"/> Agriculture/Forestry Resources	<input checked="" type="checkbox"/> Air Quality
<input type="checkbox"/> Biological Resources	<input checked="" type="checkbox"/> Cultural Resources	<input type="checkbox"/> Energy
<input type="checkbox"/> Geology/Soils	<input type="checkbox"/> Greenhouse Gas Emissions	<input type="checkbox"/> Hazards/Hazardous Materials
<input type="checkbox"/> Hydrology/Water Quality	<input type="checkbox"/> Land Use/Planning	<input type="checkbox"/> Mineral Resources
<input type="checkbox"/> Noise	<input type="checkbox"/> Population/Housing	<input type="checkbox"/> Public Services
<input type="checkbox"/> Recreation	<input type="checkbox"/> Transportation	<input checked="" type="checkbox"/> Tribal Cultural Resources
<input type="checkbox"/> Utilities/Service Systems	<input type="checkbox"/> Wildfire	<input type="checkbox"/> Mandatory Findings of Significance

8.0 DETERMINATION (TO BE COMPLETED BY THE LEAD AGENCY)

On the basis of this initial evaluation:

<input type="checkbox"/>	I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
<input checked="" type="checkbox"/>	I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
<input type="checkbox"/>	I find that the proposed project MAY have a significant effect on the environment, and an environmental impact report is required.
<input type="checkbox"/>	I find that the proposed project MAY have a "potential impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
<input type="checkbox"/>	I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.



Signature

Josh Kinkade

Printed Name:

1/3/21

Date

City of Folsom

For:

9.0 ENVIRONMENTAL INITIAL STUDY CHECKLIST

Responses to the following questions and related discussion indicate if the proposed project will have or will potentially have a significant adverse impact on the environment, either individually or cumulatively with other projects. All phases of project planning, implementation, and operation are considered. Mandatory Findings of Significance are located in Section 9.XXI below.

- A. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- B. "Less Than Significant with Mitigation" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Than Significant Impact." The lead agency must describe the mitigation measures and briefly explain how they reduce the effect to a less than significant level (mitigation measures from earlier analyses may be cross-referenced).
- C. "Less Than Significant Impact" applies where the project creates no significant impacts, only less than significant impacts.
- D. "No Impact" applies where a project does not create an impact in that category. "No Impact" answers do not require an explanation if they are adequately supported by the information sources cited by the lead agency which show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project would not expose sensitive receptors to pollutants, based on a project specific screening analysis).

I. AESTHETICS

AESTHETICS:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Except as provided in Public Resources Code Section 21099, would the project:				
a) Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The project site is currently a small cemetery, with associated landscaping, outbuildings, and access roads. Lands to the south and west contain woodland habitat typical of riparian communities in the Sierra Nevada foothills. Soils at the project site are comprised of dredge tailings and other fill material. Tailing piles between the site and Folsom Boulevard prevent the site from being visible from that street. To the west is also the Jedidiah Smith Memorial Trail that runs along the eastern shore of Lake Natoma. To the north is a small residential neighborhood with single family dwellings. Folsom Boulevard runs in a north/south line just east of the property. East of Folsom Boulevard is a large, developed area containing single family homes, apartment complexes, a mobile home park, and some small businesses. The more regional setting is primarily characterized by residential development with a commercial shopping center to the east.

The project would be located in an existing metal shed on the grounds of the existing Lakeside Memorial Lawn Cemetery. The shed can be reached by following Mormon Street to its terminus, making a slight left turn, and continuing for approximately 100 feet down an access road. The proposed project includes the installation of one HCT Apex-250 crematory manufactured by Hartwick Combustion. The shed would be modified to accommodate this device, but its footprint would not be expanded. It currently covers 1,071 square feet. A small exhaust stack would be added to the roof of the shed. This stack would be approximately 19.5 feet above grade, and would project approximately 10 feet above the existing roof of the shed. The crematory would be placed in the northwest corner of the shed. Two 250-gallon propane tanks would be installed on a proposed concrete pad along the northern side of the shed to provide power for the crematorium, as no gas lines currently exist. An existing wooden fence would shield these tanks from view from the publicly used areas of the cemetery.

Existing landscaping at the cemetery and around the shed consists of mature broad-leaved, coniferous, and palm trees. These trees also provide shade for much of the cemetery and many of the parking spaces. An irrigated lawn surrounds the existing cemetery plots and a smaller lawn surrounds the rear of the shed (i.e., the non-service entrance side). Native oak/gray pine woodland habitat exists surrounding the cemetery. No new landscaping installation or modification is proposed. Native habitat in the vicinity of the project would not be disturbed. No built footprint would be expanded.

An existing brick and wrought iron fence marks the edge of the cemetery from the Forrest Street side, but does not extend the length of Mormon Street. A wooden fence currently separates the front side of the shed (facing the access road) from the back side and extends both north and south of the shed. The fencing south of the shed further extends to block the access road and restrict access between the lawn to the west of the shed and the access road to the east of the shed. No changes to fencing are proposed as part of this action.

The access side of the shed that would be used for crematory operations is shielded from view from the rest of the cemetery by wooden fencing. A gravel berm shields views of the shed from the east, including from Folsom Boulevard. No external modifications to the shed are proposed.

Evaluation of Aesthetics

a) Have a substantial adverse effect on a scenic vista?

No Impact. Neither the project site nor the surrounding areas are scenic vistas due to the presence of existing nearby commercial and residential developments. Further, neither the project site, nor views to or from the project site, have been designated as important scenic resources by the City of Folsom or any other public agency. Additionally, the site of proposed modifications is already largely shielded from public view and would remain so. Therefore, the proposed development would not interfere with or degrade a scenic vista, and no impact would occur.

b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?

No Impact. There are no state or locally designated scenic highways in the vicinity of the proposed project site (Caltrans 2020). Implementation of the proposed project would not adversely affect scenic resources within a designated scenic highway, and no impact would occur.

c) In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

Less than Significant Impact. The crematory would be placed inside a shed that already exists on the property and that is already mostly shielded from public view. The only external modifications would be the addition of two 250-gallon propane tanks on a concrete pad near the edge of the building and the addition of a small exhaust stack to the roof of the shed. Given that external modifications would be very minor and that the building is already mostly shielded from view, any impacts would be less than significant.

- d) Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

No Impact. The project would not result in any external glow or light source. No impact would occur.

II. AGRICULTURE AND FORESTRY RESOURCES

AGRICULTURE AND FORESTRY RESOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non- forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

No agricultural activities or timber management occur on the project site or in adjacent areas, and the project site is not designated for agricultural or timberland uses. The California Important Farmland Finder classifies the project site as “Urban and Built Up” and “Other Land” (i.e., not farmland or potential farmland) (CDC 2020c).

The Natural Resources Conservation Service (NRCS) soil survey report generated for the project site indicates that no Prime or Unique Farmland or Farmland of Statewide Importance occurs on the project site (NRCS 2020).

Evaluation of Agriculture and Forestry Resources

- a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

No Impact. The project site is not designated as Prime Farmland, Unique Farmland, or Farmland of Statewide importance (Farmland), pursuant to the California Important Farmland Finder (CDC 2020c). Therefore, no impact would occur.

b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?

No Impact. The project site is not zoned for agricultural use or enacted into a Williamson Act contract. Therefore, no impact would occur.

c) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use?

No Impact. The project site is not zoned or designated as farmland, and the surrounding land uses are primarily residential developments and open space as part of an urban greenbelt. Therefore, the nature and location of the project would not directly or indirectly result in the conversion of Farmland to non-agricultural uses. No impact would occur.

d) Result in the loss of forest land or conversion of forest land to non-forest use?

No impact. No changes to the landscape are proposed, no removal of trees is proposed, and no expansion of a building footprint is proposed. No impact would occur.

e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?

No Impact. No changes to the landscape are proposed, no removal of trees is proposed, and no expansion of a building footprint is proposed. No impact would occur.

III. AIR QUALITY

AIR QUALITY:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Where available, the significance criteria established by the applicable air quality management district or air pollution control district may be relied upon to make the following determinations. Would the project:				
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

The Air Quality section of this document is based upon the approach, methodology, results, and conclusions outlined in the project-specific Air Quality and Greenhouse Gas Assessment (HELIX 2020) and the subsequent addendum analysis (HELIX 2021); both documents were prepared by HELIX Environmental Planning, Inc. and are included as **Appendix B**.

Environmental Setting

The City of Folsom lies within the Sacramento Valley Air Basin (SVAB), near the southeastern edge. The SVAB consists of all or parts of eleven counties spanning from Solano and Sacramento counties in the south to Shasta County in the north. The Sacramento Metropolitan Air Quality Management District (SMAQMD) is responsible for implementing emissions standards and other requirements of federal and state laws for Sacramento County, including the project area.

The climate of the SVAB is characterized by hot, dry summers and mild, rainy winters. During the year, the temperature may range from 20 to 115 degrees Fahrenheit with summer highs usually in the 90s and winter lows occasionally below freezing. Average annual rainfall is about 20 inches with snowfall being very rare. The prevailing winds are moderate in strength and vary from moist breezes from the south to dry land flows from the north. The mountains surrounding the Sacramento Valley create a barrier to airflow, which can trap air pollutants in the valley when certain meteorological conditions are present, and a temperature inversion (areas of warm air overlying areas of cooler air) exists. Air stagnation in the autumn and early winter occurs when large high-pressure cells lie over the valley. The lack of surface wind during these periods and the reduced vertical flow caused by less surface heating reduces the influx of outside air and allows pollutants to become concentrated in the air. The surface concentrations of pollutants are highest when these conditions are combined with increased levels of smoke or when temperature inversions trap cool air, fog, and pollutants near the ground. The ozone season (May through October) in the SVAB is characterized by stagnant morning air or light winds with

the breeze arriving in the afternoon out of the southwest from the San Francisco Bay. Usually the evening breeze transports the airborne pollutants to the north out of the SVAB. During about half of the days from July to September, however, a phenomenon called the “Schultz Eddy” prevents this from occurring. Instead of allowing for the prevailing wind patterns to move north carrying the pollutants out of the valley, the Schultz Eddy causes the wind pattern and pollutants to circle back southward. This phenomenon’s effect exacerbates the pollution levels in the area and increases the likelihood of violating the federal and state air quality standards (SMAQMD 2020a).

Regulatory Setting

Criteria Pollutants

Ambient air quality is described in terms of compliance with state and national standards, and the levels of air pollutant concentrations considered safe, to protect the public health and welfare. These standards are designed to protect people most sensitive to respiratory distress, such as asthmatics, the elderly, very young children, people already weakened by other disease or illness, and persons engaged in strenuous work or exercise. The U.S. Environmental Protection Agency (USEPA), the federal agency that administrates the Federal Clean Air Act of 1970, as amended in 1990, has established national ambient air quality standards (NAAQS) for several air pollution constituents known as criteria pollutants, including: ozone (O₃); carbon monoxide (CO); coarse particulate matter (PM₁₀; particles 10 microns or less) and fine particulate matter (PM_{2.5}; particles 2.5 microns or less); sulfur dioxide (SO₂); and lead (Pb). As permitted by the Clean Air Act, California has adopted the more stringent California ambient air quality standards (CAAQS) and expanded the number of regulated air constituents. Ground-level ozone is not emitted directly into the environment but is generated from complex chemical and photochemical reactions between precursor pollutants, primarily reactive organic gases (ROGs; also known as volatile organic compounds [VOC]),¹ and oxides of nitrogen (NO_x). PM₁₀ and PM_{2.5} are generated from a variety of sources, including road dust, diesel exhaust, fuel combustion, tire and brake wear, construction operations and windblown dust. In addition, PM₁₀ and PM_{2.5} can also be formed through chemical and photochemical reactions of precursor pollutants in the atmosphere.

The California Air Resources Board (CARB) is required to designate areas of the state as attainment, nonattainment, or unclassified for the ambient air quality standards. An “attainment” designation for an area signifies that pollutant concentrations do not violate the standard for that pollutant in that area. A “nonattainment” designation indicates that a pollutant concentration violated the standard at least once. An “unclassified” designation indicates that insufficient data was available to determine the status. The air quality attainment status of Sacramento County is shown in **Table 2**.

Table 2. Sacramento County Attainment Status

Pollutant	State of California Attainment Status	Federal Attainment Status
Ozone (1-hour)	Nonattainment	No Federal Standard
Ozone (8-hour)	Nonattainment	Nonattainment
Coarse Particulate Matter (PM ₁₀)	Nonattainment	Attainment
Fine Particulate Matter (PM _{2.5})	Attainment	Nonattainment

¹ CARB defines and uses the term ROGs while the USEPA defines and uses the term VOCs. The compounds included in the lists of ROGs and VOCs and the methods of calculation are slightly different. However, for the purposes of estimating criteria pollutant precursor emissions, the two terms are often used interchangeably.

Pollutant	State of California Attainment Status	Federal Attainment Status
Carbon Monoxide (CO)	Attainment	Attainment
Nitrogen Dioxide (NO ₂)	Attainment	Attainment
Lead	Attainment	Attainment
Sulfur Dioxide (SO ₂)	Attainment	Attainment
Sulfates	Attainment	No Federal Standard
Hydrogen Sulfide	Unclassified	No Federal Standard
Visibility Reducing Particles	Unclassified	No Federal Standard

Sources: SMAQMD 2020a.

Sacramento County is designated as nonattainment for the state and federal ozone standards, the state PM₁₀ standards, and the federal PM_{2.5} standards. The SMAQMD is responsible for implementing emissions standards and other requirements of federal and state laws in Sacramento County. Attainment plans for meeting the federal air quality standards are incorporated into the State Implementation Plan (SIP), which is subsequently submitted to the USEPA, the federal agency that administrates the Federal CAA of 1970, as amended in 1990. The current air quality plan applicable to the project, the *Sacramento Regional 2008 NAAQS 8-Hour Ozone Attainment and Reasonable Further Progress Plan* (Regional Ozone Plan), was developed by the SMAQMD and adjacent air district to describe how the air districts in and near the Sacramento metropolitan area will continue the progress toward attaining state and national ozone air quality standards (SMAQMD 2017).

Toxic Air Contaminants

Toxic air contaminants (TAC) are a diverse group of air pollutants that may cause or contribute to an increase in deaths or in serious illness or that may pose a present or potential hazard to human health. TACs can cause long-term chronic health effects such as cancer, birth defects, neurological damage, asthma, bronchitis, or genetic damage, or short-term acute effects such as eye watering, respiratory irritation (a cough), runny nose, throat pain, and headaches. TACs are considered either carcinogenic or noncarcinogenic based on the nature of the health effects associated with exposure to the pollutant. For carcinogenic TACs, there is no level of exposure that is considered safe and impacts are evaluated in terms of overall relative risk expressed as excess cancer cases per one million exposed individuals. Noncarcinogenic TACs differ in that there is generally assumed to be a safe level of exposure below which no negative health impact is believed to occur. These levels are determined on a pollutant-by-pollutant basis.

The Health and Safety Code (§39655[a]) defines TAC as “an air pollutant which may cause or contribute to an increase in mortality or in serious illness, or which may pose a present or potential hazard to human health.” All substances that are listed as hazardous air pollutants pursuant to subsection (b) of Section 112 of the CAA (42 United States Code Sec. 7412[b]) are designated as TACs. Under State law, the California Environmental Protection Agency (CalEPA), acting through CARB, is authorized to identify a substance as a TAC if it determines the substance is an air pollutant that may cause or contribute to an increase in mortality or an increase in serious illness, or that may pose a present or potential hazard to human health.

Crematoriums are a potential source of TACs as a result of trace metals and organic compounds that accumulate in the body throughout a person’s life and are released during combustion of human remains, and as a result of trace organic compounds that are formed in the combustion process. These TACs include: metals and inorganics (i.e., arsenic, beryllium, cadmium, chromium, copper, hydrogen

fluoride, lead, mercury, nickel, selenium, zinc); VOCs (i.e., benzene, toluene, xylenes, vinyl chloride); aldehydes (i.e., acetaldehyde, formaldehyde); polyaromatic hydrocarbons (PAHs); polychlorinated dibenzodioxins (dioxins; PCDD); and polychlorinated dibenzofurans (furans; PCD). Prolonged exposure to significant concentrations of these TACS can result in a variety of adverse health effects including cancers, chronic conditions, and/or acute conditions, depending on the substance and level of exposure. Based on the results of the Health Risk Assessment (HRA), described below, hexavalent chromium and mercury are the primary drivers of the health risks from crematory emissions because the health risks from crematory emissions of these substances are one or more orders of magnitude greater than the health risks from other TACs in crematory emissions.

Increased Cancer Risks – Hexavalent Chromium. Hexavalent chromium is a toxic form of the element chromium. Hexavalent chromium compounds are man-made and widely used in many different industries. Prolonged exposure to airborne hexavalent chromium may result in lung cancer. Although exposure to high levels of airborne hexavalent chromium may result in irritation or damage to the nose, throat, and lungs, breathing small amounts of hexavalent chromium even for long periods does not cause respiratory tract irritation in most people (Occupational Safety and Health Administration [OSHA] 2006).

Non-Cancer Chronic and Acute Health Risks – Mercury. Mercury is a naturally occurring element that is found in its elemental form (commonly known as quicksilver), in organic compounds which accumulate in fish and shellfish, and in inorganic compounds mainly occurring in contaminated drinking water. Mercury is a neurotoxin that can result in a range of chronic neurological disorders and developmental issues. The specific health effects of mercury are dependent on the form and amount of mercury in the exposure, the duration of the exposure, and the age of the individual (USEPA 2020b).

Sensitive Receptors

Some land uses are considered more sensitive to air pollution than others due to the types of population groups or activities involved and are referred to as sensitive receptors. Examples of these sensitive receptors are residences, schools, hospitals, and daycare centers. CARB and the Office of Environmental Health Hazard Assessment (OEHHA) have identified the following groups of individuals as the most likely to be affected by air pollution: the elderly over 65, children under 14, infants (including in utero in the third trimester of pregnancy), and persons with cardiovascular and chronic respiratory diseases such as asthma, emphysema, and bronchitis (CARB 2005; OEHHA 2015).

Residential areas are considered sensitive receptors to air pollution because residents (including children and the elderly) tend to be at home for extended periods of time, resulting in sustained exposure to any pollutants present. Children and infants are considered more susceptible to health effects of air pollution due to their immature immune systems, developing organs, and higher breathing rates. As such, schools are also considered sensitive receptors, as children are present for extended durations and engage in regular outdoor activities.

The closest existing sensitive receptors to the project site are multiple single-family residences adjacent to the cemetery to the north, between 450 and 750 feet from the proposed crematory location, and mobile homes across Folsom Boulevard to the east, approximately 700 feet from the proposed crematory location; see Figure 3, *Receptor Locations*, attached to the air quality report. The closest schools to the project site are the Folsom Montessori School approximately 3,200 feet (0.6 miles) to the northeast and the Golden Valley Charter River School. That school is located across Lake Natoma from

the project site and, though it is approximately 2.7 miles away by car, its physical location is about 3,000 feet (just over one-half mile) northwest of the project site.

Methods

Criteria Pollutant Emissions

Criteria pollutant and precursor emissions for long-term operation of the proposed crematory were calculated using propane combustion emissions factors from the USEPA AP-42 Compilation of Emissions Factors Chapter 1.5 (USEPA 2008), and crematory emissions factors provided by the SMAQMD, which combined USEPA AP-42 data and the USEPA Factor Information Retrieval Program (SMAQMD 2020b).

Crematory Health Risks

Potential health risks to nearby sensitive receptors from the emission of TACs during operation of the proposed crematory were analyzed after consultation with the SMAQMD and in accordance with the OEHHA Air Toxics Hot Spots Program Guidance Manual for Preparation of Health Risk Assessments (OEHHA 2015).

TAC Emissions

Toxic emissions from the cremation process were estimated based on emissions factors provided by the SMAQMD and on maximum cremation process rates provided by Caring Service Group of 200 pounds per hour and 100,000 pounds per year. The TAC emissions factors provided by SMAQMD were based on data in a test report from CARB that measured emissions from two propane-fired crematories (SMAQMD 2020b)

Dispersion Modeling

Localized concentrations of TACs were modeled using Lakes AERMOD View version 9.8.3. The Lakes program utilizes USEPA's AERMOD gaussian air dispersion model version 19191. Plot files from AERMOD using unitized emissions (one gram per second) from the crematory stack were imported into CARB's Hotspots Analysis and Reporting Program (HARP), Air Dispersion Modeling and Risk Tool (ADMRT) version 19121. The ADMRT calculated ground-level concentrations of TACs utilizing the imported plot files and the annual and hourly emissions inventory (provided in detail in Attachment A to the Air Quality report).

Source Parameters

Based on data provided by the crematory manufacturer, emissions from the proposed crematory were modeled as a point source emitting from the exhaust stack at 19.5 feet above the ground. The stack diameter was set at 20 inches, the exhaust gas temperature was set to 1080 degrees Fahrenheit (°F), the gas exit velocity was set to 14.7 feet per second, and the stack was assumed to have a rain cap resulting in a near-zero initial vertical gas velocity. Downwash from the existing shed housing the proposed crematory was modeled using the Building Profile Input Program (BPIP – a building preprocessing program for AERMOD).

Meteorological Data

SMAQMD provides pre-processed meteorological data suitable for use with AERMOD (SMAQMD 2014) for projects within Sacramento County. The available data set most representative of conditions in the project vicinity was from the Sacramento Executive Airport station, approximately 19 miles southwest of the project site. The Sacramento Executive Airport set includes 5 years of data collected between 2010 to 2014. Rural dispersion coefficients were selected in the model to reflect the existing undeveloped and open nature of the immediate project vicinity. A wind rose for the Sacramento Executive Airport shows an average speed of 6.6 miles per hour from the south (Iowa Environmental Mesonet 2019). The wind rose graphic is included in Attachment B to the air quality report.

Terrain Data

United States Geological Survey (USGS) National Elevation Dataset (NED) files with a 10-meter resolution covering an area approximately 500 meters (1,640 feet) around the project site were used in the model to cover the analysis area. Terrain data was imported to the model using AERMAP (a terrain preprocessing program for AERMOD).

Receptor Modeling

To develop risk isopleths (linear contours showing equal level of risk) and ensure that the area of maximum impact was captured, receptors were placed in a cartesian grid 690 meters by 490 meters (approximately 2,264 feet by 1,608 feet), centered on the proposed crematory with a grid spacing of 10 meters (33 feet) and a receptor height (flagpole height) of 1.2 meters (4 feet) above the ground. Additional discrete receptors were placed at the residential property line of the 37 closest identified sensitive receptors and the 4 closest off-site worker buildings. See Figure 3 for the discrete receptor locations relative to the TAC source.

Risk Determination

Health risks resulting from localized concentration of TACs emitted by the proposed crematory were estimated using the ADMRT. The latest cancer slope factors, chronic Recommended Exposure Limits (REL), acute RELs and exposure paths for all TACs, as designated by CARB, are included in the ADMRT. For the residential cancer risk, an exposure duration of 30 years was selected in accordance with the OEHHA (2015) guidelines. In accordance with OEHHA guidelines, the model conservatively assumes that residents would be standing and breathing outdoors at the location of the property line closest to the crematory every day between 17 and 21 hours per day (depending on the age group, starting with infants in utero in the third trimester of pregnancy) for 30 years. For off-site worker cancer risk, an exposure duration of 25 years was selected with an assumption of 8 hours per day, 5 days per week of exposure while standing outside. The mandatory minimum exposure pathways and the OEHHA derived breathing intake rate percentile method were selected.

Significance Criteria

The following potential air quality impacts are based on Appendix G of the CEQA Guidelines, a significant impact is identified if the project would result in any of the following:

- a) *Conflict with or obstruct implementation of the applicable air quality plan?*

- b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?
- c) Expose sensitive receptors to substantial pollutant concentrations?
- d) Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

While the final determination of whether or not a project has a significant effect is within the purview of the lead agency pursuant to CEQA Guidelines Section 15064(b), the SMAQMD has adopted screening tables and thresholds which lead agencies can use to determine the significance of a development project's short-term construction and long-term operational pollutant emissions. The SMAQMD's project-level thresholds of significance for mass emissions of criteria pollutant and precursors and exposure to TACs are shown in **Table 3**.

Table 3. SMAQMD Significance Thresholds

Pollutant	Operational Emissions Threshold
ROG	65 pounds per day
NO _x	65 pound per day
PM10	80 pounds per day/14.6 tons per year ¹
PM2.5	82 pounds per day/15 tons per year ¹
TAC Exposure Incremental Increased Cancer Risk	10 in 1 million
TAC Exposure Non-Cancer Hazard Index	1

Source: SMAQMD 2020c

¹ Thresholds for PM is zero unless all feasible best available control technology/best management practices (BACT/BMPs) are applied.

Evaluation of Air Quality

- a) Conflict with or obstruct implementation of the applicable air quality plan?

Less Than Significant Impact. Consistency with the air quality plan is determined by whether the project would hinder implementation of control measures identified in the air quality plan or would result in growth of population or employment that is not accounted for in local and regional planning. The SMAQMD's Regional Ozone Plan and the SIP are the applicable air quality plans for the projects developed within Sacramento County.

The project would be consistent with the General Plan land use designation of Open Space, but the project would require a conditional use permit to install and operate a crematory in the Open Space and Conservation zoning designation of the project site. The project would not result in population growth in the City and employment growth would be limited to a few personnel to operate the crematory. Therefore, the project would be consistent with the local and regional growth assumptions used in developing the Regional Ozone Plan and the SIP. In addition, as described in impact discussion b), below, the project would not result in a cumulatively considerable increase of any criteria pollutant. Therefore, the project would not conflict with or obstruct implementation of the applicable air quality plan and the impact would be less than significant.

- b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is in non-attainment under an applicable federal or state ambient air quality standard?

Construction (Short-Term) Emissions

Less Than Significant Impact with Mitigation Incorporated. Construction of the project would involve the use of a crane for several hours to unload the chiller and crematory from the truck, and the use of a mini excavator or skid steer loader for one day and one truck load of concrete to install a small pad for the two propane tanks.

According to the SMAQMD's CEQA Guide, projects that are 35 acres or less in size generally will not exceed the SMAQMD's construction NO_x or PM thresholds of significance. However, all construction projects regardless of the screening level are required to implement the SMAQMD's Basic Construction Emission Control Practices (also known as Best Management Practices [BMP]; SMAQMD 2020b). The BMPs satisfy the requirements of SMAQMD's Rule 403, *Fugitive Dust*, which requires every reasonable precaution not to cause or allow the emissions of fugitive dust from being airborne beyond the property line from which the emission originates. ROG emissions during construction are generally associated with the application of architectural coatings. The project does not propose any new structures and would not require substantial amounts of painting and would not result in significant emissions of ROGs. Therefore, construction of the project would not result in a cumulatively considerable net increase of any criteria pollutant and the impact would be less than significant with implementation of Mitigation Measure AIR-01.

Mitigation Measure AIR-01: Implement SMAQMD's Basic Construction Emission Control Practices.

City approval of grading and/or improvement plans for the proposed project shall include the following SMAQMD Basic Construction Emission Control Practices:

- All exposed surfaces shall be watered two times daily. Exposed surfaces include, but are not limited to soil piles, graded areas, unpaved parking areas, staging areas, and access roads.
- Cover or maintain at least two feet of free board space on haul trucks transporting soil, sand, or other loose material on the site. Any haul trucks that would be traveling along freeways or major roadways shall be covered.
- Use wet power vacuum street sweepers to remove any visible trackout mud or dirt onto adjacent public roads at least once a day. Use of dry power sweeping is prohibited.
- Limit vehicle speeds on unpaved roads to 15 miles per hour.
- All roadways, driveways, sidewalks, parking lots shall be paved as soon as possible. In addition, building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
- Minimize idling time either by shutting equipment off when not in use or reducing the time of idling to 5 minutes (as required by the state airborne toxics control measure [Title 13, Section 2485 of the California Code of Regulations]). Provide clear signage that posts this requirement for workers at the entrances to the site.

- Maintain all construction equipment in proper working condition according to manufacturer's specifications. The equipment shall be checked by a certified mechanic and determine to be running in proper condition before it is operated.

Operation (Long-Term) Emissions

Less than Significant. The project would result in long-term operational emissions from vehicles that drive to and from the project and from operation of the crematory.

Because there are no crematories currently operating in Folsom, demand for cremation services is filled by transporting the deceased to facilities outside of the City. Therefore, operations of the project would not result in new vehicle trips (nor the associated emissions in the region). Instead, the project would replace existing regional vehicle trips with shorter trips (and reduced associated emissions).

Operation of a propane-fired crematory would be considered a new stationary source of emissions. The project may be subject to SMAQMD's Rule 201, *General Permit Requirements*, and Rule 202, *New Source Review*. The project would be required to implement best available control technology (BACT) for the minimization of emissions. BACT for crematories is incorporated into the product design in the form of controls which ensure maintenance of the correct temperatures and cycle times, and a secondary combustion chamber which ensures oxygenation and complete combustions of all fuels. As described in the Methods sections above, Criteria pollutant and precursor emissions for long-term operation of the proposed crematory were calculated using propane combustion emissions factors from AP-42 and crematory emissions factors provided by SMAQMD. The project's calculated criteria and precursor operational emissions are compared to the SMAQMD thresholds in **Table 4**. A printout of the calculation sheets is included in Attachment A of the air quality report.

Table 4. Operational Criteria Pollutant and Precursor Emissions

Pollutant	Project Emissions	SMAQMD Threshold	Exceed Threshold?
Daily Emissions (pounds per day)			
ROG	0.1	65	No
NO _x	1.2	65	No
CO	0.9	None	No
SO _x	0.4	None	No
PM ₁₀	0.3	80	No
PM _{2.5}	0.3	82	No
Annual Emissions (tons per year)			
ROG	0.01	None	No
NO _x	0.15	None	No
CO	0.11	None	No
SO _x	0.05	None	No
PM ₁₀	0.03	14.6	No
PM _{2.5}	0.03	15	No

Source: SMAQMD 2020b; SMAQMD 2020c

As shown in **Table 4**, the project's operational emissions of criteria pollutants and precursors would not exceed the SMAQMD daily or annual thresholds. Therefore, the project's operational emissions would

not result in a cumulatively considerable net increase of any criteria pollutant and impacts would be less than significant.

c) Expose sensitive receptors to substantial pollutant concentrations?

Less Than Significant Impact. Crematories are a potential source of TACs as a result of trace metals and organic compounds that accumulate in the body and are released during combustion, and trace organic compounds that are formed in the combustion process. An HRA was conducted to determine potential community health risks from exposure to TACs emitted from the proposed crematory, as described in the Methods section above.

Health risks associated with cancer from development projects are estimated using the incremental excess cancer risk expressed as cancer cases per one million exposed individuals. The incremental excess cancer risk is an estimate of the chance a person exposed to specific sources of a TACs may have of developing cancer from that exposure beyond the individual's risk of developing cancer from existing background levels of pollutants in the ambient air. For context, the average cancer risk from TACs in the ambient air for an individual living in an urban area of California is 830 in 1 million (CARB 2015). Cancer risk estimates do not mean, and should not be interpreted to mean, that a person will develop cancer from estimated exposures to toxic air pollutants.

Health risks associated with chronic and acute effects from a development project are quantified using the maximum hazard index. A hazard index is the potential exposure to a substance divided by the reference exposure level (the level at which no adverse effects are expected). A hazard index of less than one indicates no adverse health effects are expected from the potential exposure to the substance. The maximum hazard index is the sum of hazard indices for pollutants with non-cancer health effects that have the same or similar adverse health effects.

The modeled point of maximum impact for the project (geographic point outside of the project site with the highest estimated incremental cancer risk and maximum hazard index) would be a point near the project boundary approximately 96 feet southeast of the proposed crematory exhaust stack, at approximately Universal Transverse Mercator (UTM) coordinates Zone 10, 657982 meters east, 4281757 meters north. The maximum health risk exposure at this point would be a residential incremental cancer risk of 3.2 in 1 million and a residential non-cancer chronic hazard index of 0.09. This point of maximum impact is in an area zoned as Open Space Conservation District containing dredge tailings from past gold mining. No residents or workers are anticipated to be at the point of maximum impact for prolonged periods.

The maximum estimated community incremental excess cancer, chronic and acute health risks due to exposure to the project TAC emissions from long term operation of the proposed crematory are presented in **Table 5**. These estimates are conservative (health protective) and assume that the resident or worker is outdoors for the entire exposure period. The modeled locations of the Maximum Exposed Individual Resident (MEIR) and the point of maximum impact, along with the residential cancer risk isopleths (contours of equal risk), are shown in Figure 4, *Cancer Risks*. The complete HRA model output, including tables of health risks for all modeled discrete receptors and isopleth figures for incremental cancer risk, non-cancer chronic hazard index and acute hazard index are included as Attachment B to the air quality report.

Table 5. Maximum Exposed Individual Incremental Cancer Risk and Hazard Index

	MEI Resident Cancer Risk	MEI Worker Cancer Risk	MEI Resident Chronic Hazard Index	MEI Worker Chronic Hazard Index	MEI Acute Hazard Index
Results	0.6 in 1 million	<0.1 in 1 million	0.02	0.02	0.20
Threshold	10 in 1 million	10 in 1 million	1	1	1
Exceed Threshold?	No	No	No	No	No

Source: Lakes AERMOD View version 9.8.3 and CARB ADMRT version 19121. See Attachment B for model inputs, outputs, and risk isopleths.

MEI = Maximum Exposed Individual.

As shown in **Table 5**, the maximum incremental increased cancer risks and maximum non-cancer chronic and acute hazard index due to exposure to TACs from long-term operation of the proposed crematory would not exceed the SMAQMD thresholds. Therefore, operation of the project would not result in the exposure of sensitive receptors to substantial TAC concentrations and the impact would be less than significant.

- d) Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

Less Than Significant Impact Diesel equipment could generate diesel exhaust odors during construction activities. The generation of odors during the construction period would be temporary, intermittent, and dispersed within a short distance from the active work area. Once operational, potential odors from human remains prior to cremation would be minimized either by immediately processing remains or by temporarily storing remains in the proposed refrigeration chiller. Operation of the crematory would not be a significant source odors or other emissions due to the BACT features of the crematory, including process temperature and cycle time controls, as well as secondary combustion chambers which ensure the complete combustion of all solids, liquids, and gaseous fuels. Therefore, the project would not result in other emissions (such as those leading to odors) adversely affecting a substantial number of people and the impact would be less than significant.

IV. BIOLOGICAL RESOURCES

BIOLOGICAL RESOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Wildlife or US Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial adverse effect on federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The project site features open space habitat consisting of maintained grass with an open canopy of a variety of native and exotic tree species. The property that encompasses the project site features an open cemetery, lawns, associated landscaping, and the existing shed in which the crematory would be installed. Existing landscaping at the cemetery and around the shed consists of mature broad-leaved, coniferous, and palm trees. Lands to the south and west of the property contain native oak/gray pine woodland habitat typical of riparian communities in the Sierra Nevada foothills. To the west of the project site, the Jediah Smith Memorial Trail and Lake Natoma run on a north/south axis. The open spaces to the south and west are a part of the American River Parkway operated by the California Department of Parks and Recreation.

There are no jurisdictional wetlands, riparian, or other special status habitats located on or immediately adjacent to the project site.

Regulatory Framework Related to Biological Resources

The City of Folsom regulates urban development through standard construction conditions and through mitigation, building, and construction requirements set forth in the Folsom Municipal Code. Required of all projects constructed throughout the City, compliance with the requirements of the City's standard conditions and the provisions of the Municipal Code avoids or reduces many potential environmental effects. No Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan has been approved for the City of Folsom.

State and Federal Endangered Species Acts

Special status species are protected by state and federal laws. The California Endangered Species Act (CESA; California Fish and Game Code Sections 2050 to 2097) protects species listed as threatened and endangered under CESA from harm or harassment. This law is similar to the Federal Endangered Species Act of 1973 (FESA; 16 USC 1531 et seq.) which protects federally threatened or endangered species (50 CFR 17.11, and 17.12; listed species) from take. For both laws, take of the protected species may be allowed through consultation with and issuance of a permit by the agency with jurisdiction over the protected species.

California Code of Regulations and California Fish and Game Code

The official listing of endangered and threatened animals and plants is contained in the California Code of Regulations Title 14 § 670.5. A state candidate species is one that the California Fish and Game Code has formally noticed as being under review by the California Department of Fish and Wildlife (CDFW) for inclusion on the state list pursuant to Sections 2074.2 and 2075.5 of the California Fish and Game Code. CDFW also designates Species of Special Concern that are not currently listed or candidate species.

Legal protection is also provided for wildlife species in California that are identified as "fully protected animals." These species are protected under Sections 3511 (birds), 4700 (mammals), 5050 (reptiles and amphibians), and 5515 (fishes) of the California Fish and Game Code. These statutes prohibit take or possession of fully protected species at any time. The CDFW is unable to authorize incidental take of fully protected species when activities are proposed in areas inhabited by these species. The CDFW has informed non-federal agencies and private parties that they must avoid take of any fully protected species. However, Senate Bill (SB) 618 (2011) allows the CDFW to issue permits authorizing the incidental take of fully protected species under the CESA, so long as any such take authorization is issued in conjunction with the approval of a Natural Community Conservation Plan that covers the fully protected species (California Fish and Game Code Section 2835).

California Native Plant Protection Act

The California Native Plant Protection Act of 1977 (California Fish and Game Code Sections 1900 to 1913) requires all state agencies to use their authority to implement programs to conserve endangered and otherwise rare species of native plants. Provisions of the act prohibit the taking of listed plants from the wild and require notification of CDFW at least 10 days in advance of any change in land use other than changing from one agricultural use to another, which allows CDFW to salvage listed plants that would otherwise be destroyed.

Nesting and Migratory Birds

Nesting birds are protected by state and federal laws. California Fish and Game Code (§3503, 3503.5, and 3800) prohibits the possession, incidental take, or needless destruction of any bird nests or eggs; Fish and Game Code §3511 designates certain bird species “fully protected” (including all raptors), making it unlawful to take, possess, or destroy these species except under issuance of a specific permit. Under the Migratory Bird Treaty Act (MBTA) of 1918 (16 USF §703-711), migratory bird species and their nests and eggs that are on the federal list (50 CFR §10.13) are protected from injury or death, and project-related disturbance must be reduced or eliminated during the nesting cycle.

City of Folsom Tree Preservation Ordinance

Requirements related to biological resources also include protection of existing trees and specifies measures necessary to protect both ornamental and native oak trees.

Chapter 12.16 of the Folsom Municipal Code, Tree Preservation, further regulates the cutting or modification of trees, including oaks and specified other trees; requires a Tree Permit prior to cutting or modification; and establishes mitigation requirements for cut or damaged trees (City of Folsom 2018b). The Tree Preservation Ordinance establishes policies, regulations, and standards necessary to ensure that the City will continue to preserve and maintain its “urban forests”. Anyone who wishes to perform “Regulated Activities” on “Protected Trees” must apply for a permit with the City. Regulated activities include:

- Removal of a Protected Tree
- Pruning/trimming of a Protected Tree
- Grading or trenching within the Protected zone

Protected trees include:

- Native oak trees with a diameter of 6 inches or larger at breast height for single trunk trees or 20 inches or larger at combined diameter at breast height of native oak multi-trunk trees
- Heritage oak trees are native oaks with a trunk diameter of 19 inches or larger at breast height or native oaks with a multi-trunk diameter of 38 inches or larger at breast height
- Landmark trees are a tree or group of trees determined by the City Council to be a significant community benefit
- Street trees within the tree maintenance strip or contained on the master tree list

Jurisdictional Waters

Any person, firm, or agency planning to alter or work in “waters of the U.S.,” including the discharge of dredged or fill material, must first obtain authorization from the U.S. Army Corps of Engineers (USACE) under Section 404 of the Clean Water Act (CWA). Section 401 requires an applicant for a federal license or permit that allows activities resulting in a discharge to waters of the U.S. to obtain a state certification that the discharge complies with other provisions of the CWA. The Regional Water Quality Control Board (RWQCB) administers the certification program in California. The RWQCB also regulates discharges of pollutants or dredged or fill material to waters of the State which are more broadly defined than waters of the U.S.

Biological Resources Present in the Project Site

Land Cover Type

The land cover type present on the project site is mostly maintained lawn with an open overstory of native and exotic trees. The land is within the Open Space/Public (OS/P) Primary Area of the Historic District with underlying zoning of Open Space and Conservation (OSC). Land cover adjacent to the project site is primarily developed to the north and east, oak/gray pine woodland to the south and west, along with the Jedediah Smith Memorial Trail and Lake Natoma that run on a north-south axis to the west of the project site.

Wildlife

The project site provides habitat for disturbance-tolerant wildlife species typical of urban and suburban areas. Species present likely include resident and migratory passerines, raptors, and waterfowl, along with small mammals and reptiles adapted to a moderate level of human activity.

Special-Status Species with the Potential to Occur

The regionally occurring special-status species in the Folsom area are typically associated with aquatic habitats including perennial waterbodies, wetlands, and/or vernal pools, or are associated with relatively undisturbed contiguous stands of oak or riparian woodland. The project site is developed and lacks any aquatic habitats. Species expected to use the site would be highly adaptable common species tolerant of disturbance and urban areas.

No special-status wildlife species are expected to occur on the project site with the possible exception of a special-status bird using the project site as a temporary stopover in transit to or from more suitable habitats.

Other Migratory Birds and Nesting Birds

While no special-status bird species are expected to nest on the project site, marginal habitat is present on the site for a variety of common bird species that nest in trees, on buildings, or on the ground in urban and suburban areas.

Protected Trees

No site grading or removal of any trees, protected or otherwise, is proposed.

Jurisdictional Waters

No potential waters of the U.S. and/or State are present on the project site.

Evaluation of Biological Resources

- a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?

Less Than Significant. No habitat modifications are proposed. No sensitive species are expected to use the site, although birds protected under the MBTA may use the vicinity of the site for roosting, foraging, and nesting. While the delivery and installation of the crematory would likely result in a small increase in vehicles and workers visiting the site, those increases are expected to be insignificant relative to the number of workers and members of the public who visit the cemetery each day. Birds roosting in nearby trees may be temporarily flushed by the arrival of workers or equipment, but any birds using the site are likely already accustomed to a moderate level of human activity. A less than significant impact would occur.

- b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?

No Impact. No external modifications to the shed are proposed beyond the addition of two 250-gallon propane tanks on a concrete pad along the edge of the building and the addition of the proposed stack to the roof. Those modifications would occur in an area already subject to vehicle and worker visits and maintenance activity and would not affect any native habitat in the vicinity of the project site. No modifications to any habitat, vegetation, or landscaping are proposed. Therefore, no impact would occur.

- c) Have a substantial adverse effect on federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal wetlands, etc.) through direct removal, filling, hydrological interruption, or other means?

No Impact. No potential waters of the U.S. or State exist on the project site. No modification of any habitat is proposed. Therefore, there would be no impact.

- d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

No Impact. No external modifications are proposed except for the installation of two 250-gallon propane tanks on a concrete pad adjacent to a building already in use as a service shed. No modification of any landscaping, habitat, or vegetation is proposed as part of this project. There would be no impact.

- e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

No Impact. No modifications to, or removals of, any habitat, vegetation, trees, or landscaping are proposed. Therefore, no impact would occur.

- f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

No Impact. No Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan has been approved for the City of Folsom. Therefore, no impacts to an existing adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan would occur.

V. CULTURAL RESOURCES

CULTURAL RESOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Disturb any human remains, including those interred outside of dedicated cemeteries?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

The Cultural Resources section of this document is based upon the approach, methodology, and conclusions outlined in the project-specific Cultural Resources Inventory Report prepared by ECORP Consulting, Inc. (2020). All phases of the cultural resources investigation were conducted or supervised by Registered Professional Archaeologist (RPA) Lisa Westwood, who meets the Secretary of the Interior’s Professional Qualifications Standards for prehistoric and historical archaeology. Fieldwork and report contributions were conducted by Staff Archaeologist Laurel Zickler-Martin, RPA. Though the document in its entirety is incorporated by reference, the report itself is confidential and is not included as an appendix to this Initial Study.

Environmental Setting

To meet the regulatory requirements of this project, the cultural resources investigation was conducted pursuant to the provisions for the treatment of cultural resources contained within Section 106 of the National Historic Preservation Act (NHPA) and in CEQA (Public Resources Code [PRC] § 21000 et seq.). The goal of NHPA and CEQA is to develop and maintain a high-quality environment that serves to identify the significant environmental effects of the actions of a proposed project and to either avoid or mitigate those significant effects where feasible. CEQA pertains to all proposed projects that require State or local government agency approval, including the enactment of zoning ordinances, the issuance of conditional use permits, and the approval of development project maps. The NHPA pertains to projects that entail some degree of federal funding or permit approval.

The NHPA and CEQA (Title 54 U.S. Code [USC] Section 100101 et seq. and Title 14, California Code of Regulations [CCR], Article 5, § 15064.5) apply to cultural resources of the historical and pre-contact periods. Any project with an effect that may cause a substantial adverse change in the significance of a cultural resource, either directly or indirectly, is a project that may have a significant effect on the environment. As a result, such a project would require avoidance or mitigation of impacts to those affected resources. Significant cultural resources must meet at least one of four criteria that define eligibility for listing on either the California Register of Historical Resources (CRHR) (PRC § 5024.1, Title 14 CCR, § 4852) or the National Register of Historic Places (NRHP) (36 Code of Federal Regulations [CFR] 60.4):

- 1) is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- 2) is associated with the lives of persons important in our past;
- 3) embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values; or
- 4) has yielded, or may be likely to yield, information important in prehistory or history.

Cultural resources eligible for listing on the NRHP are considered Historic Properties under 36 Code of Federal Regulations Part 800 and are automatically eligible for the CRHR. Resources listed on or eligible for inclusion in the CRHR are considered Historical Resources under CEQA.

The City of Folsom Standard Construction Specifications were developed and approved by the City of Folsom in May 2004 and updated in April 2015. They include Article 11 - Cultural Resources, which provides direction on actions to be taken in the event that materials are discovered that may ultimately be identified as a historical or archaeological resource, or human remains (City of Folsom 2015).

Ethnography

Following is a brief summary providing a context in which to understand the background and relevance of resources that may occur in the general project area. This section is not intended to be a comprehensive review of the current resources available; rather, it serves as a general overview. Further details can be found in ethnographic studies, mission records, and major published sources.

Regional Background

California has been occupied by humans for approximately the past 10,000 years. Early groups between 10,000 and 8,000 years before present (BP) were largely mobile, small in number, and relied upon big game hunting and a limited exploitation of small game and plant resources. Between 8,000 and 5,000 BP, groups become more sedentary and stable and shifted to a greater reliance on plant resources and milling seeds and other plant matter. After about 5,000 BP, groups became more specialized, population densities increased, and regional cultures and languages developed that would form the basis for the societies encountered at the time of first European contact. Current patterns of climate and vegetation communities were in place by approximately 3,000 BP.

Nisenan or Southern Maidu

Ethnographically, the project area is in the southwestern portion of the territory occupied by the Penutian-speaking Nisenan. Nisenan inhabited the drainages of the Yuba, Bear, and American rivers, and also the lower reaches of the Feather River, extending from the east banks of the Sacramento River on the west to the mid to high elevations of the western flank of the Sierra Nevada to the east (Wilson and Towne 1978). The territory extended from the area surrounding the current city of Oroville on the north to a few miles south of the American River in the south.

Individual and extended families "owned" hunting and gathering grounds, and trespassing was discouraged (Kroeber 1925; Wilson and Towne 1978). Residence was generally patrilocal, but couples had a choice in the matter (Wilson and Towne 1978). The basic social and economic group for the

Nisenan was the family or household unit. The nuclear and/or extended family formed a corporate unit. These basic units were combined into distinct village or hamlet groups, each largely composed of consanguine relatives (Beals 1933; Littlejohn 1928). Lineage groups were important political and economic units that combined to form tribelets, which were the largest sociopolitical unit identified for Nisenan (Wilson and Towne 1978). Each tribelet had a chief or headman who exercised political control over all villages within it. Villages typically included family dwellings, acorn granaries, a sweathouse, and a dance house, owned by the chief. The role of chief seems to have been an advisory role with little direct authority (Beals 1933) but with the support of the shaman and the elders, the word of the chief became virtually the law (Wilson and Towne 1978). Tribelets assumed the name of the head village where the chief resided (Beals 1933; Levy 1978).

The office of tribelet chief was hereditary, with the chieftainship being the property of a single patrilineage within the tribelet. Tribelet populations of Valley Nisenan were as large as 500 persons (Wilson and Towne 1982), while foothill and mountain tribelets ranged between 100 and 300 persons (Littlejohn 1928; Levy 1978). Each tribelet owned a bounded tract of land and exercised control over its natural resources (Littlejohn 1928). Beals (1933) estimated that Nisenan tribelet territories averaged approximately 10 miles along each boundary, or 100 square miles, with foothill territories tending to encompass more area than mountain territories.

Nisenan practiced seasonal migration, a subsistence strategy involving moving from one area or elevation to another to harvest plants, fish, and game across contrasting ecosystems that were in relatively close proximity to each other. Valley Nisenan generally did not range beyond the valley and lower foothills, while foothill and mountain groups ranged across a more extensive area that included jointly shared territory whose entry was subject to traditional understandings of priority of ownership and current relations between the groups (d'Azevedo 1963).

Important food items included small and large game, fish, acorns, roots, pine nuts, and various hardwood nuts. Further resources were obtained from coastal groups and trans-Sierran groups through trade networks. Prescribed fire was used to maintain hunting and gathering grounds and to enhance opportunities to produce and gather acorns.

The Spanish arrived on the central California coast in 1769. Early contact with the first Spanish explorers to enter California was limited to the peripheries of Nisenan territory; they occurred mainly to the south on lands of the Miwok which had been explored by José Canizares in 1776, with only ephemeral explorations into Nisenan lands. There are no records of Nisenan groups being removed to the missions. They did, however, receive escapees from the missions, as well as pressure of displaced Miwok populations on their southern borders. The first known occupation by Euro-Americans was marked by American and Hudson Bay Company fur trappers in the late 1820s establishing camps in Nisenan territories. This occupation was thought to have been peaceful (Wilson and Towne 1978).

However, in the coming decades disease decimated the Nisenan of the Sacramento Valley, and many of the survivors retreated into the hills. Both they and mountain groups of Nisenan were met with persecution and attacks from settlers following the 1848 discovery of gold. The remaining Nisenan were relegated to working in agriculture, logging, ranching, or domestic pursuits (Wilson and Towne 1978). They and their descendants faced poor living and working conditions in the coming decades, although some customs and traditional practices have been preserved through the 21st century.

Regional History

The first European to visit California was Spanish maritime explorer Juan Rodriguez Cabrillo in 1542. He visited San Diego Bay, Catalina Island, San Pedro Bay, and the northern Channel Islands. The English adventurer Francis Drake visited the Miwok Native American group at Drake's Bay or Bodega Bay in 1579.

Colonization of California began with the Spanish Portolá land expedition. The expedition, led by Captain Gaspar de Portolá of the Spanish army and Father Junipero Serra, a Franciscan missionary, explored the California coast from San Diego to the Monterey Bay Area in 1769. As a result of this expedition, Spanish missions to convert the native population, presidios (forts), and pueblos (towns) were established. The Franciscan missionary friars established 21 missions in Alta California (the area north of Baja California) beginning with Mission San Diego in 1769 and ending with the mission in Sonoma established in 1823. The purpose of the missions and presidios was to establish Spanish economic, military, political, and religious control over the Alta California territory. No missions were established in the Central Valley; the closest were in the Bay Area. The Spanish did not establish any settlements in the Central Valley.

After Mexico became independent from Spain in 1821, what is now California became the Mexican province of Alta California with its capital at Monterey. In 1827, American trapper Jedediah Smith traveled along the Sacramento River and into the San Joaquin Valley to meet other trappers of his company who were camped there, but no permanent settlements were established by the fur trappers (Thompson and West 1880).

The Mexican government closed the missions in the 1830s and former mission lands, as well as previously unoccupied areas, were granted to retired soldiers and other Mexican citizens for use as cattle ranches. Much of the land along the coast and in the interior valleys became part of Mexican land grants or "ranchos" (Robinson 1948).

John Sutter, a European immigrant, built a fort at the confluence of the Sacramento and American rivers in 1839 and petitioned the Mexican governor of Alta California for a land grant, which he received in 1841. Sutter built a flour mill and grew wheat near the fort (Bidwell 1971). Gold was discovered in the flume of Sutter's lumber mill at Coloma on the South Fork of the American River in January 1848 (Marshall 1971). The discovery of gold initiated the 1849 California Gold Rush, which brought thousands of miners and settlers to the Sierra foothills east and southeast of Sacramento.

The American period began when the Treaty of Guadalupe Hidalgo was signed between Mexico and the U.S. in 1848. As a result of the treaty, Alta California became part of the U.S. as the territory of California. Rapid population increase occasioned by the Gold Rush of 1849 allowed California to become a state in 1850. Most Mexican land grants were confirmed to the grantees by U.S. courts, but usually with more restricted boundaries.

Project Area History

The project area is located within the northern portion of the former 35,521-acre Rio de los Americanos land grant, which stretches from Folsom Lake in the northeast to a southwestern point nearly reaching modern-day Florin Road, approximately 3.3 miles south of central Rosemont and 3.8 miles east of Florin. In 1848, Captain Joseph Folsom pursued ownership of the Rio de los Americanos. He died in 1855, and the land grant was subsequently sold, piecemeal, for developments in agriculture, mining endeavors,

and quarrying of granite. Originally named Granite City, the city of Folsom was named for the captain in the year he died.

City of Folsom History

The first railroad in California was built from Sacramento to Folsom in 1856 by the Sacramento Valley Railroad Company (Robertson 1998). Other railroads soon connected Folsom with additional communities in the Sacramento Valley and surrounding foothills. Folsom became a transportation hub and supply center for gold miners.

Folsom State Prison opened in 1880 on 40 acres of land (California Department of Corrections [CDCR] 2010a). When it opened, it housed 44 inmates in the State's first high-security prison. Although authorized by the State legislature in 1858, construction did not begin until 1878. A dam on the American River and a hydroelectric generation facility were built by inmates (CDCR 2010b). Electricity from the Folsom Powerhouse was transmitted 22 miles to Sacramento on July 13, 1895 (American Society of Mechanical Engineers 1976). Folsom was incorporated as a City in 1945. Folsom Dam was built in 1955, creating Folsom Lake. The dam was for flood control and to provide hydroelectricity. The largest employer in the area is Intel Corporation, which built a facility in the southern part of Folsom in 1984. Folsom continues to grow as an upscale community within the Sacramento Metropolitan Area.

History of Folsom Mining

The vicinity of the project area was used historically for mining, largely by the Natomas Company, who employed broad scale dredge mining in the first half of the twentieth century.

During the early mining period of the late 1840s and early 1850s, only the creeks and streams were mined, using pans, rockers, and hand-dug shallow diggings. Two historic gold mining districts were present in Sacramento County - the Folsom, or American River, District and the Michigan Bar District (Clark 2005). In 1853, the Natoma Water and Mining Company built a system of ditches, north of the current project area, to feed water from the American River and nearby creeks into the prairie and pasture lands that were known to have gold rich deposits. Most of the shallow gold deposits had been exhausted by 1865, and drift mining, which consisted of digging shafts down to depths of 20 feet and below, resumed until the late 1890s.

The project area itself is surrounded on the west and south by dredge mining tailings and the parcel is situated along a perennial waterway; these locations were appealing locations for miners to seek gold bearing deposits. All mining operations in the immediate vicinity of the project site had ceased by 1962.

Chinese Influence on Folsom

Chinese workers, some already present in California, greatly increased in numbers following the discovery of gold. Chinese miners often utilized their skills and diligence to successfully pursue mining claims that had been overlooked by white miners. Much of the money that was made from mining was sent in remittances to family members still in China. In 1878, there were over 3,500 Chinese mining in and around Folsom. When the gold began to run out, the Chinese worked at many other jobs, including such tasks as building the first Delta levees and constructing the transcontinental railroad. They also developed small businesses becoming laundrymen, cooks, storekeepers, farmers, and fishermen. Folsom once had a Chinese community numbering about 2,500 people, complete with businesses and community institutions. The Chung Wah Chinese Cemetery is listed on the National Register of Historic

Places and is a state registered landmark. It is adjacent to the project area (PAR Environmental Services, Inc. 1995).

History of Lakeside Memorial Lawn

Not much information in academic or gray literature exists on the history of the Lakeside Memorial Lawn Cemetery. The oldest section of the Cemetery evolved from the Negro Bar mining camp internments, with the first internment being in 1849 or shortly thereafter. The Cemetery is associated with the Miller Family. Jacob Miller, a German immigrant, opened a furniture and casket shop at 709 Sutter Street in 1869 and then transitioned the business into caskets and undertaking by the early 1880s. After his passing in 1905, ownership of the business transferred first to his son, Oscar Miller, then to longtime employee Robert Claney in 1962 (Scott 2020).

The cemetery represents a combination of several old Folsom cemeteries, including the Masonic, Odd Fellows, Jewish, Citizen's, and Cook's cemeteries. Lakeside Memorial Lawn is Folsom's only active historic cemetery.

Cultural Resources Surveys

The Area of Potential Effects (APE) consists of the horizontal and vertical limits of a project and includes the area within which significant impacts or adverse effects to Historical Resources or Historic Properties could occur as a result of the project. The APE is defined for projects subject to regulations implementing Section 106 (federal law and regulations). For projects subject to the CEQA, the term project area is used rather than APE. For the purpose of this document, the terms "project area" and APE are interchangeable. When referring to the larger Lakeside Memorial Lawn facility, within which the project area is situated, the term property is used.

Records Searches

ECORP requested a records search for the property from the North Central Information Center (NCIC) of the California Historical Resources Information System (CHRIS) at California State University-Sacramento on October 30, 2020 (NCIC search #SAC-20-152). The purpose of the records search was to determine the extent of previous surveys within a 0.25-mile (400-meter) radius of the property, and whether previously documented pre-contact or historic archaeological sites, architectural resources, or traditional cultural properties exist within this area.

In addition to the official records and maps for archaeological sites and surveys in Sacramento County, the following historic references were also reviewed: *Historic Property Data File for Sacramento County* (OHP 2012); *The National Register Information System* (National Park Service [NPS] 2020); *Office of Historic Preservation, California Historical Landmarks* (OHP 2019); *California Historical Landmarks* (OHP 1996 and updates); *California Points of Historical Interest* (OHP 1992 and updates); *Directory of Properties in the Historical Resources Inventory* (1999); *Caltrans Local Bridge Survey* (Caltrans 2019); *Caltrans State Bridge Survey* (Caltrans 2018); and *Historic Spots in California* (Kyle 2002).

Other references examined include a RealQuest Property Search and historic General Land Office (GLO) land patent records (Bureau of Land Management [BLM] 2020). Several historic maps and historic and recent aerial photographs were also reviewed.

The results of the records search indicate that the property has been previously surveyed for cultural resources, but the survey was performed 25 years ago under obsolete standards, and long prior to the

consideration of the type of project activity being currently proposed. Therefore, a pedestrian survey of the property was conducted for the current project under current protocols.

Native American Heritage Commission Sacred Lands File Coordination

ECORP contacted the California Native American Heritage Commission (NAHC) on October 26, 2020 to request a search of the Sacred Lands File for the property. This search was to determine whether or not Sacred Lands have been recorded by California Native American tribes within the property, because the Sacred Lands File is populated by members of the Native American community who have knowledge about the locations of tribal resources. In requesting a search of the Sacred Lands File, ECORP solicited information from the Native American community regarding tribal cultural resources, but the responsibility to formally consult with the Native American community lies exclusively with the federal and local agencies under applicable State and federal law. Results of the search were received on November 10, 2020. The search failed to reveal the presence of Native American cultural resources in the project area. For more information, including a description of official consultation with Native tribes, see Section 9.0.XVIII, Tribal Cultural Resources.

Other Interested Party Consultation

ECORP mailed letters to the Sacramento County Historical Society and the Folsom Historical Society on October 26, 2020 to solicit comments or obtain historical information that the repository might have regarding events, people, or resources of historical significance in the area. No responses to the letters sent to the Folsom Historical Society or the Sacramento County Historical Society have been received as of the preparation of this document.

Pedestrian Survey

On November 2, 2020, ECORP subjected the property to pedestrian survey under the guidance of the *Secretary of the Interior's Standards for the Identification of Historic Properties* (NPS 1983) using transects spaced 15 meters apart. ECORP expended less than half of one person-day in the field. At that time, the ground surface was examined for indications of surface or subsurface cultural resources. The general morphological characteristics of the ground surface were inspected for indications of subsurface deposits that may be manifested on the surface, such as circular depressions or ditches. Whenever possible, the locations of subsurface exposures caused by such factors as rodent activity, water or soil erosion, or vegetation disturbances were examined for artifacts or for indications of buried deposits. No subsurface investigations or artifact collections were undertaken during the pedestrian survey. The project area was photographed, and survey coverage mapped using a handheld Global Positioning System receiver.

Ground visibility in the cemetery itself was very limited, as the entire area is either paved or covered in manicured lawn; the only visible soil was immediately surrounding headstones and in sparse patches at the edges of the lawn.

Built Environment Resources

ECORP researched the shed itself to determine if it is old enough to warrant further evaluation as a cultural resource by an architectural historian. According to modern aerial photographs of the property, the shed was installed sometime between May 1993 and August 1998. As further supported by field inspection, the shed is not old enough to be considered a potential cultural resource, and therefore, it

was not recorded or considered further. Should the proposed project include demolition or remodeling of the shed, such activity would not have an impact on a cultural resource.

Evaluation of Cultural Resources

- a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?

Less Than Significant Impact. Historical resources are outside of the site of the proposed project. No precontact or historic resources were discovered during the pedestrian survey conducted by ECORP. The existing shed is not old enough to warrant consideration as a potential historic or cultural resource. Therefore, project impacts to historic resources would be less than significant.

- b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?

Less Than Significant Impact with Mitigation. In accordance with CEQA Guidelines, ECORP has assessed the project area for the presence of archaeological resources. The project site itself is not in an area otherwise suspected to contain unknown archaeological resources. The site survey and surveys of written records, historical maps and photographs, and outreach to groups with knowledge of the area's history all suggest that no known or previously unknown archaeological resources would be encountered or disturbed during construction. Ground disturbing activity would be limited to shallow ground clearing and site prep for the installation of a concrete pad to support two propane tanks. Still, the potential exists for inadvertent discovery of archaeological resources during project construction. The implementation of standard archaeological resource construction mitigation (Mitigation Measures CUL-01 and CUL-02) would ensure that potential impacts would be less than significant.

Mitigation Measure CUL-01: Avoid impacts to previously unknown archaeological resources.

Prior to the initiation of ground disturbing activity, a qualified professional archaeologist shall be retained to develop and deliver a contractor awareness training program to construction supervisors. The purpose of the training is to ensure that contractors are aware of the need to limit their activity, including equipment storage, staging, parking, and ground disturbance to only those locations identified as work areas on the official site plans.

Prior to the initiation of ground disturbing activity, a qualified professional archaeologist shall be retained to monitor the installation of temporary high-visibility exclusionary fencing along the toe of existing mine tailings features adjacent to the shed. The fencing shall remain in place until all project activities are completed. City inspectors shall include a verification of the fencing during all required inspections. In the event that exclusionary fencing has failed, the construction supervisor must re-install or repair the fence within 24 hours.

Mitigation Measure CUL-02: Minimize impacts to any previously unknown archaeological resources discovered during construction.

If subsurface deposits believed to be cultural in origin are discovered during construction, all work must halt within a 50-foot radius of the discovery. A qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for pre-contact and historic archaeologist, shall be retained to evaluate the significance of the find, and shall have the authority to

modify the no-work radius as appropriate, using professional judgment. The following notifications shall apply, depending on the nature of the find:

- If the professional archaeologist determines that the find does not represent a cultural resource, work may resume immediately, and no agency notifications are required.
- If the professional archaeologist determines that the find does represent a cultural resource from any time period or cultural affiliation, he or she shall immediately notify the City to consult on a finding of eligibility and implement appropriate treatment measures, if the find is determined to be a Historical Resource under CEQA, as defined in Section 15064.5(a) of the CEQA Guidelines or a historic property under Section 106 NHPA, if applicable. Work may not resume within the no-work radius until the City, through consultation as appropriate, determines that the site either: 1) is not an Historical Resource under CEQA, as defined in Section 15064.5(a) of the CEQA Guidelines; or 2) that the treatment measures have been completed to its satisfaction.

c) Disturb any human remains, including those interred outside of dedicated cemeteries?

Less Than Significant Impact with Mitigation. Though the project site is located on the property of the Lakeside Memorial Lawn Cemetery, no human remains are known to exist in the immediate vicinity of the project site. No evidence of potential human remains outside of marked graves was found in the project area during the cultural resources site survey by ECORP's archaeologist. Ground disturbing activity would be limited to shallow ground clearing and site prep for the installation of a concrete pad to support two propane tanks. However, there is always the possibility that subsurface construction activities associated with the proposed project, specifically the preparation of the site for the small concrete pad, could potentially damage or destroy previously undiscovered human remains. This is a potentially significant impact. However, if human remains were discovered, implementation of Mitigation Measure CUL-03 would reduce this potential impact to a less than significant level.

Mitigation Measure CUL-03: Avoid and minimize impacts related to accidental discovery of human remains.

If subsurface deposits believed to be cultural or human in origin are discovered during construction, all work must halt within a 50-foot radius of the discovery. A qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for pre-contact and historic archaeologist, shall be retained to evaluate the significance of the find, and shall have the authority to modify the no-work radius as appropriate, using professional judgment. The following notifications shall apply, depending on the nature of the find:

- If the find includes human remains, or remains that are potentially human, he or she shall ensure reasonable protection measures are taken to protect the discovery from disturbance (AB 2641). The archaeologist shall notify the Sacramento County Coroner (per §7050.5 of the Health and Safety Code). The provisions of §7050.5 of the California Health and Safety Code, §5097.98 of the California PRC, and Assembly Bill 2641 will be implemented. If the Coroner determines the remains are Native American and not the result of a crime scene, the Coroner will notify the NAHC, which then will designate a Native American Most Likely Descendant (MLD) for the project (§5097.98 of the PRC). The designated MLD will have 48 hours from the time access to the property is granted to make recommendations concerning treatment of the remains. If the landowner does not agree with the recommendations of the MLD, the NAHC can mediate

(§5097.94 of the PRC). If no agreement is reached, the landowner must rebury the remains where they will not be further disturbed (§5097.98 of the PRC). This will also include either recording the site with the NAHC or the appropriate Information Center; using an open space or conservation zoning designation or easement; or recording a reinternment document with the county in which the property is located (AB 2641). If the Coroner determines that the remains are human but are not Native American, then the Coroner will direct subsequent steps to address the discovery. Work may not resume within the no-work radius until the City, through consultation as appropriate, determines that the treatment measures have been completed to its satisfaction.

VI. ENERGY

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

Electricity

California's electricity needs are satisfied by a variety of entities, including investor-owned utilities, publicly owned utilities, electric service providers and community choice aggregators. In 2019, the California power mix totaled 277,704 gigawatt hours (GWh). In-state generation accounted for 200,475 GWh, or 72 percent, of the state's power mix. The remaining electricity came from out-of-state imports (CEC 2020a). **Table 6** provides a summary of California's electricity sources as of 2019.

Table 6. California Electricity Sources 2019

Fuel Type	Percent of California Power
Coal	2.96%
Large Hydro	14.62%
Natural Gas	34.23%
Nuclear	8.98%
Oil	0.01%
Other (Petroleum Coke/Waste Heat)	0.15%
Renewables	31.70%

Source: CEC 2020a

Natural Gas

Natural gas provides the largest portion of the total in-state capacity and electricity generation in California, with nearly 45 percent of the natural gas burned in California used for electricity generation in a typical year. Much of the remainder was consumed in the residential, industrial, and commercial sectors for uses such as cooking, space heating, and as an alternative transportation fuel. In 2012, total natural gas demand in California for industrial, residential, commercial, and electric power generation was 2,313 billion cubic feet per year (bcf/year), up from 2,196 bcf/year in 2010 (CEC 2020b).

Transportation Fuels

Transportation accounts for a major portion of California's energy budget. Automobiles and trucks consume gasoline and diesel fuel, which are nonrenewable energy products derived from crude oil. Gasoline is the most used transportation fuel in California, with 97 percent of all gasoline being consumed by light-duty cars, pickup trucks, and sport utility vehicles (SUV). In 2015, 15.1 billion gallons of gasoline were sold in California (CEC 2020c). Diesel fuel is the second most consumed fuel in California, used by heavy-duty trucks, delivery vehicles, buses, trains, ships, boats, and farm and construction equipment. In 2015, 4.2 billion gallons of diesel were sold in California (CEC 2020d).

Proposed Project

Potential energy use of the proposed crematory and cooler were estimated for the proposed project using assumptions provided by the manufacturer and the applicant. During projected operation, the crematory would use approximately 900.00 MMBTU (million British Thermal Units) of energy and 9,835.9 gallons of propane per year. The cooler would use approximately 15,000 kWhr (kilowatt hour) of electricity or 51.18 MMBTU of energy per year. The total energy use of the proposed crematory and cooler would be approximately 951 MMBTU per year. Additional minor increases in energy consumption may result from added time which would require lighting within and around the shed to accommodate any workers while operating the facility, and a minor increase in gasoline and/or diesel usage as remains are brought to/from the crematory and as workers drive to and from the site.

Regulatory Framework

State Regulations

California Building Standards Code (California Code of Regulations, Title 24)

The 2019 Building Energy Efficiency Standards, comprising Title 24, Parts 1 and 6, of the California Code of Regulations, is mandatory statewide. Local government agencies may adopt and enforce energy efficiency standards for newly constructed buildings, additions, alterations, and repairs provided the California Energy Commission finds that the standards will require buildings to consume no more energy than permitted by Title 24, Part 6. Such local standards may include adopting the requirements of Title 24, Part 6 before their effective date, requiring additional energy conservation measures, or setting stricter energy budgets.

Local Regulations

City of Folsom General Plan

The City of Folsom 2035 General Plan Utilities Element provides the following goals and policies relative to energy.

Goal PFS 8.1: Provide for the energy and telecommunications needs of Folsom and decrease the dependence on nonrenewable energy sources through energy conservation, efficiency, and renewable resource strategies now and in the future.

- PFS 8.1.3 Renewable Energy: Promote efforts to increase the use of renewable energy resources such as wind, solar, hydropower, and biomass both in the community and in City operations, where feasible.
- PFS 8.1.4 Regional Energy Conservation: Partner with neighboring jurisdictions and local energy utilities (e.g., SMUD and PG&E) to develop, maintain, and implement energy conservation programs.
- PFS 8.1.5 PACE Program: Assist in implementing the Property Assessed Clean Energy (PACE) financing programs to provide residential and commercial property owners with energy efficiency and renewable energy financing opportunities.
- PFS 8.1.6 Energy-Efficient Lighting: Reduce the energy required to light Folsom's parks and public facilities by employing energy-efficient lighting technology.
- PFS 8.1.7 Energy Conservation in City Operations: Strive to achieve an overall 20 percent reduction in City facility energy usage by continuing to install energy efficiency upgrades in City facilities (buildings, parks, and infrastructure) and implementing programs to measure and track energy usage in City facilities.

Folsom Municipal Code

Chapter 14.19 of the City of Folsom Municipal Code, entitled ENERGY CODE, adopts by reference the California Energy Code, 2019 Edition, published as Part 6, Title 24, California Code of Regulations to require energy efficiency standards for structures.

Evaluation of Energy

- a) Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

Less Than Significant Impact. Construction of the project would involve the use of a crane for several hours to unload the chiller and crematory from the truck, and the use of a mini excavator or skid steer loader for a day and one truck load of concrete to install a small pad for the two propane tanks. Construction equipment would be relatively small, given the small size of the project, and construction would be of short duration. Construction equipment would require gasoline, diesel, and potentially other fuel sources to operate. Additionally, a small number of workers would need to drive to and from the site.

Construction of the project would incorporate on-site energy conservation features. The following practices would be implemented during project construction to reduce waste and energy consumption:

- Follow maintenance schedules to maintain equipment in optimal working order and rated energy efficiency, which would include, but not be limited to, regular replacement of filters, cleaning of compressor coils, burner tune-ups, lubrication of pumps and motors, proper vehicle maintenance, etc.;
- Reduce on-site vehicle idling; and,

- In accordance with CALGreen criteria as well as state and local laws, at least 50 percent of on-site construction waste and ongoing operational waste would be diverted from landfills through reuse and recycling.

The project's construction-related energy usage would not represent a significant demand on energy resources because it is temporary in nature and small in scale. Therefore, the project's construction-phase energy impacts would be less than significant.

Operation of the proposed project would increase the consumption of energy, primarily related to propane used to power the crematory and to a lesser extent from electricity used to power the cooler. During projected operation, the crematory would use approximately 900.00 MMBTU of energy and 9,835.9 gallons of propane per year. The cooler would use approximately 15,000 kWhr of electricity or 51.18 MMBTU of energy per year. The total energy use of the proposed crematory and cooler would be approximately 951 MMBTU per year. Additional minor increases in energy would include electricity to light the space when workers are present and a minor increase in worker vehicle trips to and from the site.

Additionally, adequate energy facilities are already located within and adjacent to the site serving the existing uses. The cemetery is currently served with an electricity supply from the Sacramento Municipal Utilities District (SMUD). Electrical connections already exist for the shed, and may be upgraded as needed as part of the proposed project. Thus, the incremental increase associated with implementation of the project would not require the construction of new energy facilities or sources of energy that would not otherwise be needed to serve the region. Therefore, energy impacts from project operation would be less than significant.

- b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

No Impact. The proposed project would not conflict with or obstruct a state or local plan for renewable energy efficiency. The project would conform to all applicable state, federal, and local laws and codes. Therefore, the proposed project would have no impact.

VII. GEOLOGY AND SOILS

GEOLOGY AND SOILS:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv. Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct and indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

Geology

Information in the "Geology" subsection is derived from County of Sacramento's General Plan Safety Element (2017) unless noted otherwise. The project site is located near the border of the Great Valley and the Sierra Nevada Geomorphic Provinces. Specifically, the site is located within the Alluvial Plain Geomorphic Subunit of the Great Valley Province, just southwest of the boundary marking the start of the Sierra Nevada Province. Quaternary deposits of up to two million years old make up the soil of this subunit, which overlies layers of clay hardpans.

The project site lies within a seismically active region, as California has numerous faults that are considered active. An active fault is defined by the State Mining and Geology Board as one that has had surface displacement within Holocene time (about the last 11,000 years). Alquist-Priolo Earthquake Fault Zones are regulatory zones, delineated by the State Geologist, within which site-specific geologic studies are required to identify and avoid fault rupture hazards prior to subdivision of land and/or construction of most structures for human occupancy. There are no Alquist-Priolo Earthquake Fault Zones within Sacramento County. The nearest faults of any type to the project site are part of the Foothills Fault Zone's North Central Reach Section, and range from about 1.6 million to 130,000 years in age. (USGS 2014). They run north/northwest from Shingle Springs (El Dorado County) to Auburn (Placer County) and continue northward. They are not likely to be active. The nearest faults with recent earthquake activity, which are the most likely to cause shaking felt in the project area, are the Green Valley Fault Zone and the Greenville Fault Zone. Some faults in this area have experienced displacement within the past 200 years and are likely to be active; the nearest faults in these zones run north/northwest from Mt. Diablo to the southern Napa Valley (CGS 2020) and are located approximately 60 miles to the southwest of the project site.

Soils

The soil map unit for the project site is 245-Xerorthents, dredge tailings, 2 to 50 percent slopes. (NRCS 2020).

City Regulation of Geology and Soils

The City of Folsom regulates the effects of soils and geological constraints on urban development primarily through enforcement of the California Building Code, which requires the implementation of engineering solutions for constraints to urban development posed by slopes, soils, and geology. The City has additionally adopted a Grading Code (Folsom Municipal Code Section 14.29) that regulates grading citywide to control erosion, storm water drainage, revegetation, and ground movement.

Evaluation of Geology and Soils

- a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:
 - i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

Less Than Significant Impact. There are no known active faults crossing the property, and the project site is not located within an Alquist-Priolo Earthquake Fault Zone. Therefore, ground rupture is unlikely at the subject property, and impacts would be less than significant.

- ii. Strong seismic ground shaking?

Less Than Significant Impact. Though the project site is in an area of relatively low risk from most earthquakes, an earthquake of moderate to high magnitude generated within the region could still cause considerable ground shaking at the site (County of Sacramento 2017). To minimize potential ground shaking effects, crematory installation should be done in accordance with any relevant

provisions of the 2019 California Building Code, along with all safety recommendations from the manufacturer. Conformance to the current building code recommendations would minimize potential ground shaking impacts to a less-than-significant level.

iii. Seismic-related ground failure, including liquefaction?

Less Than Significant Impact. Soils most susceptible to liquefaction are clean, loose, saturated, uniformly graded, fine-grained sands. Soils at the project site are comprised of dredge tailings and other fill material; thus the soil is likely not loose nor uniformly graded. Further, only a small amount of superficial ground disturbance is proposed. Any impacts would be less than significant.

iv. Landslides?

No impact. The project site is generally flat, ranging in elevation from approximately 175 to 185 feet. The project is not located adjacent to any steep or unstable areas. No impact would occur.

b) Result in substantial soil erosion or the loss of topsoil?

Less Than Significant Impact. The only ground-disturbing work undertaken during this project would be the installation of two 250-gallon propane tanks and a concrete pad that would cover approximately 38.3 square feet of ground. All other work would take place inside an existing shed. Given the small area of soil disturbed, the short duration of the work to install the tanks, and the fact that the applicant is required to ensure that any relevant BMPs for soil conservation are adhered to, any impact is expected to be less than significant.

c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?

Less Than Significant Impact. The site is not unstable and the project area is nearly flat. Though the project is located in an area that has a medium to high potential for subsidence (County of Sacramento 2017), soil at the project site is generally comprised of dredge tailings and other fill material (NRCS 2020). Given that, the risk for future subsidence at the project site is low. Further, the project would not disturb significant areas of ground (disturbance would be limited to approximately 38.3 square feet), would take place mostly within an existing structure, and would not add an excessive amount of weight to the site. Therefore, potential impacts from project implementation would be less than significant.

d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?

Less Than Significant Impact. New ground disturbing activity and construction are not proposed as part of the project, with the exception of the construction of an approximately 38.3 square foot concrete pad to support two 250-gallon propane tanks. All other activity would take place within an existing shed. Given that no issues with expansive soils have been identified regarding the existing shed or its immediate surroundings, and that new foundation construction as part of the proposed project would be limited to a small concrete pad, any impacts would be less than significant.

- e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?

No Impact. Though no sanitary sewer line currently exists, there is no demand for one on the project site. No demand for the disposal of septic waste would be created as a result of this project. As no septic systems exist or are proposed, no impact would occur.

- f) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

No Impact. The proposed project area is not located in an area that is considered likely to have paleontological resources present. Paleontological resources (fossils) are the remains and/or traces of prehistoric life. Fossils are typically preserved in layered sedimentary rocks, and the distribution of fossils is a result of the sedimentary history of the geologic units within which they occur. Vertebrate fossils have been documented in nine different locations within Sacramento County. The finds encompass several hundred specimens, all within the Riverbank Formation. Because of the large number of vertebrate fossils that have been recovered from the Riverbank Formation from Sacramento County and throughout the Central Valley, this formation is considered to have high sensitivity under criteria established by the Society of Vertebrate Paleontology (1995). Likewise, the Mehrten and Lone formations located within the 2035 Plan Evaluation Area may be considered to be sensitive for the presence of paleontological resources. Other geologic formations found in the 2035 Folsom Plan Evaluation Area, such as the Laguna Formation, mine/dredge tailings, and Holocene alluvium along local drainage features, would not be expected to contain fossils. The only type of soil found at the site is composed of dredge tailings and other urban fill material, and would not be expected to contain fossils. Further, very little ground disturbance is proposed.

Fossils of plants, animals, or other organisms of paleontological significance have not been discovered within the project area, nor has the project area been identified as being within any of the areas mentioned above where such discoveries are likely. Therefore, the project would not result in impacts to paleontological resources or unique geologic features.

VIII. GREENHOUSE GAS EMISSIONS

GREENHOUSE GAS EMISSIONS:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

The Greenhouse Gas Emissions section of this document is based upon the approach, methodology, results, and conclusions outlined in the project-specific Air Quality and Greenhouse Gas Assessment prepared by HELIX Environmental Planning (HELIX 2020). The Air Quality and Greenhouse Gas Assessment is included as **Appendix B**.

Environmental Setting

Global climate change refers to changes in average climatic conditions on Earth including temperature, wind patterns, precipitation, and storms. Global temperatures are moderated by atmospheric gases. These gases are commonly referred to as greenhouse gasses (GHG) because they function like a greenhouse by letting sunlight in but preventing heat from escaping, thus warming the Earth’s atmosphere.

GHGs are emitted by natural processes and human (anthropogenic) activities. Anthropogenic GHG emissions are primarily associated with: the burning of fossil fuels during motorized transport; electricity generation; natural gas consumption; industrial activity; manufacturing; and other activities such as deforestation, agricultural activity, and solid waste decomposition.

The GHGs defined under California’s Assembly Bill (AB) 32, described below, include carbon dioxide (CO₂), methane (CH₄), nitrous oxide (N₂O), hydrofluorocarbons (HFC), perfluorocarbons (PFC), and sulfur hexafluoride (SF₆). Each GHG differs in its ability to absorb heat in the atmosphere based on the lifetime, or persistence, of the gas molecule in the atmosphere. Estimates of GHG emissions are commonly presented in carbon dioxide equivalents (CO₂e), which weigh each gas by its global warming potential (GWP). Expressing GHG emissions in CO₂e takes the contribution of all GHG emissions to the greenhouse effect and converts them to a single unit equivalent to the effect that would occur if only CO₂ were being emitted. GHG emissions quantities in this analysis are presented in metric tons (MT) of CO₂e. For consistency with United Nations Standards, modeling and reporting of GHGs in California and the U.S. use the GWPs defined in the Intergovernmental Panel on Climate Change’s (IPCC) Fourth Assessment Report (IPCC 2007), as shown in **Table 7**.

Table 7. Global Warming Potential and Atmospheric Lifetimes

Greenhouse Gas	Atmospheric Lifetime (years)	GWP
Carbon Dioxide (CO ₂)	50-200	1
Methane (CH ₄)	12	25
Nitrous Oxide (N ₂ O)	114	298
HFC-134a	14	1,430
PFC: Tetrafluoromethane (CF ₄)	50,000	7,390
PFC: Hexafluoroethane (C ₂ F ₆)	10,000	12,200
Sulfur Hexafluoride (SF ₆)	3,200	22,800

Source: IPCC 2007.

HFC: hydrofluorocarbon; PFC: perfluorocarbon

Regulatory Setting

The primary GHG reduction legislation and plans (applicable to the project) at the State, regional, and local levels are described below. Implementation of California's GHG reduction mandates is primarily under the authority of the California Air Resources Board (CARB) at the state level, SMAQMD and the Sacramento Area Council of Governments (SACOG) at the regional level, and the City at the local level.

Executive Order S-3-05

On June 1, 2005, Executive Order (EO) S-3-05 proclaimed that California is vulnerable to climate change impacts. It declared that increased temperatures could reduce snowpack in the Sierra Nevada, further exacerbate California's air quality problems, and potentially cause a rise in sea levels. To avoid or reduce climate change impacts, EO S-3-05 calls for a reduction in GHG emissions to the year 2000 level by 2010, to year 1990 levels by 2020, and to 80 percent below 1990 levels by 2050. Executive Orders are not laws and can only provide the governor's direction to state agencies to act within their authority to reinforce existing laws.

Assembly Bill 32 – Global Warming Solutions Act of 2006

The California Global Warming Solutions Act of 2006, widely known as AB 32, requires that CARB develop and enforce regulations for the reporting and verification of statewide GHG emissions. CARB is directed by AB 32 to set a GHG emission limit, based on 1990 levels, to be achieved by 2020. The bill requires CARB to adopt rules and regulations in an open public process to achieve the maximum technologically feasible and cost-effective GHG emission reductions.

Executive Order B-30-15

On April 29, 2015, EO B-30-15 established a California GHG emission reduction target of 40 percent below 1990 levels by 2030. The EO aligns California's GHG emission reduction targets with those of leading international governments, including the 28 nation European Union. California is on track to meet or exceed the target of reducing GHGs emissions to 1990 levels by 2020, as established in AB 32. California's new emission reduction target of 40 percent below 1990 levels by 2030 will make it possible to reach the goal established by EO S-3-05 of reducing emissions 80 percent under 1990 levels by 2050.

Senate Bill 32

Signed into law by Governor Brown on September 8, 2016, Senate Bill (SB) 32 (Amendments to the California Global Warming Solutions Action of 2006) extends California's GHG reduction programs beyond 2020. SB 32 amended the Health and Safety Code to include Section 38566, which contains language to authorize CARB to achieve a statewide GHG emission reduction of at least 40 percent below 1990 levels by no later than December 31, 2030. SB 32 codified the targets established by EO B-30-15 for 2030, which set the next interim step in the State's continuing efforts to pursue the long-term target expressed in EO B-30-15 of 80 percent below 1990 emissions levels by 2050.

California Air Resources Board

On December 11, 2008, the CARB adopted the Climate Change Scoping Plan (Scoping Plan) as directed by AB 32. The Scoping Plan proposes a set of actions designed to reduce overall GHG emissions in California to the levels required by AB 32. Measures applicable to development projects include those related to energy-efficiency building and appliance standards, the use of renewable sources for electricity generation, regional transportation targets, and green building strategy. Relative to transportation, the Scoping Plan includes nine measures or recommended actions related to reducing vehicle miles traveled (VMT) and vehicle GHGs through fuel and efficiency measures. These measures would be implemented statewide rather than on a project-by-project basis (CARB 2008).

In response to EO B-30-15 and SB 32, all state agencies with jurisdiction over sources of GHG emissions were directed to implement measures to achieve reductions of GHG emissions to meet the 2030 and 2050 targets. The mid-term target is critical to help frame the suite of policy measures, regulations, planning efforts, and investments in clean technologies and infrastructure needed to continue driving down emissions (CARB 2014). In December 2017, CARB adopted the 2017 Climate Change Scoping Plan Update, the Strategy for Achieving California's 2030 Greenhouse Gas Target, to reflect the 2030 target set by EO B 30 15 and codified by SB 32 (CARB 2017).

Sacramento Metropolitan Air Quality Management District

The SMAQMD provides direction and recommendations for the analysis of GHG impacts of a project and approach to mitigation measures in its CEQA Air Quality Guidelines (SMAQMD 2020a).

Sacramento Area Council of Governments

As required by the Sustainable Communities and Climate Protection Act of 2008 (SB 375), SACOG has developed the 2020 Metropolitan Transportation Plan and Sustainable Communities Strategy. This plan seeks to reduce GHG and other mobile source emissions through coordinated transportation and land use planning to reduce VMT.

City of Folsom

As part of the 2035 General Plan, the City of Folsom prepared an integrated Greenhouse Gas Emissions Reduction Strategy (GHG Strategy) to identify and reduce current and future community GHG emissions and those associated with the City's municipal operations. Adopted on August 28, 2018, the GHG Strategy also serves as the City's "plan for the reduction of greenhouse gases", per Section 15183.5 of the CEQA Guidelines, which provides the opportunity for tiering and streamlining of project-level emissions for certain types of discretionary projects subject to CEQA review that are consistent with the

General Plan. The GHG Strategy includes goals and strategies to reduce community and municipal GHG emissions, compared to the 2005 baseline year, by 15 percent in 2020, 51 percent in 2035, and 80 percent in 2050 (City of Folsom 2018a; City of Folsom 2018c).

Significance Criteria

The following potential air quality impacts are based on Appendix G of the CEQA Guidelines, a significant impact is identified if the project would result in any of the following:

- a) *Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?*
- b) *Conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases?*

In accordance with CEQA Guidelines Sections 15064(h)(3), 15130(d), and 15183(b), a project's incremental contribution to a cumulative GHG emissions effect may be determined not to be cumulatively considerable if it complies with the requirements of a qualified plan for the reduction of greenhouse gases. The City General Plan Policy NCR 3.2.8 provides criteria for project-level streamlining and tiering (City of Folsom 2018a):

Projects subject to environmental review under CEQA may be eligible for tiering and streamlining the analysis of GHG emissions, provided they are consistent with the GHG reduction measures included in the GHG Strategy contained in the General Plan and EIR. The City may review such projects to determine whether the following criteria are met:

- Proposed project is consistent with the current general plan land use designation for the project site;
- Proposed project incorporates all applicable GHG reduction measures (as documented in the Climate Change Technical Appendix to the General Plan EIR) as mitigation measures in the CEQA document prepared for the project; and,
- Proposed project clearly demonstrates the method, timing and process for which the project will comply with applicable GHG reduction measures and/or conditions of approval, (e.g., using a CAP/GHG reduction measures consistency checklist, mitigation monitoring and reporting plan, or other mechanism for monitoring and enforcement as appropriate).

Evaluation of Greenhouse Gas Emissions

- a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Less Than Significant Impact. To determine consistency with the City's GHG Strategy, the criteria outlined in the Greenhouse Gas Reduction Strategy Consistency Checklist are shown and discussed in **Table 8**.

Table 8. GHG Reduction Strategy Consistency Checklist

Checklist Item	Consistent?	Discussion
Part 1: Land Use Consistency		
A. The proposed project is consistent with the City's 2035 General Plan land use and zoning designations. If "Yes," proceed to Part 2 of the Checklist.	Yes	The project would be located within the footprint of an existing building in an existing cemetery in an area designated Open Space in the General Plan and zoned Open Space/Public (OS/P) Primary Area of the Historic District with underlying zoning of Open Space and Conservation District (OSC). According to the City Zoning Code Chapter 17.52.550 and Chapter 17.39, a cemetery is an allowed use in both the OS/P Primary Area and OSC zone with a use permit. While the project may require a new conditional use permit, the project would not require a General Plan amendment or rezone. The project would be consistent with existing project site use and land use designation the General Plan.
Part 2: GHG Reduction Measures Consistency		
E-1: Improve Building Energy Efficiency in New Development	Not Applicable	The project does not propose new buildings or substantial modifications to existing buildings.
E-2: Water Heater Replacement in Existing Residential Development	Not Applicable	The project is not an existing residential development.
E-3: Improve Building Energy Efficiency in Existing Development	Not Applicable	The project's proposed equipment would be installed within an existing metal shed and would not include any conditioned or occupied building space.
E-4: Increase Use of Renewable Energy in Existing Development	Not Applicable	The project's proposed equipment would be installed within an existing metal shed. No expansion or retrofit of existing buildings are proposed.
T-1: Reduce VMT Through Mixed and High-Density Land Use	Not Applicable	The project does not propose, and the project site open space land use designation and zoning does not permit, high density development and mixed uses.
T-2: Improve Streets and Intersections for Multi-Modal Use and Access	Not Applicable	The project does not include construction of new streets or improvement to existing streets.
T-3: Adopt Citywide TDM Program	Not Applicable	The project is not a residential, office, commercial retail, public facility or school development. The project would not include new parking spaces.
T-5: Reduce Minimum Parking Standards	Not Applicable	The project would not include new parking spaces.
T-6: Require the Use of High-Performance Renewable Diesel in Construction Equipment	Not Applicable	The project would require minimal off-road diesel construction equipment. At most, a small excavator or skid steer loader may be used for a few hours to prepare an area for a small concrete pad.
T-8: Install Electric Vehicle Charging Stations	Not Applicable	The project is not a residential development, does not propose new parking spaces, and existing parking spaces at the project building are less than 10.
SW-1: Increase Solid Waste Diversion	Not Applicable	The project would involve minimal construction activity and would not result in substantial construction waste which could be diverted.

W-1: Increase Water Efficiency in New Residential Development	Not Applicable	The project is not a new residential development and the project does not propose new indoor or outdoor water uses.
W-2: Reduce Outdoor Water Use	Not Applicable	The project does not propose substantial addition, alteration, or expansion to existing facilities or new outdoor water uses.

Source: City of Folsom 2018d

As presented in **Table 8**, the project would be consistent with the project site general plan land use designation and none of the GHG reduction measures listed in the GHG Strategy are applicable to the project. Therefore, the project would be consistent with the City’s GHG Strategy and the project would not generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment. The impact would be less than significant.

b) Conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

Less Than Significant Impact. As discussed in criterion a), above, the project would be consistent with the City’s integrated General Plan and GHG Strategy. The GHG strategy was developed to meet the City’s GHG reduction targets which were formulated to meet the statewide GHG mandates of AB 32 and SB 32. Therefore, the project would not conflict with an applicable plan adopted for the purposes of reducing GHG emissions and the impact would be less than significant.

IX. HAZARDS AND HAZARDOUS MATERIALS

HAZARDS AND HAZARDOUS MATERIALS:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g) Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

The project property is currently developed as a cemetery; the project site is within and immediately adjacent to an existing maintenance shed. The project site has no known past land uses associated with potentially hazardous sites.

The proposed project would include the installation of two 250-gallon propane tanks immediately adjacent to the existing shed. Propane is considered a hazardous material in that is extremely flammable and may cause burns, irritations, and/or asphyxiation if humans come into direct contact with significant quantities of it (USDOT 2016). Federal and state laws include provisions for the safe handling of hazardous substances. The federal Occupational Safety and Health Administration (OSHA) administers requirements to ensure worker safety. Construction activity must also be in compliance with California OSHA regulations (Occupational Safety and Health Act of 1970).

Nearby schools include the Golden Valley Charter River School (approximately 2.7 miles west of the project site²), Folsom Montessori School (0.6 miles east), Sutter Middle School (1.0 mile east), Folsom Lake High School (0.9 miles east) and Folsom Middle School (2.5 miles east).

The following databases were reviewed for the project site and surrounding area to identify potential hazardous contamination sites: the USEPA's Envirofacts online database (USEPA 2020a); California Department of Toxic Substance Control's EnviroStor online database (DTSC 2020); and the USEPA's Superfund National Priorities List (USEPA 2020c). Based on the results of the databases reviewed, the project site is not listed as a hazardous waste site. No Superfund sites are located on or near the project site. According to the EnviroStor database, there are two potentially hazardous sites near the project site:

- City of Folsom Corporate Yard Landfill. Located approximately 0.3 miles north of the project site. Underwent voluntary cleanup. No further action required.
- A&S Custom Plating Co. Located 0.6 miles northeast of the project site. Underwent evaluation. No further action required.

No private or public airports are located within the City of Folsom. The nearest public airfield is Mather Airport, located approximately 11.7 miles southwest of the project site. Cameron Airpark is a public use airport located approximately 13 miles northeast of the project site, and McClellan Airport is a privately-owned public use airport located approximately 17 miles west of the project site.

The City of Folsom Fire Department provides fire protection services. There are four fire stations providing fire/rescue and emergency medical services within the City of Folsom with a fifth station planned near the eastern city limits. Station 35 is the nearest station to the project site and is located at 535 Glenn Drive, approximately 1.5 miles east of the project site. Station 36 is second nearest to the project site and is located at 9700 Oak Avenue, approximately 2.3 miles north of the project site. The Fire Department responds to over 6,000 requests for service annually with an average of 16.4 per day (City of Folsom 2020). The project site is easily accessible to fire service personnel. Consistent with the City's Multi-Hazard Emergency Management Plan, the City of Folsom maintains pre-designated emergency evacuation routes along major streets and thoroughfares (City of Folsom 2005).

The project is not located in or near a State Responsibility Area or in a Very High Fire Hazard Severity Zone (CAL FIRE 2020; CSG 2020). Vegetation on the property is irrigated and includes maintained lawns and well-spaced trees with a generally open canopy and limbs pruned near ground level.

Evaluation of Hazards and Hazardous Materials

- a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?
- b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

Less Than Significant Impact. The project would involve the installation of two 250-gallon propane tanks

² The school is located across Lake Natoma from the project site and, though it is approximately 2.7 miles away by car, its physical location is about 3,000 feet (just over one-half mile) northwest of the project site.

immediately adjacent to the existing maintenance shed to power the crematory. Propane is flammable and has the potential to negatively impact human health if people are directly exposed to the liquid, gas, and/or vapors in the cases of large leaks or spills (USDOT 2016). Further, significant damage to the tanks, failure of safety mechanisms, and/or the presence of an ignition source may make the tanks an explosion hazard. However, this is very unlikely for a number of reasons. The tanks would be secured in place on a concrete pad, marked conspicuously, and placed in an area at low risk of impact from any vehicle or piece of equipment. They would not be located in an area of the cemetery frequented by the public. All installation, maintenance, and operations would be done by trained individuals in accordance with the manufacturer's recommendations and state regulations. The tanks would be regularly inspected to ensure soundness and proper function. Delivery of propane and filling of the tanks would be done only by licensed professionals following all applicable regulations and best practices.

No existing hazardous materials have been identified on the project site, and the site has no known history of past land uses associated with potentially hazardous sites. Construction of the proposed project would result in a small increase in the generation, storage, and disposal of hazardous wastes. During project construction, oil, gasoline, diesel fuel, paints, solvents, and other hazardous materials may be used. If spilled, these substances could pose a risk to the environment and to human health.

Following construction, household hazardous materials (such as various cleansers, paints, solvents, pesticides, and automobile fluids) may occasionally be used or brought into the vicinity of the site as part of routine maintenance. The routine transport, use, and disposal of hazardous materials are subject to local, state, and federal regulations to minimize risk and exposure.

Further, the City has set forth its hazardous materials goals and policies in the Hazardous Materials Element of the General Plan. The preventative policies protect the health and welfare of residents of Folsom through management and regulation of hazardous materials. Consequently, use of the listed materials above for their intended purpose would not pose a significant risk to the public or environment, and impacts would be less than significant for questions a) and b).

- c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?

No impact. The project site is not located within one-quarter mile of an existing or proposed school.

- d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

No Impact. The project site is not listed as a hazardous waste site on Envirofacts (USEPA 2020a), EnviroStor (DTSC 2020), or the EPA's Superfund National Priorities List (USEPA 2020c). Therefore, project implementation would have no impact for question d).

- e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?

No Impact. The project site is not located within an Airport Land Use Plan area, and no public or private airfields are within two miles of the project site. Therefore, the proposed project would not result in a

safety hazard or excessive noise for people residing or working in the project area, and no impact would occur.

- f) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Less Than Significant Impact. Consistent with the City's Multi-Hazard Emergency Management Plan, the City of Folsom maintains pre-designated emergency evacuation routes along major streets and thoroughfares (City of Folsom 2005). The proposed project would not modify any pre-designated emergency evacuation route or preclude their continued use as an emergency evacuation route. Emergency vehicle access would be maintained throughout the project site to meet the Fire Department standards for fire engine maneuvering, location of fire engine to fight a fire, rescue access to the units, and fire hose access to all sides of the building. Therefore, project impacts to the City's adopted emergency plans would be less than significant.

- g) Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

Less Than Significant Impact. The project site is located in an urbanized area in the City of Folsom and is provided urban levels of fire protection by the City. Landscaping on the property is well-irrigated, well-spaced, trimmed, pruned, and generally maintained. To the north of the site is a residential neighborhood, to the east is gravel/rock cover and Folsom Boulevard, and to the south and west is a greenbelt that runs parallel to Lake Natoma on a north/south axis. The natural spaces are small, with a relatively open and discontinuous canopy. The project is not likely to cause any ignition, given that the crematory will not emit sparks, and any ignition caused by other factors could be quickly controlled by the City of Folsom Fire Department and would not spread great distances given the land use and vegetation surrounding and occupying the site. As an existing facility, Lakeside Memorial Lawn maintains adequate fire response infrastructure for both current operations and the proposed project. The City of Folsom Fire Department reviewed the project application and did not raise any concerns regarding the adequacy of water supply or site access. Therefore, the proposed project would not expose people or structures to a significant risk of loss due to wildland fires, and impacts would be less than significant.

X. HYDROLOGY AND WATER QUALITY

HYDROLOGY AND WATER QUALITY:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:				
i. Result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. Substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off- site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional resources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv. Impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

The project site is currently a small cemetery, with associated landscaping, outbuildings, and access roads. Lands to the south and west contain woodland habitat typical of riparian communities in the Sierra Nevada foothills. To the west is also the Jedidiah Smith Memorial Trail that runs along the eastern shore of Lake Natoma. The trail, also known as the American River Bike Trail, connects Folsom Lake (north of the project site) to the confluence of the American and Sacramento Rivers in Downtown Sacramento. It is a part of the American River Parkway that is operated by the California Department of Parks and Recreation. To the north of the project site is a small residential neighborhood with single-family dwellings. Folsom Boulevard runs in a north/south line just east of the property. East of Folsom Boulevard is a large, developed area containing single-family homes, apartment complexes, a mobile home park, and some small businesses. The more regional setting is primarily characterized by residential development with a commercial shopping center to the east.

The project site is generally flat, ranging from about 175 to 185 feet in elevation throughout. Precipitation is the only apparent source of surface water as there are no wetlands or streams located on the project site.

The project site currently retains stormwater onsite and then discharges it into Lake Natoma. There is no connection with a City sewer system. Demand for water disposal, of either sanitary waste or stormwater, is projected to be unchanged by the proposed action.

Construction of the proposed project would disturb only the amount of soil required to install a concrete pad of approximately 38.3 square feet to secure two 250-gallon propane tanks adjacent to the existing shed.

Federal Emergency Management Agency (FEMA) flood insurance rate maps were reviewed for the project's proximity to a 100-year floodplain. The proposed project is on FEMA panel 06067C0116H, effective August 16, 2012. The project site is not located within a 100-year floodplain (FEMA 2020). The project is not located in a tsunami inundation zone (CDC 2020a).

The site is not located in an area of important groundwater recharge. Domestic water in the City is provided solely by surface water sources, and the City is the purveyor of water to the project area.

Regulatory Framework Relating to Hydrology and Water Quality

The City is a signatory to the Sacramento Countywide NPDES permit for the control of pollutants in urban stormwater. Since 1990, the City has been a partner in the Sacramento Stormwater Quality Partnership, along with the County of Sacramento and the Cities of Sacramento, Citrus Heights, Elk Grove, Galt, and Rancho Cordova. These agencies are implementing a comprehensive program involving public outreach, construction and industrial controls (i.e., BMPs), water quality monitoring, and other activities designed to protect area creeks and rivers. This program would be unchanged by the proposed project, and the project would be required to implement all appropriate program requirements.

In addition to these activities, the City maintains the following requirements and programs to reduce the potential impacts of urban development on stormwater quality and quantity, erosion and sediment control, flood protection, and water use. These regulations and requirements would be unchanged by the proposed project.

Standard construction conditions required by the City include:

- *Water Pollution* – requires compliance with City water pollution regulations, including NPDES provisions.
- *Clearing and Grubbing* – specifies protection standards for signs, mailboxes, underground structures, drainage facilities, sprinklers and lights, trees and shrubbery, and fencing. Also requires the preparation of a SWPPP to control erosion and siltation of receiving waters.
- *Reseeding* – specifies seed mixes and methods for reseeding of graded areas.

Additionally, the City enforces the following requirements of the Folsom Municipal Code as presented in **Table 9**.

Table 9. City of Folsom Municipal Code Sections Regulating the Effects on Hydrology and Water Quality from Urban Development

CODE SECTION	CODE NAME	EFFECT OF CODE
8.70	Stormwater Management and Discharge Control	Establishes conditions and requirements for the discharge of urban pollutants and sediments to the storm-drainage system; requires preparation and implementation of Stormwater Pollution Prevention Plans.
13.26	Water Conservation	Prohibits the wasteful use of water; establishes sustainable landscape requirements; defines water use restrictions.
14.20	Green Building Standards Code	Adopts by reference the California Green Building Standards Code (CALGreen Code), 2016 Edition, excluding Appendix Chapters A4, A5, and A6.1 published as Part 11, Title 24, C.C.R. Purpose of the Folsom Green Building Standards Code is to promote and require the use of building concepts having a reduced negative impact or positive environmental impact and encouraging sustainable construction practices.
14.29	Grading Code	Requires a grading permit prior to the initiation of any grading, excavation, fill or dredging; establishes standards, conditions, and requirements for grading, erosion control, stormwater drainage, and revegetation.
14.32	Flood Damage Prevention	Restricts or prohibits uses that cause water or erosion hazards, or that result in damaging increases in erosion or in flood heights; requires that uses vulnerable to floods be protected against flood damage; controls the modification of floodways; regulates activities that may increase flood damage or that could divert floodwaters.
14.33	Hillside Development Standards	Regulates urban development on hillsides and ridges to protect property against losses from erosion, ground movement and flooding; to protect significant natural features; and to provide for functional and visually pleasing development of the city's hillsides by establishing procedures and standards for the siting and design of physical improvements and site grading.

Source: City of Folsom 2018b.

Evaluation of Hydrology and Water Quality

- a) Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?

Less Than Significant Impact. No work would occur on the site outside of the existing shed with the exception of installing two 250-gallon propane tanks immediately adjacent to the existing shed. The tanks would cover an area of ground of approximately 38.3 square feet and thus may disturb 38.3 square feet of soil during installation. There would be an addition of approximately 38.3 square feet of impervious surface, following the completion of construction since the tanks would be built on a concrete foundation. No significant increase in wastewater or runoff is expected as a result of the project. The temporary disturbance of a small amount of soil and the potential addition of an impervious surface (approximately 38.3 square feet in both cases) would render any impacts less than significant.

- b) Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

Less Than Significant Impact. The project does not propose any new building construction or the addition of any impervious surfaces, except for up to 38.3 square feet to be covered by propane tanks' concrete foundation. No other soil disturbance and no grading or compaction are anticipated. The small change in impervious surfaces would render any impacts to infiltration at the site or groundwater recharge to be less than significant.

- c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:
- i. Result in substantial erosion or siltation on- or off-site?

Less Than Significant Impact. Approximately 38.3 square feet of soil may be disturbed for a short time during construction. Any resulting erosion impact would be less than significant.

- ii. Substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off- site?

Less Than Significant Impact. Approximately 38.3 square feet of impervious surfaces would be added during construction. There would be no other addition or expansion of impervious surfaces, and existing drainage patterns and systems would not be altered. Any impact would be less than significant.

- iii. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional resources of polluted runoff?

Less Than Significant Impact. Approximately 38.3 square feet of impervious surfaces would be added during construction. There would be no other addition or expansion of impervious surfaces, and existing drainage patterns and systems would not be altered. No additional wastewater is expected to be generated by the proposed action. Existing systems are adequate to deal with existing levels of runoff. Any impact would be less than significant.

- iv. Impede or redirect flood flows?

No impact. The proposal would not alter or block any existing watercourse or drainage feature, nor would it block or impede the drainage of any floodwater from the property during times of heavy rain. There would be no impact.

- d) In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

No impact. The project site is not located within a 100-year floodplain nor in a tsunami inundation zone or seiche zone. No impact would occur.

- e) Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

Less Than Significant Impact. The project would include the disturbance of up to approximately 38.3 square feet of soil and the installation of the same amount of impervious surfaces on the project site. All other work would be conducted inside of an established building. Given the small area of soil disturbed and impervious surfaces added, any impacts to groundwater infiltration rates or groundwater quality are expected to be less than significant.

XI. LAND USE AND PLANNING

LAND USE AND PLANNING:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Cause significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

Land use in the project area is regulated by the City of Folsom through the various plans and ordinances adopted by the City. These include the City of Folsom General Plan and the City of Folsom Municipal Code, including the Zoning Code. The project site is designated as Open Space (OS) in the City of Folsom General Plan. The following General Plan policies apply to the Open Space designation:

LU-1.1.8: Preserve Natural Assets: Maintain the existing natural vegetation, landscape features, open space, and viewsheds in the design of new developments.

LU-1.1.9: Preserve Historic Resources: Recognize the importance of history in the City of Folsom, and preserve historic and cultural resources throughout the city, to the extent feasible.

LU-1.1.10: Network of Open Space: Ensure designated open space is connected whenever feasible with the larger community and regional network of natural systems, recreational assets, and viewsheds

The proposed action would not be in conflict with the OS designation or the above policies, as disturbances would be largely limited to an existing building and a small area of existing disturbance that is shielded from public view..

The project site is currently zoned Open Space/Public (OS/P) Primary Area of the Historic District with underlying zoning of Open Space and Conservation (OSC). The applicant is seeking a Conditional Use Permit from the City to authorize their installation of a crematory.

Evaluation of Land Use and Planning

a) Physically divide an established community?

No Impact. No new building or road construction is proposed. The project site is located within an existing cemetery outside of established residential communities. No impact would occur.

- b) Cause significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

Less Than Significant Impact. The project site is designated as OS in the City of Folsom General Plan. The proposed action would not conflict with the intended uses of that designation. The project site is currently zoned Open Space/Public (OS/P) Primary Area of the Historic District with underlying zoning of Open Space and Conservation District (OSC). . The applicant is seeking a Conditional Use Permit (CUP) from the City to authorize their installation of a crematory. Granting of the CUP from the City would render any impacts less than significant.

XII. MINERAL RESOURCES

MINERAL RESOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The Folsom area regional geologic structure is defined by the predominantly northwest- to southeast-trending belt of metamorphic rocks and the strike-slip faults that bound them. The structural trend influences the orientation of the feeder canyons into the main canyons of the North and South Forks of the American River. This trend is interrupted where the granodiorite plutons outcrop (north and west of Folsom Lake) and where the metamorphic rocks are blanketed by younger sedimentary layers (west of Folsom Dam) (Wagner et al. 1981 in Geotechnical Consultants 2003). The four primary rock divisions found in the area are: ultramafic intrusive, metamorphic, granodiorite intrusive, and volcanic mud flows (Geotechnical Consultants 2003).

The presence of mineral resources within the City has led to a long history of gold extraction, primarily placer gold. No areas of the City are currently designated for mineral resource extraction (CDC 2020b).

Evaluation of Mineral Resources

- a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?
- b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan?

No Impact. The proposed project is not located in a zone of known mineral or aggregate resources. No active mining operations are present on or near the site. Implementation of the project would not interfere with the extraction of any known mineral resources. Thus, no impacts would result, and no mitigation would be necessary for questions a) and b).

XIII. NOISE

NOISE:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The project site is located southwest of the intersection of Forrest Street/Natoma Street and Folsom Boulevard, within an existing shed on the grounds of the Lakeside Memorial Lawn Cemetery. The proposed project would include the installation and operation of a crematory within the existing shed, and the installation of a small concrete pad and two propane tanks adjacent to the shed to power the crematory. According to the manufacturer, the crematory would generate approximately 60 decibels (dB) of noise during normal operation. Noise-sensitive land uses are land uses that may be subject to stress and/or interference from excessive noise, including residences, hospitals, schools, hotels, resorts, libraries, sensitive wildlife habitat, or similar facilities where quiet is an important attribute of the environment. Noise receptors (receivers) are individual locations that may be affected by noise. Noise-sensitive land uses in the project vicinity include nearby residences along Young Wo Circle, approximately 420 feet to the north/northwest, and the Folsom Village Mobile Home Park, located approximately 1,000 feet to the east across Folsom Boulevard.

Noise Terminology and Metrics

All noise level or sound level values presented herein are expressed in terms of decibels (dB), with A weighting (dBA) to approximate the hearing sensitivity of humans. Time-averaged noise levels are expressed by the symbol LEQ, with a specified duration.

The amplitude of pressure waves generated by a sound source determines the loudness of that source. Sound pressure amplitude is measured in micro-Pascals (mPa). One mPa is approximately one hundred billionth (0.0000000001) of normal atmospheric pressure. Sound pressure amplitudes for different kinds of noise environments can range from less than 100 to 100,000,000 mPa. Because of this wide range of values, sound is rarely expressed in terms of mPa. Instead, a logarithmic scale is used to

describe sound pressure level (SPL) in terms of dBA. The threshold of hearing for the human ear is about 0 dBA, which corresponds to 20 mPa.

Because decibels are logarithmic units, SPL cannot be added or subtracted through standard arithmetic. Under the decibel scale, a doubling of sound energy corresponds to a 3 dBA increase. In other words, when two identical sources are each producing sound of the same loudness, the resulting sound level at a given distance would be 3 dBA higher than from one source under the same conditions. For example, if one automobile produces an SPL of 70 dB when it passes an observer, two cars passing simultaneously would not produce 140 dBA—rather, they would combine to produce 73 dBA. Under the decibel scale, three sources of equal loudness together produce a sound level 5 dBA louder than one source.

Under controlled conditions in an acoustical laboratory, the trained, healthy human ear is able to discern 1 dBA changes in sound levels, when exposed to steady, single-frequency (“pure-tone”) signals in the mid-frequency (1,000 Hz–8,000 Hz) range. In typical noisy environments, changes in noise of 1 to 2 dBA are generally not perceptible. It is widely accepted, however, that people begin to detect sound level increases of 3 dB in typical noisy environments. Further, a 5 dBA increase is generally perceived as a distinctly noticeable increase, and a 10 dBA increase is generally perceived as a doubling of loudness.

Regulatory Framework

City of Folsom General Plan

The City of Folsom 2035 General Plan Safety and Noise Element provides the following goals and policies relative to noise that are applicable to this project:

GOAL SN 6.1: Protect the citizens of Folsom from the harmful effects of exposure to excessive noise and to protect the economic base of Folsom by preventing the encroachment of incompatible land uses within areas affected by existing noise-producing uses.

- SN 6.1.1 Noise Mitigation Strategies: Develop, maintain, and implement strategies to abate and avoid excessive noise exposure in the city by requiring that effective noise mitigation measures be incorporated into the design of new noise-generating and noise-sensitive land uses.
- SN 6.1.2 Noise Mitigation Measures: Require effective noise mitigation for new development of residential or other noise sensitive land uses to reduce noise levels as follows:
 - 2. For non-transportation-related noise sources: achieve compliance with the performance standards contained within Table SN-1 [Table 10].

Table 20. Noise Compatibility Standards

Land Use	Exterior Noise Level Standard for Outdoor Activity Areas ^a	Interior Noise Level Standard	
	L _{dn} /CNEL, dB	L _{dn} /CNEL, dB	L _{eq} , dB ^b
Residential (Low Density Residential, Duplex, Mobile Homes)	60 ^c	45	N/A
Residential (Multi Family)	65 ^d	45	N/A
Transient Lodging (Motels/Hotels)	65 ^d	45	N/A

Land Use	Exterior Noise Level Standard for Outdoor Activity Areas ^a	Interior Noise Level Standard	
	L _{dn} /CNEL, dB	L _{dn} /CNEL, dB	L _{eq} , dB ^b
Mixed-Use Developments	70	45	N/A
Schools, Libraries, Churches, Hospitals, Nursing Homes, Museums	70	45	N/A
Theaters, Auditoriums	70	N/A	35
Playgrounds, Neighborhood Parks	70	N/A	N/A
Golf Courses, Riding Stables, Water Recreation, Cemeteries	75	N/A	N/A
Office Buildings, Business Commercial and Professional	70	N/A	45
Industrial, Manufacturing, and Utilities	75	N/A	45

Notes: Where a proposed use is not specifically listed on this table, the use shall comply with the noise exposure standards for the nearest similar use as determined by the Community Development Department.

- a. Outdoor activity areas for residential developments are considered to be the back yard patios or decks of single-family residential units, and the patios or common areas where people generally congregate for multifamily development. Outdoor activity areas for nonresidential developments are considered to be those common areas where people generally congregate, including outdoor seating areas. Where the location of outdoor activity areas is unknown, the exterior noise standard shall be applied to the property line of the receiving land use.
- b. As determined for a typical worst-case hour during periods of use.
- c. Where it is not possible to reduce noise in outdoor activity areas to 60 dB, L_{dn}/CNEL or less using a practical application of the best-available noise reduction measures, an exterior level of up to 65 dB, L_{dn}/CNEL may be allowed provided that available exterior noise level reduction measures have been implemented and interior noise levels are in compliance with this table.
- d. Where it is not possible to reduce noise in outdoor activity areas to 65 dB, L_{dn}/CNEL or less using a practical application of the best-available noise reduction measures, an exterior level of up to 70 dB, L_{dn}/CNEL may be allowed provided that available exterior noise level reduction measures have been implemented and interior noise levels are in compliance with this table.

Folsom Municipal Code

Chapter 8.42 of the City of Folsom Municipal Code, entitled Noise Control, provides exterior noise level performance standards for stationary noise sources. In addition, this chapter also provides noise source exemptions which are applicable to this project.

8.42.040 Exterior noise standards.

- A. It is unlawful for any person at any location within the incorporated area of the city to create any noise, or to allow the creation of any noise, on property owned, leased, occupied or otherwise controlled by such person which causes the exterior noise level when measured at any affected single- or multiple-family residence, school church, hospital or public library situated in either the incorporated or unincorporated area to exceed the noise level standards as set forth in **Table 11**.

Table 11. Exterior Noise Level Standards

Noise Level Category	Cumulative Number of minutes in any 1-hour time period	Daytime (dB) (7 a.m. – 10 p.m.)	Nighttime (dB) (10 p.m. – 7 a.m.)
1	30	50	45
2	15	55	50
3	5	60	55
4	1	65	60
5	0	70	65

Note: dB = A-weighted decibels

Source: City of Folsom Code, Noise Control 1993.

- B. In the event the measured ambient noise level exceeds the applicable noise level standard in any category above, the applicable standard shall be adjusted so as to equal the ambient noise level.
- C. Each of the noise level standards specified above shall be reduced by 5 dB for simple tone noises, noises consisting primarily of speech or music, or for recurring noises.
- D. If the intruding noise source is continuous and cannot reasonably be discontinued or stopped for a time period whereby the ambient noise level can be measured, the noise level measured while the source is in operation shall be the noise level standards as specified above.

Noise Source Exemptions (Section 8.42.060)

Section 8.42.060 of the City of Folsom Municipal Code establishes the following activities that are considered exempt from the associated exterior noise provisions:

- A. Activities conducted in unlighted public parks, public playgrounds and public or private school grounds, during the hours of 7 a.m. to dusk, and in lighted public parks, public playgrounds and public or private school grounds, during the hours of 7 a.m. to 11 p.m., including but not limited to school athletic and school entertainment events;
- B. Any mechanical device, apparatus, or equipment used, related to or connected with emergency activities or emergency work;
- C. Noise sources associated with construction, provided such activities do not take place before 7 a.m. or after 6 p.m. on any day except Saturday or Sunday, or before 8 a.m. or after 5 p.m. on Saturday or Sunday;
- D. Noise sources associated with the maintenance of residential property provided such activities take place between the hours of seven a.m. to dusk on any day except Saturday or Sunday, between the hours of 8 a.m. to dusk on Saturday or Sunday;
- E. Noise sources associated with agricultural activities on agricultural property;
- F. (Section Expired)
- G. Noise sources associated with the collection of waste or garbage from property devoted to commercial or industrial uses;

H. Any activity to the extent regulation thereof has been preempted by state or Federal law.

Evaluation of Noise

- a) Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Less Than Significant Impact. Construction of the project would involve the use of a crane for several hours to unload the chiller and crematory from the truck, and the use of a mini excavator or skid steer loader for a day and one truck load of concrete to install a small pad for the two propane tanks. Construction equipment would be relatively small, given the small size of the project, and construction would be of short duration. Noise generated by construction may exceed the levels permitted by section 8.42.040 of the Folsom Municipal Code; however, construction activities are exempt from those requirements provided that they take place between 7:00 a.m. and 6:00 p.m. on any day except Saturday or Sunday, or between 8:00 a.m. and 5:00 p.m. on Saturday or Sunday (Folsom Municipal Code 8.42.060). The project applicant is required to comply with these requirements and ensure that all construction activities were limited to those windows.

The crematory is expected to generate 60 dB of noise during normal operations, which is below the 75 dB level authorized for cemeteries under item SN 6.1.2 of the City of Folsom 2035 General Plan Safety and Noise Element. Operation would be limited to between the hours of 7:00 a.m. and 10:00 p.m., so 60 dB would be the lowest standard applied by Folsom Municipal Code Section 8.42.040 for noise reaching the nearest sensitive receptor for 30 cumulative minutes of any hour during that time frame. That standard would further be lowered to 45 dB for recurring noises. The nearest sensitive receptors to the project site are the residences located along Young Wo Circle, approximately 420 feet to the north/northwest of the proposed crematory. A rough estimate of noise levels reaching these receptors was calculated. This discussion assumes that the 60 dB measurement provided by the manufacturer refers to noise levels ten feet from the proposed crematory during operation, and assumes spherical spreading of sound from the source to the receiver (i.e., 6 decibel decrease for each doubling of distance from the noise source). An additional offset for atmospheric absorption of -1.5 dB per thousand feet was applied to the computations.

Noise from the proposed crematory's normal operations would attenuate to approximately 27.5 dB by the time it reached the nearest sensitive receptors. This does not account for the added muffling effect of the shed containing the crematory. Consequently, the proposed project would not generate a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of applicable standards, and impacts from the project would be less than significant.

- b) Generation of excessive groundborne vibration or groundborne noise levels?

Less Than Significant Impact. Construction of the project would involve the use of a crane for several hours to unload the chiller and crematory from the truck, and the use of a mini excavator or skid steer loader for a day and one truck load of concrete to install a small pad for the two propane tanks. Construction equipment would be relatively small, given the small size of the project, and construction would be for a short duration. Operation of the proposed crematory is not expected to create any new sources of vibration that could be felt outside of the immediate vicinity of the device. Therefore, any impacts would be less than significant.

- c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

No Impact. No public airports or private airstrips are located within two miles of the project site. Therefore, residents of the proposed project would not be exposed to excessive noise levels from air activity, and no impact would occur.

XIV. POPULATION AND HOUSING

POPULATION AND HOUSING:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The proposed project includes the installation of a crematory within an existing maintenance shed at an existing cemetery. It also involves the installation of two 250-gallon propane tanks adjacent to one side of the shed.

Evaluation of Population and Housing

- a) Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?

No Impact. The project would not expand any existing service apart from providing an opportunity to cremate deceased individuals within the City of Folsom. It would not expand or provide any public service nor alter public access to any site, nor create significant new employment opportunities. No impact would occur.

- b) Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

No Impact. This project does not involve the demolition, alteration, or replacement of any housing. It would not affect local conditions to the degree that any residents would be compelled to move away. Therefore, no impact would occur.

XV. PUBLIC SERVICES

PUBLIC SERVICES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
a) Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The proposed project is in an area currently served by urban levels of utilities and services. Public services provided by the City of Folsom in the project area include fire, police, school, library, and park services. The site is served by all public utilities including domestic water, wastewater treatment, and storm water utilities.

The City of Folsom Fire Department provides fire protection services. There are four fire stations providing fire/rescue and emergency medical services within the City of Folsom with a fifth station planned near the eastern city limits. Station 35 is the nearest station to the project site and is located at 535 Glenn Drive, approximately 1.5 miles east of the project site. Station 36 is second nearest to the project site and is located at 9700 Oak Avenue, approximately 2.3 miles north of the project site. The Fire Department responds to over 6,000 requests for service annually with an average of 16.4 per day (City of Folsom 2020). The City of Folsom Police Department is located at 46 Natoma Street, approximately 1.5 miles northeast of the project site.

The project site is located within the Folsom Cordova Unified School District. Nearby schools include the Golden Valley Charter River School (approximately 2.7 miles west of the project site), Folsom Montessori School (0.6 miles east), Sutter Middle School (1.0 mile east), Folsom Lake High School (0.9 miles east) and Folsom Middle School (2.5 miles east). The nearest recreational feature to the site is the Jedediah Smith Memorial Trail which runs adjacent to the property near its west side. The trail runs along the shores of Lake Natoma and continues on to provide bike access to the City of Sacramento. The land to the south and west of the property, including the land surrounding the trail and the lake, is a part of the American River Parkway that is operated by the State of California Department of Parks and Recreation.

The Sacramento Municipal Utilities District (SMUD) would continue to supply electricity to the project site. The City of Folsom provides potable water and irrigation water to the site. The project site does not currently have a gas line; two 250-gallon propane tanks would be installed to provide fuel for the crematory. The project site does not currently have a sanitary sewer line.

The City of Folsom has a program of maintaining and upgrading existing utility and public services within the City. Similarly, all private utilities maintain and upgrade their systems as necessary for public convenience and necessity, and as technology changes.

Evaluation of Public Services

a) Fire protection?

Less Than Significant Impact. The City of Folsom Fire Department is capable of responding to structure and wildland fires in addition to hazardous materials incidents. As a professionally staffed department with two stations nearby, they would be able to respond quickly and effectively in the unlikely event of a fire or hazardous materials incident at the project site. Emergency vehicle access would be maintained throughout the project site to meet the Fire Department standards for fire engine maneuvering, location of fire engine to fight a fire, rescue access, and fire hose access to all sides of the building. The proposed project would not significantly increase fire service demands or render the current service level to be inadequate, and impacts would be less than significant.

b) Police protection?

Less Than Significant Impact. The proposed project would not increase public access to the site and would only marginally increase the presence of workers at the site. As such, no increase in calls for service to the police department is expected under normal operating conditions. In the unlikely event of a fire or other emergency involving the crematory or the propane tanks, police would likely be called to assist with incident command and to control access to the site. The Folsom Police Department and other mutual aid departments would have sufficient resources and manpower to accommodate such an assignment, and a Less Than Significant Impact would occur.

c) Schools?

No impact. The project would not induce population growth and would not increase or decrease demand for any school facilities. No impact would occur.

d) Parks?

No impact. The project would not induce population growth and would not increase or decrease demand for any park facilities. No impact would occur.

e) Other public facilities?

No impact. The project would not induce population growth and would not significantly increase or decrease demand for any public services or utilities. No impact would occur.

XVI. RECREATION

RECREATION:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The nearest recreational opportunity to the project side is adjacent to the western boundary of the property. The Jedediah Smith Memorial Trail and Lake Natoma both run along a north/south axis just west of the project site. The open space surrounding the lake and trail in the project vicinity is comprised of oak/gray pine woodland typical of the Sierra Nevada Foothills. The trail continues to the City of Sacramento and allows bike and pedestrian access. It is part of the American River Parkway. The portion of the parkway nearest the project area is administered by the California Department of Parks and Recreation.

Evaluation of Recreation

a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

No Impact. The proposed project would not induce population growth or increase tourism or public access or demand to any recreational site. It would not impair the quality of any existing site. No impact would occur.

b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

No Impact. The proposed project does not include any recreational facilities, nor would it induce demand for new recreational facilities. No impact would occur.

XVII. TRANSPORTATION

TRANSPORTATION:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

Parking and Access

The project site can be accessed from either northbound or southbound on Folsom Boulevard or from Westbound Natoma Street approaching this intersection with Folsom Boulevard. The site would be accessed by proceeding a very short distance west on Forrest Street to reach the entry gate to the cemetery and turning left onto Mormon Street.

Diagonal parking spaces can be found along both sides of Mormon Street. At the terminus of Mormon Street, members of the public may continue straight onto a main cemetery access road to find an additional parking lot. All existing parking spaces would be maintained. Access to the project site directly would continue to be provided by a smaller existing access road located at the southwestern terminus of Mormon Street. Both the smaller access road and the main access road can be reached at the terminus of Mormon Street, but the two roads do not form a continuous loop because of a fence line separating them. No new parking spaces or parking facilities would be constructed.

As the crematorium would not be located in or near a funeral home and would be separate from any funeral services or public gatherings provided by the project applicant, access would only need to accommodate a small number of staff members with business at the site.

Roadway System

Brief descriptions of the key roadways serving the project site are provided below.

Folsom Boulevard is a four-lane arterial (with additional turn lanes as needed) that operates at a posted speed limit of 50 mph within the project vicinity. It is non-divided south of the intersection with Natoma Street/Forrest Street and becomes divided north of the intersection. A light rail track runs parallel to Folsom Boulevard. The intersection of Folsom Boulevard and Natoma Street/Forrest Street is controlled by a traffic light. About five miles south of that intersection, Folsom Boulevard provides access to US

Route 50. Folsom Boulevard crosses Lake Natoma about one mile north of the intersection with Natoma Street/Forrest Street.

Natoma Street is a two-lane, non-divided road that operates at a posted speed limit of 25 mph within the project vicinity. It intersects with Folsom Boulevard on the eastern side of the intersection nearest the project site. On the western side, the road becomes known as *Forrest Street* and remains a non-divided, two lane road with turn lanes as needed. Natoma Street provides access to residential and mixed-use neighborhoods to the east, and Forrest Street provides access to the cemetery, a small number of residential neighborhoods, and additional parks and businesses.

Transit, Light Rail, Bicycle, and Pedestrian Facilities

Transit services in the City of Folsom are provided by the Folsom Stage Line bus service which, as of February 4, 2019, is now operated by Sacramento Regional Transit. The Folsom Stage Line bus service provides both Fixed-Route and Dial-A-Ride services exclusively within the Folsom city limits, Monday through Friday. The nearest bus stop is approximately 0.5 miles north of the project site on Folsom Boulevard and is served by the Route 10 bus. The next nearest stop is approximately 0.7 miles south of the project site along Folsom Boulevard and is served by the Route 30 bus.

Light rail access to the site is provided by the Sacramento Regional Transit District, Light Rail to Folsom (Gold Line). The nearest stations are approximately 0.5 miles north and 0.7 miles south of the project site, respectively. Both stations are along the eastern side of Folsom Boulevard.

Bicycles can access the site from either northbound or southbound on Folsom Boulevard, or westbound from Natoma Street. Additional bike access is provided by the Jedidiah Smith Memorial Trail, which can reach Forrest Street via Young Wo Circle just west of the project site.

Pedestrians can access the site through the same routes as described for bicycles. Crossing signals and crosswalks are provided at the intersection of Folsom Boulevard and Natoma Street/Forrest Street.

Airports

No private or public airports are located within the City of Folsom. The nearest public airfield is Mather Airport, located approximately 11.7 miles southwest of the project site. Cameron Airpark is a public use airport located approximately 13 miles northeast of the project site, and McClellan Airport is a privately-owned public use airport located approximately 17 miles west of the project site.

Emergency Access

The City of Folsom identifies most major streets in the City as emergency evacuation routes. The proposed project would not modify any major street and/or preclude their continued use as an emergency evacuation route.

Evaluation of Transportation

- a) Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?

No Impact. The project does not propose any alterations to any path of access for vehicle, transit, rail, bicycle, or pedestrian facilities. The project would not conflict with any plan, policy, or ordinance affecting the above categories. No impact would occur.

- b) Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?

No Impact. The proposed action would not be a destination accessible for members of the public and would only be visited by a small number of workers during construction and operation. The project would not result in a significant increase in vehicle miles traveled, and is located within one half mile of a major public transit stop (the Historic Folsom Station) which provides both bus and light rail service. No impact would occur.

- c) Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

No Impact. The project does not propose any alterations to any access roads or other routes, and would not increase traffic to a point that additional risk on existing routes would be incurred. No impact would occur.

- d) Result in inadequate emergency access?

No Impact. No alterations to any access road or right of way are proposed. Emergency vehicle access would be maintained throughout the project site to meet the Fire Department standards for fire engine maneuvering, location of fire engine to fight a fire, rescue access, and fire hose access to all sides of the building. No impact would occur.

XVIII. TRIBAL CULTURAL RESOURCES

TRIBAL CULTURAL RESOURCES:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:				
i. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ii. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

For discussion of the history of Native American use of the project area, see Section 9.0.V., Cultural Resources. This section is based on the Tribal Consultation Record for Compliance with Assembly Bill (AB) 52 and CEQA for the Lakeside Memorial Lawn Storage Shed Project, City of Folsom (ECORP 2021). That document is included as **Appendix C**, and a summary is provided below.

Regulatory Setting

Tribal Cultural Resources are defined in Section 21074 of the California PRC as sites, features, places, cultural landscapes (geographically defined in terms of the size and scope), sacred places, and objects with cultural value to a California Native American tribe that are either included in or determined to be eligible for inclusion in the CRHR, or are included in a local register of historical resources as defined in subdivision (k) of Section 5020.1, or are a resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Section 5024.1. Section 1(b)(4) of AB 52 established that only California Native American tribes, as defined in Section 21073 of the California PRC, are experts in the identification of Tribal Cultural Resources and impacts thereto.

AB 52 requires that the City of Folsom (City) provide notice to any California Native American tribes that have requested notice of projects subject to CEQA review and consult with tribes that responded to the notice within 30 days of receipt with a request for consultation. Section 21073 of the Public Resources Code (PRC) defines California Native American tribes as “a Native American tribe located in California that is on the contact list maintained by the NAHC for the purposes of Chapter 905 of the Statutes of 2004.” This includes both federally and non-federally recognized tribes. For the City of Folsom, these include the following tribes that previously submitted general request letters, requesting such noticing:

- Wilton Rancheria (letter dated January 13, 2020);
- Lone Band of Miwok Indians (letter dated March 2, 2016); and,
- United Auburn Indian Community (UAIC) of the Auburn Rancheria (letter dated November 23, 2015).

The purpose of consultation is to identify Tribal Cultural Resources (TCR) that may be significantly impacted by the proposed project and to allow the City to avoid or mitigate significant impacts prior to project approval and implementation. Section 21074(a) of the PRC defines TCRs, for the purpose of CEQA, as:

Sites, features, places, cultural landscapes (geographically defined in terms of the size and scope), sacred places, and objects with cultural value to a California Native American tribe that are either of the following:

- a) Included or determined to be eligible for inclusion in the California Register of Historical Resources; and/or
- b) Included in a local register of historical resources as defined in subdivision (k) of Section 5020.1; and/or
- c) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Section 5024.1. In applying the criteria set forth in subdivision (c) of Section 5024.1, for the purposes of this paragraph, the lead agency shall consider the significance of the resource to a California Native American tribe.

Because criteria A and B also meet the definition of a Historical Resource under CEQA, a TCR may also require additional consideration as a Historical Resource. TCRs may or may not exhibit archaeological, cultural, or physical indicators and can only be identified by a culturally affiliated tribe, which has been determined under State law to be the subject matter expert for TCRs.

CEQA requires that the City initiate consultation with tribes at the commencement of the CEQA process to identify TCRs. Furthermore, because a significant effect on a TCR is considered a significant impact on the environment under CEQA, consultation is required to develop appropriate avoidance, impact minimization, and mitigation measures. Therefore, in accordance with the requirements summarized above, the City carried out, or attempted to carry out, tribal consultation for the project.

City Consultation

Within 14 days of initiating CEQA review for the project, on November 25, 2020, the City sent project notification letters to the three California Native American tribes named above, which had previously submitted general consultation request letters pursuant to Section 21080.3.1(d) of the PRC. Each tribe

was provided a brief description of the project and its location, the contact information for the City's authorized representative, and a notification that the tribe has 30 days to request consultation.

The Lone Band of Miwok Indians did not respond to the City's notification letter, and therefore, the threshold for carrying out tribal consultation with that tribe under PRC 21080.3.1(e) was not met.

On December 11, 2020, and within the 30-day response timeframe, the City received an automated email from UAIC that acknowledged receipt of the City's notification letter, thanked the City for consulting with UAIC, and attached the tribe's consultation record for the project. The response did not include any information on TCRs and indicated that the Tribal Historic Preservation Department would review the project and respond; however, no further communication was received from UAIC. Because the tribe failed to provide comments or engage with the City pursuant to PRC 21082.3(d)(2), the City considers this consultation requirement complete.

On December 1, 2020, and within the 30-day response timeframe, a Wilton Rancheria representative responded by email to the City's initial notification letter and requested to formally initiate consultation under AB 52. She requested additional information regarding the project's environmental review process and provided Wilton Rancheria's recommended mitigation measures for TCRs, though she did not indicate that there were known TCRs within the project area. City staff shared additional details with the representative, including the Cultural Resources Inventory Report (ECORP 2020). Despite several attempts by the City to schedule a meeting with tribal representatives, the representatives did not engage with the City at any of these meeting times. Because the tribe failed to engage meaningfully with the City after a reasonable and good-faith effort composed of multiple attempts to meet with the tribe, pursuant to PRC 21082.3(d)(2), the City considers this consultation requirement complete.

All information relevant to the City's AB 52 consultation process is documented in Appendix C.

Should Wilton Rancheria, or any other culturally affiliated tribe, submit public comments, the City will consider them in accordance with Section 11(b) of AB 52; however, after completing the required notification and consultation procedures specified in AB 52 and the PRC, the City has not been provided any information about TCRs that could be affected by the proposed project. Therefore, the determination of impacts to TCRs is drawn from other lines of evidence, as summarized below.

Information about potential impacts to TCRs was drawn from the ethnographic context, the results of a search of the Sacred Lands File of the Native American Heritage Commission (NAHC), and the results of a cultural resources inventory prepared by ECORP (2020). The methods and results of these efforts are provided in ECORP (2020) and are hereby incorporated by reference (see summary in Section V, Cultural Resources).

Evaluation of Tribal Cultural Resources

- a) Cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:
 - i. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?
 - ii. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources

Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe?

Less Than Significant with Mitigation.

The ethnographic information reviewed for the project, including ethnographic maps, does not identify any villages, occupational areas, or resource procurement locations in or around the current project area. In addition, the Sacred Lands File failed to identify any sacred lands or tribal resources in or near the project area. The cultural resources survey did not reveal any Native American archaeological sites within or adjacent to the proposed project area. Finally, as summarized above, two of the three tribes notified of the project responded to the City's offer to consult; however, none provided any information about TCRs in the project area. This is not unexpected, as the project is in a highly disturbed environment and does not involve substantial ground disturbance.

In reviewing the lines of evidence summarized above, this project would not have an impact on known TCRs. There exists an extremely low potential for the discovery of previously unknown TCRs during project construction, but if TCRs were to be encountered, the project activity could result in a significant impact. Implementation of unanticipated discovery procedures, as provided in mitigation measure TCR-1 below, would reduce that impact to less than significant.

Mitigation Measure TCR-01: Unanticipated Discovery of Tribal Cultural Resources.

If potentially significant TCRs are discovered during ground disturbing construction activities, all work shall cease within 50 feet of the find. A Native American Representative from traditionally and culturally affiliated Native American Tribes that requested consultation on the project shall be immediately contacted and invited to assess the significance of the find and make recommendations for further evaluation and treatment, as necessary. If deemed necessary by the City, a qualified cultural resources specialist meeting the Secretary of Interior's Standards and Qualifications for Archaeology, may also assess the significance of the find in joint consultation with Native American Representatives to ensure that Tribal values are considered. Work at the discovery location cannot resume until the City, in consultation as appropriate and in good faith, determines that the discovery is either not a TCR, or has been subjected to culturally appropriate treatment, if avoidance and preservation cannot be accommodated.

XIX. UTILITIES AND SERVICE SYSTEMS

UTILITIES AND SERVICE SYSTEMS:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
a) Require or result in the relocation or construction of new water or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunication facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The cemetery is currently serviced with potable water and irrigation water from the City of Folsom. There is no need to seek a "will serve" letter as the City currently provides water and the crematory will not substantially increase flow demand. The City also provides solid waste collection and disposal services; the project is not expected to result in a significantly increased demand for solid waste removal.

The cemetery is currently served with an electricity supply from SMUD. Electrical connections already exist for the shed, and may be upgraded as needed as part of the proposed project. Installation and operation of the crematory would not result in a significant increase in demand for electricity on the project site.

The cemetery, including the shed, does not have an existing sewer line. This project would not require access to, nor construction of, a sewer line. Two 250-gallon propane tanks would be constructed along the northern edge of the shed to provide power for the crematorium.

Stormwater flows on the site are retained and drained to Lake Natoma. There would be no change in the hydrologic regime of the project site due to the installation or operation of the proposed project.

The City of Folsom employs a design process that includes coordination with potentially affected utilities as part of project development. The City of Folsom coordinates with the appropriate utility companies to plan and potentially expand existing utilities in the project area, including water, sewer, telephone, gas, and electricity. All utility services should be able to accommodate the proposed project as no increased demand is expected except for propane, which the applicant will purchase.

Evaluation of Utilities and Service Systems

- a) Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

Less Than Significant Impact. Existing levels of service are expected to be adequate for most utilities, as the project applicant does not predict a significant increase in demand. The only exception is propane. The site does not currently have a gas line, so the applicant is proposing to construct two 250-gallon propane tanks adjacent to the shed to power the crematory.

Since existing levels of service are adequate for all requirements except gas, and the applicant would supply the only additional utility requirement (propane) independently of utility companies and in accordance with all manufacturer's recommendations and safety practices, any impact would be less than significant.

- b) Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?

Less Than Significant Impact. The applicant does not predict a significantly increased demand for water as a result of this project. The project consists solely of the installation of a crematory and supporting systems, and would not require significantly increased water for sanitation, irrigation, consumption, or any other uses. Any impacts would be less than significant.

- c) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

No Impact. The site does not currently have any sewer lines. No need for a sanitary sewer would arise as a result of this project. Any stormwater that accumulates onsite is disposed of locally into the adjacent Lake Natoma. There would not be an increase in the amount of impervious surfaces on the site apart from the addition of two 250-gallon propane tanks and a 38.3 square foot concrete foundation to support them immediately adjacent to one of the buildings. There are no foreseeable changes to the hydrologic regime or to stormwater quality or quantity. No impact would occur.

- d) Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

No Impact. No increase in solid waste generation is expected as a result of this project apart from potentially a small and temporary increase during construction; any waste generated would be removed and disposed of by the contractor or the applicant. No impact would occur.

- e) Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

No Impact. No change to the amount of solid waste generated on the property or its disposal is anticipated as a result of this project. The City of Folsom provides solid waste, recycling, and hazardous materials collection services to its residential and business communities. In order to meet the State mandated 50 percent landfill diversion requirements stipulated under AB 939, the City has instituted several community-based programs. The City offers a door-to-door collection program for household hazardous and electronic waste, in addition to six "drop-off" recycling locations within the City.

After processing, solid waste is taken to the Kiefer Landfill, the primary municipal solid waste disposal facility in Sacramento County. The landfill facility sits on a 1,084-acre site in the community of Sloughhouse and has a remaining capacity of 112.9 million cubic yards. The estimated cease operation date for the landfill is January 1, 2064 (CalRecycle 2018). Kiefer Landfill has sufficient capacity to accommodate the solid waste disposal needs of the City of Folsom. No impact would occur.

XX. WILDFIRE

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:				
a) Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The project site is not located in or near a state responsibility area or lands classified as very high fire hazard severity zones (CAL FIRE 2020; CSG 2020).

Evaluation of Wildfire

- a) Substantially impair an adopted emergency response plan or emergency evacuation plan?
- b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?
- c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?
- d) Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

No Impact. The project site is not located in or near a state responsibility area or lands classified as very high fire hazard severity zones. Therefore, no impact would occur for questions a) through d).

XXI. MANDATORY FINDINGS OF SIGNIFICANCE

MANDATORY FINDINGS OF SIGNIFICANCE:	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are significant when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of past, present and probable future projects)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Evaluation of Mandatory Findings of Significance

- a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Less Than Significant Impact with Mitigation. The preceding analysis indicates that the proposed project has the potential to adversely affect air quality, cultural resources, and tribal cultural resources. See Sections 9.III, 9.V, and 9.XVIII of this Initial Study for discussion of the proposed project’s potential impacts on these environmental issue areas. With implementation of the mitigation measures identified in those Sections and reiterated below, and compliance with City programs and requirements identified in this report, impacts would be reduced to a less than significant level. No significant or potentially significant impacts would remain.

Evaluation of air quality impacts: Construction of the project would involve short-term/temporary emissions via the use of a crane for several hours to unload the chiller and crematory from the truck, and the use of a mini excavator or skid steer loader for a one day and one truck load of concrete to install a small pad for the two propane tanks.

According to the SMAQMD’s CEQA Guide, projects that are 35 acres or less in size generally will not exceed the SMAQMD’s construction NOX or PM thresholds of significance. However, all construction

projects regardless of the screening level are required to implement the SMAQMD's Basic Construction Emission Control Practices (also known as Best Management Practices [BMP]; SMAQMD 2020b). The BMPs satisfy the requirements of SMAQMD's Rule 403, Fugitive Dust, which requires every reasonable precaution not to cause or allow the emissions of fugitive dust from being airborne beyond the property line from which the emission originates. Construction of the project would not result in a considerable net increase of any criteria pollutant and the impact would be less than significant with implementation of **Mitigation Measure AIR-01**.

Evaluation of cultural resources impacts: A database records search was conducted for the project site, including a 0.25-mile buffer area, at the North Central Information Center (NCIC) of the California Historical Resources Information System (CHRIS) at California State University-Sacramento. Additionally, a pedestrian survey of the project site was conducted by ECORP Staff Archaeologist Laurel Zickler-Martin, RPA. Although no evidence of cultural resources of significance were noted on project site, the City recognizes that sensitive and/or protected resources could be unintentionally discovered during project construction. Further, workers must be aware of sensitive cultural resources in the vicinity of the project area (but not on the project site) that must be protected. With implementation of **Mitigation Measures CUL-01, CUL-02, and CUL-03**, the impacts would be reduced to a less than significant level and potentially cumulative impacts would be avoided.

Evaluation of tribal cultural resources impacts: The City of Folsom sent project notification letters to three California Native American tribes. One tribe did not respond, one responded but did not provide any information regarding TCRs, and one requested additional information and discussion but, following a good faith effort by the City, did not meet with staff and did not provide information regarding TCRs. The City relied on other methods, including those outlined in the Cultural Resources report (ECORP 2020, see Section 9.V and 9.XVIII), to evaluate the potential presence of TCRs. Although there is no evidence of tribal cultural resources occurring or having the potential to occur on the project site, the City recognizes that sensitive and/or protected resources could be unintentionally discovered during project construction. With implementation of **Mitigation Measure TCR-01**, the impacts would be reduced to a less than significant level and potentially cumulative impacts would be avoided.

- b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are significant when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of past, present and probable future projects)?

Less Than Significant Impact with Mitigation. While the project would indirectly contribute to cumulative impacts associated with increased urban development in the City and region, these impacts have previously been evaluated by the City and considered in development of the City's General Plan as set forth in this Initial Study. Key areas of concern are discussed in detail below.

Evaluation of air quality impacts: Construction of the project would involve short-term/temporary emissions via the use of a crane for several hours to unload the chiller and crematory from the truck, and the use of a mini excavator or skid steer loader for a one day and one truck load of concrete to install a small pad for the two propane tanks.

According to the SMAQMD's CEQA Guide, projects that are 35 acres or less in size generally will not exceed the SMAQMD's construction NOX or PM thresholds of significance. However, all construction projects regardless of the screening level are required to implement the SMAQMD's Basic Construction Emission Control Practices (also known as Best Management Practices [BMP]; SMAQMD 2020b). The

BMPs satisfy the requirements of SMAQMD's Rule 403, Fugitive Dust, which requires every reasonable precaution not to cause or allow the emissions of fugitive dust from being airborne beyond the property line from which the emission originates. Construction of the project would not result in a cumulatively considerable net increase of any criteria pollutant and the impact would be less than significant with implementation of **Mitigation Measure AIR-01**.

Evaluation of cumulative cultural resources impacts: A database records search was conducted for the project site, including a 0.25-mile buffer area, at the North Central Information Center (NCIC) of the California Historical Resources Information System (CHRIS) at California State University-Sacramento. Additionally, a pedestrian survey of the project site was conducted by ECORP Staff Archaeologist Laurel Zickler-Martin, RPA. Although no evidence of cultural resources of significance were noted on project site, the City recognizes that sensitive and/or protected resources could be unintentionally discovered during project construction. Further, workers must be aware of sensitive cultural resources in the vicinity of the project area (but not on the project site) that must be protected. With implementation of **Mitigation Measures CUL-01, CUL-02, and CUL-03**, the impacts would be reduced to a less than significant level and potentially cumulative impacts would be avoided.

Evaluation of cumulative tribal cultural resources impacts: The City of Folsom sent project notification letters to three California Native American tribes. One tribe did not respond, one responded but did not provide any information regarding TCRs, and one requested additional information and discussion but, following a good faith effort by the City, did not meet with staff and did not provide information regarding TCRs. The City relied on other methods, including those outlined in the Cultural Resources report (ECORP 2020, see Section 9.V and 9.XVIII), to evaluate the potential presence of TCRs. Although there is no evidence of tribal cultural resources occurring or having the potential to occur on the project site, the City recognizes that sensitive and/or protected resources could be unintentionally discovered during project construction. With implementation of **Mitigation Measure TCR-01**, the impacts would be reduced to a less than significant level and potentially cumulative impacts would be avoided.

- c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

Less Than Significant Impact. Because of site conditions, existing City regulations, and regulation of potential environmental impacts by other agencies, the proposed project would not have the potential to cause substantial adverse effects on human beings as demonstrated in the evaluation contained in this Initial Study. Therefore, impacts would be less than significant.

10.0 MITIGATION MONITORING AND REPORTING PROGRAM

A Mitigation Monitoring and Reporting Program (MMRP) has been prepared by the City per Section 15097 of the CEQA Guidelines and is presented in **Appendix D**.

11.0 INITIAL STUDY PREPARERS

City of Folsom

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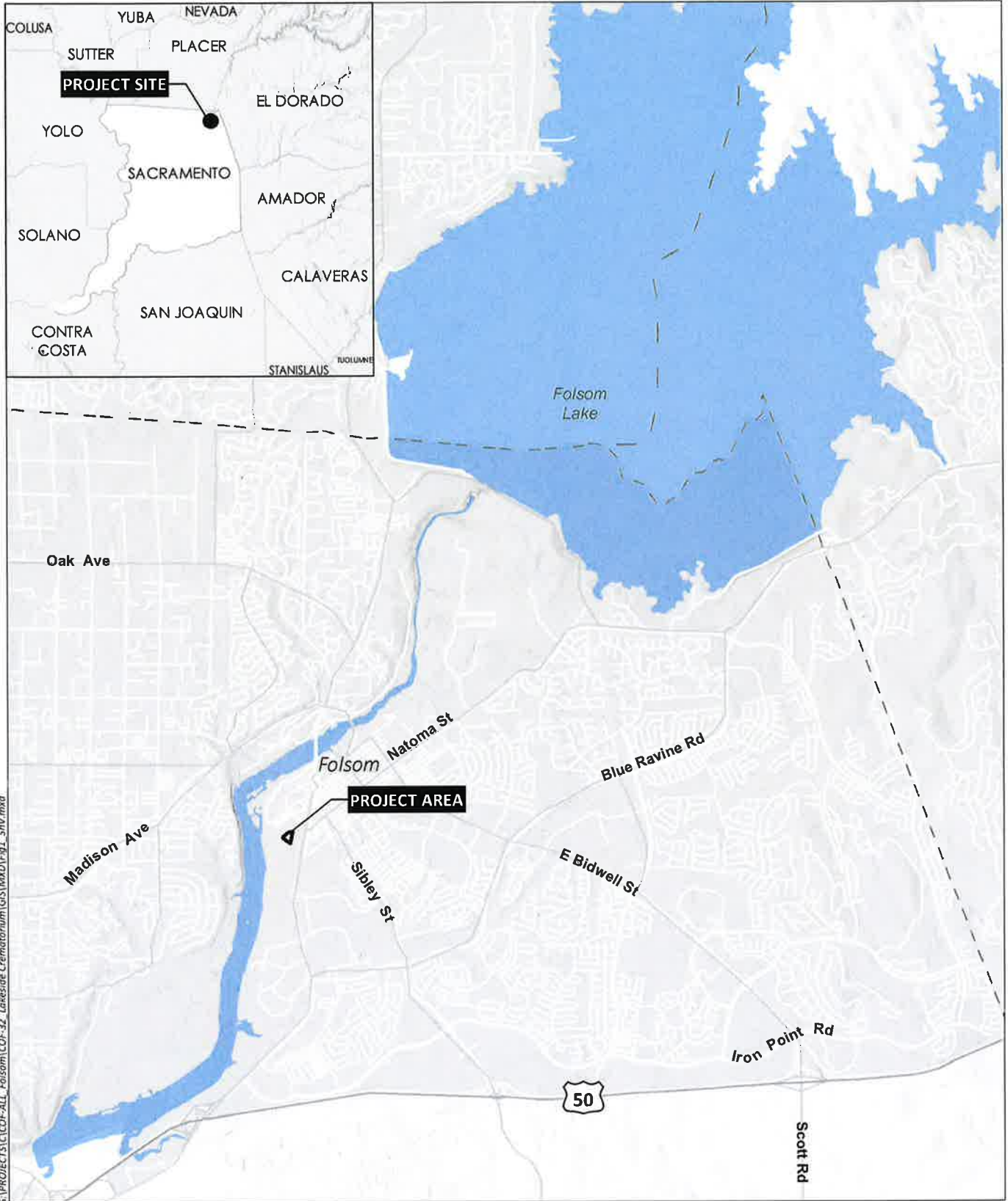
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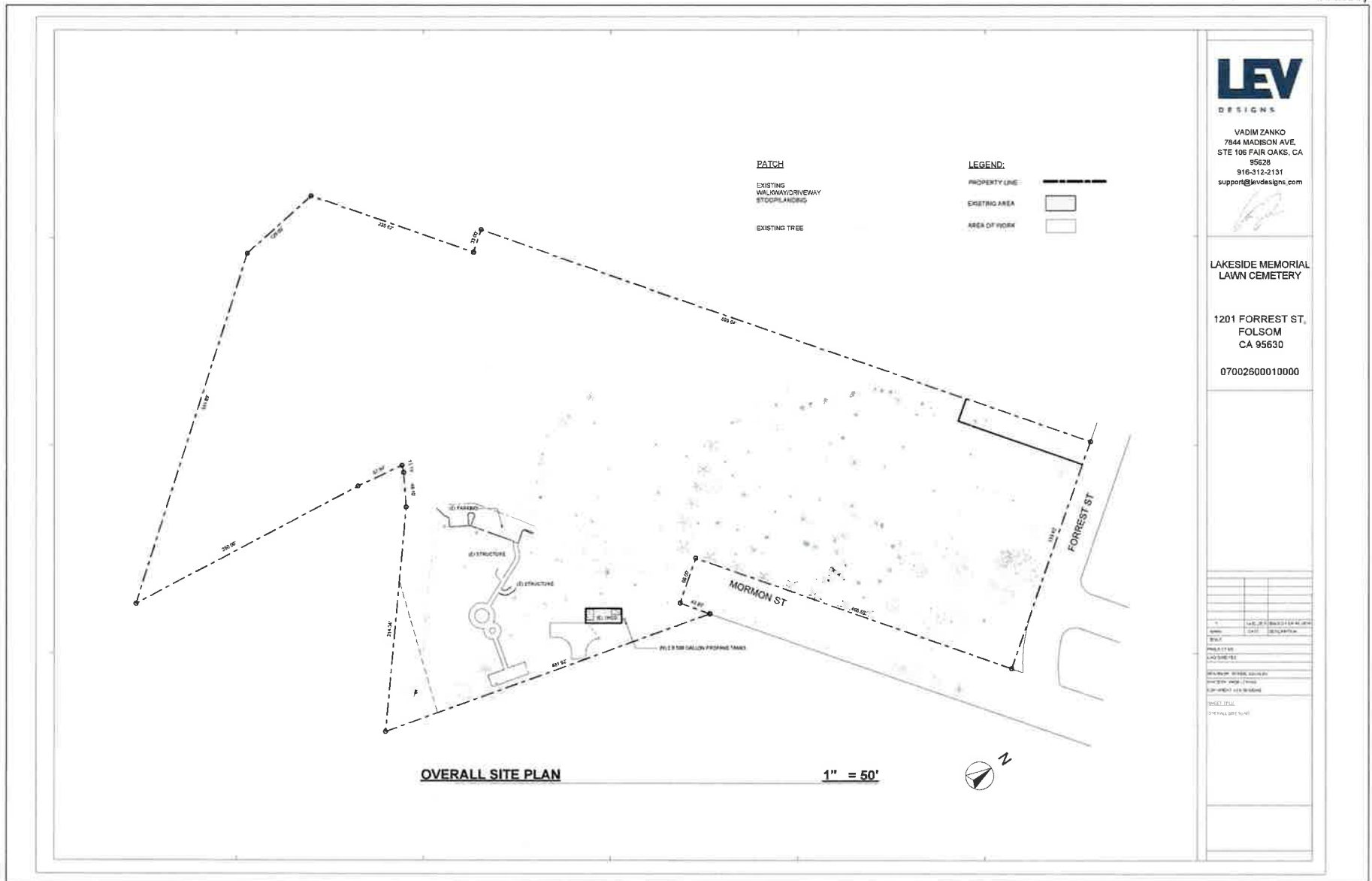
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Appendix A

Figures





Source: LEV Designs 2020



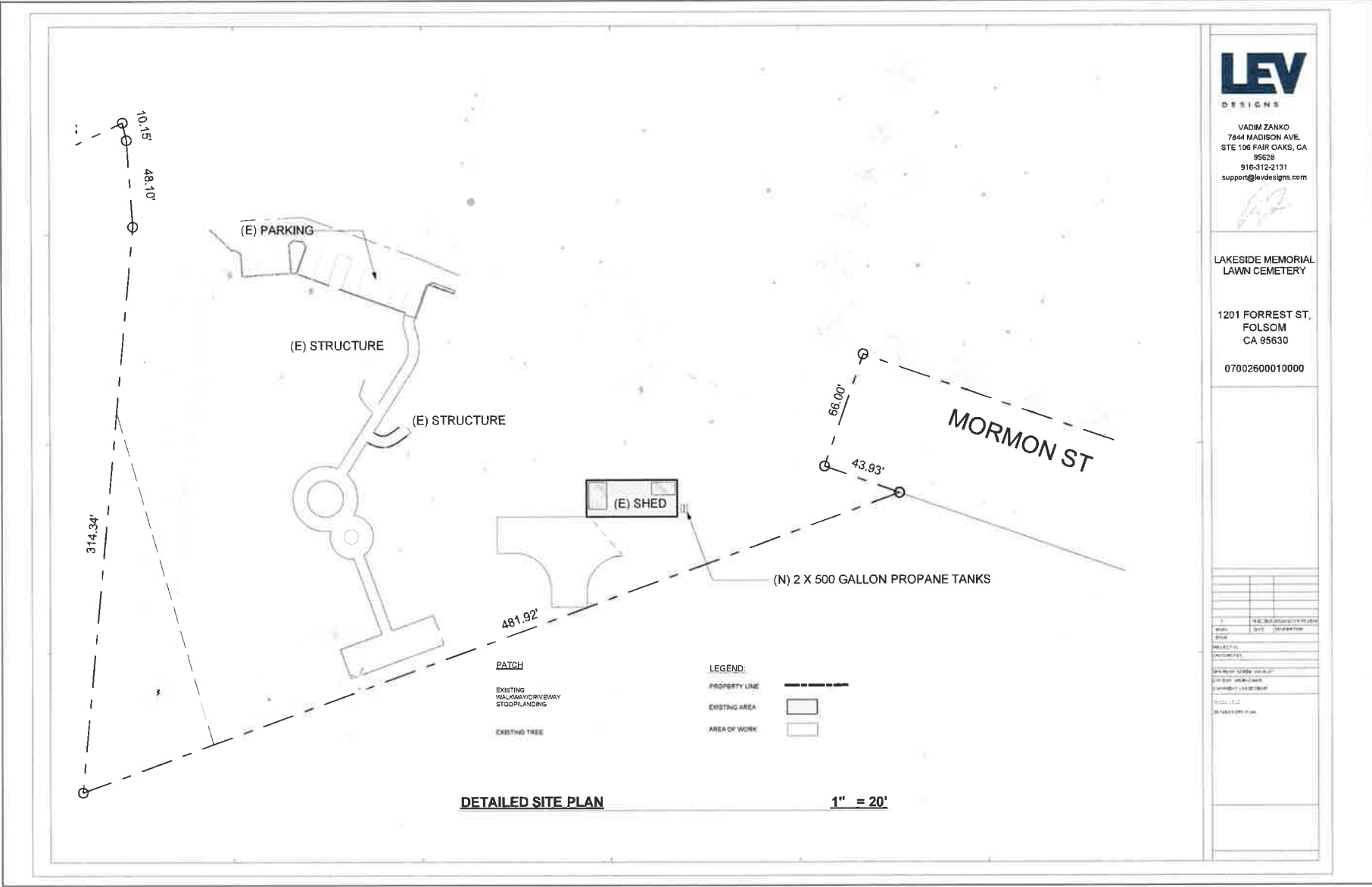
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95628
916-312-2131
support@levdesigns.com

LAKESIDE MEMORIAL
LAWN CEMETERY

1201 FORREST ST,
FOLSOM
CA 95630

07002600010000

NO.	DATE	DESCRIPTION
1	05/10/2022	PRELIMINARY
2	05/10/2022	FINAL



Source: LEV Designs 2020

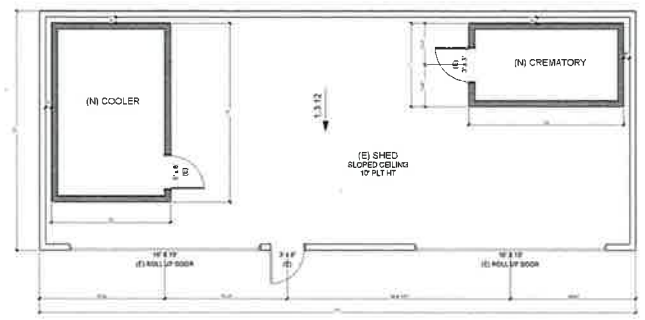


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LAWN CEMETERY

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FOLSOM
CA 95630

07002600010000



EXISTING FLOOR PLAN

1/4" = 1'-0"

- FLOOR PLAN NOTES.**
1. VERIFY ALL DIMENSIONS AND LOCATIONS WITH THE FIELD SURVEY.
 2. VERIFY ALL DIMENSIONS AND LOCATIONS WITH THE FIELD SURVEY.
 3. VERIFY ALL DIMENSIONS AND LOCATIONS WITH THE FIELD SURVEY.
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 9. VERIFY ALL DIMENSIONS AND LOCATIONS WITH THE FIELD SURVEY.
 10. VERIFY ALL DIMENSIONS AND LOCATIONS WITH THE FIELD SURVEY.

LEGEND
 (E) EXISTING
 (N) NEW

WALL LEGEND
 (E) 12" M.C.
 (N) 12" M.C.

PLAN AREA SQUARE FOOTAGE BREAKDOWN
 EXISTING SHED AREA 1,071 SQ. FT.

NO.	DESCRIPTION	DATE	BY	CHECKED BY
1	ISSUED FOR PERMIT			
2	ISSUED FOR PERMIT			
3	ISSUED FOR PERMIT			
4	ISSUED FOR PERMIT			
5	ISSUED FOR PERMIT			
6	ISSUED FOR PERMIT			
7	ISSUED FOR PERMIT			
8	ISSUED FOR PERMIT			
9	ISSUED FOR PERMIT			
10	ISSUED FOR PERMIT			

Source: LEV Designs 2020

Appendix B

Air Quality and Greenhouse Gas Emissions Assessment

HELIX Environmental Planning, Inc.
11 Natoma Street, Suite 150
Folsom, CA 95630
916.365.8700 tel
619.462.0552 fax
www.helixepi.com



December 4, 2020

Project # COF-32

Mr. Scott Johnson, AICP
Planning Manager
City of Folsom, Community Development Department
50 Natoma Street
Folsom, CA 95630

Subject: Folsom Lakeside Crematorium Project Air Quality and Greenhouse Gas Emissions Assessment

Dear Mr. Johnson:

HELIX Environmental Planning, Inc. (HELIX) has assessed the air quality and greenhouse gas (GHG) emissions associated with the construction and operation of the proposed Folsom Lakeside Crematorium Project (project), including a health risk assessment (HRA) to evaluate potential community health risks from the project's emissions. The analysis has been prepared to support environmental review under the California Environmental Quality Act (CEQA).

PROJECT LOCATION

The project would be constructed within an approximately 12-acre parcel in the City of Folsom (City) in Sacramento County, California. The project site is located west of the intersection of Forrest Street/Natoma Street with Folsom Boulevard, within the existing Lakeside Memorial Lawn Cemetery (See Figure 1, *Regional Location*, attached to this letter report).

PROJECT DESCRIPTION

The project would consist of installation of an HCT Apex-250 crematory, a 10-foot by 15-foot cooler, and associated electrical and propane improvements in an existing metal shed on the grounds of the existing Lakeside Memorial Lawn Cemetery. The shed would be modified to accommodate the equipment, but the shed would not be expanded beyond the existing 1,071 square feet footprint. Two 250-gallon propane tanks would be installed on a small concrete pad along the northern side of the shed to provide power for the crematory (see Figure 2, *Detailed Site Plan*, attached to this letter report).

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December 4, 2020

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AIR QUALITY ANALYSIS

Environmental Setting

The City of Folsom lies within the Sacramento Valley Air Basin (SVAB), near the southeastern edge. The SVAB consists of all or parts of eleven counties spanning from Solano and Sacramento counties to the south, and Shasta County to the north. The Sacramento Metropolitan Air Quality Management District (SMAQMD) is responsible for implementing emissions standards and other requirements of federal and state laws for Sacramento County, including the project area.

The climate of the SVAB is characterized by hot dry summers and mild rainy winters. During the year the temperature may range from 20 to 115 degrees Fahrenheit with summer highs usually in the 90s and winter lows occasionally below freezing. Average annual rainfall is about 20 inches with snowfall being very rare. The prevailing winds are moderate in strength and vary from moist breezes from the south to dry land flows from the north. The mountains surrounding the Sacramento Valley create a barrier to airflow, which can trap air pollutants in the valley when certain meteorological conditions are right and a temperature inversion (areas of warm air overlying areas of cooler air) exists. Air stagnation in the autumn and early winter occurs when large high-pressure cells lie over the valley. The lack of surface wind during these periods and the reduced vertical flow caused by less surface heating reduces the influx of outside air and allows pollutants to become concentrated in the air. The surface concentrations of pollutants are highest when these conditions are combined with increased levels of smoke or when temperature inversions trap cool air, fog and pollutants near the ground. The ozone season (May through October) in the SVAB is characterized by stagnant morning air or light winds with the breeze arriving in the afternoon out of the southwest from the San Francisco Bay. Usually the evening breeze transports the airborne pollutants to the north out of the SVAB. During about half of the days from July to September, however, a phenomenon called the "Schultz Eddy" prevents this from occurring. Instead of allowing for the prevailing wind patterns to move north carrying the pollutants out of the valley, the Schultz Eddy causes the wind pattern and pollutants to circle back southward. This phenomenon's effect exacerbates the pollution levels in the area and increases the likelihood of violating the federal and state air quality standards (SMAQMD 2020a).

Regulatory Setting

Criteria Pollutants

Ambient air quality is described in terms of compliance with state and national standards, and the levels of air pollutant concentrations considered safe, to protect the public health and welfare. These standards are designed to protect people most sensitive to respiratory distress, such as asthmatics, the elderly, very young children, people already weakened by other disease or illness, and persons engaged in strenuous work or exercise. The U.S. Environmental Protection Agency (USEPA), the federal agency that administrates the Federal Clean Air Act of 1970, as amended in 1990, has established national ambient air quality standards (NAAQS) for several air pollution constituents known as criteria pollutants, including: ozone (O₃); carbon monoxide (CO); coarse particulate matter (PM₁₀; particles 10 microns or less) and fine particulate matter (PM_{2.5}; particles 2.5 microns or less); sulfur dioxide (SO₂); and lead (Pb). As permitted by the Clean Air Act, California has adopted the more stringent California ambient air quality standards (CAAQS) and expanded the number of regulated air constituents. Ground-level ozone is not emitted directly into the environment but is generated from complex chemical and photochemical

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December 4, 2020

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reactions between precursor pollutants, primarily reactive organic gases (ROGs; also known as volatile organic compounds [VOCs]),¹ and oxides of nitrogen (NO_x). PM₁₀ and PM_{2.5} are generated from a variety of sources, including road dust, diesel exhaust, fuel combustion, tire and brake wear, construction operations and windblown dust. In addition, PM₁₀ and PM_{2.5} can also be formed through chemical and photochemical reactions of precursor pollutants in the atmosphere.

The California Air Resources Board (CARB) is required to designate areas of the state as attainment, nonattainment, or unclassified for the ambient air quality standards. An “attainment” designation for an area signifies that pollutant concentrations do not violate the standard for that pollutant in that area. A “nonattainment” designation indicates that a pollutant concentration violated the standard at least once. An “unclassified” designation indicates that insufficient data was available to determine the status. The air quality attainment status of Sacramento County is shown in Table 1, *Sacramento County Attainment Status*.

Table 1
SACRAMENTO COUNTY ATTAINMENT STATUS

Pollutant	State of California Attainment Status	Federal Attainment Status
Ozone (1-hour)	Nonattainment	No Federal Standard
Ozone (8-hour)	Nonattainment	Nonattainment
Coarse Particulate Matter (PM ₁₀)	Nonattainment	Attainment
Fine Particulate Matter (PM _{2.5})	Attainment	Nonattainment
Carbon Monoxide (CO)	Attainment	Attainment
Nitrogen Dioxide (NO ₂)	Attainment	Attainment
Lead	Attainment	Attainment
Sulfur Dioxide (SO ₂)	Attainment	Attainment
Sulfates	Attainment	No Federal Standard
Hydrogen Sulfide	Unclassified	No Federal Standard
Visibility Reducing Particles	Unclassified	No Federal Standard

Sources: SMAQMD 2020a.

Sacramento County is designated as nonattainment for the state and federal ozone standards, the state PM₁₀ standards, and the federal PM_{2.5} standards. The SMAQMD is responsible for implementing emissions standards and other requirements of federal and state laws in Sacramento County. Attainment plans for meeting the federal air quality standards are incorporated into the State Implementation Plan (SIP), which is subsequently submitted to the USEPA, the federal agency that administrates the Federal CAA of 1970, as amended in 1990. The current air quality plan applicable to the project, the *Sacramento Regional 2008 NAAQS 8-Hour Ozone Attainment and Reasonable Further Progress Plan* (Regional Ozone Plan), was developed by the SMAQMD and adjacent air district to describe how the air districts in and near the Sacramento metropolitan area will continue the progress toward attaining state and national ozone air quality standards (SMAQMD 2017).

¹ CARB defines and uses the term ROGs while the USEPA defines and uses the term VOCs. The compounds included in the lists of ROGs and VOCs and the methods of calculation are slightly different. However, for the purposes of estimating criteria pollutant precursor emissions, the two terms are often used interchangeably.

Letter to Mr. Scott Johnson
December 4, 2020

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Toxic Air Contaminants

Toxic air contaminants (TACs) are a diverse group of air pollutants that may cause or contribute to an increase in deaths or in serious illness or that may pose a present or potential hazard to human health. TACs can cause long-term chronic health effects such as cancer, birth defects, neurological damage, asthma, bronchitis, or genetic damage, or short-term acute effects such as eye watering, respiratory irritation (a cough), runny nose, throat pain, and headaches. TACs are considered either carcinogenic or noncarcinogenic based on the nature of the health effects associated with exposure to the pollutant. For carcinogenic TACs, there is no level of exposure that is considered safe and impacts are evaluated in terms of overall relative risk expressed as excess cancer cases per one million exposed individuals. Noncarcinogenic TACs differ in that there is generally assumed to be a safe level of exposure below which no negative health impact is believed to occur. These levels are determined on a pollutant-by-pollutant basis.

The Health and Safety Code (§39655[a]) defines TAC as “an air pollutant which may cause or contribute to an increase in mortality or in serious illness, or which may pose a present or potential hazard to human health.” All substances that are listed as hazardous air pollutants pursuant to subsection (b) of Section 112 of the CAA (42 United States Code Sec. 7412[b]) are designated as TACs. Under State law, the California Environmental Protection Agency (CalEPA), acting through CARB, is authorized to identify a substance as a TAC if it determines the substance is an air pollutant that may cause or contribute to an increase in mortality or an increase in serious illness, or that may pose a present or potential hazard to human health.

Crematories are a potential source of TACs as a result of trace metals and organic compounds that accumulate in the body throughout a person’s life and are released during combustion of human remains, and as a result of trace organic compounds that are formed in the combustion process. These TACs include: metals and inorganics (i.e., arsenic, beryllium, cadmium, chromium, copper, hydrogen fluoride, lead, mercury, nickel, selenium, zinc); VOCs (i.e., benzene, toluene, xylenes, vinyl chloride); aldehydes (i.e., acetaldehyde, formaldehyde); polyaromatic hydrocarbons (PAHs); polychlorinated dibenzodioxins (dioxins; PCDDs); and polychlorinated dibenzofurans (furans; PCDFs). Prolonged exposure to significant concentrations of these TACS can result in a variety of adverse health effects including cancers, chronic conditions, and/or acute conditions, depending on the substance and level of exposure. Based on the results of the HRA, described below, hexavalent chromium and mercury are the primary drivers of the health risks from crematory emissions because the health risks from crematory emissions of these substances are one or more orders of magnitude greater than the health risks from other TACs in crematory emissions.

Increased Cancer Risks – Hexavalent Chromium. Hexavalent chromium is a toxic form of the element chromium. Hexavalent chromium compounds are man-made and widely used in many different industries. Prolonged exposure to airborne hexavalent chromium may result in lung cancer. Although exposure to high levels of airborne hexavalent chromium may result in irritation or damage to the nose, throat, and lungs, breathing small amounts of hexavalent chromium even for long periods does not cause respiratory tract irritation in most people (Occupational Safety and Health Administration [OSHA] 2006).

Non-Cancer Chronic and Acute Health Risks – Mercury. Mercury is a naturally occurring element that is found in its elemental form (commonly known as quicksilver), in organic compounds which accumulate

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in fish and shellfish, and in inorganic compounds mainly occurring in contaminated drinking water. Mercury is a neurotoxin that can result in a range of chronic neurological disorders and developmental issues. The specific health effects of mercury are dependent on the form and amount of mercury in the exposure, the duration of the exposure, and the age of the individual (USEPA 2020).

Sensitive Receptors

Some land uses are considered more sensitive to air pollution than others due to the types of population groups or activities involved and are referred to as sensitive receptors. Examples of these sensitive receptors are residences, schools, hospitals, and daycare centers. CARB and the Office of Environmental Health Hazard Assessment (OEHHA) have identified the following groups of individuals as the most likely to be affected by air pollution: the elderly over 65, children under 14, infants (including in utero in the third trimester of pregnancy), and persons with cardiovascular and chronic respiratory diseases such as asthma, emphysema, and bronchitis (CARB 2005; OEHHA 2015).

Residential areas are considered sensitive receptors to air pollution because residents (including children and the elderly) tend to be at home for extended periods of time, resulting in sustained exposure to any pollutants present. Children and infants are considered more susceptible to health effects of air pollution due to their immature immune systems, developing organs, and higher breathing rates. As such, schools are also considered sensitive receptors, as children are present for extended durations and engage in regular outdoor activities.

The closest existing sensitive receptors to the project site are multiple single-family residences adjacent to the cemetery to the north, between 450 and 750 feet from the proposed crematory location, and mobile homes across Folsom Boulevard to the east, approximately 700 feet from the proposed crematory location, see Figure 3, *Receptor Locations*, attached to this letter report. The closest school to the project site is the Folsom Montessori School approximately 3,200 feet (0.6 miles) to the northeast.

Methods

Criteria Pollutant Emissions

Criteria pollutant and precursor emissions for long-term operation of the proposed crematory were calculated using propane combustion emissions factors from the USEPA AP-42 Compilation of Emissions Factors Chapter 1.5 (USEPA 2008), and crematory emissions factors provided by the SMAQMD, which combined USEPA AP-42 data and the USEPA Factor Information Retrieval Program (SMAQMD 2020b).

Crematory Health Risks

Potential health risks to nearby sensitive receptors from the emission of TACs during operation of the proposed crematory were analyzed after consultation with the SMAQMD and in accordance with the OEHHA Air Toxics Hot Spots Program Guidance Manual for Preparation of Health Risk Assessments (OEHHA 2015).

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TAC Emissions

Toxic emissions from the cremation process were estimated based on emissions factors provided by the SMAQMD and on maximum cremation process rates provided by Caring Service Group of 200 pounds per hour and 100,000 pounds per year. The TAC emissions factors provided by SMAQMD were based on a data in a test report from CARB that measured emissions from two propane-fires crematories (SMAQMD 2020b)

Dispersion Modeling

Localized concentrations of TACs were modeled using Lakes AERMOD View version 9.8.3. The Lakes program utilizes USEPA's AERMOD gaussian air dispersion model version 19191. Plot files from AERMOD using unitized emissions (one gram per second) from the crematory stack were imported into CARB's Hotspots Analysis and Reporting Program (HARP), Air Dispersion Modeling and Risk Tool (ADMRT) version 19121. The ADMRT calculated ground-level concentrations of TACs utilizing the imported plot files and the annual and hourly emissions inventory (provided in detail in Attachment A to this letter report).

Source Parameters

Based on data provided by the crematory manufacturer, emissions from the crematory were modeled as a point source emitting from the exhaust stack at 19.5 feet above the ground. The stack diameter was set at 20 inches, the exhaust gas temperature was set to 1080 degrees Fahrenheit (°F), the gas exit velocity was set to 14.7 feet per second, and the stack was assumed to have a rain cap resulting in a near-zero initial vertical gas velocity. Downwash from the existing shed housing the proposed crematory was modeled using the Building Profile Input Program (BPIP – a building preprocessing program for AERMOD).

Meteorological Data

SMAQMD provides pre-processed meteorological data suitable for use with AERMOD (SMAQMD 2014) for projects within Sacramento County. The available data set most representative of conditions in the project vicinity was from the Sacramento Executive Airport station, approximately 19 miles southwest of the project site. The Sacramento Executive Airport set includes 5 years of data collected between 2010 to 2014. Rural dispersion coefficients were selected in the model to reflect the existing undeveloped and open nature of the immediate project vicinity. A wind rose for the Sacramento Executive Airport shows an average speed of 6.6 miles per hour from the south (Iowa Environmental Mesonet 2019). The wind rose graphic is included in Attachment B to this letter.

Terrain Data

United States Geological Survey (USGS) National Elevation Dataset (NED) files with a 10-meter resolution covering an area approximately 500 meters (1,640 feet) around the project site were used in the model to cover the analysis area. Terrain data was imported to the model using AERMAP (a terrain preprocessing program for AERMOD).

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Receptor Modeling

To develop risk isopleths (linear contours showing equal level of risk) and ensure that the area of maximum impact was captured, receptors were placed in a cartesian grid 690 meters by 490 meters (approximately 2,264 feet by 1,608 feet), centered on the proposed crematory with a grid spacing of 10 meters (33 feet) and a receptor height (flagpole height) of 1.2 meters (4 feet) above the ground. Additional discrete receptors were placed at the residential property line of the 37 closest identified sensitive receptors and the 4 closest off-site worker buildings. See Figure 3 for the discrete receptor locations relative to the TAC source.

Risk Determination

Health risks resulting from localized concentration of TACs emitted by the proposed crematory were estimated using the ADMRT. The latest cancer slope factors, chronic Recommended Exposure Limits (RELs), acute RELs and exposure paths for all TACs, as designated by CARB, are included in the ADMRT. For the residential cancer risk, an exposure duration of 30 years was selected in accordance with the OEHHA (2015) guidelines. In accordance with OEHHA guidelines, the model conservatively assumes that residents would be standing and breathing outdoors at the location of the property line closest to the crematory every day between 17 and 21 hours per day (depending on the age group, starting with infants in utero in the third trimester of pregnancy) for 30 years. For off-site worker cancer risk, an exposure duration of 25 years was selected with an assumption of 8 hours per day, 5 days per week of exposure while standing outside. The mandatory minimum exposure pathways and the OEHHA derived breathing intake rate percentile method were selected.

Significance Criteria

The following potential air quality impacts are based on Appendix G of the State CEQA Guidelines, a significant impact is identified if the project would result in any of the following:

- a) *Conflict with or obstruct implementation of the applicable air quality plan?*
- b) *Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?*
- c) *Expose sensitive receptors to substantial pollutant concentrations?*
- d) *Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?*

While the final determination of whether or not a project has a significant effect is within the purview of the lead agency pursuant to CEQA Guidelines Section 15064(b), the SMAQMD has adopted screening tables and thresholds which lead agencies can use to determine the significance of a development project's short-term construction and long-term operational pollutant emissions. The SMAQMD's project-level thresholds of significance for mass emissions of criteria pollutant and precursors and exposure to TACs are shown in Table 2, *SMAQMD Significance Thresholds* (SMAQMD 2020c).

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Table 2
SMAQMD SIGNIFICANCE THRESHOLDS

Pollutant	Operational Emissions Threshold
ROG	65 pounds per day
NO _x	65 pound per day
PM10	80 pounds per day/14.6 tons per year ¹
PM2.5	82 pounds per day/15 tons per year ¹
TAC Exposure Incremental Increased Cancer Risk	10 in 1 million
TAC Exposure Non-Cancer Hazard Index	1

Source: SMAQMD 2020c

¹ Thresholds for PM is zero unless all feasible best available control technology/best management practices (BACT/BMPs) are applied.

Air Quality Impact Analysis

a) Conflict with or obstruct implementation of the applicable air quality plan?

Less than significant. Consistency with the air quality plan is determined by whether the project would hinder implementation of control measures identified in the air quality plan or would result in growth of population or employment that is not accounted for in local and regional planning. The SMAQMD's Regional Ozone Plan and the SIP are the applicable air quality plans for the projects developed within Sacramento County.

The project would be consistent with the General Plan land use designation of Open Space, but the project would require a conditional use permit to install and operate a crematory in the Open Space and Conservation zoning designation of the project site. The project would not result in population growth in the City and employment growth would be limited to a few personnel to operate the crematory. Therefore, the project would be consistent with the local and regional growth assumptions used in developing the Regional Ozone Plan and the SIP. In addition, as described in impact discussion b), below, the project would not result in a cumulatively considerable increase of any criteria pollutant. Therefore, the project would not conflict with or obstruct implementation of the applicable air quality plan and the impact would be less than significant.

b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

Construction (Short-Term) Emissions

Less than Significant. Construction of the project would involve the use of a crane for several hours to unload the chiller and crematory from the truck, and the use of a mini excavator or skid steer loader for a day and one truck load of concrete to install a small pad for the two propane tanks.

According to the SMAQMD's CEQA Guide, projects that are 35 acres or less in size generally will not exceed the SMAQMD's construction NO_x or PM thresholds of significance. However, all construction projects regardless of the screening level are required to implement the SMAQMD's Basic Construction Emission Control Practices (also known as Best Management Practices [BMPs]; SMAQMD 2020b). The BMPs satisfy the requirements of SMAQMD's Rule 403, *Fugitive Dust*, which requires every reasonable

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precaution not to cause or allow the emissions of fugitive dust from being airborne beyond the property line from which the emission originates. ROG emissions during construction are generally associated with the application of architectural coatings. The project does not propose any new structures and would not require substantial amounts of painting and would not result in significant emissions of ROGs. Therefore, construction of the project would not result in a cumulatively considerable net increase of any criteria pollutant and the impact would be less than significant.

Operation (Long-Term) Emissions

Less than Significant. The project would result in long-term operational emissions from vehicles that drive to and from the project and from operation of the crematory.

Because there are no crematoriums currently operating in Folsom, demand for cremation services is filled by transporting the deceased to facilities outside of the City. Therefore, operations of the project would not result in new vehicle trips (nor the associated emissions in the region). Instead, the project would replace existing regional vehicle trips with shorter trips (and reduced associated emissions).

Operation of a propane-fired crematory would be considered a new stationary source of emissions. The project may be subject to SMAQMD's Rule 201, *General Permit Requirements*, and Rule 202, *New Source Review*. The project would be required to implement best available control technology (BACT) for the minimization of emissions. BACT for crematories is incorporated into the product design in the form of controls which ensure maintenance of the correct temperatures and cycle times, and a secondary combustion chamber which ensures oxygenation and complete combustions of all fuels. As described in the Methods sections, above, Criteria pollutant and precursor emissions for long-term operation of the proposed crematory were calculated using propane combustion emissions factors from AP-42 and crematory emissions factors provided by SMAQMD. The project's calculated criteria and precursors operational emissions are compared to the SMAQMD thresholds in Table 3, *Operational Criteria Pollutant and Precursor Emissions*, a printout of the calculation sheets is included in Attachment A to this letter.

Table 3
OPERATIONAL CRITERIA POLLUTANT AND PRECURSOR EMISSIONS

Pollutant	Project Emissions	SMAQMD Threshold	Exceed Threshold?
Daily Emissions (pounds per day)			
ROG	0.1	65	No
NO _x	1.2	65	No
CO	0.9	None	No
SO _x	0.4	None	No
PM ₁₀	0.3	80	No
PM _{2.5}	0.3	82	No
Annual Emissions (tons per year)			
ROG	0.01	None	No
NO _x	0.15	None	No
CO	0.11	None	No
SO _x	0.05	None	No

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Pollutant	Project Emissions	SMAQMD Threshold	Exceed Threshold?
PM ₁₀	0.03	14.6	No
PM _{2.5}	0.03	15	No

Source: SMAQMD 2020b; SMAQMD 2020c

As shown in Table 3, the project's operational emissions of criteria pollutants and precursors would not exceed the SMAQMD daily or annual thresholds. Therefore, the project's operational emissions would not result in a cumulatively considerable net increase of any criteria pollutant and impacts would be less than significant.

c) *Expose sensitive receptors to substantial pollutant concentrations?*

Less than Significant. Crematories are a potential source of TACs as a result of trace metals and organic compounds that accumulate in the body and are released during combustion, and trace organic compounds that are formed in the combustion process. An HRA was conducted to determine potential community health risks from exposure to TACs emitted from the proposed crematory, as described in the Methods section above.

Health risks associated with cancer from development projects are estimated using the incremental excess cancer risk expressed as cancer cases per one million exposed individuals. The incremental excess cancer risk is an estimate of the chance a person exposed to specific sources of a TACs may have of developing cancer from that exposure beyond the individual's risk of developing cancer from existing background levels of pollutants in the ambient air. For context, the average cancer risk from TACs in the ambient air for an individual living in an urban area of California is 830 in 1 million (CARB 2015). Cancer risk estimates do not mean, and should not be interpreted to mean, that a person will develop cancer from estimated exposures to toxic air pollutants.

Health risks associated with chronic and acute effects from a development project are quantified using the maximum hazard index. A hazard index is the potential exposure to a substance divided by the reference exposure level (the level at which no adverse effects are expected). A hazard index of less than one indicates no adverse health effects are expected from the potential exposure to the substance. The maximum hazard index is the sum of hazard indices for pollutants with non-cancer health effects that have the same or similar adverse health effects.

The modeled point of maximum impact for the project (geographic point outside of the project site with the highest estimated incremental cancer risk and maximum hazard index) would be a point near the project boundary approximately 96 feet southeast of the proposed crematory exhaust stack, at approximately Universal Transverse Mercator (UTM) coordinates Zone 10, 657982 meters east, 4281757 meters north. The maximum health risk exposure at this point would be a residential incremental cancer risk of 3.2 in 1 million and a residential non-cancer chronic hazard index of 0.09. This point of maximum impact is in an area zoned as Open Space Conservation District containing dredge tailings from past gold mining. No residents or workers are anticipated to be at the point of maximum impact for prolonged periods.

The maximum estimated community incremental excess cancer, chronic and acute health risks due to exposure to the project TAC emissions from long term operation of the proposed crematory are

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presented in Table 4, *Maximum Exposed Individual Incremental Cancer Health Risk and Hazard Index*. These estimates are conservative (health protective) and assume that the resident or worker is outdoors for the entire exposure period. The modeled locations of the Maximum Exposed Individual Resident (MEIR) and the point of maximum impact, along with the residential cancer risk isopleths (contours of equal risk), are shown in Figure 4, *Cancer Risks*. The complete HRA model output, including tables of health risks for all modeled discrete receptors and isopleth figures for incremental cancer risk, non-cancer chronic hazard index and acute hazard index are included as Attachment B to this letter report.

Table 4
MAXIMUM EXPOSED INDIVIDUAL INCREMENTAL CANCER RISK AND HAZARD INDEX

	MEI Resident Cancer Risk	MEI Worker Cancer Risk	MEI Resident Chronic Hazard Index	MEI Worker Chronic Hazard Index	MEI Acute Hazard Index
Results	0.6 in 1 million	<0.1 in 1 million	0.02	0.02	0.20
Threshold	10 in 1 million	10 in 1 million	1	1	1
Exceed Threshold?	No	No	No	No	No

Source: Lakes AERMOD View version 9.8.3 and CARB ADMRT version 19121. See Attachment B for model inputs, outputs, and risk isopleths.

MEI = Maximum Exposed Individual.

As shown in Table 4, the maximum incremental increased cancer risks and maximum non-cancer chronic and acute hazard index due to exposure to TACs from long term operation of the proposed crematory would not exceed the SMAQMD thresholds. Therefore, operation of the project would not result in the exposure of sensitive receptors to substantial TAC concentrations and the impact would be less than significant.

d) Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

Diesel equipment could generate diesel exhaust odors during construction activities. The generation of odors during the construction period would be temporary, would last for a few days and would be dispersed within a short distance from the active work area. Once operational, potential odors from human remains prior to cremation would be minimized by either by immediately processing remains or by temporarily storing remains in the proposed refrigeration chiller. Operation of the crematory would not be a significant source odors or other emissions because the BACT features of the crematory, including process temperature and cycle time controls, and secondary combustion chambers which ensure the complete combustion of all solids, liquids, and gaseous fuels. Therefore, the project would not result in other emissions (such as those leading to odors) adversely affecting a substantial number of people and the impact would be less than significant.

GREENHOUSE GAS EMISSIONS

Environmental Setting

Global climate change refers to changes in average climatic conditions on Earth including temperature, wind patterns, precipitation, and storms. Global temperatures are moderated by atmospheric gases.

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These gases are commonly referred to as greenhouse gasses (GHGs) because they function like a greenhouse by letting sunlight in but preventing heat from escaping, thus warming the Earth's atmosphere.

GHGs are emitted by natural processes and human (anthropogenic) activities. Anthropogenic GHG emissions are primarily associated with: the burning of fossil fuels during motorized transport; electricity generation; natural gas consumption; industrial activity; manufacturing; and other activities such as deforestation, agricultural activity, and solid waste decomposition.

The GHGs defined under California's Assembly Bill (AB) 32, described below, include carbon dioxide (CO₂), methane (CH₄), nitrous oxide (N₂O), hydrofluorocarbons (HFCs), perfluorocarbons (PFCs), and sulfur hexafluoride (SF₆). Each GHG differs in its ability to absorb heat in the atmosphere based on the lifetime, or persistence, of the gas molecule in the atmosphere. Estimates of GHG emissions are commonly presented in carbon dioxide equivalents (CO₂e), which weigh each gas by its global warming potential (GWP). Expressing GHG emissions in CO₂e takes the contribution of all GHG emissions to the greenhouse effect and converts them to a single unit equivalent to the effect that would occur if only CO₂ were being emitted. GHG emissions quantities in this analysis are presented in metric tons (MT) of CO₂e. For consistency with United Nations Standards, modeling and reporting of GHGs in California and the U.S. use the GWPs defined in the Intergovernmental Panel on Climate Change's (IPCC) Fourth Assessment Report (IPCC 2007), as shown in Table 5, *Global Warming Potential and Atmospheric Lifetimes*.

Table 5
GLOBAL WARMING POTENTIAL AND ATMOSPHERIC LIFETIMES

Greenhouse Gas	Atmospheric Lifetime (years)	GWP
Carbon Dioxide (CO ₂)	50-200	1
Methane (CH ₄)	12	25
Nitrous Oxide (N ₂ O)	114	298
HFC-134a	14	1,430
PFC: Tetrafluoromethane (CF ₄)	50,000	7,390
PFC: Hexafluoroethane (C ₂ F ₆)	10,000	12,200
Sulfur Hexafluoride (SF ₆)	3,200	22,800

Source: IPCC 2007.

HFC: hydrofluorocarbon; PFC: perfluorocarbon

Regulatory Setting

The primary GHG reduction regulatory legislation and plans (applicable to the project) at the State, regional, and local levels are described below. Implementation of California's GHG reduction mandates is primarily under the authority of the California Air Resources Board (CARB) at the state level, SMAQMD and the Sacramento Area Council of Governments (SACOG) at the regional level, and the City at the local level.

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Executive Order S-3-05

On June 1, 2005, Executive Order (EO) S-3-05 proclaimed that California is vulnerable to climate change impacts. It declared that increased temperatures could reduce snowpack in the Sierra Nevada, further exacerbate California's air quality problems, and potentially cause a rise in sea levels. To avoid or reduce climate change impacts, EO S-3-05 calls for a reduction in GHG emissions to the year 2000 level by 2010, to year 1990 levels by 2020, and to 80 percent below 1990 levels by 2050. Executive Orders are not laws and can only provide the governor's direction to state agencies to act within their authority to reinforce existing laws.

Assembly Bill 32 – Global Warming Solution Act of 2006

The California Global Warming Solutions Act of 2006, widely known as AB 32, requires that CARB develop and enforce regulations for the reporting and verification of statewide GHG emissions. CARB is directed by AB 32 to set a GHG emission limit, based on 1990 levels, to be achieved by 2020. The bill requires CARB to adopt rules and regulations in an open public process to achieve the maximum technologically feasible and cost-effective GHG emission reductions.

Executive Order B-30-15

On April 29, 2015, EO B-30-15 established a California GHG emission reduction target of 40 percent below 1990 levels by 2030. The EO aligns California's GHG emission reduction targets with those of leading international governments, including the 28 nation European Union. California is on track to meet or exceed the target of reducing GHGs emissions to 1990 levels by 2020, as established in AB 32. California's new emission reduction target of 40 percent below 1990 levels by 2030 will make it possible to reach the goal established by EO S-3-05 of reducing emissions 80 percent under 1990 levels by 2050.

Senate Bill 32

Signed into law by Governor Brown on September 8, 2016, Senate Bill (SB) 32 (Amendments to the California Global Warming Solutions Action of 2006) extends California's GHG reduction programs beyond 2020. SB 32 amended the Health and Safety Code to include Section 38566, which contains language to authorize CARB to achieve a statewide GHG emission reduction of at least 40 percent below 1990 levels by no later than December 31, 2030. SB 32 codified the targets established by EO B-30-15 for 2030, which set the next interim step in the State's continuing efforts to pursue the long-term target expressed in EO B-30-15 of 80 percent below 1990 emissions levels by 2050.

California Air Resources Board

On December 11, 2008, the CARB adopted the Climate Change Scoping Plan (Scoping Plan) as directed by AB 32. The Scoping Plan proposes a set of actions designed to reduce overall GHG emissions in California to the levels required by AB 32. Measures applicable to development projects include those related to energy-efficiency building and appliance standards, the use of renewable sources for electricity generation, regional transportation targets, and green building strategy. Relative to transportation, the Scoping Plan includes nine measures or recommended actions related to reducing vehicle miles traveled (VMT) and vehicle GHGs through fuel and efficiency measures. These measures would be implemented statewide rather than on a project by project basis (CARB 2008).

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In response to EO B-30-15 and SB 32, all state agencies with jurisdiction over sources of GHG emissions were directed to implement measures to achieve reductions of GHG emissions to meet the 2030 and 2050 targets. The mid-term target is critical to help frame the suite of policy measures, regulations, planning efforts, and investments in clean technologies and infrastructure needed to continue driving down emissions (CARB 2014). In December 2017, CARB adopted the 2017 Climate Change Scoping Plan Update, the Strategy for Achieving California's 2030 Greenhouse Gas Target, to reflect the 2030 target set by EO B 30 15 and codified by SB 32 (CARB 2017).

Sacramento Metropolitan Air Quality Management District

The SMAQMD provides direction and recommendations for the analysis of GHG impacts of a project and approach to mitigation measures in its CEQA Air Quality Guidelines (SMAQMD 2020a).

Sacramento Area Council of Governments

As required by the Sustainable Communities and Climate Protection Act of 2008 (SB 375), SACOG has developed the 2020 Metropolitan Transportation Plan and Sustainable Communities Strategy. This plan seeks to reduce GHG and other mobile source emissions through coordinated transportation and land use planning to reduce VMT.

City of Folsom

As part of the 2035 General Plan, the City of Folsom prepared an integrated Greenhouse Gas Emissions Reduction Strategy (GHG Strategy) to identify and reduce current and future community GHG emissions and those associated with the City's municipal operations. Adopted on August 28, 2018, the GHG Strategy also serves as the City's "plan for the reduction of greenhouse gases", per Section 15183.5 of the CEQA Guidelines, which provides the opportunity for tiering and streamlining of project-level emissions for certain types of discretionary projects subject to CEQA review that are consistent with the General Plan. The GHG Strategy includes goals and strategies to reduce community and municipal GHG emissions, compared to the 2005 baseline year, by 15 percent in 2020, 51 percent in 2035, and 80 percent in 2050 (City 2018a; City 2018b).

Significance Criteria

The following potential air quality impacts are based on Appendix G of the State CEQA Guidelines, a significant impact is identified if the project would result in any of the following:

- a) *Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?*
- b) *Conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases?*

In accordance with CEQA Guidelines Sections 15064(h)(3), 15130(d), and 15183(b), a project's incremental contribution to a cumulative GHG emissions effect may be determined not to be cumulatively considerable if it complies with the requirements of a qualified plan for the reduction of

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greenhouse gases. The City General Plan Policy NCR 3.2.8 provides criteria for project-level streamlining and tiering (City 2018a):

Projects subject to environmental review under CEQA may be eligible for tiering and streamlining the analysis of GHG emissions, provided they are consistent with the GHG reduction measures included in the GHG Strategy contained in the General Plan and EIR. The City may review such projects to determine whether the following criteria are met:

- Proposed project is consistent with the current general plan land use designation for the project site;
- Proposed project incorporates all applicable GHG reduction measures (as documented in the Climate Change Technical Appendix to the General Plan EIR) as mitigation measures in the CEQA document prepared for the project; and,
- Proposed project clearly demonstrates the method, timing and process for which the project will comply with applicable GHG reduction measures and/or conditions of approval, (e.g., using a CAP/GHG reduction measures consistency checklist, mitigation monitoring and reporting plan, or other mechanism for monitoring and enforcement as appropriate).

Greenhouse Gas Emissions Impact Analysis

a) *Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?*

Less than significant. To determine consistency with the City's GHG Strategy the criteria outlined in the Greenhouse Gas Reduction Strategy Consistency Checklist are shown and discussed in Table 6, *GHG Strategy Checklist* (City 2018c).

Table 6
GHG STRATEGY CONSISTENCY CHECKLIST

Checklist Item	Consistent?	Discussion
Part 1: Land Use Consistency		
A. The proposed project is consistent with the City's 2035 General Plan land use and zoning designations. If "Yes," proceed to Part 2 of the Checklist.	Yes	The project would be located within the footprint of an existing building in an existing cemetery in an area designated Open Space in the General Plan and zoned Open Space and Conservation District (OSC). According to the City Zoning Code Chapter 17.39, a cemetery is an allowed use in the OSC zone with a use permit. While the project may require a new conditional use permit, the project would not require a General Plan amendment or rezone. The project would be consistent with existing project site use and land use designation the General Plan.

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Checklist Item	Consistent?	Discussion
Part 2: GHG Reduction Measures Consistency		
E-1: Improve Building Energy Efficiency in New Development	Not Applicable	The project does not propose new buildings or substantial modifications to existing buildings.
E-2: Water Heater Replacement in Existing Residential Development	Not Applicable	The project is not an existing residential development.
E-3: Improve Building Energy Efficiency in Existing Development	Not Applicable	The project's proposed equipment would be installed within an existing metal shed and would not include any conditioned or occupied building space .
E-4: Increase Use of Renewable Energy in Existing Development	Not Applicable	The project's proposed equipment would be installed within an existing metal shed. No expansion or retrofit of existing buildings are proposed.
T-1: Reduce VMT Through Mixed and High-Density Land Use	Not Applicable	The project does not propose, and the project site open space land use designation and zoning does not permit, high density development and mixed uses .
T-2: Improve Streets and Intersections for Multi-Modal Use and Access	Not Applicable	The project does not include construction of new streets or improvement to existing streets.
T-3: Adopt Citywide TDM Program	Not Applicable	The project is not a residential, office, commercial retail, public facility or school development. The project would not include new parking spaces .
T-5: Reduce Minimum Parking Standards	Not Applicable	The project would not include new parking spaces.
T-6: Require the Use of High-Performance Renewable Diesel in Construction Equipment	Not Applicable	The project would require minimal off-road diesel construction equipment. At most, a small excavator or skid steer loader may be used for a few hours to prepare an area for a small concrete pad .
T-8: Install Electric Vehicle Charging Stations	Not Applicable	The project is not a residential development, does not propose new parking spaces, and existing parking spaces at the project building are less than 10.
SW-1: Increase Solid Waste Diversion	Not Applicable	The project would involve minimal construction activity and would not result in substantial construction waste which could be diverted.
W-1: Increase Water Efficiency in New Residential Development	Not Applicable	The project is not a new residential development and the project does not propose new indoor or outdoor water uses.
W-2: Reduce Outdoor Water Use	Not Applicable	The project does not propose substantial addition, alteration, or expansion to existing facilities or new outdoor water uses.

Source: City 2018c

As discussed in Table 6, the project would be consistent with the project site general plan land use designation and none of the GHG reduction measures listed in the GHG Strategy are applicable to the project. Therefore, the project would be consistent with the City's GHG Strategy and the project would not generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment. The impact would be less than significant.

Letter to Mr. Scott Johnson
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- b) *Conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases?*

Less Than Significant. As discussed in criterion a), above, the project would be consistent with the City's integrated General Plan and GHG Strategy. The GHG strategy was developed to meet the City's GHG reduction targets which were formulated to meet the statewide GHG mandates of AB 32 and SB 32. Therefore, the project would not conflict with an applicable plan adopted for the purposes of reducing GHG emissions and the impact would be less than significant.

SUMMARY

The project's emissions of criteria pollutants and precursors would be below SMAQMD thresholds and would result in a less than significant impact. Community health risks resulting from emissions of TACs from the project's operation of a crematory were evaluated in an HRA following OEHHA guidelines. Project TAC emissions would not result in increased health risks beyond the SMAQMD thresholds and the impact would be less than significant. The project would not be a substantial source of objectional odors and odor impacts would be less than significant. The project would be consistent with the City's integrated General Plan and GHG Strategy and GHG emissions impacts would be less than significant. The project would not conflict an applicable plan adopted for the purposes of reducing GHG emissions and the impact would be less than significant.

Sincerely,



Martin Rolph
Air Quality Specialist



Victor Ortiz
Senior Air Quality Specialist

Attachments:

- Figure 1: Regional Location
- Figure 2: Detailed Site Plan
- Figure 3: Receptor Locations
- Figure 4: Cancer Risk
- Attachment A: Emissions Calculation Sheets
- Attachment B: HRA Model Output
- Attachment C: Addendum to the Folsom Lakeside Crematorium Project Air Quality and Greenhouse Gas Emissions Assessment

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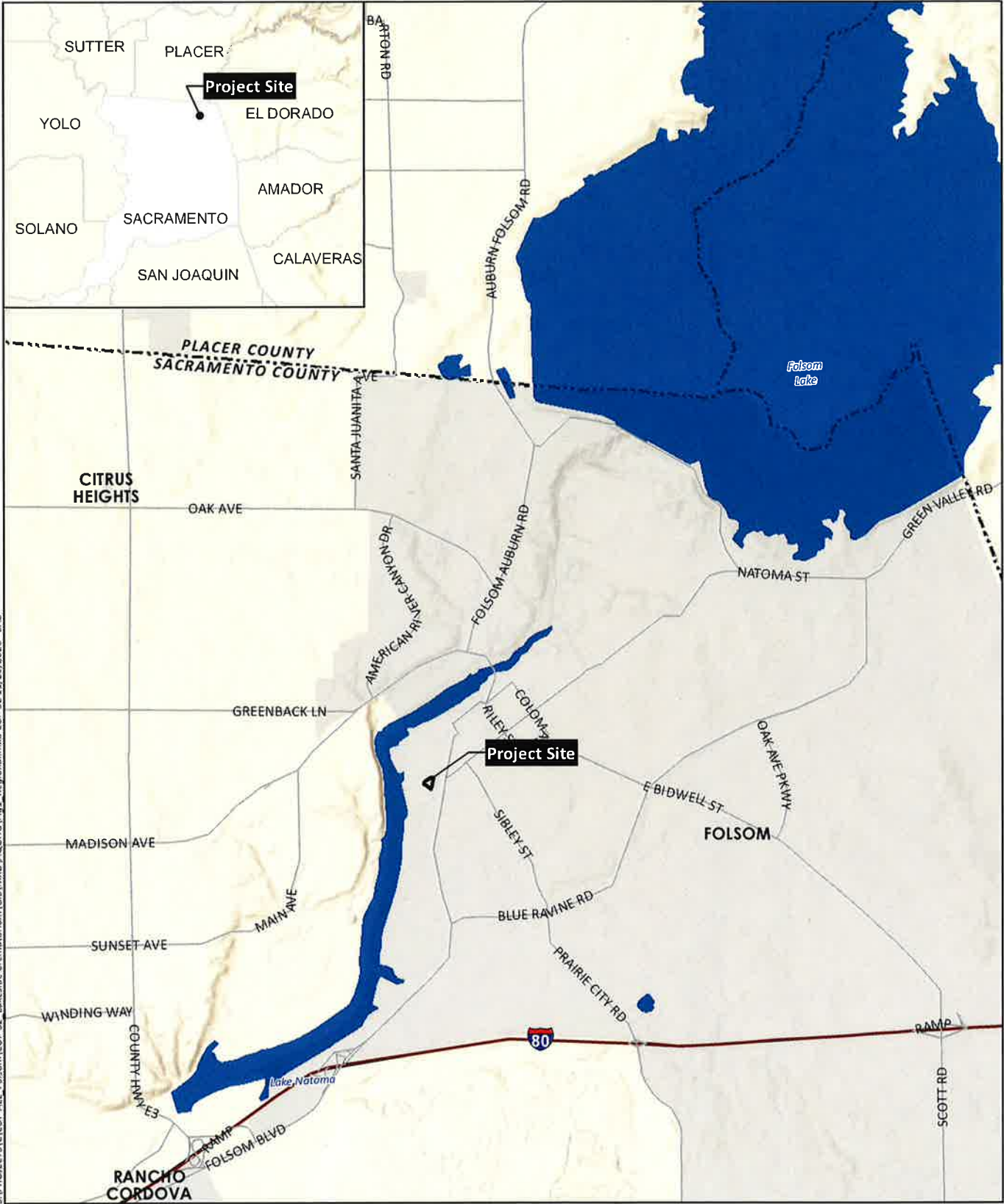
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Folsom Lakeside Crematorium

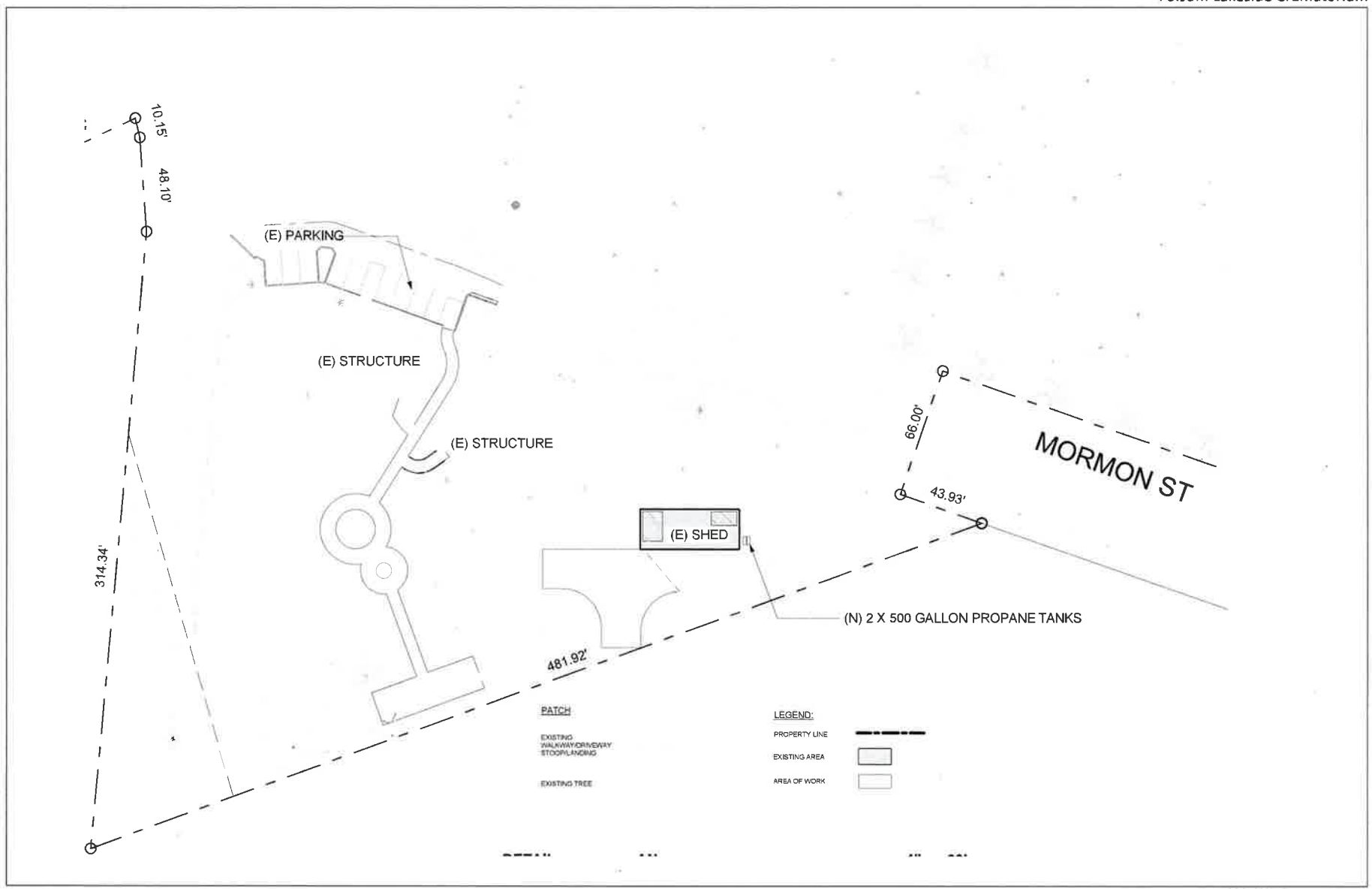


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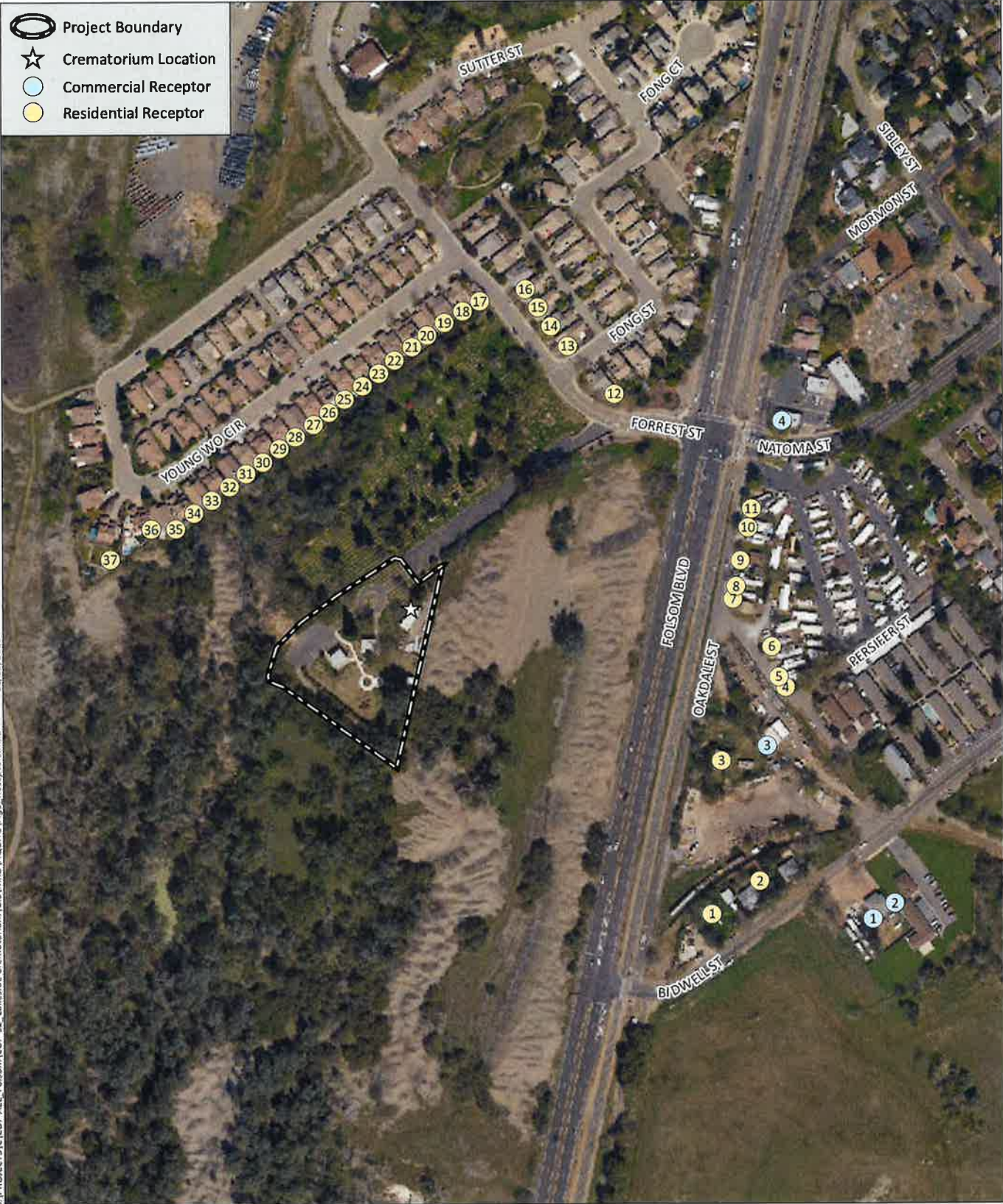
Source: Base Map Layers (ESRI, 2013)

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Source: LEV Designs 2020

-  Project Boundary
-  Crematorium Location
-  Commercial Receptor
-  Residential Receptor



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Source: Aerial (Maxar, 2019)





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Source: Aerial (Maxar, 2019)

Attachment A

Emissions Calculation Sheets

Crematory Criteria Pollutant Emissions

Emissions from Propane Combustion

KBTU/Cycle	1,800
KBTU/Gallon	91.502
Gallons/Cycle	19.672
Cycles/Day	2
Cycles/Year	500

Pollutant	Emission Factor (lbs/1000 gal) ¹	Emissions (lbs/day)	Emissions (tons/year)
ROG ³	1	0.04	0.005
NOx	13	0.51	0.064
SOx	0.054	0.00	0.000
PM10	0.7	0.03	0.003
PM2.5 ⁴	0.7	0.03	0.003
CO	7.5	0.30	0.037

Emissions from Combustion of Human Remains

	lbs/day	lbs/yr
Maximum Throughput	400	100,000

Pollutant	Emission Factor (lbs/ton) ²	Emissions (lbs/day)	Emissions (tons/year)
ROG	0.299	0.06	0.007
NOx	3.560	0.71	0.089
SOx	2.170	0.43	0.054
PM10	1.130	0.23	0.028
PM2.5 ⁴	1.130	0.23	0.028
CO	2.950	0.59	0.074

Total Emissions

Pollutant	Emissions (lbs/day)	Emissions (tons/year)
ROG	0.1	0.01
NOx	1.2	0.15
SOx	0.4	0.05
PM10	0.3	0.03
PM2.5	0.3	0.03
CO	0.9	0.11

Notes:

1. Emissions factors for propane from USEPA AP-42 Chapter 1, External Combustion Sources, Section 1.5 Liquefied Petroleum Gas Combustion, Table 1.5-1.
2. Emissions from combustion of human remains provided by SMAQMD and are from USEPA Factor Information REtrieval (FIRE) Program Data System (3/08).
3. ROG fraction of TOC for propane combustion unavailable, ROG assumed to be equal to TOC.

CREMATORY TAC EMISSIONS

Max hourly throughput (lbs)	200
Max annual throughput (lbs)	100,000

Substance	Test Results (in lbs/lbs charge) ¹	lbs/hr	lbs/year
Acetaldehyde	3.64E-07	7.27E-05	3.64E-02
Arsenic	2.52E-07	5.04E-05	2.52E-02
Benzene	1.77E-07	3.54E-05	1.77E-02
Beryllium	1.14E-08	2.28E-06	1.14E-03
Cadmium	8.59E-08	1.72E-05	8.59E-03
Chromium (Hex)	9.57E-08	1.91E-05	9.57E-03
Copper	2.17E-07	4.34E-05	2.17E-02
Formaldehyde	9.50E-08	1.90E-05	9.50E-03
Hydrogen Fluoride	4.01E-06	8.02E-04	4.01E-01
Lead	5.17E-07	1.03E-04	5.17E-02
Mercury ²	2.77E-05	4.16E-03	2.77E+00
Nickel	2.99E-07	5.98E-05	2.99E-02
Selenium	1.72E-07	3.44E-05	1.72E-02
Toluene	5.73E-06	1.15E-03	5.73E-01
Vinyl Chloride	1.85E-08	3.70E-06	1.85E-03
Xylenes	9.63E-08	1.93E-05	9.63E-03
Zinc	2.76E-06	5.51E-04	2.76E-01
Total PAHs	2.64E-08	5.28E-06	2.64E-03
Benzo[a]anthracene	6.67E-11	1.33E-08	6.67E-06
Benzo[a]pyrene	2.45E-10	4.90E-08	2.45E-05
Benzo[b]fluoranthene	5.61E-11	1.12E-08	5.61E-06
Benzo[k]fluoranthene	5.06E-11	1.01E-08	5.06E-06
Chrysene	3.49E-10	6.98E-08	3.49E-05
Dibenzo[a,h]anthracene	4.52E-11	9.04E-09	4.52E-06
Indeno[1,2,3-cd]pyrene	5.39E-11	1.08E-08	5.39E-06
Total PCDDs	1.50E-10	3.00E-08	1.50E-05
2,3,7,8-TCDD	5.11E-13	1.02E-10	5.11E-08
1,2,3,7,8-PeCDD	1.49E-12	2.98E-10	1.49E-07
1,2,3,4,7,8-HxCDD	1.77E-12	3.54E-10	1.77E-07
1,2,3,6,7,8-HxCDD	2.55E-12	5.10E-10	2.55E-07
1,2,3,7,8,9-HxCDD	3.16E-12	6.32E-10	3.16E-07
1,2,3,4,6,7,8-HpCDD	2.42E-11	4.84E-09	2.42E-06
Total PCDFs	2.31E-10	4.61E-08	2.31E-05
2,3,7,8-TCDF	3.43E-12	6.86E-10	3.43E-07
1,2,3,7,8-PeCDF	1.91E-12	3.81E-10	1.91E-07
2,3,4,7,8-PeCDF	5.82E-12	1.16E-09	5.82E-07
1,2,3,4,7,8-HxCDF	6.18E-12	1.24E-09	6.18E-07
1,2,3,6,7,8-HxCDF	5.49E-12	1.10E-09	5.49E-07
1,2,3,7,8,9-HxCDF	1.07E-11	2.15E-09	1.07E-06
2,3,4,6,7,8-HxCDF	2.23E-12	4.45E-10	2.23E-07
1,2,3,4,6,7,8-HpCDF	2.94E-11	5.89E-09	2.94E-06
1,2,3,4,7,8,9-HpCDF	1.79E-12	3.58E-10	1.79E-07

Notes:

1. Emissions factors provided by SMAQMD and are from CARB Test Report No. C-90-004, Evaluation Test on Two Propane-Fired Crematories at Camellia Memorial Lawn Cemetery (Oct. 29, 1992).

Attachment B

HRA Model Output

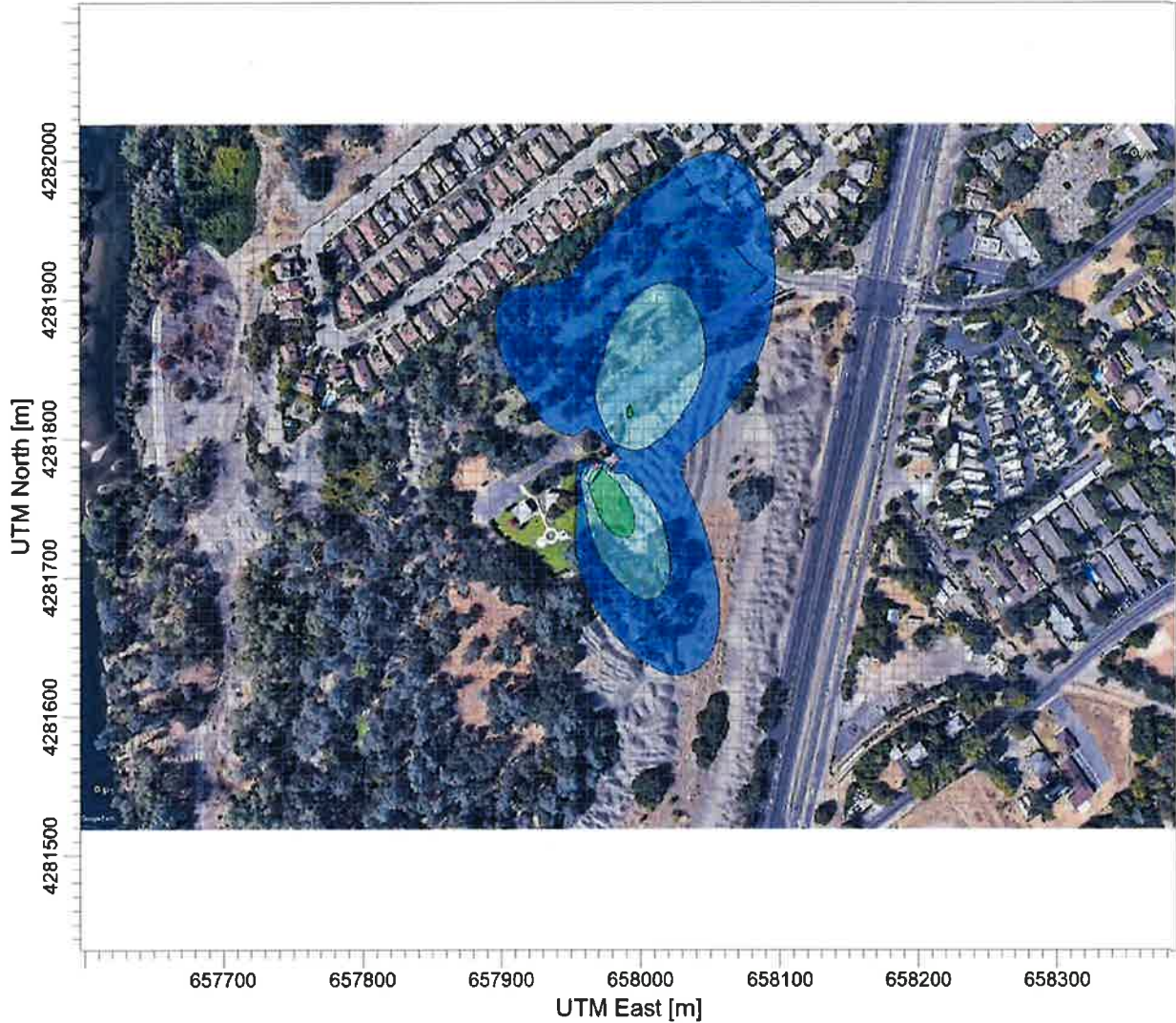
Residential Cancer Risk

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REC	GRP	NETID	X	Y	RISK_SUM	SCENARIO
R1	ALL		658172	4281577	1.30E-07	30YrCancerDerived_Inh_FAH3to70
R2	ALL		658204.3	4281599	9.77E-08	30YrCancerDerived_Inh_FAH3to70
R3	ALL		658177.6	4281681	1.02E-07	30YrCancerDerived_Inh_FAH3to70
R4	ALL		658221	4281731	6.16E-08	30YrCancerDerived_Inh_FAH3to70
R5	ALL		658216.2	4281738	6.25E-08	30YrCancerDerived_Inh_FAH3to70
R6	ALL		658211.2	4281758	6.15E-08	30YrCancerDerived_Inh_FAH3to70
R7	ALL		658184.6	4281790	7.50E-08	30YrCancerDerived_Inh_FAH3to70
R8	ALL		658186.7	4281798	7.55E-08	30YrCancerDerived_Inh_FAH3to70
R9	ALL		658189.5	4281816	7.98E-08	30YrCancerDerived_Inh_FAH3to70
R10	ALL		658194.3	4281838	8.82E-08	30YrCancerDerived_Inh_FAH3to70
R11	ALL		658196.8	4281851	9.45E-08	30YrCancerDerived_Inh_FAH3to70
R12	ALL		658103.2	4281928	4.65E-07	30YrCancerDerived_Inh_FAH3to70
R13	ALL		658071.8	4281960	5.80E-07	30YrCancerDerived_Inh_FAH3to70
R14	ALL		658060.4	4281973	5.87E-07	30YrCancerDerived_Inh_FAH3to70
R15	ALL		658051.7	4281986	5.62E-07	30YrCancerDerived_Inh_FAH3to70
R16	ALL		658043.1	4281998	5.21E-07	30YrCancerDerived_Inh_FAH3to70
R17	ALL		658012.3	4281990	4.88E-07	30YrCancerDerived_Inh_FAH3to70
R17	ALL		658000.9	4281983	4.77E-07	30YrCancerDerived_Inh_FAH3to70
R19	ALL		657988.3	4281975	4.58E-07	30YrCancerDerived_Inh_FAH3to70
R20	ALL		657977	4281966	4.42E-07	30YrCancerDerived_Inh_FAH3to70
R21	ALL		657966.5	4281958	4.25E-07	30YrCancerDerived_Inh_FAH3to70
R22	ALL		657954.9	4281949	4.10E-07	30YrCancerDerived_Inh_FAH3to70
R23	ALL		657944.2	4281940	4.07E-07	30YrCancerDerived_Inh_FAH3to70
R24	ALL		657933.3	4281932	4.17E-07	30YrCancerDerived_Inh_FAH3to70
R25	ALL		657921.4	4281923	4.44E-07	30YrCancerDerived_Inh_FAH3to70
R26	ALL		657910.8	4281914	4.80E-07	30YrCancerDerived_Inh_FAH3to70
R27	ALL		657900.6	4281906	4.93E-07	30YrCancerDerived_Inh_FAH3to70
R28	ALL		657888.2	4281897	4.44E-07	30YrCancerDerived_Inh_FAH3to70
R29	ALL		657877.8	4281889	3.79E-07	30YrCancerDerived_Inh_FAH3to70
R30	ALL		657866.5	4281880	2.97E-07	30YrCancerDerived_Inh_FAH3to70
R31	ALL		657855.3	4281872	2.28E-07	30YrCancerDerived_Inh_FAH3to70
R32	ALL		657844.1	4281863	1.73E-07	30YrCancerDerived_Inh_FAH3to70
R33	ALL		657832.5	4281854	1.34E-07	30YrCancerDerived_Inh_FAH3to70
R34	ALL		657820.3	4281845	1.06E-07	30YrCancerDerived_Inh_FAH3to70
R35	ALL		657808	4281834	8.38E-08	30YrCancerDerived_Inh_FAH3to70
R36	ALL		657791.5	4281834	6.80E-08	30YrCancerDerived_Inh_FAH3to70
R37	ALL		657764	4281814	4.64E-08	30YrCancerDerived_Inh_FAH3to70

PROJECT TITLE:

Residential Incremental Cancer Risk



COMMENTS: Risk in chances per million	SOURCES: 1	COMPANY NAME: HELIX Environmental Planning	
	RECEPTORS: 3541		
	OUTPUT TYPE: Risk	SCALE: 1:4,963 	
	MAX:	DATE: 11/21/2020	PROJECT NO.: COF-32

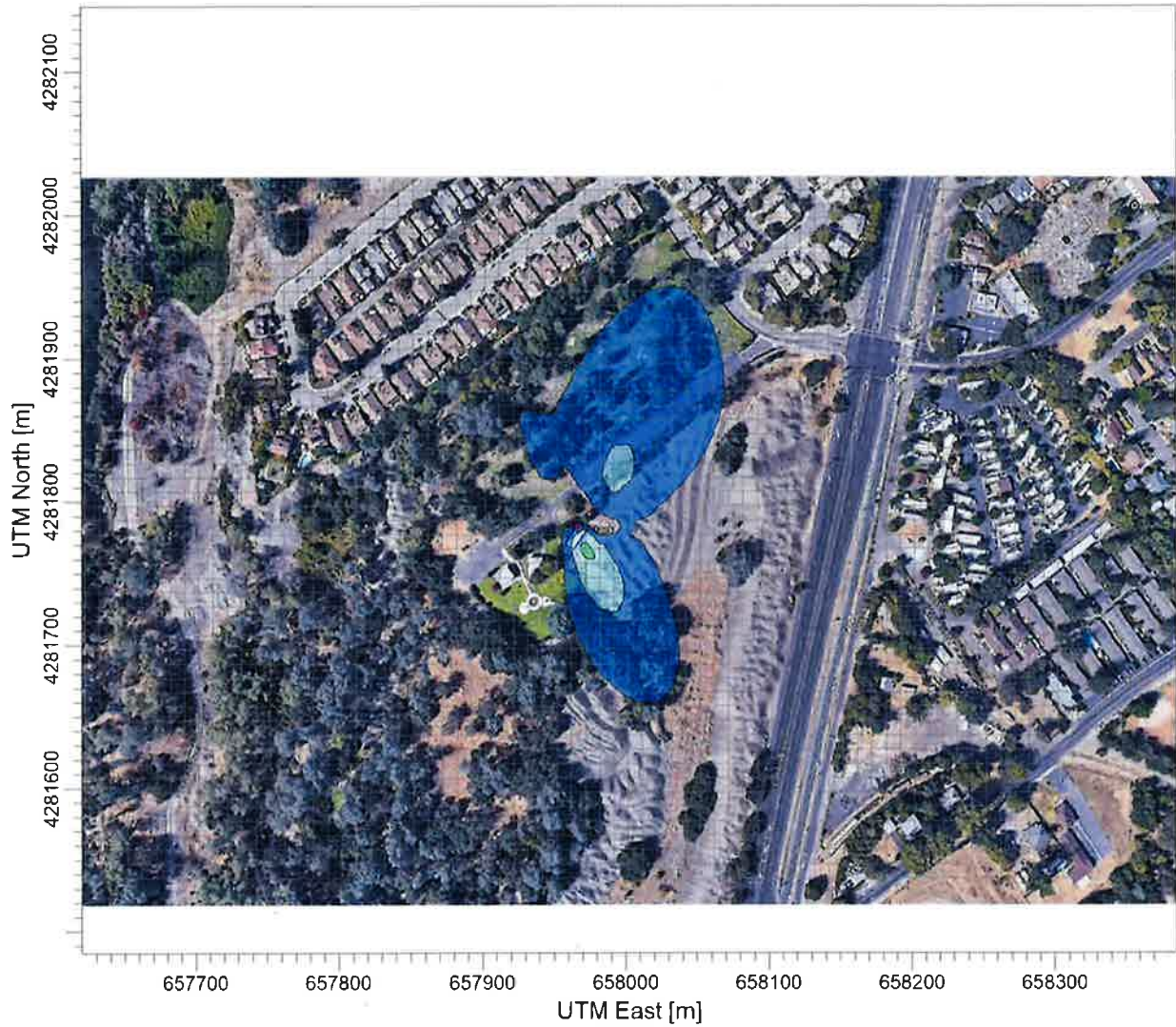
Residential Chronic Risk

*HARP - HRACalc v19044 11/20/2020 9:18:14 AM - Chronic Risk

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R2	ALL		658204.3	4281599	NonCancerChronicDerived_Inh	2.72E-03
R3	ALL		658177.6	4281681	NonCancerChronicDerived_Inh	2.85E-03
R4	ALL		658221	4281731	NonCancerChronicDerived_Inh	1.71E-03
R5	ALL		658216.2	4281738	NonCancerChronicDerived_Inh	1.74E-03
R6	ALL		658211.2	4281758	NonCancerChronicDerived_Inh	1.71E-03
R7	ALL		658184.6	4281790	NonCancerChronicDerived_Inh	2.08E-03
R8	ALL		658186.7	4281798	NonCancerChronicDerived_Inh	2.10E-03
R9	ALL		658189.5	4281816	NonCancerChronicDerived_Inh	2.22E-03
R10	ALL		658194.3	4281838	NonCancerChronicDerived_Inh	2.45E-03
R11	ALL		658196.8	4281851	NonCancerChronicDerived_Inh	2.63E-03
R12	ALL		658103.2	4281928	NonCancerChronicDerived_Inh	1.29E-02
R13	ALL		658071.8	4281960	NonCancerChronicDerived_Inh	1.61E-02
R14	ALL		658060.4	4281973	NonCancerChronicDerived_Inh	1.63E-02
R15	ALL		658051.7	4281986	NonCancerChronicDerived_Inh	1.56E-02
R16	ALL		658043.1	4281998	NonCancerChronicDerived_Inh	1.45E-02
R17	ALL		658012.3	4281990	NonCancerChronicDerived_Inh	1.36E-02
R17	ALL		658000.9	4281983	NonCancerChronicDerived_Inh	1.33E-02
R19	ALL		657988.3	4281975	NonCancerChronicDerived_Inh	1.27E-02
R20	ALL		657977	4281966	NonCancerChronicDerived_Inh	1.23E-02
R21	ALL		657966.5	4281958	NonCancerChronicDerived_Inh	1.18E-02
R22	ALL		657954.9	4281949	NonCancerChronicDerived_Inh	1.14E-02
R23	ALL		657944.2	4281940	NonCancerChronicDerived_Inh	1.13E-02
R24	ALL		657933.3	4281932	NonCancerChronicDerived_Inh	1.16E-02
R25	ALL		657921.4	4281923	NonCancerChronicDerived_Inh	1.23E-02
R26	ALL		657910.8	4281914	NonCancerChronicDerived_Inh	1.34E-02
R27	ALL		657900.6	4281906	NonCancerChronicDerived_Inh	1.37E-02
R28	ALL		657888.2	4281897	NonCancerChronicDerived_Inh	1.23E-02
R29	ALL		657877.8	4281889	NonCancerChronicDerived_Inh	1.05E-02
R30	ALL		657866.5	4281880	NonCancerChronicDerived_Inh	8.27E-03
R31	ALL		657855.3	4281872	NonCancerChronicDerived_Inh	6.34E-03
R32	ALL		657844.1	4281863	NonCancerChronicDerived_Inh	4.81E-03
R33	ALL		657832.5	4281854	NonCancerChronicDerived_Inh	3.72E-03
R34	ALL		657820.3	4281845	NonCancerChronicDerived_Inh	2.95E-03
R35	ALL		657808	4281834	NonCancerChronicDerived_Inh	2.33E-03
R36	ALL		657791.5	4281834	NonCancerChronicDerived_Inh	1.89E-03
R37	ALL		657764	4281814	NonCancerChronicDerived_Inh	1.29E-03

PROJECT TITLE:

Residential Maximum Non-Cancer Chronic Hazard Index



COMMENTS: Risk in maximum hazard index	SOURCES: 1	COMPANY NAME: HELIX Environmental Planning	
	RECEPTORS: 3541		
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	MAX:	DATE: 11/21/2020	PROJECT NO.: COF-32

AERMOD View - Lakes Environmental Software

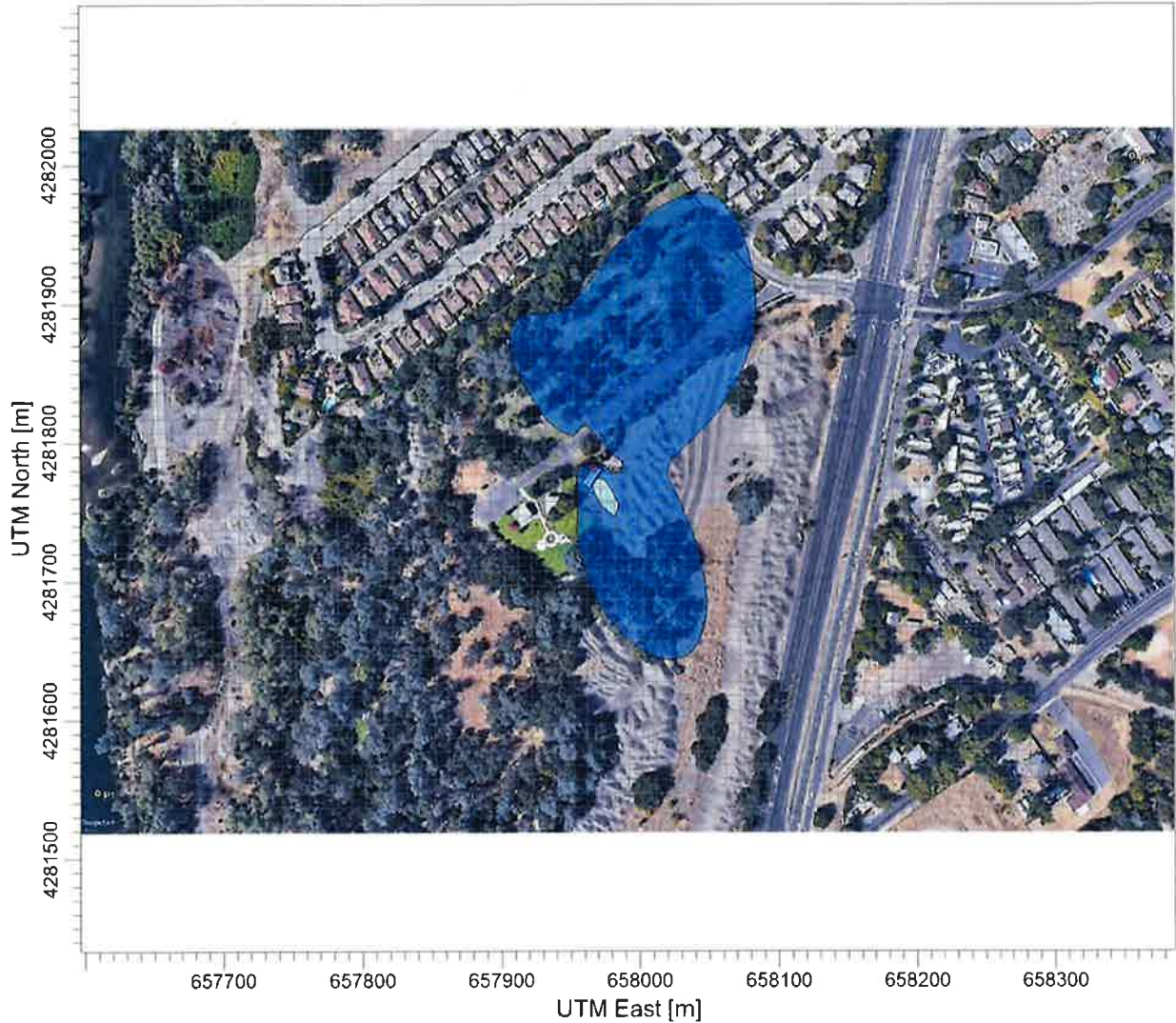
Worker Cancer Risk

*HARP - HRACalc v19044 11/20/2020 8:51:22 AM - Cancer Risk

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C2	ALL		658296.2	4281585	9.41E-09	25YrCancerDerived_InhSoilDerm
C2	ALL		658208.6	4281691	1.30E-08	25YrCancerDerived_InhSoilDerm
C4	ALL		658217.1	4281910	2.02E-08	25YrCancerDerived_InhSoilDerm

PROJECT TITLE:

Worker Incremental Cancer Risk



0.100

0.500

COMMENTS:

Risk in chances per million

SOURCES:

1

COMPANY NAME:

HELIX Environmental Planning

RECEPTORS:

3541

OUTPUT TYPE:

Risk

SCALE:

1:4,963



MAX:

DATE:

11/21/2020

PROJECT NO.:

COF-32

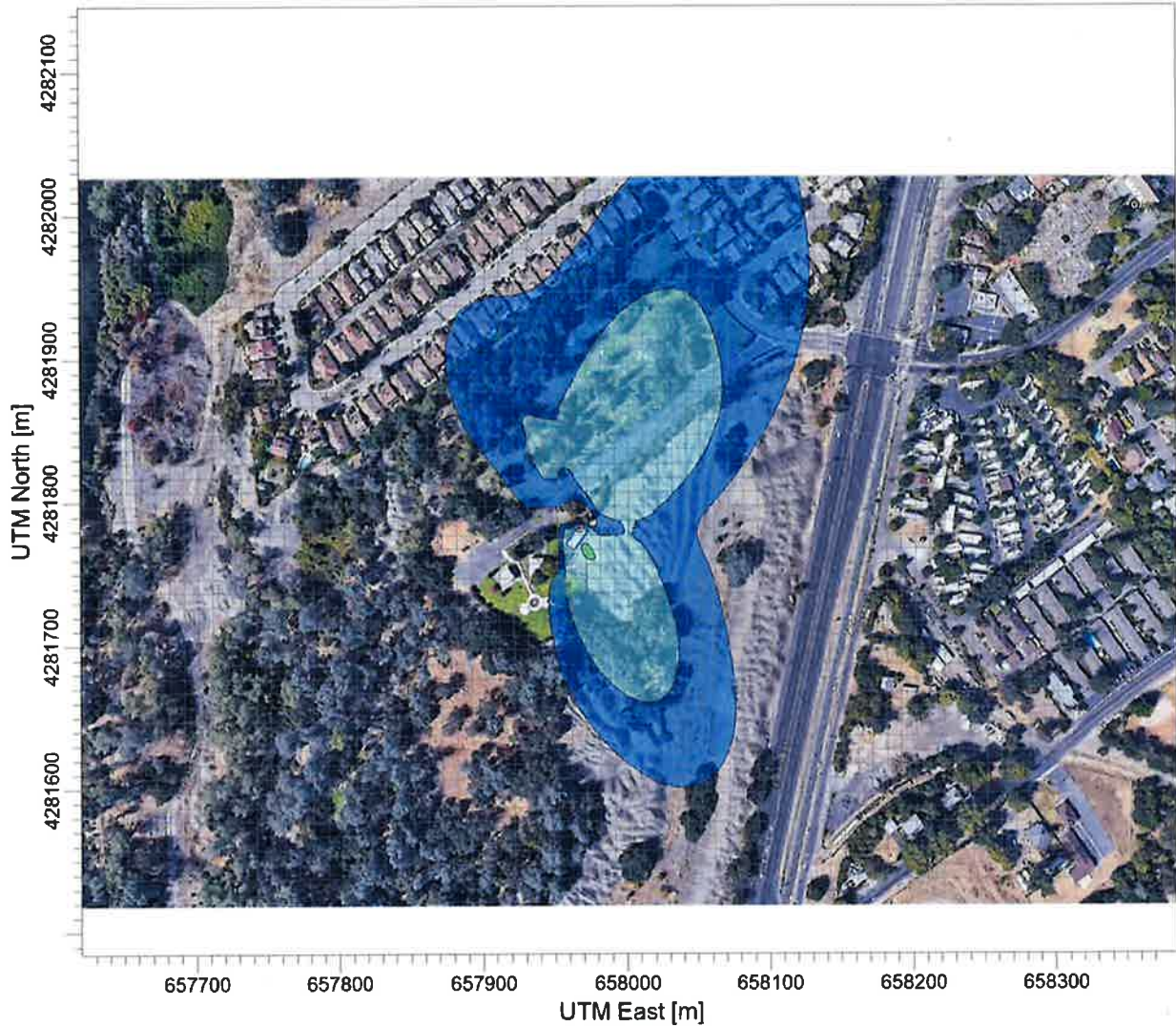
Worker Chronic Risk


*HARP - HRACalc v19044 11/20/2020 8:52:49 AM - Chronic Risk

REC	GRP	NETID	X	Y	SCENARIO	MAXHI
C1	ALL		658281.4	4281574	NonCancerChronicDerived_InhSoilDerm	8.37E-03
C2	ALL		658296.2	4281585	NonCancerChronicDerived_InhSoilDerm	7.57E-03
C2	ALL		658208.6	4281691	NonCancerChronicDerived_InhSoilDerm	1.05E-02
C4	ALL		658217.1	4281910	NonCancerChronicDerived_InhSoilDerm	1.62E-02

PROJECT TITLE:

Worker Maximum Non-Cancer Chronic Hazard Index



COMMENTS: Risk in maximum hazard index	SOURCES: 1	COMPANY NAME: HELIX Environmental Planning	
	RECEPTORS: 3541		
	OUTPUT TYPE: Hazard Index	SCALE: 1:4,808 0  0.1 km	
	MAX:	DATE: 11/21/2020	PROJECT NO.: COF-32

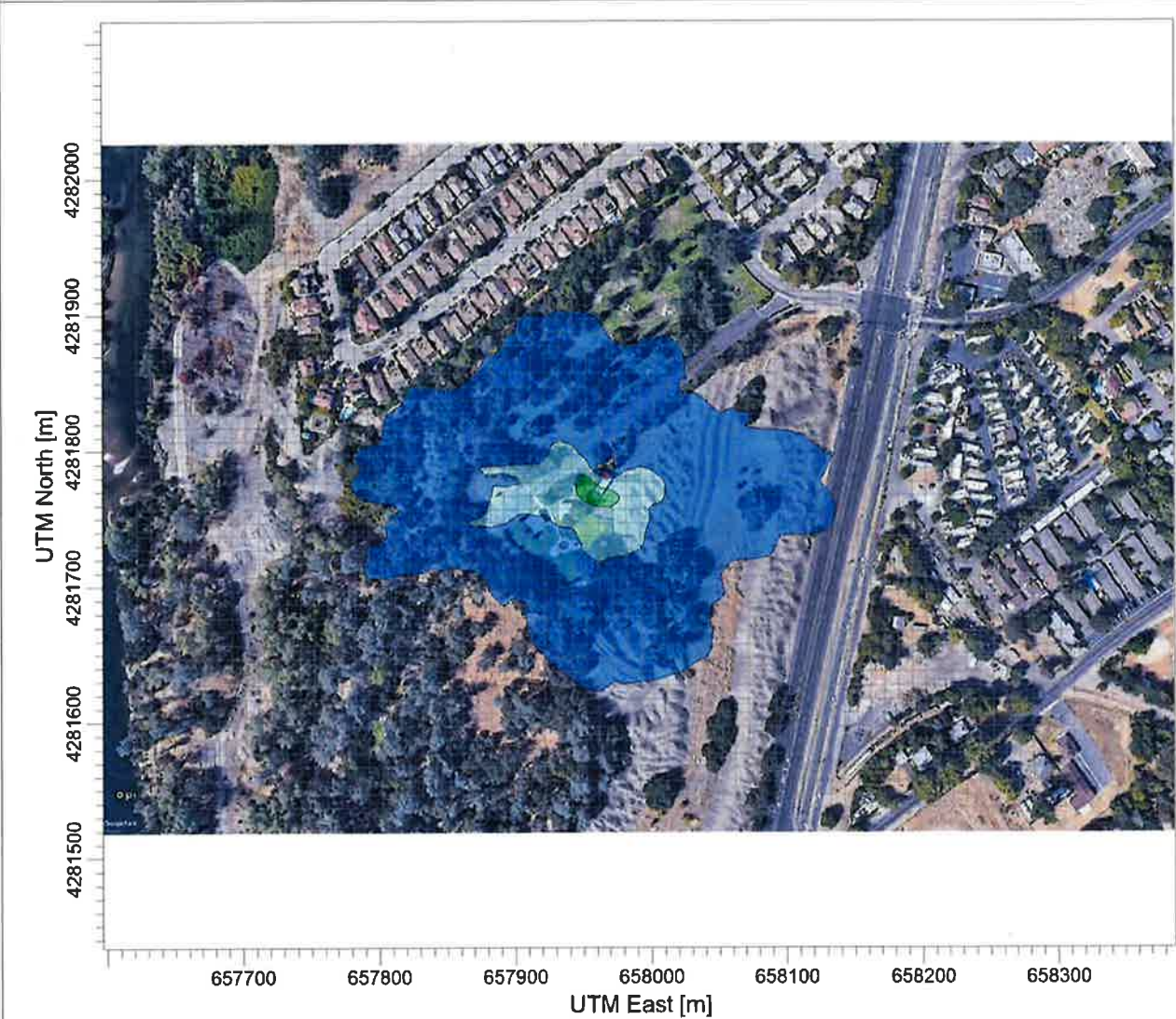
Acute Risk

*HARP - HRACalc v19044 11/20/2020 8:38:30 AM - Acute Risk

REC	GRP	NETID	X	Y	SCENARIO	MAXHI
R1	ALL		658172	4281577	NonCancerAcute	7.21E-02
R2	ALL		658204.3	4281599	NonCancerAcute	6.97E-02
R3	ALL		658177.6	4281681	NonCancerAcute	9.60E-02
R4	ALL		658221	4281731	NonCancerAcute	8.54E-02
R5	ALL		658216.2	4281738	NonCancerAcute	8.64E-02
R6	ALL		658211.2	4281758	NonCancerAcute	8.93E-02
R7	ALL		658184.6	4281790	NonCancerAcute	1.03E-01
R8	ALL		658186.7	4281798	NonCancerAcute	1.06E-01
R9	ALL		658189.5	4281816	NonCancerAcute	9.65E-02
R10	ALL		658194.3	4281838	NonCancerAcute	9.39E-02
R11	ALL		658196.8	4281851	NonCancerAcute	9.03E-02
R12	ALL		658103.2	4281928	NonCancerAcute	1.08E-01
R13	ALL		658071.8	4281960	NonCancerAcute	1.16E-01
R14	ALL		658060.4	4281973	NonCancerAcute	1.14E-01
R15	ALL		658051.7	4281986	NonCancerAcute	1.11E-01
R16	ALL		658043.1	4281998	NonCancerAcute	1.07E-01
R17	ALL		658012.3	4281990	NonCancerAcute	1.15E-01
R17	ALL		658000.9	4281983	NonCancerAcute	1.21E-01
R19	ALL		657988.3	4281975	NonCancerAcute	1.25E-01
R20	ALL		657977	4281966	NonCancerAcute	1.33E-01
R21	ALL		657966.5	4281958	NonCancerAcute	1.44E-01
R22	ALL		657954.9	4281949	NonCancerAcute	1.53E-01
R23	ALL		657944.2	4281940	NonCancerAcute	1.59E-01
R24	ALL		657933.3	4281932	NonCancerAcute	1.72E-01
R25	ALL		657921.4	4281923	NonCancerAcute	1.74E-01
R26	ALL		657910.8	4281914	NonCancerAcute	1.82E-01
R27	ALL		657900.6	4281906	NonCancerAcute	1.92E-01
R28	ALL		657888.2	4281897	NonCancerAcute	1.79E-01
R29	ALL		657877.8	4281889	NonCancerAcute	1.79E-01
R30	ALL		657866.5	4281880	NonCancerAcute	1.79E-01
R31	ALL		657855.3	4281872	NonCancerAcute	1.67E-01
R32	ALL		657844.1	4281863	NonCancerAcute	1.66E-01
R33	ALL		657832.5	4281854	NonCancerAcute	1.62E-01
R34	ALL		657820.3	4281845	NonCancerAcute	1.98E-01
R35	ALL		657808	4281834	NonCancerAcute	1.78E-01
R36	ALL		657791.5	4281834	NonCancerAcute	1.57E-01
R37	ALL		657764	4281814	NonCancerAcute	1.17E-01
C1	ALL		658281.4	4281574	NonCancerAcute	5.58E-02
C2	ALL		658296.2	4281585	NonCancerAcute	5.51E-02
C2	ALL		658208.6	4281691	NonCancerAcute	8.41E-02
C4	ALL		658217.1	4281910	NonCancerAcute	7.37E-02

PROJECT TITLE:

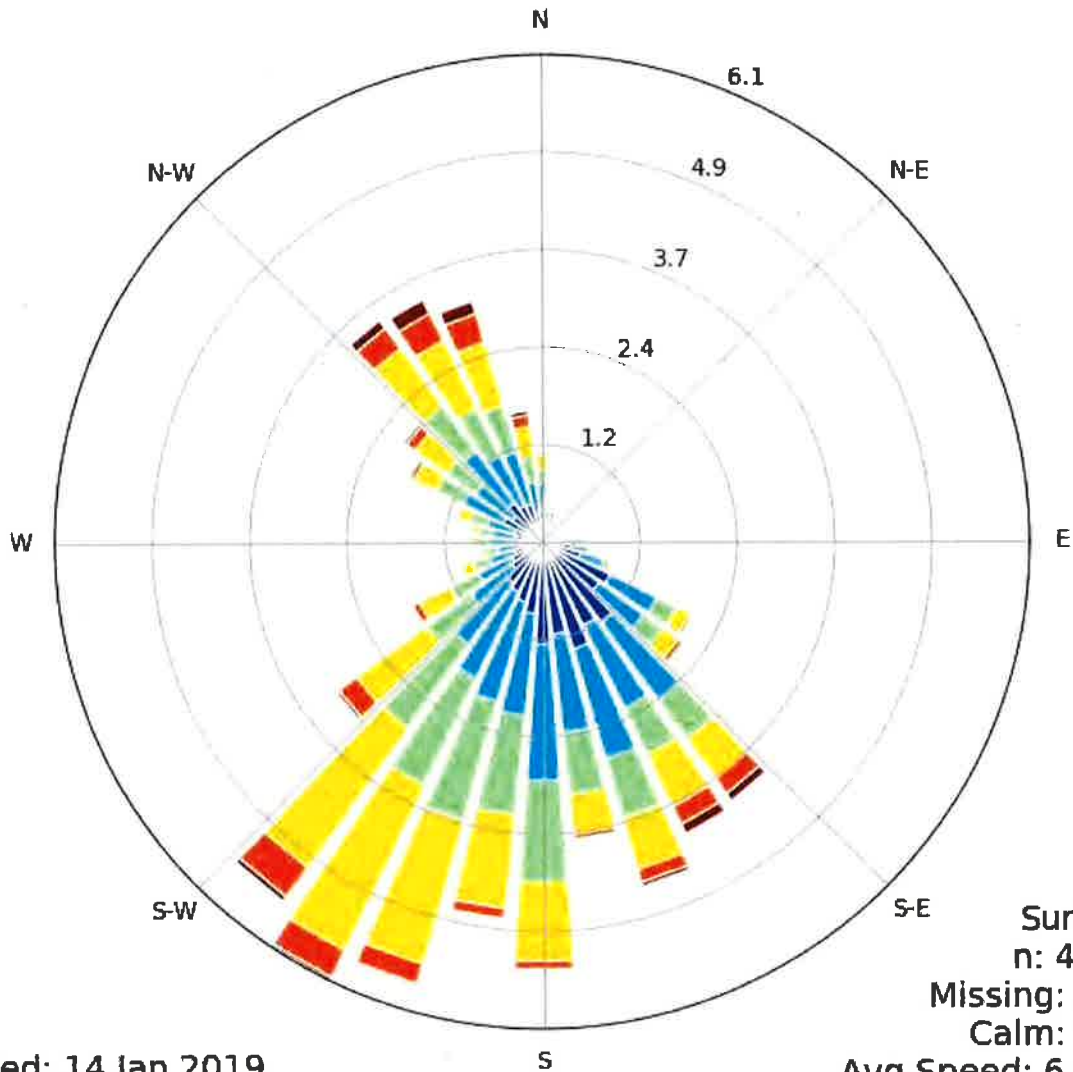
Non-Cancer Acute Maximum Hazard Index



COMMENTS: Risk in maximum hazard index	SOURCES: 1	COMPANY NAME: HELIX Environmental Planning	
	RECEPTORS: 3541		
	OUTPUT TYPE: Hazard Index	SCALE: 1:4,963 	
	MAX:	DATE: 11/21/2020	PROJECT NO.: COF-32



[SAC] SACRAMENTO/EXECUTIV
Windrose Plot [All Year]
Period of Record: 01 Jan 1970 - 14 Jan 2019



Generated: 14 Jan 2019



Control Pathway

AERMOD

Dispersion Options

Titles C:\Users\mdrol\Desktop\COF-32 HRA\COF-32 Lakeside Crematorium Lakes\	
Dispersion Options <input checked="" type="checkbox"/> Regulatory Default <input type="checkbox"/> Non-Default Options	Dispersion Coefficient Rural
	Output Type <input checked="" type="checkbox"/> Concentration <input type="checkbox"/> Total Deposition (Dry & Wet) <input type="checkbox"/> Dry Deposition <input type="checkbox"/> Wet Deposition
	Plume Depletion <input type="checkbox"/> Dry Removal <input type="checkbox"/> Wet Removal
	Output Warnings <input type="checkbox"/> No Output Warnings <input type="checkbox"/> Non-fatal Warnings for Non-sequential Met Data

Pollutant / Averaging Time / Terrain Options

Pollutant Type OTHER - MULTIPLE	Exponential Decay Option not available
Averaging Time Options Hours <input checked="" type="checkbox"/> 1 <input type="checkbox"/> 2 <input type="checkbox"/> 3 <input type="checkbox"/> 4 <input type="checkbox"/> 6 <input type="checkbox"/> 8 <input type="checkbox"/> 12 <input type="checkbox"/> 24 <input type="checkbox"/> Month <input checked="" type="checkbox"/> Period <input type="checkbox"/> Annual	Terrain Height Options <input type="checkbox"/> Flat <input checked="" type="checkbox"/> Elevated SO: Meters RE: Meters TG: Meters
Flagpole Receptors <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Default Height = 1.20 m	

Control Pathway

AERMOD

Optional Files



Re-Start File



Init File



Multi-Year Analyses



Event Input File



Error Listing File

Detailed Error Listing File

Filename: COF-32 Lakeside Crematorium Lakes.err

Source Pathway - Source Inputs

AERMOD

Point Sources

Source Type	Source ID	X Coordinate [m]	Y Coordinate [m]	Base Elevation (Optional)	Release Height [m]	Emission Rate [g/s]	Gas Exit Temp. [K]	Gas Exit Velocity [m/s]	Stack Inside Diameter [m]
POINT	STACK1	657967.00 Stack	4281782.00	50.33	5.97	1.00000	855.37	4.47	0.51

Source Pathway

AERMOD

Building Downwash Information

Source ID: <u>STACK1</u>							
Heights [m] (10 to 360 deg)							
10-60 deg	3.44	3.44	3.44	3.44	3.44	3.44	3.44
70-120 deg	3.44	3.44	3.44	3.44	3.44	3.44	3.44
130-180 deg	3.44	3.44	3.44	3.44	3.44	3.44	3.44
190-240 deg	3.44	3.44	3.44	3.44	3.44	3.44	3.44
250-300 deg	3.44	3.44	3.44	3.44	3.44	3.44	3.44
310-360 deg	3.44	3.44	3.44	3.44	3.44	3.44	3.44
Widths [m] (10 to 360 deg)							
10-60 deg	12.51	10.24	7.65	8.26	10.81	13.03	
70-120 deg	14.85	16.22	17.10	17.46	17.29	16.59	
130-180 deg	16.66	17.25	17.31	16.84	15.87	14.41	
190-240 deg	12.51	10.24	7.65	8.26	10.81	13.03	
250-300 deg	14.85	16.22	17.10	17.46	17.29	16.59	
310-360 deg	16.66	17.25	17.31	16.84	15.87	14.41	
Lengths [m] (10 to 360 deg)							
10-60 deg	17.46	17.29	16.59	16.66	17.25	17.31	
70-120 deg	16.84	15.87	14.41	12.51	10.24	7.65	
130-180 deg	8.26	10.81	13.03	14.85	16.22	17.10	
190-240 deg	17.46	17.29	16.59	16.66	17.25	17.31	
250-300 deg	16.84	15.87	14.41	12.51	10.24	7.65	
310-360 deg	8.26	10.81	13.03	14.85	16.22	17.10	
Along Flow [m] (10 to 360 deg)							
10-60 deg	-15.63	-15.49	-14.88	-14.41	-14.07	-13.30	
70-120 deg	-12.12	-10.58	-8.72	-6.59	-4.26	-1.81	
130-180 deg	-0.93	-1.19	-1.41	-1.59	-1.72	-1.80	
190-240 deg	-1.82	-1.79	-1.71	-2.26	-3.18	-4.01	
250-300 deg	-4.72	-5.29	-5.69	-5.92	-5.97	-5.84	
310-360 deg	-7.34	-9.63	-11.62	-13.26	-14.50	-15.30	
Across Flow [m] (10 to 360 deg)							
10-60 deg	0.34	-0.85	-2.02	-3.21	-4.22	-5.11	
70-120 deg	-5.84	-6.39	-6.75	-6.90	-6.85	-6.59	
130-180 deg	-6.08	-5.44	-4.64	-3.70	-2.65	-1.51	
190-240 deg	-0.34	0.85	2.02	3.21	4.22	5.11	
250-300 deg	5.84	6.39	6.75	6.90	6.85	6.59	
310-360 deg	6.08	5.44	4.64	3.70	2.65	1.51	

Source Pathway

AERMOD

Emission Rate Units for Output

For Concentration

Unit Factor:	1E6
Emission Unit Label:	GRAMS/SEC
Concentration Unit Label:	MICROGRAMS/M**3

Receptor Pathway

AERMOD

Receptor Networks

Note: Terrain Elevations and Flagpole Heights for Network Grids are in Page RE2 - 1 (If applicable)
Generated Discrete Receptors for Multi-Tier (Risk) Grid and Receptor Locations for Fenceline Grid are in Page RE3 - 1 (If applicable)

Uniform Cartesian Grid

Receptor Network ID	Grid Origin X Coordinate [m]	Grid Origin Y Coordinate [m]	No. of X-Axis Receptors	No. of Y-Axis Receptors	Spacing for X-Axis [m]	Spacing for Y-Axis [m]
UCART1	657622.00	4281537.00	70	50	10.00	10.00

Discrete Receptors

Discrete Cartesian Receptors

Record Number	X-Coordinate [m]	Y-Coordinate [m]	Group Name (Optional)	Terrain Elevations	Flagpole Heights [m] (Optional)
1	658171.99	4281576.80		56.25	
2	658204.25	4281599.32		56.72	
3	658177.63	4281680.58		56.34	
4	658220.99	4281730.59		57.89	
5	658216.22	4281737.74		58.30	
6	658211.15	4281758.13		58.69	
7	658184.55	4281789.89		58.01	
8	658186.65	4281798.48		58.07	
9	658189.46	4281816.10		58.09	
10	658194.31	4281838.45		58.23	
11	658196.77	4281850.98		58.27	
12	658103.22	4281927.78		55.35	
13	658071.77	4281959.88		54.86	
14	658060.39	4281973.04		54.92	
15	658051.68	4281985.75		55.07	
16	658043.07	4281998.03		55.31	
17	658012.30	4281989.53		53.82	
18	658000.94	4281982.74		53.45	
19	657988.34	4281974.95		53.12	
20	657977.03	4281966.32		52.45	
21	657966.52	4281958.41		51.82	
22	657954.91	4281949.27		51.24	
23	657944.16	4281940.37		50.78	
24	657933.29	4281931.72		50.35	
25	657921.43	4281922.83		49.89	
26	657910.81	4281913.81		49.47	

Receptor Pathway

AERMOD

27	657900.56	4281905.53	49.05
28	657888.20	4281897.13	48.45
29	657877.82	4281889.10	47.97
30	657866.46	4281879.84	47.51
31	657855.34	4281872.06	47.00
32	657844.10	4281863.28	46.48
33	657832.49	4281853.90	46.03
34	657820.26	4281845.37	45.60
35	657807.97	4281834.37	45.10
36	657791.51	4281834.17	44.60
37	657763.95	4281813.59	43.54
38	658281.43	4281574.43	57.67
39	658296.17	4281585.05	58.20
40	658208.56	4281690.93	56.99
41	658217.08	4281910.37	58.57

Plant Boundary Receptors

Meteorology Pathway

AERMOD

Met Input Data

Surface Met Data

Filename: ..\Exec 10-14 N1MD.SFC
 Format Type: Default AERMET format

Profile Met Data

Filename: ..\Exec 10-14 N1MD.PFL
 Format Type: Default AERMET format

Wind Speed

Wind Speeds are Vector Mean (Not Scalar Means)

Wind Direction

Rotation Adjustment [deg]:

Potential Temperature Profile

Base Elevation above MSL (for Primary Met Tower): 10.00 [m]

Meteorological Station Data

Stations	Station No.	Year	X Coordinate [m]	Y Coordinate [m]	Station Name
Surface		2010			SACRAMENTO/EXECUTIVE ARPT
Upper Air		2010			OAKLAND/WSO AP

Data Period

Data Period to Process

Start Date: 1/1/2010 Start Hour: 1 End Date: 12/31/2014 End Hour: 24

Wind Speed Categories

Stability Category	Wind Speed [m/s]	Stability Category	Wind Speed [m/s]
A	1.54	D	8.23
B	3.09	E	10.8
C	5.14	F	No Upper Bound

Output Pathway

AERMOD

Tabular Printed Outputs

Short Term Averaging Period	RECTABLE Highest Values Table										MAXTABLE Maximum Values Table	DAYTABLE Daily Values Table
	1st	2nd	3rd	4th	5th	6th	7th	8th	9th	10th		
1	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		No

Contour Plot Files (PLOTFILE)

Path for PLOTFILES: COF-32 Lakeside Crematorium Lakes.AD

Averaging Period	Source Group ID	High Value	File Name
1	ALL	1st	01H1GALL.PLT
Period	ALL	N/A	PE00GALL.PLT

HARP Project Summary Report 11/21/2020 10:12:35 AM

PROJECT INFORMATION

HARP Version: 19121

Project Name: COF-32 LAKESIDE CREMATORIUM HARP

HARP Database: NA

EMISSION INVENTORY

No. of Pollutants:39

No. of Background Pollutants:0

Emissions

ScrID	StkID	ProID	PolID	PolAbbrev	Multi	Annual Ems (lbs/yr)	MaxHr Ems (lbs/hr)	MWAF
STACK1		0	0	75070 Acetaldehyde	1	0.0364	7.27E-05	1
STACK1		0	0	71432 Benzene	1	0.0177	3.54E-05	1
STACK1		0	0	7440417 Beryllium	1	0.00114	2.28E-06	1
STACK1		0	0	7440439 Cadmium	1	0.00859	1.72E-05	1
STACK1		0	0	18540299 Cr(VI)	1	0.00957	1.91E-05	1
STACK1		0	0	7440508 Copper	1	0.0217	4.34E-05	1
STACK1		0	0	50000 Formaldehyde	1	0.0095	1.90E-05	1
STACK1		0	0	7664393 HF	1	0.401	0.000802	1
STACK1		0	0	7439921 Lead	1	0.0517	0.000103	1
STACK1		0	0	7439976 Mercury	1	2.77	0.00416	1
STACK1		0	0	7440020 Nickel	1	0.0299	5.98E-05	1
STACK1		0	0	7782492 Selenium	1	0.0172	3.44E-05	1
STACK1		0	0	108883 Toluene	1	0.573	0.00115	1
STACK1		0	0	75014 Vinyl Chloride	1	0.00185	3.70E-06	1
STACK1		0	0	1330207 Xylenes	1	0.00963	1.93E-05	1
STACK1		0	0	7440666 Zinc	1	0.276	0.000551	1
STACK1		0	0	56553 B[a]anthracene	1	6.67E-06	1.33E-08	1
STACK1		0	0	50328 B[a]P	1	2.45E-05	4.90E-08	1
STACK1		0	0	205992 B[b]fluoranthen	1	5.61E-06	1.12E-08	1
STACK1		0	0	207089 B[k]fluoranthen	1	5.06E-06	1.01E-08	1
STACK1		0	0	218019 Chrysene	1	3.49E-05	6.98E-08	1
STACK1		0	0	53703 D[a,h]anthracen	1	4.52E-06	9.04E-09	1
STACK1		0	0	193395 In[1,2,3-cd]pyr	1	5.39E-06	1.08E-08	1
STACK1		0	0	1746016 2,3,7,8-TCDD	1	5.11E-08	1.02E-10	1
STACK1		0	0	40321764 1-3,7,8PeCDD	1	1.49E-07	2.98E-10	1
STACK1		0	0	39227286 1-4,7,8HxCDD	1	1.77E-07	3.54E-10	1
STACK1		0	0	57653857 1-3,6-8HxCDD	1	2.55E-07	5.10E-10	1
STACK1		0	0	19408743 1-3,7-9HxCDD	1	3.16E-07	6.32E-10	1
STACK1		0	0	35822469 1-4,6-8HpCDD	1	2.42E-06	4.84E-09	1
STACK1		0	0	51207319 2,3,7,8-TCDF	1	3.43E-07	6.86E-10	1
STACK1		0	0	57117416 1-3,7,8PeCDF	1	1.91E-07	3.81E-10	1
STACK1		0	0	57117314 2-4,7,8PeCDF	1	5.82E-07	1.16E-09	1
STACK1		0	0	70648269 1-4,7,8HxCDF	1	6.18E-07	1.24E-09	1
STACK1		0	0	57117449 1-3,6-8HxCDF	1	5.49E-07	1.10E-09	1
STACK1		0	0	72918219 1-3,7-9HxCDF	1	1.07E-06	2.15E-09	1
STACK1		0	0	60851345 2-4,6-8HxCDF	1	2.23E-07	4.45E-10	1
STACK1		0	0	67562394 1-4,6-8HpCDF	1	2.94E-06	5.89E-09	1
STACK1		0	0	55673897 1-4,7-9HpCDF	1	1.79E-07	3.58E-10	1
STACK1		0	0	7440382 Arsenic	1	0.0252	5.04E-05	1

POLLUTANT HEALTH INFORMATION

Health Database: C:\HARP2\Tables\HEALTH17320.mdb

Health Table Version: HEALTH19252

Official: True

PolID	PolAbbrev	InhCancer	OralCancer	AcuteREL	InhChronicREL	OralChronicREL	InhChronic8HRREL
75070	Acetaldehyde	0.01			470	140	300
71432	Benzene	0.1			27	3	3
7440417	Beryllium	8.4				0.007	0.002
7440439	Cadmium	15				0.02	0.0005
18540299	Cr(VI)	510	0.5			0.2	0.02
7440508	Copper				100		
50000	Formaldehyde	0.021			55	9	9
7664393	HF				240	14	0.04
7439921	Lead	0.042	0.0085				
7439976	Mercury				0.6	0.03	0.00016
7440020	Nickel	0.91			0.2	0.014	0.011
7782492	Selenium					20	0.005
108883	Toluene				37000	300	
75014	Vinyl Chloride	0.27			180000		
1330207	Xylenes				22000	700	
7440666	Zinc						
56553	B[a]anthracene	0.39	1.2				
50328	B[a]P	3.9	12				
205992	B[b]fluoranthen	0.39	1.2				
207089	B[k]fluoranthen	0.39	1.2				
218019	Chrysene	0.039	0.12				
53703	D[a,h]anthracen	4.1	4.1				
193395	In[1,2,3-cd]pyr	0.39	1.2				
1746016	2,3,7,8-TCDD	130000	130000		4.00E-05	1.00E-08	
40321764	1-3,7,8PeCDD	130000	130000		4.00E-05	1.00E-08	
39227286	1-4,7,8HxCDD	13000	13000		0.0004	1.00E-07	
57653857	1-3,6-8HxCDD	13000	13000		0.0004	1.00E-07	
19408743	1-3,7-9HxCDD	13000	13000		0.0004	1.00E-07	
35822469	1-4,6-8HpCDD	1300	1300		0.004	1.00E-06	
51207319	2,3,7,8-TCDF	13000	13000		0.0004	1.00E-07	
57117416	1-3,7,8PeCDF	3900	3900		0.0013	3.30E-07	
57117314	2-4,7,8PeCDF	39000	39000		0.00013	3.30E-08	
70648269	1-4,7,8HxCDF	13000	13000		0.0004	1.00E-07	
57117449	1-3,6-8HxCDF	13000	13000		0.0004	1.00E-07	
72918219	1-3,7-9HxCDF	13000	13000		0.0004	1.00E-07	
60851345	2-4,6-8HxCDF	13000	13000		0.0004	1.00E-07	
67562394	1-4,6-8HpCDF	1300	1300		0.004	1.00E-06	
55673897	1-4,7-9HpCDF	1300	1300		0.004	1.00E-06	
7440382	Arsenic	12	1.5		0.2	0.015	3.50E-06
							0.015

LIST OF AIR DISPERSION FILES

AERMOD Input File:

AERMOD Output File:

AERMOD Error File:

Plotfile list

LIST OF RISK ASSESSMENT FILES

Health risk analysis files (\hra\)

AcuteGLCList.csv
AcuteHRAInput.hra
AcuteNCAcuteRisk.csv
AcuteNCAcuteRiskSumByRec.csv
AcuteOutput.txt
AcutePathwayRec.csv
AcutePoIDB.csv
ResCancerCancerRisk.csv
ResCancerCancerRiskSumByRec.csv
ResCancerGLCList.csv
ResCancerHRAInput.hra
ResCancerOutput.txt
ResCancerPathwayRec.csv
ResCancerPoIDB.csv
ResChronicGLCList.csv
ResChronicHRAInput.hra
ResChronicNChronicRisk.csv
ResChronicNChronicRiskSumByRec.csv
ResChronicOutput.txt
ResChronicPathwayRec.csv
ResChronicPoIDB.csv
WorkCancerCancerRisk.csv
WorkCancerCancerRiskSumByRec.csv
WorkCancerGLCList.csv
WorkCancerHRAInput.hra
WorkCancerOutput.txt
WorkCancerPathwayRec.csv
WorkCancerPoIDB.csv
WorkChronicGLCList.csv
WorkChronicHRAInput.hra
WorkChronicNChronicRisk.csv
WorkChronicNChronicRiskSumByRec.csv
WorkChronicOutput.txt
WorkChronicPathwayRec.csv
WorkChronicPoIDB.csv

Attachment C

Addendum to the Folsom Lakeside Crematorium Project Air Quality and Greenhouse Gas Emissions Assessment

HELIX Environmental Planning, Inc.
 11 Natoma Street, Suite 150
 Folsom, CA 95630
 916.365.8700
www.helixepi.com



November 5, 2021

Project 02576.00032.001

Mr. Scott Johnson, AICP
 Planning Manager
 City of Folsom, Community Development Department
 50 Natoma Street
 Folsom, CA 95630

Subject: Addendum to the Folsom Lakeside Crematorium Project Air Quality and Greenhouse Gas Emissions Assessment

Dear Mr. Johnson:

HELIX Environmental Planning, Inc. (HELIX) completed the *Folsom Lakeside Crematorium Project Air Quality and Greenhouse Gas Emissions Assessment* in July 2021 (HELIX 2021). Following the completion of the July 2021 analysis, Caring Services Group (Applicant) has requested alterations to the operating hours and number of daily cremations to occur on site. This Addendum provides an updated analysis based on these alterations.

PREVIOUS ANALYSIS

The July 2021 letter report assessed the air quality and greenhouse gas (GHG) emissions associated with the construction and operation of the proposed Folsom Lakeside Crematorium Project (project), including a health risk assessment (HRA) to evaluate potential community health risks from the project's emissions. The analysis was prepared to support environmental review under the California Environmental Quality Act (CEQA).

The analysis assumed a maximum cremation process rate of 200 pounds per hour, 400 pounds per day, and 100,000 pounds per year based on information provided by Applicant. The project's emissions of criteria pollutants and precursors were found to be below Sacramento Metropolitan Air Quality Management District (SMAQMD) thresholds and result in a less than significant impact. Community health risks resulting from emissions of toxic air contaminants (TACs) from the project's operation of a crematory were evaluated in an HRA following the Office of Environmental Health Hazard Assessment (OEHHA) guidelines. Project TAC emissions were found to result in less than significant impacts. The project was not found to be a substantial source of objectional odors and odor impacts were disclosed as less than significant. The project was found to be consistent with the City's integrated General Plan and GHG Strategy and GHG emissions impacts were disclosed as being less than significant. The project was found to not conflict with an applicable plan adopted for the purposes of reducing GHG emissions and the impact was disclosed as less than significant.

Letter to Mr. Scott Johnson, AICP
November 5, 2021

Page 2 of 4

REVISED ANALYSIS

As discussed previously, the Applicant has requested alterations to the operating hours and number of daily cremations to occur on the site. The Applicant has requested an increase in the daily process rate from the previously analyzed 400 pounds per day to a new value of 800 pounds per day. There are no changes to the project that would affect the construction analysis previously conducted. The analysis that follows focuses on daily operational emissions.

The HRA previously conducted to evaluate potential community health risks from the project's TAC emissions relies on the maximum hourly emissions rate and the average annual emissions generated by project operations. The hourly cremation process rate was previously set based on the maximum hourly capacity of the crematory; therefore, there is no change to the hourly process rate or maximum hourly emissions. The Applicant has not requested alteration to the total number of cremations to occur per year; therefore, there is no change to the annual cremation process rate or average annual emissions profile. Therefore, the potential health risks from the project would remain the same as previously disclosed.

Methods

Criteria pollutant and precursor emissions for long-term operation of the proposed crematory were calculated using propane combustion emissions factors from the USEPA AP-42 Compilation of Emissions Factors Chapter 1.5 (USEPA 2008), and crematory emissions factors provided by the SMAQMD, which combined USEPA AP-42 data and the USEPA Factor Information Retrieval Program (SMAQMD 2020a).

Air Quality Impact Analysis

Operation of a propane-fired crematory would be considered a new stationary source of emissions. The project may be subject to SMAQMD's Rule 201, *General Permit Requirements*, and Rule 202, *New Source Review*. The project would be required to implement best available control technology (BACT) for the minimization of emissions. BACT for crematories is incorporated into the product design in the form of controls which ensure maintenance of the correct temperatures and cycle times, and a secondary combustion chamber which ensures oxygenation and complete combustions of all fuels. As described in the Methods sections, above, criteria pollutant and precursor emissions for long-term operation of the proposed crematory were calculated using propane combustion emissions factors from AP-42 and crematory emission factors provided by SMAQMD. The project's calculated criteria and precursors operational emissions are compared to the SMAQMD thresholds in Table 1, *Operational Criteria Pollutant and Precursor Emissions*, and the calculation output sheets are included in Attachment A to this letter.

Letter to Mr. Scott Johnson, AICP
November 5, 2021

Page 3 of 4

Table 1
OPERATIONAL CRITERIA POLLUTANT AND PRECURSOR EMISSIONS

Pollutant	Project Emissions	SMAQMD Threshold	Exceed Threshold?
<i>Daily Emissions (pounds per day)</i>			
ROG	0.2	65	No
NO _x	2.4	65	No
CO	1.8	None	No
SO _x	0.9	None	No
PM ₁₀	0.5	80	No
PM _{2.5}	0.5	82	No

Source: SMAQMD 2020a; SMAQMD 2020b

As shown in Table 1, the project's operational emissions of criteria pollutants and precursors would not exceed the SMAQMD daily thresholds. Therefore, the project's operational emissions would not result in a cumulatively considerable net increase of any criteria pollutant and impacts would be less than significant.

SUMMARY

The project's daily emissions of criteria pollutants and precursors would remain below SMAQMD thresholds and would result in a less than significant impact. All other quantified emissions and significance determinations remain unchanged from what was presented in the July 2021 *Folsom Lakeside Crematorium Project Air Quality and Greenhouse Gas Emissions Assessment*.

Sincerely,



Victor Ortiz
Senior Air Quality Specialist

Attachments:

Attachment A: Emissions Calculation Sheets

Letter to Mr. Scott Johnson, AICP
November 5, 2021

Page 4 of 4

REFERENCES

Sacramento Metropolitan Air Quality Management District (SMAQMD). 2020a. Electronic communications between HELIX Environmental Planning (Victor Ortiz) and the Sacramento Metropolitan Air Quality Management District (Venk Reddy); attachments to communications containing crematory emissions calculations. August and November.

2020b. SMAQMD Thresholds of Significance Table. April. Available at:
<http://www.airquality.org/LandUseTransportation/Documents/CH2ThresholdsTable4-2020.pdf>.

U.S. Environmental Protection Agency (USEPA). 2008. AP 42, Fifth Edition, Volume I Chapter 1: External Combustion Sources, 1.5 Liquefied Petroleum Gas Combustion. Available at:
[https://www.epa.gov/sites/production/files/2020-09/documents/1.5 liquefied petroleum gas combustion.pdf](https://www.epa.gov/sites/production/files/2020-09/documents/1.5%20liquefied%20petroleum%20gas%20combustion.pdf).

Crematory Criteria Pollutant Emissions

Emissions from Propane Combustion

KBTU/Cycle	1,800
KBTU/Gallon	91.502
Gallons/Cycle	19.672
Cycles/Day	4
Cycles/Year	500

Pollutant	Emission Factor (lbs/1000 gal) ¹	Emissions (lbs/day)
ROG ³	1	0.08
NOx	13	1.02
SOx	0.054	0.00
PM10	0.7	0.06
PM2.5 ⁴	0.7	0.06
CO	7.5	0.59

Emissions from Combustion of Human Remains

	lbs/day
Maximum Throughput	800

Pollutant	Emission Factor (lbs/ton) ²	Emissions (lbs/day)
ROG	0.299	0.12
NOx	3.560	1.42
SOx	2.170	0.87
PM10	1.130	0.45
PM2.5 ⁴	1.130	0.45
CO	2.950	1.18

Total Emissions

Pollutant	Emissions (lbs/day)
ROG	0.2
NOx	2.4
SOx	0.9
PM10	0.5
PM2.5	0.5
CO	1.8

Notes:

1. Emissions factors for propane from USEPA AP-42 Chapter 1, External Combustion Sources, Section 1.5 Liquefied Petroleum Gas Combustion, Table 1.5-1.
2. Emissions from combustion of human remains provided by SMAQMD and are from USEPA Factor Information REtrieval (FIRE) Program Data System (3/08).
3. ROG fraction of TOC for propane combustion unavailable, ROG assumed to be equal to

Appendix C

Tribal Consultation Record



January 8, 2021

Robert Edgerton, AICP CEP
 HELIX Environmental Planning, Inc.
 11 Natoma Street, Suite 155
 Folsom, California 95630

RE: Tribal Consultation Record for Compliance with Assembly Bill 52 and CEQA for the Lakeside Memorial Lawn Storage Shed Project, City of Folsom

Dear Mr. Edgerton:

The California Environmental Quality Act (CEQA), as amended in 2014 by Assembly Bill 52 (AB 52), requires that the City of Folsom provide notice to any California Native American tribes that have requested notice of projects subject to CEQA review, and consult with tribes that responded to the notice within 30 days of receipt with a request for consultation. Section 21073 of the Public Resources Code (PRC) defines California Native American tribes as "a Native American tribe located in California that is on the contact list maintained by the NAHC for the purposes of Chapter 905 of the Statutes of 2004." This includes both federally and non-federally recognized tribes. For the City, these include the following tribes that previously submitted general request letters, requesting such noticing:

- Wilton Rancheria (letter dated January 13, 2020);
- Lone Band of Miwok Indians (letter dated March 2, 2016); and
- United Auburn Indian Community (UAIC) of the Auburn Rancheria (letter dated November 23, 2015).

The purpose of consultation is to identify Tribal Cultural Resources (TCRs) that may be significantly impacted by the proposed Project, and to allow the City to avoid or mitigate significant impacts prior to Project approval and implementation. Section 21074(a) of the PRC defines TCRs for the purpose of CEQA as:

Sites, features, places, cultural landscapes (geographically defined in terms of the size and scope), sacred places, and objects with cultural value to a California Native American tribe that are either of the following:

- a) included or determined to be eligible for inclusion in the California Register of Historical Resources; and/or
- b) included in a local register of historical resources as defined in subdivision (k) of Section 5020.1; and/or

- c) a resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Section 5024.1. In applying the criteria set forth in subdivision (c) of Section 5024.1, for the purposes of this paragraph the lead agency shall consider the significance of the resource to a California Native American tribe.

Because the first two criteria also meet the definition of a Historical Resource under CEQA, a TCR may also require additional consideration as a Historical Resource. TCRs may or may not exhibit archaeological, cultural, or physical indicators and can only be identified by a culturally affiliated tribe, which has been determined under State law to be the subject matter expert for TCRs.

CEQA requires that the City initiate consultation with tribes at the commencement of the CEQA process to identify TCRs. Furthermore, because a significant effect on a TCR is considered a significant impact on the environment under CEQA, consultation is required to develop appropriate avoidance, impact minimization, and mitigation measures. Therefore, in accordance with the requirements summarized above, the City carried out, or attempted to carry out, tribal consultation for the Project. The methods and results of tribal consultation are summarized below, and a copy of the complete non-confidential administrative record is provided in Attachment A.

1.0 SUMMARY OF CONSULTATION

Within 14 days of initiating CEQA review for the Project, on November 25, 2020, the City sent Project notification letters to the three California Native American tribes named above, which had previously submitted general consultation request letters pursuant to Section 21080.3.1(d) of the PRC. Each tribe was provided a brief description of the Project and its location, the contact information for the City's authorized representative, and a notification that the tribe has 30 days to request consultation.

1.1 Lone Band of Miwok Indians

The lone Band of Miwok Indians did not respond to the City's notification letter, and therefore, the threshold for carrying out tribal consultation with that tribe under PRC 21080.3.1(e) was not met.

1.2 UAIC

On December 11, 2020, and within the 30-day response timeframe, the City received an automated email from the United from UAIC that acknowledged receipt of the City's notification letter, thanked the City for consulting with UAIC, and attached the tribe's consultation record for the project. The response did not include any information on TCRs and indicated that the Tribal Historic Preservation Department would review the Project and respond; however, no further communication was received from UAIC. Because the tribe failed to provide comments or engage with the City pursuant to PRC 21082.3(d)(2), the City considers this consultation requirement complete.

1.3 Wilton Rancheria

On December 1, 2020, and within the 30-day response timeframe, Wilton Rancheria representative Mariah Mayberry responded to the City's initial notification letter by email requesting to formally initiate

consultation under AB 52 on the Project. In her response, Ms. Mayberry stated that the tribe would like to discuss the type of environmental review that is being conducted for the Project, Project alternatives, any significant effects, and mitigation measures for any direct, indirect, or cumulative impacts that the Project may cause to tribal cultural resources. Ms. Mayberry also requested a copy of the cultural resource assessment and results of the record search. Although she did not indicate or suggest that there are TCRs within the Project Area, Ms. Mayberry provided Wilton Rancheria's recommended mitigation measures for TCRs.

On December 14, 2020, the City formally initiated consultation with Wilton Rancheria by inviting Ms. Mayberry to a virtual meeting on December 17, 2020. In the City's initiation letter to the tribe, Associate Planner Josh Kinkade further clarified that the purpose of the Project is to remodel an existing shed, and that there will be no mass grading or excavation associated with the Project. Additionally, Mr. Kinkade provided a link to the Cultural Resources Inventory Report, prepared by ECORP Consulting, Inc. (2020) and a copy of the meeting agenda. Mr. Kinkade requested that if Ms. Mayberry is unable to attend that she contact him to reschedule the meeting to another time that is mutually agreeable.

On December 15, 2020, Ms. Mayberry contacted the City to indicate that she is unavailable to meet at the scheduled time. The City offered another meeting on January 5, 2021, at 8:30 a.m. On January 4, 2021, Ms. Mayberry contacted the City and asked that the meeting be scheduled for 10:00 a.m. instead, and the City accepted. After Ms. Mayberry or any other tribal representatives failed to report to the meeting at the scheduled time, the meeting was terminated by the City after 20 minutes. At approximately noon on January 5, Ms. Mayberry emailed the City to request availability for January 6, and the City offered 3:30 p.m. for another meeting. Ms. Mayberry did not attend the meeting and did not respond to the City to reschedule again. Because the tribe failed to engage meaningfully with the City after a reasonable and good-faith effort composed of multiple attempts to meet with the tribe, pursuant to PRC 21082.3(d)(2), the City considers this consultation requirement complete. Should Wilton Rancheria, or any other culturally affiliated tribe, submit public comments, the City will consider them in accordance with Section 11(b) of AB 52; however, after completing the required notification and consultation procedures specified in AB 52 and the PRC, the City has not been provided any information about TCRs that could be affected by the proposed Project. Therefore, the determination of impacts to TCRs is drawn from other lines of evidence, as summarized below.

1.4 Recommended Findings

Information about potential impacts to TCRs was drawn from the ethnographic context, the results of a search of the Sacred Lands File of the Native American Heritage Commission (NAHC), and the results of a cultural resources inventory prepared by ECORP (2020). The methods and results of these efforts are provided in ECORP 2020 and are hereby incorporated by reference. In summary, the ethnographic information reviewed for the Project, including ethnographic maps, does not identify any villages, occupational areas, or resource procurement locations in or around the current Project Area. In addition, the Sacred Lands File failed to identify any sacred lands or tribal resources in or near the Project Area. The cultural resources survey did not reveal any Native American archaeological sites within or adjacent to the proposed Project Area. Finally, as summarized above, two of the three tribes notified of the Project responded to the City's offer to consult; however, none provided any information about TCRs in the

Project Area. This is not unexpected, as the Project is in a highly disturbed environment and does not involve substantial ground disturbance. As noted in the City's initial notification letter and follow up correspondence with tribes:

"the purpose of this project is to remodel an existing shed to install crematory equipment inside of it, which is a high-temperature furnace with associated equipment. The existing driveway area adjacent to the shed will be subject to minor improvements, but there is no mass grading or major excavation associated with the remodeling of the shed. There will be no new construction of buildings or structures" (Josh Kinkade to Mariah Mayberry, December 14, 2020).

In reviewing the lines of evidence summarized above, this Project will not have an impact on known TCRs. There exists an extremely low potential for the discovery of previously unknown TCRs during Project construction, but if TCRs were to be encountered, the Project activity could result in a significant impact. Implementation of unanticipated discovery procedures, as provided in mitigation measure TCR-1 below, would reduce that impact to less than significant.

TCR-1: Unanticipated Discovery of Tribal Cultural Resources. If potentially significant Tribal Cultural Resources (TCRs) are discovered during ground disturbing construction activities, all work shall cease within 50 feet of the find. A Native American Representative from traditionally and culturally affiliated Native American Tribes that requested consultation on the project shall be immediately contacted and invited to assess the significance of the find and make recommendations for further evaluation and treatment, as necessary. If deemed necessary by the City, a qualified cultural resources specialist meeting the Secretary of Interior's Standards and Qualifications for Archaeology, may also assess the significance of the find in joint consultation with Native American Representatives to ensure that Tribal values are considered. Work at the discovery location cannot resume until the City, in consultation as appropriate and in good faith, determines that the discovery is either not a TCR, or has been subjected to culturally appropriate treatment, if avoidance and preservation cannot be accommodated.

If you have any questions, you may reach me by phone at (916) 782-9100 or by email at LWestwood@ecorpconsulting.com.

Sincerely,



Lisa Westwood, RPA
Vice President and Director of Cultural Resources

Attachment A: Non-Confidential Tribal Consultation Record

REFERENCES CITED:

ECORP Consulting, Inc. 2020. *Cultural Resources Inventory Report for the Lakeside Memorial Lawn Storage Shed Project, City of Folsom, California.*

ATTACHMENT A

Non-Confidential Tribal Consultation Record

Folsom Crematorium

AB 52 Log

City received a general request letter dated November 23, 2015 from United Auburn Indian Community.

City received a general request letter dated January 13, 2020 from Wilton Rancheria.

City received a general request letter dated March 2, 2016 from Lone Band of Miwok Indians.

November 25, 2020: Initial notices were mailed to UAIC, Lone, and Wilton Rancheria. The 30-day response window closes on December 25, 2020.

December 1, 2020: City received an email from Ms. Mariah Mayberry with Wilton Rancheria formally requesting consultation under AB 52. In her response, Ms. Mayberry requested the opportunity for a tribal representative to participate in cultural resource surveys and requested copies of all cultural resource assessments and results of record searches. The tribe also provided their recommended mitigation measures to the City.

December 11, 2020: City staff received an email from UAIC containing a PDF form that acknowledged receipt of the notice of opportunity to consult on the project, but the tribe did not request nor defer to engage in consultation.

December 14, 2020: City initiated consultation with Wilton by email, invited to consultation meeting on 12/17. Wilton requested to reschedule to 1/5/2021.

January 5, 2021: City held consultation meeting, however Wilton did not attend.



Daniel

MIWOK United Auburn Indian Community
MAIDU of the Auburn Rancheria

Gene Whitehouse
Chairman

John L. Williams
Vice Chairman

Danny Rey
Secretary

Brenda Adams
Treasurer

Calvin Moman
Council Member

November 23, 2015

City of Folsom Representative
50 Natoma Street
Folsom, CA 95630

RE: AB 52 Notification Request, California Environmental Quality Act Public Resources Code section 21080.3, subd. (b) Request for Formal Notification of Proposed Projects within the United Auburn Indian Community (UAIC) of the Auburn Rancheria's Geographic Area of Traditional and Cultural Affiliation

Dear City of Folsom Representative:

In accordance with Public Resources Code Section 21080.3.1, subd. (b), The United Auburn Indian Community (UAIC) of the Auburn Rancheria, which is traditionally and culturally affiliated with a geographic area within your agency's geographic area of jurisdiction, requests formal notice of and information on proposed projects for which your agency will serve as a lead agency under the California Environmental Quality Act (CEQA), Public Resources Code Section 21000 et seq.

Enclosed with this letter is a copy of a map that depicts the ancestral territory that the UAIC is traditionally and culturally affiliated with. UAIC's traditionally and culturally affiliated geographic area is supported by, and has been developed through, multiple lines of evidence including oral tradition, history, ethnography, geography, linguistic, kinship, biology, archaeology, anthropology, folklore, other relevant information and expert opinion, and Congressional action through the Auburn Indian Restoration Act of 1994 (H.R. 4228 [103rd]).

Pursuant to Public Resources Code section 21080.3.1, subd. (b), and until further notice, we hereby designate the following person as the tribe's lead contact person for purposes of receiving notices of proposed projects from your agency:

Lead Contact:
Gene Whitehouse,
Chairman
10720 Indian Hill Road
Auburn, CA 95603
916-883-2320

Copies to:
Jason Camp
Tribal Historic Preservation Officer
10720 Indian Hill Road
Auburn, CA 95603
(530) 883-2320
jcamp@auburnrancheria.com

Marcos Guerrero
Cultural Resources Manager
10720 Indian Hill Road
Auburn, CA 95603
(530) 883-2364
mguerrero@auburnrancheria.com

We request that all notices be sent via certified U.S. Mail with return receipt and that your notices specify a lead contact person for your agency. Following receipt and review of the information your agency provides, within the 30-day period outlined in Public Resources Code section 21080.3.1, subd. (d), the UAIC may request consultation, as defined by Public Resources Code section 21080.3.1, subd. (b), pursuant to Public Resources Code section 21080.3.2 to discuss issues including the type of environmental review to be conducted, project alternatives, significant effects of the project and mitigation measures for any project impacts (direct, indirect and cumulative) a specific project may cause to tribal cultural resources.

For your information, UAIC's policy is to be present during project cultural resource surveys, including initial pedestrian surveys, to identify tribal cultural resources. UAIC's policy is also to be provided all existing cultural resource assessments, including the request for and results of any records search that may have been conducted prior to the initial survey or consultation meeting. Finally, UAIC's general policy is preservation in place and avoidance of tribal cultural resources, and any subsurface testing or data recovery must not occur without first consulting with UAIC and receiving UAIC's written consent.

We recommend that your agency retain this correspondence in your permanent files. If you have any questions or need additional information, please contact Marcos Guerrero, Cultural Resources Manager, at (530) 883-2364 or by email at mguerrero@auburnrancheria.com.

Sincerely,



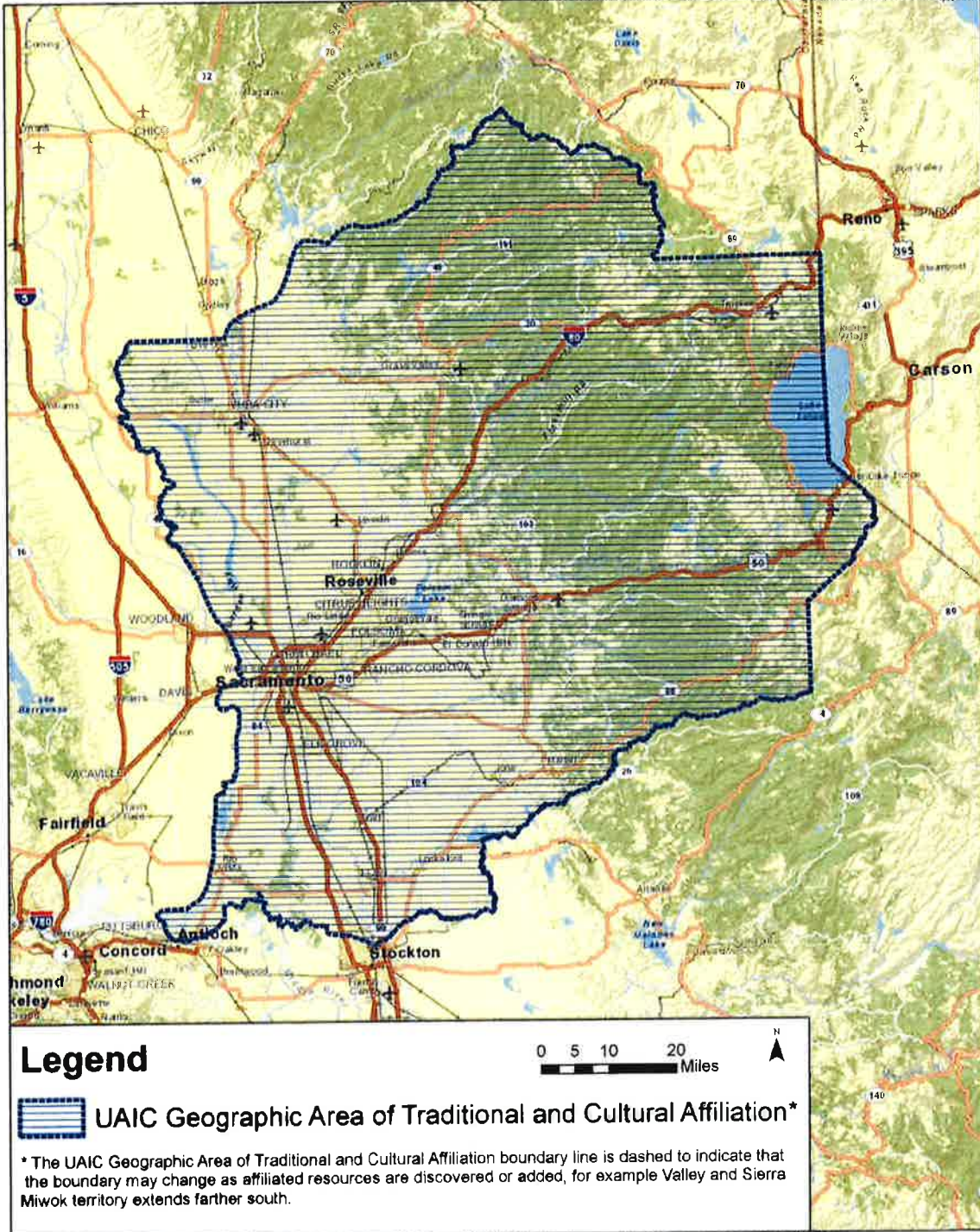
Gene Whitehouse,
Chairman

CC: Jason Camp, THPO
Marcos Guerrero, CRM
Cynthia Gomez, NAHC

UAIC Geographic Area of Traditional and Cultural Affiliation (for the purposes of California AB 52)

This area includes all of Amador, El Dorado, Nevada, Placer, Sacramento, Sutter and Yuba counties as well as portions of Butte, Plumas, San Joaquin, Sierra, Solano, and Yolo counties.

Map Created on 10/28/2015 by the UAIC Tribal Preservation Department



This map is no substitute for direct consultation with UAIC prior to considering any proposed project or commencing any archaeological activities in or around sensitive areas.

Note: While we make every effort to identify Tribal Cultural Resources that exist within the UAIC Geographic Area of Traditional and Cultural Affiliation, it is highly probable that there are additional, older sites that we have not yet identified due to restricted access or other reasons or that agricultural or construction activities have distributed burials and cultural materials beyond the previously known boundaries of these sites. Even if these materials are in a disturbed condition, they still retain cultural value to UAIC and should be respected and protected. Because of this, thorough survey with a qualified Native American Monitor to confirm site boundaries and search for unknown sites is critical. This survey should be conducted after consultation with the Tribe and prior to the final determination of the type of environmental document to be used.

Wilton Rancheria



9728 Kent Street, Elk Grove, CA 95624

January 13, 2020

City of Folsom
50 Natoma St
Folsom, CA 95630

RE: California Environmental Quality Act Public Resources Code section 21080.3, subd. (b) Request for Formal Notification of Proposed Projects Within Wilton Rancheria Tribe's Geographic Area of Traditional and Cultural Affiliation

Dear Sir or Madam,

As of the date of this letter, in accordance with Public Resources Code Section 21080.3.1, subd. (b), Wilton Rancheria, which is traditionally and culturally affiliated with a geographic area within your agency's geographic area of jurisdiction, requests formal notice of and information on proposed projects for which your agency will serve as a lead agency under the California Environmental Quality Act (CEQA), Public Resources Code section 21000 et seq.

Pursuant to Public Resources Code section 21080.3.1, subd. (b), and until further notice, we hereby designate the following person as the tribe's lead contact person for purposes of receiving notices of proposed projects from your agency:

Attn: Chairman Raymond C. Hitchcock / Director
Ralph Hatch Wilton Rancheria, Cultural Preservation
Department
9415 Rancheria Drive
Wilton, CA 95693 crd@wiltonrancheria-nsn.gov
rhatch@wiltonrancheria-nsn.gov

We request that all notices be sent via certified U.S. Mail with return receipt. Following receipt and review of the information your agency provides, within the 30-day period proscribed by Public Resources Code section 21080.3.1, subd. (d), the Wilton Rancheria may request consultation, as defined by Public Resources Code section 21080.3.1, subd. (b), pursuant to Public Resources Code section 21080.3.2 to mitigate any project impacts a specific project may cause to tribal cultural resources.

If you have any questions or need additional information, please contact our lead contact person listed above.

Respectfully,

Ralph Hatch

Ralph Troy Hatch
Executive Director of Cultural Preservation



Ione Band of Miwok Indians

A Federally Recognized Sovereign Tribe

2 March 2016

City of Folsom
Community Development Dept.
David Miller Director
50 Natoma St.
Folsom Calif. 95630

RE: Formal Request for Tribal Consultation Pursuant to the California Environmental Quality Act (CEQA), Public Resources Code section 21080.3.1, subs. (b), (d) and (e) for City of Folsom

Dear , Mr. Miller

This letter constitutes a formal request for tribal consultation for the first phase of planning under the provisions of the California Environmental Quality Act (CEQA) (Public Resources Code section 21080.3.1 subdivisions (b), (d) and (e)) for the mitigation of potential project impacts to tribal cultural and environmental resources for the above referenced project. The Ione Band of Miwok Indians requests formal notice and information for all projects within your agency's jurisdiction.

The Ione Band of Miwok Indians requests consultation on the following topics listed below, which shall be included in consultation if requested (Public Resources Code section 21080.3.2, subd. (a)):

- Alternatives to the project
- Recommended mitigation measures
- Significant effects of the project

The Ione Band of Miwok Indians also requests consultation on the following discretionary topics listed below (Public Resources Code section 21080.3.2, subd. (a)):

- Type of environmental review necessary
- Significance of tribal cultural resources, including any regulations, policies or standards used by your agency to determine significance of tribal cultural resources
- Significance of the project's impacts on tribal cultural resources
- Project alternatives and/or appropriate measures for preservation or mitigation that we may recommend, including, but not limited to:

- (1) Avoidance and preservation of the resources in place, pursuant to Public Resources Code section 21084.3, including, but not limited to, planning and construction, geotechnical tests, utility location, and pedestrian surveys to avoid harming the resources (including water, endangered tribal plant resources, and endangered animal resources), and to protect the cultural and natural context, or planning greenspace, parks or other open space, to incorporate the resources with culturally appropriate protection and management criteria;



lone Band of Miwok Indians

A Federally Recognized Sovereign Tribe

(2) Treating the resources with culturally appropriate dignity, taking into account the tribal cultural values and meaning of the resources, including but not limited to the following:

- Protecting the cultural character and integrity of the resource
- Protecting the traditional use of the resource
- Protecting the confidentiality of the resource

(3) Permanent conservation easements or other interests in real property, with culturally appropriate management criteria for the purposes of preserving or utilizing the resources or places

(4) Protecting the resource

Additionally, the lone Band of Miwok Indians would like to receive any cultural resources assessments or other assessments that have been completed on all or part of the project's potential "area of project effect" (APE), including, but not limited to:

1. The results of any record search that may have been conducted at an Information Center of the California Historical Resources Information System (CHRIS), including, but not limited to:

- A listing of any and all known cultural resources that have already been recorded on or adjacent to the APE
- Copies of any and all cultural resource records and study reports that may have been provided by the Information Center as part of the records search response
- Notification of whether the probability is low, moderate, or high that cultural resources are located in the APE
- Notification if a records search indicates a low, moderate or high probability that unrecorded cultural resources are located in the potential APE
- Notification if a survey is recommended by the Information Center to determine whether previously unrecorded cultural resources are present

2. The results of any archaeological inventory survey that was conducted, including:

- Any report that may contain site forms, site significance, and suggested mitigation measures
- All information regarding site locations, Native American human remains, and associated funerary objects; such information should be placed in a separate confidential addendum, and not be made available for public disclosure in accordance with Government Code Section 6254.10.



Lone Band of Miwok Indians

A Federally Recognized Sovereign Tribe

3. The results of any Sacred Lands File (SFL) check conducted through the Native American Heritage Commission. The request form can be found at <http://www.dot.ca.gov/hq/env/cultural/#templates> under Compliance Document Templates. Click on the link *Sacred Lands Inventory Form* to download the pdf. USGS 7.5- minute quadrangle name, township, range, and section are required for the search.
4. Any ethnographic studies conducted for any area including all or part of the potential APE
5. Any geotechnical reports regarding all or part of the potential APE

We would like to remind your agency that CEQA Guidelines section 15126.4, subdivision (b)(3) states that preservation-in-place is the preferred manner of mitigating impacts to archaeological sites. Section 15126.4, subd. (b)(3) of the CEQA Guidelines has been interpreted by the California Court of Appeal to mean that "feasible preservation in place must be adopted to mitigate impacts to historical resources of an archaeological nature unless the lead agency determines that another form of mitigation is available and provides superior mitigation of impacts." *Madera Oversight Coalition v. County of Madera* (2011) 199 Cal.App.4th 48, disapproved on other grounds, *Neighbors for Smart Rail v. Exposition Metro Line Construction Authority* (2013) 57 Cal.4th 439.

The Lone Band of Miwok Indians expects to begin consultation within 30 days of your receipt of this letter. Please contact the Cultural Committee of the Lone Band of Miwok Indians.

Thank you.

Sincerely,

Randy Yonemura
Cultural Committee Chair
P.O. Box 699
9252 Bush St., Suite 2
Plymouth, CA 95669
Tel. (209) 245-5800
Email: Randy_yonemura@yahoo.com



From: Scott Johnson <sjohnson@folsom.ca.us>
Sent: Thursday, April 18, 2019 8:53 AM
To: [REDACTED]
Subject: FW: New Contact Info

FYI

Scott A. Johnson, AICP
Planning Manager
Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6206



From: Cynthia Turner <Cynthia@ionemiwok.net>
Sent: Thursday, April 18, 2019 8:41 AM
To: Scott Johnson <sjohnson@folsom.ca.us>
Subject: New Contact Info

Good Morning

We received your letter; Randy Yonemura is no longer our Chairwoman at the Lone Band Of Miwoks.

The new contact is Sara D. Setshwaelo – Chairwomen

Thank You,

Cynthia Turner
Administrative Assistant
Office: (209) 245-5800 x403
Cell: (209)418-8435

Lone Bank of Miwok Indians
9252 Bush Street
PO Box 699
Plymouth, CA 95669



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

November 25, 2020

Sara D. Setshwaelo
Chairwoman
Ione Band of Miwok Indians
9252 Bush Street
P.O. Box 699
Plymouth, CA 95669

RE: Notice of Opportunity to Consult under Assembly Bill 52 for the Lakeside Memorial Lawn Storage Shed Project, City of Folsom (File # PN 20-160)

Dear Chairwoman Setshwaelo:

The City of Folsom is initiating environmental review under the California Environmental Quality Act (CEQA) for the Lakeside Memorial Lawn Crematorium Project. Igor Semenyuk, on behalf of Lakeside Memorial Lawn (applicant), is proposing to repurpose an existing corrugated steel storage shed at Lakeside Memorial Lawn, located at 1201 Forrest Street. The project includes the installation of a human crematorium in an existing shed, located northeast of the existing mausoleum structures in a previously disturbed maintenance area. Minor improvements to the small existing access road adjacent to the shed are also proposed. Project site plans are enclosed for your reference.

Assembly Bill 52 (AB 52) and Section 21080.3.1(d) of the California Public Resources Code (PRC) require that we respond to your written request to be notified of projects in our jurisdiction that will be reviewed under CEQA. Your name was provided to us as the point of contact for your tribe. We are hereby notifying you of an opportunity to consult with us regarding the potential for this project to impact Tribal Cultural Resources, as defined in Section 21074 of the PRC. The purposes of tribal consultation under AB 52 are to determine, as part of the CEQA review process, whether or not Tribal Cultural Resources are present within the project area, and if so, whether or not those resources will be significantly impacted by the proposed project. If Tribal Cultural Resources may be significantly impacted, then consultation will also help to determine the most appropriate way to avoid or mitigate those impacts.

In accordance with Section 21080.3.1(d) of the PRC, you have 30 days from the receipt of this letter to either request or decline consultation in writing for this project. Please send your written response to my attention at the City of Folsom, Community Development Department, 50 Natoma Street, Folsom, 95630. You may also reach me by email at jkinkade@folsom.ca.us or at 916-461-6209. In your response, please reference the following project name: Lakeside Memorial Lawn Storage Shed, PN 20-160. If I do not receive a response within 30 days, then we will proceed.

Thank you and we look forward to your response.

Respectfully,

Josh Kinkade
Associate Planner
City of Folsom

50 NATOMA STREET
FOLSOM, CA 95630

WWW.FOLSOM.CA.US



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

November 25, 2020

Gene Whitehouse
Chairman
United Auburn Indian Community of the Auburn Rancheria
10720 Indian Hill Road
Auburn, CA 95603

RE: Notice of Opportunity to Consult under Assembly Bill 52 for the Lakeside Memorial Lawn Storage Shed Project, City of Folsom (File # PN 20-160)

Dear Chairman Whitehouse:

The City of Folsom is initiating environmental review under the California Environmental Quality Act (CEQA) for the Lakeside Memorial Lawn Crematorium Project. Igor Semenyuk, on behalf of Lakeside Memorial Lawn (applicant), is proposing to repurpose an existing corrugated steel storage shed at Lakeside Memorial Lawn, located at 1201 Forrest Street. The project includes the installation of a human crematorium in an existing shed, located northeast of the existing mausoleum structures in a previously disturbed maintenance area. Minor improvements to the small existing access road adjacent to the shed are also proposed. Project site plans are enclosed for your reference.

Assembly Bill 52 (AB 52) and Section 21080.3.1(d) of the California Public Resources Code (PRC) require that we respond to your written request to be notified of projects in our jurisdiction that will be reviewed under CEQA. Your name was provided to us as the point of contact for your tribe. We are hereby notifying you of an opportunity to consult with us regarding the potential for this project to impact Tribal Cultural Resources, as defined in Section 21074 of the PRC. The purposes of tribal consultation under AB 52 are to determine, as part of the CEQA review process, whether or not Tribal Cultural Resources are present within the project area, and if so, whether or not those resources will be significantly impacted by the proposed project. If Tribal Cultural Resources may be significantly impacted, then consultation will also help to determine the most appropriate way to avoid or mitigate those impacts.

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Thank you and we look forward to your response.

Respectfully,

Josh Kinkade
Associate Planner
City of Folsom

50 NATOMA STREET
FOLSOM, CA 95630

WWW.FOLSOM.CA.US



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

November 25, 2020

Ralph Hatch
Director of Cultural Preservation Department
Wilton Rancheria
9415 Rancheria Drive
Wilton, CA 95693

RE: Notice of Opportunity to Consult under Assembly Bill 52 for the Lakeside Memorial Lawn Storage Shed Project, City of Folsom (File # PN 20-160)

Dear Director Hatch:

The City of Folsom is initiating environmental review under the California Environmental Quality Act (CEQA) for the Lakeside Memorial Lawn Crematorium Project. Igor Semenyuk, on behalf of Lakeside Memorial Lawn (applicant), is proposing to repurpose an existing corrugated steel storage shed at Lakeside Memorial Lawn, located at 1201 Forrest Street. The project includes the installation of a human crematorium in an existing shed, located northeast of the existing mausoleum structures in a previously disturbed maintenance area. Minor improvements to the small existing access road adjacent to the shed are also proposed. Project site plans are enclosed for your reference.

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Thank you and we look forward to your response.

Respectfully,

Josh Kinkade
Associate Planner
City of Folsom

50 NATOMA STREET
FOLSOM, CA 95630

WWW.FOLSOM.CA.US



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

November 25, 2020

Raymond C. Hitchcock
Wilton Rancheria
9415 Rancheria Drive
Wilton, CA 95693

RE: Notice of Opportunity to Consult under Assembly Bill 52 for the Lakeside Memorial Lawn Crematorium Project, City of Folsom (File # PN 20-160)

Dear Chairman Hitchcock:

The City of Folsom is initiating environmental review under the California Environmental Quality Act (CEQA) for the Lakeside Memorial Lawn Crematorium Project. Igor Semenyuk, on behalf of Lakeside Memorial Lawn (applicant), is proposing to repurpose an existing corrugated steel storage shed at Lakeside Memorial Lawn, located at 1201 Forrest Street. The project includes the installation of a human crematorium in an existing shed, located northeast of the existing mausoleum structures in a previously disturbed maintenance area. Minor improvements to the small existing access road adjacent to the shed are also proposed. Project site plans are enclosed for your reference.

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Thank you and we look forward to your response.

Respectfully,

Josh Kinkade
Associate Planner
City of Folsom

50 NATOMA STREET
FOLSOM, CA 95630

WWW.FOLSOM.CA.US

1201 FORREST ST IN FOLSOM, CA 95630

PROJECT DESCRIPTION & SCOPE OF WORK

SCOPE OF WORK
SEE PERMIT FOR A NEW CREMATORY AND COOLER IN AN EXISTING SHED.

APN	07002800010000
ZONING	OSC
OCCUPANCY GROUP	
CONSTRUCTION TYPE	VB
NO. OF FLOORS	1
LOT AREA	523581 SQ. FT. / 12.02 ACRES



VADIM ZANKO
7844 MADISON AVE
STE 106 FAJR OAKS, CA
95628
916-312-2131
support@levdesigns.com



LAKESIDE MEMORIAL
LAWN CEMETERY

1201 FORREST ST,
FOLSOM
CA 95630

07002600010000

PROJECT DIRECTORY

DESIGNER
VADIM ZANKO
LEV DESIGNS
(916)-312-2131
vzanko@levdesigns.com

OWNER

SHEET INDEX

SHEET	DESCRIPTION
A1.0	COVER PAGE
A2.0	OVERALL SITE PLAN
A2.1	DETAILED SITE PLAN
A3.0	EXISTING FLOOR PLAN

BUILDING INFORMATION

ALL WORK SHALL FULLY COMPLY WITH BUT NOT BE LIMITED TO:
 A. 2019 CALIFORNIA BUILDING CODE (CBC)
 B. 2019 CALIFORNIA PLUMBING CODE (CPC)
 C. 2019 CALIFORNIA MECHANICAL CODE (CMC)
 D. 2019 CALIFORNIA ELECTRIC CODE (CEC)
 E. 2019 CALIFORNIA FIRE CODE (FC)
 F. 2019 CALIFORNIA REFERENCED STANDARDS CODE
 G. 2019 GREEN BUILDING STANDARDS CODE (GB)
 H. 2019 CALIFORNIA ENERGY
 I. 2019 ENERGY STANDARDS
 J. ALL OTHER APPLICABLE CODES AND ORDINANCES (CITY AND COUNTY).
 GOVERNING AUTHORITIES AND CODES TAKE PRECEDENCE OVER DRAWINGS
 AND SPECIFICATIONS. REPORT DISCREPANCIES TO THE ARCHITECT IMMEDIATELY.

ABBREVIATIONS

IPHS	1 PILE / 1 SHELF
SI	2 SHELVES
ABV.	ABOVE
A.F.F.	ABOVE FINISHED FLOOR
BDRM	BEDROOM
BVL	BELOW
B.C.	BOTTOM OF CEILING
CLG.	CASEMENT WINDOW
CSMT.	CONCRETE
CONC.	CONCRETE
DET.	DETAIL
DS	DOWNSPOUT
DS BLW	DOWNSPOUT BELOW
F.F.	FACE OF FRAMING
FLR	FLOOR
FOUN.	FOUNDATION
FR. DR	FRENCH DOOR
FX	FIXED WINDOW
HDR	HEADER
HL-HT	HEEL HEIGHT
HT	HEIGHT
LNDRY	LAUNDRY
LAV.	LAVATORY
LINE	LINE
MEN	MENSROOM
N.I.C.	NOT IN CONTRACT
O.H.	OPPOSITE HAND
OPT.	OPTIONAL
P.T. LUMBER	PRESSURE TREATED LUMBER
RF	ROOF
RF. BLW	ROOF BELOW
S.S.D.	SEE STRUCTURAL DRAWINGS
SH	SINGLE HUNG
SL	SLIDER WINDOW
T.	TEMPERED GLASS
T&C	TONGUE AND GROOVE
TEMP	TEMPERED GLASS
T.O. PAD	TOP OF GRADED PAD
T.O.P.	TOP OF PLATE
T.O.C.	TOP OF CURB
T.O. SLAB	TOP OF SLAB
T.O.S.F.	TOP OF SUBFLOOR
T.O.B.	TOP OF BEAM
TYP.	TYPICAL
U.O.M.	UNLESS OTHERWISE NOTED
VERT.	VERTICAL
WD.	WINDOW
WDW.	WINDOW
WP	WEATHER PROOF
W.I.F.	VERIFY IN FIELD

SQUARE FOOTAGE BREAKDOWN

SQUARE FOOTAGE BREAKDOWN		PERCENT
EXISTING SHED AREA		1.0%
REMODIFIED SHED AREA		34.17% (1176.00 SQ. FT.)

SPECIAL INSPECTIONS

DEFERRED SUBMITTALS:

SEPARATE PERMIT:

VICINITY MAP



NO.	MARK	IN 2022	REVISION
1		DATE	DESCRIPTION

PROJECT NO:
DATE DWG FILE:
DRAWN BY: SERGIO KOVALOV
CHECKED BY: VADIM ZANKO
COPYRIGHT: LEV DESIGNS
SHEET TITLE:
COVER PAGE:

A1.0



VADIM ZANKO
7844 MADISON AVE.
STE 106 FAIR OAKS, CA
95628
916-312-2131
support@levdesigns.com

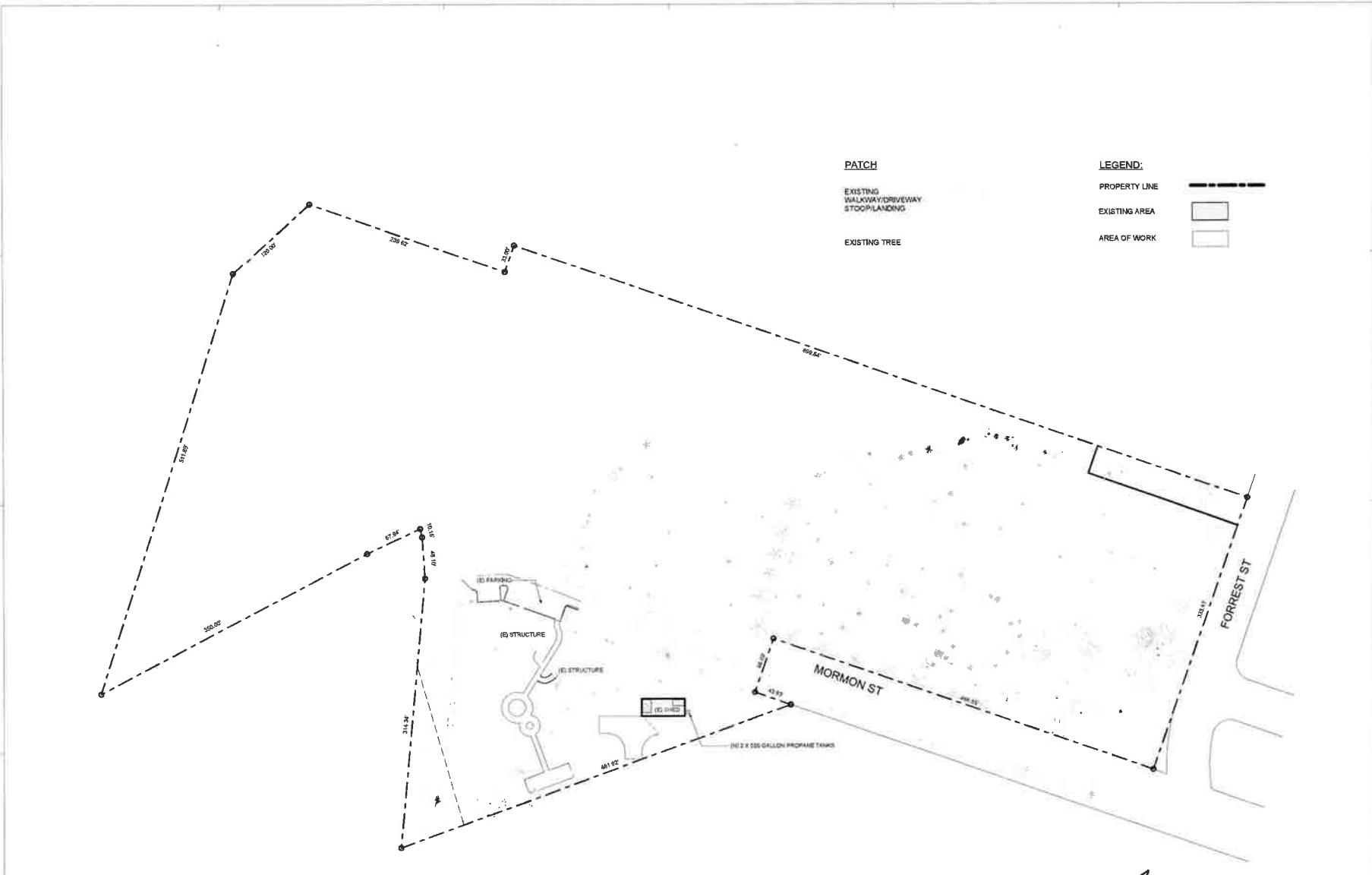
LAKESIDE MEMORIAL
LAWN CEMETERY

1201 FORREST ST,
FOLSOM
CA 95630

07002600010000

NO.	REVISION	DATE	DESCRIPTION
1	ISSUED FOR PERMITS		
2			
3			
4			
5			
6			
7			
8			
9			
10			

DRAWN BY: DENISE KOWALOV
 CHECKED BY: VADIM ZANKO
 COPYRIGHT: LEV DESIGN
 SHEET TITLE:
 OVERALL SITE PLAN



OVERALL SITE PLAN

1" = 50'



A2.0

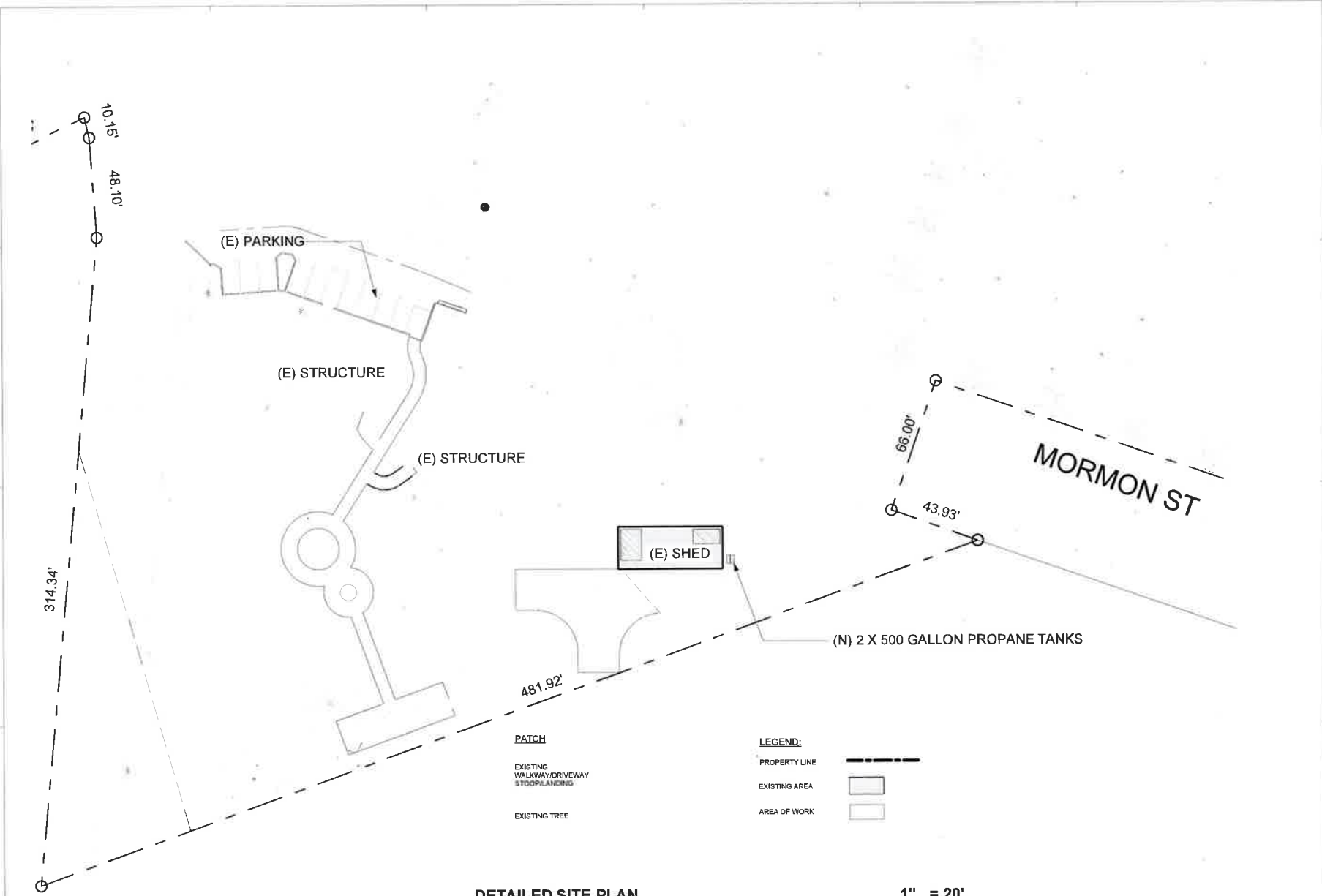


VADIM ZANKO
 7844 MADISON AVE.
 STE 106 FAIR OAKS, CA
 95628
 916-312-2131
 support@levdesigns.com

LAKESIDE MEMORIAL
 LAWN CEMETERY

1201 FORREST ST,
 FOLSOM
 CA 95630

07002600010000



DETAILED SITE PLAN

1" = 20'

PATCH

- EXISTING WALKWAY/DRIVEWAY STOOP/LANDING
- EXISTING TREE

LEGEND:

- PROPERTY LINE
- EXISTING AREA
- AREA OF WORK

NO.	DATE	DESCRIPTION

PROJECT NO: 042-000-001
 DRAWN BY: VADIM ZANKO
 CHECKED BY: VADIM ZANKO
 DATE: 05/10/2022
 PROJECT: LAKESIDE MEMORIAL LAWN CEMETERY
 SHEET TITLE: DETAILED SITE PLAN

A2.1

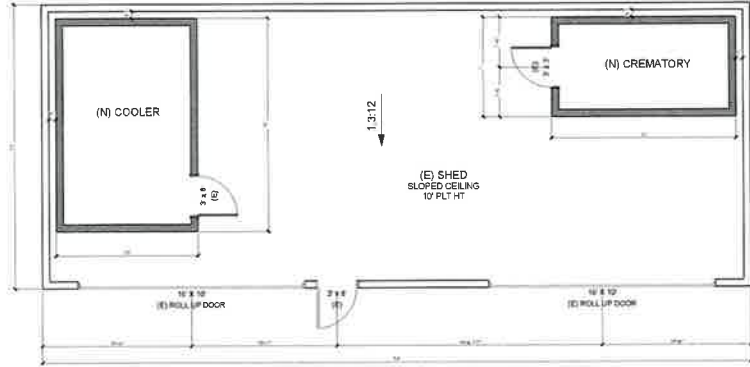


VADIM ZANKO
7844 MADISON AVE.
STE 108 FAIR OAKS, CA
95628
916-312-2131
support@levdesigns.com

LAKESIDE MEMORIAL
LAWN CEMETERY

1201 FORREST ST.
FOLSOM
CA 95630

07002600010000



EXISTING FLOOR PLAN **1/4" = 1'-0"**

- FLOOR PLAN NOTES:**
1. WINDOWS AND DOORS NOTED IN FEET (E.G. 30x42-3/8" CT).
 2. DOORS ARE NOTED WITHOUT FRAME. CONTRACTOR TO VERIFY WITH MANUFACTURER FOR DOOR FRAME SEE PRIOR TO FRAMING DOOR OPENINGS.
 3. DO NOT SCALE OFF PLANS.
 4. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.
 5. WALL INSULATION TO BE A MINIMUM OF R-13 BATTING AND ATTYE INSULATION TO BE R-8 BATT OR EQUIT.
 6. ON-SITE VERIFICATION OF ALL DIMENSIONS AND CONDITIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND THE SUB-CONTRACTORS AND/OR SUBSIDIARIES AND PRECEDENT OVER SCALE. EACH CONTRACTOR OR SUB-CONTRACTOR SHALL REPORT TO ARCHITECT IMMEDIATELY ALL CONDITIONS WHICH PREVENT THE PROPER EXECUTION OF THEIR WORK.
 7. DISCREPANCIES TO BE NOTIFIED IMMEDIATELY BY CONTRACTOR OR SUB-CONTRACTOR SHOULD ANY DISCREPANCY OR OTHER QUESTION ARISE PERTAINING TO THE WORKING DRAWINGS AND/OR SPECIFICATIONS. THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR THE RESULTS OF ANY ERRORS, OMISSIONS OR OMBESSIONS WHICH THE CONTRACTOR FAILED TO NOTIFY THE ARCHITECT/DRAWER BEFORE CONSTRUCTION AND/OR FABRICATION OF THE WORK.

LEGEND

EXISTING WINDOW

WALL LEGEND

16" CMU WALL

24" CMU WALL

PLAN AREA SQUARE FOOTAGE BREAKDOWN:
EXISTING SHED AREA 1,071 SQ. FT.

NO.	REVISION	DATE	DESCRIPTION

PROJECT NO. _____
DATE DRAWN _____
DRAWN BY: SERGIO KOVALOV
CHECKED BY: VADIM ZANKO
COMPANY: LEV DESIGNS
SHEET TITLE:
EXISTING FLOOR PLAN

A3.0

City of Folsom
50 Natoma Street
Folsom, CA 95630

CERTIFIED MAIL®



7019 2970 0001 3731 6891

Gene Whitehouse
10720 Indian Hill Rd.
Auburn, CA 95603

05/10/2022 Item No. 18.

FIRST-CLASS



02 1P
0001179600 NOV 25 2020
MAILED FROM ZIP CODE 95677
US POSTAGE
\$ 006.90⁰

CITY OF FOLSOM
50 NATOMA ST.
FOLSOM, CA 95630

FIRST-CLASS



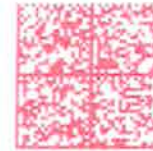
02 1P
0001179600 NOV 25 2020
MAILED FROM ZIP CODE 95677
US POSTAGE
\$ 000.50⁰

Jason Camp
10720 Indian Hill Rd.
Auburn, CA 95603

City of Folsom
50 Natoma Street
Folsom, CA 95630

Sara D. Setshwaelo
9252 Bush Street
PO BOX 699
Plymouth, CA 95669

FIRST-CLASS



05/10/2022 Item No. 18.
PITNEY BOWES
US POSTAGE
02 1P \$ 000.50⁰
0001179600 NOV 25 2020
MAILED FROM ZIP CODE 95677

CITY OF FOLSOM
50 NATOMA ST.
FOLSOM, CA 95630

MARCOS GUERRERO
10720 Indian Hill Rd.
AUBURN, CA 95603

FIRST-CLASS



PITNEY BOWES
US POSTAGE
02 1P \$ 000.50⁰
0001179600 NOV 25 2020
MAILED FROM ZIP CODE 95677

CITY OF FOLSOM
50 NATOMA STREET
FOLSOM, CA 95630

05/10/2022 Item No. 18.

FIRST-CLASS



US POSTAGE

02 1P
0001179600 NOV 25 2020
MAILED FROM ZIP CODE 95677

\$ 000.50⁰

RAYMOND C. HITCHCOCK
9415 RANCHERIA DR.
WILTON, CA 95693

CITY OF FOLSOM
50 NATOMA ST.
FOLSOM, CA 95630

FIRST-CLASS



US POSTAGE

02 1P
0001179600 NOV 25 2020
MAILED FROM ZIP CODE 95677

\$ 000.50⁰

RALPH HATCH
9415 RANCHERIA DR.
WILTON, CA 95693

05/10/2022 Item No. 18.

U.S. Postal Service™
CERTIFIED MAIL® RECEIPT
Domestic Mail Only

For delivery information, visit our website at www.usps.com®.

OFFICIAL USE

7019 2970 0001 3731 6891

Certified Mail Fee	\$ 3.55
Extra Services & Fees (check box, add fee as appropriate)	
<input checked="" type="checkbox"/> Return Receipt (hard copy)	\$ 2.85
<input type="checkbox"/> Return Receipt (electronic)	\$
<input type="checkbox"/> Certified Mail Restricted Delivery	\$
<input type="checkbox"/> Adult Signature Required	\$
<input type="checkbox"/> Adult Signature Restricted Delivery	\$
Postage	\$.50
Total Postage and Fees	\$ 6.90

Postmark
Here

Sent To
Gene Whitehouse
Street and Apt. No., or PO Box No.
10720 Indian Hill Rd.
City, State, ZIP+4®
Auburn, CA 95603
PS Form 3800, April 2015 PSN 7530-02-000-9047 See Reverse for Instructions

SENDER: COMPLETE THIS SECTION

- Complete items 1, 2, and 3.
- Print your name and address on the reverse so that we can return the card to you.
- Attach this card to the back of the mailpiece, or on the front if space permits.

1. Article Addressed to:
Gene Whitehouse
10720 Indian Hill Rd.
Auburn, CA 95603



9590 9402 5809 0034 9782 65

2. Article Number (Transfer from service label)
019 2970 0001 3731 6891

COMPLETE THIS SECTION ON DELIVERY

A. Signature
 Gene Whitehouse Agent
 Addressee

B. Received by (Printed Name) *Gene Whitehouse* C. Date of Delivery *11/15/20*

D. Is delivery address different from item 1? Yes
If YES, enter delivery address below: No

3. Service Type

<input type="checkbox"/> Adult Signature	<input type="checkbox"/> Priority Mail Express®
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail™
<input checked="" type="checkbox"/> Certified Mail®	<input type="checkbox"/> Registered Mail Restricted Delivery
<input type="checkbox"/> Certified Mail Restricted Delivery	<input checked="" type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Collect on Delivery	<input type="checkbox"/> Signature Confirmation™
<input type="checkbox"/> Collect on Delivery Restricted Delivery	<input type="checkbox"/> Signature Confirmation Restricted Delivery
<input type="checkbox"/> Insured Mail	
<input type="checkbox"/> Insured Mail Restricted Delivery (over \$500)	

PS Form 3811, July 2015 PSN 7530-02-000-9053

Domestic Return Receipt

[REDACTED]

From: Josh Kinkade <jkinkade@folsom.ca.us>
Sent: Thursday, December 10, 2020 11:12 AM
To: [REDACTED]
Subject: FW: Lakeside Memorial Lawn Crematorium Project
Attachments: 1_Mitigation_Measures_CEQA_Avoidance.docx; 2_Mitigation_Measures_CEQA NativeAmericanMonitors.docx; 3_Mitigation_Measures_CEQA_Discoveries.docx; 4_Mitigation_Measures_CEQA_Construction_Worker_Awareness_Training 04-19-19.docx

Follow Up Flag: Follow up
Flag Status: Flagged

Thanks,

Josh Kinkade
Associate Planner

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6209



CITY OF FOLSOM
DISTINCTIVE BY NATURE



www.folsom.ca.us



FOLSOM ZONING UPDATE
[Find out more!](#)

From: Cultural Resource Department Inbox <crd@wiltonrancheria-nsn.gov>
Sent: Tuesday, December 1, 2020 12:31 PM
To: Josh Kinkade <jkinkade@folsom.ca.us>
Cc: Cultural Resource Department Inbox <crd@wiltonrancheria-nsn.gov>
Subject: Lakeside Memorial Lawn Crematorium Project

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good afternoon,

This letter is notice that Wilton Rancheria would like to initiate consultation under AB 52.

We would like to discuss the topics listed in Cal. Public Resources Code section 21080.3.2(a), including the type of environmental review to be conducted for the project; project alternatives; the project's significant effects; and mitigation measures for any direct, indirect, or cumulative impacts the project may cause to tribal cultural resources. As consultation progresses, we may also wish to discuss design options that would avoid impacts to tribal cultural resources; the scope of any environmental document that is prepared for the project; pre-project surveys; and tribal cultural resource identification, significance evaluations and culturally-appropriate treatment.

This letter is also a formal request to allow Wilton Rancheria tribal representatives to observe and participate in all cultural resource surveys, including initial pedestrian surveys for the project. Please send us all existing cultural resource assessments, as well as requests for, and the results of, any records searches that may have been conducted prior to our first consultation meeting. If tribal cultural resources are identified within the project area, it is Wilton Rancheria's policy that tribal monitors must be present for all ground disturbing activities. Finally, please be advised that our preference is to preserve tribal cultural resources in place and avoid them whenever possible. Subsurface testing and data recovery must not occur without first consulting with Wilton Rancheria and receiving Wilton Rancheria's written consent.

In the letter Josh Kinkade is identified as the lead contact person for consultation on the proposed project. Mariah Mayberry will be Wilton Rancheria's point of contact for this consultation. Please contact Mariah by phone (916) 683-6000 ext. 2023 or email at mmayberry@wiltonrancheria-nsn.gov to begin the consultation process.

Thank you for involving Wilton Rancheria in the planning process at an early stage. We ask that you make this letter a part of the project record and we look forward to working with you to ensure that tribal cultural resources are protected.

Sincerely,



Mariah Mayberry

Wilton Rancheria

Tel: 916.683.6000 ext 2023 | Fax: 916.683.6015

9728 Kent Street | Elk Grove | CA | 95624

mmayberry@wiltonrancheria-nsn.gov

wiltonrancheria-nsn.gov

From: Josh Kinkade <jkinkade@folsom.ca.us>
Sent: Monday, December 14, 2020 4:26 PM
To: [REDACTED]
Cc: Robert Edgerton (RobertE@helixepi.com)
Subject: FW: Lakeside Memorial Lawn Crematorium Project

Josh Kinkade
Associate Planner

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6209



CITY OF FOLSOM
DISTINCTIVE BY NATURE



www.folsom.ca.us



FOLSOM ZONING UPDATE
[Find out more!](#)

From: Josh Kinkade
Sent: Monday, December 14, 2020 4:25 PM
To: Cultural Resource Department Inbox <crd@wiltonrancheria-nsn.gov>
Cc: Scott Johnson <sjohnson@folsom.ca.us>
Subject: RE: Lakeside Memorial Lawn Crematorium Project

Dear Ms. Mayberry,

Thank you for your response, below, regarding the above-referenced project. We appreciate the tribe's interest in this project and welcome the opportunity to discuss this project in further detail with you. We are hereby initiating consultation with you under Assembly Bill 52 by inviting you to a virtual meeting via Microsoft Teams at 8:30 a.m. on December 17, 2020. If you are unable to attend, please contact me to schedule an alternate date.

In order to facilitate our discussion, I would like to provide some additional information to you. First, the purpose of this project is to remodel an existing shed to install crematory equipment inside of it, which is a high-temperature furnace with associated equipment. The existing driveway area adjacent to the shed will be subject to minor improvements, but there is no mass grading or major excavation associated with the remodeling of the shed. There will be no new construction of buildings or structures.

Second, as requested, below is a link from which you can download a copy of the draft cultural resources technical report for the project, titled "Cultural Resources Inventory Report for the Lakeside Memorial Lawn Storage Shed Project, Folsom, California," prepared by ECORP Consulting, Inc. (November 2020). Please note that this report includes confidential information that is restricted from public distribution by state and federal law. If you are having trouble accessing the documents, you may have to open the link below in Internet Explorer or Microsoft Edge. Let me know if the link isn't working.

<https://ecorpconsulting-my.sharepoint.com/:f/p/lwestwood/EhToQu3F6Y5IvGZuP81ACEoB5nGB7feXGXTV445bLeBiRA?e=0on1Ef>

The link above also includes the link to the virtual meeting and will include a copy of the meeting agenda. If you have any questions, I can be reached by email at jkinkade@folsom.ca.us or by phone at (916)-461-6209. Thank you and we look forward to consulting with you.

Josh Kinkade
Associate Planner

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6209



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DISTINCTIVE BY NATURE



www.folsom.ca.us



**FOLSOM
ZONING UPDATE**
[Find out more!](#)

From: Cultural Resource Department Inbox <crd@wiltonrancheria-nsn.gov>
Sent: Tuesday, December 1, 2020 12:31 PM
To: Josh Kinkade <jkinkade@folsom.ca.us>
Cc: Cultural Resource Department Inbox <crd@wiltonrancheria-nsn.gov>
Subject: Lakeside Memorial Lawn Crematorium Project

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good afternoon,

This letter is notice that Wilton Rancheria would like to initiate consultation under AB 52.

We would like to discuss the topics listed in Cal. Public Resources Code section 21080.3.2(a), including the type of environmental review to be conducted for the project; project alternatives; the project's significant effects; and mitigation measures for any direct, indirect, or cumulative impacts the project may cause to tribal cultural resources. As consultation progresses, we may also wish to discuss design options that would avoid impacts to tribal cultural resources; the scope of any environmental document that is prepared for the project; pre-project surveys; and tribal cultural resource identification, significance evaluations and culturally-appropriate treatment.

This letter is also a formal request to allow Wilton Rancheria tribal representatives to observe and participate in all cultural resource surveys, including initial pedestrian surveys for the project. Please send us all existing cultural resource assessments, as well as requests for, and the results of, any records searches that may have been conducted prior to our first consultation meeting. If tribal cultural resources are identified within the project area, it is Wilton Rancheria's policy that tribal monitors must be present for all ground disturbing activities. Finally, please be advised that our preference is to preserve tribal cultural resources in place and avoid them whenever possible. Subsurface testing and data recovery must not occur without first consulting with Wilton Rancheria and receiving Wilton Rancheria's written consent.

In the letter Josh Kinkade is identified as the lead contact person for consultation on the proposed project. Mariah Mayberry will be Wilton Rancheria's point of contact for this consultation. Please contact Mariah by phone (916) 683-6000 ext. 2023 or email at mmayberry@wiltonrancheria-nsn.gov to begin the consultation process.

Thank you for involving Wilton Rancheria in the planning process at an early stage. We ask that you make this letter a part of the project record and we look forward to working with you to ensure that tribal cultural resources are protected.

Sincerely,



Mariah Mayberry

Wilton Rancheria

Tel: 916.683.6000 ext 2023 | Fax: 916.683.6015

9728 Kent Street | Elk Grove | CA | 95624

mmayberry@wiltonrancheria-nsn.gov

wiltonrancheria-nsn.gov

[REDACTED]

From: [REDACTED]
Sent: Friday, December 11, 2020 3:36 PM
To: [REDACTED]
Subject: FW: Lakeside Memorial Lawn Storage Shed (PN 20-160) Notification Confirmation
Attachments: Thank you for consulting with the UAIC.pdf

[REDACTED]

From: [REDACTED]
Sent: Friday, December 11, 2020 3:36 PM
To: Robert Edgerton (RobertE@helixepi.com) <roberte@helixepi.com>
Subject: FW: Lakeside Memorial Lawn Storage Shed (PN 20-160) Notification Confirmation

FYI, UAIC responded with a simple acknowledgement that they logged in our letter – no response regarding consultation yet.

[REDACTED]

From: Josh Kinkade <jkinkade@folsom.ca.us>
Sent: Friday, December 11, 2020 3:31 PM
To: [REDACTED]
Cc: Scott Johnson <sjohnson@folsom.ca.us>
Subject: FW: Lakeside Memorial Lawn Storage Shed (PN 20-160) Notification Confirmation

[REDACTED]

Here is another letter that came in.

Thanks,

Josh Kinkade
Associate Planner

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6209



FOLSOM
DISTINCTIVE BY NATURE



www.folsom.ca.us



From: DoNotReply@auburnrancheria.com <DoNotReply@auburnrancheria.com>
Sent: Friday, December 11, 2020 2:14 PM
To: Josh Kinkade <jkinkade@folsom.ca.us>
Subject: Lakeside Memorial Lawn Storage Shed (PN 20-160) Notification Confirmation

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.



The United Auburn Indian Community thanks you for your commitment to consultation for the following project:

Lakeside Memorial Lawn Storage Shed (PN 20-160)

You will find a copy of your consultation submission attached for your records.

Our Tribal Historic Preservation Department will review the project and respond as soon as possible. If you need to speak with someone regarding the project or your submission, please contact the Tribal Office at (530) 883-2390.

The United Auburn Indian Community is now accepting electronic consultation requests and project notifications. To learn more, [click here](#).

**This is an automated email. Replies to this address will not be received.

Nothing in this e-mail is intended to constitute an electronic signature for purposes of the Electronic Signatures in Global and National Commerce Act (E-Sign Act), 15, U.S.C. §§ 7001 to 7006 or the Uniform Electronic Transactions Act of any state or the federal government unless a specific statement to the contrary is included in this e-mail.



Thank you for consulting with the UAIC Please complete one form for each notification.

How to submit a consultation notification or project update:

1. One form must be completed for each project.
2. Forms cannot be saved and completed at a later time.
3. Include all relevant project information.
4. Upload file attachments. Multiple files can be attached.
5. Submit form.
6. You will receive a submission receipt via email when submission is complete. UAIC prefers our online submission form over certified or hard copy letters.

Contact the Tribal Office at (530) 883-2390 for questions or concerns. Ask for Tribal Historic Preservation or use the [contact form located on our website](#).

Contact Information

Consulting on Behalf of* City of Folsom
Lead Agency, Consulting Firm, Tribe

Mailing Address

Street Address
Address Line 2
City State / Province / Region
Postal / Zip Code

Point of Contact for Consultation* Josh Kinkade
Primary Contact Name

Point of Contact Email* jkinkade@folsom.ca.us

Second Point of Contact Yes
Is there more than one point of contact for this project?

Regulatory

Consulting Under* This project fall under the following regulatory requirements:

Federal State of California Federal and State
 Other

California Regulations* Select all that apply

Assembly Bill 52 (PRC §21080.3.1)
 Senate Bill 18
 Environmental Quality Act (CEQA)
 Forest Practice Rules
 CalNAGPRA
 Assembly Bill 168
 Other

Project Notification Information

Project Name * Lakeside Memorial Lawn Storage Shed (PN 20-160)
Please include Name and Reference Number (if applicable)

This is a *

- New Project
- Public Hearing
- Notice of Availability (NOA)
- Other
- Notice of Preparation (NOP)
- Existing Project
- Request for Information

Project Description Please include a brief project description

Location Please include county, city, and address (if available)

Project Documents

Documents uploaded to this form are secure and only accessible by the Tribal Historic Preservation team

Notification Attach notification letters or announcement
12022020 Folsom Lakeside Memorial Lawn Storage Shed.pdf 350.5KB
50mb maximum upload size (per file)

Reports Attach project reports, project descriptions, or supporting documents
50mb maximum upload size (per file)

Location Map Attach maps and location files. Shape files are preferred
File extensions allowed: pdf, jpg, png, kmz, lpk, dbf, prj, shp, abn, sbx, xml, shx, cpg.
NOTE: 50mb maximum upload size (per file).

Send Submission Receipt To

Primary Contact Secondary Contact Different Email

***This form submission page is offered for the convenience of consulting agencies, developers, and their respective consultants. UAIC reviews all submissions received, but makes no guarantee that submission via this online form satisfies any particular consultation or notice requirement that exists under state or federal law.

AGENDA

City of Folsom and Wilton Rancheria AB 52 Consultation Meeting for the Lakeside Memorial Lawn Storage Shed Project

Date: January 5, 2021

Time: 10:00 am

Location: Microsoft Teams

Host: Josh Kinkade, City of Folsom, (916) 461-6209

Meeting Objective: for the City of Folsom to share project information with the tribe and receive information from the tribe about potential impacts to tribal cultural resources to inform the CEQA document.

Overall Goal: for the City to make a decision about the project in a manner that is mindful of, and takes into consideration, impacts to tribal cultural resources.

Agenda:

1. Introductions
2. Project Orientation and Overview (City)
 - Purpose and Need for the Proposed Project
 - Project Description
 - Type and Scope of Environmental Review under CEQA
 - Alternatives and Design Options Considered
 - Anticipated Project Schedule
 - Summary of the Cultural Resources Survey (transmitted electronically to Wilton Rancheria on December 14, 2020)
3. Discussion of Tribal Cultural Resources (Wilton Rancheria)
 - Any Tribal Cultural Resources within the Project Area? If so:
 - Description and Location
 - Significant Effects, if any?
 - Opportunities to Avoid, if present?
 - Mitigation Measures for any Direct, Indirect, or Cumulative Impacts, if they will occur?
4. Action Items

The screenshot shows a web browser window displaying the OneDrive interface. The browser's address bar shows the URL: `ecorpcconsulting-my.sharepoint.com/personal/lwestwood_ecorpcconsulting_com/_layouts/15/onedrive.aspx?id=%2Fpersonal%2Fwestwood_eco...`. The OneDrive header includes the user name 'Lisa Westwood', a search bar, and navigation options like '+ New', 'Upload', 'Share', 'Copy link', 'Download', and 'Automate'. The main content area is titled 'My files' and shows a folder named 'Lakeside Crematorium - AB 52 Transfer Folder'. Inside this folder, there is a table of files:

Name	Modified	Modified By	File size	Sharing
2020-162_Folsom Crematorium_11-23-202...	Yesterday at 3:07 PM	Lisa Westwood	10.5 MB	Shared
Agenda_AB 52 Meeting_Lakeside Crematori...	A few seconds ago	Lisa Westwood	174 KB	Shared
Meeting Link.docx	Yesterday at 3:10 PM	Lisa Westwood	12.0 KB	Shared

At the bottom of the interface, there are links for 'Get the OneDrive apps' and 'Return to classic OneDrive'.

From: FreeConferenceCall Services <noreply@freeconferencecall.com>
Sent: Tuesday, January 5, 2021 10:19 AM
To: [Redacted]
Subject: FreeConferenceCall.com Detail Report



See The Top 3 Reasons People Contribute and Learn How Our Community Helps Us Keep FreeConferenceCall.com Free

Learn More

Account Information

Date: January 5, 2021 10:10:23 AM
Dial-in number: (712) 770-5505
Access code: 967306
Account: #s5002120

Audio

Caller	Service Type	Start Time	End Time	Duration
+1 916 461 6209 - CITY OF FOLSOM		10:10:23 AM	10:18:49 AM	9m
+1 916 316 1456 - LISA WESTWOOD		10:10:35 AM	10:18:46 AM	9m
+1 916 782 9100 - Ecorp SUGNET		10:10:37 AM	10:18:45 AM	9m
+1 916 365 8700 - HELIX		10:10:48 AM	10:18:48 AM	8m


Number of attendees: 4
Toll minutes: 35m

Note: All times in Pacific Time

Thank you for choosing FreeConferenceCall.com, the most recognized conferencing brand on the planet. Enjoy the conference? [Refer A Friend](#) today. If you have any questions, please call our Customer Service Department at (844) 844-1322 or email us at support@freeconferencecall.com.

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P O Box 41069 Long Beach, CA 90853

Tel:  (844) 844-1322

Fax: (562) 432-5250



Appendix D

Mitigation Monitoring and Reporting Program

MITIGATION MONITORING AND REPORTING PROGRAM LAKESIDE MEMORIAL LAWN CREMATORIUM

Purpose of Mitigation Monitoring and Reporting Program: The California Environmental Quality Act (CEQA), Public Resources Code Section 21081.6, requires that a Mitigation Monitoring and Reporting Program (MMRP) be established upon completing findings. CEQA stipulates that “the public agency shall adopt a reporting or monitoring program for the changes to the project which it has adopted or made a condition of project approval in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation.”

This MMRP has been prepared in compliance with Section 21081.6 of CEQA to ensure that all required mitigation measures are implemented and completed according to schedule and maintained in a satisfactory manner during the construction and operation of the project, as required. A table (attached) has been prepared to assist the responsible parties in implementing the MMRP. The table identifies individual mitigation measures, monitoring/mitigation timing, the responsible person/agency for implementing the measure, and space to confirm implementation of the mitigation measures. The numbering of mitigation measures follows the numbering sequence found in the Initial Study and Mitigated Negative Declaration.

The City of Folsom is the lead agency for the project under CEQA and shall administer and implement the MMRP. The City is responsible for review of all monitoring reports, enforcement actions, and document disposition. The City shall rely on information provided by the project site observers/monitors (e.g., construction manager, project manager, biologist, archaeologist, etc.) as accurate and up-to-date and shall provide personnel to field check mitigation measure status, as required.

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**MITIGATION MONITORING AND REPORTING PROGRAM FOR THE
LAKESIDE MEMORIAL LAWN CREMATORIUM**

Mitigation Measure	Monitoring / Mitigation Timing	Reporting / Responsible Party	Verification of Compliance	
			Initials	Date
AIR QUALITY				
<p>Mitigation Measure AIR-01: Implement SMAQMD’s Basic Construction Emission Control Practices. City approval of grading and/or improvement plans for the proposed project shall include the following SMAQMD Basic Construction Emission Control Practices:</p> <ul style="list-style-type: none"> • All exposed surfaces shall be watered two times daily. Exposed surfaces include, but are not limited to soil piles, graded areas, unpaved parking areas, staging areas, and access roads. • Cover or maintain at least two feet of free board space on haul trucks transporting soil, sand, or other loose material on the site. Any haul trucks that would be traveling along freeways or major roadways shall be covered. • Use wet power vacuum street sweepers to remove any visible trackout mud or dirt onto adjacent public roads at least once a day. Use of dry power sweeping is prohibited. • Limit vehicle speeds on unpaved roads to 15 miles per hour. • All roadways, driveways, sidewalks, parking lots shall be paved as soon as possible. In addition, building pads shall be laid as soon as possible after grading unless seeding or soil binders are used. • Minimize idling time either by shutting equipment off when not in use or reducing the time of idling to 5 minutes (as required by the state airborne toxics control measure [Title 13, Section 2485 of the California Code of Regulations]). Provide clear signage that posts this requirement for workers at the entrances to the site. • Maintain all construction equipment in proper working condition according to manufacturer’s specifications. The equipment shall be checked by a certified mechanic and determine to be running in proper condition before it is operated. 	<p>Prior to and during construction – this mitigation measure shall be included in all construction documents for implementation during construction.</p>	<p>City of Folsom; Project Applicant; Construction Contractor</p>		

CULTURAL RESOURCES				
<p>Mitigation Measure CUL-01: Avoid impacts to previously unknown archaeological resources. Prior to the initiation of ground disturbing activity, a qualified professional archaeologist shall be retained to develop and deliver a contractor awareness training program to construction supervisors. The purpose of the training is to ensure that contractors are aware of the need to limit their activity, including equipment storage, staging, parking, and ground-disturbance to only those locations identified as work areas on the official site plans.</p> <p>Prior to the initiation of ground disturbing activity, a qualified professional archaeologist shall be retained to monitor the installation of temporary high-visibility exclusionary fencing along the toe of existing mine tailings features adjacent to the shed. The fencing shall remain in place until all project activities are completed. City inspectors shall include a verification of the fencing during all required inspections. In the event that exclusionary fencing has failed, the construction supervisor must re-install or repair the fence within 24 hours.</p>	<p>Prior to and during construction – this mitigation measure shall be included in all construction documents for implementation during construction.</p>	<p>City of Folsom; Archaeologist or Qualified Cultural Resource Monitor; Construction Contractor</p>		
<p>Mitigation Measure CUL-02: Minimize impacts to any previously unknown archaeological resources during construction. If subsurface deposits believed to be cultural in origin are discovered during construction, all work must halt within a 50-foot radius of the discovery. A qualified professional archaeologist, meeting the Secretary of the Interior’s Professional Qualification Standards for pre-contact and historic archaeologist, shall be retained to evaluate the significance of the find, and shall have the authority to modify the no-work radius as appropriate, using professional judgment. The following notifications shall apply, depending on the nature of the find:</p> <ul style="list-style-type: none"> • If the professional archaeologist determines that the find does not represent a cultural resource, work may resume immediately, and no agency notifications are required. • If the professional archaeologist determines that the find does represent a cultural resource from any time period or cultural affiliation, he or she shall immediately notify the City to consult on a finding of eligibility and implement appropriate 	<p>Prior to and during construction – this mitigation measure shall be included in all construction documents for implementation during construction.</p>	<p>City of Folsom; Archaeologist or Qualified Cultural Resource Monitor; Construction Contractor</p>		

<p>treatment measures, if the find is determined to be a Historical Resource under CEQA, as defined in Section 15064.5(a) of the CEQA Guidelines or a historic property under Section 106 NHPA, if applicable. Work may not resume within the no-work radius until the City, through consultation as appropriate, determines that the site either: 1) is not an Historical Resource under CEQA, as defined in Section 15064.5(a) of the CEQA Guidelines; or 2) that the treatment measures have been completed to its satisfaction.</p>				
<p>Mitigation Measure CUL-03: Avoid and minimize impacts related to accidental discovery of human remains. If subsurface deposits believed to be cultural or human in origin are discovered during construction, all work must halt within a 50-foot radius of the discovery. A qualified professional archaeologist, meeting the Secretary of the Interior’s Professional Qualification Standards for pre-contact and historic archaeologist, shall be retained to evaluate the significance of the find, and shall have the authority to modify the no-work radius as appropriate, using professional judgment. The following notifications shall apply, depending on the nature of the find:</p> <ul style="list-style-type: none"> • If the find includes human remains, or remains that are potentially human, he or she shall ensure reasonable protection measures are taken to protect the discovery from disturbance (AB 2641). The archaeologist shall notify the Sacramento County Coroner (per §7050.5 of the Health and Safety Code). The provisions of §7050.5 of the California Health and Safety Code, §5097.98 of the California PRC, and Assembly Bill 2641 will be implemented. If the Coroner determines the remains are Native American and not the result of a crime scene, the Coroner will notify the NAHC, which then will designate a Native American Most Likely Descendant (MLD) for the project (§5097.98 of the PRC). The designated MLD will have 48 hours from the time access to the property is granted to make recommendations concerning treatment of the remains. If the landowner does not agree with the recommendations of the MLD, the NAHC can mediate (§5097.94 of the PRC). If no agreement is reached, the landowner must rebury the remains where they will not be further disturbed (§5097.98 of the PRC). This will also include either recording the site with the NAHC or the appropriate Information Center; using an open space or conservation zoning designation or easement; or recording a reinternment document with the county in which the property is located (AB 2641). If the Coroner determines that the remains are 	<p>Prior to and during construction – this mitigation measure shall be included in all construction documents for implementation during construction.</p>	<p>City of Folsom; Archaeologist or Qualified Cultural Resource Monitor; Construction Contractor</p>		

<p>human but are not Native American, then the Coroner will direct subsequent steps to address the discovery. Work may not resume within the no-work radius until the City, through consultation as appropriate, determines that the treatment measures have been completed to its satisfaction.</p>				
<p>TRIBAL CULTURAL RESOURCES</p>				
<p>Mitigation Measure TCR-01: Inadvertent discovery of TCRs. If potentially significant TCRs are discovered during ground disturbing construction activities, all work shall cease within 50 feet of the find. A Native American Representative from traditionally and culturally affiliated Native American Tribes that requested consultation on the project shall be immediately contacted and invited to assess the significance of the find and make recommendations for further evaluation and treatment, as necessary. If deemed necessary by the City, a qualified cultural resources specialist meeting the Secretary of Interior’s Standards and Qualifications for Archaeology, may also assess the significance of the find in joint consultation with Native American Representatives to ensure that Tribal values are considered. Work at the discovery location cannot resume until the City, in consultation as appropriate and in good faith, determines that the discovery is either not a TCR, or has been subjected to culturally appropriate treatment, if avoidance and preservation cannot be accommodated.</p>	<p>Prior to and during demolition and construction – this mitigation measure shall be included in all construction documents for implementation during demolition or construction.</p>	<p>City of Folsom; Native American Representative/ Monitor or Qualified Cultural Resource Monitor; Construction Contractor</p>		

Historic District Commission
Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
February 16, 2022

ATTACHMENT 12
Images of Crematoriums Adjacent to Residential
Uses in the Region



NORTH SACRAMENTO FUNERAL HOME AND CREMATORY (SACRAMENTO)



SIERRA VIEW FUNERAL CHAPEL AND CREMATORY (FAIR OAKS)



EVERGREEN MEMORIAL (SACRAMENTO)- VIEW FROM ADJACENT RESIDENCE



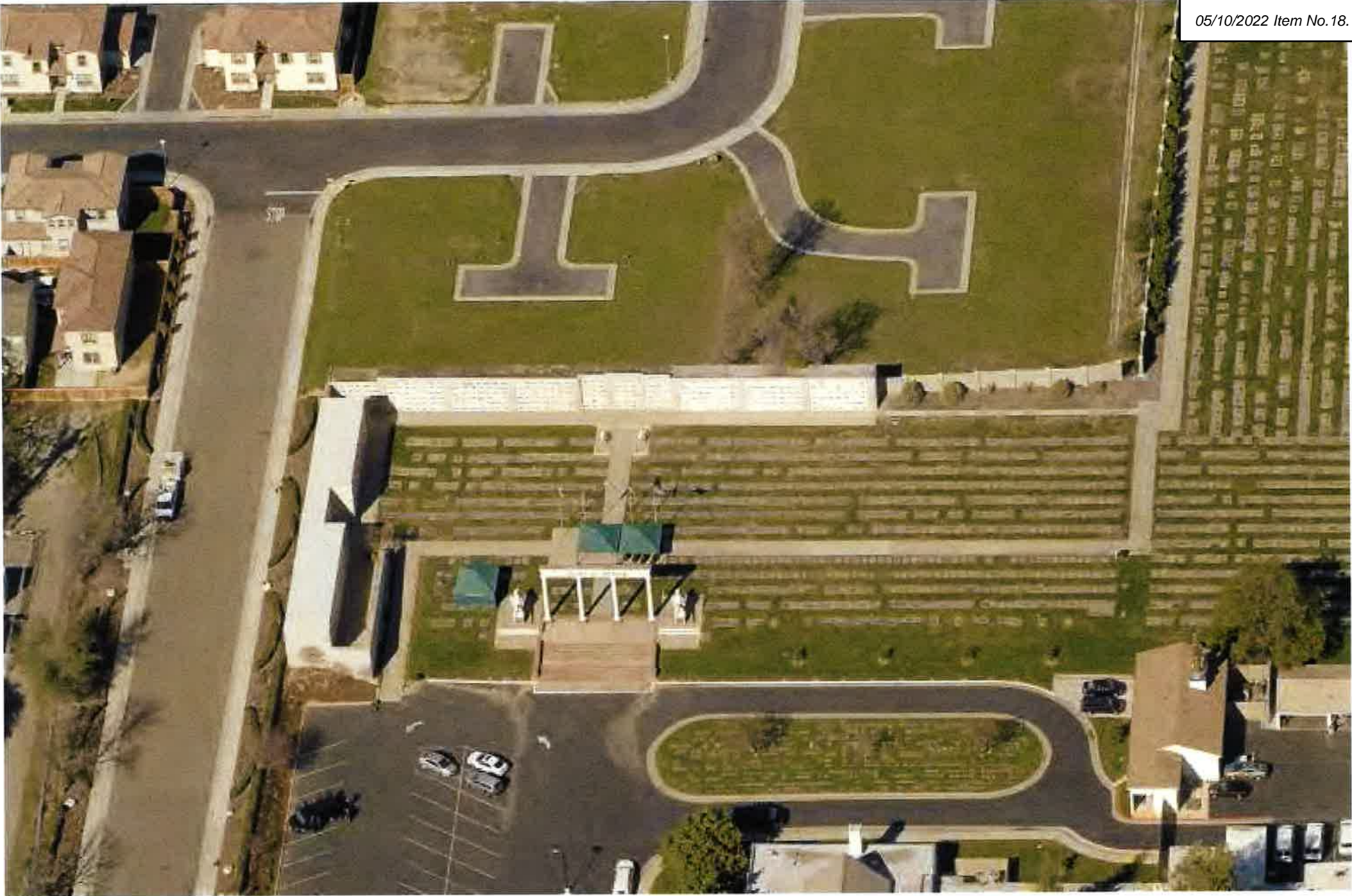
EVERGREEN MEMORIAL (SACRAMENTO)



ST. MARY'S CEMETERY AND FUNERAL CENTER (SACRAMENTO)



SACRAMENTO MEMORIAL LAWN (SACRAMENTO)



AERIAL OF MOUNT VERNON MEMORIAL PARK & MORTUARY (FAIR OAKS)



AERIAL OF SUNSET LAWN CHAPEL OF THE CHIMES (SACRAMENTO)



AERIAL OF EAST LAWN MEMORIAL PARK (SACRAMENTO)



LAKESIDE COLONIAL CHAPEL (MARYSVILLE)- RUN BY APPLICANT



CHAPEL OF THE TWIN CITIES (YUBA CITY)- RUN BY APPLICANT



JONES MORTUARY (LAKEPORT)- RUN BY APPLICANT

Historic District Commission
Lakeside Memorial Lawn Crematorium Conditional Use Permit (PN 19-182)
February 16, 2022

ATTACHMENT 13
Public Comments Received Prior to Staff Report
Publication

Dear sirs,

I use the Folsom trail systems very regularly for fun and health. The Folsom Historic District is a unique treasure with renowned trails, quality eating and entertainment venues. When I visit (at least twice per week) I often stop for a bite to eat after biking the trails. Should this crematorium be allowed to operate, I will be forced to consider alternate locals for my recreational biking. I simply will not tolerate smoke and ash from burning bodies in my health regiment. Most in the bike club, I belong to, feel the same way. Please be mindful of the jewel you have made in developing Historic Folsom as a tourist and recreation hub. If this plan is allowed to progress, I and my friends will be taking our recreation dollars elsewhere. Thank you. I am confident you will make the right decision here.

Sincerely,

Andrew Cherniski


a. m. Cherniski

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/11/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
- HIGHLY CONCERNED ABOUT **EXTREME FIRE RISK** CAUSED BY LP TANKS IN OPEN SPACE.**

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study *fails* to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. With only one way in and out for emergency vehicles, One fire will harm thousands. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

a.m. Cherniski

Contact Information:

wolfsister46@yahoo.com

Josh Kinkade

From: Scott Johnson
Sent: Thursday, August 26, 2021 9:28 PM
To: Josh Kinkade
Subject: Fwd: Folsom Crematorium proposal - in opposition

Sent from my T-Mobile 5G Device
 Get [Outlook for Android](#)

From: Anne Trim [REDACTED]
Sent: Thursday, August 26, 2021 9:16:27 PM
To: Scott Johnson <sjohnson@folsom.ca.us>
Subject: Folsom Crematorium proposal - in opposition

You don't often get email from atrim6@gmail.com. [Learn why this is important](#)

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Dear Scott

Thank you so much for all that you do to serve the community of Folsom. I am a resident and frequent (twice weekly) user of the Lake Natoma trail system with much appreciation for nature. I have obtained your contact information through a website that aims to share preferences on opposing the Lakeside Crematorium proposal. I would like to express my interest in leveraging a more win-win opportunity for development in our community by exploring alternative options for the crematorium site.

Quick list for your reference on why a win-win situation is beneficial:

- Retail sites that sit vacantly would appreciate the revenue and lease commitment where residents and nature are not impacted
- Lake Natoma is one of the remaining lakes in Folsom with consistent water flow, animals, trails, and appreciation of visitors

The Precautionary Principle (Friis, 2019) in terms of Environmental Health states the four aspects should be exhausted in respect of new or further development:

- 1) Taking preventive action to harm the environment, people, or wildlife
- 2) Shifting the burden of proof
- 3) Explore alternatives
- 4) Public participation in the decision

I would respectfully like to implore you to explore alternatives if not already in pursuit. There are retail leasing spaces that sit vacant with no residents, beautiful nature, or athletics nearby that a crematorium could be accommodated in Folsom. The win-win is that a crematorium could be placed at a vacant retail location while not disturbing residents or trail enthusiasts.

Folsom has a trail system that invites residents and visitors to explore and with the imposing threat of a crematorium built in the prime location of the trail, it would be a disadvantage to Folsom to lose the inviting vistas and Historic Folsom revenue for diners/ pedestrians.

References

City of Folsom (2021). "Lakeside Crematorium Draft IS-MND". <https://www.folsom.ca.us/home/showpublisheddocument/6087/637553756605700000>.

Friis, R. H. (2019). Essentials of environmental health (3rd ed). Retrieved from <https://content.ashford.edu>.

HUMANS

BOGS AND

BABIES

AND YOU WILL HAVE HEALTHY

CREMATORY

THE TAKE SIDE

NO TO SAY

CREMMAORY

LAKESIDE

THE



STAY NO TO

HEALTHY AIR,

IT YOU LIKE

Josh Kinkade

From: Scott Johnson
Sent: Friday, July 23, 2021 8:13 AM
To: Josh Kinkade
Subject: Fwd: Concern Regarding Historic Area

Sent from my Sprint Samsung Galaxy S10.
 Get [Outlook for Android](#)

From: Ashley Martinez [REDACTED]
Sent: Friday, July 23, 2021, 7:48 AM
To: Scott Johnson
Subject: Concern Regarding Historic Area

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To Whom It May Concern:

I would like to start by saying how much my family adores this town (particularly historic) and appreciates the work that has gone into making it such a wonderful place to live. We moved into The Preserve in 2017 and chose an older home over new construction because of the amazing location. Being able to walk to Sutter Street, paddle board to Negro Bar and utilize the endless miles of trails has been worth every penny. We have two children who are 12 and 10 that have spent the last 3 years playing basketball at the park, searching for turkeys and deer at sunset and endless hours biking around the neighborhood. We have raised them to value nature, community and their health. They spend many hours outdoors, playing together or on family walks/rides. Even through all of the Covid uncertainty, the hardest weeks for us were those where they could not play due to smoke and poor air quality. The ability to decompress in nature was greatly healing to our family.

We enjoy hosting outdoor gatherings- the first thing we added to our home was a covered patio area to expand our living space to include the outside. Having come from the Midwest, we have a true understanding of how beautiful the weather is here- even in the summer heat. We have a whole house fan and windows are open most evenings to allow the delta breeze to cool everything down.

While I attended the meeting with Igor and I greatly appreciated his patience in hearing questions (and even some erratic accusations), I am still against a crematorium being placed so close to where my children play, enjoy nature and even read outdoors. Having lived in Rancho Cordova next to a rendering plant, I can attest to being told that I would NEVER smell it, that there were very high quality filters being ran at all times and that we could always call the city with a complaint. I can also attest to the fact that we DID often smell it, normally during weekends or dinner hours- when it was very unlikely that someone would come to check that regulations were being followed. There were even times when we could smell it with all of our windows closed, coming in through the stove ventilation. I was told this was a result of wind changes that caused the cloud to rest over our neighborhood. While the smell was offputting, what concerns me more is what we DON'T smell but is still lingering in the air. The proximity of this to where families call home and children play is concerning. The study that was done was based on two burns a day but Igor himself said there was no way to know how many would actually be done on any given day. This is not acceptable, in my opinion.

Over the three years that we have lived here, many families with young children have moved in and more have brought new life to this neighborhood. I imagine that many of them have the same concerns for their little

ones lung development. I have been so happy to see our neighborhood revitalized by new families with children moving in - it feels very unfair to be putting something like this in an older residential community. I can't imagine that this would even be suggested in an area like The Palladio. I appreciate your service to our city and I am happy to answer any questions you may have.

Ashley Martinez

██████████ Fong St
████████████████████

Barbara Krieger
 RE/MAX Gold
 [REDACTED] Arena Blvd. [REDACTED]
 Sacramento, CA 95834

September 13, 2021

Re: Lake Natoma Shores

Dear City of Folsom,

I recently listed a property in the beautiful Historical District of Lake Natoma Shores. We had our first showing at the property and the agent called and left me a message telling me her clients loved the home and wanted to live in the Historic District, but had one concern she wanted to ask me about. When I called her back, she said her buyers were worried about the crematorium going in next door. I explained the situation to her and let her know her clients could check with city planning for more information. I don't know if they followed up or not, but they ended up not submitting an offer specifically because of the crematorium.

I also had an open house the first weekend on the market. Several people came through, and there were about eight groups of buyers who were all concerned about the crematorium. They all loved the home, but I never heard from them nor their agents again. Out of all the neighbors and groups of people that came through, there was only one person who liked the house and did not care about the crematorium. However, her husband was not with her. When she left, she told me she was going to tell him about the house to see if they could purchase it. I never heard from her again either.

I am not a Folsom resident, but am writing this letter out of concern for the neighborhood. Having personally seen every single buyer that walked through this beautiful home get turned off from the highly desirable location because of an undesirable crematorium should concern every single representative of the people, the nearby homes and the neighborhood itself as an entity. I wonder why the city has allowed the cemetery owner to get this far in the planning phase of the project, despite the multiple and loud cries of the Folsom residents and groups, who should not have to trouble themselves with such a threat while living in such a popular, sophisticated and historically protected area. I am absolutely astonished that this is occurring at all.

Please don't hesitate to contact me if you need further information or details of my experience. I am attaching the text conversation between myself and the first agent (with her permission) who showed the house first with the buyers who decided not to write an offer. I am also attaching the lockbox showing activity since we've been on the market. Besides my open houses, there has been only one other agent showing (during the open house) since we went on the market, and she would not tell me why her buyers did not submit an offer. There have been no other inquiries since.

Thank you for time, and hopefully your deep concern for what is happening under your watch.

Sincerely,



Barbara Krieger, Realtor

[REDACTED] Cell

[REDACTED] Office

RE/MAX Gold Real Estate Agent
 License #01317657 Since 2001



(Message)
Thu, Sep 9, 2:46 PM

Yep you're good to go tonight on Fong Street

Thank you!

Thank you too! Let me know how it goes



Will do

Monday 2:13 PM

Did they get spooked by the crematorium by fong st? Just curious cuz I'm gonna write a letter for the neighborhood 🤔

Yeah it was a bit of a concern for them I'm sorry!



SupraWEB
Management Website

Supra Showing Activity Report

Listing Details

MLS#: 221112974
Address: 1324 Fong St
Folsom, CA 95630

KeyBox Serial#: 52070776
Assigned By: Barbara Krieger - 01RMXG3Z
Agent: Barbara Krieger - 68711

Log on to SupraWEB
[Print Page](#)

Date	Key Serial	Keyholder	Keyholder Phone	Office	Office Phone
9/12/2021 5:52:00 PM	5284543	Barbara Krieger	[REDACTED]	RE/MAX Gold Natomas	[REDACTED]
9/11/2021 4:40:00 PM	5284543	Barbara Krieger	[REDACTED]	RE/MAX Gold Natomas	[REDACTED]
9/9/2021 5:27:00 PM	6681931	Haley McGlasson	[REDACTED]	Keller Williams Realty Folsom	[REDACTED]
Total Transactions: 3					Back to Top

* The information in this report shows the past 90 days of showing activity for keys that have performed an eSync with the Supra system. For a complete history of keybox activity you may also read a keybox with your key.

Contact Supra Support Toll-Free at 877-699-6787 between 5:00 AM and 7:00 PM, Pacific Time
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Note: I emailed
copies to each commissioner
and City Council member.

¹²
August ~~21~~, 2021

Barbara Manwell
1310 Fong Street
Folsom, CA 95630

Mike Kozlowski
Mayor City of Folsom
50 Natoma St.
Folsom, CA 95630

Dear Mike,

My name is Barbara Manwell. I have lived at [REDACTED] Fong Street since November 1995. It is important that you understand my opposition to a crematorium being installed at the Lakeside Cemetery in my neighborhood. The majority of houses in the Preserve subdivision have at least one person who is at risk for breathing polluted air: children 14 years or younger and Seniors 65 years or older.

The City is aware that homeowners have opposed a crematorium in the past. The developer has stated it will not have an adverse effect on the neighborhood and the needs of the entire City must be considered. This is a question of the air I breathe, the air a 92 year old woman out for her daily walk breathes, the air a toddler taking her first steps breathes, the air a group of Seniors meeting for yoga three days a week in the Young Wo Park breathe, the air three boys racing through the neighborhood on bikes breathe. We are an outdoor group of citizens.

The developers' declaration has information to support his application from the manufacturer of the crematorium, Hartwick Combustion Technologies, and HELIX Environmental Planning. Both businesses receive money from the developers. This is not reassuring to me.

Igor Semenyuk, representing Lakeside Cemetery in our recent ZOOM meeting happily informed the participants in the informational crematorium meeting that he had recently sold his property in Old Town for \$800,000. Making a profit is important to this business man. Having safe air to breathe is important to me.

Please deny Lakeside's application for a crematorium.

Sincerely yours,

Barbara Manwell

From: [Elaine Andersen](#)
To: [Josh Kinkade](#)
Subject: FW: No Crematorium
Date: Friday, August 6, 2021 8:22:48 AM

From: bob lu [REDACTED]
Sent: Sunday, August 1, 2021 1:38 PM
To: Elaine Andersen <eandersen@folsom.ca.us>
Subject: No Crematorium

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Ms. Andersen,

As longtime residents of Natoma Shores, we are asking you to not allow our beautiful neighborhood to be destroyed. The prospect of having a crematorium in the Lakeside Cemetery is very upsetting and is wrong for so many reasons. For starters, our home is a "Receptor" and we are very concerned about the pollution and the health hazards associated with it. Such a facility should not be built this close to a residential neighborhood. In addition, this will lower the property value of our homes. How will we be compensated? If there are health issues arising from the smoke will we be able to be compensated for that as well?

I hope you would honestly consider how you would react if the crematorium was built in your neighborhood and not allow this to happen.

The Barnett Family/Young Wo Circle

Josh Kinkade

From: Scott Johnson
Sent: Tuesday, February 8, 2022 9:06 AM
To: Josh Kinkade
Subject: FW: Concerned Historic District Resident - Lakeside Memorial Lawn

Scott A. Johnson, AICP
Planning Manager
Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6206



From: Ben Gamache [REDACTED]
Sent: Tuesday, February 8, 2022 9:02 AM
To: Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; kcolepolicy@gmail.com; daronbr@pacbell.net; danwestmit@yahoo.com; ankhelyi@comcast.net; johnfelts@e55tech.com; m.dascallos@yahoo.com
Subject: Fwd: Concerned Historic District Resident - Lakeside Memorial Lawn

Some people who received this message don't often get email from [REDACTED] [Learn why this is important](#)

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Dear Historic District Commissioners:

I am writing to express my deep concern at the conditional use permit submitted by Lakeside Memorial Lawn, owned by Miller Funeral Home, to install a crematory.

The Historic District is the crown jewel of Folsom. Its history, incredible landscape, unique shopping and dining experiences, and community provide so much to the city of Folsom. The thought of a crematory burning 800 pounds of human bodies **per day** will have untold negative physical, environmental, and fiscal implications that will damage both the city and the community.

Our air quality in the Valley has been the worst on record. How can we justify adding more particulate, more carbon, to our already hurting environment? How can we justify putting the citizens at risk? How can we tarnish the shining gem that is Old Folsom?

I implore you and the other staff members to consider all that is to be lost by granting this permit. I thank you for your consideration and welcome any feedback.

Sincerely,
Ben Gamache
Historic District Home Owner

August 2, 2021

Dear Mr. Josh Kincaid,

I, together with my neighbors at Lake Natoma Shores, a community next to the Lakeside Memorial Lawn Cemetery am asking you to halt the proposed construction of a crematorium there. The crematorium will be located adjacent to: a residential neighborhood, the Old Town District and the American River Parkway. This area is part of Folsom's historic district and resides near many outdoor activities promoted by our city as family friendly, safe and "distinctive by nature". It entails a farmers market, outdoor concerts; city sponsored festivals and sits adjacent to the American River Parkway that is actively used by many walkers, runners, and bicyclists daily. The Parkway serves as a window to the natural world, for all those that live and visit Folsom.

In researching the toxicity impacts of a crematorium I went to the National Collaborating Center for Environmental Health and found the following information, as per an article written in the US National Library of Medicine:

"Cremation is a combustion process whereby a casket and human remains are incinerated at a high temperature in a closed chamber. The process of corpse cremation generates numerous harmful air pollutants, including particulate matter, sulfur dioxide, nitrogen oxides, volatile organic compounds, and heavy metals. These pollutants are carcinogenic and could have **severe effects** on human health and the surrounding environment." (*)

It is of great concern to me that scientific papers have been written proving that the emissions from crematorium incineration are toxic. And that the level of toxicity is a danger to people's health and well-being, has destroyed surrounding water sheds & water quality and is of great harm to the natural environment.

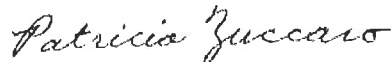
The proposed crematorium owners may see a viable commercial need for such an operation, but I portend, not in the Old Town District, adjacent to a residential neighborhood and a State Parkway. It is best suited for a heavy industrial park where the zoning and utility systems can better accommodate any toxic air emissions, toxic water runoff and any potential fire danger generated by such an operation.

Please consider our concerns and let that serve as your guide forward.

Sincerely,



Bert Pittari,
Resident Lake Natoma Shores



Patricia Zuccaro
Resident Lake Natoma Shores

(*)"Toxic atmospheric pollutants from crematoria ovens: characterization, emission factors and modeling"

>Environ Sci Pollut Res Int. 2020 Dec

Josh Kinkade

From: Elaine Andersen
Sent: Friday, January 14, 2022 8:28 AM
To: Josh Kinkade
Subject: FW: Proposed Crematorium

From: Bert p [REDACTED]
Sent: Friday, January 14, 2022 7:29 AM
To: Elaine Andersen <eandersen@folsom.ca.us>
Subject: Proposed Crematorium

You don't often get email from bertp599@gmail.com. [Learn why this is important](#)

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January 14, 2022
Dear Ms. Anderson,

I, together with my neighbors at Lake Natoma Shores, a community next to the Lakeside Memorial Lawn Cemetery am asking you to halt the proposed construction of a crematorium there. The crematorium will be located adjacent to a residential neighborhood, the Old Town District, and the American River Parkway. This area is part of Folsom's historic district and resides near many outdoor activities promoted by our city as family-friendly, safe, and "distinctive by nature". It entails a farmers market, outdoor concerts; city-sponsored festivals, and sits adjacent to the American River Parkway that is actively used by many walkers, runners, and bicyclists daily. The Parkway serves as a window to the natural world, for all those that live and visit Folsom.

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It is of great concern to me that scientific papers have been written proving that the emissions from crematorium incineration are toxic. And that the level of toxicity is a danger to people's health and well-being, has destroyed surrounding watersheds & water quality and is of great harm to the natural environment.

The proposed crematorium owners may see a viable commercial need for such an operation, but I portend, not in the Old Town District, adjacent to a residential neighborhood and a State Parkway. It is best suited for a heavy industrial park where the zoning and utility systems can better accommodate any toxic air emissions, toxic water runoff, and any potential fire danger generated by such an operation.

Please consider our concerns and let that serve as your guide forward.

Sincerely,

Bert Pittari, Patricia Zuccaro
Resident Lake Natoma Shores Resident Lake Natoma Shores

(*)"Toxic atmospheric pollutants from crematoria ovens: characterization, emission factors, and modeling"

>*Environ Sci Pollut Res Int. 2020 Dec*

Dear People in charge

I am writing in response to the notice that the Folsom Cemetery is planning to install an Crematorium. I have a home that is on Young Wo circle that would be adversely impacted.

I have a daughter who lives on Sunrise Boulevard near the Sacramento Rendering Plant, and when it is in production the fumes it produces make it unbearable to stay out side. From what I understand the crematorium would not have an air scrubber, which would put all of lower Folsom [that includes Sutter Street] in the direct path of the same noxious fumes that come from the Sacramento animal rendering plant!

I understand that the disease Mad Cow syndrome came from cows eating food made of other dead cow carcasses. What will fume and ash from Human cremation do to other Humans who breath in these by products.

Today we are told to wear N95 Mask to go out into forest fire smoke that has invisible particles, how will we feel safe that we are not being exposed to the same toxic and more lethal by products. I would also like to know how the curators of the Muir House Museum, and the Chamber of Commerce of Sutter street would feel about the impacts of these fumes on Tourism in their areas.

After reading the city planning commissions report on the impacts of this new project, I would like to know if the commission has taken a field trip to crematory to actually see it impact to adjacent areas. In reading the commissions report, on page 19 of 11 , section D leaves a question as to what a substantial number of people are and if wind conditions are taken into account. I was also quite surprised at the number of pounds of toxic chemicals and elements per hours of use that would be released into the atmosphere, as listed on page 27of 51 of this same report.

Home owner on Young Wo Circle

Bob Baker , phone number [REDACTED]

February 4, 2022

City of Folsom
 Community Development Department
 50 Natoma Street
 Folsom, CA 95630
 via email to: Pam Johns – pjohns@folsom.ca.us; Josh Kincade – jkincade@folsom.ca.us

SUBJECT: Lakeside Crematorium - Comments on Initial Study/MND

Dear Ms. Johns:

This letter provides my comments on the Lakeside Memorial Lawn Crematorium Initial Study/Mitigated Negative Declaration (IS/MND) dated January 2022.

1. **The IS/MND correctly identifies the City Council as having approval decision authority for the project; but is contradicted by the public hearing notice.** As limited by the City Charter, the Historic District Commission (HDC) is an *advisory* commission only and does not have the authority to make a final approval decision. This is recognized in the IS/MND on page 7 (section 5.0, “Required Approvals”) which specifically states that the “Folsom City Council” will act as the lead CEQA agency and consider approval of the project entitlements. However, the public hearing notice issued for the project discusses that an appeal would be required for consideration by the City Council. The public hearing notice incorrectly identifies the HDC has having final approval authority and should be corrected to be consistent with the City Charter limitations on HDC authority.
2. **A design review application is required but has not been submitted.** The proposed shed modification requires design review pursuant to Folsom Municipal Code (FMC) section 17.52.300, “Design Review,” which requires design review for, “B. All exterior renovations, remodeling, modification or addition to existing structures” and FMC Section 17.52.310 provides design review application submittal (including design review application fee) requirements. The IS/MND does not identify *design review* as a necessary entitlement and the project application on the Community Development Department’s Pending Development Applications is only for a use permit. An application for design review containing the required submittals must be submitted and design review application fees paid before the City provides further processing of this project. Importantly, such design review would then consider whether the building that would undergo a substantial expansion of use to house the proposed crematorium is consistent with Historic District design standards and guidelines or if the design of that structure requires additional modifications to comply with Historic District design standards and guidelines.
3. **The IS/MND incorrectly identifies the project as consistent with the General Plan, yet the General Plan has no land use designation for either a cemetery or a crematorium, therefore the analysis cannot tier from the General Plan EIR.** The IS/MND incorrectly states that the project would be consistent with the General Plan land use designation of Open Space. A review of the General Plan intent for the Open Space land use designation reveals that there is no basis for concluding that a crematorium is consistent with the General Plan Open Space land use designation. The General Plan

February 4, 2022

mentions "cemeteries" just one time and crematoriums not at all.¹ Table LU-5 of the General Plan (page 2-7) defines the Open Space designation as, "The Open Space land use designation encompasses the preserved natural open space areas of Folsom." Throughout the General Plan, policies encourage that development incorporate areas of open space. It is unreasonable to suggest that the intent of those policies is that such open space areas could or might be used for siting a cemetery and a crematorium.² The IS/MND interpretation and the subsequent analysis which tiers from the General Plan EIR are fundamentally flawed. The IS/MND must be revised to eliminate the erroneous statements about General Plan consistency, and the impact analyses must fully evaluate the project without attempting to tier from the General Plan EIR. Further, while FMC section 17.52.550 identifies "cemeteries" as a permitted use in the Open Space/Public primary area of the Historic District (subject to a conditional use permit when proposed by a private entity), the FMC does not extend the definition of cemetery to a crematorium. Furthermore, the FMC is subordinate to, and may not conflict with, the General Plan, therefore, expanding the unspecified FMC definition of a cemetery to include a crematorium would even further stray from the General Plan's Open Space definition and is impermissible.

4. **The IS/MND must identify whether the project would involve public attendance at services at the Lakeside Memorial cemetery and/or other locations within the City and, if so, define the parameters and evaluate impacts associated with such services.** The IS/MND provides no information on whether cremations (up to 4 per day and 500 per year) would or could be attended by family, friends, or other members of the public. If no such attendance will be permitted, a condition of any use permit for this project must specifically state that such attendance is prohibited and must include a mechanism to ensure the prohibition is enforced. Alternatively, if such attendance will be permitted, the IS/MND must be revised to discuss the maximum anticipated attendance at each cremation and evaluate the impacts associated with vehicle trips, noise, parking capacity, neighborhood circulation and pedestrian safety, effects on other services and activities at the cemetery, and other factors associated with public attendance. Furthermore, the IS/MND does not discuss whether cremations at the proposed Lakeside Crematorium would result in an increase in memorial services either at Lakeside cemetery or elsewhere in Folsom. If cremations at Lakeside cemetery would result in an increase in services at other locations in Folsom (e.g., the funeral home on Scott Street), similar evaluations of potential impacts associated with vehicle trips, noise, parking capacity, neighborhood circulation and pedestrian safety and other factors associated with those services must be addressed.
5. **The IS/MND fails to recognize the visibility of the existing shed and proposed modifications from public view locations (Folsom Boulevard) and the impacts of such visibility on visual quality and locally designated historic resources.** The IS/MND aesthetics and cultural resources analyses are fundamentally flawed by failing to recognize that **the existing structure is visible from public viewpoints** including Folsom Boulevard and the bluffs on the north side of Lake Natoma. Page 2 of the IS/MND incorrectly states that "[t]ailing piles between the site and Folsom Boulevard

¹ The single General Plan cemetery reference pertains to Noise Compatibility Standards (Table SN-1) which is unrelated to establishing land use designations and uses

² Example: Policy LU 3.1.1 - "Encourage mixed-use development in nodes located at major intersections that include housing, open space, and offices." The IS/MND's interpretation would suggest that the expectation of that policy is for those open space areas to be eligible for siting a crematorium.

February 4, 2022

prevent the site from being visible from that street.” This is incorrect. The roof and upper portion of the structure in which the crematorium is proposed to be located and on which an exhaust flume would be installed is clearly visible from Folsom Boulevard at the intersection of Natoma Street. Furthermore, between Folsom Boulevard and the structure are cobble mine tailings that are identified in the City of Folsom Cultural Resources Inventory as import local historic resources. In fact, this section of tailings is one of the most prominent locations of representative historic mine tailings visible to the largest number of viewers anywhere in the City. The impact of the project’s modification to the existing structure would be visible in the background of these tailings and must be identified and evaluated in terms of impacts to the quality of views of the tailings and impacts to the historic quality of the tailings viewshed.

6. **The air quality and health risk analysis does not use best available information and requires more certainty regarding project design.** The air quality and health risk analysis circulated with the IS/MND uses a wind rose (wind direction information) from the Sacramento Executive Airport. The Sacramento Executive Airport is nearly 20 miles from the project site and is more strongly influenced by the Delta and Central Valley wind patterns than the project site’s location along the American River corridor near the Sierra Nevada foothills. More representative wind flow data from a location nearer the site (e.g., Mather Field) is available and should be used for any air quality and health risk assessment conducted for the project to ensure that locally accurate wind directions and speeds are used in the analysis. Furthermore, the health risk analysis states that the analysis *assumes* a rain cover will be installed on the crematorium exhaust stack. However, the application and representative photograph of the exhaust stack do not discuss or identify the intent for such a cover. If a cover is not present during operation of the crematorium, the emissions and resulting health risk analysis conclusions would vary from those presented in the IS/MND. Clarification and certainty for this project design component is needed. If the rain cover is to be in place during cremations, an analysis of potential localized effects of downdraft on memorial lawn visitors while cremations should also be provided.
7. **The IS/MND does not adequately evaluate potential impacts on nesting and foraging bald eagles and other special-status bird and bat species.** The IS/MND provides no discussion of the annually active bald eagle nest located just 0.5 mile north of the project site and the potential effects of the project and exhaust stack on foraging behavior of the eagles or other protected bird and bat species. While the IS/MND discusses that effects of vehicles and workers at the site would not adversely affect migratory birds, the analysis does not address the potential effects on foraging activity of the furnace exhaust heat blast with an assumed exhaust gas temperature of 1,080 degrees Fahrenheit (°F) and a gas exit velocity of 14.7 feet per second that would occur for up to 90 minutes up to four times a day. This analysis is necessary to determine if the project would adversely affect this bald eagle nesting and foraging area or otherwise adversely affect other special-status bird or bat species that may use the area.
8. **The IS/MND analysis of fire risk is inadequate and warrants a definitive determination by the City Fire Department and California State Parks.** The IS/MND analysis of potential fire risk associated with the project is insufficient. The discussion of potential exposure to wildland fire risks downplays and fails to provide a meaningful analysis of the project’s potential fire risk. The IS/MND discusses that the “project site is located in an urbanized area in the City of Folsom.” In fact, the project

February 4, 2022

site contains and is located immediately adjacent to substantial oak woodland areas and oak canopy adjacent to the building proposed to house an exhaust flume with an assumed exhaust gas temperature of 1,080 degrees Fahrenheit (°F) and a gas exit velocity of 14.7 feet per second. Within and adjacent to the site are oak canopy linkages to the large oak woodland open spaces to the north, west, and south of the project. The IS/MND states that, "the project is not likely to cause any ignition, given that the crematory will not emit sparks." Evidence providing a definitive conclusion that the crematory – a facility design for burning and with an exhaust flume – will not emit sparks or other hot particles of potential ignition source is needed. Furthermore, the IS/MND discusses that the City Fire Department reviewed the project and did not raise any concerns regarding *water supply or site access*. This begs the question of whether the Fire Department raised other concerns and even whether the Fire Department reviewed and considered the project. The project proposal to install and operated a large furnace in an open space area adjacent to oak woodlands with residents beyond, warrants specific review and documented feedback from the Folsom Fire Department specifically confirming that the Fire Department has carefully reviewed the project and all potential fire risk issues. Also, because the project site is immediately adjacent to lands managed by State Parks, similar definitive review and input from State Parks wildland fire experts should be documented and included in the analysis.

Sincerely,



Bob Delp
Historic District Resident
Folsom, CA 95630
[REDACTED]

February 7, 2022

City of Folsom
 Community Development Department
 50 Natoma Street
 Folsom, CA 95630
 via email to: Pam Johns – pjohns@folsom.ca.us; Josh Kincade – jkincade@folsom.ca.us

SUBJECT: Lakeside Crematorium – Supplemental Comments on Initial Study/MND

Dear Ms. Johns:

On February 4, 2022, I submitted a letter with comments on the Lakeside Crematorium project Initial Study/Mitigated Negative Declaration (IS/MND). In my February 4 comments I expressed concerns with, among other things, visibility of the project shed and fire risk. Having viewed the property more closely with physical observations from Folsom Boulevard and review of aerial imagery available online, it is evident that the IS/MND fails to disclose critical information related to existing conditions at the site that are relevant to the project environmental impact analyses.

Figure 1, “Excerpt of IS/MND Figure 3 Detailed Site Plan,” on the following page is an excerpt of the IS/MND Figure 3 to which I have added red linework and labels. Figure 2, “Photograph of Existing Site Conditions,” on the following page is a photograph I took on Sunday February 6, 2022, from the south side of Folsom Boulevard just east of the Natoma Street intersection and facing northwest toward the project site. The black linework on Figure 1 shows what the IS/MND considers to be the existing site condition. However, as illustrated in red, there is another building adjacent to the project shed and located near or on the property boundary that is not identified or discussed in the IS/MND. As shown on Figure 2, that building is a metal structure that is taller and larger in footprint than the project “shed” illustrated on Figure 1. Review of aerial photograph on Google Earth indicates that sometime in 2020 or 2021 a concrete pad was installed and the second metal structure was erected. That the structure is one or very near the property line and within 5 feet or less of the project shed.

This additional structure provides a substantial constraint to the ability of emergency response vehicle access to areas surrounding the project shed. Since the IS/MND fails to identify the structure, the IS/MND also fails to identify its use and fails to consider the implications of this structure and its constraint on emergency access. Furthermore, the presence of this structure and its bearing on the project’s cumulative visual and other impacts must be considered in the environmental review.

An understanding of the project site’s actual existing conditions is necessary for any meaningful review of the project’s impacts. The IS/MND’s failure to even acknowledge the existence of the largest building on the property, let alone consider it in the impact analysis, warrants substantial revision to the IS/MND to adjust the analysis to account for this structure.

Sincerely,



Bob Delp
 Historic District Resident
 Folsom, CA 95630
 [REDACTED]

February 7, 2022

FIGURES

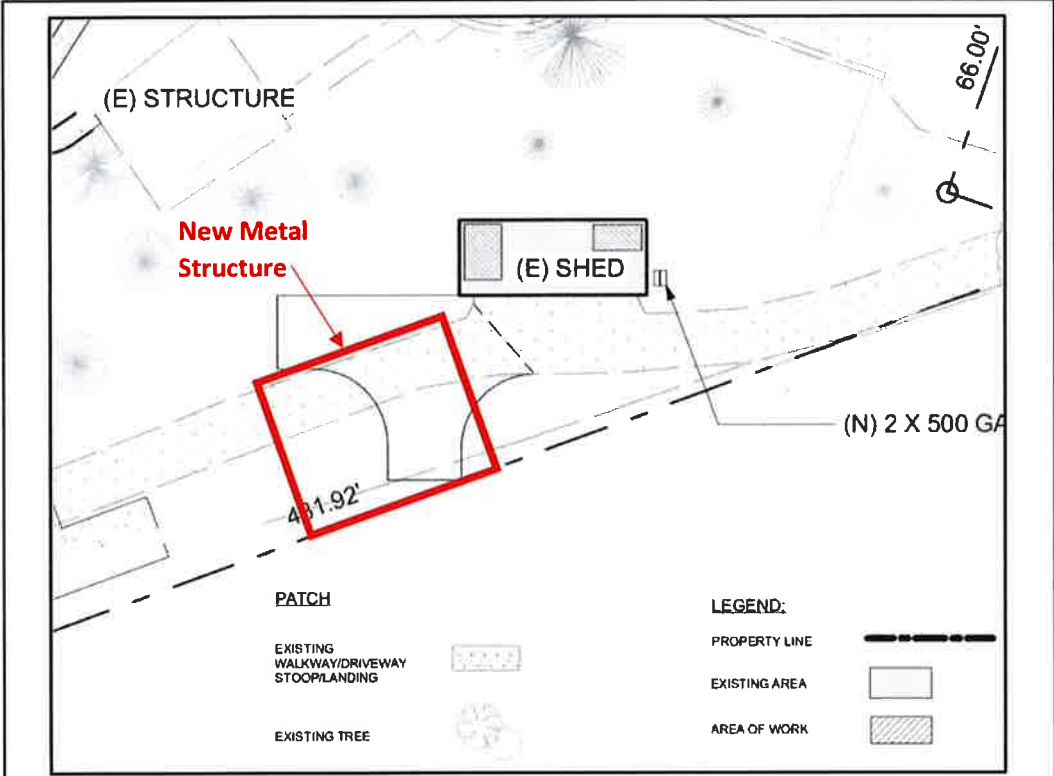


Figure 1. Excerpt of IS/MND Figure 3 Detailed Site Plan



Figure 2. Photograph of Existing Site Conditions

Josh Kinkade

From: Bob Delp [REDACTED]
Sent: Friday, September 24, 2021 10:21 AM
To: Pam Johns; Josh Kinkade
Cc: Sari Dierking; Elaine Andersen
Subject: Re: Lakeside Crematorium

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Pam and Josh:

I remain interested in receiving your input on the questions and information about the proposed crematorium requested in my emails below. The feedback I'm requesting is related to basic information about the project that is fundamental to your Department's review of the project, so it's unclear why you would not be able to easily provide it or post it to the Pending Development Applications webpage. (It is troubling, to say the least, that your Department maintains a webpage specifically named "Pending Development Applications" and yet for some reason the project application isn't posted and it seems you are unable to provide it upon request even months after an Initial Study was prepared and circulated.)

After reviewing the General Plan specifically to identify its intent for the Open Space land use designation, it is obvious that there is simply no basis for concluding that a crematorium is consistent with the General Plan Open Space land use designation. Your Department's interpretation (as presented in the April 2021 Initial Study) is incorrect and could have long-term damaging consequences regardless of the outcome of the currently proposed crematorium project. If you actually have a solid basis for your interpretation, not only would I and others in the community like to see that, but I would also request that you take that interpretation to the City Council for their concurrence in advance of further consideration of the current or any other individual cemetery or crematorium project.

You must be aware that the General Plan mentions "cemeteries" just one time (and crematoriums not at all), and that is in reference to Noise Compatibility Standards (Table SN-1) having nothing to do with land use designations. Table LU-5 of the General Plan (page 2-7) defines the Open Space designation as, "***The Open Space land use designation encompasses the preserved natural open space areas of Folsom.***" Throughout the General Plan, policies encourage development to incorporate areas of open space. No reasonable person can argue that the intent of those policies was that those open space areas could or might be used for siting a cemetery, let alone a crematorium. (Example: Policy LU 3.1.1 - "Encourage mixed-use development in nodes located at major intersections that include housing, open space, and offices." Are you prepared to argue that the expectation in that policy is that those open space areas would be eligible for siting a crematorium?)

There very well may be a need for one or more crematoriums in the Folsom area. When a particular type of land use hasn't previously been anticipated in the General Plan but the need or interest in that type land use is identified, I would think that as planners you would recognize that the City should undertake a process to assess whether that type of land use is appropriate for the City and, if so, at what location(s) in the City that type of land use might best be located. That should not be done on a individual project basis, but through a

more broad (City-wide) planning and General Plan (and perhaps also zoning code) amendment process that would engage the community and ultimately be decided by the City Council.

To avoid ramifications that go beyond a simple yes or no decision on the currently proposed crematorium, I am asking that your Department retract the April 2021 Initial Study and its incorrect interpretation of General Plan consistency. If your Department proceeds with a City-wide planning exercise to determine whether and where cemeteries and/or crematoriums should be eligible for siting, please include me in your public notices for that process.

Thank you,
-Bob Delp

BCC: Delp Interested Parties List

Bob Delp
916-██████████
██████████

From: Bob Delp ██████████
Sent: Tuesday, September 21, 2021 5:05 PM
To: Pam Johns <pjohns@folsom.ca.us>; Josh Kinkade <jkinkade@folsom.ca.us>
Cc: Sari Dierking <sdierking@folsom.ca.us>
Subject: Re: Lakeside Crematorium

Pam and Josh:

I am still interested in receiving your feedback on the questions in my email to you on Sept 14 (below).

Additionally, can you provide the existing CUP (approvals, allowable uses, conditions, etc.) for the cemetery?

Also, I have reviewed the General Plan and do not see *any* discussion or references in the City General Plan or zoning code that suggests an intent or expectation that a crematorium is an allowable use in conjunction with designated Open Space areas, and nothing in the General Plan to indicate that even cemeteries without crematoriums are an allowable use in the Open Space land use designation (and nothing in the General Plan EIR, from which the crematorium Initial Study tiers, to suggest that crematoriums were an assumed use in Open Space or anywhere else in the City in the General Plan EIR analysis). Can you provide an explanation of staff's basis for suggesting crematoriums are an allowable use in Open Space areas and also how you interpret development of a crematorium as being acceptable in Open Space that is within a SACOG transit priority area?

Your feedback on this would be most helpful.

Thank you,
-Bob

Bob Delp
916-██████████
██████████

From: Bob Delp
Sent: Tuesday, September 14, 2021 11:37 AM
To: Pam Johns <pjohns@folsom.ca.us>; Josh Kinkade <jkinkade@folsom.ca.us>
Cc: Sari Dierking <sdierking@folsom.ca.us>
Subject: Lakeside Crematorium

Hi, Pam and Josh.

I see the April 2021 Lakeside Memorial Lawn Crematorium draft IS/MND on the Pending Development Applications page, but no other information about the project is posted there. I have a few questions I'm hoping you can answer:

1. What date did the City deem the application complete?
- 2.
3. Are the project application materials available and can they be posted on the Pending Development Applications webpage?
- 4.
5. What is the status of the City's review of the project?

4. Are there any permit streamlining act deadlines that apply to this project?
- 5.
6. Was the IS/MND filed with the State Clearinghouse and did any agencies comment? (I don't see it on the CEQAnet database.)

Thanks in advance for any feedback you can provide.
-Bob

Bob
916- [REDACTED]
[REDACTED]

Josh Kinkade

From: Scott Johnson
Sent: Sunday, January 9, 2022 1:52 PM
To: Josh Kinkade
Subject: Fwd: Crematorium
Attachments: IMG_20211108_154501538.jpg; IMG_20210720_153825154_BURST000_COVER_TOP.jpg

Sent from my T-Mobile 5G Device

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From: breanne higgins [REDACTED]
Sent: Sunday, January 9, 2022 1:32:35 PM
To: Kelly Mullett <kmullett@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Scott Johnson <sjohnson@folsom.ca.us>
Subject: Crematorium

Some people who received this message don't often get email from breannehig@gmail.com. [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello everyone,

I'm am writing this email to express my concerns over the potential Lakeside Memorial Lawn Crematorium within the preserves neighborhood. We are still very much opposed to this proposal.

I love this small community I live in. These people aren't just neighbors, they are friends and family. My sister, brother in law, mom, dad, husband, one year old daughter, niece and nephew, live within this community. In addition, I am 6 months pregnant. I am writing this letter for them.

We moved to folsom last year to be all together. A close family's dream come true. We watch each other's children and do daily life together. As a mother I have learned, it takes a village and the neighboring village to raise a child. This community is my neighboring village. It's truly something special.

I have worked at Mercy Hospital of Folsom for the last 12 years as a nutritionist and my husband is a RN in the Emergency Department at Mercy Hospital of San Juan. We work for this community, even through the pandemic. We risk our health and the health of our family for the community. These past two year was incredibly difficult and we sacrificed a lot.

If this crematorium is put in, we would not only be exposing ourselves to health risks at work, but now at home. Where would our sanctuary be?

We are a working class community, with many public servants living here: healthcare workers, law enforcement, retired fireman and military, just to name a few.

Now is the time for the community to pay it forward. Please protect our neighborhood, my babies, my family and friends from this crematorium. Put it outside city limits, away from not only my community, but others as well. Put it in a designated area. Let's protect each other.

The bad air quality, it's the top concern. Contaminated air will be surrounding this neighborhood, causing potential health related issues to our children, elderly and vulnerable population. I worry about any emissions this crematory will expel into the air. Can my children's health really be guaranteed? Can you guarantee that? Should they have to live and play in a area where they watch bodies being burned?

"White" smoke in the sky, indicating they are in the process of burning a body. I don't want that for my children or their friends in the neighborhood.

This is a neighborhood that the kids play outside together. "Like the good old days" the kids ride bikes, go to the park, scooter around the block. They aren't inside on the computer, they don't have phones, they aren't playing video games. I'm very proud of that. Kids being kids - it's a beautiful thing. If this crematorium is put in, I worry about the kids being outside. Running around breathing in the contaminates or not being allowed to play outside during certain days/hours. What will the kids do?? My guess is go to electronics, which is disappointing.

As a neighborhood we are rallying together to try and stop this proposal from going any further. We were strong and will continue to fight for our friends and family.

I ask for your help in preventing this crematorium from coming into our neighborhood. I ask you to help protect my family and our health.

This crematory should not be in a or near a residential area. It should be in a industrial zone, not in open space. We were already lied to once by Igor. They have submitted an addendum to the proposal and have increased the daily rate from 400 lbs to 800 lbs a day of human remains. Will this continue to increase? What else will be adjusted?

I thank you for taking the time to read this. I have attached a picture of my baby Greta with her cousins. They are on their grandmother's front yard (please note the cemetery/potential crematory directly in the background) which is three houses down from mine.

[Show quoted text](#)

Josh Kinkade

From: Kelly Mullett
Sent: Wednesday, July 21, 2021 8:32 AM
To: Josh Kinkade
Subject: FW: Crematorium
Attachments: IMG_20210720_153825154_BURST000_COVER_TOP.jpg

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
 50 Natoma Street, Folsom, CA 95630
 O: 916.461.6231
 F: 916.355.7274



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FOLSOM
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www.folsom.ca.us

From: breanne higgins [REDACTED]
Sent: Tuesday, July 20, 2021 9:16 PM
To: Kelly Mullett <kmullett@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Scott Johnson <sjohnson@folsom.ca.us>
Subject: Crematorium

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I love this small community I live in. These people aren't just neighbors, they are friends and family. My sister, brother in law, mom, dad, husband, one year old daughter, niece and nephew, live within this community. I am writing this letter for them.

We moved to folsom last year to be all together. A close family's dream come true. We watch each other's children and do daily life together. As a new mother I have learned, it takes a village and the neighboring village to raise a child. This community is my neighboring village. It's truly something special.

I have worked at Mercy Hospital of Folsom for the last 12 years as a nutritionist and my husband is a RN in the Emergency Department at Mercy Hospital of San Juan. We work for this community, even through the pandemic. We

risk our health and the health of our family for the community. This past year was incredibly difficult and we sacrificed a lot.

If this crematorium is put in, we would not only be exposing ourselves to health risks at work, but now at home. Where would our sanctuary be?

We are a working class community, with many public servants living here: healthcare workers, law enforcement, retired fireman and military, just to name a few.

Now is the time for the community to pay it forward. Please protect our neighborhood, my baby, my family and friends from this crematorium. Put it outside city limits, away from not only my community, but others as well. Put it in a designated area. Let's protect each other.

The bad air quality, it's the top concern. Contaminated air will be surrounding this neighborhood, causing potential health related issues to our children, elderly and vulnerable population. I believe a third party report still needs to be done?

As I consider having a second child, I am fearful of being pregnant and breathing in the contaminated air. What would it do to my unborn baby? I would have to move. How could I take such a chance? Too risky.

This is a neighborhood that the kids play outside together. "Like the good old days" the kids ride bikes, go to the park, scooter around the block. They aren't inside on the computer, they don't have phones, they aren't playing video games. I'm very proud of that. Kids being kids - it's a beautiful thing. If this crematorium is put in, I worry about the kids being outside. Running around breathing in the contaminates or not being allowed to play outside during certain days/hours. What will the kids do?? My guess is go to electronics, which is disappointing.

I ask for your help in preventing this crematorium from coming into our neighborhood. I ask you to help protect my family and our health.

I thank you for taking the time to read this. I have attached a picture of my baby Greta with her cousins. They are on their grandmother's front yard (please note the cemetery directly in the background) which is three houses down from mine.

With much appreciation,
Breanne Higgins

Josh Kinkade

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Sent: Wednesday, July 21, 2021 8:32 AM
To: Josh Kinkade
Subject: FW: Crematorium
Attachments: IMG_20210720_153825154_BURST000_COVER_TOP.jpg

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



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www.folsom.ca.us

From: breanne higgins [REDACTED]
Sent: Tuesday, July 20, 2021 9:16 PM
To: Kelly Mullett <kmullett@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Scott Johnson <sjohnson@folsom.ca.us>
Subject: Crematorium

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With much appreciation,
Breanne Higgins



Josh Kinkade

From: Kelly Mullett
Sent: Tuesday, August 3, 2021 8:21 AM
To: Josh Kinkade
Subject: FW: Folsom Crematorium

FYI

Kelly Mullett*Administrative Assistant*

Community Development Department
 50 Natoma Street, Folsom, CA 95630
 O: 916.461.6231
 F: 916.355.7274



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From: Brian Paciotti [REDACTED]
Sent: Monday, August 2, 2021 6:32 PM
To: Kelly Mullett <kmullett@folsom.ca.us>
Subject: Folsom Crematorium

You don't often get email from bmpaciotti@gmail.com. [Learn why this is important](#)

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Dear Folsom Historic District,

I live with the Preserve neighborhood, and I recently learned from my neighbors that the Lakeside Memorial Lawn (owned by the Caring Service Group) has applied for a conditional use permit to install a crematory on the grounds.

I live at [REDACTED] Young Wo Circle. The region where the crematory will sit (currently a shed) is within about 400-500 feet from our home. My wife Dawn, and our twelve-year-old son Austin, have lived here for 12 years—we enjoy the lake, our wonderful neighbors, and the Folsom historical areas nearby. The Chinese cemetery in our neighborhood is an awesome tribute to the past Chinese immigrants. Unsurprisingly, we prefer our current situation, and we prefer to not smell or breath the output from the crematorium. Although our personal situation is obviously import to our family (me), the message of this letter concerns the greater public good of conserving history and ecological landscapes (us).

I understand the societal need for crematoriums—we need them. However, it makes more sense as a community to keep industrial operations separate from neighborhoods, historical regions, and precious ecological reserves. There are societies that have chosen a hands-off approach to regulation. In one large US city, I recall seeing a brothel, chemical plant, churches, and residences all in the same neighborhood. Folsom is not like this at all—we have a planned community that includes some of the best parks, bike paths, historical districts, and neighborhoods in the country. Thus, people move to Folsom and will continue to move here in the future due to our desirable planned community.

Yet what will residents think if they internalize the brand of “distinctive by nature”, come to enjoy our historical district, and then learn that the city allowed an unattractive industrial process to occur directly in the heart of the most precious area of the city?

I strongly believe that the Folsom Historical District and others need to work together to ensure that our city follows its brand—we are distinctive by nature. Moreover, we are distinctive by our historical treasures. Let us work together to live our distinctive and precious brand.

Best,

Brian Paciotti

Ph.D. Ecology, UC Davis. M.S. Healthcare Informatics, UC Davis Medical Center

██████████ Young Wo Circle

Folsom, CA 95630

January 30, 2022

Dear Folsom Historic District,

I live with the Preserve neighborhood, and I recently learned from my neighbors that the Lakeside Memorial Lawn (owned by the Caring Service Group) has applied for a conditional use permit to install a crematory on the grounds.

I live at [REDACTED] Young Wo Circle. The region where the crematory will sit (currently a shed) is within about 400-500 feet from our home. My wife Dawn, and our twelve-year-old son Austin, have lived here for 12 years—we enjoy the lake, our wonderful neighbors, and the Folsom historical areas nearby. The Chinese cemetery in our neighborhood is an awesome tribute to the past Chinese immigrants. Unsurprisingly, we prefer our current situation, and we prefer to not smell or breath the output from the crematorium. Although our personal situation is obviously import to our family (me), the message of this letter concerns the greater public good of conserving history and ecological landscapes (us).

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I strongly believe that the Folsom Historical District and others need to work together to ensure that our city follows its brand—we are distinctive by nature. Moreover, we are distinctive by our historical treasures. Let us work together to live our distinctive and precious brand.

Best,

Brian Paciotti

Ph.D. Ecology, UC Davis. M.S. Healthcare Informatics, UC Davis Medical Center

[REDACTED] Young Wo Circle

Folsom, CA 95630

From: Elaine Andersen
To: Josh Kinkade
Subject: FW: Crematorium
Date: Tuesday, February 1, 2022 9:45:51 AM

From: carol hart [REDACTED]
Sent: Tuesday, February 1, 2022 9:45 AM
To: kerri@atlanticcorrosionengineers.com; Elaine Andersen <eandersen@folsom.ca.us>; rrrodriguez@folsom.ca.us; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>
Subject: Crematorium

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Dear Members of the Folsom City Board,

I am a resident of Folsom Preserve. I live close to where the proposed crematorium would be built. I oppose this plan, because of the

the danger to our air quality. I am most concerned for our children, who play outdoors, and could be harmed by increased poor air

quality. This community strongly opposes the crematorium, and agrees that it should be built elsewhere. Certainly there are other

more open areas where this could be built...far away from communities. If you have not visited our community to see how close our

homes are to the cemetery, I would ask you to do so.

Thank you,

Carol Hart
[REDACTED] Young Wo Circle
Folsom, CA 95630



Central Valley Regional Water Quality Control Board

7 February 2022

Josh Kinkade
 City of Folsom
 50 Natoma Street
 Folsom, CA 95630
jkinkade@folsom.ca.us

COMMENTS TO REQUEST FOR REVIEW FOR THE MITIGATED NEGATIVE DECLARATION, LAKESIDE MEMORIAL LAWN CREMATORIUM PROJECT, SCH#2022010039, SACRAMENTO COUNTY

Pursuant to the State Clearinghouse's 5 January 2022 request, the Central Valley Regional Water Quality Control Board (Central Valley Water Board) has reviewed the *Request for Review for the Mitigated Negative Declaration* for the Lakeside Memorial Lawn Crematorium Project, located in Sacramento County.

Our agency is delegated with the responsibility of protecting the quality of surface and groundwaters of the state; therefore, our comments will address concerns surrounding those issues.

I. Regulatory Setting

Basin Plan

The Central Valley Water Board is required to formulate and adopt Basin Plans for all areas within the Central Valley region under Section 13240 of the Porter-Cologne Water Quality Control Act. Each Basin Plan must contain water quality objectives to ensure the reasonable protection of beneficial uses, as well as a program of implementation for achieving water quality objectives with the Basin Plans. Federal regulations require each state to adopt water quality standards to protect the public health or welfare, enhance the quality of water and serve the purposes of the Clean Water Act. In California, the beneficial uses, water quality objectives, and the Antidegradation Policy are the State's water quality standards. Water quality standards are also contained in the National Toxics Rule, 40 CFR Section 131.36, and the California Toxics Rule, 40 CFR Section 131.38.

The Basin Plan is subject to modification as necessary, considering applicable laws, policies, technologies, water quality conditions and priorities. The original Basin Plans were adopted in 1975, and have been updated and revised periodically as required, using Basin Plan amendments. Once the Central Valley Water Board has adopted a Basin Plan amendment in noticed public hearings, it must be approved by the State Water Resources Control Board (State Water Board), Office of

DENISE KADARA, ACTING CHAIR | PATRICK PULUPA, EXECUTIVE OFFICER

Lakeside Memorial Lawn
Crematorium Project
Sacramento County

- 2 -

7 February 2022

Administrative Law (OAL) and in some cases, the United States Environmental Protection Agency (USEPA). Basin Plan amendments only become effective after they have been approved by the OAL and in some cases, the USEPA. Every three (3) years, a review of the Basin Plan is completed that assesses the appropriateness of existing standards and evaluates and prioritizes Basin Planning issues. For more information on the *Water Quality Control Plan for the Sacramento and San Joaquin River Basins*, please visit our website:

http://www.waterboards.ca.gov/centralvalley/water_issues/basin_plans/

Antidegradation Considerations

All wastewater discharges must comply with the Antidegradation Policy (State Water Board Resolution 68-16) and the Antidegradation Implementation Policy contained in the Basin Plan. The Antidegradation Implementation Policy is available on page 74 at:

https://www.waterboards.ca.gov/centralvalley/water_issues/basin_plans/sacsjr_2018_05.pdf

In part it states:

Any discharge of waste to high quality waters must apply best practicable treatment or control not only to prevent a condition of pollution or nuisance from occurring, but also to maintain the highest water quality possible consistent with the maximum benefit to the people of the State.

This information must be presented as an analysis of the impacts and potential impacts of the discharge on water quality, as measured by background concentrations and applicable water quality objectives.

The antidegradation analysis is a mandatory element in the National Pollutant Discharge Elimination System and land discharge Waste Discharge Requirements (WDRs) permitting processes. The environmental review document should evaluate potential impacts to both surface and groundwater quality.

II. Permitting Requirements

Construction Storm Water General Permit

Dischargers whose project disturb one or more acres of soil or where projects disturb less than one acre but are part of a larger common plan of development that in total disturbs one or more acres, are required to obtain coverage under the General Permit for Storm Water Discharges Associated with Construction and Land Disturbance Activities (Construction General Permit), Construction General Permit Order No. 2009-0009-DWQ. Construction activity subject to this permit includes clearing, grading, grubbing, disturbances to the ground, such as stockpiling, or excavation, but does not include regular maintenance activities performed to restore the original line, grade, or capacity of the facility. The Construction General Permit requires the development and implementation of a Storm Water Pollution Prevention Plan (SWPPP). For more information on the Construction General Permit, visit the State Water Resources Control Board website at:

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http://www.waterboards.ca.gov/water_issues/programs/stormwater/constpermits.shtml

Phase I and II Municipal Separate Storm Sewer System (MS4) Permits¹

The Phase I and II MS4 permits require the Permittees reduce pollutants and runoff flows from new development and redevelopment using Best Management Practices (BMPs) to the maximum extent practicable (MEP). MS4 Permittees have their own development standards, also known as Low Impact Development (LID)/post-construction standards that include a hydromodification component. The MS4 permits also require specific design concepts for LID/post-construction BMPs in the early stages of a project during the entitlement and CEQA process and the development plan review process.

For more information on which Phase I MS4 Permit this project applies to, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/centralvalley/water_issues/storm_water/municipal_permits/

For more information on the Phase II MS4 permit and who it applies to, visit the State Water Resources Control Board at:

http://www.waterboards.ca.gov/water_issues/programs/stormwater/phase_ii_municipal.shtml

Industrial Storm Water General Permit

Storm water discharges associated with industrial sites must comply with the regulations contained in the Industrial Storm Water General Permit Order No. 2014-0057-DWQ. For more information on the Industrial Storm Water General Permit, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/centralvalley/water_issues/storm_water/industrial_general_permits/index.shtml

Clean Water Act Section 404 Permit

If the project will involve the discharge of dredged or fill material in navigable waters or wetlands, a permit pursuant to Section 404 of the Clean Water Act may be needed from the United States Army Corps of Engineers (USACE). If a Section 404 permit is required by the USACE, the Central Valley Water Board will review the permit application to ensure that discharge will not violate water quality standards. If the project requires surface water drainage realignment, the applicant is advised to contact the Department of Fish and Game for information on Streambed Alteration Permit requirements. If you have any questions regarding the Clean Water Act

¹ Municipal Permits = The Phase I Municipal Separate Storm Water System (MS4) Permit covers medium sized Municipalities (serving between 100,000 and 250,000 people) and large sized municipalities (serving over 250,000 people). The Phase II MS4 provides coverage for small municipalities, including non-traditional Small MS4s, which include military bases, public campuses, prisons and hospitals.

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Section 404 permits, please contact the Regulatory Division of the Sacramento District of USACE at (916) 557-5250.

Clean Water Act Section 401 Permit – Water Quality Certification

If an USACE permit (e.g., Non-Reporting Nationwide Permit, Nationwide Permit, Letter of Permission, Individual Permit, Regional General Permit, Programmatic General Permit), or any other federal permit (e.g., Section 10 of the Rivers and Harbors Act or Section 9 from the United States Coast Guard), is required for this project due to the disturbance of waters of the United States (such as streams and wetlands), then a Water Quality Certification must be obtained from the Central Valley Water Board prior to initiation of project activities. There are no waivers for 401 Water Quality Certifications. For more information on the Water Quality Certification, visit the Central Valley Water Board website at: https://www.waterboards.ca.gov/centralvalley/water_issues/water_quality_certification/

Waste Discharge Requirements – Discharges to Waters of the State

If USACE determines that only non-jurisdictional waters of the State (i.e., "non-federal" waters of the State) are present in the proposed project area, the proposed project may require a Waste Discharge Requirement (WDR) permit to be issued by Central Valley Water Board. Under the California Porter-Cologne Water Quality Control Act, discharges to all waters of the State, including all wetlands and other waters of the State including, but not limited to, isolated wetlands, are subject to State regulation. For more information on the Waste Discharges to Surface Water NPDES Program and WDR processes, visit the Central Valley Water Board website at: https://www.waterboards.ca.gov/centralvalley/water_issues/waste_to_surface_water/

Projects involving excavation or fill activities impacting less than 0.2 acre or 400 linear feet of non-jurisdictional waters of the state and projects involving dredging activities impacting less than 50 cubic yards of non-jurisdictional waters of the state may be eligible for coverage under the State Water Resources Control Board Water Quality Order No. 2004-0004-DWQ (General Order 2004-0004). For more information on the General Order 2004-0004, visit the State Water Resources Control Board website at:

https://www.waterboards.ca.gov/board_decisions/adopted_orders/water_quality/2004/wqo/wqo2004-0004.pdf

Dewatering Permit

If the proposed project includes construction or groundwater dewatering to be discharged to land, the proponent may apply for coverage under State Water Board General Water Quality Order (Low Threat General Order) 2003-0003 or the Central Valley Water Board's Waiver of Report of Waste Discharge and Waste Discharge Requirements (Low Threat Waiver) R5-2018-0085. Small temporary construction dewatering projects are projects that discharge groundwater to land from excavation activities or dewatering of underground utility vaults. Dischargers seeking coverage

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under the General Order or Waiver must file a Notice of Intent with the Central Valley Water Board prior to beginning discharge.

For more information regarding the Low Threat General Order and the application process, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/board_decisions/adopted_orders/water_quality/2003/wqo/wqo2003-0003.pdf

For more information regarding the Low Threat Waiver and the application process, visit the Central Valley Water Board website at:

https://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/waivers/r5-2018-0085.pdf

Limited Threat General NPDES Permit

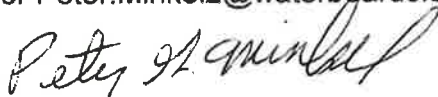
If the proposed project includes construction dewatering and it is necessary to discharge the groundwater to waters of the United States, the proposed project will require coverage under a National Pollutant Discharge Elimination System (NPDES) permit. Dewatering discharges are typically considered a low or limited threat to water quality and may be covered under the General Order for *Limited Threat Discharges to Surface Water* (Limited Threat General Order). A complete Notice of Intent must be submitted to the Central Valley Water Board to obtain coverage under the Limited Threat General Order. For more information regarding the Limited Threat General Order and the application process, visit the Central Valley Water Board website at:

https://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/general_orders/r5-2016-0076-01.pdf

NPDES Permit

If the proposed project discharges waste that could affect the quality of surface waters of the State, other than into a community sewer system, the proposed project will require coverage under a National Pollutant Discharge Elimination System (NPDES) permit. A complete Report of Waste Discharge must be submitted with the Central Valley Water Board to obtain a NPDES Permit. For more information regarding the NPDES Permit and the application process, visit the Central Valley Water Board website at: <https://www.waterboards.ca.gov/centralvalley/help/permit/>

If you have questions regarding these comments, please contact me at (916) 464-4684 or Peter.Minkel2@waterboards.ca.gov.



Peter G. Minkel
Engineering Geologist

cc: State Clearinghouse unit, Governor's Office of Planning and Research,
Sacramento



**CHINESE
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AMERICA**

Chinese Historical Society of America
965 Clay Street
San Francisco, CA 94108
Phone: (415) 391-1188 x101
Fax: (415) 391-1150
Email: info@chsa.org

September 1, 2021

Historic District Commissioners
City of Folsom
50 Natoma Street
Folsom, CA 95630
Memorial Lawn Crematorium,

RE: Lakeside

Project Number: PN 19-182

Dear Historic District Commissioners:

Thank you for giving the Chinese Historical Society of America (CHSA) the opportunity to comment on the above-proposed project. CHSA is a non-profit organization the Mission of which is to *“collect, preserve, and illuminate the history of Chinese in America by serving as a center for research, scholarship and learning to inspire a greater appreciation for, and knowledge of, their collective experience through exhibitions, public programs, and any other means for reaching the widest audience.”* CHSA is the oldest Chinese historical society in America and maintains a museum concerning Chinese history and culture in San Francisco. By way of this letter, CHSA wishes to voice its strong opposition to the Lakeside Memorial Lawn Crematorium Project.

Our position on the Helix Initial Study (IS) and Mitigated Negative Declaration (MND), adopted by the City of Folsom, is that the IS and MND do not adequately consider the planning, implementation and operation of the Crematorium Project. For example, the following cultural resources were not identified or evaluated: that the Chung Wah Cemetery has been placed on the National Register and California State Register of Historic Places; that the Young Wo Chinese Cemetery has been placed on the California Registry of Historic Points of Interest; and that, in addition, both Cemeteries have been being placed on the Sacramento County Cemetery Commission's Registry of Pioneer Cemeteries. The integrity of these culturally unique cemeteries are of historical and archaeological importance to a specific time in history: the building of Folsom, the establishment of the State of California, and the opening up of the West.

Also, the Helix IS/MND does not discuss the indirect social changes the crematorium's operation would have on people's perception of spiritual worship in the Chung Wah and Young Wo Cemeteries, resulting in an unfortunate modern misinterpretation of historic spiritual and religious practices.

But fortunately, Folsom's Chinese community is well aware of this issue. For example,



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on September 27, 2002 Ms. June Chan submitted written public comment on an earlier Crematorium Project application made by the Miller Funeral Home, a prior owner of the Lakeside Memorial Lawn Cemetery. (Her letter appears on page 68 of the 01.15.03 Folsom Historic District Commission Staff Report.)

Ms. Chan expressed fears that the crematorium's operational impact could cause a social change in the perception of the Chung Wah and Young Wo Cemeteries. Ms. Chan's letter questioned how the crematorium's operations could be reconciled alongside the Chung Wah Cemetery in order to ensure the Chung Wah's historical and archaeological integrity, "Because of cremated ashes in the air – either from the crematorium chimney; when ashes are put/poured into a container; through clean-up of ash residue waste disposal or cleaning up of interior crematorium – the potential impact over time would be significant to both Chung Wah Memorial Cemetery and Young Wo Memorial Cemetery. Remnant ashes that are at the crematorium will be in the air and with the winds will have to land someplace such as the Chinese burial mounds."

In her letter Ms. Chan asserts that any disturbance by the proposed Crematorium's operations (ashes, smoke, or the thought of ashes and smoke) on her ancestors graves would affect her, and other Chinese descendants, in a very spiritual way. "These circa Chinese Gold Rush cemeteries for their descendants today, continue to be held in solemn spiritual reverence and profound sacred esteem for those inter(n)red eternally." Local historians estimate that there are between 250-600 Chinese pioneers buried at the Chung Wah Cemetery.

The crematorium applicant has applied for a Conditional Use Permit (CUP) , to build and operate a crematorium in a Conservation/Open Space zone. However, cremation was not consistent with Chinese funerary customs and practice. Also, and as noted in the Findings of the old 01.15.03 Folsom Historic District Commission Staff Report at page 4, cremation was not an integral part of Gold Rush pioneer cemetery customs, either. Still further, and as also noted in that 01.15.03 Staff Report, a crematorium is not aligned with the Folsom Historic District Design and Development Guidelines, or with Folsom's General Plan Conservation/Open Space zoning.

On December 5, 1988 CHSA participated, along with Ms. June Chan, in the City of Folsom's dedication of the Young Wo Cemetery. A bronze marker at the entrance to the Young Wo Cemetery reads in part:

"The Young Wo Cemetery 1883-1925 ...Buried here are Chinese pioneers who struggled for economic survival and human dignity and in so doing helped build Folsom and the West."

Thank you for considering CHSA's comments. For the reasons stated above, CHSA believes that the 2021 Lakeside Memorial Lawn Crematorium Project should be denied and



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request that this letter be entered into the Public Comment record for consideration by the Historic District Commission and any other public bodies which may consider this matter.

Very Truly Yours,

Justin Hoover
Executive Director
Chinese Historical Society of America

Cc: Mr. Josh Kincade, Project Planner jkinkade@folsom.ca.us
Mr. Steve Banks, Principal Planner sbanks@folsom.ca.us
Mr. **Scott Johnson**, Planning Manager sjohnson@folsom.ca.us

Josh Kinkade

From: Scott Johnson
Sent: Monday, February 7, 2022 6:50 PM
To: Josh Kinkade
Subject: Fwd: Proposal for crematorium in Folsom

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From: Christine Holmes [REDACTED]
Sent: Monday, February 7, 2022 6:48:29 PM
To: Christine Holmes [REDACTED]
Cc: kerri@atlanticcorrosionengineers.com <kerri@atlanticcorrosionengineers.com>; Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; kcolepolicy@gmail.com <kcolepolicy@gmail.com>; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; justin@revolutionsdocs.com <justin@revolutionsdocs.com>; danwestmit@yahoo.com <danwestmit@yahoo.com>; ankhelyi@comcast.net <ankhelyi@comcast.net>; johnfelts@e55tech.com <johnfelts@e55tech.com>; Elaine Andersen <eandersen@folsom.ca.us>
Subject: Re: Proposal for crematorium in Folsom

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Regarding the metals and pollutants I referenced:

In addition to harmless compounds such as water vapor, emissions include carbon dioxide, carbon monoxide, nitrogen oxide, sulphur dioxide, hydrogen chloride gas, hydrogen fluoride, mercury vapour . Organic compounds such as benzenes, furans, acetone are also emitted and these react with the hydrogen chloride and hydrogen fluoride under combustion conditions to form polychlorinated dibenzodioxins (PCDDs) and polychlorinated dibenzofurans (PCDFs) both of which are carcinogens. **A study by the Cremation Association of North America has found that filtering crematorium fumes has little effect on the toxins released.**

Sent from iCloud

On Feb 7, 2022, at 6:40 PM, Christine Holmes [REDACTED] wrote:

Dear City Planning Officials and Council Members:

I am adamantly opposed to the crematorium proposed by Lakeside Memorial. I bike and hike that area frequently, and I also enjoy visiting historic downtown Folsom, sitting outside and having a lovely meal and beverage. A facility such as is proposed would certainly negatively affect my enjoyment of my city and the

very thought of traces of metals and chemical pollutants' effects on Lake Natoma and the American River are horrifying and sad. Our waterways are already so threatened and struggling. But there are so many more reasons not to allow this to go forward.

A CREMATORIUM WOULD BE A NONCONFORMING USE OF ZONED OPEN SPACE, DESIGNATED HISTORICAL SITES.

The proposed crematorium would be installed and operated in designated open space that is **not zoned for commercial, industrial use, within 600 feet of residential homes**. By definition open space zoning districts **maintain community open space resources for purposes ranging from conservation, to preserving community land use options**. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the zoned land. This proposed industrial function will be conducted on and next to nationally registered historical sites and has disregarded the National Chinese Historical Society's history with the grounds. This effort will harm the living to profit off of the dead.

AIR QUALITY, SMELL, PARTICULATE MATTER, FIRE RISK WILL MAKE HISTORY.

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, increased fire risk, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. According to the Sacramento County's Air Quality Management District, the crematorium would be self-regulated, with **the County having very little oversight no oversight of the mercury-based emissions**. Visually, the metal shed that will be used for cremation, the resulting smoke and heat waves are incredibly **out of character** for the Historic District. Each individual's senses are different and scent may be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy. The increased fire risk created by the proposed 500 gallon propane tanks puts hundreds of residents who rely on a singular evacuation point in grave danger.

TOXICITY LEVELS WILL BE UNSAFE FOR CHILDREN, ELDERLY AND THE VULNERABLE.

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Our local experts have warned that such toxins become the **most dangerous when vaporized**. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies **only** to average, healthy adults. The Study **fails** to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community. Particulate matter (PM) identified in the study was based on measures taken at the Executive Airport in Sacramento--not in Folsom. As machinery ages, particulate matter released can quickly become "very unhealthy" by air quality standards and pollution at that

level calls for limited and short outdoor activities. There has not been a full Environmental Impact Study (EIR) conducted to date to inform the conditions to be considered. There are serious concerns about the daily impact on air quality in the Historic District posing a serious threat to those with heart and lung conditions, unborn and developing children, teens, and older adults. Shifting wind patterns will impact Sutter Street visitors and business, and recreational areas along Natoma Lake and the surrounding trails. Several hundred permanent residents including young children, veterans, and the elderly will have their health jeopardized.

PLEASE PROTECT HISTORIC FOLSOM. PROTECT OUR BEAUTIFUL TRAILS. PROTECT THE LAKE AND RIVER. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,
Christine Holmes
[REDACTED] BONHILL DR.
Folsom CA
[REDACTED]

Sent from iCloud

Josh Kinkade

From: Steven Banks
Sent: Thursday, August 19, 2021 8:18 AM
To: Josh Kinkade
Subject: FW: Public comment regarding proposed permit for crematorium.

FYI

From: [REDACTED]
Sent: Thursday, August 19, 2021 8:15 AM
To: Steven Banks <sbanks@folsom.ca.us>; daron_bracht@ifolsom.com; kcole@folsom.ca.us; kdewel@folsom.ca.gov; dwest@folsom.ca.gov; MAnkhelyi@folsom.ca.gov; jfelts@folsom.ca.gov; MDascallos@folsom.ca.gov; [REDACTED]
Subject: Public comment regarding proposed permit for crematorium.

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Cindy Baker
 1342 Young Wo Circle
 Folsom

RE: Opposition to Proposed Crematorium in Lakeside Cemetery
 July 28, 2021

Dear Folsom Historic District Commissioners,

I'm very opposed to the plans for a crematorium being proposed on the Lakeside Cemetery property. I respect the Miller Funeral Home, although I am not familiar with the Caring Services Group chain that bought out the Claneys. Millers has buried four generations of my family and I am entrusting them to bury me as well. They take wonderful care of the cemetery grounds, one of the oldest and most beautiful historic cemeteries in northern California. Their mausoleum additions have been well executed and are a pleasure to visit. I understand the financial motivation to build a crematorium on the cemetery site, land they already own, to create a permanent income stream. However, it creates an undue burden on the people living and recreating nearby. Emissions and odors will impact our fresh air and property values will be negatively impacted.

I live just a few hundred yards from their proposed site and their project description of operations indicate they would be conducting cremations nearly every day of the year with multiple cremations daily. My house is inundated by the odor when the Kikkoman plant (which is much farther away) brews soy beans. The crematorium application exhibit showing the detection of emissions within the neighborhood, the cemetery and across Folsom Boulevard is disturbing. Their permit application was denied in recent years and all the reasoning at that time remains relevant.

Other crematoriums in the region do emit noticeable odors and emissions. I spoke recently with a retired Sacramento City Fire Department employee who told me that when the crematorium on Stockton

Boulevard incinerates a particularly obese person, they get complaint calls due to odor as a column of dark smoke rises out of the stack. As disgusting as that sounds, this would be the reality we would be forced to live with. It would be an undue burden for residents to have to breath the output nearly every day of the year. I walk daily in the neighborhood and often entertain outside. I certainly don't want to lose the enjoyment of fresh air I currently enjoy. Additionally, its presence would have to be disclosed on any sales agreement, deterring buyers and reducing property values. Even visitors to the cemetery would be exposed to the output, including myself. I frequently visit five family members buried at Lakeside, visits that would become unpleasant if there are emissions blowing where the applicant's detection maps indicate.

I think it's critical that Millers Funeral Home find a more appropriate location in an area zoned for industrial or commercial use. I believe building a crematorium within yards of homes, the historic cemeteries (Lakeside and Chung Wah) and Lake Natoma State Park space is inappropriate and has too negative an impact on local residents. This project benefits only the funeral home, while negatively impacting the community.

Sincerely,
Cindy Baker

Cindy Baker

Josh Kinkade

From: Scott Johnson
Sent: Sunday, January 30, 2022 1:19 PM
To: Josh Kinkade
Subject: Fwd: HDC Public Hearing 2/16/22 - Public Comment re: Crematorium

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From: Cindy Pharis [REDACTED]
Sent: Sunday, January 30, 2022 1:17:44 PM
To: Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; kcolepolicy@gmail.com <kcolepolicy@gmail.com>; justin@revolutionsdocs.com <justin@revolutionsdocs.com>; danwestmit@yahoo.com <danwestmit@yahoo.com>; ankhelyi@comcast.net <ankhelyi@comcast.net>; johnfelts@e55tech.com <johnfelts@e55tech.com>; m.dascallos@yahoo.com <m.dascallos@yahoo.com>
Subject: HDC Public Hearing 2/16/22 - Public Comment re: Crematorium

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I oppose the proposed crematorium at the Lakeside Memorial Cemetery. I would appreciate that my comments below be read into the record during public comment:

As a longtime resident of the Folsom Historic District, I would like to express my concerns and opposition regarding the proposed crematorium at the Lakeside Memorial Cemetery. The homeowners who live within feet of the Lakeside Memorial Cemetery are actively voicing their concerns and rallying together against this proposal, I am listening to their concerns and so should you! If a crematorium were proposed in your backyard I'm sure that you would oppose it too. There are too many valid concerns regarding air quality, increased traffic, noise, nonconforming use of land and zoning, and long term affects to residents including children and the elderly.

A crematorium at this location will add more traffic to the only street that provides ingress and egress to the adjoining neighborhood and access to Folsom Blvd. This location is already impacted by the many people who enjoy the trails along Lake Natoma. The future growth of the Corporation Yard's existing property is also a concern that should be considered before adding more growth, noise, and pollution to an already overburdened residential community that lies within feet of this proposed crematorium.

Please consider the residents who are loyal, tax paying citizens who deserve the respect from our City representatives. We rely on you to hear our concerns and to be our voices when making the right decisions that affect our lives, our future and our property here in Folsom. Please vote "No" on the crematorium.

Cindy Pharis
 Folsom Historic District Resident
 HFRA Board Member

Josh Kinkade

From: Scott Johnson
Sent: Friday, January 14, 2022 2:45 PM
To: Josh Kinkade
Subject: Fwd: Lakeside Memorial Lawn crematory

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From: Daniel Winkelman [REDACTED]
Sent: Friday, January 14, 2022 2:35:05 PM
To: Scott Johnson <sjohnson@folsom.ca.us>
Subject: Lakeside Memorial Lawn crematory

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Folsom Historic District Commissioners

I implore you to deny approval of the proposed Lakeside Memorial Lawn crematory.


My two concerns:

I live directly adjacent to the cemetery and have grave concerns about what effect fumes from the crematory will have on my health. Burning 800 lbs of human remains each day in a residential neighborhood is deplorable. No matter what claims of air filtration for the crematory, some undetected harmful chemical is bound to be passed into the air and on to my property

Two large capacity propane tanks are proposed for the project. They will be placed in an urban/wilderness interface area vulnerable to high heat fires. The tanks would be in constant threat of a BLEVE fire. (boiling liquid expanding vapor explosion). These propane tank fires have an explosive force capable of hurling a car into our neighborhood.

Please vote to stop this project,

Daniel Winkelman

 Young Wo Circle

Folsom, CA 95630

From: Elaine Andersen
To: Josh Kinkade
Subject: FW: Proposed crematorium at Lakeside Cemetery
Date: Monday, February 7, 2022 7:15:36 AM

-----Original Message-----

From: Daniel Shively [REDACTED] >
Sent: Sunday, February 6, 2022 5:19 PM
To: Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; kcolepolicy@folsom.ca.us; justin@revolutionsdocs.com; danwestmit@yahoo.com; ankhelyi@comcast.net; johnfelts@e55tech.com; m.dascallos@yahoo.com; Elaine Andersen <eandersen@folsom.ca.us>; kerry@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; yks@folsom.ca.us; Mike Kozlowski <mkozlowski@folsom.ca.us>
Subject: Proposed crematorium at Lakeside Cemetery

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Dear Sirs and Madams,

As a resident of Folsom and particularly the Preserve development, we strongly urge that you not approve the crematorium permit for the Lakeside Cemetery. We are less than a 1000 feet from the proposed crematorium and many neighbors in and out of our development are even closer. We have no problems with the idea of cremation; but this facility does not belong in a residential neighborhood. It should be placed in an industrial area away from houses and families. Many people, for various reasons, are upset at this prospect and I hope you will consider this when voting on the project. We don't want it here!

Sincerely,
Dann and Shari Shively
Young Wo Circle

January 11, 2022

Historic District Commissioner

Re: Lakeside Crematorium

As you are aware, The hearing for the Proposed Crematorium is February 16, 2022. You are also aware that this is a sensitive issue for the Historic District, especially the Preserve/Natoma Shores Neighborhood.

The Preserve Neighborhood is a community bound together by strong relationships and common interests. This proposed crematorium has in the last year and a half put our small community on edge and diminished our quality of life.

If you haven't been in the neighborhood for awhile I suggest you come and visit. Walk around and reacquaint yourself with the Lakeside Cemetery, Chung Wah Cemetery, Young Wo cemetery, Dredger Diggings Preserve, Veterans Hall, Murer House, Lake Natomas, our small park and of course, the Residents.

I am available anytime to act as your tour guide if you so choose.

Thanks for your time

Dave Higgins

██████████ Fong St.
████████████████████

February 11, 2022

City Council Members
City Manager
Historic District Commissioners

I have read Mr. Semenyuk's " **Scope of work - Installing a Crematory**" document from the Historic Folsom Residence Association (HFRA) email I received back in December 2020. This document was not dated however it was signed by Mr. Igor Semenyuk. I also participated in a HFRA Zoom meeting on July 15, 2021 with Igor Semenyuk of Caring Service Group and Peter Hartwick from Hartwick Combustion Technologies. They informed us about "how wonderful it would be for Folsom to have it's own Crematorium."

Mr. Semenyuk talked about the need and the service he could render. He stated in the 12-2020 document;

" There are several large cultural communities residing and moving into Folsom. Regretfully, we do not have the ability nor the capability to serve the Sikh, Hindu, Buddhist or other cremation based cultures as they require an onsite crematory so they may be able to exercise their rights and customs. Currently, there is no crematory in the City of Folsom, so they must find these services outside their city."

After reviewing the revised CEQA application on page 81 "Parking and Access" third paragraph, it states:

"As the crematorium would not be located in or near a funeral home and would be separate from any funeral services or public gatherings provided by the applicant. Access would only need to accommodate a small number of staff members with business at the site"

In the CEQA document, section 4.0, Project Objectives, stated again is "Cultural and Religious Communities as one of four reasons to have a crematorium. Of the four objectives stated, the only one true and honest is the statement " **Upgrade existing facilities to capitalize on a business opportunity that has proven successful for the applicant elsewhere in California**"

Mr. Semenyuk and Caring Services Group will say whatever they think is necessary to get their crematorium approved and built.

If the above quote was true I would like to have Mr Semenyuk identify these large cultural groups so I would have a better understanding of their rights and customs. I have contacted the Consumer Affairs office in Sacramento and was informed in Sacramento County there are 16 crematories between downtown Sacramento and Lakeside Memorial Lawn. In addition there is 1 in Rocklin and 1 in Placerville. There are many crematories close by to serve the community. The cultural and religious requirements that Mr. Semenyuk states can be fulfilled at Mt. Vernon Cemetery in Fair Oaks 5.2 miles away. Mt. Vernon Cemetery advertises a cremation viewing suite.

There seems to be plenty of incinerators to fulfill this perceived regional need. Currently there are, 70.12% white, 17.18% asian, 5.89% two or more races, 3.41% african american living in Folsom. In my opinion, the cultural and religious community that would use a ritual cremation is comparably low compared to the combined population of Folsom. This is a marketing tactic and disingenuous on Mr. Semenyuk's and Care Service Group part.

In my notes from the Zoom meeting, Mr Semenyuk said "You will see a puff of white smoke in the beginning then heat waves". Peter Hartwick later in the Zoom meeting said "You will not see or smell anything" In the document Mr. Semenyuk wrote "there is no smoke or residue that leaves the chamber". I'm confused, which is it?? What am I going to see? Smoke, heat waves or what smell? Mr Semenyuk wants to install an incinerator. Mr Hartwick wants to sell an incinerator. It's all Business, it's about money. It's smoke and mirrors

Also in the Scope of Work document, Mr. Semenyuk states "they are designed to use less fuel, have lower NOx emissions, are NFPA compliant, meet Air Quality standards, and the components are UL listed." So is my toaster and BBQ but it still has puffs of smoke, creates heat waves, and smells. The question is by who and how it is tested. By the manufacturer or an outside entity? How often is the incinerator operationally inspected? Does it have wet scrubbers or just filters? If it has filters are the filters cleaned or are they thrown away? How are they disposed of?

In addition, the document states:

"In the event of an operational failure, the gas is immediately and automatically shut off. There is a manual switch as well. There has never been a crematory blowing up in the history of cremation".

That is not true. There have been dozens of reported explosions, mostly from pacemakers, within the incinerator causing damage to the Incinerators bricks, doors, and other components.

Is an explosion within the incinerator reported to the Fire Department or some other governmental agency? Will there be an inspection of the facilities after such an event by qualified service personnel or governmental agencies? Will there be a trained operator onsite during the entire cremation process? Someone to hit the manual switch when something goes wrong? What happens during a power outage or Flex Alert? Will they incinerate on "Spare the Air Days", "Red Flag" days, or our smoke filled summer wildfire days? Lots of questions and no answers.

Caring Service Group is **profit driven**, as most businesses, Their excitement for " City of Folsom will have their first Crematory" is dangerous and disingenuous. They are looking for that **competitive advantage** over their competition versus the health and well being of the community. Caring Service Group admits its "**Primary goal is to purchase funeral home business**" on their website. It's just Business.

The original application was for incinerating 200 to 400 lbs of human remains per day. Now it's 400 lbs to 800 lbs per day. Is that 7 days a week? When are they going to request a second incinerator to increase their cash flow. This project has already affected our neighborhood quality of life. This is no small town operation. This is a full fledged commercial operation that needs to be placed in a commercial setting. Remember, this land is zoned open space. Give us little guys a break.

A crematorium proposal was submitted back in 2002 by Lorin Chaney, then owner of Lakeview Memorial Cemetery and was denied by the Historic Commission on 1-15-2003. What has changed since then? It is still proposed in the same old meal shed, Crematoriums still malfunction, still have operator error, still smoke, still emit nasty odor. This project is still located in the same Zoned Open Space. We now have more residents including infants, young children and many seniors. Am I missing something? This is obviously wrong.

So, does Folsom really need an industrial **incinerator** in the **Historic District**, In a **Zoned "Open Space" designated area**, in a **historic residential neighborhood**, in a documented **"Wildland High Fire Risk"** area, near a **State Recreational Area**, near a **National Registered Historic Site** (China Mission-Chung Wuh Cemetery).

Caring Service Group and Miller Funeral Home think it would be wonderful. Really????

Thank You for your time.

David Higgins
[REDACTED] Fong St.

December 28, 2020

My Name Is Dave Higgins. I am a member of HFRA. I live near Lakeside Memorial Lawn. I have some questions and concerns about the proposed Crematory at the Lakeside Memorial lawn.

I have read the proposal submitted to the City of Folsom by Igor Semenyuk.

My first comment comes from their "Point at Issue".

The proposal states Folsom has an estimated 450 deaths per year and an estimated 300 cremations need to be performed. That would be .82 cremations a day, present time. As the document states, "including Folsom Ranch, we need to have an on-site crematory in order to facilitate it's increase. This does not take into consideration neighboring cities and their demand for cremation services. The 300 number is not real. I foresee the facility, in time, being a very busy place processing several remains a day, increasing traffic demands ie. funeral vehicles, propane delivery trucks, service trucks, funeral patrons.

Second comment comes from their "Our Proposition"

Lakeside Memorial Lawn is near residences, 117 homes in the "Preserves Neighborhood. Some in direct view of the Lakeside Memorial Lawn. This place is not that removed from residences, as stated. In addition, there are two parks, FLSRA and the playground on Sutter St.

Also, within the section, it states Lakeside Lawn does not have a gas or sewer line. The proposal states they will be installing two propane tanks. Where, how big, how often serviced by delivery trucks? I question the no need for a sewer connection or some other sort of liquid disposal.

Third comment comes from their "Cremation Process"

I am concerned the "Shed" might not be large enough to facilitate the process. I believe the shed currently houses equipment like the backhoe, garden tractor, mowers, trailers, hand tools, and chemicals used in the care of the grounds. Is there room for all? Is another building planned?

Will the remains be transferred outside the shed from the vehicle to the crematory oven in public view or within the shed, protected from public view?

Fourth comment comes from their "Scope of Work"

There is no mention of the height of the "stack".

Living in the "Preserves" neighborhood is special. Great neighbors and community. Proximity to the treasures of Folsom. Lake Natomas, The Hike and Bike Trails, Historic Downtown Folsom, Muir House, Chinese Memorial Cemetery, Veterans Hall, and Sutter St. Play Park. They are wonderful places.

However, with all these wonderful things comes a down side. We have a lot of traffic. Muir house on weekends, FLSRA access point everyday, Veterans and delivery trucks to the Veterans Hall, Occasional City of Folsom vehicles accessing the back gate to the Crop Yard. Not to mention the ever present UPS, FedEx, and Amazon Delivery Vans.

So, It is my opinion, a Crematory at Lakeside Lawn would provide a service for the community. However, it needs to address and satisfy the concerns of the Preserves Community. If the proposal is approved, limit the number of daily cremations, limit the cremation to weekdays only. Have the proposed activity and operations away from public view. Evaluate the location and size of the shed and propane tanks. The Preserve neighborhood is basically a court, one way in one way out.

On a selfish note, I believe this could hurt our home values.

Please consider and weigh all aspects of this project. Once it's approved there is no return.

Thank You for your time.

Dave Higgins
[REDACTED]

July 6, 2021

To: Historic District Commission
Folsom Planning Commission

My name is David Higgins. I live at [REDACTED] Fong St, directly across the street from Lakeside Memorial Lawn Cemetery. I am opposed to having a crematory placed in my neighborhood. A crematorium should be zoned for commercial - industrial areas NOT residential areas.

I have seen the proposal submitted by Igor Semenyuk and participated in the Zoom meeting hosted by HFRA for a crematorium at Lakeside Memorial Lawn. I have several concerns about the Crematorium.

Igor Semenyuk stated the crematory would probably operate 5 to 7 times a week. I assume this would be the initial start up number. I am concerned what the actual cremation number would be for the future. 2,3,4,5 a day? 5-7 days a week?

In the initial proposal submitted and in the Zoom meeting there was no mention of any noise. I would like to know if I am going to be subjected to a constant hum or roar from the machine.

How much propane is used during a cremation process and how often will I see propane trucks servicing the tanks and smelling the propane gas when refilling.

I won't be looking forward to the increase in traffic from the vehicles delivering the bodies, service trucks, etc. Since we have only one access point in and out of this residential area, we already live with a large amount of traffic and congestion from light rail.

Igor Semenyuk stated the only smoke emitted is at the beginning of the process when a "puff of white vapor is released." After researching crematoriums online I saw some disturbing videos of "white vapor" being exhausted out of crematorium stacks. Please see the links enclosed.

<https://www.palmbeachpost.com/article/20150716/NEWS/812066867>

https://www.weatherforddemocrat.com/opinion/letters_to_the_editor/the-crematorium-next-to-my-home/article_71e6f13d-ddaf-5f7c-a7fc-e057b98a1efc.html

<https://www.ncbi.nlm.nih.gov/pmc/articles/PMC1279940/>

https://mountainx.com/news/community-news/crematorium_smoke_triggers_complaints/

• OKC Crematorium Operating at Night "Dusting" Residential Neighborhoods

• Crematorium fires up in NE Portland

• Scottsdale neighborhood concerned about increased cremation smoke

<https://www.latimes.com/archives/la-xpm-2007-dec-26-na-cremate26-story.html>

Will there always be an operator on site during the process to shutdown the machine if a malfunction arises?

In researching who regulates and inspects crematoriums the information is rather vague. The Federal Trade Commission regulates the funeral industry, however, it does not oversee crematoriums. Nobody seems to. It appears they are under the radar. So, is the fox guarding the hen house? Do we just trust they are going to do the right thing? I don't think so. The moment it is installed, it's there forever.

Speculation arises from the air quality. Can't find a definitive answer here either. Mercury from teething fillings, chemicals from embalmed bodies, medical prosthesis (pace makers), chemotherapy and radiation by-products from cancer patients. Are any of these materials released into the air or are they consumed 100% by the fire.

My home is approximately 575 feet from the proposed crematorium. Other homes are as close as 450 feet. Not very far. I suspect the smoke stack will be visible from my porch. I believe some of my neighbors will also have a view of the smoke stack.

Lakeside Memorial Lawn also shares this small neighborhood with the Chinese Cemetery, Veterans Hall, Muir House, access to the State Rec area and a quaint park and playground. It is my opinion all these facilities will be impacted. I also believe the crematorium will affect our property values.

Please don't allow the crematorium in our residential area. It belongs in a commercial-Industrial area. Would you like it in your neighborhood?

Thank You for your time,

David Higgins

Josh Kinkade

From: Elaine Andersen
Sent: Sunday, August 8, 2021 11:36 AM
To: Josh Kinkade
Subject: FW: No to the Crematorium

From: david higgins [REDACTED]
Sent: Sunday, August 8, 2021 8:55 AM
To: daronbr@pacbell.net; danwestmit@yahoo.com; ankhelyi@comcast.net; kcolepolicy@gmail.com; kevin.duewel@gmail.com; m.dascallos@yahoo.com; johnfelts@e55tech.com; Scott Johnson <sjohnson@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Elaine Andersen <eandersen@folsom.ca.us>
Subject: No to the Crematorium

Some people who received this message don't often get email from dh441568@gmail.com. [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

August 8, 2021

Folsom Historic Commission
Folsom Planning Department

My name is Dave Higgins, I am a retired Fire Captain from the City of Fremont, Ca. with over 30 years of experience, 10 of which I was a member of the HazMat Response Team. I live next to Lakeside Memorial Lawn where the proposed crematorium is planned. I am writing to express my concerns.

My concern centers around the two 250 gallon propane tanks proposed. This proposed facility is 30 feet from the Open Space/Wildland Zoned part of Folsom. 600 feet from my house. Should these tanks be exposed to fire, physical damage, or vandalism this neighborhood would be in serious trouble. Protection of the tanks, in my opinion, would be difficult.

There is a fire hydrant on the street at the entrance to Lakeside Memorial Lawn. The distance from the hydrant to the old shed is approximately 650 feet. The driveway, one way in and out is narrow, however, it could support two pieces of apparatus if not blocked by fire hose or parked vehicles. Clear access to the tanks would be difficult because of the tanks location. The tanks will be blocked by a fence, shrubbery, and trees. Placement of hoselines to reach the tanks could prove hazardous for firefighters.

Propane being a compressed gas expands 270:1 when released. This vapor is heavier than air and will creep along the ground and settle in low pockets. Should a leak occur at night a large section of the Preserve Neighborhood could be impacted. The Preserves Neighborhood has one access / exit corridor that passes directly in front of Lakeside Memorial Lawn. Should an event occur not all of the residents are getting out.

According to the "City of Folsom Community Wildfire Protection Plan" dated April 2013, The Sacramento County Multi-Hazard Mitigation Plan identifies Folsom as having the greatest density of housing subject to wildfire in Sacramento County. (page 5)

Folsom is a registered community at risk. The Folsom Fire Department has identified the greatest threat to the community from fire would be a fast moving wildfire in the brush and oak woodland fuel bed that line the American River where it flows through Folsom. (page 6)

The City of Folsom also has the responsibility to insure future planning and zoning decisions for development adjacent to open space areas including sufficient provisions for the clearance required to protect new and future structures. These provisions may include adequate setbacks, buffer areas, or other measures to reduce the wildfire risks. (page 7) In my opinion, the authors of this document overlooked the "City of Folsom Community Wildfire Plan". The Negative Declaration states, page 61, "The project is not located in or near a State Responsibility Area or in a Very High Fire Hazard Severity Zone (Calfire2020;csg2020)". On page 92, "Environmental Setting", "The project site is not located in or near a state responsibility area or lands classified as a very high fire hazard severity zone". It seems the

Negative Declaration overlooked or failed to seek out all relative information as to the fire hazards in the area.

In the past couple of months there have been several grass/wildland fire incidents in Folsom and within the Lake Natoma Recreation area.

- On June 2, 2021 storage shed at Negro Bar and the exterior of the Cliff House Restaurant was set on fire.
- On June 5, 2021, 3.5 acres was burned off Prairie City Road near the transient encampment.
- On July 7, 2021 encampment fire near Blue Ravine and Sibley.
- On July 16,2021 several acres of vegetation burned in the Humbug-Willow Creek with reported difficulty accessing.
- On July 23, Folsom Fire Department requested the City to declare a "Local Fire Emergency" .
Already this year, 47 brush fires, majority human caused.

Several times I have witnessed transient activity at the lake and in the nearby woodlands. I believe a fire from a transient camp or vandalism could impact the propane tanks. 500 gallons of propane is a lot of propane for a residential neighborhood with limited access.

I am not opposed to crematoriums. I am opposed to crematoriums in residential neighborhoods. "Folsom Distinctive by Nature ". This project belongs in a commercial industrial zoned area.

Thank You for your time,

Dave Higgins

Propane Tank Hazards

Miscellaneous Fireline Hazards

Liquefied Propane Gas (LPG) tanks are commonly found in the wildland-urban interface and present hazards to firefighters in that environment. LPG tanks may be found in a number of other environments such as motor homes,

travel trailers, grills, camp stoves, lanterns, etc. Directly attacking LPG tank fires is a structural fire task involving hazardous materials and should only be attempted by trained personnel using full structural personal protective



equipment and equipped with a volume of water adequate to safely attack the fire.

Boiling Liquid Expanding Vapor Explosions (BLEVE)

- The most recognized hazard with LPG tanks is BLEVE (Boiling Liquid Expanding Vapor Explosions) or sudden complete failure of the tank. Some training courses have directed responders to approach the tank from the sides, believing that the force of the explosion will occur on the ends of the tank. However, this is not a guarantee that you will be safe from projectiles or missiles from the explosion, as they may travel in ALL directions up to 2,500 feet away. Leave the area immediately if you smell propane; hear a rising sound from venting safety devices or see discoloration or deformation of the tank. If you leave the area, get at least 2,500 feet away and do not go down wind or down slope of the leaking propane. BLEVEs are a major hazard to emergency responders!

Fuel Reduction Around Tanks

- Wildland firefighters may take action to prevent direct flame impingement on LPG tanks by removing wildland fuels in the area. However, be aware that lines from the tank to structures may be above or below ground, and may be cut by tools or equipment. Propane gas is heavier than air, and may move along the ground at some distance, and may ignited when it reaches open flame or another ignition source. Use extreme caution when doing fuel reduction around tanks, and flag any lines you encounter.

Other Wildland Fire Considerations

- Do not position engines or other apparatus near LPG tanks or downwind / down slope from tanks.
- Do not deploy fire shelters near LPG tanks or downwind / down slope from tanks. Cooling Tanks
- In light fuels such as grasses, where any heat exposure to the tank will be very limited, rapid application of cooling water on the outside of the tank above the liquid level can reduce the likelihood of container failure by lowering the external temperature of the shell of the exposed tank. Water should not be directed at the valve safety devices, due to the potential of "icing" the valve closed.
- In heavy fuels where long duration heat exposure to the LPG tank is likely, evacuate all personnel and equipment 2,500 feet away and not down slope or down wind. NFPA says that direct flame impingement protection requires water flow of at least 500 gpm from an unmanned monitor nozzle. This is a situation for properly trained, equipped and supported structural firefighters.

References: [Propane Safety Web Site](#), [NIOSH Web Site](#), [National Propane Gas Association's Web Site](#), [NFPA Web Site](#)

Have an idea? Have feedback? Share it.

EMAIL | [Facebook](#) | MAIL: 6 Minutes for Safety Subcommittee • 3833 S. Development Ave • Boise, ID 83705 | FAX: 208-387-5250

February 4, 2022

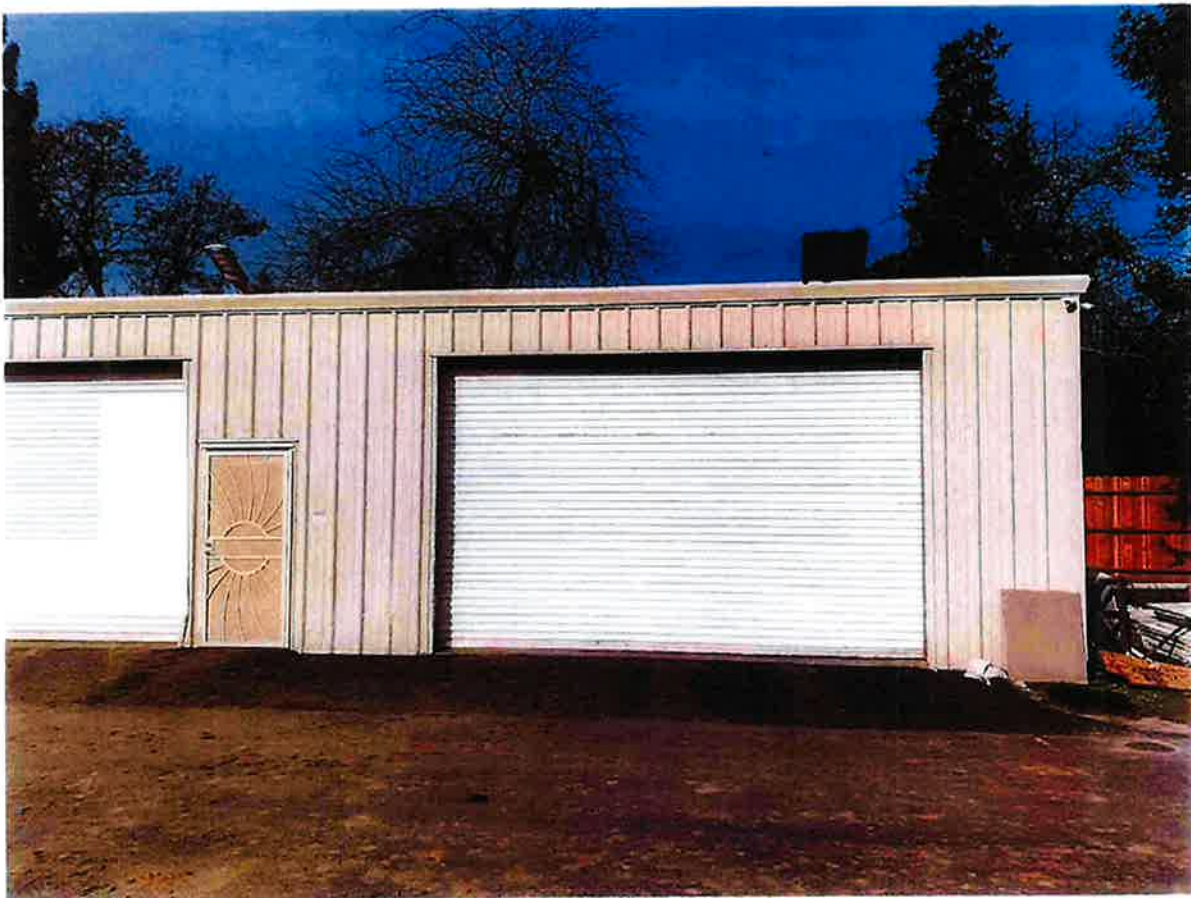
Historic District Commissioners
City Council Members
City Manager
Kelly Mullett

My name is Dave Higgins, I live across the street from Lakeside Memorial Lawn where the proposed crematorium is to be built. Over the course of summer to the present time I have taken pictures of activity at the cemetery. I wish to share.

This picture was submitted by the Caring Service Group and Miller Funeral Home's 2-27-2020 application for the Conditional Use Permit to install a crematory. Two years ago. This is what the metal storage shed and surrounding grounds looked like in 2-27-2020.



This picture was also submitted 2-27-2020 to illustrate the look and size of the smokestack on the roof. The amateurish hand drawn chimney does not truly illustrate the true look or height of the proposed stack. According to the Negative Declaration the stack is to be 19 ½ feet total height from grade.



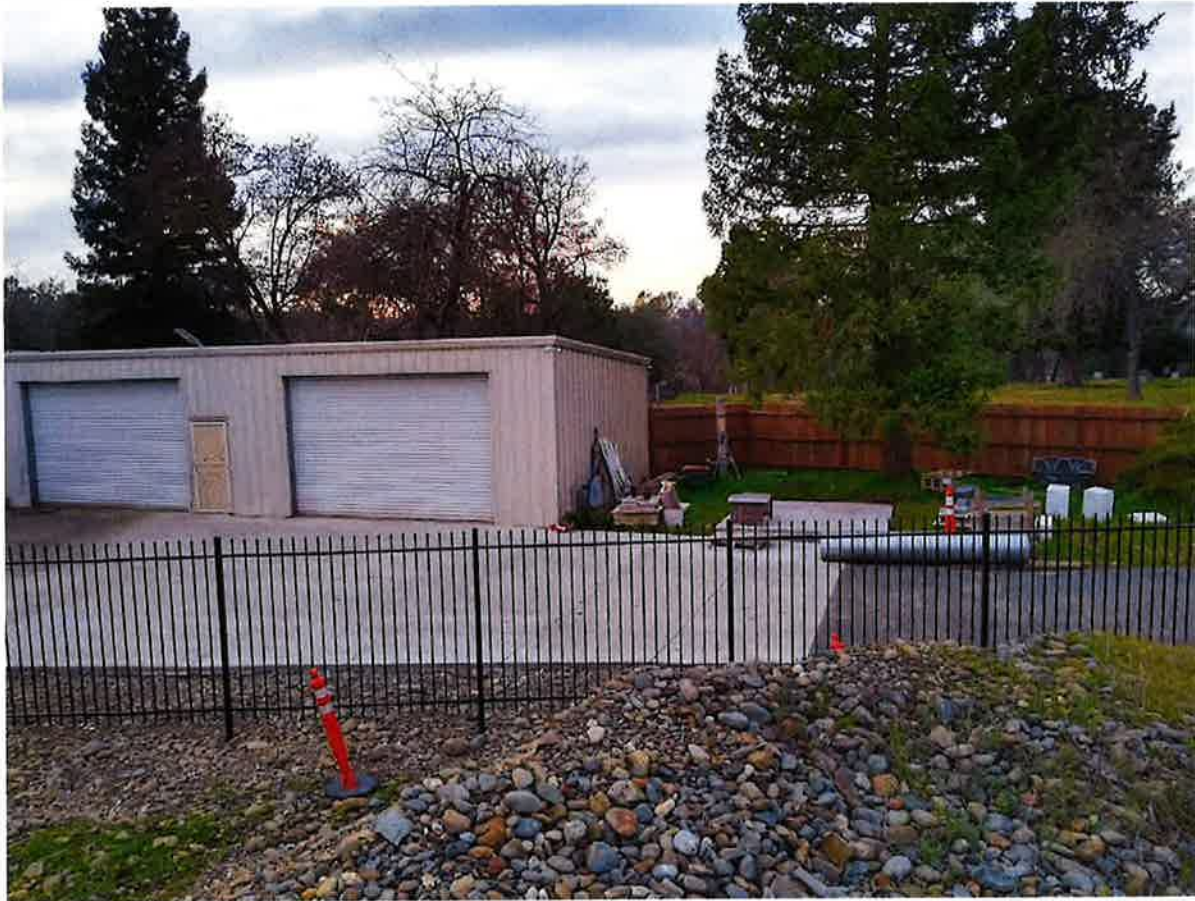
This picture was taken January 13th, 2022. This is what the maintenance grounds look like currently. The new storage shed is in the background and the existing shed is on the right. The metal security fencing surrounds the grounds and has a locked security gate. Note the Dredger Tailing Pile condition. It has been disturbed over the years.



This picture was taken January 13th, 2022. This picture illustrates the current look of the maintenance grounds, sheds and security fencing. The metal shed on the right is where the proposed crematory is to be installed.



This picture shows the propane tank pad right of the building under the large tree. This photograph was taken January 13th, 2022. The application site plan confirms the location of the propane pad. In addition, the site plan calls for 2 x 500 gallon tanks, not the 2 x 250 gallon tanks in the Negative Declaration text.



This photograph taken January 13th 2021 displays another angle of the location of the propane tanks pad. Blocked by a wooden fence, metal security fencing, a large redwood tree and the metal shed.



This picture was taken on August 4th, 2021. This is Lakeside Memorial Lawn groundskeeper, Valdimir Semenyuk, driving out of the cemetery with a 250 gallon propane tank. This picture is disturbing on many levels. It is my opinion that the Caring Service is very confident in the outcome of the Conditional Use Permit. If indeed this tank is for the Crematory it is very arrogant for the Caring Service Group to show no discretion in their activities.



Pictures are worth a thousand words.

Thank You
Dave Higgins

Josh Kinkade

From: Deb Ozdinski [REDACTED]
Sent: Monday, August 16, 2021 1:29 PM
To: Josh Kinkade
Subject: Crematorium Project

[You don't often get email from [REDACTED] Learn why this is important at <http://aka.ms/LearnAboutSenderIdentification.>]

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Josh,

Thanks for taking my call this morning.

As we discussed the Caring Services Group (Miller Funeral Home) applicant is applying to place a crematorium in a shed on that Lakeside Cemetery site.

What I want to do is remind you of our discussion that this crematorium affects so many people. Not only the city, the environment, homes close by, the river, Lake Natoma State Park and the other historic cemetery.

I am requesting the Applicant must also contact the owners of all the niches and family members that have buried their loved ones and disclose the proposed project to them. These people should be informed just like other projects MUST send out notices. YES, this is a unique situation and YES it should get unique attention. There is no excuse this far in advance that this added Notice can't be done. Again, this is a unique situation.

I own my own niche for my husband and I. Other siblings of mine living today have purchased niches along the same wall that is closest to the metal shed. When I visit my parents, we stand and talk to them, and what we would be having to endure is a crematorium staring us in the face. How unpleasant do you think that will be? It would affect our use and enjoyment of the cemetery and visiting our loved ones. It makes me sick to think of visiting them while knowing bodies are being cremated just feet from my parents remains.

I please, please ask that the Applicant and City must take additional time and work to notify the people involved that purchased in that cemetery. There must be records of what they sold and to whom. If they have no records or lost records that tells you they have poor business practices.

Again, please make this a condition that those folks are informed and heard. They have the right to know! I am certain the numbers of people opposing this project will be ten times the numbers you would get otherwise. We purchase a spot for our loved ones to rest and now to think we may need to sell niches and move our parents. This is what we are faced with.

Please review with appropriate city folks and let me know what has been decided.

Just remember if you were in our shoes how would you feel? Ask those Planners that question too. It's the right thing to do.

Regards.

Deb and Greg Ozdinski
[Redacted] Water View Way
Folsom

Josh Kinkade

From: Deborah Grassl [REDACTED]
Sent: Thursday, January 27, 2022 2:51 PM
To: Josh Kinkade
Subject: Re: Crematorium Noticing

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Josh,

I noticed that this Project's CUP is not included in the public review documents IS/MND released. CEQA refers to the CUP being an integral part of the public comment review period. The Governor's Office of Research and Planning wrote a guide titled, 'The Conditional Use Permit.' It states on page 3, paragraph 3, " ...If the proposal is not exempt from environmental review, the city or county is required to prepare either a negative declaration indicating that the conditional use permit will have no significant effect, or an Environmental Impact Report (EIR) which describes the potential negative impacts of the proposal and the means to avoid or lessen those impacts.

Please see the CEQA references to the public's right to review below :

CEQA CALIFORNIA PUBLIC RESOURCES CODE, DIVISION 13.ENVIRONMENTAL QUALITYChapter 1: Policy

§ 21003. PLANNING AND ENVIRONMENTAL REVIEW PROCEDURES; DOCUMENTS; REPORTS; DATA BASE; ADMINISTRATION OF PROCESS

The Legislature further finds and declares that it is the policy of the state that:

(a) Local agencies integrate the requirements of this division with planning and environmental review procedures otherwise required by law or by local practice so that all those procedures, to the maximum feasible extent, **run concurrently, rather than consecutively** ,...

§ 21003.1. ENVIRONMENTAL EFFECTS OF PROJECTS; COMMENTS FROM PUBLIC AND PUBLIC AGENCIES TO LEAD AGENCIES; AVAILABILITY OF INFORMATION

The Legislature further finds and declares it is the policy of the state that:

(a) Comments from the public and public agencies on the environmental effects of a project shall be made to lead agencies as soon as possible in the review of **environmental documents, including, but not limited to**, draft environmental impact reports and **negative declarations**, in order to allow the lead agencies to identify, at the earliest possible time in the environmental review process, potential significant effects of a project, alternatives, and mitigation measures which would substantially reduce the effects.

(b) Information relevant to the significant effects of a project, alternatives, and mitigation measures which substantially reduce the effects shall be made available as soon as possible by lead agencies, other public agencies, and interested persons and organizations.

(c) Nothing in subdivisions (a) or (b) reduces or otherwise limits public review or comment periods currently prescribed either by statute or in guidelines prepared and adopted pursuant to Section 21083 for environmental documents, **including, but not limited to**, draft environmental impact reports and **negative declarations**.

§ 21064.5. MITIGATED NEGATIVE DECLARATION

“Mitigated negative declaration” means a negative declaration prepared for a project when the initial study has identified potentially significant effects on the environment, but

(1) revisions in the project plans or proposals made by, or agreed to by, the applicant **before the proposed negative declaration and initial study are released for public review** would avoid the effects or mitigate the effects to a point where clearly no significant effect on the environment would occur, and

(2) there is no substantial evidence in light of the whole record before the public agency that the project, as revised, may have a significant effect on the environment.

§ 21080. DIVISION APPLICATION TO DISCRETIONARY PROJECTS; NONAPPLICATION; NEGATIVE DECLARATIONS; ENVIRONMENTAL IMPACT REPORT PREPARATION

(a) Except as otherwise provided in this division, this division shall apply to **discretionary projects** proposed to be carried out or approved by public agencies, including, but not limited to, the enactment and amendment of zoning ordinances, the issuance of zoning variances, **the issuance of conditional use permits**, and the approval of tentative subdivision maps ...

(c) If a lead agency determines that a proposed project, not otherwise exempt from this division, would not have a significant effect on the environment, the lead agency shall adopt a negative declaration to that effect. The negative declaration shall be prepared for the proposed project in **either** of the following circumstances:

(1) There is no substantial evidence, in light of the whole record before the lead agency, that the project may have a significant effect on the environment.

(2) An initial study identifies potentially significant effects on the environment, **but**

(A) revisions in the project plans or proposals made by, or agreed to by, the applicant **before the proposed negative declaration and initial study are released for public review** would avoid the effects or mitigate the effects to a point where clearly no significant effect on the environment would occur....

§ 21092. PUBLIC NOTICE OF PREPARATION OF ENVIRONMENTAL IMPACT REPORT OR NEGATIVE DECLARATION; PUBLICATION

(b)(1) The notice shall specify the period during which comments will be received on the draft environmental impact report or negative declaration, and shall include the date, time, and place of any public meetings or hearings on the proposed project, a brief description of the proposed project and its location, the significant effects on the environment, if any, anticipated as a result of the project, the address where copies of the draft environmental impact report or **negative declaration, and all documents referenced in the draft environmental impact report or negative declaration, are available for review**, description of how the draft environmental impact report or negative declaration can be provided in an electronic format.

Josh, I request that you post the Conditional Use Permit in accordance to CEQA, and adjust the period of time for public review for this Project.

Deborah Grassl

On 1/7/22 10:20 AM, Josh Kinkade wrote:

Hello,

If you are receiving this email, you have previously emailed or sent a letter to City of Folsom staff and/or City Council members and their Historic District Commission representatives regarding the proposed Lakeside Memorial Lawn Crematory project. Because you are an interested party, I am sharing the attached public hearing notice (which is being published in the February 6th Folsom Telegraph and sent to property owners within a 300-foot radius of the project site) and informing you that the Historic District Commission is considering the Conditional Use Permit (CUP) and Initial Study/Mitigated Negative Declaration (IS/MND) for the project at the February 16, 2022 Historic District Commission meeting.

The final version of the IS/MND, as well as additional project documentation, is located here: <https://www.folsom.ca.us/government/community-development/planning-services/current-project-information>. Physical copies of the document will also be made available at City Hall, at the Community Development Counter. The public comment period for the IS/MND begins on January 7, 2022 and ends on February 7, 2022.

The staff report for the CUP will be available here at least 5 days prior to the February 16 hearing date: <https://www.folsom.ca.us/government/community-development/planning-services/historic-district-commission>. A physical copy of the document will also be made available at City Hall, at the Community Development Counter.

Note that if you have emailed or mailed a comment to staff, or if your letter/email to City Council members or Historic District Commissioners was forwarded to staff, that comment will be included in the staff report for the CUP and will be available to the commission and general public prior to the hearing on February 16th. All additional comments received prior to the publication of the staff report will also be published in the staff report. You may also make a verbal comment at the February 16th meeting (up to 3 minutes in length per person).

Thank you,

Josh Kinkade
Associate Planner

Community Development Department

Deborah Grassl
 Young Wo Circle
 Folsom, CA 95630

February 7, 2022

Historic District Commission
 City of Folsom
 50 Natoma Street
 CA 95630

RE 2022 Lakeside Memorial Lawn Crematorium Folsom,
 Project PN-19-182 and 2002 Lakeside Cemetery
 Crematorium Project PN-02-058

Dear Commissioners:

I would like to make a comment on the above-mentioned 2022 Lakeside Memorial Lawn Project Application. In 2002, the Lakeside Cemetery Crematorium made an identical application based on an Initial Study and Mitigated Negative Declaration prepared by the City of Folsom. Those 2002 documents survive any subsequent consideration of an application with an identical project that, has no new substantial information, based on the holding in *Friends of the College of San Mateo Gardens v. San Mateo County Community College* (2016) 1 Cal. 5th 937. :

OPINION J. Kruger “To ensure that governmental agencies and the public are adequately informed about the environmental impact of public decisions, the California Environmental Quality Act (CEQA) (*Public Resources Code 21000 et seq.*) requires a lead agency to prepare an environmental impact report (EIR) before approving a new project that “may have a significant effect on the environment” (*id.* 21151 (a)). When changes are proposed to a project for which an EIR has already been prepared, the agency must prepare a Subsequent or Supplemental EIR only if the changes are “[s]ubstantial” and require “major revisions” of the previous EIR. (*Id.* 21166.) **Guidelines ... extend this subsequent review framework to projects for which a negative declaration was initially adopted, and no EIR was prepared...(CEQA Guidelines 15162.)”**

According to the above court’s ruling, the existing 2002 Mitigated Negative Declaration (MND) stands and must be the controlling document for an identical project.

Staff’s 2002 Findings on the CUP application were recommended for denial based the MND’s lack of substantial environmental information on: whether the operation of the crematorium would or would not affect the health, safety and welfare of the neighborhood residents; that a crematorium was not a compatible use with a historic cemetery because it did not agree with the Historic District Design and Development Guidelines or General Plan goals of history resource preservation of Folsom’s three unique historic resources (Chung Wah, Lakeside Cemetery’s 8 historic cemeteries, and State Preserve Dredger Tailings) , and, because of the substantial reaction against the crematorium by the residents and history preservation groups.

Because the 2022 Lakeside Memorial Lawn Crematorium Project MND and CUP do not have any standing the 2022 MND and CUP should be denied by the Historic District Commission based on (*Friends of the College of San Mateo Gardens v. San Mateo County Community College* (2016) 1 Cal. 5th 937).

If the Lakeside Cemetery wants to re-apply they can do so with the 2002 MND and a new CUP application. However, the Findings for the 2002 CUP denial would still be unanswered and a new CUP application would still have to answer to those deficiencies - by doing the CEQA environmental work that the MND failed to provide.

I recommend HDC deny the 2022 Lakeside Memorial Lawn Cemetery Project with a recommendation to go back and do an Environmental Impact Report. If substantial information regarding the environmental studies should provide answers as to why the Staff recommended denial of the CUP originally, then reapply with a Subsequent EIR. A Subsequent EIR would have legal standing before HDC. The general public, residents surrounding the cemetery and history preservation groups are vigilant and will settle for nothing less.

Sincerely,

Deborah Grassl

cc: kcolepolicy@gmail.com; justin@revolutionsdocs.com; danwestmit@yahoo.com; ankhelyi@comcast.net; johnfelts@e55tech.com; m.dascallos@yahoo.com; jkincade@folsom.ca.us; kmullett@folsom.ca.us

Deborah Grassl
Young Wo Circle
Folsom, CA 95630

February 7, 2022

Historic District Commission
City of Folsom
50 Natoma Street
Folsom, CA 95630

RE Lakeside Memorial Lawn Crematorium Project
PN-19-182

Dear Commissioners:

I would like to make three comments with regards to *standards of guidance* that control the approval or denial of the above mentioned Project and its request for a Conditional Use Permit (CUP). And then summarize a recommendation for denial of the Lakeside Memorial Lawn Crematorium Mitigated Negative Declaration and CUP operation.

Standards of Guidance: California's Hierarchy of Planning Laws

1) Under State of California law, charter cities have the legal authority to issue conditional use permits (CUP) using their own written *standards of guidance*, with one proviso: the *standards of guidance* must be contained in the charter city's Municipal Zoning Code, which must be in compliance with its adopted General Plan, which must be in compliance with the California Environmental Quality Act (CEQA) state laws.

The validity of a charter city's CUP approval process derives from compliance with this hierarchy of planning laws (*Neighborhood Action Group v. County of Calaveras (1984) 156 Cal. App.3d 1176*). A charter city can name any number of documents as *standards of guidance* as long as they are contained in their Municipal Zoning Code, in compliance with their General Plan; and in compliance with CEQA. The reason for this hierarchy of planning laws is to check any unbridled discretionary act or decision by an administrative agency, and to help guide the administrative body's Findings.

The City of Folsom is a charter city that authorizes the Historic District Commission to approve CUPs for projects in the Historic District. Some of Folsom's *standards of guidance* are not in compliance with California's hierarchy of planning law.

2) Folsom Municipal Zoning Code (FMC) 17.52 HD-Historic District is a good example.

The FMC 17.52 was crafted from a Master Environmental Impact Report (MEIR), taken from the Historic District Specific Plan mentioned in the 1988 General Plan as almost completed. When finished, this Plan was to be adopted by the City Council and become the controlling *standard of guidance* for the Historic District.

The Historic District Specific Plan was NOT adopted by the City Council. However, the Plan and MEIR were later used to develop the City's Zoning Code 17.52 HD-Historic District, and to craft the Historic District Design and Development Guidelines, both as a *standards of guidance* for the Historic

District. The unadopted Historic District Specific Plan and MEIR, were still used as *standards of guidance* for Folsom's Municipal Code 17.52 and the Historic District Guidelines.

The historic record for how this *standard of guidance* came into being can be found in the (c.1995) Historic District Design and Development Guidelines Introduction, Chapter 1.03 CEQA Compliance:

“The City conducted environmental review of a proposed Historic District Specific Plan. Due to the nature and complexity of the Plan and the projects which will follow under it, a Master Environmental Impact Report (MEIR) was prepared. The City ultimately elected to implement zoning changes to Chapter 17.52 of the Folsom Municipal Code and *design standards* through the adoption of these Design and Development Guidelines. Many of the provisions of the proposed Historic District Specific Plan and the amendments to Chapter 17.52 are contained within these *guidelines*. Accordingly, the City Council relied upon the MEIR for evaluation and consideration of environmental impacts.

... The intent is to allow the creation of an environmental impact analysis and mitigation foundation in an MEIR for a broad planning project which will then enable significant reduced CEQA documentation for future projects built according to Chapter 17.52 and the Design and Development Guidelines.”

The Historic Specific District Plan and MEIR, the foundational instruments for FMC 17.52 and the Historic District Design and Development Guidelines, are in direct conflict with Folsom's General Plan because they were/are used as *standards of guidance* but were not adopted by the City Council into the General Plan. Up until 2 weeks ago, the Historic District Specific Plan was on the City's website as a Resource for the public who need information from the Planning webpage.

3) The Folsom Historic District Commission (HDC) has been given the authority to perform discretionary actions, i.e. the granting of a major CUP, without *standards of guidance* to facilitate mandated deliberation (**CEQA Guidelines Section 15121**) on whether a project qualifies for a major CUP as expressed in its FINDINGS (***Protecting Our Water and Environmental Resources v. County of Stanislaus (2020) 10 Cal.5th 479***).

Accordingly, the City of Folsom describes its *standards of guidance* for issuance of a CUP from its Historic District Design and Development Guidelines however one must go to FMC 17.60.101 Issuance Conditions and applicable laws, and to FMC 1.08 Enforcement of the Folsom Municipal Code and Other Applicable Laws to find standards of guidance for both the Planning Commission and the Historic District Commission. None of these webpages on the City's website refer to CEQA as a controlling *standard of guidance* triggered by the discretionary act of approving a CUP.

CEQA is referred to in the General Plan Update PEIR as a controlling *standard of guidance* for discretionary acts. So, the FMC is in direct conflict with Folsom's General Plan Update PEIR. Here is a conflict in Folsom's planning law that enables unbridled discretionary decisions on Historic District projects.

Summary of Recommendation

The Lakeside Memorial Lawn Crematorium Project and its CUP application is just such a project that requires CEQA deliberation and consideration, which is contained in the General Plan Update PEIR, but not in the FMC.

Approval of the Lakeside Memorial Lawn Crematorium Project and its CUP operations would have very big impacts on residents' health, safety and welfare who live in the neighborhood, and big impacts on treasured historic resources based on the 2022 Mitigated Negative Declaration. I recommend careful CEQA study of the 2003 Initial Study Staff Report and Findings on the last time that the Lakeside Memorial Lawn Cemetery made the same application for a CUP and was denied. Please read the 2003 Findings on the CUP before you deliberate and make your Findings.

To summarize the questions that CEQA could generate on the 2022 MND (which are identical to the Findings in the 2003 MND), and that would satisfy the obligation to show deliberation and where Findings (sans Staff Findings) came from, are:

Is there enough definitive information contained in the 2022 Mitigated Negative Declaration (MND) that is a health study, evidence based, published in a respected scientific medical journal, showing that this crematorium operation will not directly impact, or indirectly impact the health, safety and welfare of the 300+ residents that live next to it?; Do you have enough evidence that says it's okay to ignore General Plan policies, Historic District Design and Development Guidelines' policies regarding preservation of, and protection of, unique historic resources?; Is your decision respectful of the pioneers' and Chinese cultures and their funerary rights? Does your decision reflect the respect the National Register of Historic Places, Sacramento County's Cemetery Commission, the City of Folsom's Master Preservation List of Historic Resources have for the Lakeside Lawn Cemeteries and the Chung Wah Cemetery? And, based on the evidence in the whole record, can you definitively say the Crematorium's operation would have no impact on Folsom's residents performing sacred ablutions graveside, visiting their interred loved ones, and living next to treasured cemeteries that are inundated with fine particles generated by 800 lbs of burning human flesh each day?

The operation of a crematorium at Lakeside Memorial Lawn Cemetery is not a compatible use with historic cemeteries, or proved to have no impact on the health, safety and welfare of the community.

I recommend denial based on the 2003 Staff Findings that still contain the original, unanswered CEQA questions when applied to the new, 2022 Lakeside Memorial Lawn Crematorium Mitigated Negative Declaration and CUP operation.

Sincerely,

Deborah Grassl

cc email: Historic District Commissioners

kcolepolicy@gmail.com; justin@revolutionsdocs.com; danwestmit@yahoo.com;

ankhelyi@comcast.net; johnfelts@e55tech.com; m.dascallos@yahoo.com; jkincade@folsom.ca.us

Josh Kinkade

From: Deborah Grassl [REDACTED]
Sent: Friday, July 2, 2021 4:56 PM
To: Josh Kinkade
Cc: Terry Sorensen; Steve
Subject: Re: Zoom meeting with Igor Semenyuk and HFRA/Lake Natoma Shores residents

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thanks Josh. I don't know how I missed that the Initial Study and MND were combined.

A comment on the IS/MND: There is scant information on the crematorium's cumulative future impacts regarding traffic, circulation, air quality and wildfire safety.

Example: Igor didn't seem to know how often activities at the Cemetery generate a high level of cars parking on Forrest, Young Wo and Sutter Streets. He thinks that the Cemetery has 50 parking spaces and that is all he needs for future ceremonies and interments that future cremations will generate. In addition, he thinks that although he will conduct 500 cremations a year, this might generate 30 actual ceremonies and interments at the Cemetery...

Example: When asked about the Level of Service change at the intersection of Folsom Blvd and Forrest anticipated by the increased level of cars heading into the Cemetery to attend ceremonies and interments generated by the 30 cremations, he didn't know...

Example: When asked about what kind of wildfire design he used for the crematorium, he said that as far as he knew there weren't any wildfire problems. (He was unaware of Folsom's Community Wildfire Protection Plan. And although the IS stated that the Folsom Fire Chief signed off on the project, there was no input from the Fire Chief in the IS.) He then described how the blast zone of one of the propane tanks was 600' and he thought the propane tanks would be far enough away from homes to not blow them up. He didn't mention the National Registry of Historic Place Chung Wah Cemetery, the State Park filled with dry grasses and trees - or his own Cemetery filled with trees and dry grasses in the back abutting the State Park lands and on the old abandoned Figueroa Street behind 21 Young Wo Circle homes...

Example: Couldn't find any reference about if the crematorium will operate on days when our area is impacted by wildfire smoke or Spare The Air days. When asked if he realized that we are in a non-attainment area and that Folsom doesn't meet State or County government regulations to reducing overall air pollution, Igor didn't know whether the crematorium's air pollution footprint would help the City of Folsom meet the General Plan air quality goals or not...

These future operational impacts are important to people living at The Preserve, Lake Natoma Shores and the Historic District - not so much the IS's construction procedures of the actual building of the crematorium.

We would like Igor to get the environmental information for the cumulative effects of his future operation. That information will tell us whether the quality of our lives will be impacted by a crematorium or not.

Thanks,

Deborah Grassl
 [REDACTED]

From: [Elaine Andersen](#)
To: [Josh Kinkade](#)
Subject: FW: Proposed Crematorium
Date: Friday, August 6, 2021 8:22:31 AM

From: Debra Williams [REDACTED]
Sent: Wednesday, July 28, 2021 3:10 PM
To: Elaine Andersen <eandersen@folsom.ca.us>
Subject: Proposed Crematorium

You don't often get email from [REDACTED]. [Learn why this is important](#)

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July 28, 2021

Dear Ms. Anderson,

I am writing to voice my concerns regarding the proposed Lakeside Memorial Lawn crematorium project. The crematorium would be built and operate just a few hundred feet from my home. After reading the Initial Study/Mitigated Negative Declaration posted on the Folsom City website and doing further research of reliable sources, I am not convinced that the effects would be "less than significant" for those living so near the project. I have serious concerns regarding the impact to air quality, including the release of toxins such as mercury that an operating crematorium would have on my family (my 83 year old mother lives on the same street), and other residents of the neighborhood.

My mother has lived on this street for 23 years and I have lived four doors up the street from her and been a teacher at the public middle school just one mile from our neighborhood since 2006. I ask that this proposal be denied and that the business owners find a location that is not so near a residential area. Please consider how you would feel if you were in our shoes. I imagine that most persons involved in this decision making process and those who would make a profit from the business, would not want their own family living so close to it.

Thank you very much for your time and attention to this very sensitive and important issue.

Sincerely,

Debra Williams

[REDACTED] Young Wo Circle
Folsom, CA 95630
[REDACTED]

Josh Kinkade

From: Diana Matheny <zinlady1@yahoo.com>
Sent: Tuesday, August 31, 2021 4:16 PM
To: Josh Kinkade
Cc: [REDACTED]
Subject: Re: Proposed Crematory CUP

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Dear Josh Kinkade:

Lakeside Memorial Lawn, owned by the Caring Service Group, has applied for a conditional use permit to install a crematory on the grounds, here in our backyard in Historic Folsom. This has serious implications for the physical, environmental, and fiscal health of our community and the Folsom Historic District.

Our homes and business in Historic Folsom have been here for many years. We do not want to jeopardize our beautiful community with the consequents of having a crematory installed.

Rancho Cordova has the Rendering Plant. The business and residents who live in the area complain of the horrible smell from the rendering plant. It's stinky and you don't want to go outside in your backyard. I've researched crematory online with different cities who have had complaints from residents with dark smoke and smell. We definitely do not want this in our neighborhood.

Historic Folsom is a charming place where people come to live, enjoy the outdoors with biking, and walking. The Historic Folsom Sutter Street attracts tourist and locals. If the crematory is approved this will change the Historic Folsom charm and lose the tourist and locals supporting the business on Sutter Street. It will also bring down the value of our homes.

We love and live here locally in the Historic Folsom area. I asked you to do the right thing and **deny** the conditional use permit to install a crematory on the grounds at Lakeside Memorial Lawn, owned by Caring Service Group.

Can you suggest to relocate somewhere else like Folsom Ranch on the other side of Hwy 50? Of course not near homes. There is plenty of open space land to build a crematory in that area.

Do not build crematory in a residential area. Its not fair to the home owners.

Please provide my email to the Historic Commission who will make the decision to deny or grant the decision of the permit.

Thank you

Diana Luzader

[REDACTED] Wool St, Folsom Ca

916

Sent from my iPhone

On Aug 31, 2021, at 10:19 AM, Josh Kinkade <jkinkade@folsom.ca.us> wrote:

Diana,

I had your message forwarded to me regarding the proposed crematory facility at the Lakeside Memorial Lawn. The Conditional Use Permit (CUP) is subject to a vote by the Historic District Commission. We present a staff report to the commission along with any comments that have been received prior to the meeting, so if you wish to present any written comments to the decision-makers, you can email them to me and I can include them in the packet that we sent to the commission. Current information about the project, including the draft Initial Study that discusses the project in detail, can be found here: <https://www.folsom.ca.us/government/community-development/planning-services/current-project-information>

Thanks,

Josh Kinkade
Associate Planner

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6209

<image001.png>

<image002.png>

<image003.png>

<image004.png>
www.folsom.ca.us

<image005.jpg>

Josh Kinkade

From: Scott Johnson
Sent: Friday, August 27, 2021 11:47 AM
To: Josh Kinkade
Subject: Fwd: No Historic Folsom Crematory

Sent from my T-Mobile 5G Device
Get [Outlook for Android](#)

From: Diana Matheny [REDACTED]
Sent: Friday, August 27, 2021 11:37:29 AM
To: Scott Johnson <sjohnson@folsom.ca.us>
Subject: No Historic Folsom Crematory

You don't often get email from zinlady1@yahoo.com. [Learn why this is important](#)

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Diana Luzader

[REDACTED] Wool Street

Folsom, CA 95630

August 27, 2021

Scott Johnson

Folsom, Planning Manager

Dear Scott Johnson :

Lakeside Memorial Lawn, owned by the Caring Service Group, has applied for a conditional use permit to install a crematory on the grounds, here in our backyard in Historic Folsom. This has serious implications for the physical, environmental, and fiscal health of our community and the Folsom Historic District.

Our homes and business in Historic Folsom have been here for many years. We do not want to jeopardize our beautiful community with the consequents of having a crematory installed.

Rancho Cordova has the Rendering Plant. The business and residents who live in the area complain of the horrible smell from the rendering plant. It's stinky and you don't want to go outside in your backyard. Historic Folsom is a charming place where people come to live, enjoy the outdoors with biking, and walking. The Historic Folsom Sutter Street attracts tourist and locals. If the crematory is approved this will change the Historic Folsom charm and lose the tourist and locals supporting the business on Sutter Street. It will also bring down the value of our homes.

We love and live here locally in the Historic Folsom area. I asked you to do the right thing and **deny** the conditional use permit to install a crematory on the grounds at Lakeside Memorial Lawn, owned by Caring Service Group. Please provide your feedback as soon as possible.

Sincerely ,

Diana Luzader

Josh Kinkade

From: Scott Johnson
Sent: Wednesday, February 2, 2022 2:53 PM
To: Josh Kinkade
Subject: FW: Vote NO on Folsom Crematorium Proposal

Scott A. Johnson, AICP

Planning Manager

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6206



From: Edwin Grattan [REDACTED]
Sent: Wednesday, February 2, 2022 2:52 PM
To: Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; kcolepolicy@gmail.com; danwestmit@yahoo.com; ankhelyi@comcast.net; johnfelts@e55tech.com; m.dascallos@yahoo.com; Elaine Andersen <eandersen@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>
Subject: Fw: Vote NO on Folsom Crematorium Proposal

You don't often get email from [REDACTED]. [Learn why this is important](#)

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To: City Council representatives and Folsom Historic District Commissioners

Re: Vote NO on Folsom Crematory Proposal
Public hearing scheduled for February 16, 2022

I am writing to you as a concerned resident in the Folsom Historic District. I am strongly opposed to the conditional use permit to install a crematorium on the grounds of a residential open space. I believe allowing a permanent, industrial incinerator in the residential open space does not meet the criteria of the Folsom Historic District.

I further believe this contradicts the city of Folsom motto, 'Distinctive by Nature'. I urge you to vote NO on the proposed Folsom Crematorium. Please consider the impacts to the environment, physical health of the residents and the fiscal health of the Folsom Historic District.

Vote NO on the Folsom Crematory Proposal.

Sincerely,

Edwin Grattan
Historic Folsom Resident

Josh Kinkade

From: Erika Hamer [REDACTED]
Sent: Wednesday, January 12, 2022 5:12 PM
To: danwestmit@yahoo.com; daronbr@pacbell.net; kcolepolicy@gmail.com; kevin.duewel@gmail.com; m.dascallos@yahoo.com; johnfelts@e55tech.com; Mike Kozlowski; Sarah Aquino; YK Chalamcherla; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez; Elaine Andersen; Josh Kinkade
Subject: Crematorium
Attachments: Erika bluff photo.jpg

Some people who received this message don't often get email from erika.onwards@gmail.com. [Learn why this is important](#)

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To whom it may concern,

I live on the bluffs in Orangevale overlooking Lake Natoma. I have learned that a crematorium may be put in the Lakeside Cemetery next to the lake. This is a terrible idea.

I understand this is zoned as open space, and it should be kept natural. I paddle board frequently on the lake, enjoy being outside walking the paths and relishing the beauty I am so lucky to have in my backyard. A crematorium does not belong near residents and visitors to Folsom recreation, shopping and dining. Nor does this crematorium belong in an area that is next to a state park. Nobody wants to see or smell puffs of human remains/smoke while they are trying to enjoy the outdoors and neighboring areas; this should be put within an industrial zone or the like. Please help us keep the view and open space natural and (live) people-centered.

On another note, I would be furious and deeply disappointed if I heard this crematorium would be installed across or down the actual street from my home. Would you like it near yours?

Thank you for your time and attention to this important community matter.

Sincerely,
Erika Hamer
[REDACTED]

From: [Kelly Mullett](#)
To: [Josh Kinkade](#)
Subject: FW: crematorium
Date: Tuesday, November 23, 2021 10:21:03 AM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



From: Evelyn Gates [REDACTED]
Sent: Saturday, November 20, 2021 4:33 PM
To: danwestmit@yahoo.com; daronbr@pacbell.net; ankhelyi@comcast.net; kcolepolicy@gmail.com; kevin.duewel@gmail.com; m.dascallos@yahoo.com; Mike Kozlowski <mkozlowski@folsom.ca.us>; johnfelts@e55tech.com; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Elaine Andersen <eandersen@folsom.ca.us>; Josh Kinkade <jkinkade@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>
Subject: crematorium

Some people who received this message don't often get email from [REDACTED] [Learn why this is important](#)

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I am 8 years old, and I really don't want a crematorium in my neighborhood. My family lives in this neighborhood, and I don't want all of us to breathe bad air. I just moved here, and I don't want to move again. My grandparents live directly across from the cemetery. And my cousin lives 5 houses away from my grandparents, and she's only one! Please don't put a crematorium in my neighborhood there are lots of kids in the neighborhood.

Sincerely,
Evelyn Gates



ACTION NEEDED: NOTICE OF PUBLIC HEARING

Lakeside Memorial Lawn cemetery off Folsom Blvd. next to Sutter Street, owned by **Miller Funeral Home**, has applied for a conditional use permit to install a crematory here in the heart of our community. Their proposal has changed--they have doubled the number of bodies they propose burning each day. Burning 800 pounds of human bodies per day has serious implications for the physical, environmental, and fiscal health of the Folsom Historic District. Allowing a permanent, industrial incinerator in open space does not meet the criteria of the Historic District.

The time for action is **NOW!**

1. Mark Your Calendars!

- The public hearing is scheduled for **February, 16, 2022 at 5:00 pm.**
- Our attendance and participation is critical to ensure our interests are represented.

2. Write a Letter & Sign the Community Petition

- The public review period, where public comment is collected, is from January 7 - February 7.
- Write a letter to our Historic District and City Council elected representatives to tell them to vote "NO!"
- Sign the community petition at noburningbodies.com

3. Phone a Friend (or 3)!

- Please ask other community members, friends and family to write letters, sign the petition and attend on February 16th!

4. Want to Help?

- Email protectthepreserves@gmail.com to sign up to help!

Noburningbodies.com

Contact Folsom Historic District Commissioners:

Scott Johnson, Planning Manager	sjohnson@folsom.ca.us
Steve Banks, Principal Planner	sbanks@folsom.ca.us
Kelly Mullett, Administrative Assistant	kmullett@folsom.ca.us
Kathleen Cole - Vice Chair	kcolepolicy@gmail.com
[REDACTED]	[REDACTED]
Daniel West	danwestmlt@yahoo.com
Mickey Ankhelyi	ankhelyi@comcast.net
John Felts	johnfelts@e55tech.com
Mark Dascallos	m.dascallos@yahoo.com

Contact City Officials via Email:

Elaine Anderson, City Manager	eandersen@folsom.ca.us
Kerri Howell, Mayor	kerri@atlanticcorrosionengineers.com
Rosario Rodriguez, Vice Mayor	rrodriguez@folsom.ca.us
Sarah Aquino, Council Member	saquino@folsom.ca.us
YK Chalamcherla, Council Member	ykc@folsom.ca.us
Mike Kozlowski, Council Member	mkozlowski@folsom.ca.us

Contact City Officials via USPS Mail:

50 Natoma Street
Folsom, CA 95630

Contact Historic Folsom Residents Association:

thefra@gmail.com

Josh Kinkade

From: Kelly Mullett
Sent: Wednesday, July 7, 2021 3:37 PM
To: Josh Kinkade
Subject: FW: Lakeside Memorial Lawn Crematorium

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
 50 Natoma Street, Folsom, CA 95630
 O: 916.461.6231
 F: 916.355.7274



CITY OF FOLSOM
 DISTINCTIVE BY NATURE



www.folsom.ca.us

From: Genie Moeszinger [REDACTED]
Sent: Wednesday, July 7, 2021 3:06 PM
To: Kelly Mullett <kmullett@folsom.ca.us>
Subject: Lakeside Memorial Lawn Crematorium

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Dear K. Mullett,

I am a resident of the "Historic District" just across Folsom Blvd. within walking distance to old town Sutter St. I live on Sutter St. in one of the few historic homes that were spared when the Natoma Crossing Bridge was constructed. I love the neighborhood that I reside in. At first glance you would think it must be a dream come true area to call home. Well, we have had to accept and learn to live with relentless loud Light Rail dinging for 10 consecutive minutes on the half hour of every hour all day long and well into the evening EVERY single day. Roaring motorcycles that feel and sound like a freight train going through our home at all hours of the day, perhaps due to the close proximity of the Harley Davidson retail establishment just down the street, regardless, impossible to EVER enjoy a quiet summer morning or evening in our sweet yard. Add to the mind numbing roar of the motorcycles and we have constant noise pollution from the never ending backup of traffic on Folsom Blvd., not to mention the choking toxic exhaust and unhealthy fumes from this staggering amount of congestion of traffic, that truly does not lighten up until maybe 3:00am in the early morning and the quiet only lasts a couple of hours until it all begins again. This brings me to the proposal of building a crematorium on site of the Lakeside Memorial Lawn Cemetery that is not a very big cemetery to begin with. The Preserves is a lovely neighborhood with a beautiful mix of elderly and young and everything in between. I can tell you though, that not many of us can take much more. I have had to accept an Air B&B next door (currently now a Beauty Salon), the homeless trying to set up residence in city owned triangles of tiny land, arrogant and selfish bicyclists that absolutely refuse to respect the numerous "please walk your bikes" signs and fly at breakneck speed through the pedestrian walkway access to the Light Rail Station and to old town Sutter St. I have heard and witnessed many close calls and am terrified of the day when a young child or an elderly will be hit by one of these self centered and entitled individuals that are very

reckless and dangerous, thinking they have found a fabulous shortcut to the bike trail along the river. The crematorium will, needless to say, be the "last straw" for many. I, for one, will not stay. I already ceaselessly worry about the toxic exhaust and dangerous unhealthy fumes from the close proximity to busy Folsom Blvd. It will truly be the one last thing that so many will see as one thing just too much to have to accept and make the best of.

Sincerely,
A quiet lovely senior citizen

From: [Elaine Andersen](#)
To: [Josh Kinkade](#)
Subject: FW: Lakeside crematorium.
Date: Monday, February 7, 2022 7:15:21 AM

From: Heather Hayes [REDACTED]
Sent: Saturday, February 5, 2022 3:00 PM
To: Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; kcolepolicy@gmail.com; danwestmit@yahoo.com; ankhelyi@comcast.net; johnfelts@e55tech.com; mdascallos@yahoo.com; Elaine Andersen <eandersen@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>; thefra@gmail.com
Subject: Lakeside crematorium.

You don't often get email from [REDACTED] [Learn why this is important](#)

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Dear Folsom City Officials,

I have grown to cherish our city with it's well thought out planning and infrastructure. I guess that's why I'm so confused that the city would even consider building a crematorium literally one block from my house. In fact I'm quite frustrated that on a Saturday instead of enjoying time with my 9 month old and teenage sons I'm having to write an email defending what seems like a common sense decision. Building a crematorium one block from my house will not only do psychological harm to myself and my family but will also worsen the air quality in my neighborhood where I am trying to raise my family. We already have some of the worst air quality in the country. With annual wildfires causing horrible conditions every year that on many days it's not even recommended that we go outside. I cannot believe that you would even consider adding to that horrible pollution with the smoke of dead bodies being billowed into my backyard as we try to enjoy our lives in historic Folsom. As we try to BBQ with family and friends, all the while breathing the smoke of the deceased.

Please do the right thing and reject this proposal. Have the crematorium be moved to a remote and rural location where it belongs. Please protect the health and well being of my family and preserve the sanctity of historic Folsom.

Heather Hayes

Dear Mr. Johnson,

I am writing in regard to the proposed crematorium at Lakeside Cemetery. I am a resident of the Preserve neighborhood in Folsom.

My first concern with having a crematorium in my neighborhood is the release of harmful toxins into the air during the cremation process and the long term effects on me and my family. One such example is the mercury from the dental amalgam fillings being vaporized and released into the atmosphere. The emissions of mercury will not be regulated. There is no national standard for mercury emissions from a crematorium. There is no known lower level of toxicity of mercury. As stated in the 2019 Final Senate Crematoria Committee Report of Georgia:

“ while there are emissions of other chemicals during the cremation process, mercury is of the most concern to communities near crematoriums. When mercury is burned, it becomes a colorless and odorless gas that can travel long distances. While mercury exposure has the potential to cause a variety of health problems, the brain and kidneys are especially vulnerable.”

<https://www.senate.ga.gov/sro/Documents/StudyCommRpts/12CrematoriaStudy.pdf>

The following 2020 study from the Canadian Journal of Public Health highlights the fact that crematoriums are a source of air pollution and should be monitored due to cumulative effects.

“This study underlines that the main concern about mercury from crematoriums is not acute exposure to immediately dangerous ground-level concentrations but long-term indirect exposure from the contamination of the environment and the food chain, although more difficult to assess quantitatively. It is important to inform decision-making around environmental permitting and pollution prevention to motivate further regulation. Crematoriums are a source of air pollution as a result of combustion of caskets and human bodies, as well as companion clothing, and should be subject to the regulatory framework covering all sources of air pollutants, notably including mercury. Environmental monitoring of mercury emission from crematoriums is warranted.”

<https://www.ncbi.nlm.nih.gov/pmc/articles/PMC7728964>

Another concern is the cremation of bodies that have had radiation treatment therapy or chemotherapy. If there are no ongoing emissions regulations, how is it determined there is no negative impact to the residents or environment?

A recent study from Journal of American Medical Association (JAMA) reported on an incident of radioactive contamination to the facility equipment and an employee that occurred in Arizona crematorium when a cancer patient was cremated.

“As of 2006 (the most recently reported data), 18.6 million nuclear medicine procedures were performed in the United States, with nearly 40 million performed worldwide.¹ Safety regulations are well established for radiopharmaceutical administration in living patients. However, radiopharmaceuticals present a unique and often overlooked postmortem safety challenge. Cremating an exposed patient volatilizes the radiopharmaceutical, which can then be inhaled by workers (or released into the adjacent community) and result in greater exposure than from a living patient. Regulations for cremation of exposed patients vary by state, as well as internationally, and there are no regulations at the federal level in the United States.”

<https://jamanetwork.com/journals/jama/fullarticle/2725673>

It just seems common sense to not place a crematorium in close proximity to residents, a state park and a lake. Besides the above environmental concerns, there is the concern of fire or accidents at crematoriums. Here are a few examples. Many more can be found with a simple internet search:

Human Remains Spewed into Air After Accident at San Diego Crematorium, May 2018

<https://abc7news.com/crematorium-accident-human-remains-san-diego-county-smoke-with/3519060/>

Investigation Underway After Fire At Sacramento Crematorium, June 2019

<https://sacramento.ebslocal.com/2019/06/11/nor-cal-crematory-south-sac-fire/>

Erie funeral home avoids serious damage from roof fire; cremation suspected as cause

<https://www.goerie.com/story/news/local/2021/11/24/funeral-home-fire-erie-firefighters-respond-roof-fire-potentially-linked-cremation-burton/8747480002/>

Thank you for your consideration, Helen Walsh

**HERITAGE PRESERVATION LEAGUE OF FOLSOM
INITIAL STUDY REVIEW**

February 4, 2022

PROJECT: 1201 Forrest Street, Lakeside Memorial Lawn Cemetery in the State Open Space Area along Lake Natoma (Project File: 19-182)

REQUEST: Design Review including the installation of a crematorium in an existing metal shed and the installation of two freestanding propane tanks adjacent to the shed.
Conditional Use Permit to operate the Crematorium at Lakeside Cemetery.

PROJECT

HISTORY: The Public Review Period for the Initial Study and Mitigated Negative Declaration started on January 7, 2022 and ends on February 7, 2022.

A similar application was processed through the City in 2002 and 2003.
(Project File: 02-258)

BACKGROUND

Lakeside Cemetery was started by the miners at Negro Bar. In the days that followed the early gold rush the state property became a Pioneer Cemetery with a cluster of individual burial sites. As documented by Sacramento County Cemetery Advisory Commission, the cemetery incorporates areas designated to the Jewish community, the Masons, the Oddfellows, the Cook Family, the Citizens of Folsom and a special area reserved for infants and young children. Cremations were not a part of the traditions during these times.

On November 24, 1998, Folsom's City Council approved the Historic Preservation Master Plan. As a part of this document, Lakeside Cemetery was placed on the Preliminary Cultural Resources Inventory of properties and structures eligible for local listing. In 2006, the Masons and Oddfellows cemeteries were processed for listing and added to the City's official Inventory.

THE PREVIOUS CREMATORIUM APPLICATION

When a crematorium addition was proposed for Lakeside Cemetery in 2002, planning staff recommended that the Historic District Commission should deny the Conditional Use Permit based on the age and history of the cemetery. The following issues were raised:

- The cemetery can provide unique information about the early days of Folsom and the various groups of citizen that lived in the City during this time.
- A crematorium was not included with Pioneer Cemeteries and a crematorium addition will impact the historic character of Lakeside Cemetery.
- Sacramento County Historic Cemetery Commission has identified Lakeside Cemetery as locally significant. *(Six individual sub-areas of Lakeside Cemetery are included with Sacramento County Cemetery List.)*

INITIAL STUDY

The Cultural Resources Chapter of the current Initial Study fails to mention that the City of Folsom and Sacramento County considers Lakeside Cemetery a locally significant historic site. It also does not describe the early use of the cemetery by various local groups of citizens.

The mitigation measures for cultural resources (CUL-01, 02, and 03) does not address the expanded use of the cemetery property. Instead these mitigation measures provide standard language for projects that require excavation (including the uncovering of archeological resources and human remains). Because the crematorium project will only require leveling of the ground before a concrete pad is installed (for fuel tanks), these mitigation measures does not apply to the proposed crematorium project.

CONCLUSION

The Lakeside Crematorium project has not changed since the previous application in 2002. The same findings that staff prepared in 2002 to justify a recommendation for denial still applies.

- The Heritage Preservation League of Folsom urges the Historic District Commission to deny the Lakeside Crematorium project.

The Initial Study does not discuss how the existing project will impact the historic significance of the Pioneer Cemetery. Any conclusion that environmental issues can be covered by a Mitigated Negative Declaration is therefore premature.

- The Heritage Preservation League of Folsom recommends that the Historic District Commission deem the Initial Study regarding the Lakeside Crematorium project incomplete.

- Attachments:
1. Findings for Denial PN02-258
 2. Letter from the Chairman of Sacramento County Cemetery Advisory Commission

Attachment 1

Findings for Denial
PN02-258

STAFF RECOMMENDATION

Staff recommends denial of a Conditional Use Permit to operate a crematorium at an existing cemetery at 1201 Forrest Street in the Historic District, based on the following findings.

HISTORIC DISTRICT COMMISSION ACTION

MOVE TO DENY THE CONDITIONAL USE PERMIT TO OPERATE A CREMATORIUM AT AN EXISTING CEMETERY AT 1201 FORREST STREET IN THE HISTORIC DISTRICT.

FINDINGS FOR DENIAL

- A. NOTICE HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE USE APPLIED FOR IS DETRIMENTAL TO THE HEALTH, SAFETY, OR GENERAL WELFARE OF PERSONS RESIDING OR WORKING IN THE NEIGHBORHOOD, AND DETRIMENTAL OR INJURIOUS TO PROPERTY AND IMPROVEMENTS IN THE NEIGHBORHOOD AND THE GENERAL WELFARE OF THE CITY BECAUSE THE INTRODUCTION OF THIS USE WILL IMPACT THE HISTORICAL CHARACTER OF THE EXISTING CEMETERY AND HISTORICAL USE OF THE AREA.
- C. THE CONGLOMERATION OF HISTORIC CEMETERIES, COMBINED WITH THE CALIFORNIA STATE DREDGER TAILINGS DATING BACK TO THE 1850'S, CREATE A RARE COMBINATION OF UNIQUE CULTURAL RESOURCES THAT WILL BE IMPACTED BY THIS PROPOSAL.
- D. THE USE OF THE PROPOSED PROJECT IS NOT CONSISTENT WITH GOAL 2 OF THE DESIGN AND DEVELOPMENT GUIDELINES IN THAT IT DOES NOT MAINTAIN THE HISTORIC USE OF THE SITE. IN ADDITION, THE PROJECT DOES NOT FURTHER DESIGN AND DEVELOPMENT GUIDELINE POLICIES 2.1, 2.2, AND 2.3 IN THAT:
 1. COMMISSIONERS FROM SACRAMENTO COUNTY HISTORIC CEMETERY COMMISSION HAVE IDENTIFIED THIS SITE AS LOCALLY SIGNIFICANT, AND COMMISSIONERS INTEND TO PRESENT LAKESIDE CEMETERY TO THE SACRAMENTO BOARD OF SUPERVISORS FOR DESIGNATION AS AN HISTORIC CEMETERY.
 2. APPROVAL OF A CONDITIONAL USE PERMIT FOR A CONTEMPORARY USE THAT WILL JEOPARDIZE THE ELIGIBILITY STATUS OF THE SITE WOULD WORK TO DISCOURAGE, RATHER THAN TO ENCOURAGE, NATIONAL REGISTER NOMINATION.
 3. BASED ON A LETTER FROM JAMES A. PURCELL, CHAIRMAN OF THE SACRAMENTO COUNTY HISTORIC CEMETERY COMMISSION DATED JANUARY 2, 2003, A CREMATORIUM IS NOT A CONSISTENT USE WITH A HISTORIC CEMETERY.

Attachment 2

Letter from the Chairman of
Sacramento County Cemetery Advisory Commission

Sacramento County Cemetery Advisory Commission
4800 Broadway, Suite 100
Sacramento CA 95820

City of Folsom Historic District Commission
50 Natoma Street
Folsom, CA 95630

Attn: Jane Talbot

Dear Ms Talbot::

The Sacramento County Cemetery Advisory Commission is in receipt of your Notice of Public Hearing regarding PN 02-258 Conditional Use Permit and Mitigated Negative Declaration 1201 Forrest Street.

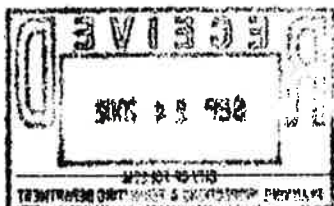
The commission is charged with the duty to encourage the preservation and designation of historical cemeteries. We are currently preparing a list of those cemeteries in Sacramento County, which should be considered historic. It is my opinion that Lakeview Cemetery will be one of the cemeteries in Sacramento County that will be on the list that is to be presented to the Board of Supervisors for designation as an historic cemetery.

While no official action has yet been taken by either The Cemetery Advisory Commission or The Board of Supervisors regarding Lakeview Cemetery we ask that you consider the above mentioned conditional use permit in the context of Lakeview's historic significance and endeavor to preserve its historic elements.

Sincerely,



James A. Purcell, Chairman
Cemetery Advisory Commission



Chair, James A. Purcell

Vice Chair, Dr. Robert La Perriere

Josh Kinkade

From: Kelly Mullett
Sent: Friday, August 27, 2021 8:13 AM
To: Josh Kinkade
Subject: Fwd: Concern for pending Lakeside Memorial Crematorium

FYI

From: Isaac Monical [REDACTED]
Sent: Thursday, August 26, 2021 9:28:54 PM
To: Mike Kozlowski <mkozlowski@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; 1156ykc@folsom.ca.us <1156ykc@folsom.ca.us>; kerri@atlanticcorrosionengineers.com <kerri@atlanticcorrosionengineers.com>; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; thehfra@gmail.com <thehfra@gmail.com>
Subject: Concern for pending Lakeside Memorial Crematorium

Some people who received this message don't often get email from isaacmonical@gmail.com. [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Council members, Planning Commission members, and Residence Association members,

The Lakeside Memorial Lawn crematorium proposal is deeply troubling to me and my family for a number of reasons. The Caring Service Group is not a small business and states outright on their website that they're in the business of buying up and aggregating small funeral home businesses. This model further distances the business from its community's concerns and it shows with the "Initial Study/Mitigated Negative Declaration" dated April 2021.

The TAC (Toxic Air Contaminants) assessment in Appendix B of the "Lakeside Memorial Lawn Crematorium Initial Study/Mitigated Negative Declaration" dated April 2021 is flawed and irresponsible. Most specifically in terms of the evaluation of the impact to "Sensitive Receptors". The report by Helix Environmental Planning notes that the adjacent residential houses are as close as 450 feet which is not that far! That's about the width of 5 or 6 lots in our neighborhood, 5 houses down the street. The report makes it sound like the combustion stack is remotely located away from our neighborhood which couldn't be farther from the truth. I walk the neighborhood regularly with my 1 and 3 year old children and it's a short walk from the closest house to the proposed site which is where my son likes to occasionally ride his balance bike. The Historic District Historical Society write-up of the cemetery (<https://www.folsomhistoricalsociety.org/post/lakeside-cemetery>) concludes with "The next time you are in town I highly suggest you take a visit to the cemetery; the older burial sites are beautiful, and its quiet location makes a perfect Fall afternoon walk." This will certainly change when there are constant emissions adjacent to the cemetery and increased vehicular activity carting the deceased to and from the crematorium "shed".

The report also neglects to recognize that the American River Bike Trail is adjacent to the proposed site where numerous people of all ages including sensitive groups, i.e. elderly and families with young children religiously use the trail. The proposed industrial process does not fit within the open space plan of the adjacent area. It should not be used for a pollution buffer zone, it's a recreation area. While using the trail, the folks that are exercising will be subject to toxic emissions, at times while breathing heavily. The Air Quality Board recommends restricted exertion levels of people when air quality is poor, so to introduce a source of constant emissions renders the trail effectively unusable, especially by sensitive individuals.

The assumption that the meteorological data used from the Sacramento Executive Airport station, almost 20 miles away, which regularly gets the delta breeze where the planned site does not, appears to be laughably unethical. The canyon near the proposed site has significantly different geography. It often has stagnated air that collects along the river trail which can be witnessed when exercising along the trail in the summer and winter months. At these times when the air is perfectly still, combustion gas from leaf blowers from the adjacent business parking lots simply stagnates right on the trail. Trail users should not have to worry about breathing in toxic hydrocarbon and heavy metal combustion products along with new unpleasant smells. The near-zero initial vertical gas velocity assumption of the stack configuration in the source parameters paragraph will only exacerbate the emissions settling issue on the trail and adjacent neighborhood. The 500 meter radius geography sample used in the analysis is not enough to capture the adjacent yet substantial cliffs in the region and could be interpreted to be an attempt to replicate geography near the airport which would also be unethical in terms of being non-representative.

For a city that wants to define itself as distinctive by nature, this is a far cry from the current mission statement. The benefits to the community are substantially outweighed by the safety risk and misuse of the planned site.

I strongly encourage the Folsom City Leadership to reevaluate the applicant's intentions to monetize a currently quaint property with a toxic industrial process directly adjacent to a family oriented neighborhood and world class nature trail.

Sincerely,

Isaac Monical

██████████ Fong Ct
Folsom, CA 95630
██████████

Josh Kinkade

From: Jackson Gates [REDACTED]
Sent: Sunday, January 16, 2022 11:26 AM
To: Danwestmit@yahoo.com; ankhelyi@comcast.net; kcolepolicy@gmail.com; kevin.duewel@gmail.com; m.dascallos@yahoo.com; johnfelts@e55tech.com; Mike Kozlowski; Sarah Aquino; YK Chalamcherla; kerri@atlanticcorrosionengineers.com; rrodriguez@folsom.ca; Elaine Andersen; Josh Kinkade; kkmullet@folsom.ca.us
Subject: crematorium

Some people who received this message don't often get email from jacksongates11@gmail.com. [Learn why this is important](#)

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Hi my name is jackson I am very very sad. Every day me and my neighbor friends play outside and I am not sure if that will be able to continue in the future . I am 10 years old and had my birthday party here. Will I be able to have another birthday party outside? will animals stay in our neighborhood?

Jackson
young wo circle

From: [Steven Banks](#)
To: [Josh Kinkade](#)
Subject: FW: Crematories are located only in industrial and light industrial zoning in other localities . If you feel Folsom should be an exception to this, I suggest you help the Miller group to locate adjacent to Serrano. Jim Tiberti
Date: Tuesday, January 18, 2022 8:05:54 AM

FYI

-----Original Message-----

From: James Tiberti [REDACTED]
Sent: Monday, January 17, 2022 11:55 AM
To: Steven Banks <sbanks@folsom.ca.us>
Subject: Crematories are located only in industrial and light industrial zoning in other localities . If you feel Folsom should be an exception to this, I suggest you help the Miller group to locate adjacent to Serrano. Jim Tiberti

[You don't often get email from [REDACTED]. Learn why this is important at <http://aka.ms/LearnAboutSenderIdentification>.]

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Sent from my iPhone

Josh Kinkade

From: Scott Johnson
Sent: Tuesday, February 1, 2022 12:54 PM
To: Josh Kinkade
Subject: Fwd: Proposed - Lakeside Cemetery Crematorium

Sent from my T-Mobile 5G Device
 Get [Outlook for Android](#)

From: JANICE BRIAL [REDACTED]
Sent: Tuesday, February 1, 2022 12:15:29 PM
To: Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; kcolepolicy@gmail.com <kcolepolicy@gmail.com>; justin@revolutionsdocs.com <justin@revolutionsdocs.com>; danwestmit@yahoo.com <danwestmit@yahoo.com>; ankhelyi@comcast.net <ankhelyi@comcast.net>; johnfelts@e55tech.com <johnfelts@e55tech.com>; m.dascallos@yahoo.com <m.dascallos@yahoo.com>
Subject: Proposed - Lakeside Cemetery Crematorium

You don't often get email from jantrav@aol.com. [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To: The Folsom Historic District Commission

The Caring Service Group and the Miller Funeral Home have applied for a conditional use permit to install a crematorium in an area that is zoned open space in the Folsom Historic District. I am very opposed to their plan of burning hundreds of bodies a year. Burning up to 800 pounds of bodies each day has serious implications for the physical, environmental, and fiscal health of our community.

I live in The Preserve/Lake Natoma Shores (over 100 homes) area which is located next to Lakeside Cemetery. Forrest Street, which is the sole entry point to our neighborhood, is narrow and curved. I have lived here over 25 years and at this point in time, it is frequently challenging to even enter or exit my neighborhood for a variety of reasons: Light Rail increased the traffic problems; The current traffic from our neighbors and visitors to the state park, the Lakeside Cemetery, the Murer House and the VFW cause time-consuming inconveniences that are sometimes dangerous. Please do not approve of the crematorium which will cause more danger and additional traffic.

The crematorium has proposed two, 250 gallon propane tanks above ground, in a wild fire danger area. There have been fires in the state park over the years. Countless people come to our neighborhood daily to walk, run, ride bikes, trikes, boating and to enjoy Lake Natoma. Please do not put those of us who live here and the people that come here to enjoy the state park and lake, in danger by approving the crematorium.

In addition, I am extremely concerned about the air quality if the crematorium is approved. Poor air quality, smell, increased fire risk and toxic particulate matter will cause lasting damage on the physical and environmental health of our community. According to the Sacramento County's Air Quality Management District, the crematorium would be self-regulated, with the County having very little

oversight over the mercury-based emissions. The Initial Study by HELIX Environmental Planning Inc., notates levels for harmful toxins including chromium, mercury, and organics. Our local experts have warned that those toxins are most dangerous when vaporized. The smaller the particulate matter, the more dangerous it becomes. The report designates the level as “not significant.” That only applies to the average, healthy adults. The study does not report significant and potentially deadly levels for unborn children, developing children, elderly, and those with existing health conditions. I am a senior citizen with health issues and there are many young children that live here and many more that come to play in the nearby Preserve Mini Park on Sutter Street.

Please protect Historic Folsom. I am not opposed to a crematorium in a rural or industrial area. I am extremely opposed to a crematorium next to open space, neighborhoods, and a recreational area where people come here to enjoy Lake Natoma and our lovely Historic Folsom.

Sincerely,

Janice Brial
[REDACTED] Sutter St.
Folsom, CA 95630

Josh Kinkade

From: Daron Bracht <daronbr@pacbell.net>
Sent: Tuesday, March 2, 2021 11:09 AM
To: Josh Kinkade; Steven Banks
Subject: Fwd: Proposed - Lakeside Cemetery Crematorium

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----- Forwarded Message -----

Subject: Proposed - Lakeside Cemetery Crematorium
Date: Tue, 23 Feb 2021 12:12:21 -0800
From: JANICE BRIAL [REDACTED]
To: daronbr@pacbell.net

To: Chairman Daron Bracht
 Chairman of the Folsom Historic District Commission

Dear Chairman Bracht,

It is my understanding that there is going to be a meeting about a proposed Crematorium at Lakeside Cemetery, at the Folsom Historic District Commission soon. It has been challenging to try to be up to date about what is going on during the last year of COVID. I am not aware of the developer contacting anyone in my neighborhood about the crematorium. I wonder if they have contacted the state park that is also near by?

I live in The Preserve/Lake Natoma Shores which is located next to Lakeside Cemetery. There are over 100 homes. Also, people using The Murer House, Lakeside Cemetery, the VFW and the Preserve Mini Park only have **one way** to drive in or out of this area on Forrest St. Forrest St. is small, curved and has very narrow parts. I have lived here over 25 years and at this point in time, it is frequently challenging to even enter or exit my neighborhood.

The state park is also next to this neighborhood. We have many people coming through riding their bikes, trikes, scooters, walking and running through our neighborhood. We also have traffic with people driving to Young Wo Circle and parking next to the state park and accessing the state park for biking, kayaking, running, walking, etc.

One frequent problem in trying to get in and out of Forrest Street, is the frequent train traffic. At this point in time, I believe that a train comes and goes to Old Town Folsom every 30 minutes (to and from Old Town Folsom). I have been told that the train traffic is going to be increased to every 15 minutes. When that happens, it will be much more difficult to exit and enter Forrest Street, which is the only way all of the above mentioned drivers can get in and out of the neighborhood.

A driver frequently must wait to exit Forrest St, for the traffic lights to cycle through 2 times, because a train is either going to or coming from Old Town Folsom. All of this is tedious and inconvenient. However, the biggest concern is when there is an actual emergency such as a fire at the state park (which has happened) or a fire or other emergency

anywhere in this area. People could be **trapped** and unable to safely leave the **ONLY EXIT** on Forrest St. It seems only sensible that nothing else should be built in this area when a safe exit does not exist in case of any type of emergency.

Drivers who are familiar with the complicated intersection at Folsom Blvd, Natoma Street/Forrest St. and are attempting to leave Forrest Street, know that there are two very small slots to exit next to Folsom Blvd. The right slot in the lane, is for right turns only. The left slot of the lane, is for turning left on Folsom Blvd. or going straight to Natoma St. Unfortunately, the individuals who are unfamiliar with the intersection frequently sit in the right hand lane slot and block the only exit that does not require a green light to leave. Any increased traffic to the Lakeside Cemetery will cause the intersection to be even more dangerous.

Our hard working Folsom Police Department also uses the entrance at Forrest St. and pulls over vehicles that have been speeding on Folsom Blvd., or breaking the law in other ways. It is rather routine to see one or more vehicles pulled over on Forrest St., opposite the entrance to Lakeside Cemetery. It is crowded and a driver must cross, to drive to the other side of the street, to get by.

I am not aware that the Lakeside Cemetery has notified any of the nearby residents or businesses about the proposed crematorium. We would all be involved in our attempts to exit and enter our neighborhood with any increased traffic. It seems like such a facility should only be installed in an industrial or commercial area, not near a neighborhood and state park.

My other concern is air quality. I am sure there must be safety precautions regarding fumes and dangerous particles going into the air with crematoriums. However, things happen, and sometimes even the best made plans do not work. We already have unsafe days and are sometimes told to not even walk outdoors. We are hundreds of people of all ages from babies to senior citizens. Many of us already have existing asthma, allergies and other breathing issues. Please do not allow a crematorium to be put into this lovely area where people are exercising (walking, biking, running, etc.) in our neighborhood and also going to and from the state park. I moved here to have a healthy life style near beautiful Lake Natoma and charming Old Town Folsom.

Please share my letter with the rest of the Historic District Commission. Please let me know that you received this email.

Thank you for your service.

Sincerely,

Janice Brial
Sutter St.
Folsom, CA

916

Josh Kinkade

From: Scott Johnson
Sent: Wednesday, February 2, 2022 2:21 PM
To: Josh Kinkade
Subject: FW: Vote NO on Folsom Crematorium Proposal

Scott A. Johnson, AICP

Planning Manager

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6206



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From: Jennifer Grattan [REDACTED]
Sent: Wednesday, February 2, 2022 2:20 PM
To: Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; kcolepolicy@gmail.com; danwestmit@yahoo.com; ankhelyi@comcast.net; johnfelts@e55tech.com; m.dascallos@yahoo.co; Elaine Andersen <eandersen@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykachalamcherla@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>
Subject: Fwd: Vote NO on Folsom Crematorium Proposal

Some people who received this message don't often get email from jennifer.a.grattan@gmail.com. [Learn why this is important](#)

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To: City Council Representatives and Folsom Historic District Commissioners

Re:Vote NO on Folsom Crematory Proposal
Public hearing scheduled for February 16, 2022

I am writing to you as a concerned resident in the Folsom Historic District. I am strongly opposed to the conditional use permit to install a crematorium on the grounds of a residential open space. I believe allowing a permanent, industrial incinerator in the residential open space does not meet the criteria of the Folsom Historic District.

I further believe this contradicts the city of Folsom motto, 'Distinctive by Nature'. I urge you to vote NO on the proposed Folsom Crematorium. Please consider the impacts to the environment, physical health of the residents and the fiscal health of the Folsom Historic District.

Please preserve the environment, the physical health of the residents and the community as a whole.

Vote NO on the Folsom Crematory Proposal.

Kindly,

Jennifer Grattan
Historic Folsom Resident

Josh Kinkade

From: Steven Banks
Sent: Friday, February 4, 2022 10:20 AM
To: Josh Kinkade
Subject: FW: NO crematorium

FYI

-----Original Message-----

From: jeri livesay [REDACTED]
Sent: Friday, February 4, 2022 10:00 AM
To: Steven Banks <sbanks@folsom.ca.us>
Subject: NO crematorium

[You don't often get email from [REDACTED]. Learn why this is important at <http://aka.ms/LearnAboutSenderIdentification>.]

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Please vote NO....our air quality does not need gramma and grampas ashes floating around.....vote NO NO NO

Sent from my iPad

Josh Kinkade

From: Kelly Mullett
Sent: Friday, February 4, 2022 10:05 AM
To: Josh Kinkade
Subject: FW: NO crematorium

-----Original Message-----

From: jeri livesay [REDACTED]
Sent: Friday, February 4, 2022 10:02 AM
To: Kelly Mullett <kmullett@folsom.ca.us>
Subject: NO crematorium

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Please vote NO.....our air quality is already compromised enough throughout the year....we don't want this.....vote
NO NO NO

Sent from my iPad

Josh Kinkade

From: Scott Johnson
Sent: Friday, February 4, 2022 9:58 AM
To: Josh Kinkade
Subject: FW: No crematorium

Scott A. Johnson, AICP
Planning Manager
Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6206

<https://gcc02.safelinks.protection.outlook.com/?url=http%3A%2F%2Fwww.folsom.ca.us%2F&data=04%7C01%7Cjkinkade%40folsom.ca.us%7Cdbce6f94165b412ec21408d9e807e46b%7C1cfb4b4a254c47b48448af71335fd6c0%7C0%7C0%7C637795942874787606%7CUnknown%7CTWFpbGZsb3d8eyJWIjoiMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTiI6IjEhaWwiLCJXVCi6Mn0%3D%7C3000&psdata=65kuXCKzqzk1GDwwL5DX6tmMNN4ziVZS4VrOgLG6IB2Q%3D&reserved=0>

-----Original Message-----

From: jeri livesay [REDACTED]
Sent: Friday, February 4, 2022 9:57 AM
To: Scott Johnson <sjohnson@folsom.ca.us>
Subject: No crematorium

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Please vote NO.....our air quality cannot have ashes.....NO NO NO Sent from my iPad

Josh Kinkade

From: Elaine Andersen
Sent: Friday, February 4, 2022 12:30 PM
To: Josh Kinkade
Subject: FW: NO crematorium

-----Original Message-----

From: jeri livesay [REDACTED]
Sent: Friday, February 4, 2022 10:08 AM
To: Elaine Andersen <eandersen@folsom.ca.us>
Subject: NO crematorium

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Please vote NO.....our air quality is already compromised we don't need even more issues with ashes hindering our oxygen.....vote NO NO NO Sent from my iPad

Josh Kinkade

From: Scott Johnson
Sent: Wednesday, February 2, 2022 1:14 PM
To: Josh Kinkade
Subject: Fwd: The Preserve Crematorium

Sent from my T-Mobile 5G Device
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From: Jessica Foster [REDACTED]
Sent: Wednesday, February 2, 2022 12:35:13 PM
To: Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; kcolepolicy@gmail.com <kcolepolicy@gmail.com>; justin@revolutionsdocs.com <justin@revolutionsdocs.com>; danwestmit@yahoo.com <danwestmit@yahoo.com>; ankhelyi@comcast.net <ankhelyi@comcast.net>; johnfelts@e55tech.com <johnfelts@e55tech.com>; m.dascallos@yahoo.com <m.dascallos@yahoo.com>; Elaine Andersen <eandersen@folsom.ca.us>; kerri@atlanticcorrosionengineers.com <kerri@atlanticcorrosionengineers.com>; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; Mike Kozlowski <mkozowski@folsom.ca.us>
Subject: The Preserve Crematorium

You don't often get email from arose4jess@yahoo.com. [Learn why this is important](#)

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Hi there,

I am writing to you as a plea to vote "no" on the crematorium proposal in the residential Preserve neighborhood of Historic Folsom. My mom lives there and we frequently visit her with our small children, especially in the Spring and Summer when the weather is nice and we can play outside at her house and nearby the river.

A crematorium doesn't belong there. The noise of machinery, the increase of cars in and out of the neighborhood, AND the pollution in the air will devastate the livelihood of the people who live and visit there.

A crematorium should be placed in an industrial area. Please vote in favor of the residents who need you to protect their neighborhood.

Thank you,

Jessica Grob

[Sent from Yahoo Mail for iPhone](#)

From the desk of Joan Boyle

I am writing to you today because it has come to my attention that Lakeside Memorial Lawn Cemetery is working to put a crematorium on the property located at the intersection of Folsom Blvd and Forrest St. The **crematorium** will be housed in a **shed**, serviced by **two 250-gallon propane tanks**, and have a **smoke stack** installed for ventilation.

Lakeside Memorial Lawn Cemetery is part of the Miller Funeral Home which is owned by Caring Services Group. When looking at Caring Services Group's website their opening statement is, and I am quoting here, "**Caring Services Group was established in 2010 with a primary goal of purchasing Funeral Home businesses.**" With this as their primary goal, they do not appear to be a family-owned business with the best interests of the community at heart, but more a corporation that only cares about their bottom line. I attended a Zoom meeting where Igor Semenyuk, the Chief Operations Officer for Caring Services Group, was speaking on the topic. When asked why they wanted to place the crematory in the Lakeside Memorial Lawn he said something to the effect that 'the location is cost effective'. Again, no regard for the community or it's people, just their bottom line.

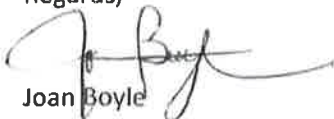
Lakeside Memorial Lawn is currently designated as Open Space which excludes it from operating a crematorium. The owners are applying for a Conditional Use Permit to change that. This is very concerning for a number of reasons, including but not limited to:

- **Toxicity levels** – harmful toxins such as chromium, mercury and organics become most dangerous when vaporized. While the initial report by HELIX Environmental Planning, Inc. notes the levels as "not significant" this applies to only average healthy adults. The study fails to report the effect on unborn children, developing children, the elderly and those with existing health conditions.
- **Air Quality** – poor air quality, smell and toxic particulate matter will cause lasting damage on the physical, environmental and fiscal health of our community.
- **Fire Danger** – two 250 gallon above ground propane tanks pose a real threat to the area. The proposed crematory site is located within a wildfire danger area. It is also adjacent to the American River Bike Trail, Lake Natoma and a residential neighborhood. If there were to be a wildfire or vandalism, the propane tanks could cause untold damage to the area.
- **Traffic** – there is only one ingress and egress to the area. This intersection of Folsom Blvd and Forrest St is already compromised by the light rail train interruptions. Additional business, or the need for an emergency evacuation could be very problematic.

Please deny the Conditional Use Permit to operate a crematorium at this location.

In closing, I am not opposed to a crematorium. But it should be placed in an appropriately zoned area, not in Historic Folsom, adjacent to the American River Bike Trail and Lake Natoma.

Regards,


Joan Boyle

From the desk of Joan Boyle

It is clear from this picture that the shed, actually both of them, are visible from Folsom Blvd. How can the report conclude that "Tailing piles between the site and Folsom Boulevard prevent the site from being visible from that street."? That is not a true statement. You can clearly see the shed, and the smokestack is not even installed yet. That's at least another 10 feet in the air, but the report says the smokestack was taken into consideration. How can the report be trusted?

Second:

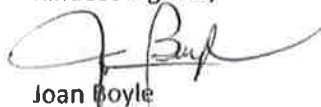
- **In Section 9.0 Environmental Initial Study Checklist Subsection XVII. Transportation** it states "As the crematorium would not be located in or near a funeral home and would be separate from any funeral services or public gatherings provided by the project applicant, access would only need to accommodate a small number of staff members with a business at the site." While in the **Lakeside Memorial Lawn, Scope of work – Installing a Crematory – Point at Issue** it states "There are several large cultural communities residing and moving into Folsom. Regretfully, we do not have the ability nor the capability to serve the Sikh, Hindu, Buddhist or other cremation-based cultures as they require an on-site crematory so that they are able to exercise their funeral rights and customs. Currently there is no crematory in the city of Folsom so they must find the services outside the city."
 - Is the proposed crematorium going to be opened the public so "the Sikh, Hindu, Buddhist or other cremation-based cultures as they require an on-site crematory so that they are able to exercise their funeral rights and customs", or is it just open to a "small number of staff members"?
 - If it is the former, will the Initial Study be updated to reflect that? I have reached out to the consultant to ensure clarity on this

I have yet to hear back from Josh on this topic. But, if the primary purpose of the Conditional Use Permit is to have "the capability to serve the Sikh, Hindu, Buddhist or other cremation-based cultures as they require an on-site crematory so that they are able to exercise their funeral rights and customs" but, as the Mitigated Negative Declaration says, only a few workers will be on site, then there's no need to put the crematorium at the cemetery. It should be located in a properly zoned area. At best these are two completely contradictory statements. It appears to me that this issue needs to be resolved prior to voting on the request.

I understand this is a difficult decision you are championed to make. I implore you to consider the nature of Historic Folsom, the American River Bike Trail, Lake Natoma, as well as the welfare of the residents when making your decision.

Thank you for your time.

Kindest regards,



Joan Boyle

From the desk of Joan Boyle

Hi. Let me introduce myself, my name is Joan Boyle and I am a resident of the Preserve neighborhood. I am writing to you today regarding the upcoming vote on the proposed crematorium at the Lakeside Memorial Lawn Cemetery.

I have spent some time reading through documents from both Caring Services Group and the City of Folsom, including The Scope of Work and the Initial Study/Mitigated Negative Declaration, and I have some concerns and questions. I sent a number of questions to Josh Kinkade, the Project Planner at the City of Folsom, regarding statements and conclusions made in the Initial Study/Mitigated Negative Declaration. Below I have highlighted two of my questions that are particularly concerning to me (Josh's responses are in red).

First:

- **In Section 9.0 Environmental Initial Study Checklist Subsection I. Aesthetics** it also states "Tailing piles between the site and Folsom Boulevard prevent the site from being visible from that street." Note: Attached is a photo showing that the shed (even without the smokestack) is visible from Folsom Blvd.
 - Was the installation of the smokestack taken into account when this statement was made? **Yes**
- **In Section 9.0 Environmental Initial Study Checklist Subsection I. Aesthetics c)** concludes "Less Than Significant Impact". I would argue that the view of a smokestack in a Historic cemetery, adjacent to the American River Bike Trail and a residential neighborhood is significant.
 - What criteria was used to determine the impact is "Less Than Significant"? **It was determined that the smokestack would be adequately screened by existing walls and landscaping. If the commission would want additional screening, they can add that to the conditions of approval as part of their decision on whether to approve the project.**



Josh Kinkade

From: Joan Boyle [REDACTED]
Sent: Monday, January 17, 2022 3:47 PM
To: Josh Kinkade
Cc: Joan Boyle
Subject: Lakeside Memorial Lawn Crematory Initial Study questions
Attachments: Shed - Folsom Blvd.jpg; Shed - Forrest St.jpg

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Dear Josh,

Hi. Let me introduce myself, my name is Joan Boyle and I am a resident of the Preserve neighborhood. It has come to my attention that Lakeside Memorial Lawn Cemetery is working to put a crematorium on the property located at the intersection of Folsom Blvd and Forrest St. I am reviewing the Initial Study/Mitigated Negative Declaration to try and understand the details of the proposal. I have a few questions. This is all very new to me, so I apologize in advance if my questions are basic.

Just for background, I did attend a Zoom meeting where Igor Semenyuk, the Chief Operations Officer for Caring Services Group, was speaking on this topic. I mention this, because in some of my questions I will be referencing things Igor said in that meeting.

Okay so, here we go...

- **In Section 11.0 Initial Study Preparers** it states that you are one of the preparers of the document along with HELIX Environmental Planning Inc. and ECORP Consulting, Inc. But Igor said over and over again in the Zoom meeting that he paid \$30,000 for the report.
 - Who prepares the report?
 - Who pays for the report?
 - If Caring Services Group is paying for the report how is impartiality achieved in determining the conclusions presented in the report?
- **In Section 3.3 Project Characteristics** in the Initial Study dated April 2021 it states "The applicant anticipates one or two cremations on most business days (Monday through Friday) and expects that the total will not exceed 500 cremations per year." while in the Initial Study dated January 2022 it states "The applicant anticipates 1 - 4 cremations on business days (Monday through Friday) with the total number of cremations not exceeding 500 per year."
 - Why was this verbiage changed?
 - Who requested this verbiage change?
- **In Section 3.3 Project Characteristics** it states that "The applicant anticipates 1 - 4 cremations on business days (Monday through Friday) with the total number of cremations not exceeding 500 per year." But in the Zoom meeting Igor commented that it could be up to 5 per day.
 - Is this Initial Study/Mitigated Negative Declaration study and all its subsequent findings (including Air Quality), based on 1, 2, 3 or 4 cremations a day?

- Who monitors the number of cremations performed daily to ensure the total number of annual cremations do not exceed 500?
- **In Section 3.3 Project Characteristics** it also states that HCT Apex-250 crematory will be installed in a shed, and “The shed would be modified to accommodate this device, but the shed’s footprint would not be expanded.”
 - What modification(s) are being made to the shed?
- **Section 3.3 Project Characteristics** it also states that “Two 250-gallon propane tanks would be installed on a proposed concrete pad along the northern side of the shed to provide power for the crematorium, as no gas lines currently exist on the property.” In Appendix A it states “(N) 2 X 500 gallon propane tanks”
 - What is the size of the propane tanks 250 gallons or 500 gallons?
- **In Section 9.0 Environmental Initial Study Checklist Subsection I. Aesthetics** it states “A small exhaust stack would be added to the roof of the shed. The stack would be approximately 19.5 feet above grade, and would project approximately 10 feet above the existing roof of the shed.”
 - This statement uses the words **small** and **approximately**. Why is this statement so vague?
 - Shouldn’t Caring Services Group be able to provide specific enough information to calculate exactly how tall the smokestack would be, and provide that information to the writer of this report?
 - Example: If the smoke stack is installed 19.5 feet above grade and the shed is a single story building of approx. 12 feet, won’t the stack be more realistically about 15 feet above the existing roof? Doing the math - an 8 foot crematory (estimate) plus 19.5 foot smoke stack, minus a 12 foot tall shed equals 15.5 feet. I know it doesn’t seem like much, but aesthetically when you live in the neighborhood, a 15 foot smokestack towering above a 12 foot shed is much different than a ‘small’ smokestack.
- **In Section 9.0 Environmental Initial Study Checklist Subsection I. Aesthetics** it also states “Tailing piles between the site and Folsom Boulevard prevent the site from being visible from that street.” Note: Attached are two photos showing that the shed (even without the smokestack) is visible from Folsom Blvd as well as Forrest St.
 - Was the installation of the smokestack taken into account when this statement was made?
- **In Section 9.0 Environmental Initial Study Checklist Subsection I. Aesthetics c)** concludes “Less Than Significant Impact”. I would argue that the view of a smokestack in a Historic cemetery, adjacent to the American River Bike Trail and a residential neighborhood is significant.
 - What criteria was used to determine the impact is “Less Than Significant”?
- **In Section 9.0 Environmental Initial Study Checklist Subsection III. Air Quality** there is so much information in this section, so my questions on this section are very general.
 - The initial report by HELIX Environmental Planning, Inc. notes the levels as “not significant”. It is my understanding that this applies to average healthy adults. Is that true?
 - If so, does the study have findings that report the effect on unborn children, developing children, the elderly and those with existing health conditions?
- **In Section 9.0 Environmental Initial Study Checklist Subsection XVII. Transportation** it states “As the crematorium would not be located in or near a funeral home and would be separate from any funeral services or public gatherings provided by the project applicant, access would only need to accommodate a small number of staff members with a business at the site.” While in the **Lakeside Memorial Lawn, Scope of work – Installing a Crematory – Point at Issue** it states “There are several large cultural communities residing and moving into Folsom. Regretfully, we do not have the ability nor the capability to serve the Sikh, Hindu, Buddhist or other cremation-based cultures as they require an on-site crematory so that they are able to exercise their funeral rights and customs. Currently there is no crematory in the city of Folsom so they must find the services outside the city.”

- Is the proposed crematorium going to be open the public so “the Sihk, Hindu, Buddhist or other cremation-based cultures as they require an on-site crematory so that they are able to exercise their funeral rights and customs”, or is it just open to a “small number of staff members”?
- If it is the former, will the Initial Study be updated to reflect that?

Thank you in advance for your time. If you have any questions please feel free to contact me at [REDACTED]

Kind regards,
-Joan Boyle



Josh Kinkade

From: Kelly Mullett
Sent: Friday, August 27, 2021 8:59 AM
To: Josh Kinkade
Subject: FW: Crematorium

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



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From: Joanne Dudgeon [REDACTED]
Sent: Friday, August 27, 2021 8:53 AM
To: Kelly Mullett <kmullett@folsom.ca.us>
Subject: Crematorium

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My husband and myself, Stephen and Joanne Dudgeon are against the crematorium being considered for the Folsom area. Please think about the children and citizens of our community and how it will adversely affect us.

We vote NO for this crematorium.

Joanne Dudgeon
[REDACTED] Crestridge Lane
Folsom, Ca. 95630

Josh Kinkade

From: Scott Johnson
Sent: Saturday, January 29, 2022 12:14 PM
To: Josh Kinkade
Subject: Fwd: Proposed Crematorium, please read

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From: Joy Hays [REDACTED]
Sent: Saturday, January 29, 2022 11:47:24 AM
To: Scott Johnson <sjohnson@folsom.ca.us>
Cc: Joy Hays [REDACTED]
Subject: Proposed Crematorium, please read

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Dear Planning Manager Scott Johnson:

I am writing again today as a very concerned citizen and resident of Historic Folsom. The issue is the proposed crematorium at Lakeside Cemetery near the intersection of Folsom Boulevard and Forrest Street. Because the Commission represents all of the people of Historic Folsom, I urge you to deny the building of the proposed crematorium at this location.

My concerns include fire safety and air quality. The City of Folsom has signed agreements. Because the City needed a certain amount of Open Space, Lakeside Cemetery was zoned as **Open Space**. This seemed fitting in that the Cemetery adjoins an historic Chinese heritage cemetery, and the American River Bike Trail. Access to our adjacent Preserve residential community is limited, with one way in, and one way out. Fire engine access to the neighborhood can be difficult. It is fitting that the City of Folsom signed the document declaring our whole area as a **high-risk fire zone**.

Our neighborhood hosts a trailhead to the American River Bike Trail, and Lake Natoma. The recreational opportunities of both bring many people to our neighborhood. (And money to the local businesses.) Biking or hiking along the trail, one can see that the dead wood from the drought and heavy winds is everywhere. Some clearing has been done, but still much tinder remains. **I encourage you to come walk or bike this area.** Right now, we enjoy green grass. But from early spring through what we hope will be a rainy season, the grasses are extremely dry, tinder waiting to be ignited. Individually we pray that no one drops a match, or all of this area will be up in flames. It is common knowledge that we are in, and presumably will stay in a **major drought**. This **potential flammability** is a reality which intensifies each season.

Imagine the residents' concerns when we heard of the project which will put a blast furnace and two propane tanks above ground in the middle of all this! And with no additional water lines! The cemetery management seems to think this proposal is a done deal. No one plans for an **"accident"**

to happen, but we all know that accidents do happen, and then questions are asked, “**Who approved such a thing?!**” Should just one of these tanks ignite, the blast range is 1800 feet, and the resulting fires could wipe out most of Historic Folsom. Do you want the loss of life, in addition to property, both residential and commercial, attached to **your** approval of such a risky project?

Air quality is another concern. Last summer, we all suffered from forest fire smoke, (notably smoke trapped in our Preserve neighborhood) to the point that we were advised we should not leave our houses. Because of what smoke does to our lungs, and to our children’s health, we were hesitant to go shop or eat at, the multiple Folsom small businesses that depend on locals’ patronage to survive. This economic challenge for our merchants was in addition to Covid slowdowns and closures. Do we need to add to the economic issues by building a crematorium walking distance away? Smoke of human remains does escape a crematorium, and cremating obese human bodies emits a dark smoke that smells horribly. The wind will carry these fumes to all of Historic Folsom. Air quality affects both human and fiscal health of our Historic Folsom. Would **you** choose to eat outside at a restaurant on Sutter Street and breathe burning bodies, or drive minutes and eat at the Palladio?

An additional concern is that the cemetery management has expressed an entitled **attitude by starting construction before the approval has been issued.** Might that attitude extend to **safety issues?** Lakeside Cemetery originally projected an average of one cremation a day, **now doubled!** but what can stop the increase to **multiple cremations per day?** When asked about permits, the reply was “Oh, the city will take care of that.” Does Lakeside Cemetery speak for the Historic Folsom Commission? Does it speak for the City of Folsom? Do we need more air pollution in this area, let alone **escaping fumes of human remains? No, we do not.**

I do not oppose a crematorium, but it **must not be built** in this **Open Space and High-Risk Fire Zone.** It should be approved and built in an **industrial area** with adequate access to **water, fire prevention and containment.** Thank you for your time and your attention to this very important issue.

Sincerely,

Joy Hays

██████ Young Wo Circle

Folsom, CA 95630

January 29, 2022

cc: sjohnson@folsom.ca.us

Historical Commissioners
 To June Chan, Last of Folsom's Chinese goldrush Chinese Community (Chinatown's Mayor Oak Chan) Oak Chan
 From original gold rush residents (Chinatown's Mayor Oak Chan) Oak Chan
 Represents elementary school (Prewitt street's name sake school). I Represent My

NO LAKESIDE MEMORIAL LAWN CREMATORIUM!

entire Family alive, still living and dead and buried in Folsom CA

Issue summary and background: Lakeside Memorial Lawn has applied for a conditional use permit to install and operate a crematorium on designated open space. This has significant negative implications for the physical, environmental and fiscal health of our community.
Issue petitioned for: We, the undersigned, are highly concerned citizens who urge our leaders to act now to vote 'NO' on the permit application.

Petitioner Name	Signature	Address	Comment	Date
June Chan and the Oak Chan Family	June C. Chan June C. Chan	140 Fargo way Folsom, Ca 95630	It would enter an endangered both Chinese	

Gold Rush
 a historical and sacred-religious site designed to honor the 50,000 Chinese gold miners who worked in the area.
 NO CREMATORIUM
 My mind has not changed since the 19th and 21st century. How much more must these early days Chinese gold miner Memorial Cemeteries suffer. Unfairly respect fully in memory of the city of Folsom the 3000 Chinese Crematorium for my own family & relatives.
 I am all on the Lake side.
 5/10/22

Josh Kinkade

From: Adam and Katie Musfelt [REDACTED]
Sent: Thursday, January 27, 2022 3:56 PM
To: danwestmit@yahoo.com; kcolepolicy@gmail.com; kevin.duewel@gmail.com;
m.dascallos@yahoo.com; johnfelts@e55tech.com; Mike Kozlowski; Sarah Aquino; YK Chalamcherla;
kerri@atlanticcorrosionengineers.com; Rosario Rodriguez; Elaine Andersen; Josh Kinkade; Kelly
Mullett
Subject: Crematorium

[You don't often get email from akmusfelt@yahoo.com. Learn why this is important at <http://aka.ms/LearnAboutSenderIdentification>.]

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Commission and Council Members,

Folsom is a second home to me; I spend a lot of time there as I have many life-long friends, including my children's godparents, who live in the housing development next to Lakeside Cemetery. Some of the things I love most about Folsom are the trails along the river and quintessential Sutter Street. When visiting, we are almost always outside enjoying the fresh air, kind people and beautiful scenery that Folsom has to offer. During the pandemic we have celebrated numerous events outdoors including backyard birthday parties, playdates in the little park, baby showers, and family gatherings where we would take walks on the trails and to Sutter Street to pick up food. It is a unique city rich with history and charm, that would be greatly diminished by the establishment of a crematorium at the cemetery.

It would be devastating if this crematorium is put in. I can't imagine anyone would want to visit the resting place of their loved ones at the cemetery, while breathing in the smell of the crematorium. None of us would want to gather outside together or let our children play at the playground when it's a burn day. The people who walk and bike on the trails and eat at the restaurants on Sutter Street would disappear on these days as well. People will stop visiting Folsom.

Ask yourselves how would you feel if you or your loved ones lived or owned a business next to a crematorium? Our quality of life will have a significant negative impact if this crematorium is allowed to be built; families and businesses will suffer repercussions for years to come. Please keep letting us live life and enjoy the clean air in the neighborhood, on the trails, and when spending time on Sutter Street.

Thank you,

Katie Musfelt

Josh Kinkade

From: Dave Higgins [REDACTED]
Sent: Tuesday, July 20, 2021 7:31 AM
To: danwestmit@yahoo.com; daronbr@pacbell.net; ankhelyi@comcast.net; kcolepolicy@gmail.com; kevin.duewel@gmail.com; m.dascallos@yahoo.com; johnfelts@e55tech.com; Mike Kozlowski; Sarah Aquino; YK Chalamcherla; kerri@atlanticcorrosionengineers.com; rrodriquez@folsom.ca.us; Elaine Andersen; Josh Kinkade
Subject: Proposed Lakeside Memorial Lawn Crematorium

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July 20, 2021

NO.... to the Crematorium

My Name is Kim Higgins. I live directly across from the entrance of Lakeside Memorial Lawn. My backyard and kitchen window is in direct sight of the cemetery driveway. I believe, with a crematorium at Lakeview we will receive the most impact from the increased use of the facility.

Memorial Lawn has been a good neighbor. My husband and I enjoy the open space and the wildlife that frequents there. We respect it. The Preserves Neighborhood is very small, unique and quaint. We have a park for children and a beautiful lake with eagles and diverse wildlife, hiking and bike trails, and a short walk to downtown Historic Folsom. It's all a gem.

However, it is a busy place. Veterans Hall, Muir House, Chinese Cemetery, Access to Lake Natomas, our park and playground brings with it alot of traffic. Not to mention the endless UPS, Fedex, Amazon and delivery trucks to the Vets Hall. We have only one way in and out of this neighborhood.

My husband and I purchased our home three years ago. During those years we encouraged our adult children with their families to move into the Preserves Neighborhood. Our lives with our kids and grandkids is a dream come true. With the proposal of the Crematorium at the cemetery it has become a game changer to the quality of life we have become accustomed to. Members of my family suffer from severe asthma and allergies. I am very concerned for their well being.

I watched the Zoom meeting presented by HFRA on June 20. Igor Semenyuk and Peter Hartwick explained that we won't know when a cremation is taking

place. Mr. Semenyuk stated all we might see is an "initial puff of white smoke" and heat waves. Mr. Hartwick stated there won't be any smoke or smell. I am concerned that there is still going to be particulate matter in the air.

In the meeting Mr. Semenyuk guesstimated a possibility of 2 to 3 bodies a day. What would the number of cremations be in the future with our current population growth. More cremations, more service trucks, more emissions from more smoke stacks?

I realize Miller Funeral Home is a business. It makes good business sense to them to put a crematorium there. But a crematorium does not belong in a residential neighborhood. I am sure they can find a great location in a commercial-industrial area where they can operate day and night.

Should this crematorium get installed I will be looking at the American flag that flies in the cemetery to see which way the wind is blowing. This will dictate what type of day to expect. Will we be inside or outside? I suspect our quality of life will be diminished during operating hours because we will be trapped in our homes for health reasons.

I believe our property values will be affected by having a crematorium so close. According to an article from "Applied Economics Magazine" by Mark Agee, dated June 19, 2008, "Data Spans 27 months of house sales: 7 months before and 20 months after the startup of crematory operations. Results indicate that proximity, measured in terms of direction and distance from the crematory, imparts a statistically significant negative impact on average house sale prices - an increase of 0.3% to 3.6% of average sale price for every tenth mile increase up to a half mile in distance from the crematory."

California disclosure law requires us to disclose. Not many people desire to live near one. Do you?

We love our small unique neighborhood. The Preserves Neighborhood is where we are planning to spend the rest of our lives. All our family is here. Our quality of life will be affected if the crematorium goes forward. Honestly, would you want this in your backyard?

Thank You
Kim Higgins

To whom it concerns,

Folsom "Distinctive by Nature". While we move deeper into the twenty first century we have become more aware of the choices we must make to protect the future. Small choices might not make immediate impacts but we understand that the small choices we make today add up to the large impacts we seek.

I am writing regarding the proposed crematorium planned at lakeside memorial lawn cemetery. Many people will have written to you about the theoretical risk associated with the carcinogens living near one. While my concern is about protecting the future and we have a small chance to make a meaningful change today. At the same time eliminating all risk associated with a crematorium.

Reverend Desmond Tutu provided direction on this matter. Upon his recent death he chose to use aquamation to promote green burial. Reducing greenhouse gases by 90% associated with normal cremation. Some estimates state that each time aquamation is used it is the equivalent of taking a car off the road for 1000 miles in co2 creation. As cremation becomes closer to being the standard choice in California these numbers start to add up.

Some might be concerned about water use. Per one manufacture website each use requires about 100 gallons of water. We are in the process of adding 10,000 homes which per your own literature uses on average of 600gallons of water a day. So this extra amount used would be the equivalent of one home if used to full capacity.

I actually would prefer that the city of Folsom would choose to not allow any crematorium projects moving forward and encourage the use of this proven technology. In fact we already have an aquamation facility in Folsom. Sacramento Pet Aquamation is located on Bidwell provides this service for residents cherished pets.

Now is the time to make this choice. It is much harder to change directions on this path once you allow the crematorium to be put in operation. The decision to preferably not allow a crematorium in the city of Folsom and encourage the use of green burial technology is not something to make a decision on in two decades. It is today.

Thank you,
Kyal Von Gunten
1335 Fong St

To whom it concerns,

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Thank you,
Kyal Von Gunten
[REDACTED] Fong St

Josh Kinkade

From: Karen Sanabria
Sent: Friday, August 6, 2021 7:42 AM
To: Josh Kinkade; Kelly Mullett
Subject: FW: CREMATORIUM

FYI

From: Kyal VonGuten [REDACTED]
Sent: Thursday, August 5, 2021 7:52 PM
To: Karen Sanabria <ksanabria@folsom.ca.us>
Subject: CREMATORIUM

You don't often get email from kyalpv@gmail.com. [Learn why this is important](#).

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August 4, 2021

TO: City Council Members
 Historic District Commission
 Planning Department - Scott Johnson

My name is Kyal VonGuten. I live next to Lakeside Memorial Lawn on [REDACTED] Fong St.

Lakeside Memorial Lawn proposed crematorium is 460 feet from the closest residence. The majority of the homes are on Young Wo Circle.

I have read the assessment report from Helix Environmental Planning, appendix "B" and I have anxiety about what will occur in my neighborhood.

In the "Air Quality and Greenhouse Gas Emissions Assessment", appendix "B" written by Helix Environmental Planning, page 5, under "**Sensitive Receptors**", they state the closest residences are 450 feet. They are correct, there are nine homes on Young Wo Circle that fall within the distance of 450 feet. From there it fans out to all of the Preserves Neighborhood.

"Sensitive Receptors" Corporate speak for people, people with homes, families and lives.

According to an article by Julliette O'keeffe, dated march 24, 2020, "Crematoria Emissions and Air Quality Impacts". page 4, item 3

What is standard practice for siting of crematorium in proximity to residential areas?

Table 1 identifies the many factors affecting emissions from crematoria. Ground level concentrations can also be affected by local prevailing wind direction and topography. In North America, there are no standard requirements for crematoria setback distances and no minimum separation distances are set at a federal level in either the US or Canada. Crematoria are regulated at the provincial/territorial level and regional or municipal authorities determine whether minimum setbacks are required based on relevant planning and environmental considerations.

The literature search for public agency resources and the grey literature identified many different practices, with some selected examples from around the world listed in Table 3.

Table 3. Selected example setback distances for crematoria from around the world

Country/Jurisdiction	Minimum distance
England and Wales (UK Cremation Act) ⁴⁶	200 yards (183 m) between a crematorium and any dwelling house and 50 yards from a public highway to protect residents from nuisance smoke and fumes and provide privacy to funeral proceedings
West Australia ⁴⁷	200-300 m between crematoria and sensitive land uses
South Australia and the Australian Capital Territory ^{48, 49}	150 m minimum separation distance
South Africa, Department of Health ⁵⁰	500 m from any habitable building
US (Sacramento County, California) ⁵¹	500 feet (152 m) from any agricultural-residential, residential, or interim residential zoning district

Back to the Helix Report. On page 2 of the document, under Air Quality, it explains the "Environmental Setting" of Folsom. This is important data.

AIR QUALITY ANALYSIS - Environmental Setting

The City of Folsom lies within the Sacramento Valley Air Basin (SVAB), near the southeastern edge. The SVAB consists of all or parts of eleven counties spanning from Solano and Sacramento counties to the south, and Shasta County to the north.

The Sacramento Metropolitan Air Quality Management District (SMAQMD) is responsible for implementing emissions standards and other requirements of federal and state laws for Sacramento County, including the project area. The climate of the SVAB is characterized by hot dry summers and mild rainy winters. During the year the temperature may range from 20 to 115 degrees Fahrenheit with summer highs usually in the 90s and winter lows occasionally below freezing. Average annual rainfall is about 20 inches with snowfall being very rare. The prevailing winds are moderate in strength and vary from moist breezes from the south to dry land flows from the north. The mountains surrounding the Sacramento Valley create a barrier to airflow, which can trap air pollutants in the valley when certain meteorological conditions are right and a temperature inversion (areas of warm air overlying areas of cooler air) exists.

Air stagnation in the autumn and early winter occurs when large high-pressure cells lie over the valley. The lack of surface wind during these periods and the reduced vertical flow caused by less surface heating reduces the influx of outside air and allows pollutants to become concentrated in the air. The surface concentrations of pollutants are highest when these conditions are combined with

increased levels of smoke or when temperature inversions trap cool air, fog and pollutants near the ground.

The ozone season (May through October) in the SVAB is characterized by stagnant morning air or light winds with the breeze arriving in the afternoon out of the southwest from the San Francisco Bay. Usually the evening breeze transports the airborne pollutants to the north out of the SVAB. During about half of the days from July to September, however, a phenomenon called the "Schultz Eddy" prevents this from occurring. Instead of allowing for the prevailing wind patterns to move north carrying the pollutants out of the valley, the Schultz Eddy causes the wind pattern and pollutants to circle back southward. This phenomenon's effect exacerbates the pollution levels in the area and increases the likelihood of violating the federal and state air quality standards (SMAQMD 2020a)

This data is important to note as pollutants, particulate matter, ozone and smoke would most likely lay within the neighborhood because of the elevation decrease to Lake Natomas.

Regulatory Setting - Criteria Pollutants

As permitted by the Clean Air Act, California has adopted the more stringent California ambient air quality standards (CAAQS) and expanded the number of regulated air constituents. **Ground-level ozone is not emitted directly into the environment but is generated from complex chemical and photochemical reactions between precursor pollutants, primarily reactive organic gases (ROGs; also known as volatile organic compounds [VOCs]), 1 and oxides of nitrogen (NOX). PM10 and PM2.5 are generated from a variety of sources, including road dust, diesel exhaust, fuel combustion, tire and brake wear, construction operations and windblown dust. In addition, PM10 and PM2.5 can also be formed through chemical and photochemical reactions of precursor pollutants in the atmosphere.**

The California Air Resources Board (CARB) is required to designate areas of the state as attainment, nonattainment, or unclassified for the ambient air quality standards. An "attainment" designation for an area signifies that pollutant concentrations do not violate the standard for that pollutant in that area.

A "nonattainment" designation indicates that a pollutant concentration violated the standard at least once.

This data is valuable because it is an evaluation of today, before the crematorium is operational. This ozone and particulate matter pollution most likely comes from the busy traffic on Folsom Blvd. With a crematorium initially proposing 2 bodies or 400lbs of matter a day, 100,000 lbs a year, one can only imagine the additional contaminants flowing into The Preserves Neighborhood. In my opinion, once established, Caring Service Group is only going to increase the number of cremations in the future.

Please consider the residents of the Preserves Neighborhood's quality of life. Thank you,

Kyal VonGunten

Josh Kinkade

From: LJ Laurent [REDACTED]
Sent: Thursday, January 6, 2022 1:07 PM
To: Josh Kinkade; Pam Johns
Cc: Lydia Konopka; Steve Krahn; daoffice@sacda.org; ernest.conant@usbr.gov; Drew Lessard
Subject: PN 19 182 crematory CUP NegDec Filed for "enactment"

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To: Folsom community development director; Asso. planner
cc: Asst City Clerk for file; City Engineer for "public project files"
From: Laurette Laurent
January 6, 2022

Re: Public Notice PN 19 182 Requests for Investigations filed
in re HDC "design review group" CUP final Approval

It has never been my practice to offer Expertise in Land Use Issues, to those who are intent upon Violations of such Laws. However, please accept Notice herein. Formal Requests for Investigation have been filed with appropriate Agencies. Lists of detailed "discrepancies" and "departures" from normal Legal Due Process, were included, as well as False Filings.

Notice of Public Hearing appears in January 6, 2022 Folsom Telegraph as Historic District Commission Legal Notice, PN 19 182. Parcel 070 0260 001 0000
Owner Lakeside Memorial Law Entity #

C0307506

08/15/1955

Site Zoning is "OPEN SPACE/OS Conservation", underlying Zone District OPEN SPACE.

Your PN 2019 182 contains strange reference to California State Laws which you purport have direct Relevance to PN 19-182. With respect to same, you would do well to consult with and obtain Written Opinion of Legal Counsel with respect to state laws cited. If you do not so so, you may place yourselves in untenable positions, or may have already done so. Have our paid city lawyers done complete due diligence, which you apparently rely upon?



CITY OF FOLSOM
DISTINCTIVE BY NATURE

NOTICE OF PUBLIC HEARING AND NOTICE TO ADOPT A MITIGATED NEGATIVE CITY OF FOLSOM HISTORIC DISTRICT

DATE OF HEARING: February 16, 2022
TIME OF HEARING: 5:00 P.M.
PLACE OF HEARING: City Council Chambers, 50 Natoma Street, Folsom, CA

NOTICE IS HEREBY GIVEN THAT: A public hearing will be held before the Historic District Commission of the City of Folsom to consider the merits of the following:

PROJECT NAME Lakeside Memorial Lawn Crematorium
Property Owner/Applicant: Lakeside Memorial Lawn/Igor Semenyuk
Project Location/APN: 1201 Forrest Street / 070-0260-001-0000
Planning No.: PN-19-182
Staff Contact: Josh Kinkade, Associate Planner, 916-461-6200
jkinkade@folsom.ca.us
Entitlements: a. Conditional Use Permit

Project Description: The proposed project includes operation of a crematorium building, a 1,071-square foot metal shed located at 1201 Forrest Street, within the Lakeside Memorial Lawn cemetery. The site is zoned OS/P (Open Space/Public Primary Area) with a General Plan designation of OSC (Open Space and Conservation) and has a General Plan designation of Historic District. The Historic District Commission will take final action on this request unless the City Council.





cemetary. The site is zoned OPR (Open Space and Conservation) and has a General Plan designation of OSC (Open Space and Conservation) and has a General Plan designation of Historic District Commission will take final action on this request unless the the City Council.

Environmental Review: An Initial Study and Mitigated Negative Declaration accordance with the requirements of the California Environmental Quality Act review period begins January 7, 2022 and ends February 7, 2022. Copies of the Declaration are available for review at the City of Folsom Planning Department Folsom CA 95630, Monday through Friday from 8:30 a.m. to 1:00 p.m. The document is available for download from the City of Folsom website at <https://www.folsom.ca.us/government/development/planning-services/current-project-information>

All persons interested in these matters are invited to present and submit statements orally at a public hearing. The environmental documents are available for review at, and further information can be obtained from the Community Development Department, 50 Natoma Street, during regular business hours. The Community Development Department can be reached by phone at (916) 461-6202. A staff report will be available at City Hall or at www.folsom.ca.us on the Thursday preceding the Public Hearing.

Pursuant to all applicable laws and regulations, including without limitation, California Government Code Section 65009 and/or California Public Resources Code Section 21177, if you wish to challenge any decision (regarding planning, zoning and/or environmental decisions), you may be limited to the issues you or someone else raised at the public hearing(s) described in this notice/agenda, or any appeal must be delivered to the City at, or prior to, the public hearing. An appeal to the City Council from the Historic District Commission may be brought by any interested party within then (10) days of the

**PAM JOHNS
COMMUNITY DEVELOPMENT DIRECTOR**

Published in the January 7, 2022 Sacramento Bee



Address is 2020 Mormon St.; Falsified in PN 19 182, and on large number of Perjury-sworn Building Permit Applications.
All LNS 18' wide lanes are subject to Folsom PUD Law enacted for creation of s.f. houses only, not 2,400F furnaces.

Josh Kinkade

From: LJ Laurent [REDACTED]
Sent: Friday, January 7, 2022 11:20 AM
To: Josh Kinkade; Pam Johns
Cc: Steve Krahn; Steven Wang; daoffice@sacda.org; Drew Lessard; Ken Cusano; Rick Hillman
Subject: PUBLIC COMMENT PN19 182 for FILES/ PRA REQUEST City Eng. APPROVED RECORDS, Furnace necessity PN 19 182 crematory CUP NegDec Filed for "enactment"

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To: Folsom city clerk office
to: Comm. Development asso planner Kinkade; manager Johns
cc: interested parties
From: Laurette Laurent
Re: PUBLIC COMMENT which includes PRA for MISSING mandatory documents not Filed with Clerk, nor with SCH, nor OPR.

Please accept the below Itemized Lists as Public Comment on the potential Failure of city and applicant to obey all Laws governing such dangers uses as 2,400F furnaces, 500 gallon explosive LPG gas storage -- on 12 acres of land with only a three inch diameter water supply pipe. Where is the Fire Marshall True Report with signature & seals?

Please be aware your Noticing email was received minutes ago, however it merely proves city staff are NON-Compliant with the Legal Noticing Requirements. Guess you don't listen to Licensed Civil Engineers nor to Licensed city lawyers. tsk tsk...

----- Forwarded Message -----

From: LJ Laurent [REDACTED]
To: Asst. Clerk <lkonopka@folsom.ca.us>; City Engineer Steve Krahn <skrahn@folsom.ca.us>
Cc: "blm_ca_web_re@blm.gov" <blm_ca_web_re@blm.gov>; ernest.conant@usbr.gov <ernest.conant@usbr.gov>; Drew Lessard <dlessard@usbr.gov>; "daoffice@sacda.org" <daoffice@sacda.org>; Ken Cusano <kcusano@folsom.ca.us>; Rick Hillman Chief FPD <rhillman@folsom.ca.us>; Fire Marshal Ono <lono@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Kerri Howell <khowell@folsom.ca.us>; Steven Wang <swang@folsom.ca.us>; The HFRA <thefra@gmail.com>; Sarariverwatch Info <info@sarariverwatch.org>
Sent: Friday, January 7, 2022, 08:34:43 AM PST
Subject: Re: PRA REQUEST City Eng. APPROVED RECORDS, Furnace necessity PN 19 182 crematory CUP NegDec Filed for "enactment"

Urgent PRA REQUEST January 7, 2022, for Folsom City Engineer Documentation, Permits, Sealed/Signed Reports, Building Permits, and all public records pertaining to three year old "application" for furnaces on OPEN SPACE Zone District surrounded by Public Open Space and s.f. homes.

To: Folsom City Engineer
Asst City Clerk Konopka

cc: interested parties OPEN SPACE Parcel protectors Adjacent to proposed 500 gallons LPG, on site with 3" water supply pipe.
 cc: silent city staff; council
 From: Laurette Laurent
 January 7, 2022

PRA REQUEST: Documents Requested: City Engineer Sealed/Signed Approved drawings for furnaces abutting federal forests/river, including Perjury Sworn Applicant Statement, City Engineer APPROVED Report of Findings of Necessity, Hardship, Unique 'circumstances', WATER SUPPLY lines, required for such furnaces 2,400 F, Fire Marshall portion of City Engineer's Report;
 All Building Permit applications [perjury sworn accurate & Complete] granted to this Parcel with Legal Address 2020 Mormon St. since 1858 Judah Map of Folsom Streets; All Memos, notes, minutes of meetings, and communications, between "Community Development director, Asso. Planner and others with City Engineer on this "Public Project" as advertised.
 Also requested are City Engineer and community development director/manager **Duties and Ordinances** establishing said CE and manager's duties.

As always, State Law allows ten days for production of these Ordinary Public Records for this "Public Project." Since this Public Project has been a Matter of Record since 2019, three whole years, surely all Required Public Records and City Engineer Approved Reports, with recommendations should be available January 18, 2022.

Second Request, Pursuant to Law Mailed Public Notices to Abutting Parcels are mandatory. Please supply Verified & CE Certified **Proof of said Mailings to all Public Agencies** and s.f. home residents abutting Parcel 070 0260 001 0000, which city identifies with False Address as being located with Ordinance-Created Lake Natoma Shores Planned Unit Development Subdivision.

Repeated, ignored Prior PRA Requests: not once in over ten years has Folsom City Engineer of Record produced even one single PRA RESPONSE for Public Projects -- which include his Seal & Signature proving he did his job of Certifying all Engineer Details of Folsom Public Projects such as this one -- which is the ultimate in License dereliction due to the obvious Hazards he is allowing, abetting, or silently watch being processed as he remains mute, inactive, etc., on his Public Project Duties.

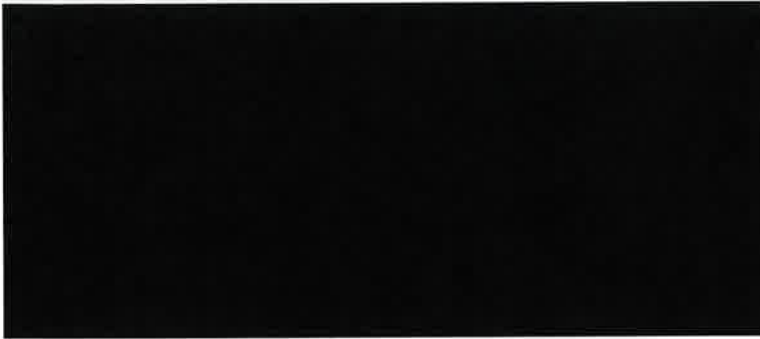
Again, city staff have had three long years of prolonging this "application" and "approved" building Permits which are based upon Perjured information, so all Public Project PN 19 182 Records and Permits should be at-hand for immediate PRA RESPONSES.

CE Records and PROOF of Open Fire Department Access for all Federal Agencies Parcels surrounding this Public Project, and Historic Site under BLM protection, and State 13 acre parcel are requested.

Certification of Safety by City Engineer with respect to Blast Radius of 500 gallons of LPG highly flammable propane gas abutting federal & state Open Space lands & forests is

also part of this PRA Request for following reasons: danger of LPG and NO Water Lines at all and no Access roads for Water Trucks:

Propane Tank Explosion



Propane Tank Explosion



Propane Tank Explosion

FYI to City Engineer: all loose objects, metal, rocks, will become projectiles capable of flying further than 1,500 feet.

PRA Request for your APPROVAL of this LPG storage with your Certified Safety Analysis is REQUESTED as mandatory portion of Application. How could residents be paying a CE who ignores our very lives and sole Water Supply and Federal Assets?

Kindly Expedite this Portion of PRA Request with immediate PRA RESPONSE as it is your License Obligation.

PRA Request concern: since mayor also holds a state License for an Engineering Field long discontinued in this state, this ought to concern entire city council. How can residents fail to regard her inactions all three years as anything but Negligence or willful disregard of License Duties?

On Thursday, January 6, 2022, 01:07:33 PM PST, LJ Laurent <ljlaurent@att.net> wrote:

To: Folsom community development director; Asso. planner
cc: Asst City Clerk for file; City Engineer for "public project files"
From: Laurette Laurent
January 6, 2022

Re: Public Notice PN 19 182 Requests for Investigations filed
in re HDC "design review group" CUP final Approval

It has never been my practice to offer Expertise in Land Use Issues, to those who are intent upon Violations of such Laws. However, please accept Notice herein. Formal Requests for Investigation have been filed with appropriate Agencies. Lists of detailed "discrepancies" and "departures" from normal Legal Due Process, were included, as well as False Filings.

Notice of Public Hearing appears in January 6, 2022 Folsom Telegraph as Historic District Commission Legal Notice, PN 19 182. Parcel 070 0260 001 0000
Owner Lakeside Memorial Law Entity #

C0307506

08/15/1955

Site Zoning is "OPEN SPACE/OS Conservation", underlying Zone District OPEN SPACE.

Your PN 2019 182 contains strange reference to California State Laws which you purport have direct Relevance to PN 19-182. With respect to same, you would do well to consult with and obtain Written Opinion of Legal Counsel with respect to state laws cited. If you do not so so, you may place yourselves in untenable positions, or may have already done so. Have our paid city lawyers done complete due diligence, which you apparently rely upon?



FOLSOM
DISTINCTIVE BY NATURE

NOTICE OF PUBLIC HEARING AND NOTICE TO ADOPT A MITIGATED NEGATIVE CITY OF FOLSOM HISTORIC DISTRICT

DATE OF HEARING: February 16, 2022
TIME OF HEARING: 5:00 P.M.
PLACE OF HEARING: City Council Chambers, 50 Natoma Street, Folsom, CA

NOTICE IS HEREBY GIVEN THAT: A public hearing will be held before the Historic District Commission of the City of Folsom to consider the merits of the following:

PROJECT NAME Lakeside Memorial Lawn Crematorium
Property Owner/Applicant: Lakeside Memorial Lawn/Igor Semenyuk
Project Location/APN: 1201 Forrest Street / 070-0260-001-0000
Planning No.: PN-19-182
Staff Contact: Josh Kinkade, Associate Planner, 916-461-6200
jkinkade@folsom.ca.us
Entitlements: a. Conditional Use Permit

Project Description: The proposed project includes operation of a crematorium, a 1,071-square foot metal shed located at 1201 Forrest Street, within the Lakeside Memorial Lawn cemetery. The site is zoned OS/P (Open Space/Public Primary Area) with a General Plan designation of OSC (Open Space and Conservation) and has a General Plan designation of Historic District. The Historic District Commission will take final action on this request unless the City Council.





cemetary. The site is zoned OSR (Open Space/Recreational Primary Area) with OSC (Open Space and Conservation) and has a General Plan designation of Historic District Commission will take final action on this request unless the the City Council.

Environmental Review: An Initial Study and Mitigated Negative Declaration accordance with the requirements of the California Environmental Quality Act review period begins January 7, 2022 and ends February 7, 2022. Copies of the Declaration are available for review at the City of Folsom Planning Department Folsom CA 95630, Monday through Friday from 8:30 a.m. to 1:00 p.m. The c downloaded from the City of Folsom website at <https://www.folsom.ca.us/government/development/planning-services/current-project-information>

All persons interested in these matters are invited to present and submit statements ora public hearing. The environmental documents are available for review at, and further in from the Community Development Department, 50 Natoma Street, during regular busin Development Department can be reached by phone at (916) 461-6202. A staff report will City Hall or at www.folsom.ca.us on the Thursday preceding the Public Hearing.

Pursuant to all applicable laws and regulations, including without limitation, California 65009 and/or California Public Resources Code Section 21177, if you wish to challeng decisions (regarding planning, zoning and/or environmental decisions), you may be limite you or someone else raised at the public hearing(s) described in this notice/agenda, or delivered to the City at, or prior to, the public hearing. An appeal to the City Council fro District Commission may be brought by any interested party within then (10) days of the

PAM JOHNS
COMMUNITY DEVELOPMENT DIRECTOR

Published in the January 7, 2022 Sacramento Bee



Address is 2020 Mormon St.; Falsified in PN 19 182, and on large number of Perjury-sworn Building Permit Applications.
All LNS 18' wide lanes are subject to Folsom PUD Law enacted for creation of s.f. houses only, not 2,400F furnaces.

=====

Just rec'd 11am 1/7/2022 Noticing email from city asso. planner:

LJ Laurent <ljlaurent@att.net>

brian.dewey@parks.ca.gov

Rodriguez Bianca@DGS,ernest.conant@usbr.gov,Drew Lessard,Matthew Ceccato,Dr. Ami Beraand 2 more...

Thu Aug 5 at 10:51 AM

To: Brian Dewey, CA State Parks

cc: Ernest Conant, USBR Mid Pac. Dir.; Wade Crowfoot;

Drew Lessard, Folsom USBR CE; Ami Bera, Matt

cc: Bianca Rodriguez, Counsel, CA DGS [for Ana M. Lasso Director]

August 5, 2021

Re: CA State-owned Parcel: encroachment, dumping, unpermitted uses & grading, by Miller Lakeside Cemetery, dba Caring Service Group.

Mr. Dewey, It has been twelve days since my first attempts to contact you personally; ten days since this email cc'd to CA DGS Counsel. People have difficulty realizing the dis-organization of State Parks, but Lake Natoma Shores Subdivision neighbors do. We know the travesty of Folsom and CA SP ignoring impacts upon Federal land & waters of American River. CA SP is "Managing Partner" of USBR Reclamation, yet SP has done nothing to protect, manage, or provide Enforcement on the surrounding federal Parcels at American River/CA State Park Managing Partner.

For that reason, I am informing USBR Mid Pacific Director Conant and Folsom USBR Manager Drew Lessard CE as well. It is unacceptable that California State Parks is failing Managing Partners Duties, and ignoring Property-Ownership Protection of the Parcel which has been the dumping ground and part of the necessary land for movement/Access to construct more unpermitted, uninspected Structures on "Folsom Open Space & Open Space Conservation" Zone District. Because cemetery owners derive a huge tax benefit from this reporting by city of Folsom, this is copied to Sacramento County authorities and powers as well.

Clearly, my experiences prove California State Parks has major issues and hence it was moved to lesser status in CA Natural Resources Department. My complaints sent via Certified Mail have gained NO Direct Responses to me, whatsoever from either SP or Natural Resources. This is extremely important to me because some of them detailed Violations of Federal Laws, on Federal Property -- which is assigned via Managing Partner Agreement to California State Parks. Research Reports have been created, submitted, but California State Parks top, Sworn LEOs, Legal Counsel, and management have never once responded during the Period September 1, 2020, and today. People whom I've known for years, in CA SP Gold Fields District, have done absolutely nothing about addressing the shortcomings and failures. California State Parks and Natural Resources Department remain nonresponsive to the Peoples' Business.

Therefore, I am asking California Department of General Services to take some action to accomplish two outstanding Issues.

1. Please take actions to stop the mis-use, encroachment onto, dumping, storing of rusty cemetery junk on the Parcel in Question which has enabled a crematory process to exist and to proceed without Permits nor Inspections by city of Folsom.

2. Please ask DGS Legal Staff to fill the obvious void left by California State Parks.

Please refer SP "issues" directly to California Attorney General as there appears to be no hope SP will comply or remediate their failures with respect to obeying and enforcing Laws on Federal Land/Watershed Forest/American River.

Lake Natoma Shores Subdivision homes abut this State Parcel, as well as cemetery parcel zoned "Open Space Conservation". Folsom staff have had a crematory application in Community Development Department for approximately 18 months, without any Public Notice whatsoever. USBR Reclamation is an abutting landowner. Public **OPEN Space Zone surrounds about 80% of cemetery parcel**, and yet USBR, State of California, and Single Family home owners have NOT been mailed Notice of Proposed exceptions, change of land Uses, etc. In the **past**, when this crematory became an issue, ALL state, federal, county, and local neighbors were Notified by Mail. Sacramento County Medical Examiner excoriated owners for proposing not only a Hazardous Use, but desecrating Historic Features, abutting Chinese Cemetery which is a Registered National Historic Site, and failing to respect History and environment.

This time, construction has been on-going with mis-use of State Parcel in question enabling "access" for earth-moving machines. **Will State employees provide a Solution under the Laws?**

If we wait for State Park action, the entire federal land/water area, and our homes may vanish in wildfire from 2,200 F degree burner, with two huge liquid propane gas tanks, and on a 2 inch water-meter-constricted 3 inch diameter Water Service Pipe onto cemetery parcel.

Someone needs to stop Caring Service Group by stopping city of Folsom "quiet" enabling actions and city failure to enforce OPEN SPACE Conservation Zoning, and failure to enforce new stringent Historic Oak tree protection law. Whatever business proposals emerge, there is never a Folsom City Engineer signed & Sealed Report on laws of federal, State, County, and their own Municipal Code -- which prohibits all the 12 buildings on OSC Zone. Without a state licensed Civil Engineer participating, many laws are ignored. We pay licensed people, but they never produce Reports to protect residents, and Enforce Development, Land Use Laws.

Neighbors are justifiably very deeply concerned, as we know Folsom as a city is on the Federal WUI Register: Wildfire Urban Interface Registry.

Anyone wishing copy of links, Folsom WUI Plan, , please request them.
Managing Partner Agreement attached.

https://www.youtube.com/user/4sewerdogs/videos?view=0&sort=dd&shelf_id=0

On Tuesday, July 27, 2021, 01:42:53 PM PDT, LJ Laurent <ljl Laurent@att.net> wrote:

SACRAMENTO BEE MAY 25 2018

Crematorium mishap shoots plume of human ashes into air, San Diego officials say

BY JARED GILMOUR

MAY 24, 2018 06:03 PM



National City, California, firefighters at the scene of a crematorium accident Thursday that sent human ashes into the air.

NO FURNACE IN FOREST

City council: horrific idea, 2 huge liquid Propane Tanks

LJ Laurent [REDACTED]

brian.dewey@parks.ca.gov

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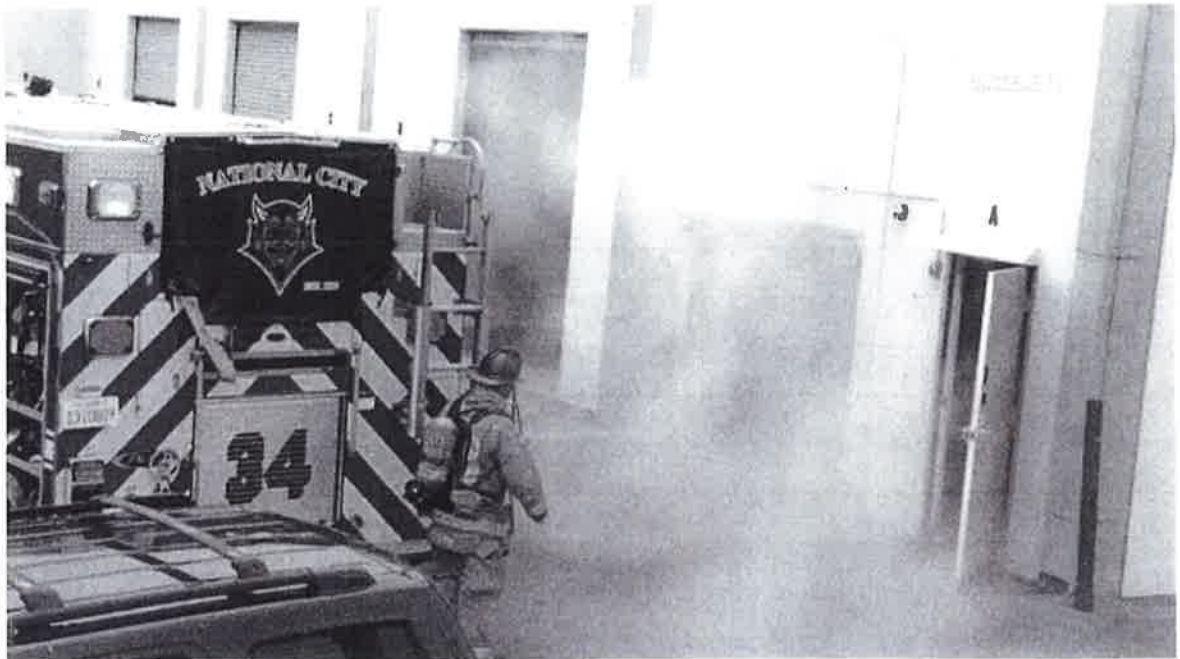
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BY JARED GILMOUR

MAY 24, 2018 06:03 PM



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NO FURNACE IN FOREST

City council: horrific idea, 2 huge liquid Propane Tanks

Josh Kinkade

From: LJ Laurent [REDACTED]
Sent: Friday, December 18, 2020 9:49 AM
To: Josh Kinkade; Steve Krahn; Aimee Nunez; Pete Piccardo; Daniel Wolfe; Bryan Holm; Scott Zangrando; Scott Johnson; Steven Banks; Desmond Parrington; Stephanie Henry; Allison Konwinski; Kristina Eicher; Lauren Ono; Mark Rackovan; Pam Johns; Elaine Andersen; Jason Browning; Dave Nugen; Ryan Neves; Don Brown; Todd Eising; Marcus Yasutake; Vaughn Fleischbein; Greg Bakken; Steven Wang; Cc: "thefra@gmail.com"; Paul Keast; HPLBoard; "pgeplanreview@pge.com"; "entitlements@smud.org"; "projectreview@airquality.org"; "HunleyC@Saccounty.net"; "cemeterycommission@saccounty.net"; "emailcfb@dca.ca.gov"; "kmtacc@hotmail.com"
Cc: ernest.conant@usbr.gov; Drew Lessard; Mike Kozlowski; Sarah Aquino; Christa Freemantle; Rick Hillman; Ken Cusano; Sue Frost; Hedges. Matt; paul.lau@SMUD.org; Stephen Green; Mary Beth Metcalf; lisa.mangat@parks.ca.gov; brandon.dawson@sierraclub.org; Eileen Sobeck; Patrick Pulupa
Subject: Cemetery: FED FOREST, homes IMPACTS of wrongs
Attachments: SAC CTY CEMETERY COMM. CREMATORIUM, 603, HDC Dec 18, 2020.odt; CEMET 1991 improper actions CUP Mods, Variance, Conditions do not match Abandonmt.odt; CEMETERY 12 HDC WITHDRAWN, CREMATORIUM.odt

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To: city of Folsom officials, employees, Mayor, Vice Mayor
 City Clerk for CIRCULATION to all City Council elected officials
 Sacramento County; et al.

From: Lorette Laurent
 December 18, 2020

Re: Application for Illegal Zoning District Usages, illegal granting of Variance from HDC Commission with NO State Authority to grant such benefits, violation of CA Govt Code Title Zoning; violation of Folsom Zoning Code District restricting NO buildings on any "open space" category -- consistent with VIOLATED State Govt Code; Failure to abide by Sunshine Laws; Failure to conduct Full IS, EIR/EIS; failure to provide PROOF of Publications for all such steps; Failure to OBTAIN State Fire Marshal Approvals due to ABUTMENT to Federal Watershed and Federal Forests; Failure to Notify CA State Parks; Failure to Notify Sacramento County Assessor office of "Proposed changes" [likewise to Cemetery Commission]; Failure to provide NOTICE to an ABUTTING RESIDENT who filed Complaints about SECRECY in this Land Usage improper SECRET Process - by FAILING to NOTIFY ME -- despite my historic and RECENT objects & Comments with regard to this SECOND time around crematory Proposal -- and my WRITTEN NOTICE to Folsom City Clerk and Planning to be given ALL HDC & Plan Commission Notices. Other violations exist, but are previously filed and resulted in PENALTIES against Applicant.

----- Original Message -----

From: Josh Kinkade <jkinkade@folsom.ca.us>

Comment: THIS IS NOT Permit Modification: because there is NO MENTION of ORIGINAL Permit. There is NO EVIDENCE a VARIANCE is due to Miller nor Lakeside because there is NO PROOF of "EASEMENT", Nor abandonment of Figueroa St. in the Parcel boundary legal description

as of September 10, 2020, city clerk, city lawyer, HAVE FAILED TO PROVIDE a Prior legal documents Exist and were approved by city attorney Steve Rudolph. No one proved said cited documents were created, reviewed by Professional lawyer, engineer – FOR LEGALITY [such as unproved Easement Leland Miller granted to 1850s Chinese cemeteries.]
Existence of EASEMENT GRANTED by Miller, to Chinese

12 Cond. 12 "Any proposed construction of Lakeside masoleums would NOT OBSTRUCT HISTORICAL EASEMENT or ACCESS to Chinese Cemetery as was AGREED TO YEARS AGO by Leland Miller former owner of Lakeside Memorial Cemetery."

PC 91-042 Findings for 1991 Permit Modification

NOTE: NO TIME LIMIT on CUP, Variance or MODIFICATIONS – Rudolph WARNED them as city attorney to put a TIME LIMIT on CUP, and Mods.

APPROVAL OF NEG DEC; VARIANCE from 50' Setback
Planner Matt Franck

Miln Construction stated it would take 50 years to fill 2000 spaces in buildings of 12,682 sq ft.

NO MORE Discussion

Maxfield moved to approve

NEG DEC

APPROVE VARIANCE FROM 50 foot required Open Space Setback

APPROVE CLANEY USE PERMIT

with CONDITIONS:

APPROVE FINDINGS including **C. Is consistent with General Plan and Zoning CODE**

D. Not harm health. Masoleums are SIMILAR TO and COMPATIBLE ADJOINING USES.

E. Unique circumstances in this area – i.e. rock piles surrounding it everywhere. “size and shape of parcel make it difficult to build in the OPEN SPACE Area” **ISN’T THAT THE POINT????**

F. VARIANCE is ESSENTIAL to PROPERTY RIGHTS. Plus, an ALLEGED TRIANGULAR shape of “parcel” “BUILDABLE AREA would be limited by shape.” **BECAUSE they think Commercial buildings belong in OPEN SPACE/ Conservation Zone???**

G. Grant of Variance will NOT affect area Residents or workers and is “is of SIMILAR CHARACTER TO USES.”

CONDIITIONS

SECTION ONE enforced by Community Development Dept CDD
CDD will do GRADIN REPORT and ARCHAEOLOGICAL studies.

ESPECIALLY OFFENSIVE: CDD will Regulate and ensure “CHINESE COMMUNITY” & George Chan is NOT denied ACCESS to Chinese Cemeteries.

CDD will ensure Lighting, Landscaping and adherence to Fish & Game Regs.
CDD HAD NO ENGINEER.

PUBLIC WORKS ENFORCEMENT AREAS:

NO MENTION OF CITY ENGINEER, nor that “PW” lacked an Engineer in charge: Director then WAS JOE LUCHI, a typist. – Bob was sidelined.

CONDITION 8 “Improvements shall be installed to the satisfaction of public works dept. PW” Luchi was sidelined City Engineer's typist, really.

Condition 9“EROSION CONTROL..... shall be directed by PW Director – Luchi – what a laugh.

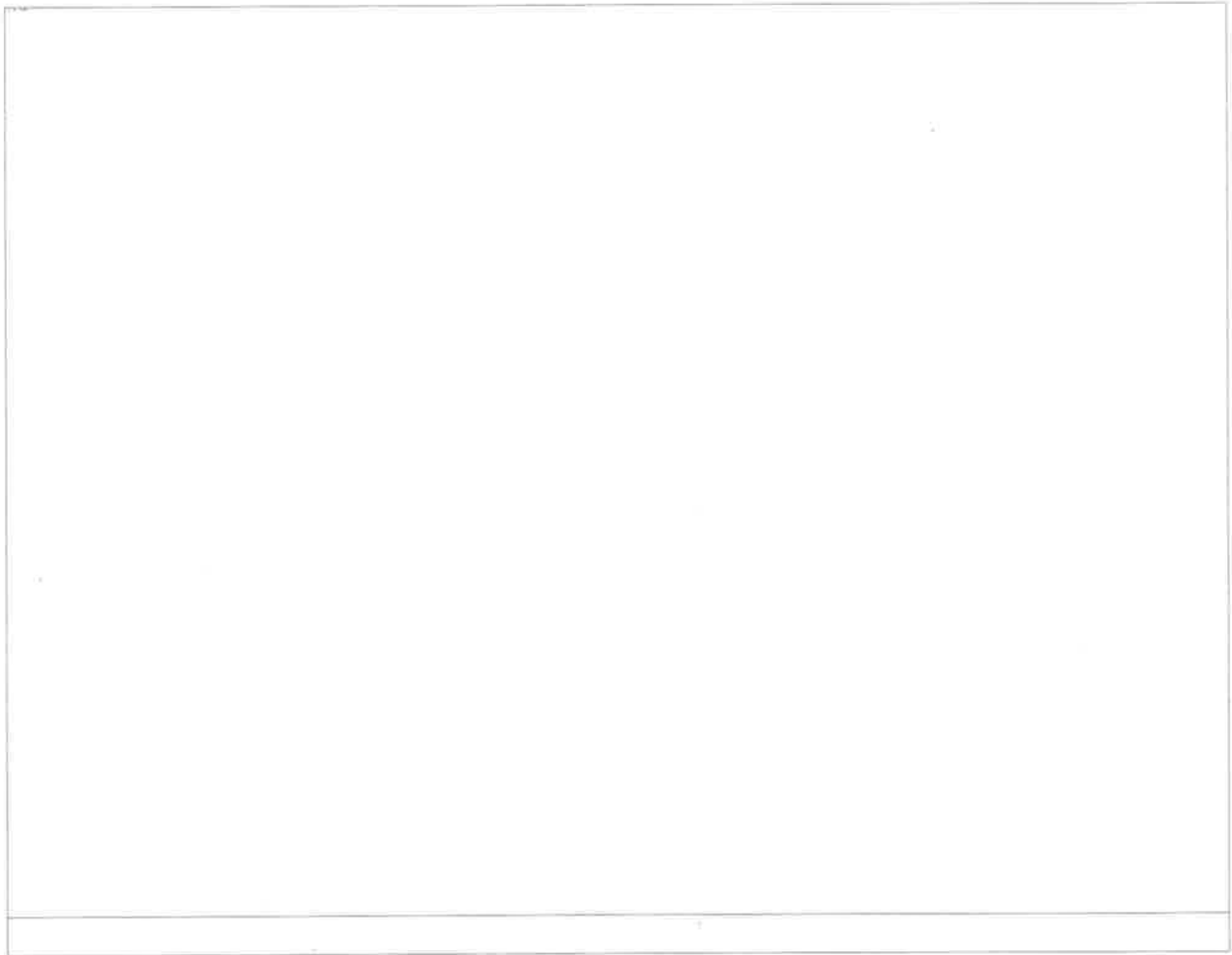
Cond. 10 Construction shall have 5 – NOT 50 Foot setback --“thereby

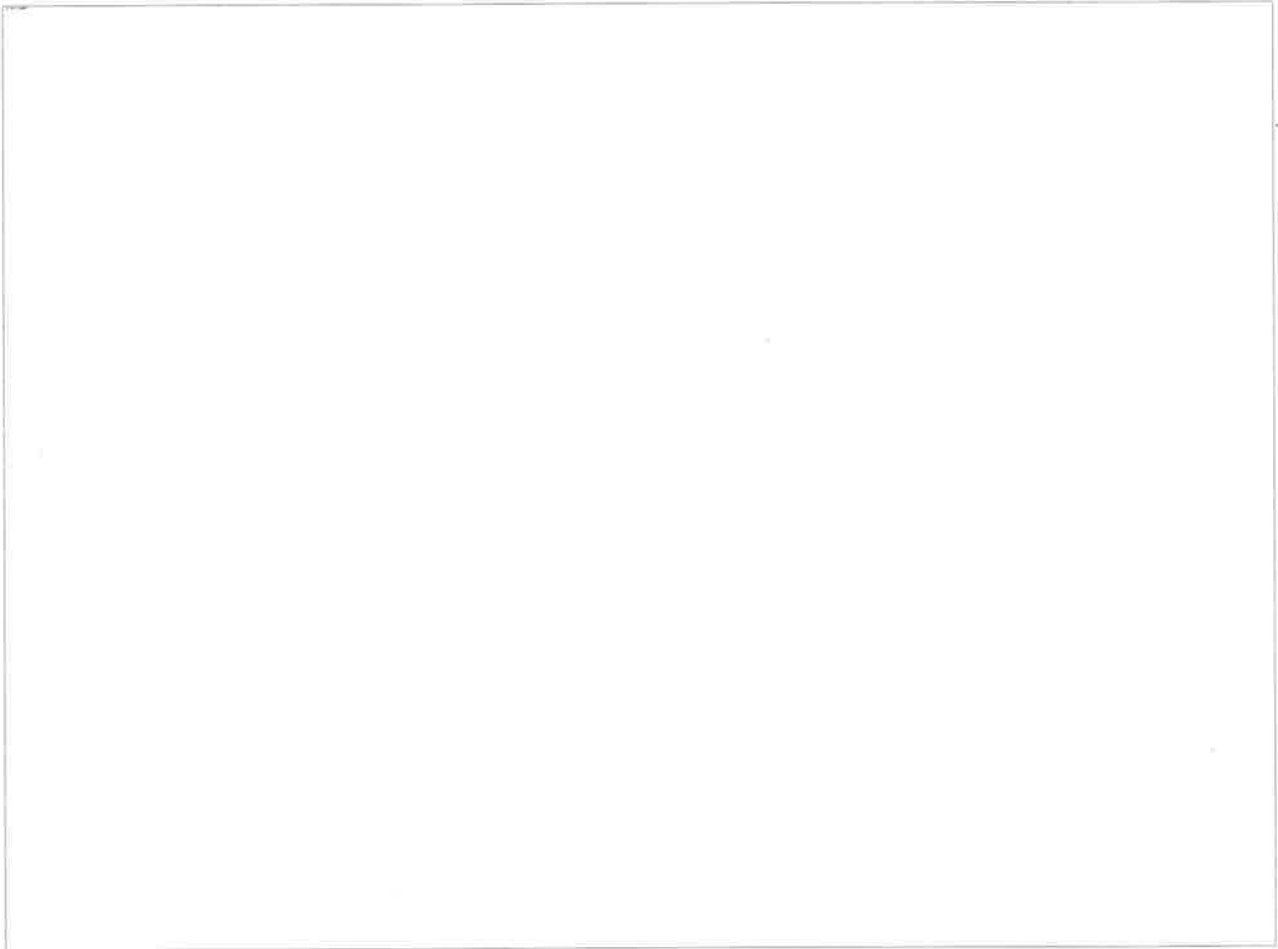
NOT CROSSING INTO Chinese cemetery ITSELF.

11 Proposed ROAD to Lakeside masoleums would BE NEAR TO ENTRANCE to Chinese cemetery thereby allowing ENTRANCE to Chinese cemetery GROUNDS>

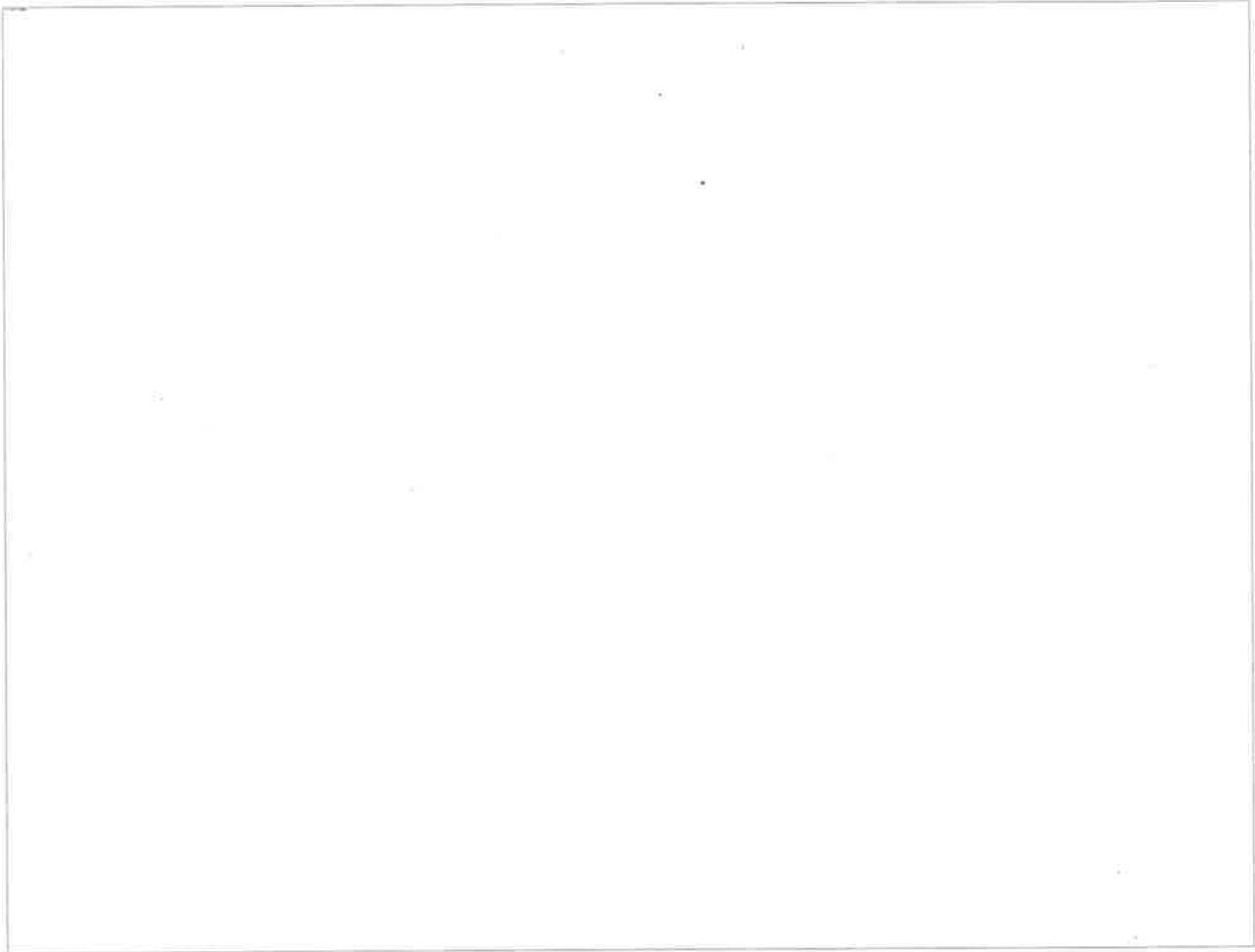
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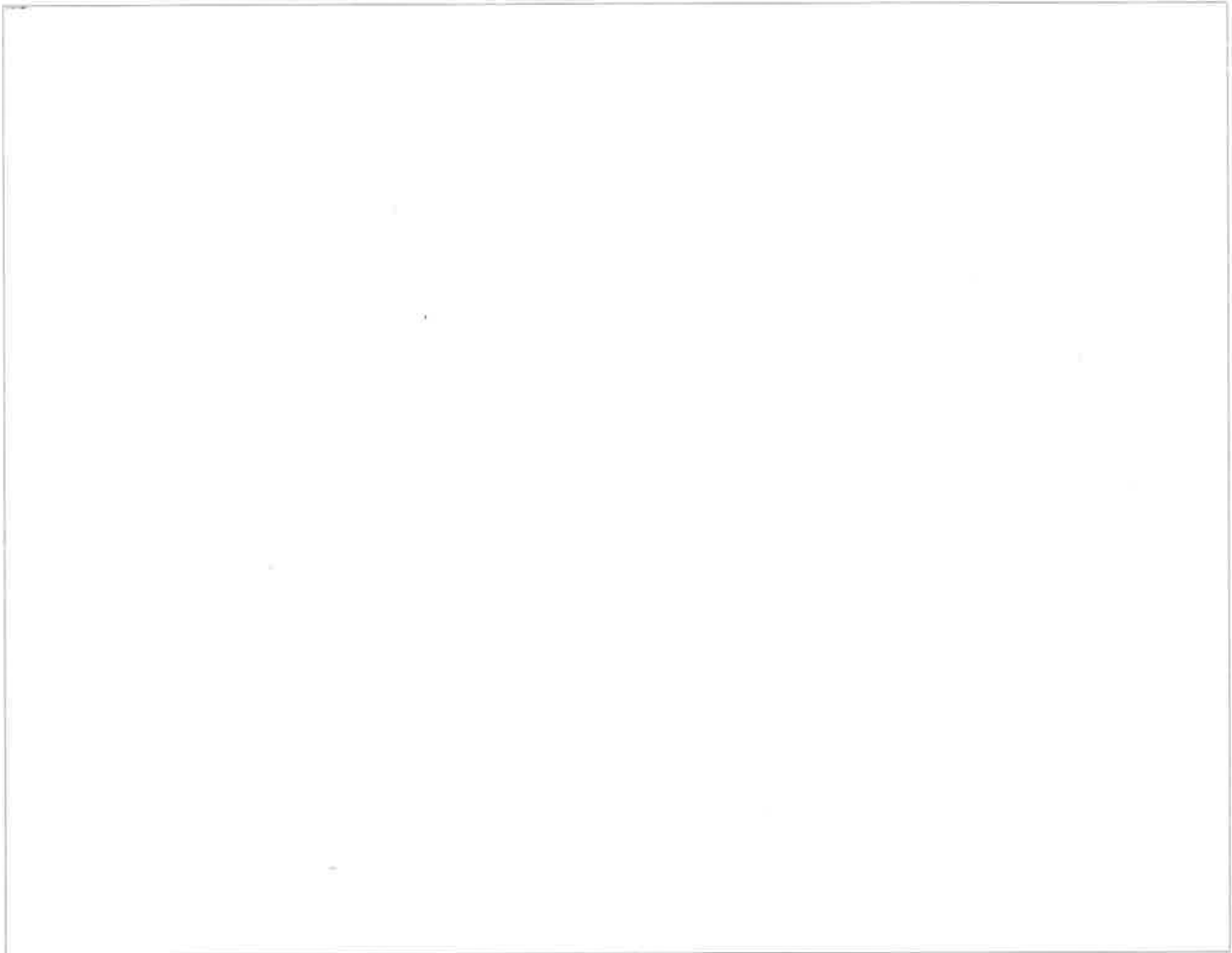
OUT OF ORDER CEMETERY 5b FINDINGS comes after below



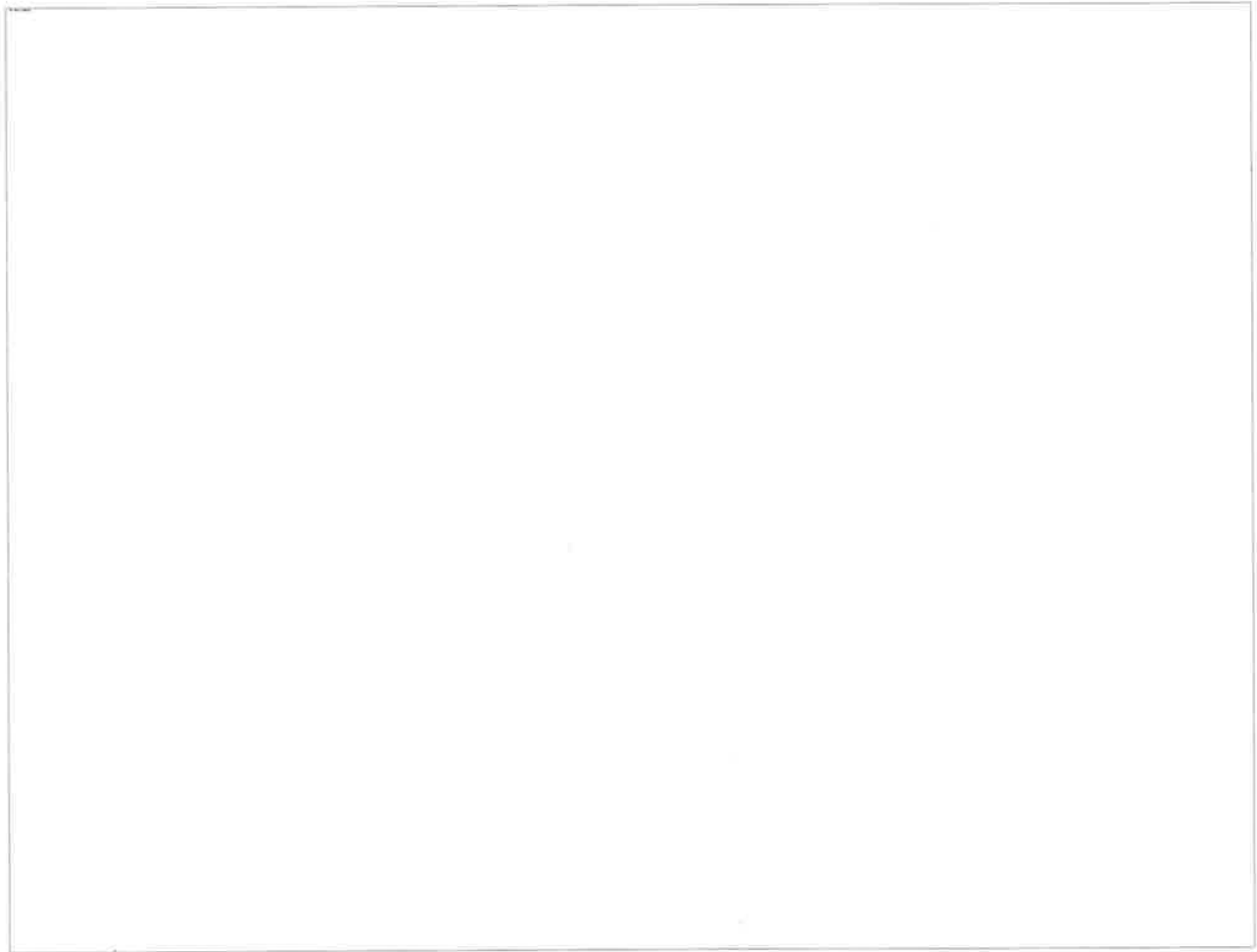






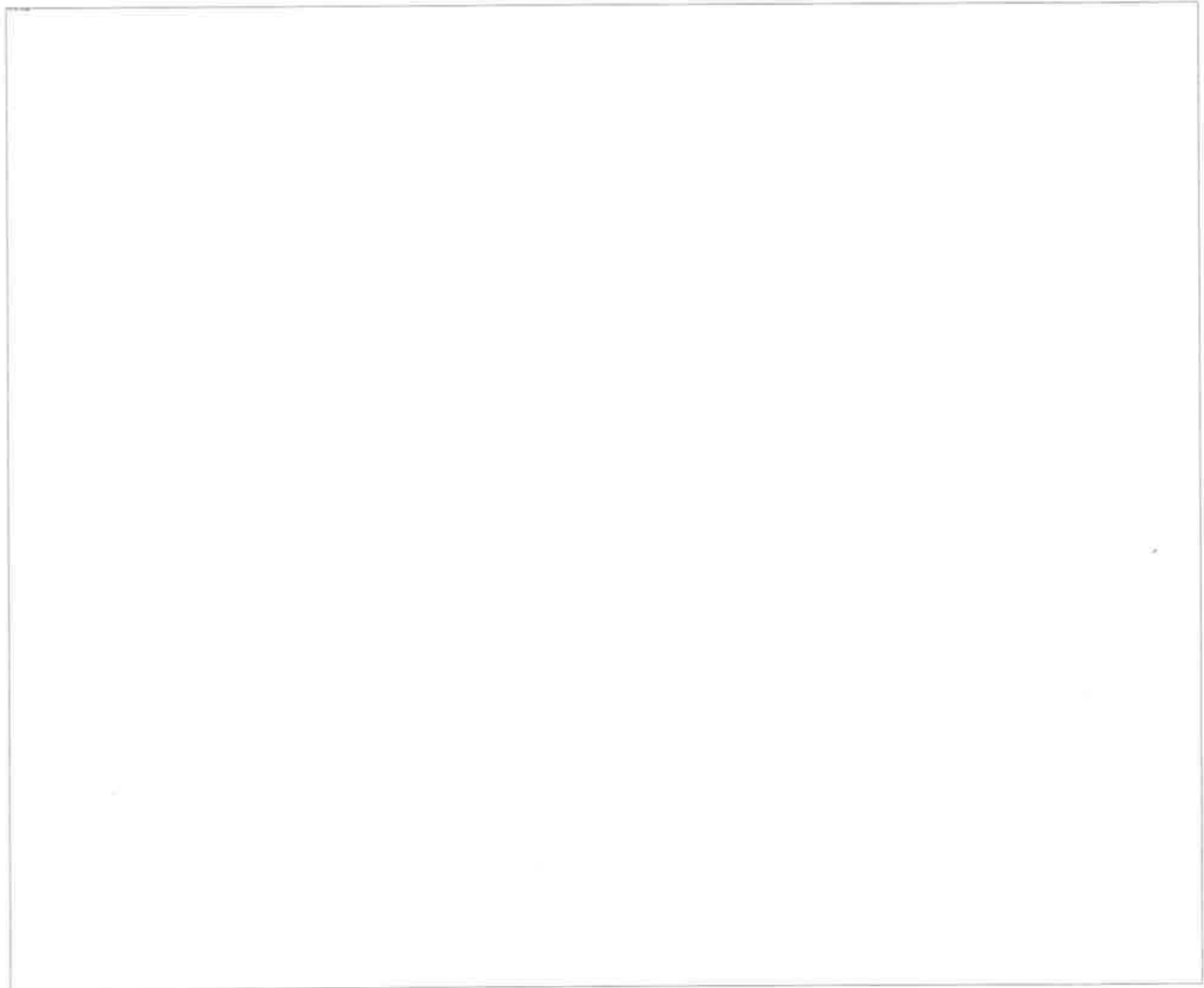


CEMETERY 6 a

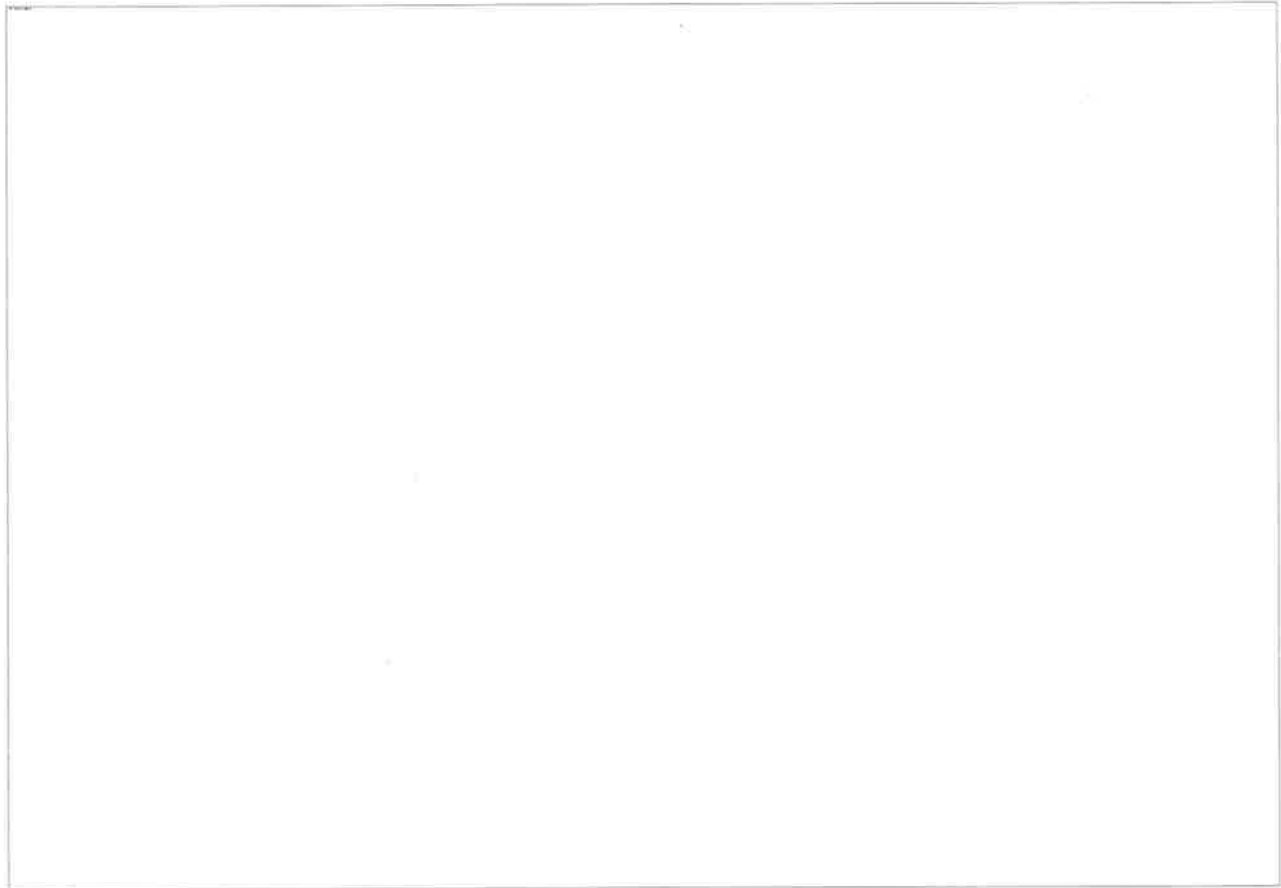


CEMETERY 6 Findings CUP 1995 similar to neighborhood

CEMETERY 6c only "PW dept OK needed for this CUP, and NO BLOCKING Chun Wah NOT OBSTRUCT OR BLOCK the HISTORICAL EASEMENT granted by LELAND MILLER Owner before Claney.



**CEMETERY 6 d PLAT OF SURVEY Rio Los Americanos Cemetery LAND GOES
RIGHT INTO MORMON St Right of Way Theo. Judah 1850 Map**



Record of Survey, O.S. Bk. 48, Pg. 4 or. t, t l t l Map 8. Chung Wah Cemetery
boundaries

CEMETERY 6 d Survey Record O.S. Bk 48, page 4 or. t, t l t l

**POR. SEC. 2, T 9N., R. 78., M.D.B.A.M. (Pno, l. t { f o navcta nto oE t-os
AMcnrc [Nost**

This Rancho Rio Americano is so old it proves Chun Wah owners are part
owners of Mormon St. George Chan was cheated.

June 15, 1995 TO: Planning Commission FROM: Loretta McMaster, Senior
planner suBJEcr: Lakeside cemetery conditionat Use permit status
Agenda Item No. 6 PC Mtg. 6-21-95 PC91-042
cemetery, National Reoister status

The Chung Wah cemetery has been approved by the California State

Historical Commission and is awaiting national action for listing. One of the major factors in a successful nomination is whether the site maintains the "sense of time and place." There is some concern that development of the Phase XI and XIII mausoleum buildings, and the equipment building, may cause this aspect to be destroyed. Conditions 4, 7, and 10 relate directly to this question. It will be difficult to design the structures and landscape the area to mask their existence completely, but careful choice of materials and design can minimize their noticeability.

Need to send to city engineer or whoever.....

Besides the items required by the conditions of approval, staff has identified the following actions which could reduce future conflicts: Archaeologist on-site when work is occurring. Recording of an access easement. A second survey of the boundary (or perhaps consultation between the recent surveyor and Mr. Chan regarding basis).

Removal of Phases XI and XIII, by Mr. Claney, from his plans. (He has indicated that it will be many years before they would be constructed.) Taking any of these actions would be a demonstration of good faith on the part of the Claney's and/or the Chan's.

Although the responsibility would lie primarily with the property owners, the City may be able to assist in some of these actions. The interested parties have been invited to be present at this meeting. Staff can also provide an update at the Planning Commission meeting of July sth if directed by the Planning Commission. PC91-042.62t PC Mts. 6-21-95 Page 2

Loretta McMasters

Agenda Item No. 6 PC Mtg. 6-21-95 PC91-042 June 15, 1995 TO: Planning Commission FROM: Loretta McMaster, Senior planner suBJEcr: Lakeside cemetery conditionat Use permit status BACKGROUND This report is presented to update the Commission on the status of the conditional use permit issued on October 2, 1991 for construction of mausoleums at the Lakeside Cemetery. A site plan, minutes and conditions of approval, and historical map are attached. Concerns regarding the project's effect on the National Register status of the adjacent Chung Wah cemetery were raised by June Chan at the May 17, 1995 Planning Commission meeting. As reported by Commissioner O'Brien at the Commission's June 7, 1995 meeting, a meeting of concerned citizens, Commissioner O'Brien and staff was held on June 2. Loren Claney of tf," Lakeside Cemetery did not attend the meeting, but staff subsequently met with

Mr. Claney at the site. Issues identified relate to grading, archaeological investigation, access, boundary location, and National Register status. The road and parking area have recently been constructed, according to approved improvement plans.

NOTE: All Claney references of Hardship claims, compliance etc. are spurious.

EVEN THEIR PARKING is on Public ROW MORMON St.

Lots of cheating occurs when a mayor has fired the City Engineer/Public Works Director Bob Blaser CE.

This proves the Federal owners and Sacramento County Assessor were never notified of all this COMMERCIAL Development and the State and federal Status protection of Chun Wah cemetery itself.

There's a major problem with this HDC thinking they have a legal force – and they don't even realize the Plan Commission made serious errors because the acting PW Director was Bob Blaser's typist Joe Luchi.

CEMETERY 7

Conditional Use Permit Modification

If the Commission feels that completion of a mausoleum is the appropriate use of this property, it would be appropriate to schedule a hearing to modify the conditional use permit to address the issues that have arisen. The applicant has requested a modification of the conditional use permit and has offered to re-design the project as necessary. Until so directed by the Commission, staff has not prepared a complete listing of additional conditions which might be adopted, but the following has been prepared to serve for preliminary consideration: No construction near Chung Wah boundary.

Application for Mormon Street abandonment, with recorded access easement to Chung Wah. Use of gray stone construction; additional screen planting, including oaks.

West-Yost review of surveys, at applicants cost but contracted with City. Fence boundaries.

Additional drainage and erosion control.
 Planning Commission minutes of May 17,
 1gg5 Planning Commission minutes of June 7,
 1gg5 Planning Commission minutes of June 21 ,
 1g9S (draft) Planning Commission minutes of July 19,
 1 ggs staff report for the June 21, 1995 Planning commission meeting

staff report for the July 1 9, 1995 Planning commission meeting
 Site exhibit

Excerpt from the Folsom Municipal Code cultural Resources Investigation
 of Lakeside Memorial cemetery Mausoiium Complex

**Project Staff Recommendation Direct staff to notice both a
 revocation hearing and a conditional use permit modification, in
 order to preserve both options for Commission action. :**

PLANNING COMMISSION ACTION MOVE TO DIRECT STAFF TO SCHEDULE
 A REVOCATION HEARING AND A HEARING ON A MODIFICATION OF THE
 CONDITIONAL USE PERMIT FOR SEPTEMBER 20, 1995, PROVIDED THAT
 THE REOUINED FEE AND ADDITIONAL INFORMATION AND DRAWINGS
 NECESSARY FOR A CONDITIONAL USE PERMIT MODIFICATION ARE
 RECEIVED FROM THE APPLICANT NO LATER THAN THURSDAY, AUGUST
 24,
 AND FAILING THAT, TO SCHEDULE ONLY THE REVOCATION HEARING ON
 SEPTEMBER 20, 1995. PCg1-042.816 Page 3

August 10, 1995

New document from PRA Response 8
 Oct. 18 1995 CONTINUANCE of item PC meeting

PRA RESPONSE 9 is May 1995 PC meeting minutes DUPLICATE

RESPONSE doc 10 has great deal of controversy including Howell
 objecting. Requests for STOP WORK order; NOTE The Preserve
 Subdivision was grading now. CONTINUANCE to consider STOP WORK
 order. Howell: were bodies found? I saw on TV BODIES WERE
 FOUND outside the white peoples' burial e.g. Jim the Cook.

QUOTE: from Minutes July 1995, continued meeting.

This issue made big news!

Sue Silver handed out a letter and maps to the Commission at the meeting and recommended the commission review them carefully and stated her source of information was cited in the Folsom Telegraph. concerned about the accuracy of the 1991 survey done but hasn't review it' She pulled the original deed from the Folsom Development Company to the chung wah company and it matches the plat exactly of china Mission which was recorded in December 1906' Concerned about of the Assessors Parcel Map, page 70-13 has a discrepancy in the footage measurement along the southeasterly line of the chinese cemetery which disparages approximately 178 feet from the original survey.

chairman Lehman suggested the commission revoke the Use permit. commissioner Howell suggested that a stop-work order be issued. End quotes.

DOCUMENT 10 PRA RESPONSE 9 3 2020

PLANNING COMMISSSTON MINUTES NOVEMBER 1 , 1995

Huge controversy and requests again for Stop Work Order and Revocation of CUP.

Adjourned due to late hour, no action.

DOCUMENT 11 PC Minutes December 1995

Very LENGTHY many pages, finally

Vice-Chairman Maxfield Moved To coNTTINUE THE rrEM To JANUARv 3 BUT LtMtlNG tr ro oNly CONSIDERING ALTERNATIVE 2 AND THE CHANGED CONDITIONS AFTER STAFF HAS HAD TIME TO LOOK THEM OVER THAT WERE SUBMITTED BY MR. PHILLIPS TONIGHT. HE ADDED THAT HE'D LIKE TO HAVE THE ITEM HEARD AFTER 9:00PM BECAUSE HE WOULDN'T BE ABLE TO ATTEND UNTIL THAT TIME. COMMISSIONER MESSNER SECONDED THE MOTION Commissioner O'Brien preferred to have the absent Commissioners present before any motion is made and would prefer having it continued. Steve Rudolph suggested that if the first goal is to resolve the issue regarding revocation, that before continuing, the first motion should be to move that there are no grounds for revocation as the Commission desires. Vice-Chairman Maxfield withdrew his motion. The second agreed. VICE-CHAIRMAN MAXFIELD MOVED THE PLANNING COMMISSION HAD THE HEARING FOR A REVOCATION OF A PERMIT AND THEY FOUND NO GROUNDS FOR REVOCATION OF A PERMIT, UNLESS SOMETHING ELSE HAPPENS AND THE ISSUE IS RAISED AGAIN. COMMISSIONER MILLER SECONDED THE MOTION AND THE MOTION CARRIED WITH THE FOLLOWING VOTE AYES: COMMISSIONERS HOWELL, MESSNER, MILLER, MAXFIELD NOES: COMMISSIONER O'BRIEN ABSTAIN: NONE ABSENT: CHAIRMAN LEHMAN, COMMISSIONER DREW MOTION THAT THE PLANNING COMMISSION FOUND NO GROUNDS FOR A REVOCATION OF THE PERMIT CARRTED 4-1-2. Commissioner O'Brien stated her opposition was because the other two members of the Planning Commission were not present. Steve Rudolph stated it appears the Commission is interested in modifying the conditions

which exist on the permit and recommended the next motion be that the Commission has found there are grounds to warrant the modification of the existing use permit. PC Mtg. 12-6-95 Page 10

COMMISSIONER MILLER MOVED THAT THE PLANNING COMMISSION HAS FOUND THERE ARE GROUNDS TO WARRANT THE MODIFICATION OF THE EXISTING USE PERMIT. VICE-CHAIRMAN MAXFIELD SECONDED THE MOTION AND THE MOTION CARRIED WITH THE FOLLOWING VOTE: AYES: NOES: ABSTAIN ABSENT: COMMISSIONERS HOWELL, O'BRIEN, MESSNER, MAXFIELD, MILLER NONE NONE CHAIRMAN LEHMAN, COMMISSIONER DREW MOTION CARRIED: 5-0-2 VICE-CHAIRMAN MAXFIELD MOVED TO CONTINUE TO THE JANUARY 3, 1996 MEETING FOR THE PREPARATION OF REVISED CONDITIONS. COMMISSIONER MESSNER SECONDED THE MOTION Commissioner Howell Alternative No. 2 only. questioned whether the motion was restricting the discussion to Vice-Chairman Maxfield stated it was Commissioner Howell stated she would prefer to leave it only to removal of the revocation issue. Vice-Chairman Maxfield felt he'd prefer to have a staff report without alternatives. Commissioner Miller stated he agreed with Vice-Chairman Maxfield that it's been fine-tuned enough between them with one set of conditions and if a concern is raised with something specific, the Planning Commission can adjust it in January. VICE-CHAIRMAN MAXFIELD MOVED TO CONTINUE TO JANUARY 3 AND ALTERNATIVE 2 WITH CONDITIONS SUBMITTED BY MR. PHILLIPS WITH STAFF FINE-TUNING THE CONDITIONS AND GIVE A REPORT BACK TOT HE COMMISSION AND TO HAVE THE PROPONENT AND OPPONENTS TIME TO LOOK AT THE CONDITIONS WITH TIME TO COMMENT ON THEM. Steve Rudolph stated the public hearing is closed but nothing is preventing the Commission from reopening the public hearing at the next meeting. The Commission doesn't have a legal obligation to open the public hearing if that was their desire. COMMISSIONER MESSNER SECONDED THE MOTION AGAIN Commissioner Howell stated her concerns for limiting the motion to Alternative 2 only. In the summary of the staff report, the wording states build out of the existing planned facilities in Alternative 1 and Alternative 2 has, what she considers to be, some unusual language referring PC Mtg. 12-6-95 Page I I to "an environmentally sensitive area". She feels Alterative 1 and 2 can overlay dramatically depending upon the definition of that environmentally sensitive area which she understands both the existing conditions and the new conditions received by Mr. Phillips is going to provide definition of exactly where that environmentally sensitive area is. She didn't feel the motion should be limited to one of the two alternatives just because of the environmentally sensitive issue. She stated she'd like to eliminate the revocation and allow both Alternative 1 and 2 to exist because Alternative 1 talks about going with what's been planned already and Alternative 2 refers to complete build out of the site with the exception of the environmentally sensitive area. Until that area is defined, ome can't say that everything can be done because you don't know whether all the property can be built on yet. VICE-CHAIRMAN MAXFIELD AMENDED HIS MOTION TO INCLUDE STAFF TO LOOK INTO THE ISSUE RAISED BY COMMISSIONER HOWELL ON THE ENVIRONMENTALLY SENSITIVE AND ARCHAEOLOGICALLY SENSITIVE AREAS AND DEFINITIONS AND FACTOR THOSE IN. COMMISSIONER MESSNER AGREED WITH THE AMENDMENT Commissioner O'Brien felt the motion was too vague. VICE-CHAIRMAN MAXFIELD CLARIFIED HIS MOTION TO READ AS FOLLOWS:

MOVE TO CONTINUE LAKESIDE MAUSOLEUM AMENDMENT OR REVOCATION OF CONDITIONAL USE PERMIT TO JANUARY 3, 1996 AND THE MATTER TO BE CONSIDERED BE ALTERNATIVE 2 AND IT'S CONDITIONS RECEIVED BY MR. PHILLIPS THIS EVENING AND STAFF FACTOR IN THE ISSUE RAISED REGARDING THE ENVIRONMENTALLY AND ARCHAEOLOGICALLY SENSITIVE AREAS AND THEIR DEFINITIONS AND FACTOR THOSE IN AND ADD CONDITIONS AS REOUED TO MAKE SURE THOSE ISSUES ARE COVERED. COMMISSIONER MESSNER SECONDED THE MOTION AND THE MOTION CARRIED WITH THE FOLLOWING VOTE AYES: NOES:

ABSTAIN ABSENT: COMMISSIONERS HOWELL, MILLER, MESSNER, MAXFIELD
COMMISSIONER O'BRIEN NONE CHAIRMAN LEHMAN, COMMISSIONER DREW

DOCUMENT 12

VERBATIM MINUTES FROM THE JULY 19, 1995 PLANNING COMMISSION MEETING ON LAKESIDE CEMETERY CONDITIONAL USE PERMIT STATUS REPORT ON CONDITIONS OF APPROVAL AT LAKESIDE OF MAUSOLEUM.

The image is a screenshot of a Microsoft Word document. At the top, a large handwritten word "Withdrawn" is written in black ink. Below it, the title "HISTORIC DISTRICT COMMISSION STAFF REPORT" is centered. The document contains a list of project details in a two-column format. In the top right corner, there is a small text box with the following information: "Agenda Item (NO. 1)", "PN02-258", and "HDC Mtg. 1-15-03". The bottom of the document shows the "PROJECT PLANNER:" section with the name "Jane Talbot, Assistant Planner". The screenshot also shows the Windows taskbar at the bottom with the date "9/3/2020" and time "3:02 PM".

Agenda Item (NO. 1)
PN02-258
HDC Mtg. 1-15-03

Withdrawn

HISTORIC DISTRICT COMMISSION STAFF REPORT

PROJECT TITLE:	Lakeside Cemetery Crematorium
PROPOSAL:	Request for approval of a Conditional Use Permit to allow for the operation of crematorium
RECOMMENDATION:	Denial
APPLICANT AND OWNER:	Lorin Clancy
LOCATION:	1201 Forrest Street
ASSESSOR'S PARCEL NO.:	070-0082-014, 070-0130-002, 070-0130-004
ATTACHED REFERENCE MATERIAL:	<ol style="list-style-type: none">1. Vicinity Map2. Site Plan, dated 12/19/973. Project Description and Crematorium Illustrations4. Documentation from the Sacramento County Historic Cemetery Commission5. Lakeside Cemetery Research Paper written by Sue Silver6. Letters from the Public7. Site Photographs
PROJECT PLANNER:	Jane Talbot, Assistant Planner

To: city of Folsom officials, employees, Mayor, Vice Mayor
 To: City Clerk for CIRCULATION to all City Council elected officials
 bcc
 From: Laurette Laurent
 December 18, 2020

Re: below circulated Public Record, filed with Sacramento County Cemetery Commission December 17, 2020.
 Application for Illegal Zoning District Usages, illegal granting of Variance from HDC Commission with NO State Authority to grant such benefits, violation of CA Govt Code Title Zoning;
 violation of Folsom Zoning Code District restricting NO buildings on any "open space" category -- consistent with VIOLATED State Govt Code;
 Failure to abide by Sunshine Laws; Failure to conduct Full IS, EIR/EIS;
 failure to provide PROOF of Publications for all such steps;
 Failure to OBTAIN Fire Marshal Approvals due to ABUTMENT to Federal Watershed and Federal Forests;
 Failure to Notify CA State Parks;
 Failure to Notify Sacramento County Assessor office of "Proposed changes" [likewise to Cemetery Commission on prior 2020 application];
 Failure to provide NOTICE to an ABUTTING RESIDENT who filed Complaints
 SECRECY in this Land Usage improper SECRET Process - by FAILING to NOTIFY ME -- despite my historic and RECENT objects & Comments with regard to this SECOND time around crematory Proposal -- and my WRITTEN NOTICE to Folsom City Clerk and Planning to be given ALL HDC & Plan Commission Notices.
 Other violations exist, but are previously filed and resulted in Penalties.

Thank you to Folsom city officials and Staff [Including "city top officials" and "city engineers" with Licenses] for proving beyond a shadow of a doubt I -- AN ABUTTING Parcel Owner to 070 0260 001 -- WAS NOT given Notice of Proposed Applications and EXISTING DISCUSSIONS of this outrageous set of Violations.

State laws are extremely explicit with respect to Permissible lower agency Zoning Code and Land Usage Laws applications, TRANSPARENCY, and Process. My Complaints were already filed. My Request for NOTIFICATIONS of all Public Processes regarding Zoning Law were ignored. What is shown below is a PUBLIC RECORD sent to Sacramento County Cemetery Commission BUT NOT to over 36 ABUTTING OWNERS, including TWO Federal Govt AGENCIES with jurisdiction.

Former City Attorney Steve Rudolph advised the Plan Commission to issue a STOP WORK ORDER for All construction of buildings on this cemetery. He further recommended POSTPONEMENT of Plan Commission vote of approval because

1. George Chan had NO opportunity to engage Counsel,
2. Private titled-parcels within the existing cemetery belonged according to Sacramento County Records, to Private persons or entities who were NAMED by County and Steve Rudolph. His legal advice was ignored. Plan Commissioners who changed to approval instead of Opposition ended up on City Council and in one other appointed position.
3. George Chan was RESTRICTED to access the Chun Wah cemetery by FOOT, and only ONE person could enter this large cemetery served at the end of Mormon Street.

All named licensed City Engineers were clearly PUT ON NOTICE yesterday, and yet DID NOTHING. Ditto the City Manager and City Lawyer whom I addressed repeatedly to DO THEIR JOBS.

If Folsom intends to CONTINUE SECRET operations with respect to all State Enabling Legislation, our Constitutional Rights and Sunshine law respect, please continue in this path and expect the consequences of all these extremely VALUABLE "gifts" to private parties. South of Hwy 50 is the same identical type of operation.

I have given and conveyed written Notice to all top staff and licensed engineers. NOT ONE of them has RESPONDED to Formal Complaints. Is this SECRECY or scores and scores of instances and formal complaints IGNORED -- even when complaints CITE the exact LAWS Violated? Mens Rea or negligence since a sitting mayor fired the final independent City Engineer and put Folsom City Charter "ONLINE ONLY", then ERASED City Engineer & CE DUTIES? Folsom FMC Chapter 17 went online with the TOTAL OMISSION of the "cemetery" Zone designation. "Crematorium" was not an allowed use in city historically. FMC 17.52 is an OVERLAY District and HDC does NOT have LEGAL Right to grant any Land Usage Exceptions/Entitlements.

This was submitted in writing to top officials 2020 in formal Complaint Notices.

603 Sutter Street is identical set of SECRECY & Violations.

Why was this submitted to PGE, BUT NOT to Sacramento County Authorities with Oversight & Jurisdiction?

Please, I ask each Licensed Engineer, do you obey the Dictates of your Law Enforcing License -- with respect to Land usage, Subdivisions, Public Infrastructure, and all State Mandated Government Codes?

Please decide whether you wish to continue in this Folsom secrecy. This week alone I asked Mark Rackovan Sr. Eng. Traffic Eng. why he refused to Enforce the November 2020 new law regarding "encroachment on city property". ALL encroachments are defined in FMC, and yet NOTHING WAS DONE to clear a dangerous Substandard Folsom Right of Way of multiples violations. New Encroachment law requires a Permit and CONTRACT to be executed.

Please also BE AWARE, if you claim this is NOT YET in the Public Domain, My/Our Constitutional Rights have been VIOLATED in all PRIOR Lakeside Cemetery gifts from Folsom. No PROOF of Publication was provided for prior instances. No formal Letters were sent to Three Federal Agencies abutting, nor to SP Managing Partner to USBR, nor to 36 ABUTTING Single Family homeowners.

When Claney clan last sought a crematory, our Subdivision brought forth testimony from a Medical expert about the Sites acceptable for a crematory. As usual, abutting parcel owners only learned of crematorium Proposal/Application via Newspaper reporters coverage. Ex post facto, according to Law.

Final Question: this time in 2020, did you ever intend even one licensed city engineer would make an Approval and Plan with his License & seal/signature? Don't you feel Elected Officials should NOT be party to Folsom employees clear, repeated wrongdoing? Don't you feel "at risk" and responsible under your BPLSG Required Duties?

No, MORE/Comment:

Legally this Parcel is on MORMON ST. since the 1850 Theodore Judah Map creating Folsom Street Map. Calling it Forrest causes visitors to jam the Eighteen foot wide Substandard "street" which is DANGEROUS and improper because LNS Subdivision was created and Enacted VIA an Ordinance which DID NOT INCLUDE any Commercial Usages, NOR parking for same.

Applicant for a special **use permit** does not have to show hardship. Instead, the focus is on simply showing that the proposed **use** meets the conditions already contained in the ordinance, whereas a **variance is, in essence, permission to "break the law."** Mar 22, 2013
Case Law citation/ Advisory overlay "committee" cannot grant it under California enabling law.

https://www.opr.ca.gov/docs/theconditionalusepermit_071997.pdf

Quote sent to Sacramento County, others: entered into "Public Domain"
 December 17, 2020

request for comments and associated drawings and pictures for the proposed Lakeside Memorial Lawn crematory located at 1201 Forrest St. A

Conditional Use Permit has been requested by the applicant to operate the crematory facility inside an existing metal shed. That entitlement will go in front of the Folsom Historic District Commission. Please send any comments or questions to me before January 15th if possible.

Named Recipients.

To: Steve Krahn <skrahn@folsom.ca.us>, Aimee Nunez <anunez@folsom.ca.us>, Pete Piccardo <ppiccardo@folsom.ca.us>, Daniel Wolfe <dwolfe@folsom.ca.us>, Bryan Holm <bholm@folsom.ca.us>, Scott Zangrando <szangrando@folsom.ca.us>, Scott Johnson <sjohnson@folsom.ca.us>, Steven Banks <sbanks@folsom.ca.us>, Desmond Parrington <dparrington@folsom.ca.us>, Stephanie Henry <shenry@folsom.ca.us>, Allison Konwinski <akonwinski@folsom.ca.us>, Kristina Eicher <keicher@folsom.ca.us>, Lauren Ono <lono@folsom.ca.us>, Mark Rackovan <mrackovan@folsom.ca.us>, Pam Johns <pjohns@folsom.ca.us>, Elaine Andersen <eandersen@folsom.ca.us>, Jason Browning <jbrowning@folsom.ca.us>, Dave Nugen <dnugen@folsom.ca.us>, Ryan Neves <rneves@folsom.ca.us>, Don Brown <dbrown@folsom.ca.us>, Todd Eising <teising@folsom.ca.us>, Marcus Yasutake <myasutake@folsom.ca.us>, Vaughn Fleischbein <vfleischbein@folsom.ca.us>, Greg Bakken <gbakken@folsom.ca.us>, Steven Wang <swang@folsom.ca.us>

Cc: "thehfra@gmail.com" <thehfra@gmail.com>, Paul Keast
 [REDACTED] HPLBoard <hplboard@hplfolsom.org>, "pgeplanreview@pge.com" <pgeplanreview@pge.com>, "entitlements@smud.org" <entitlements@smud.org>, "projectreview@airquality.org" <projectreview@airquality.org>, "HunleyC@Saccounty.net" <HunleyC@Saccounty.net>, "cemeterycommission@saccounty.net" <cemeterycommission@saccounty.net>, "emailcfb@dca.ca.gov" <emailcfb@dca.ca.gov>, "kmtacc@hotmail.com" <kmtacc@hotmail.com>

Date: 12/17/2020 2:39 PM

Subject: Lakeside Memorial Lawn Crematory Conditional Use Permit

All,

Please find the attached request for comments and associated drawings and pictures for the proposed Lakeside Memorial Lawn crematory located at 1201 Forrest St. A Conditional Use Permit has been requested by the applicant to operate the crematory facility inside an existing metal shed. That entitlement will go in front of the Folsom Historic District Commission. Please send any comments or questions to me before January 15th if possible.

Thanks,

...

**Community Development
Department**
50 Natoma Street, Folsom,
CA 95630
O: 916.461.6209

Note to Sacramento County authorities:

PLEASE ensure ABUTTING PARCELS are notified PRIOR to secret planning & discussions of putting a FURNACE into a rickety shed abutting Federal Forest & watershed & waters of American River. USBR and Managing Partner CA SP and Bureau of Land Management are the major landowners/protectors here.

Cremation of a dead body is carried out at a temperature ranging between **1400 to 1800 degrees Fahrenheit.**

Let's show them some respect for Law, Health & Safety, and Common Sense. That old shed abuts Federal Forest which is suffering during drought. Its "emanations" contain MERCURY, and other toxics.

According to Federal Government Biological Research, there are Known Contaminants associated with burning bodies. Of course Folsom never involves Licensed Engineers; anything for a buck is OK in this place.

[.https://www.ncbi.nlm.nih.gov/pmc/articles/PMC5721279/](https://www.ncbi.nlm.nih.gov/pmc/articles/PMC5721279/)

Josh Kinkade

From: LJ Laurent [REDACTED]
Sent: Sunday, August 22, 2021 4:15 PM
To: Lydia Konopka
Cc: Steven Wang; Mike Kozlowski; Sarah Aquino; Rosario Rodriguez; Josh Kinkade; Ken Cusano; Lauren Ono
Subject: Fw: Failure Notice lots of failures apparently

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Lydia,

Folsom always had appointed citizens equipped with their own email address.

Mr. Duewel is lawyer who asked city staff [non-licensed people] for an opinion about a matter of Law. Mr. Duewel stated he wanted to know more about my Objection letter citations. In response, junior planner stated Opinions about Matters of Land Use Law. Planner erred. I did not.

Please forward this to Kevin Duewel.

If City Attorney has any problems with an appointed person being in touch PRIOR to a decision on Exceptions to Land Use Process Law, he needs to review his License Obligations.

If our elected Council members directed city staff to remove the committee members' emails at @folsom.ca.us, **this** is a prime example of why they should NOT have limited correspondence between city Appointees and city residents during a shutdown for pandemic. One year later Mr. Duewel's request for explanations of my points of law, came to light in the on-going battle to keep us safe from fire.

We in LNS are dealing with the Third Application for crematory abutting OUR Parcels, and federal Watershed Forest. There are significant violations involved, including actions covered by California Penal Code.

This needs to stop.

Thanks in advance, Laurie

Laws cited:

CA Govt Codes, B&P Codes, Subdivision Map Act.

[REDACTED]

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All these years City Attorney has failed to inform "HD" group they do NOT have legal authority to grant Variances, Conditional Use Permits, DE FACTO REZONING, or any "exceptions to Zone District laws, Standards, or Duties of City Engineer to seal/sign and certify all actions are correct under state laws..... and upon certification also by a licensed lawyer.

I know city elected officials have difficulty believing city hall people are perfectly correct and able to administer Legal Process Laws, but this crematory nonsense -- for the 3 time -- is an outrage.

Residents of Lake Natoma Shores have to look at that ugly new shed every single day, yet young staffer proceeded as if we don't even exist. What do you want to bet the tall height which "cools" the building" will become another, larger bunch of furnaces for human bodies?

Over-Extending the Legal Authority of an "overlay" group, even a lawyer voted to approval extension of Land Use Exceptions which they NEVER EVEN VERIFIED.

City Attorney Steve Rudolph told the Plan Commission back in 1992 cemetery hearings on these outrageous exceptions that he advised against. He recommended a STOP WORK ORDER, and our future council members, ignored his Work Product Advice to them. Steve Rudolph sat and gave legal Opinions at all City Council and Plan Commission meetings. That was not popular with mayor.

In fact, those on Plan Commission who switched their opposition to approval, and voted, before Mr. Chan had opportunity to engage Counsel, voted to restrict Mr. Chan's access to a National Historic Site to "one person, on foot only." It is so "Folsom."

Question: webcast: someone asked "Mrs. Rodriguez" if she had any questions. Was this our current councilperson?

My favorite crematory protests say "don't let the dead kill the living." "Like the smoke, more is coming, with Mercury in it." Children in LNS have black balloons out front.

Did you all know cemetery only got water line of mere 3" diameter with 2" limiter, because we in LNS paid for water lines? Cemetery has NO hydrants, and won't pay to extend 4000 PSI pipes which fire fighters must have.

Did you know crematory will go into the 1930's metal shed, and that's why a huge 18 foot tall new shed was built. They don't need more maintenance equipment: they butchered more than 7 trees, including the living protected Historic Oaks. They cut "grass" cutting down last winter -- less to save money paid for water through that tiny connection we enabled. They committed criminal acts which still have not been cited [to our knowledge].

Sorry, I just cannot understand how it "slipped the minds" of our city attorney and city engineer to do their advisory jobs, to supply signed opinions of legal compliance.

Mr. Duewel wanted to KNOW, prior to voting. Then he said "but it's only a design review". But that is not what it was. It included an illegal Variance or Condition Use Permit, or "grandfathering in" an old public cemetery -- according to webcast. He actually believed that junior staffer saying that "yes" it's all grandfathered in.

Poor Oak Chan, he's probably suffering in his unreachable Grave with large monument - --- back there in the inaccessible National Historic Site. Why did planner fail to ask a Licensed city professional to review his Recommendation?

Why were the Minutes approved by a person who was not even present to know there were questions?

"HD" is NO zone; their powers are not those of 'commission'. It's a pathetic violation of higher Authorities in Law, and people believe what they are told at cityhall [apparently.]

----- Forwarded Message -----

From: MAILER-DAEMON@yahoo.com <mailer-daemon@yahoo.com>
To: [REDACTED]
Sent: Sunday, August 22, 2021, 03:21:03 PM PDT
Subject: Failure Notice

Sorry, we were unable to deliver your message to the following address.

<kduewel@folsom.ca.us>:

550: 5.4.1 Recipient address rejected: Access denied. AS(201806281) [DM3GCC02FT008.eop-gcc02.prod.protection.outlook.com]

----- Forwarded message -----

Kindly supply email address, as your .com page rejected my contact.

Josh Kinkade

From: LJ Laurent [REDACTED]
Sent: Sunday, October 3, 2021 9:31 AM
To: Pam Johns; Josh Kinkade
Cc: ernest.conant@usbr.gov; Drew Lessard; opinion@sacbee.com; Eileen Sobeck; Patrick Pulupa; Dale Kasler; The HFRA; daoffice@sacda.org; Rick Hillman; Ken Cusano; Lydia Konopka
Subject: body burner FURNACE is "HEATING UP." Folsom what R U thinking?

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To: Folsom Comm. Dev. director P. Johns; planner Josh Kinkade
 Asst. City Clerk: Please share with 5 council members
 From: LJ Laurent, LNS Sub. Abutting Neighbor
 October 2, 2021

Re: Telegraph published article VS Reality of city staff

<https://goldcountrymedia.com/news/202040/proposed-folsom-crematory-opposition-continues-to-heat-up/>

Furnace Application is the third in few years. **NOT ONCE** were Abutting forest/river protection US Agencies given Notice, according to city records/minutes.

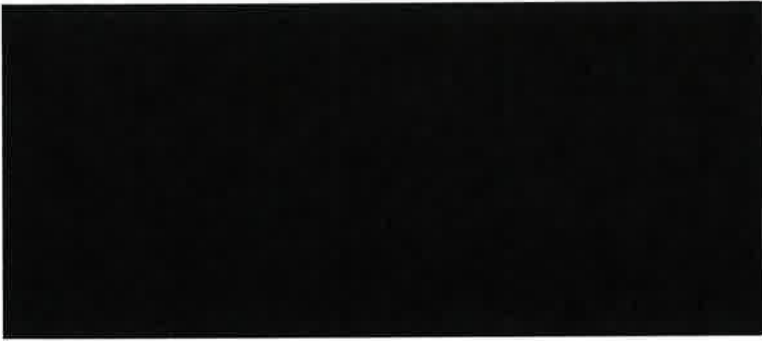
This application was filed over ONE YEAR ago, and yet Sacramento County Recorder was Never Notified of Project in Public Domain. In fact city reports this parcel 070 0260 001 as "Open Space/OS Conservation Zone District." Tax bill 10/2020 was REDUCED by \$1.2 Million, to help "open space".

New co-owners Caring Service Group, has no city Business License. They filed all their Building Permits using FALSE ADDRESSes, and in some cases, a totally non-existent Parcel Number. The Zone District is clearly parcel is NOT OPEN SPACE Conservation - which means ZERO BUILDINGS. They built and plan more huge Commercial buildings on Parcel, a FURNACE, and huge LPG Gas Tanks.

Clearly Applicants are not concerned with "legal niceties." Since city made residents pay for a "study" which recommended "Approval" of this Zone District Exception", it is clear the city is at the foundation of this chicanery.

LNS Residents were charged with Code Violations for lawful protest and WARNING Signs. Cemetery got away, forever, with NO Posting of Application for Exception for body burning 2,400 F furnace adjacent to Federal Forest, river, homes.

[Proposed Folsom crematory opposition continues to heat up](#)



Proposed Folsom crematory opposition continues to heat up

As residents eagerly wait for a meeting date to be announced, the opposition against the proposal for a new Fols...

"Folsom city staff originally anticipated a September hearing on the matter, but Planning Commission and Historical Commission hearings were then canceled for September.

"We had initially targeted a Sept. 15 date, but it was never officially scheduled for that date," said **Folsom planner Josh Kincade**. "We don't currently have a meeting date set but there will be a notice (in local newspapers) 20 days prior to the hearing and the notice will be physically sent to property owners within 300 feet of the project site at the same time."

<https://www.youtube.com/watch?v=Lr15rPHEmeQ>

"heating up is right" fire & debris would spread for miles.

Folsom does NOT publish "city planner JOB description".

Only "Park planner" is available now. It is LESS demanding than city planner, and pays less, but here it is.

Salary 2018 \$58,353.84 - \$89,071.08 Annually

THE POINT IS: that is BIG BUCKS for persons who might not even KNOW basic Public Notice laws, and comprehend their **Obligations** which come with that kind of money.

Comm Dev director makes \$15,388 per Month salary, or about **\$190,000 a year in salary**. That is a lot of money PLUS huge benefits. And we don't even get PROPER, Legally-mandated Posted Notices and USPS Mail Notices. We get citations.

As a Federal Tax Payer, I firmly believe our USBR, DOI, and BLM federal employees DESERVE a NOTICE of a body-burning FURNACE be delivered to their Offices via Certified Mail.

Folsom acts like an insulated power-structure with very little respect for Laws, Rights, and Protection of Water Supply and our Environment.

Folsom staff have OK'd the 'study' for Approval. No Fire Dept objections were in that study. No ONE cited Zoning Code, Fire Code, Street ACCESS Code, etc. Development staff give an impression to us, that they think our Fire and Police people are as disposable as those bodies who represent Revenue from OPEN SPACE Zone.

You get the message? This is a highly paid group who have incentives to deliver the "Folsom Version" of laws.

Residents? We get the "wrong end of the match-book."

If you knew the Extent of the Expertise behind LNS Objections, you might be ashamed of yourselves. But, at those reimbursement rates and rewards, guess it is unlikely.

LNS residents exist between the unbelievably dry Federal Forest & Watershed of American River, and the incredibly DANGEROUS gas tanks and furnace on Parcel Zoned OPEN SPACE/OS Conservation. If even one of those huge LPG tanks explodes, Folsom Blvd., Light rail, and lots of drivers and businesses in old sutter, will all take the "hit" with us. VIEW VIDEO!

Folsom, It this hell, or what?

Below is only city Job Description online, for a "planner".

EXAMPLES OF ESSENTIAL DUTIES:

NOTE: The following are the duties performed by employees in this classification. However, employees may perform other related duties at an equivalent level. An individual in the classification does not necessarily perform all the

- Prepare preliminary construction drawings, specifications, and cost estimates for landscape architectural projects and bid documents including the grading, drainage, paving, irrigation, plant materials, and play equipment.
- Participate in site analyses, master planning, designing, cost estimating, and reviewing landscape architectural plans and drawings for accuracy and conformance with design and check calculations used in designs and estimates.
- Review, check, and make necessary corrections to landscape architectural site plan drawings, designs, and estimates as prepared by consultants to ensure conformance to original design.
- Inspect park, recreational, and trail site projects during construction to ensure construction is performed according to plans and specifications.
- Interpret city standards and specifications regarding trail construction.
- Prepare a variety of charts, graphs, maps, plans, and other illustrative presentations to the Parks and Recreation Commission and the Finance Commission, reports, and studies.
- Monitor and approve project expenditures and maintain project accounts.
- Analyze proposed projects for conformance with adopted city landscape architectural standards.
- Meet and confer with developers, contractors, and engineers.
- Prepare grant applications and necessary graphics for a variety of projects.
- Compile a variety of information and data for staff reports and studies.
- Perform related duties as assigned.

Josh Kinkade

From: Scott Johnson
Sent: Friday, January 28, 2022 5:23 PM
To: Josh Kinkade
Subject: Fwd: Crematorium

Sent from my T-Mobile 5G Device
Get [Outlook for Android](#)

From: Liz Byer [REDACTED]
Sent: Friday, January 28, 2022 5:10:40 PM
To: Scott Johnson <sjohnson@folsom.ca.us>
Subject: Crematorium

[You don't often get email from [REDACTED] Learn why this is important at
<http://aka.ms/LearnAboutSenderIdentification.>]

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Please stop this crematorium now! Summers are barely tolerable due to the fires now! This will destroy our property value just like the meat rendering plant south of Rancho Cordova!

Liz and Andrew Byer
[REDACTED] Persifer street

Sent from my iPhone

Josh Kinkade

From: Elaine Andersen
Sent: Friday, January 28, 2022 5:09 PM
To: Josh Kinkade
Subject: Fwd: Crematorium

Begin forwarded message:

From: Liz Byer [REDACTED]
Date: January 28, 2022 at 5:07:49 PM PST
To: Elaine Andersen <eandersen@folsom.ca.us>
Subject: Crematorium

[You don't often get email from lizbyer717@icloud.com. Learn why this is important at <http://aka.ms/LearnAboutSenderIdentification>.]

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Please stop the crematorium in old Town! This will destroy our reputation! Summers are getting harder and harder to enjoy as it is with fires! Please stop this now!

The Byers
[REDACTED] Persifer street

Sent from my iPhone

Marie Rice

██████████ Young Wo Circle

Folsom, California 95630

July 27, 2021

Dear Mr. Kinkaid:

I understand the owner of the Lakeside Memorial Lawn cemetery located at the entrance of "The Preserve" neighborhood off Folsom Boulevard is planning to put a crematorium in his facility. He states his plans are to run his crematorium Monday through Friday each week, estimating the cremation of three bodies per day. This will require the use of two-250-pound propane tanks. He assures his process is safe and compliant with fire and environmental safety standards.

While this may be true, accidents can and do happen. From 2012 until May 2021, there have been some seventeen (17) crematory accidental fires in this country, that I am aware of. The most recent took place on 05-11-2021 in Corona, California. The actual cause of this fire was not made clear. The one before that was in May 2018, in San Diego County. Firefighters were able to put out the fire, but the fire spoke's person confirmed the continued presence of "sand clouds of human remains in the sky". This is what nearby residents were breathing in.

There were several others before these I could detail, but in the interest of space, will refrain.

Especially in this time of climate change and easily ignited fires, a crematorium in a residential neighborhood, in the middle of a small city, *is an unnecessary public safety risk*. As it is, our landscape is so dry that we must be vigilant throughout the summer months to prevent fires.

Should a fire break out in this prospective crematorium, the people in this entire neighborhood would be at risk. There is only one exit, exactly where this cemetery is located. Residents could be trapped.

I have additional concern about air quality. The Amigone Cemetery cremation center in upstate New York was shut down in 2010 by the State's Department of Environmental Conservation because of "heavy black smoke spewing from the crematorium". Equipment repairs were made, and the crematorium was re-opened in 2019. Even still, nearby residents continued to experience distinctive odors and dust particles on windowsills, they said were "human remains". This has been disturbing enough that these residents report reluctance still to leave their windows open or spend time outside.

We ask you to please not permit this crematory project of the Lakeside Cemetery to occur. It simply is NOT SAFE in this location that is near people and their homes that have only one exit, near the fire source itself.

Sincerely,


Marie Rice

DR. MARIKO MCGARRY

1204 Forrest Street, Folsom, CA 95630 • 530-545-9055 • mariko.mcgarry@gmail.com

February 1, 2022

Dear Historic District Commissioners, City Manager, and City Planners,

Thank you for the time and attention you are dedicating to the voters' serious concerns about the proposed Miller's Funeral Home crematorium at Lakeside Memorial Cemetery. I am writing to you as a permanent resident of Forrest Street, living directly across from the Cemetery. I am greatly worried about the impact of your decisions to come on my family, one year old daughter, and high-risk neighbors. I have reviewed Lakeside's project proposal and Environmental Impact Report conducted by Helix Enterprises and sought professional environmental science and medical consultation to inform my position on this matter. The concerns I am requesting inform your decision to vote "NO" include:

1. **Nonconforming use** of commercial, industrial incinerator in open space and in contradiction to the City's master plan;
2. **Inequitable consideration** of crematorium proposal in the Historic District when other open space in Folsom is not being considered for approval driven by profit motives;
3. **Proximity** of the proposed crematorium next to residential zone to several historic sites in the Historic District, with potential to negatively impact cultural practices and significance;
4. **Negative impact the proposed crematorium has already had on home values** in the historic district, with several realtors/brokers documenting decrease in buyers and offers due to proposal;
5. **Assumption that there is a safe level toxicity exposure** to developing children, pregnant women, elderly, medically vulnerable, and veterans in the immediate surrounding area of the Cemetery from vaporized toxins;
6. **The absence of historical relevance**—if external French doors are not permitted in the Historic District because they would not be found in the early 1900's, how can a commercial, industrial incinerator be acceptable?
7. **Impact of immeasurable sight, smell and ingestion of particulate matter** on use of trail, river, Historic District, and Sutter Street;
8. **Use and appearance** of shed for crematorium that is permitted for commercial storage only;
9. Increase to existing **extreme fire risk with one evacuation route for residents**;
10. **No emergency services plan or allocation of resources** to support potential increase in demand for fire and police as required by CEQA guidelines;
11. **Insufficient fencing and security** on site as required by CEQA guidelines;
12. **False identification** of proposed location as not next to State land or increased fire risk in Initial Study and **absence of additional design measures** to create defensible space as required by CEQA guidelines.
13. **Stark differences** between crematorium application to City in 2020 and the work that Miller's Funeral Home has done at Lakeside Memorial Cemetery without approval;
14. Presence of **permanent, industrial pollutant** in Historic District;
15. **Absence of ongoing monitoring** of air quality testing of particulate matter, employee safety, and testing by air quality chemist, pediatric toxicologist, and geriatric toxicologist;

DR. MARIKO MCGARRY

1204 Forrest Street, Folsom, CA 95630 ♦ 530-545-9055 ♦ mariko.mcgarry@gmail.com

16. **No monitoring of mercury** which is odorless and colorless--yet poses biggest risk to children and unborn babies who can experience disorders such as autism, Asperger's, developmental delays and other neurological issues.
17. **Significant delays by the City** in publishing application, conditional use permit, and reports to the public;



The Initial Study/Mitigated Negative Declaration conducted by the City of Folsom Community Development Department and Helix Environmental Planning related for January 2020, identifies the "Maximum exposed individual resident," in the photo above. That purple dot sits directly on our **one-year old's bedroom window**. I am hard pressed to find an elected official in this City who has attested to being comfortable with even the smallest risk posed to their children. I am even harder pressed to find an elected official who has attested to the belief that this proposal would be accepted in any *other* residential or open space zone in Folsom. From an equity perspective, the voters and children of Empire Ranch would never have to self-organize, self-fund, and self-advocate as the voters and children of the Historic District have been forced to do for the last two years.

The Historic District and the City of Folsom either believes and will testify to their commitment to maintain community open space resources for the purposes ranging from conservation, to preserving community land use options, or it is time to change the definition of open space zones and the public marketing of the City.

As a Historic District Resident, voter, professional, and parent, I cannot express the need to vote "No" on the Lakeside Memorial Lawn Crematorium enough. *Protect history. Protect the City's legacy. Protect our children.*

Sincerely,

Mariko McGarry, Ph.D.

DR. MARIKO MCGARRY

Forrest Street, Folsom, CA 95630

February 1, 2022

Dear Historic District Commissioners, City Manager, and City Planners,

Thank you for the time and attention you are dedicating to the voters' serious concerns about the proposed Miller's Funeral Home crematorium at Lakeside Memorial Cemetery. I am writing to you as a permanent resident of Forrest Street, living directly across from the Cemetery. I am greatly worried about the impact of your decisions to come on my family, one year old daughter, and high-risk neighbors. I have reviewed Lakeside's project proposal and Environmental Impact Report conducted by Helix Enterprises and sought professional environmental science and medical consultation to inform my position on this matter. The concerns I am requesting inform your decision to vote "NO" include:

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DR. MARIKO MCGARRY

Forrest Street, Folsom, CA 95630 • •

16. **No monitoring of mercury** which is odorless and colorless--yet poses biggest risk to children and unborn babies who can experience disorders such as autism, Asperger's, developmental delays and other neurological issues.
17. **Significant delays by the City** in publishing application, conditional use permit, and reports to the public;



The Initial Study/Mitigated Negative Declaration conducted by the City of Folsom Community Development Department and Helix Environmental Planning related for January 2020, identifies the "Maximum exposed individual resident," in the photo above. That purple dot sits directly on our **one-year old's bedroom window**. I am hard pressed to find an elected official in this City who has attested to being comfortable with even the smallest risk posed to their children. I am even harder pressed to find an elected official who has attested to the belief that this proposal would be accepted in *any other* residential or open space zone in Folsom. From an equity perspective, the voters and children of Empire Ranch would never have to self-organize, self-fund, and self-advocate as the voters and children of the Historic District have been forced to do for the last two years.

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As a Historic District Resident, voter, professional, and parent, I cannot express the need to vote "No" on the Lakeside Memorial Lawn Crematorium enough. Protect history. Protect the City's legacy. Protect our children.

Sincerely,

Mariko McGarry, Ph.D.

Mary Johnson

██████████ Young Wo Circle Folsom CA ██████████

January 30, 2022

City of Folsom Historic District Commission

RE: Opposition to the proposed Folsom Lakeside Crematorium Project

Dear Commissioners,

I am writing this letter to voice my concerns regarding the proposal to build a crematorium right behind my house. This is the second letter that I have sent regarding my opposition to the crematorium. My concerns include negative impacts on my health and the health of the community, enjoyment of my property, the likely devaluation of the value of my property and degradation of our natural environment.

I have read the letter from Igor Semenyuk titled "Scope of Work- Installing a Crematory" and take issue with many of it. First and foremost, the crematory is not "removed from any residences and Parks", my home is literally feet from the proposed crematory site and the property borders State Park land with historical and major natural resources and wildlife, including endangered Bald Eagles. In his "Cremation Process" section he states "there is no smoke or residue that leaves the chamber" This is a completely false statement contradicted by the study he paid for and was completed by Helix Environmental Planning. There will be toxic emissions, particulates and a significant amount of CO2 released into the environment from the cremation process.

I am a retired Oncology/Hematology RN and Hospice RN, so I have cared for patients prior to and during the death process. There are multiple medical implants, catheters, joint replacements and dental work that when cremated and release toxic substances. Not to mention the toxic pharmaceuticals (chemotherapy, radioactive pellets and drugs) that remain in human tissue and are vaporized during the cremation process. I have read the Assessment completed by Helix Environmental Planning and am concerned that the above referenced toxic substances were lumped in Reactive Organic Compounds (ROC) or (PM) without stating how dangerous these emissions can be. The Report on the Air Quality and Greenhouse Gas impacts of the Crematorium and the Health Risk Assessment does not address our concerns about Public Health and Greenhouse gas emissions posed by the crematorium. The report done for CEQA (California Environmental Quality Act) is not complete and does not fully support a full environmental review process.

I was concerned that the Helix report only briefly mentions the release of mercury from cremation. No estimates were provided and the data mentioned was from the manufacturer of the crematorium unit. A report on the NIH website states that "Crematoria represent a significant source of mercury emissions to the environment". "The danger with mercury is long term indirect exposure from the environment and the food chain". This is because mercury can accumulate over time in our tissues and becomes a potent neurotoxin. It's also linked to decreased intelligence in children. Mercury is also known to cause kidney damage. We live in a neighborhood where we eat fruits and vegetables from our yards and people and wildlife catch and consume fish from Lake Natoma. The Lake Natoma/ American River is a major source of drinking water for over a million people or more. Why wasn't this mentioned or reviewed?

Another area that wasn't covered in the Helix report was the the amount of greenhouse gasses that will be produced, specifically CO2 . There are multiple Initiatives from the state, federal and international agencies that are attempting to reduce greenhouse gas production and the Crematorium will be a significant producer of CO2 . The crematory proposes to burn propane for the cremation process, per their report, each cycle burns 19.672 gallons with a proposed 4 cycles a day and 500 cycles per year. From the U.S Energy Information Administration, each gallon of propane burned produces 12.61 lbs of CO2. Multiply that by 19.67 gallons per cycle times 4 cycles per day. It comes to 992 lbs of CO2 produced per day. 500 cycles per year produces 124,032 lbs of CO2. This is a large amount of CO2 production in our neighborhood which adds to our existing elevated levels of CO2. Why was this not fully evaluated? Have alternatives been fully considered? Maybe Igor Semenyuk could be a leader in the industry by investing in Aquamation for human remains, which is a significantly less polluting and has a lower carbon footprint.

I also take issue with the statement from the Helix report that states " The project was not found to be a substantial source of objectionable odors and odor imparts." There is no methodology, background or citations given to base that statement on. Do we have to rely on their word or will they provide more information on which to base that claim? From the articles I have read, odors and smoke are the main complaint from neighbors who live near crematoriums.

My personal concerns are that the smell from burning bodies will be a daily occurrence and will interfere with the enjoyment and value of my property. I love to spend time gardening, generally enjoying my backyard, which backs up to the cemetery and is very close to the proposed crematory. This will be impacted by the presence and smell of crematorium operations. I am also concerned about the health impact of breathing the emissions from cremations. Breathing residue from human bodies being burned, including substances which are toxic to humans and wildlife and particulates have been shown to have major health adverse impacts and are known to shorten lives. There are personal financial consequences too. Having a Crematorium behind my home will decrease my resale value as most future purchasers will find it upsetting and distasteful to have a crematorium out back.

I do hope this letter helps you to realize that the crematorium project will have negative health and pollution consequences and that it does not belong in our neighborhood and historic Folsom. Please demand a greater articulation of the consequences of this proposal. Better yet, please deny the project.

Sincerely,

Mary Johnson

Josh Kinkade

From: Scott Johnson
Sent: Monday, February 7, 2022 4:17 PM
To: Josh Kinkade
Subject: FW: Please Vote NO on Proposed Crematorium

Scott A. Johnson, AICP
Planning Manager
Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6206



From: John (Dave) & Mary [REDACTED]
Sent: Monday, February 7, 2022 4:16 PM
To: Scott Johnson <sjohnson@folsom.ca.us>; Steven Banks <sbanks@folsom.ca.us>
Subject: Please Vote NO on Proposed Crematorium

Some people who received this message don't often get email from [REDACTED] [Learn why this is important](#)

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Scott Johnson, Planning Manager
Steve Banks, Principal Planner

I am writing as a concerned citizen that lives in the adjacent neighborhood of the proposed crematorium. My concerns are from the heart. I have loved ones that have been laid to rest at the Lakeside Cemetery and visit often. The cemetery is a beautiful place to walk through and visit. The wildlife I have seen there over the years has brought much pleasure to me. In the last several years a pair of bald eagles have been successful in nesting and have been quite successful in growing their family and growing their species. One of my concerns is about the long-term environmental impact the crematorium emissions would have on the future successes of the bald eagle population. Air quality for the neighborhood would be another major concern, especially for those with health issues such as asthma. The neighborhood is a mix of young, growing families and retired families. The area is known for its healthy environment and healthy style of living. The industrial business just is not a good fit or in the best interest of the people of Folsom.

The site is zoned OS/P (Open Space/Public Primary Area) with underlying zoning of OSC (Open Space and Conservation) and has a General Plan designation of OS (Open Space). The Historic District Commission will take final action on this request unless the decision is appealed to the City Council. I am asking the City Council to vote NO on the proposed crematorium.

Thank you for your consideration,

Mary Matthews
[REDACTED] Young Wo Circle
Folsom, CA 95630
[REDACTED]

HFRA Mission Statement

The Historic Folsom Residents Association is a group of neighbors working together to protect, preserve, advocate for and enhance the Folsom Historic Residential District. Through community involvement and awareness, HFRA shall strive to collaborate and build strong partnerships with City Government and local entities to identify and resolve current and future neighborhood concerns affecting the Folsom Historic District. We are united by our love for the community and desire to maintain and improve the quality of life for residents and defend the historic integrity of this neighborhood for future generations.

Josh Kinkade

From: Kelly Mullett
Sent: Thursday, September 30, 2021 9:00 AM
To: Josh Kinkade
Subject: FW: Proposed Crematorium at Lakeside Cemetery

FYI

Kelly Mullett
Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



----- Forwarded Message -----

Subject: Proposed Crematorium at Lakeside Cemetery
Date: Thu, 30 Sep 2021 00:26:29 +0000 (UTC)
From: Mary Rigney [REDACTED]
To: Daron Bracht <daronbr@pacbell.net>

September 30, 2021

Dear Historic District Commissioner:

Bottom line – who benefits from having a crematorium in our neighborhood? The neighbors? The City? Caring Service Group/Miller Funeral Home? The Chinese historical sites? The Folsom Historic District? The children or elderly of our community, especially with health issues? Not likely! It’s definitely pause for speculation & concern.

Please help us, the Folsom residents within the historic district, and specifically The Preserve neighborhood, to reject the proposal to have a crematory built where we live our daily lives.

You are already well-informed of our many objections & serious concerns of having this crematory approved, so I will not list them here, but I don’t believe it’s asking too much to want clean air, clean lakes, and good health. And this doesn’t even include our worries about fires in our high-risk fire area with 500 gallons of propane present at the cemetery property for the cremations, and what invisible harmful particles might be in the air caused by the use of a crematorium 4-6 times a day. It would seem more plausible to build such a crematorium in an industrial area, or use another facility of which there are a number of them in our county.

And yes, I’m over 75 and live in a receptor area.

Thank you for listening to our concerns and worries,

Mary Rigney

██████ Young Wo Cir.

Folsom, CA 95630

████████████████████

Josh Kinkade

From: Daron Bracht <daronbr@pacbell.net>
Sent: Tuesday, March 2, 2021 11:10 AM
To: Josh Kinkade; Steven Banks
Subject: Fwd: Crematorium

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----- Forwarded Message -----

Subject: Crematorium

Date: Mon, 22 Feb 2021 19:33:19 -0800

From: Mary Rigney [REDACTED]

To: daronbr@pacbell.net

Dear Daron,

I'm hoping I'm not too late in voicing my concern about having a crematorium in our Preserve neighborhood, this being the second attempt by Miller's Funeral Home. Please pass my concerns onto those making the decisions in this serious matter.

We've already voiced our strong feelings against having dead bodies burned in our own backyards, yet we are being faced with this same issue one more time! I'm sure that it is sanitary & safe, but that is not the point. I don't see why this can't be done elsewhere in an industrial area that would be far more fitting than to disrupt an entire neighborhood who have already voted against this project.

It seems we're being hit hard right now with other propositions which would drastically alter our quiet safe haven, our main reason for choosing this area to live in the first place. Between the constant battle with the future of the Corp yard (most recently affordable housing!), the light rail adding more tracks doubling the stops at our ONE outlet for our community, and now the crematorium issue on top of it all!

Therefore, I would like to join my other neighbors in requesting reconsideration of the projects I've mentioned above. Thank you for your time & listening to my concerns, and for sharing with the those making the decisions, it's so appreciated.

Mary Rigney
[REDACTED] Young Wo Cir

Josh Kinkade

From: Scott Johnson
Sent: Tuesday, October 5, 2021 1:54 PM
To: Josh Kinkade
Subject: FW: Crematorium

FYI

Scott A. Johnson, AICP
Planning Manager
Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6206

<https://gcc02.safelinks.protection.outlook.com/?url=http%3A%2F%2Fwww.folsom.ca.us%2F&data=04%7C01%7Cjkinkade%40folsom.ca.us%7C50bf4ca4ea2e4bc1577f08d988424d8c%7C1cfb4b4a254c47b48448af71335fd6c0%7C0%7C0%7C637690640630553939%7CUnknown%7CTWFpbGZsb3d8eyJWIjoiMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTiI6IjEhaWwiLCJXVCi6Mn0%3D%7C1000&data=AiANp%2BBBMLYcwxlm2T8DcRoOeVvsajtejOHlqoOV3Y4%3D&reserved=0>

-----Original Message-----

From: Megan [REDACTED]
Sent: Tuesday, October 5, 2021 1:37 PM
To: Scott Johnson <sjohnson@folsom.ca.us>
Subject: Crematorium

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<<http://aka.ms/LearnAboutSenderIdentification>>

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Please do not allow a crematorium to be built behind my house. I have lived here for 22 years.

Megan McClure
[REDACTED] Young Wo Circle

From: [Elaine Andersen](#)
To: [Josh Kinkade](#)
Subject: FW: Vote NO -My only sitting porch faces proposed Crematorium location directly
Date: Friday, February 4, 2022 1:03:18 PM
Attachments: [Front Porch.jpg](#)
[Front Porch3.jpg](#)
[Sitting.jpg](#)
[Folsom-Vol-Day.jpg](#)

From: Missy <mrdsoccer1@aol.com>
Sent: Friday, February 4, 2022 1:01 PM
To: Elaine Andersen <eandersen@folsom.ca.us>; Elaine Andersen <eandersen@folsom.ca.us>
Subject: Vote NO -My only sitting porch faces proposed Crematorium location directly

You don't often get email from mrdsoccer1@aol.com. [Learn why this is important](#)

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Ms. Anderson:

I am a direct sight line neighbor of where the proposed crematorium is to sit. We already contend with the very unfavorable smell of the Kikkoman plant any day the wind is our direction from it, which is frequent. So this doubles our concern as, if Kikkomans can't control odor how on earth will the Crematorium be able to control theirs?

But to have to go out of my front door, look out my only front living room windows or to go sit on my only porch and stare at a stack from a Crematorium is a real slap in the face and our main concern.

I am a NICU nurse and deal with enough death on the daily at my job. I really don't appreciate having not only to deal with it at work but literally stare at it at home daily.

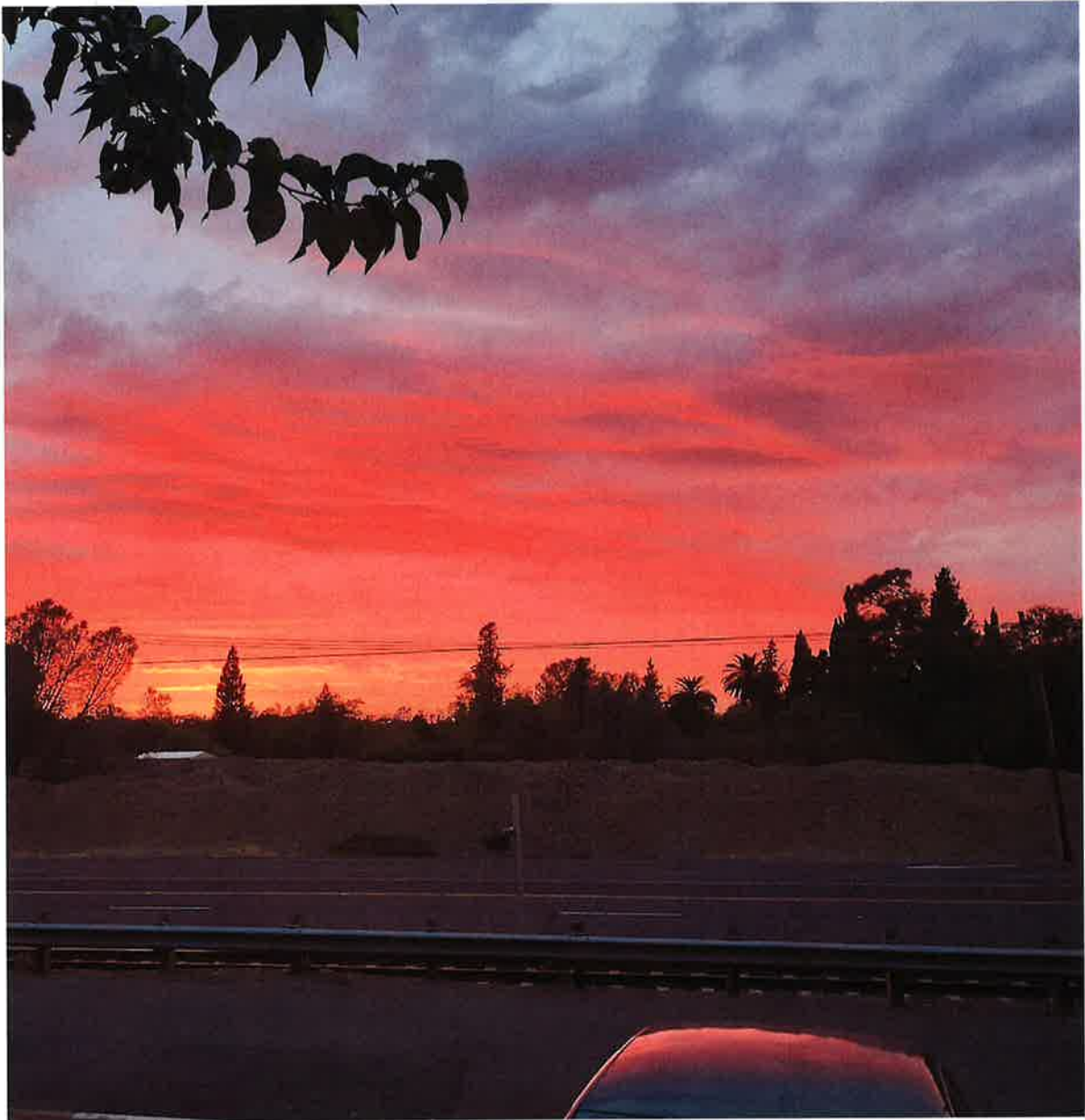
I have attached photo's that are taken from my front porch. I take pics almost weekly since the sunset is so beautiful from my front porch. This is apparently, exactly where the stack will stand. So no more front porch sitting for me, again - it's my only porch, it's my only sight line outside since our windows from our living room face that direction.

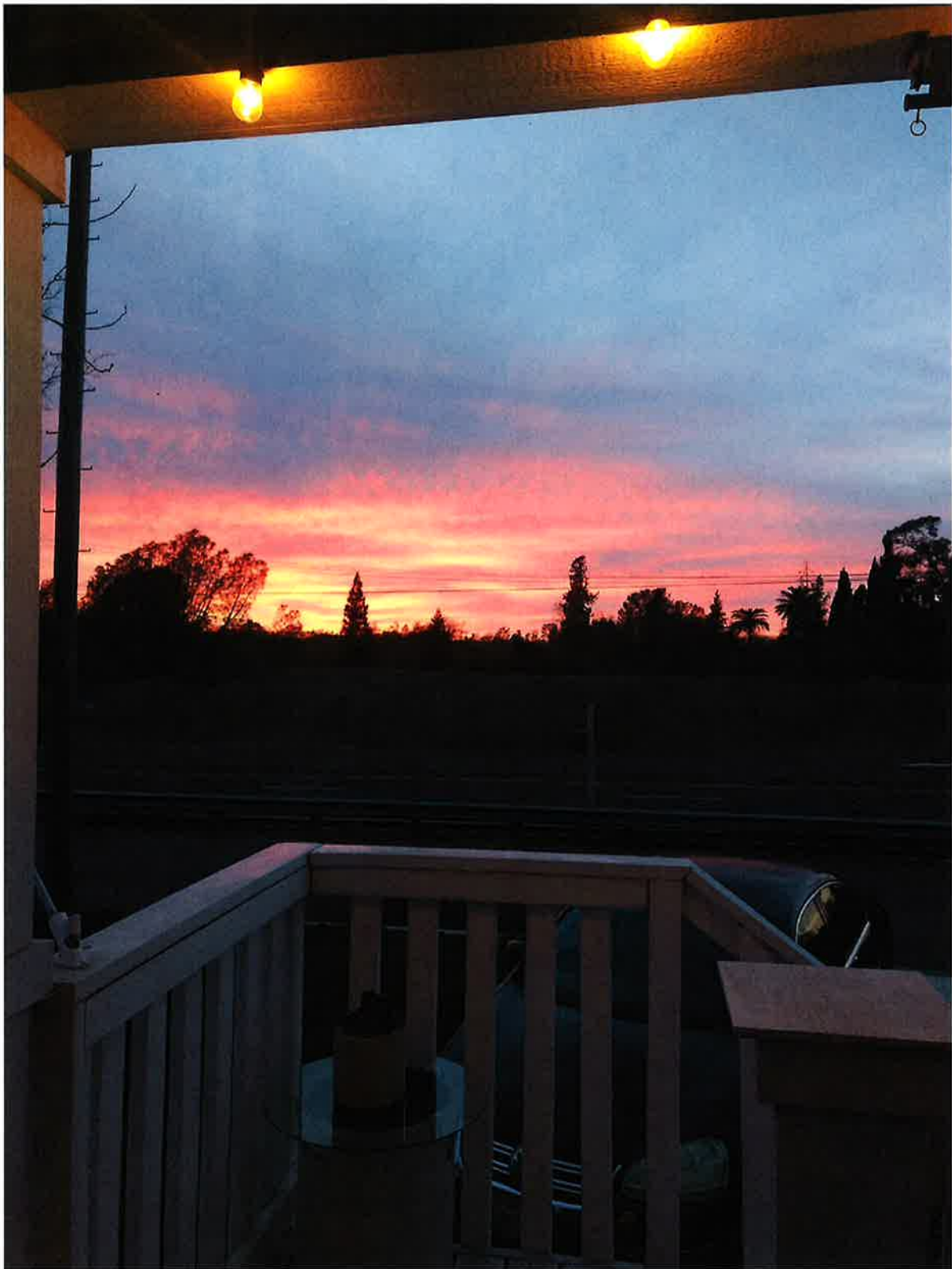
We volunteer in Folsom Days, we actively help keep an eye on the Historic Rail Yard which is our neighbor, we speak to Jim and other staff occassionally through the year. We spend as much money locally as possible.

PLEASE VOTE NO - I oppose the Crematorium location and I expect any council or official representing me to oppose it's location in this residential area.

Thank You Respectfully,

Melissa (Missy) Ingle
 and wife
 Betty (Renee) Reed
 713 Oakdale St, Folsom
 812-929-9075









From: [Elaine Andersen](#)
To: [Josh Kinkade](#)
Subject: FW: Vote NO -My only sitting porch faces proposed Crematorium location directly
Date: Friday, February 4, 2022 1:03:18 PM
Attachments: [Front Porch.jpg](#)
[Front Porch3.jpg](#)
[Sitting.jpg](#)
[Folsom-Vol-Day.jpg](#)

From: Missy [REDACTED]
Sent: Friday, February 4, 2022 1:01 PM
To: Elaine Andersen <eandersen@folsom.ca.us>; Elaine Andersen <eandersen@folsom.ca.us>
Subject: Vote NO -My only sitting porch faces proposed Crematorium location directly

You don't often get email from [REDACTED] [Learn why this is important](#)

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Ms. Anderson:

I am a direct sight line neighbor of where the proposed crematorium is to sit. We already contend with the very unfavorable smell of the Kikkoman plant any day the wind is our direction from it, which is frequent. So this doubles our concern as, if Kikkomans can't control odor how on earth will the Crematorium be able to control theirs?

But to have to go out of my front door, look out my only front living room windows or to go sit on my only porch and stare at a stack from a Crematorium is a real slap in the face and our main concern.

I am a NICU nurse and deal with enough death on the daily at my job. I really don't appreciate having not only to deal with it at work but literally stare at it at home daily.

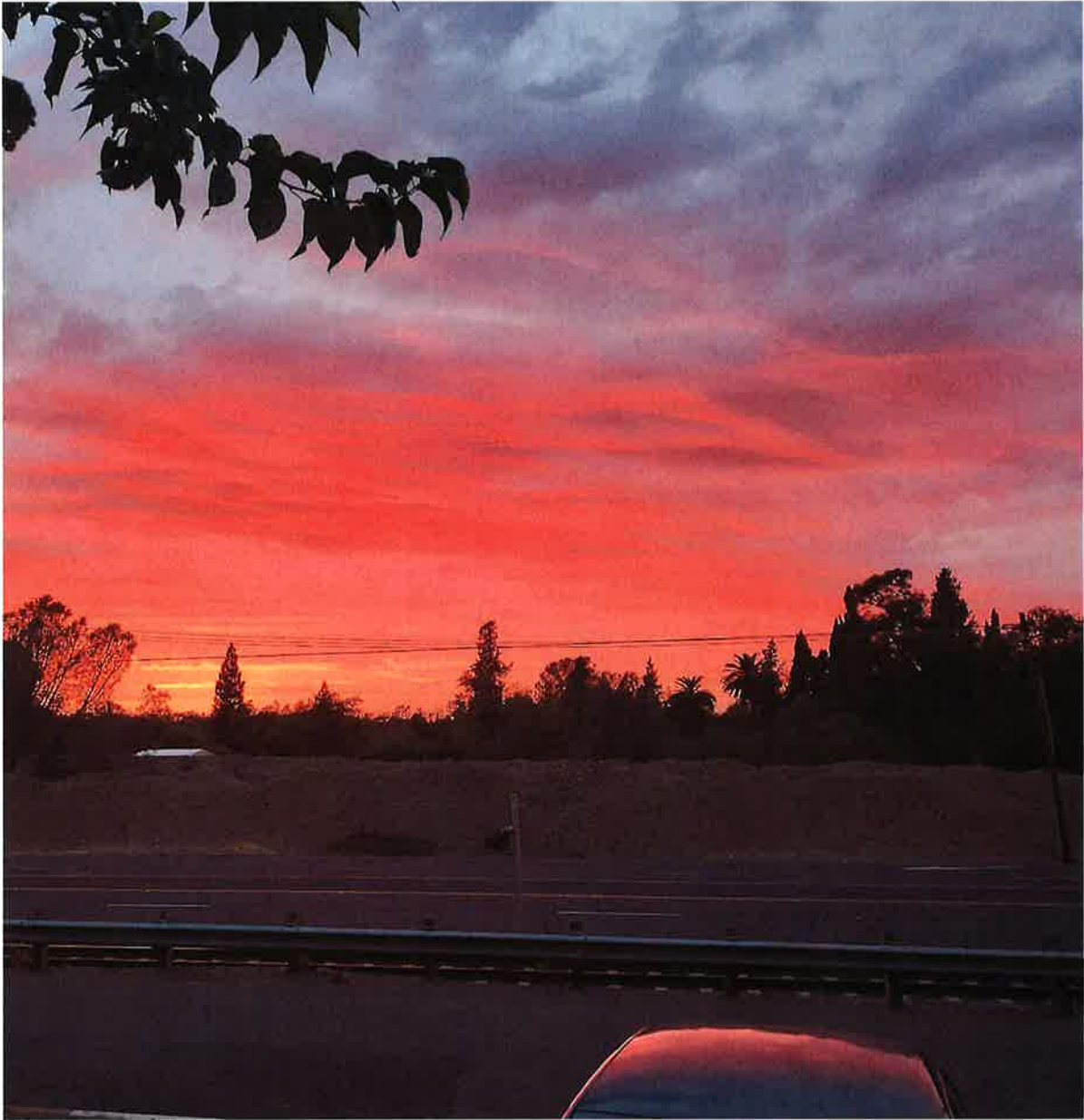
I have attached photo's that are taken from my front porch. I take pics almost weekly since the sunset is so beautiful from my front porch. This is apparently, exactly where the stack will stand. So no more front porch sitting for me, again - it's my only porch, it's my only sight line outside since our windows from our living room face that direction.

We volunteer in Folsom Days, we actively help keep an eye on the Historic Rail Yard which is our neighbor, we speak to Jim and other staff occassionally through the year. We spend as much money locally as possible.

PLEASE VOTE NO - I oppose the Crematorium location and I expect any council or official representing me to oppose it's location in this residential area.

Thank You Respectfully,

Melissa (Missy) Ingle
and wife
Betty (Renee) Reed
[REDACTED] Oakdale St, Folsom
[REDACTED]









February 3, 2022

Josh Kinkade, Associate Planner
Community Development Department
City of Folsom
50 Natoma Street
Folsom CA 95630

**RE: COMMENTS ON THE PROPOSED LAKESIDE MEMORIAL LAWN
CREMATORUIM DRAFT INITITAL STUDY/MITIGATED NEGATIVE
DECLARTION AND THE CONDITIONAL USE PERMIT**

First, I would like to state that I have lived in Folsom for 20 years and live adjacent to the Lakeside Cemetery property. Also, my family has been identified as “sensitive receptors” in the Health Risk Assessment.

I have reviewed the above-mentioned documents and I recommend **disapproval** of the draft Initial Study/Mitigated Negative Declaration based on comments below. I also recommend **disapproval** of the draft Conditional Use Permit (CUP).

Draft Initial Study/Mitigated Negative Declaration (IS/MND)

#1. Project Description, Parking and Circulation (page 2), last paragraph, it states that access would only need to accommodate a **small number of staff members** as there is limited parking onsite. However, this appears to be in conflict with statements made in the applicant’s Scope of Work. It states that ...”cremation-based cultures require on-site crematory so they may be able to exercise their funeral rights and customs.” As any cremation may attract large number of attendees, the cremations should clearly be limited to small number of staff as stated in the IS/MND.

Unless the cremations are limited to only staff attending, there could be potentially significant impacts on traffic, circulation, and parking within a small residential neighborhood with only one way in and out of the subdivision with limited amount of off-street parking (the Transportation Section XVII of the draft IS/MND also states under parking and access that only a small number of staff members will need to be accommodated).

#2. Land Use and Planning, Section XI. The proposed crematorium is totally incompatible with the existing land use zoning of Open Space. The City of Folsom’s Chapter 17.30, M-2, General Industrial District of the municipal code, lists permitted uses only allowed in M-2 districts. While crematoriums are not specially listed, yet, in M-2 districts, it does list similar uses. A crematorium should only be located in an industrial zone. This can be a significant impact on Land Use. As the proposed crematorium is located in the Historic District an analysis should have been completed to identify any impacts and conflicts with the Historic District planning guidelines.

#3. Air Quality, Section III and Hazards and Hazardous Materials Section IX. The health risk assessment and receptor modeling for potential health risks appear to only have been

conducted one time, on November 11, 2020. Air pollution patterns will vary depending on time of year and wind directions, etc. Additional air modeling should be conducted at the project site at different times of the year.

I believe additional analysis needs to be done regarding health risks to sensitive receptors from vapors emitted from dental fillings that include mercury, both from air pollution and consequential contaminated soils. There have been numerous studies identifying these health risks.

Draft Conditional Use Permit (CUP)

#1. The public noticing for this proposed CUP does not meet State Planning Law for CUP's as per Government Code Section 65905 and needs to be re-noticed. I have attached a copy of the State Training guide for CUP's. The public notice for this project is titled "Notice of Public Hearing and Notice of Intent to Adopt a Mitigated Negative Declaration." No mention of a draft CUP for review. The notice only refers to the CUP once, and as an entitlement.

#2. Further, no draft CUP was included in the public review documents listed online by the City for public comments. City staff's notification stated that the CUP will be included in a Staff Report and be available at least 5 days prior to the February 16th HDC meeting. Section 65905 states that noticing of the CUP needs to be made available at least 10 days prior to the public hearing. No Staff Report online as of February 3th.

#3. A condition of the permit should include limiting attendance of cremations to cemetery employees only as assured in the IS/MND.

It appears that there may be significant impacts from this project. While the IS/MND did not identify potentially significant impacts, the Lead Agency (City of Folsom), makes the final determination. The Lead Agency should complete a project-specific Environmental Impact Report (EIR). While the Lead Agency is relying on an existing program EIR, a project-specific EIR should instead be completed due to the uniqueness of this proposed project and its location, located in Open Space zoning in the Historic District.

Sincerely,

Nancy Fallan Oldham

Cc: Kathleen Cole, Chair, HDC
 Cc: John Lane, HDC
 Cc: Justin Raithel, HDC
 Cc: Daniel West, HDC
 Cc: Mickey Ankhelyi, HDC
 Cc: John Felts, HDC

Attachment

Governor Pete Wilson

The Planner's Training Series:
THE CONDITIONAL
USE PERMIT



Governor's Office of Planning and Research
1400 Tenth Street
Sacramento, CA 95814
(916) 445-0613

Les Grissom, *Director, Governor's Office of Planning and Research*

Antero Rivasplata, *Deputy Director, State Clearinghouse*

Robert Cervantes, *Author*

Kenneth Lee, *Planning Intern, Author*

July 1997

The Planner's Training Series

This publication is one in a series prepared by the Office of Planning and Research (OPR) on topics of general interest to planners. As with the rest of this series, its primary purpose is to provide both a reference for experienced planners and training materials for new planners, planning commissioners, and zoning board members. Citations are made to pertinent sections of the California statutes and to court decisions in order to provide the reader the opportunity to do additional research on their own. Unless otherwise noted, all statutory references are to the California Government Code.

This document and other OPR publications, along with additional information about local government planning and zoning, is available from the LUPIN (Land Use Planning Information Network) web site maintained by the California Resources Agency at <http://ceres.ca.gov/planning/>

The CONDITIONAL USE PERMIT

WHAT IS A CONDITIONAL USE PERMIT?

A CONDITIONAL USE PERMIT (CUP) allows a city or county to consider special uses which may be essential or desirable to a particular community, but which are not allowed as a matter of right within a zoning district, through a public hearing process. A conditional use permit can provide flexibility within a zoning ordinance. Another traditional purpose of the conditional use permit is to enable a municipality to control certain uses which could have detrimental effects on the community (*Neighborhood Action Group v. County of Calaveras* (1984) 156 Cal.App.3d 1176).

Consideration of a CUP is a discretionary act. A CUP application tendered by a project proponent is considered at a public hearing and, if approved, is generally subject to a number of pertinent conditions of approval. Depending on local ordinance requirements, hearings are typically held by a board of zoning, the planning commission, or a zoning administrator. The owners of property near the site are sent advance notice of the date, time, and place of the hearing.

Examples of common uses allowed with a conditional use permit can be found in any city or county zoning ordinance. For example, Santa Rosa's zoning ordinance lists uses which may be permitted within single-family residential districts with a conditional use permit. These uses include churches, public or private schools, public building or utility structures, parking lots, temporary subdivision sales offices, and community care and health care facilities. Chico's zoning ordinance lists various uses permitted with a use permit issued by either a planning director or planning commission. These uses include temporary amusement attractions, the placement of a building or structure on a lot or parcel which has been moved from another lot or parcel, public buildings and facilities, parking or access located off-site from the site being served, private recreation centers, and planned developments. Each city or county may include in their zoning ordinance a wide variety of uses which they will permit with a conditional use permit.

ENABLING LEGISLATION

The rules under which counties and general law cities may issue a conditional use permit are provided by state and case law. Charter cities are not subject to state zoning law, except in special circumstances, but

may still use its provisions (Section 65803). The following is a brief examination of the authority and rules under which local governments act in issuing use permits.

Constitutional Authority:

Local governments have the authority to enact local planning and land use regulations to protect the public health, safety, and welfare of their residents through their police power. The "police power" provides the right to adopt and enforce zoning regulations, as long as they do not conflict with state laws. The police power is the basis for charter city zoning powers. (California Constitution, Article XI, Section 7)

Statutory Authority:

California code reiterates the Constitutional police powers of cities and counties to enact zoning regulations, but has little to say about CUPs in particular.

"The legislative body of any county or city may, pursuant to this chapter, adopt ordinances that do any of the following:

"Regulate the use of buildings, structures, and land as between industry, business, residences, open space, including agriculture, recreation, enjoyment of scenic beauty, use of natural resources, and other purposes. . . ." (Section 65850(a))

"The board of zoning adjustment or zoning administrator shall hear and decide applications for conditional uses or other permits when the zoning ordinance provides therefor and establishes criteria for determining those matters . . ."

"The board of zoning adjustment or zoning administrator may also exercise any other powers granted by local ordinance and may adopt all rules and procedures

THE CONDITIONAL USE PERMIT

necessary or convenient for the conduct of the board's or administrator's business."

(Section 65901(a))

Case Law:

California case law has established a number of fundamental principles relating to conditional use permits. In addition to the basic uses permitted within a zoning district, a city or county zoning ordinance can provide other specified uses which may be permitted after consideration and resolution by an administrative agency that the proposed use is in the best interest of public convenience and necessity and will not be contrary to the public health, morals, or welfare (*Upton v. Gray* (1969) 269 Cal.App.2d 352).

Local governments must have a complete and valid general plan before they can issue conditional use permits (*Resource Defense Fund v. County of Santa Cruz* (1982) 133 Cal.App.3d 800 and *Neighborhood Action Group v. County of Calaveras* (1984) 156 Cal.App.3d 1176).

The authority to consider conditional use permits, delegated to planning commissions or other administrative bodies by elected officials, must include standards of guidance. These standards of guidance are provided to insure that the delegation of discretion to an administrative agency is not unbridled and, thus, not invalid. The doctrine of the need of an ascertainable standard to guide an administrative body applies where the legislative body of a city attempts to delegate its law-making functions (*Stoddard v. Edelman* (1970) 4 Cal.App.3d 544).

PROCEDURE

The approval of a conditional use permit is an administrative, quasi-judicial act. It is not a change of zone, but rather a project-specific change in the uses allowed on a specific property. Conditional use permits do not involve the establishment of new codes, regulations, or policies. Instead, a conditional use permit applies the provisions of the zoning ordinance and its standards to the specific set of circumstances which characterize the proposed land use. Cities and counties have the authority to establish either a board of zoning adjustment or a zoning administrator to hear and decide applications for conditional uses. Local ordinance can establish specific procedures under which a delegated board of appeals will hear and determine appeals from the decisions of the board of zoning

adjustment or the zoning administrator (Section 65903). In most jurisdictions, appeals are heard by the next highest body, eventually reaching the city council or county board of supervisors for a final decision.

Public Hearing:

Section 65905 requires a public hearing to be held on an application for a conditional use permit. At a minimum, advance public notice, an opportunity to be heard, and a fair hearing are constitutional due process rights as explained in *Horn v. County of Ventura* (1979) 24 C.3d 605.

The Government Code establishes minimum requirements for public notice for counties and general law cities. Charter cities may adopt similar provisions. The notice of a public hearing must include: the date, time, and place of the hearing; the identity of the hearing body or officer (commonly the delegated board of zoning adjustment or the zoning administrator); a description of the proposal and the conditional use permit process; and the location of the property involved (Section 65094). Notice must be mailed to all property owners within 300 feet of the proposal's site boundary at least 10 days prior to the public hearing. The names and addresses of owners are those listed on the most current county equalized assessment roll. This requirement includes the owners of property which lie outside the city limits or county line (*Scott v. Indian Wells* (1972) 6 C.3d 541). Notice must also be published once in a newspaper of general circulation at least 10 days before the hearing.

Section 65030 recognizes the importance of public participation in public hearings and expresses a clear legislative intent that local agencies insure public participation at every level of the conditional use permit process. The purposes of the public hearing is for the zoning board or zoning administrator to hear and consider the opinions of the proponent and nearby property owners prior to making their decision to either approve or deny the conditional use permit. As a quasi-judicial act, the approval of a conditional use permit requires the board or administrator to adopt written findings to support their action. Whether the proposal has been approved or denied, the decision can be appealed to a higher body, usually the board of appeals, the planning commission, or city council, in accordance with the city or county zoning ordinance. The appeals body may reverse or affirm, wholly or partly, or may modify the decision, and may make such decision as should be made, and its action is final (Section 65903).

THE CONDITIONAL USE PERMIT

Section 65903 delegates to local legislative bodies the authority to establish procedures for an appeal but not the power to restrict the right of appeal. Thus, a county zoning ordinance cannot attempt to restrict the right of appeal to solely the applicant and exclude the general public, especially adjacent property owners (*Concerned Citizens of Murphys v. Jackson* (1977) 72 Cal.App.3d 1021).

In order to encourage concurrent processing for the purpose of expediting zone changes and general plan amendments, Section 65862 provides that planning agencies may simultaneously process a consolidated application which may include a use permit, rezoning, and general plan amendment if all three applications encompass the same property.

California Environmental Quality Act:

Conditional use permits are subject to the California Environmental Quality Act (CEQA, Public Resources Code Section 21000, et seq.). Prior to the public hearing on the proposed conditional use permit, the city or county must evaluate the proposal to determine whether or not it may have any significant adverse effects on the environment. If the proposal is not exempt from environmental review, the city or county is required to prepare either a negative declaration indicating that the conditional use permit will have no significant effect, or an Environmental Impact Report (EIR) which describes the potential negative impacts of the proposal and the means to avoid or lessen those impacts. See the bibliography for a reference which can provide more information about the California Environmental Quality Act.

Permit Streamlining Act:

The Permit Streamlining Act (Section 65920, et seq.) establishes time limits within which the review and approval or denial of a conditional use permit proposal must occur. For instance, if an EIR was certified for a conditional use permit, the application must be acted upon within one hundred eighty days from the date of certification (Section 65950(a)(1)). A proposal for which a negative declaration was adopted or a CEQA exemption used must be acted upon within sixty days of that action (Section 65950(a)(2)(3)). A conditional use permit cannot be disapproved solely to comply with these deadlines.

The Permit Streamlining Act provides that failure to meet its deadlines will result in automatic approval of the conditional use permit (Section 65956(b)). However, the permit can only be deemed approved if public

notice and an opportunity to be heard had been provided either by the agency or by the applicant.

The Permit Streamlining Act does not apply to administrative appeals within a state or local agency (Section 65922). Therefore, if a permit is appealed to a higher body there is no strict time frame within which the appeal must be heard.

LIMITATIONS ON CONDITIONAL USE PERMITS

As a rule, conditional use permits do not authorize uses that the zoning ordinance does not authorize, nor uses not expressly authorized by the permit. The conditional use permit includes conditions which limit the applicant's authority to use the property. Under certain conditions, however, local governments may incorporate provisions from federal laws and broaden the range of uses permitted (*Sports Arena Properties, Inc. v. City of San Diego* (1985) 40 C.3d 808).

It is often the case that local agencies follow a general set of standards in considering a conditional use permit. These standards are generally acceptable since it is a near impossibility to devise standards to cover all possible situations in which a use permit can be issued (*Tustin Heights Association v. Board of Supervisors* (1959) 170 Cal.App.2d 619). There are several cases in which these standards have been upheld.

General Welfare Standard:

"The establishment, maintenance or conducting of the use for which a use permit is sought will not, under the particular case, be detrimental to the public welfare or injurious to property or improvements in the neighborhood" (*Hawkins v. County of Marin* (1976) 54 Cal.App.3d 586).

Nuisance Standard:

"Any use found to be objectionable or incompatible with the character of the city and its environs due to noise, dust, odors or other undesirable characteristics may be prohibited" (*Snow v. City of Garden Grove* (1961) Cal.App.2d 496).

General Plan Consistency Standard:

"Although use permits are not explicitly made subject to a general plan meeting the requirement of state law, that condition is necessarily to be implied from the hierarchical relationship of land use laws.

THE CONDITIONAL USE PERMIT

Thus, use permits are struck from the mold of the zoning law, the zoning law must comply with the adopted general plan, and the adopted general plan must conform with state law; the validity of the permit process derives from compliance with this hierarchy of planning laws (*Neighborhood Action Group v. County of Calaveras* (1984) 156 Cal.App.3d 1176).

Zoning Consistency Standard:

"To obtain a use permit, the applicant must generally show that the contemplated use is compatible with the policies in terms of the zoning ordinances, and that such use would be essential or desirable to the public convenience or welfare, and will not impair the integrity and character of the zoned district or be detrimental to the public health, safety, morals or welfare" (*O'Hagen v. Board of Zoning Adjustment* (1971) 19 Cal.App.3d 151).

In addition to the general standards discussed, there also exist other limitations on conditional use permits. Conditional use permits run with the land not the applicant (*Cohn v. County Board of Supervisors* (1955) 135 Cal.App.2d 180). That is, where conditional use permits are concerned, all related property and personal rights are freely transferable, unless expressly prohibited by law (*Anza Parking Corporation v. City of Burlingame* (1988) 195 Cal.App.3d 855). Inversely, a conditional use permit may not lawfully limit the permittee from transferring it with the land since such a condition is beyond the power of the zoning authority (*Anza*, supra).

The conditions which are imposed on a conditional use permit must be expressly attached to the permit and cannot be implied. For example, if a conditional use permit contains language that restricts a building's height to five stories and requires the developer to submit and obtain planning-commission approval of a landscaping plan, among other things, the permit itself does not imply a height limitation on trees within the development (*Pacifica Homeowners' Association v. Wesley Palms Retirement Community* (1986) 178 Cal.App.3d 1147).

**OTHER TYPES OF
CONDITIONAL USE PERMITS**

State law also allows conditional use permits for "granny" units, second dwelling units, and mobile-home parks. If a local zoning ordinance does not

provide for these cases, the ability to apply for conditional use permits allowing these uses is provided for by state law. In all cases, public notice and hearing must be provided as discussed earlier.

"Granny" Units (Section 65852.1) -

"... any city, including a charter city, county, or city and county may issue a zoning variance, special use permit, or conditional use permit for a dwelling unit to be constructed, or which is attached to or detached from, a primary residence on a parcel zoned for a single-family residence, if the dwelling unit is intended for the sole occupancy of one adult or two adult persons who are 62-years of age or over, and the area of floor space of the attached dwelling unit does not exceed 30 percent of the existing living area or the area of the floor space of the detached dwelling unit does not exceed 1,200 square feet."

Prior to approval of a conditional use permit under Section 65852.1, the city or county must find that the resident or residents meet the age criteria, and that the floor area of the proposed unit does not exceed that allowed by the statute. In accordance with the special circumstances provided in Section 65803, Section 65852.1 applies to charter cities, as well as general law cities.

Second Dwelling Units (Section 65852.2) -

"Notwithstanding Section 65901, every local agency shall grant a special use or a conditional use permit for the creation of a second unit if the second unit complies with all of the following:

"(A) The unit is not intended for sale and may be rented.

"(B) The lot is zoned for single-family or multi-family use.

"(C) The lot contains an existing single-family dwelling.

"(D) The second unit is either attached to the existing dwelling and located within the living area of the existing dwelling or detached from the existing dwelling and located on the same lot as the existing dwelling.

"(E) The increased floor area of an attached second unit shall not exceed 30 percent of the existing living area.

"(F) The total area of floor space for a detached second unit shall not exceed 1,200 square feet.

"(G) Requirements relating to height, setback, lot coverage, architectural review, site plan review, fees, charges, and other zoning requirements generally ap-

THE CONDITIONAL USE PERMIT

plicable to residential construction in the zone in which the property is located.

"(H) Local building code requirements which apply to detached dwellings, as appropriate.

"(I) Approval by the local health officer where a private sewage disposal system is being used, if required."

Section 65852.2 also provides that any local agency may, by ordinance, allow second units in single-family and multifamily residential zones. Thus, a Section 65852.2 conditional use permit is only required for second units when a local agency has not adopted an ordinance governing second units.

Mobilehome Parks (Section 65852.7) -

"A mobilehome park, as defined in Section 18214 of the Health and Safety Code, shall be deemed a permitted land use on all land planned and zoned for residential land use as designated by the applicable general plan; provided, however, that a city, county, or a city and county may require a use permit."

If a local government denies the renewal of a conditional use permit allowing a mobilehome park, the government must take specified required steps to mitigate the adverse impacts of the mobilehome park closure, pursuant to Section 65863.7.

FINDINGS

Written "findings of fact" are required in order to support the decision of the hearing body to approve or deny a conditional use permit (*Topanga Association for a Scenic Community v. County of Los Angeles* (1974) 11 C.3d 506). Findings are the legal footprints left by local decision-makers to show how the decision-making process progressed from the initial facts to the decision.

Findings are important. They "bridge the analytical gap between the raw evidence and ultimate decision" (*Topanga, supra*). If the decision is challenged, a court will examine the evidence supporting the findings to determine whether the hearing body abused its discretion when acting on a conditional use permit. Such an abuse of discretion is to be found when: (1) the agency did not proceed in a manner prescribed by law; (2) the agency's decision is not supported by findings; and (3) the agency's findings are not supported by evidence in the administrative record.

Topanga cites several purposes for making findings, among which include: (1) providing a framework

for making principled decisions, thus enhancing the integrity of the administrative process; (2) helping make analysis orderly and reducing the likelihood that the agency will randomly leap from evidence to conclusion; and (3) serving a public relations function by helping to persuade the parties that administrative decision-making is careful, reasoned, and equitable. Findings should also justify any conditions which impose fees or other exactions.

For a detailed discussion of findings requirements, see OPR's publication entitled *Bridging the Gap*.

CONDITIONS OF APPROVAL

Section 65901 empowers local decision-making bodies to take action on use permit proposals when zoning ordinances make provisions and set criteria for them. The hearing body may also modify a conditional use permit's terms by imposing new or revised conditions, if the ordinance, interim ordinance, or original conditional use permit so provides (*Garavatti v. Fairfax Planning Commission* (1971) 22 Cal.App.3d 145).

Just as there are limitations in approving a conditional use permit, there are also limitations in establishing conditions of approval. Four general rules of thumb in applying conditions of approval include: (1) the jurisdiction must be acting within its police powers; (2) the condition must substantially further a legitimate public purpose; (3) the condition must further the same public purpose for which it was imposed; and (4) the property owner may not be required to carry a disproportionate load in furthering the public purpose (*California Land-Use and Planning Law*, 9th edition).

Section 65909 provides that dedications of land, as conditions of approval, must be "reasonably related" to the use of the property for which the conditional use permit is requested. There must also be a "rough proportionality" between the extent of the condition and the particular demand or impact of the project (*Dolan v. City of Tigard* (1994) 129 L.Ed2nd 304). In addition, a performance bond cannot be required for the installation of public improvements that are not reasonably related to the property use. Limitations on impact fees are described in the Mitigation Fee Act (Section 66000, et seq.).

If a condition applied to a conditional use permit is not linked to some legitimate public need or burden the project creates, the condition imposed could be deemed a taking of property in violation of the U.S. Constitution's Fifth and Fourteenth Amendments

THE CONDITIONAL USE PERMIT

**CONDITIONAL USE
PERMIT CHECKLIST**

If a conditional use permit is to be approved, all of the following questions must be answered affirmatively.

1. Is the public hearing notice complete in its description of the project?
 Yes No
2. Has the public hearing notice been issued in accordance with all procedures?
 Yes No
3. Is the proposed use, with proposed conditions of approval, suitable for the site?
 Yes No
4. If any conditions of approval call for dedications of land, are they reasonably related to the use and its impacts?
 Yes No
5. If significant environmental effects have been identified as a result of the proposed conditional use permit, have conditions been required, or has the project been redesigned, to mitigate those effects?
 Yes No
6. Have findings been adopted to support the agency's decision, based upon substantial evidence in the record?
 Yes No
7. Are the required environmental findings being adopted?
 Yes No

(*Nollan v. California Coastal Commission* (1987) 97 L.Ed2nd 677). Where a regulatory taking has been found to occur, the courts will overturn the agency's action and may require the agency to pay the applicant compensation for the taking (*Dolan, supra*).

EXAMPLES

The following court cases illustrate when it may be proper to grant a conditional use permit and when it may not be. These cases are illustrations only and should not be used as the sole basis for granting or denying a conditional use permit.

**Cases Upholding Conditional Use
Permit Approvals****General Welfare Standard**

The general welfare standard is sufficient in granting a conditional use permit. The issuance of a conditional use permit for a low-cost rental housing for the elderly in a residential area was upheld on grounds that the proposed use would not be "detrimental to the public welfare or injurious to property or improvements in the neighborhood" (*Hawkins v. County of Marin* (1976) 54 Cal.App.3d 586).

General Plan Consistency

The absence of a valid general plan does not preclude all development activity. Section 65361 establishes the general plan extension procedure whereby local governments can proceed with development pending completion of a valid general plan. This procedure also applied to the case where a county approved minor land subdivisions without a required general plan (*Resources Defense Fund v. County of Santa Cruz* (1982) 133 Cal.App.3d 800).

Procedure/Public Notice

A conditional use permit cannot be revoked without sufficient cause. Further, prior to revocation, notice and hearing must be provided for. Thus, in the case where an applicant was given notice that the hearing would concern the expiration of the conditional use permit rather than the revocation of the permit, attempt to revoke the permit was nullified (*Community Development Commission of Mendocino County v. City of Fort Bragg* (1988) 204 Cal.App.3d 1124).

**Cases Overturning Conditional Use
Permit Approvals****General Welfare Standard**

A county zoning ordinance requiring a church in a residential zone to obtain a conditional use permit prior to allowing it to use the land was found not to abridge the constitutional right of freedom of religious wor-

THE CONDITIONAL USE PERMIT

ship. The court held that a county zoning ordinance which provides a use permit to be granted if the use will not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons or property in the neighborhood, or to the general welfare of the county, is not unconstitutional by reason of vagueness or uncertainty (*Mathews v. Board of Supervisors of Stanislaus County* (1962) 203 Cal.App.2d 800).

Nuisance Standard

The approval of a conditional use permit for the storage of houses was overturned on grounds that any use may be prohibited if found to be objectionable or

incompatible with the character of the city and its environs due to noise, dust, odors or other undesirable characteristics (*Snow v. City of Garden Grove* (1961) 188 Cal.App.2d 496).

General Plan Consistency

The issuance of a conditional use permit to a construction company for production of sand and gravel was overturned on grounds that that the general plan elements which bear on the permit are inadequate and the permit is inconsistent with pertinent provisions of an adequate general plan (*Neighborhood Action Group v. County of Calaveras* (1984) 156 Cal.App.3d 1176).

THE CONDITIONAL USE PERMIT

BIBLIOGRAPHY

For more information about conditional use permits, we recommend the following references.

Bridging the Gap: Using Findings in Local Land Use Decisions, by Robert Cervantes, second edition (Governor's Office of Planning and Research), 1989. This booklet explains the principles of findings in detail.

California Permit Handbook, (California Office of Permit Assistance), 1996-97. This handbook is a guide to the State environmental permit process and provides guidance for complying with the State's environmental quality and permit streamlining statutes, regulations, and policies.

California Zoning Practice, by Donald Hagman, et al., April 1996 Supplement by John K. Chapin (Continuing Education of the Bar, Berkeley, CA), 1969. This text reviews state zoning law in detail.

CEQA Deskbook: A Step-by-Step Guide on How to Comply with the California Environmental Quality Act, by Ronald E. Bass, et al., 1996 edition (Solano Press, Point Arena, CA). A guide to understanding the environmental review process and identifying key steps, requirements, and decision points necessary to comply with CEQA.

Curtin's California Land Use and Planning Law, by Daniel J. Curtin Jr., 1997 edition (Solano Press, Point Arena, CA), revised annually. A look at the planning, zoning, subdivision, and environmental quality laws, including conditional use permits, as interpreted by numerous court cases.

Longtin's California Land Use, 2nd edition, by James Longtin, 1996 Supplement (Local Government Publications, Malibu, CA), 1988. This reference text on planning and land use law contains an excellent discussion of the conditional use permit process and legal considerations.

Josh Kinkade

From: Daron Bracht <daronbr@pacbell.net>
Sent: Tuesday, March 2, 2021 11:12 AM
To: Josh Kinkade; Steven Banks
Subject: Fwd: Proposed Lakeside Memorial Lawn Crematory

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----- Forwarded Message -----

Subject: Proposed Lakeside Memorial Lawn Crematory
Date: Tue, 22 Dec 2020 14:34:02 -0800
From: N Oldham [REDACTED]
To: Daron Bracht <Daronbr@pacbell.net>

Hi Daron, I do not think that the proposed Lakeside Memorial Lawn Crematory project and a conditional use permit should be approved. The proposed crematory I believe is considered an industrial use and should be located only in an industrial zoned area.

As you know the Lakeside Memorial Lawn Cemetery is zoned open space/public primary area.

Further, I believe Folsom's Open Space Element of the General Plan counted on the Cemetery land in its open space requirement. The crematory is not a compatible use in open space zoning.

Folsom's General Industrial District (Chapter 17.30 M-2) states that specific uses shall only be permitted in M-2 districts. While it doesn't reference crematories it does list pottery kilns and ceramic works.

I have other concerns and comments on the proposed project including health related concerns with vapors emitted from dental mercury fillings. There are numerous studies regarding this health and environmental issue.

Can you please forward my email to the other commissioners? I know that there may be some new commissioners appointed soon. I will include my above comments and others when the draft Initial Study is made available at a Historic District Commission meeting.

Thank You.
Nancy Fallan Oldham
916 [REDACTED]

Josh Kinkade

From: Nicole Gates [REDACTED]
Sent: Tuesday, January 11, 2022 10:53 AM
To: danwestmit@yahoo.com; daronbr@pacbell.net; kcolepolicy@gmail.com; kevin.duewel@gmail.com; m.dascallos@yahoo.com; johnfelts@e55tech.com; Mike Kozlowski; Sarah Aquino; YK Chalamcherla; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez; Elaine Andersen; Josh Kinkade
Subject: Proposed Lakeside Crematorium

Some people who received this message don't often get email from niki.gates1348@gmail.com. [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

My husband, two young kids, and I bought a home in the Lake Natoma Shores neighborhood a year ago. We moved here to be closer to family, and have a better quality of life.

The neighborhood has proven to be magical. My kids have freedom, ride their bikes with the other neighborhood kids, and meet at the park to play. The neighbors gather for movie nights with an outdoor projector, piñatas in the alley, poker nights, the neighborhood band playing for the community, chili cook offs, kayaking on the lake, and holiday decorating contests.

My parents live down the street, and my sister and her family live a couple houses down from them. A new baby is expected in April.

We found out about the proposed crematorium a couple of months after we moved in. I wasn't worried at first. I never thought it would be allowed in an established historic neighborhood.

We moved to Folsom because of family and that it is "Distinctive by Nature". Please keep the Historic District special. Please protect Open Space. Please save our magical neighborhood. The crematorium should be put in an industrial area. It does not belong where kids play. If the crematorium gets put in it will destroy an entire neighborhoods' quality of life just so The Caring Services Group/Miller Funeral Home can make a profit. I don't want this to happen to my family and my neighbors (please note the smoke and odor quotes):

<https://cronkitenews.azpbs.org/2021/03/25/neighbors-hope-for-relief-from-crematorium-smoke-as-covid-19-deaths-decrease/>

Thank you,

Nicole Gates
[REDACTED] Young Wo Circle

From: [REDACTED]
To: [Josh Kinkade](#)
Subject: Lakeside Memorial Crematorium
Date: Saturday, July 31, 2021 4:11:40 PM

You don't often get email from niki.gates1348@gmail.com. [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

I am opposed to having a crematorium in my neighborhood. I have read the "Initial Study/Mitigated Negative Declaration." I am voicing my concerns about:

"The Project Objectives"

On page 7 it states; The project objectives **as expressed by the applicant**, are to:

1.) Provide cremation services for those who currently live in and around Folsom, as no such services currently exist for the city.

There are 18 crematories within an 18 mile radius of Miller Funeral Home (google maps). 12 are in the City of Sacramento proper, 1 in Rocklin, 1 in Placerville, 1 in Fair Oaks, and 2 in Carmichael. I received this information from the Consumer Affairs office in Sacramento. Mr. Semenyuk is right, there isn't a cremation service directly in Folsom, however there are other facilities close by. Does Folsom really need to have a crematory?

If it is deemed a necessary service for Folsom, the crematorium needs to be in a zoned industrial/commercial area distant from residential neighborhoods.

2.) Provide cremation services for members of the population whose customs or religions require such practices.

While this may be true, it is my opinion the number of Folsom residents requiring this service is low. Customs and religious needs can be met at Mount Vernon Memorial Park and Mortuary in Fair Oaks- 11 minutes or 5.2 miles away from Miller Funeral Home.

3.) Prepare for an increase in the demand for cremation services as cremation becomes more popular in California and as the Folsom population grows.

Mr. Semenyuk is right. Plan, prepare, and place the industrial incinerator in an appropriate location. It belongs in a zoned commercial/industrial area. There is plenty of time to research a far better location than in the Preserves Neighborhood. I see no need to rush to a decision. Lakeside Memorial Lawn is the wrong place for an industrial incinerator.

4.) Upgrade the existing facilities to capitalize on a business opportunity that has proven successful for the applicant elsewhere in California.

Mr. Semenyuk states he wants to "upgrade existing facilities". Mr. Semenyuk stated he was

going to "use an existing metal shed" to install the HCT-250 industrial incinerator and 10X15 foot walk-in chiller. This is an old shed. It is my estimation that the shed in question is at least 25 years old. It doesn't make sense to me to place machinery, maybe worth 100K, in an old metal shed. They recently built a 25x25 foot metal shed next to the old shed. It is a matter of time before Mr. Semenyuk requests to remove the old shed, build another bigger and better structure, and move the equipment to the new metal shed with possibly another incinerator.

5.) "Capitalize on a business opportunity"

That says it all. It's all about the money.

Zero regard to the residents of the Preserve Neighborhood, to their health, safety, quality of life, and the ability to thrive.

Zero regard to the Historic Value of the area.

According to "Folsom Historic Commission District Staff Report" dated 01-15-2003:

"Lakeside Memorial Lawn is actually a conglomeration of Citizen, Jewish, Masonic, Negro Bar, Cook's/American Legion, and Odd Fellows Cemeteries that date back to the 1850's. Adjacent to this site is the China Mission-Chung Wah Cemetery, a national registered historic site. These cemetery sites along with the neighboring California State Dredger Tailing Preserve, create a combination, in itself, is a extremely unique resource, according to the Commissioners of the Sacramento County Historical Cemetery Commission."

The Caring Service Group on their website states;

"Caring Service Group was established in 2010 with the primary goal of purchasing Funeral Home Business".

It's just about business, money, and expansion. It's not about local cremations, there are 18 crematories within 18 miles. It's not about providing a service for religious or cultural customs, there is a facility 5.2 miles away. It's not about growing populations.

It's to, **"CAPITALIZE ON A BUSINESS OPPORTUNITY"** with zero regard toward the community.

No to the industrial incinerator.

Thank you for your time,

Nicole Gates

Josh Kinkade

From: Kelly Mullett
Sent: Tuesday, July 20, 2021 9:27 AM
To: Josh Kinkade
Subject: FW: Proposed Lakeside Memorial Lawn Crematorium

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



CITY OF FOLSOM
DISTINCTIVE BY NATURE



www.folsom.ca.us

From: nicole higgins [REDACTED]
Sent: Tuesday, July 20, 2021 9:23 AM
To: danwestmit@yahoo.com; daronbr@pacbell.net; ankhelyi@comcast.net; kcolepolicy@gmail.com; kevin.duewel@gmail.com; m.dascallos@yahoo.com; johnfelts@e55tech.com; Mike Kozlowski <mkozlowski@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Elaine Andersen <eandersen@folsom.ca.us>; jkincaid@folsom.ca.us; Kelly Mullett <kmullett@folsom.ca.us>; thehfra@gmail.com
Subject: Proposed Lakeside Memorial Lawn Crematorium

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To whom it may concern,

These are letters from my children regarding their concerns for the proposed crematorium:

Thank you,

Evelyn Gates (7yrs) and
Jackson Gates (9yrs)

Sent from my iPhone



I do not want to ride
my bike when there is
bad air



Jackson Gates
young wo circle
Folsom

age 9

January 11, 2022

Josh Kinkade, Associate Planner

City of Folsom
Community Development Department

**RE: Lakeside Memorial Lawn Crematorium CUP and IS/MND
Planning No. P-19-182**

Words matter....and calling a crematorium 'safe' is misleading. Peter Hartwick of Peter Hartwick Combustion Technologies claims their crematoriums use less fuel, lower Nitrogen Oxide (NOx) emissions, and are compliant with NFPA and air quality standards. "Low NOx emissions"? **Any** NOx molecules during exposure to UV rays in sunlight and heat, interact with volatile organic compounds and form a serious ground level pollutant....and in the presence of rain...acid rain. There is strong evidence that NOx respiratory exposure can trigger and exacerbate existing asthma symptoms, and can even lead to the development of asthma over a longer period of time. These toxins can irritate the lungs of healthy people! And what about those with existing medical conditions such as asthma or heart disease?

In a study by the EPA, from only one crematory in Southern California, they found Carbon monoxide, nitrogen oxides, Particulate Matter (PM10), (PM2.5), mercury and other heavy metals like cadmium, lead, and nickel. Known carcinogens like dioxins and furans and even hydrogen chloride. Mercury is colorless and odorless, and there is no agreement about the safe level of it. Some say no level is safe. Mercury can be present in dental fillings (mostly from baby-boomer days) and medical devices of bodies that are cremated and is most dangerous when heated. Crematoriums are required to remove these things beforehand, but the materials become biohazard waste and so most won't bother. Who would know?

Without information of how often a crematory is working, how does one say it "meets air quality standards". Igor Semenyuk, Chief Operations Officer of Miller Funeral Home (which also owns and operates Lakeside Memorial Lawn) says they may be cremating 2-3 per day, and then claims 7 a week. Which is it? I noticed in his addendum to the proposal he has already increased the daily rate from 400 lbs to 800 lbs a day of human remains. Does that mean more per day? As the demand for cremations increase, so will the need for more working days. Once in operation, whose to know? Who regulates that? NO one!

Because of a successful lobbying effort to declassify crematoriums as solid waste incinerators, the EPA does not regulate solid-waste incinerators category pertaining to the funeral industry. States that regulate air quality standards don't require testing for specific toxins that are released during cremation. I understand that the California Air Resources does an annual inspection, but what and how do they test? What about the 364 days in between?

Additionally, besides air health, propane will be used at this proposed site in Folsom, meaning more truck traffic through our neighborhoods and although NFPA compliant, crematories aren't required to report workplace accidents to OSHA. So a mishap occurs and nothing has to be reported, unless of course it is catastrophic...then we would all know! *THIS IS AN OPEN SPACE AND RESIDENTIAL AREA...WHAT A CATASTROPHE IT WOULD BE!*

The Preserve Neighborhood has about 111 households sharing the neighborhood with the Lakeside Memorial Lawn Cemetery. Can anyone really say there are SAFE levels of toxic chemicals?

Crematories are definitely a necessity but why would they be allowed in an environmentally sensitive open space area with vegetation, historical structures and next to a fresh body of water (Lake Natoma). A designated industrial business district away from a fully occupied community of families, schools, and animals could be considered.

Please don't approve profits over human lives! I ask that you take these health issues seriously to preserve the Preserve Neighborhood and historic Folsom.

The crematorium in our neighborhood will affect all of us in the Preserve Neighborhood, and it should be treated as a **HAZARDOUS PROPOSITION** to this urban historical area of Folsom.

Pam Ceccarelli,
[REDACTED] Fong St.
Folsom, CA
[REDACTED]

January 17, 2022

Folsom City Officials

**RE: Lakeside Memorial Lawn Crematorium CUP and IS/MND
Planning No. P-19-182**

Words matter....and calling a crematorium 'safe' is misleading. Peter Hartwick of Peter Hartwick Combustion Technologies claims their crematoriums use less fuel, lower Nitrogen Oxide (NOx) emissions, and are compliant with NFPA and air quality standards. "Low NOx emissions"? **Any** NOx molecules during exposure to UV rays in sunlight and heat, interact with volatile organic compounds and form a serious ground level pollutant....and in the presence of rain...acid rain. There is strong evidence that NOx respiratory exposure can trigger and exacerbate existing asthma symptoms, and can even lead to the development of asthma over a longer period of time. These toxins can irritate the lungs of healthy people! And what about those with existing medical conditions such as asthma or heart disease?

In a study by the EPA, from only one crematory in Southern California, they found Carbon monoxide, nitrogen oxides, Particulate Matter (PM10), (PM2.5), mercury and other heavy metals like cadmium, lead, and nickel. Known carcinogens like dioxins and furans and even hydrogen chloride. Mercury is colorless and odorless, and there is no agreement about the safe level of it. Some say no level is safe. Mercury can be present in dental fillings (mostly from baby-boomer days) and medical devices of bodies that are cremated and is most dangerous when heated. Crematoriums are required to remove these things beforehand, but the materials become biohazard waste and so most won't bother. Who would know?

Without information of how often a crematory is working, how does one say it "meets air quality standards". Igor Semenyuk, Chief Operations Officer of Miller Funeral Home (which also owns and operates Lakeside Memorial Lawn) says they may be cremating 2-3 per day, and then claims 7 a week. Which is it? I noticed in his addendum to the proposal he has already increased the daily rate from 400 lbs to 800 lbs a day of human remains. Does that mean more per day? As the demand for cremations increase, so will the need for more working days. Once in operation, whose to know? Who regulates that? NO one!

Because of a successful lobbying effort to declassify crematoriums as solid waste incinerators, the EPA does not regulate solid-waste incinerators category pertaining to the funeral industry. States that regulate air quality standards don't require testing for specific toxins that are released during cremation. I understand that the California Air Resources does an annual inspection, but what and how do they test? What about the 364 days in between?

Additionally, besides air health, propane will be used at this proposed site in Folsom, meaning more truck traffic through our neighborhoods and although NFPA compliant, crematories aren't required to report workplace accidents to OSHA. So a mishap occurs and nothing has to be reported, unless of course it is catastrophic...then we would all know! *THIS IS AN OPEN SPACE AND RESIDENTIAL AREA...WHAT A CATASTROPHE IT WOULD BE!*

The Preserve Neighborhood has about 111 households sharing the neighborhood with the Lakeside Memorial Lawn Cemetery. Can anyone really say there are SAFE levels of toxic chemicals?

Crematories are definitely a necessity but why would they be allowed in an environmentally sensitive open space area with vegetation, historical structures and next to a fresh body of water (Lake Natoma). A designated industrial business district away from a fully occupied community of families, schools, and animals could be considered.

Please don't approve profits over human lives! I ask that you take these health issues seriously to preserve the Preserve Neighborhood, wildlife, and historic Folsom.

The crematorium in our neighborhood will affect all of us in the Preserve Neighborhood, and it should be treated as a **HAZARDOUS PROPOSITION** to this urban historical area of Folsom.

Pam Ceccarelli,
[REDACTED] Fong St.
Folsom, CA
[REDACTED]

July 7, 2021

Kelly Mullett,

Words matter....and calling a crematorium 'safe' is misleading. Peter Hartwick of Peter Hartwick Combustion Technologies claims their crematoriums use less fuel, lower Nitrogen Oxide (NOx) emissions, and are compliant with NFPA and air quality standards. "Low NOx emissions"? Any NOx molecules during exposure to UV rays in sunlight and heat, interact with volatile organic compounds and form a serious ground level pollutant....and in the presence of rain...acid rain. These toxins can enter the lungs, and even short term exposure can irritate the lungs of healthy people! And what about those with medical conditions such as asthma or heart disease?

Because of a successful lobbying effort to declassify crematoriums as solid waste incinerators, the EPA does not regulate solid-waste incinerators category pertaining to the funeral industry. States that regulate air quality standards don't require testing for specific toxins that are released during cremation. If there are inspections by the California Air Resources, how often is it performed and how is it tested?

In a study by the EPA, from only one crematorium in Southern California, they found Carbon monoxide, nitrogen oxides, Particulate Matter (PM10), (PM2.5), mercury and other heavy metals like cadmium, lead, and nickel. Known carcinogens like dioxins and furans and even hydrogen chloride. Mercury is colorless and odorless, and there is no agreement about the safe level of it. Some say no level is safe. Mercury can be present in dental fillings (mostly from baby-boomer days) and medical devices of bodies that are cremated and is most dangerous when heated. Crematoriums should remove these things beforehand, but the materials become biohazard waste and so most won't bother. Who would know?

Without information of how often a crematorium is working, how does one say it "meets air quality standards". Igor Semenyuk, Chief Operations Officer of Miller Funeral Home (which also owns and operates Lakeside Memorial Lawn) says they may be cremating 2-3 per day, and then says 7 a week. Which is it? As the demand for cremations increase, so will the need for more working days. Once in operation, whose to know? Who regulates that? NO one!

Additionally, besides air health, propane will be used at this proposed site in Folsom, meaning more truck traffic through our neighborhoods and although NFPA compliant, crematoriums aren't required to report workplace accidents to OSHA. So a mishap occurs and nothing has to be reported, unless of course it is catastrophic...then we would all know!

I don't have a problem with crematoriums, but like an airport, it should be in a designated business district away from a fully occupied community of families, schools, and animals. The Preserve Neighborhood has about 111 households sharing the

neighborhood with the Lakeside Memorial Lawn Cemetery. We feel there is no such thing as a safe level of a toxic chemical.

The crematorium in our neighborhood will affect all of us in the Preserve Neighborhood, and I think it should be treated as a **hazardous proposition** to this urban area of Folsom.

Please don't approve profits over human lives! I ask that you take these health issues seriously to preserve the Preserve Neighborhood.

Pam Ceccarelli,

██████████ Fong St.

Folsom, CA

916 ██████████

████████████████████

February 1, ,2022

To: The Historic District Commissioners
 Chairman Kathleen Cole, Vice Chairman Mickey Ankhelyi, Commissioners, Mark Dascallos, John Felts,
 John Lane, Justin Raithel and Daniel West
 Josh Kinkade
 Pam Johns
 Elaine Andersen
 Folsom City Council Members:
 Mayor Kerri Howell, Vice Mayor Rosario Rodriguez, Council Members, Sarah Aquino, YK Chalamcherla,
 and Mike Kozlowski

I am writing in response to public comments on the application for a Crematorium to be installed at the Lakeside Memorial Lawn Cemetery in Historic Folsom. While the Helix report focuses on the Environmental scope, our neighbors hope you will see the human impacts and aspects of this proposal. In his scope of Work- Installing a Crematory document, Mr. Semenyuk, the applicant, stated that Caring Services Group has eight Mortuaries and four Crematoriums. All four crematoriums are located inside the funeral homes per Mr. Semenyuk. Yet in historic Folsom, he is asking to operate a crematorium in a metal shed fueled by above ground propane tanks. I am surprised that The City of Folsom does not have standards that would prohibit such an unrefined concept. I also find it distasteful that remains would be stored in a cooler in the shed.

Aesthetics: Both sheds are visible from Natoma Street and Forrest Street even without the roof exhaust stack installed. People visiting their deceased loved ones situated behind the shed also can see the crematorium. This is not peaceful nor esthetically pleasant..I feel the initial study and mitigated negative declaration minimizes and/or fails to recognize the negative effects on very many people. The Lakeside Memorial Lawn Cemetery is designated as a historic cemetery. It is old, well landscaped and is a peaceful and interesting place to visit. The Chung Wah Chinese Historical Cemetery, very close by, is listed on the National Historic Register. The California State Dredger Tailings Preserve, located next to the sheds, is in itself historic and unique. These sites should not be altered by installing a crematorium. The current open space zoning is appropriate for this site and I ask you to not change it in order to operate a crematorium. Our city has a lovely historic district, I ask you to protect it's historic features as well as protecting the people living in the surrounding area.

Parking impact. Project objectives: Provide cremation services for members of the population whose customs or religion require such practices. As I understand it, the cemetery has about 50 parking spaces. If there are large groups witnessing cremations, overflow parking on Forrest Street would be a problem due to the width of the street. Two lanes down to one narrow lane. I spoke to an employee at a nearby mortuary and crematorium and asked about how many people witness cremations? His answer was anywhere from five to two hundred at a time. If his estimate is true, the current parking spaces are not adequate. Also, traffic flow is problematic at this location with light rail blocking the intersection frequently. No other exit is available. The additional parking will affect the quality of life of the residents who all use Forrest St. to access the intersection on Folsom Blvd. (intensity impact)

Fire impact: This devastating impact has been minimized. The City of Folsom Community Wildfire Protection Plan clearly outlines the fire danger in this area. The city also declared a Fire danger emergency last year. It concerns me that this much propane will be in constant use at high

-2-

temperatures so close to a populated area and major roadway. This area would not be easy to evacuate in an emergency. I would encourage you to read the wildfire report if you are not familiar with it. Why would the city agree to place 500 pounds of propane that would be used at high temperatures, five days a week, for 90 minutes to 2 hours for each cremation in a complicated environment that affects so many people? I noticed in the section on geology and soils, the mention of seismic activity. C). Be located on a geologic unit or soil that is unstable or that would become unstable as a result of the project, and potentially result in on-or off site landslide, lateral spreading, subsidence, liquefaction or collapse. Another reason above ground propane is very concerning at this location.

There needs to be an emergency evacuation plan in existence for the neighborhood. An illegal fire at the Lake would prove difficult to access for fire engines and a fire could spread quickly endangering homes and the propane tanks. While an argument might be made that there are many large propane tanks in use in the city, I would say, they are not burning at high temperatures at a constant rate. The crematory would use 9,835.9 gallons of propane a year.

Air Quality is very concerning for people like myself with respiratory problems. Spare the air days affect my asthma and I need to stay indoors. The proposed crematorium location close to residences and recreation can certainly effect our quality of life. The report refers to The Preserve/ Lake Natoma Shores neighborhood as small, however there are 118 homes here. It also lists the distance to all the nearby schools but does not seem to be concerned with the exposure of the children who live here. I wish I were more versed on CEQA, but I feel that people more qualified than myself can address those concerns as well as cultural resources and air quality.

I have some observations regarding the Initial Study and Mitigated Negative Declaration dated Jan. 2022.

Future growth of the city of Folsom: Project objectives: prepare for an increase in the demand for cremation services as cremations become more popular in California and as Folsom's population grows. This concerns me. The report examines what the needs would be at 500 cremations per year. What's to say the need doubles as do the impacts. Because of the problems noted in my letter regarding the impacts on the neighborhood, I ask you to look ahead and consider this as you make your decision.

Page 6: I am concerned about the drainage into Lake Natoma. I assume the machinery is routinely cleaned however, I don't see any mention of maintenance procedures. Since no sewer line is being planned, what happens to residue, dust etc?

On the Section of the History of Lakeside Memorial Lawn, it states not much information in academic or grey literature exists on the history of the Lakeside Lawn Memorial. In 2002-2003, Mr. Clane of Millers Mortuary applied for a conditional use permit to operate a Crematorium at the Lakeside Cemetery. Sue Silver wrote a comprehensive research paper describing the historic graves and history of the cemetery which is included in the, then city's staff report. Her report is very interesting to read and I hope you will have a chance to read it. Not long afterward, the cemetery was designated historic. The prior application was denied before the designation.

-3-

Chinese Influence on Folsom. I would have liked to see more emphasis on the Chung Wah Historic Cemetery that is located so close to Lakeside Memorial Lawn. It is on the National Historic Register, California State Registered Landmark and the City of Folsom's resource inventory. A place of this significance deserves more than a mention.

Section: 7.0. Environmental Factor's Potentially Affected. Items not checked: Aesthetics, Biological, Recreation and Wildfire. I feel they are affected.

Energy: Under lighting: I did not see the large light installed on the adjacent shed mentioned as a new source of light.

Conflicting information: evaluation of transportation: (b). No impact: the proposed action would not be a destination accessible for members of the public and would only be visited by a SMALL NUMBER of WORKERS during construction and operation. The project would not result in significant increase in vehicle miles traveled, and is located within one half mile of a major public transit stop which provides both bus and light rail service. No impact will occur.

Mr. Semenyuk: There are several large cultural communities residing and moving to Folsom. Regretfully, we do not have the ability nor the capability to serve the Sikh, Hindu, Buddhist or other cremation based cultures as they require an on site crematory so that they can exercise their funeral rights and customs. (witness cremation)

Thank you for your consideration.

Sincerely,



Pat Binley
[REDACTED] Sutter St.
Folsom, Ca. 95630

sent 8-7-2021

Dear Historic District Chairman Bracht and Commissioners, West, Ankhelyi, Cole, Duewel, Dascallos and Felts,

I am writing concerning the application for a crematorium at the Lakeside Memorial Lawn Cemetery. The cemetery is located in the Historic District of Folsom and is zoned Open Space/Conservation. This is a unique area because of the Three Historical entities located there. The Lakeside Memorial Lawn Cemetery is designated a Historic Cemetery. To the rear boundary of the cemetery is the Historic Chung Wah Chinese cemetery which is listed on the National Historic Register. And very close to the proposed Crematory metal shed site is the California State Dredger Tailings Preserve. This is probably the only site in our state that combines these aspects of our heritage in one small area. I believe the addition of a crematorium will change the character of the cemetery.

Besides the above Historical aspects, the cemetery is located close to the American River Bike trail, Lake Natoma and the residential neighborhood where I reside. A crematorium will affect many people who enjoy recreation on the trail and the lake, not just a few people.

I am concerned about Fire Danger. The proposal includes the installation of two 250 gallon above ground propane tanks. This area is located in a wildfire danger area. In fact, the City of Folsom recently declared a local emergency due to fire danger. If there were a wildfire or vandalism the propane is a dangerous threat. I am also concerned about Emergency Evacuation . Access to the bike trail and Lake Natoma is difficult for fire trucks to maneuver. Our neighborhood, in which the cemetery entrance is located, has only one entrance and the same egress. This is compounded by light rail train interruptions blocking the Folsom Blvd.-Forrest St. intersection four times every hour. Emergency evacuation could be a big problem for occupant of over 100 plus homes.

We have been researching the effects of air quality from the smoke released into the air from crematoriums and have learned that cumulative effects are harmful to unborn children, developing children, elderly and those with existing health conditions like myself.

If the crematorium is allowed, I wonder how anyone can know how many people are cremated daily and the actual condition of the air quality. The air quality report is based on two cremations per day, 5 day a week. Air quality is monitored once a year.

While there are other crematoriums not far away from Folsom, I am not against a crematorium in our city. I would hope it would be in an industrial surrounding zoned for this type of business operation, not in an area where people live, bike ride, walk, run, canoe, kayak, paddle board, fish and swim. The Sacramento State Aquatic team practices rowing on the lake and there are many bird nesting sites and animals to protect such as our bald eagles. I urge you to deny a conditional use permit and protect the citizens affected by this proposal. Thank You.

Sincerely,

Pat Binley

1209 Sutter St. Folsom, Ca

sent 8-7-2021

Dear Historic District Chairman Bracht and Commissioners, West, Ankhelyi, Cole, Duewel, Dascallos and Felts,

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Besides the above Historical aspects, the cemetery is located close to the American River Bike trail, Lake Natoma and the residential neighborhood where I reside. A crematorium will affect many people who enjoy recreation on the trail and the lake, not just a few people.

I am concerned about Fire Danger. The proposal includes the installation of two 250 gallon above ground propane tanks. This area is located in a wildfire danger area. In fact, the City of Folsom recently declared a local emergency due to fire danger. If there were a wildfire or vandalism the propane is a dangerous threat. I am also concerned about Emergency Evacuation . Access to the bike trail and Lake Natoma is difficult for fire trucks to maneuver. Our neighborhood, in which the cemetery entrance is located, has only one entrance and the same egress. This is compounded by light rail train interruptions blocking the Folsom Blvd.-Forrest St. intersection four times every hour. Emergency evacuation could be a big problem for occupant of over 100 plus homes.

We have been researching the effects of air quality from the smoke released into the air from crematoriums and have learned that cumulative effects are harmful to unborn children, developing children, elderly and those with existing health conditions like myself.

If the crematorium is allowed, I wonder how anyone can know how many people are cremated daily and the actual condition of the air quality. The air quality report is based on two cremations per day, 5 day a week. Air quality is monitored once a year.

While there are other crematoriums not far away from Folsom, I am not against a crematorium in our city. I would hope it would be in an industrial surrounding zoned for this type of business operation, not in an area where people live, bike ride, walk, run, canoe, kayak, paddle board, fish and swim. The Sacramento State Aquatic team practices rowing on the lake and there are many bird nesting sites and animals to protect such as our bald eagles. I urge you to deny a conditional use permit and protect the citizens affected by this proposal. Thank You.

Sincerely,

Pat Binley

██████████ Sutter St. Folsom, Ca

Josh Kinkade

From: Daron Bracht <daronbr@pacbell.net>
Sent: Tuesday, March 2, 2021 11:13 AM
To: Josh Kinkade; Steven Banks
Subject: Fwd: Crematorium at Lakeside Cemetary

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----- Forwarded Message -----

Subject: Crematorium at Lakeside Cemetary

Date: Mon, 21 Dec 2020 17:25:21 -0800

From: Patricia Binley [REDACTED]

To: daronbr@pacbell.net

CC: Patricia Binle [REDACTED]

To: Daron Bracht
 Chairman Folsom Historic District Commission

Dear Daron,

I am writing to you because I have learned that an application has been made to the city for a Crematorium at the Lakeside Cemetery. I understand the planning is close to completion and will be presented to the Historic District Commission soon.

I reside in The Preserve/ Lake Natoma Shores development in which some homes are located on Forrest St. across from the cemetery. Other properties back up to the cemetery from Young Wo Circle. I am concerned that many of our residents do not know of the plans that are taking place. To my knowledge we have not had any word from the city nor the developer regarding the proposal. This is compounded by the problem that we are not able to gather or meet in order to have some discussions and share information.

Speaking for myself, I have some concerns I'd like to share. I believe the cemetery is zoned open space and I wonder if a crematorium would be an allowable use for this property. Is this why the applicant is asking for a conditional use permit? It also seems unsuitable to have this type of facility adjacent to a heavily used recreational area (bike trail and Lake Natoma) and a residential neighborhood. I have questions regarding how it would be regulated if it were passed, as far as the actual amount of cremations taking place (not estimated) and safety with large amounts of propane on the property. I also am concerned with air quality regulating as we often have bad air days . I have asthma and am affected by bad air days & smoke. I also wondered if state parks have been consulted due to their property being next to the proposed area. I would be interested in their input.

I hope you can share my concerns with other commission members. I would hope if a crematorium is built in Folsom, it would be in an industrial or commercial area, not by state park land where people are recreating or living. Thank you,

Sincerely

Pat Binley [REDACTED] Sutter St. Folsom, Ca.

City of Folsom Staff and Historic District Commission
50 Natoma Street
Folsom CA 95630

December 22, 2020

Via email to:

Mickey Ankhelyi -
 ankhelyi@comcast.net

Daron Bracht -
 daronbr@pacbell.net

Pam Johns -
 pjohns@folsom.ca.us

Kelly Mullett -
 kmullett@folsom.ca.us

Kathleen Cole -
 kcolepolicy@gmail.com

Sarah Aquino
 saquino@folsom.ca.us

Josh Kincade –
 jkincade@folsom.ca.us

SUBJECT: Lakeside Cemetery Crematorium, FILE #:19-182

I am asking for your consideration to reject the conditional use permit requested by the owners of Lakeside Cemetery for the addition of a crematorium.

To be very clear, I am not opposed to a crematorium in Folsom.

In Summary, I believe the crematorium is not a conducive use for a historic cemetery on open spaced-zoned property. A crematorium would be better suited to be operated on a commercial or industrial area. Additionally, **this is planned to be an active business with hazardous gases directly adjacent to a housing development, walking trail, historical area, and nature preserve.**

This crematorium development was rejected by the City in 2002. It was a good rejection then and is good now.

Below are more details about the concerns.

1. This is a for-profit private commercial business. The crematorium will require significant infrastructure and will have consistent and growing usage. It is better suited in a commercial or industrial zoned area. (As noted in the presentation for the Zoning code changes, consistency is a key to why the Zoning code is used)
 - a. Based on wording in the request, cremations would potentially occur at a rate of 4 per day, 5 days per week, 52 weeks per year.
 - b. This would be an increase of over 2000 more car trips of employees, customers and deliveries per year.

- c. Also required are the consistent and numerous propane deliveries for this enterprise. Given the outline of the process and the forecasted demand, large quantities of propane will be needed.

This is a huge business, a factory, impacting an open space-zoned property that is next to other low impact and nature areas.

2. In Section 17.52.550* see below, crematoriums are not a listed use. It is at item #8 that you will determine if a crematorium is similar and compatible. Using the list in this section, other than being in a Cemetery, a crematorium is neither similar nor compatible. This business would be a consistent, daily, private commercial business using sophisticated equipment, stored propane, and very high temperatures. That **does not align** with the rest of the list that is much lower in activity and sophistication.
3. By allowing this crematorium to be moved to a commercial or industrial site, it will also move the increase in traffic so as to not impact the Historic District, as this is another issue of concern to the District.
4. Based on the Scope of Work document that was submitted, there is no sewer capability planned and there is no bathroom in the design document. Building this crematorium in the manner presented would be a significant development for an open spaced-zoned property. This project would continue to have a large developing impact on the property and the historic district.
5. The applicant notes in the “Our Proposition” portion of the Scope of work that it is a “...strategic placement. It is removed from any residences, parks, or schools.” This statement is incorrect. It is surrounded on two sides by park or open space. It is in an open space-zoned property and is next to a housing development. **Their Proposition is not correct** at a minimum and incredulous to most.
6. By allowing this to be built at a **commercial or industrial property**, the utilities for propane gas, electrical, and sewer are installed and are consistent in capability to that setting.
7. The Historic District is unique in our city as there is a mix of residential and business. At its basic level, that requires a mix of quality-of-life considerations of the residents and commerce for the businesses. With all of the impacting issues noted above for our district, it is much more important to **weigh the quality-of-life issues** of the residents given this proposal is so out of character for an open space area.
8. This issue was brought before the City 18 years ago and received a non-approval from the City Staff. PN02-258 is the City’s report for it. It was then withdrawn by the applicant.

Thank you for your consideration of this request.

Regards,
Paul Keast
Resident, Mormon Street, Folsom

* **17.52.550** Open space/public primary area special use and design standards.

A. Permitted Uses. When proposed by a public entity, the following uses are permitted; submittal to the historic district commission for courtesy review and comment is requested. When proposed by a private entity, a conditional use permit is required. If, in the opinion of the planning, inspections and permitting director, public or private status of the proposing entity is not clear, the historic district commission shall make a finding on the need for a conditional use permit.

1. Natural open space, including management activities necessary for conservation and safety.
2. Improved open space, including active and passive park and trail uses.
3. Transportation corridors.
4. Cemeteries.
5. Public schools, including administrative facilities.
6. Small-scale public utility structures.
7. Residential uses accessory to a public use.
8. Other public uses which are, in the opinion of the historic district commission, similar to permitted uses and compatible with surrounding uses.

In summary, please reject the Lakeside Cemetery Crematorium conditional use permit, FILE# 19-182.

Peter Lucyga
[REDACTED] Young Wo Circle
Folsom, CA 95630
[REDACTED]

July 9, 2021

Kelly Mullett
Folsom Planning Commission
50 Natoma Street
Folsom, CA 95630

SUBJECT: PENDING DEVELOPMENT APPLICATION FOR LAKESIDE MEMORIAL LAWN CREMATORIUM

Dear Kelly -

I'm writing to express my concern for the Lakeside Memorial Lawn Crematorium project, **I strongly object to approval and ask the Planning Commission to stop this project at first opportunity.**

I have read the [Initial Study/Mitigated Negative Declaration](#) and have also conducted my own research regarding Crematorium facilities sited adjacent to residential communities.

This project is a bad idea for the city and for residents in the immediate affected area, such as the *"The closest existing sensitive receptors to the project site are multiple single-family residences adjacent to the cemetery to the north, between 450 and 750 feet from the proposed crematory location"* – page 22.

My main concerns are:

1. Potential Toxic Air Contaminants (page 21 of the study)
2. Sensitive Receptors (page 22 of the study)
3. My research also highlighted problems in communities across the US similar to this proposed residential setting, citing objectionable odors and harmful effects from toxic air contaminants for susceptible adults, children and the elderly.

I have a technical background and understand how "ideal" operating specifications are developed and advertised by equipment manufacturers, such as the Hartwick Combustion HCT Apex-250 crematory. These "specifications" rely on ongoing optimal operating and maintenance calibration procedures. Maintenance lapses, operator error, and prevailing weather issues like unhealthy air days, for instance, provide unwelcome downstream impacts.

I love our Folsom community and cherish our "Distinctive by Nature" motto. Placing a Crematorium within 450 feet from the "The Preserve" residential neighborhood and adjacent the parklands and Lake Natoma is a very bad idea.

Sincerely,

Peter Lucyga

1-22-2022

City of Folsom Historic District Commission

RE: Lakeside Memorial Crematorium Project

1201 Forrest Street / 070-0260-001-0000

Planning No: PN-19-182

Commissioners,

I write to you as a concerned user of the Forrest Street neighborhood recreational opportunities and as an avid amateur historian. As you are aware, the Forrest Street and adjoining Folsom Blvd area is replete with valuable historical sites. These include the historical Chinese Cemetery preserved by the California State Park system, the Southern Pacific Superintendent's home on Oakdale St and adjacent future railroad museum, the Murer House and Education Center on Joe Murer Ct, the historic rock tailing piles located adjacent to the project location, a historic cemetery on Natomas Street, and the route of the historic Lincoln Highway along Folsom Blvd. In addition, the project abuts the Nimbus Flat State Recreation Area which includes miles of trails and recreation opportunities.

The Lakeside Memorial Crematorium Project presents several concerns:

- The location of an industrial crematorium in the Forrest Street neighborhood would be a non-conforming use under the City of Folsom zoning of Open Space/Public Primary Area and the underlying zoning of Open Space and Conservation.
- The location of an industrial crematorium in the Forrest Street neighborhood would be a non-conforming use under the Folsom City General Plan of Open Space.
- The placement of an industrial crematorium in the Forrest Street / Folsom Blvd area would be in conflict with the residential, recreational, and historic uses of the surrounding area.

- The CEQA Mitigated Negative Declaration filed for this project is deficient with regard to the air quality data. The Declaration relies on weather data from the Sacramento Executive Airport which is located over 15 miles away. The Executive Airport lies in a separate climatic zone from the City of Folsom and is affected by Delta breezes and different wind patterns. The City of Folsom is located against the base of the Sierra Foothills and suffers stagnant air patterns particularly during the summer months. These stagnant air patterns will cause the industrial pollutants to linger in the project area affecting the adjoining residential, recreational, and historical sites.

I am asking the Commission to deny the Lakeside Memorial Crematorium Project as a non-conforming use in an area of abundant historical and recreational sites.

Thank you for your attention to this matter,

Randolph Peshon
Placerville Ca

From: [REDACTED]
To: [Josh Kinkade](#)
Subject: Proposed Lakeside Crematorium
Date: Sunday, January 23, 2022 6:19:03 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

I live in the Lake Natoma Shores neighborhood where there is talk of a proposed Crematorium at The Lakeside Memorial Cemetery.

Our neighborhood is quite unique in many ways. A lot of families live here, it's the only housing community on this side of Folsom Boulevard, and also the only community that is on Lake Natoma. Our housing development is located in the canyon across from the bluffs of Lake Natoma.

The Kikkoman plant is located a mile away. A couple times a week we can smell the soy beans roasting. If we drive by the plant we don't see any smoke. Even though there isn't any smoke, the smell lingers in our neighborhood getting trapped by The Schultz Eddy phenomenon. This **causes the wind pattern to shift southward, blowing air pollutants back into the SVAB (Sacramento Valley Air Basin)**. This phenomenon exacerbates the concentration of air pollutant emissions in the air basin and contributes to violations of the ambient air quality standards. Living in the canyon under the bluff keeps the air stagnant.

The Caring Services Group (Miller Funeral Home) says there won't be any smoke and only heat waves from their industrial furnace. However, who's to say the smell and carcinogens from the furnace won't stagnate in the air from the bluffs just like the Kikkoman smell. The smell of roasting soybeans and the smell of burning flesh are two different things. This would also effect kayakers, paddle boarders, bicyclists, walkers, and birdwatchers in the State Park area which butts right up against the crematorium. How's that for trying to enjoy nature? I thought we were "Distinctive by Nature".

During the 1/11/22 city council meeting there was much concern for the preservation of the oak trees with the south of Hwy 50 development. I hope the Commission gives the Historic District, open space, and residents of The Preserves/Lake Natoma Shores community as much time, concern, and thoughtfulness as the oak trees received.

If one wanted to be environmentally conscience we would be considering Aquamation to cremation. If the population and need for after death services continues to grow we should be installing eco-friendly alternatives. We shouldn't be considering an old shed to save money. If Aquamation is good enough for Bishop Desmond Tutu, it's good for

us!

This crematorium has to be stopped in its tracks. I'm truly concerned for my family, neighbors, and Folsom residents.

Thanks,

Sean Gates

Josh Kinkade

From: Kelly Mullett
Sent: Monday, July 12, 2021 8:14 AM
To: Josh Kinkade
Subject: FW: Lakeside Memorial Lawn Crematorium Concerns

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



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From: Sean Gates [REDACTED]
Sent: Sunday, July 11, 2021 1:46 PM
To: Mike Kozlowski <mkozlowski@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; 1156yk@folsom.ca.us; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Kelly Mullett <kmullett@folsom.ca.us>; thehfra@gmail.com
Cc: david higgins [REDACTED]
Subject: Lakeside Memorial Lawn Crematorium Concerns

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

I am reaching out because I am extremely concerned about the proposal to build a crematorium at the Lakeside Cemetery...

My wife Nicole and I belong to the Historic Folsom Residents Association, and the area we live in means a lot to us.

We have two children (ages 9 and 7), and in researching online the impact of crematorium use, large amounts of mercury get released into the air, along with various other particulate matter.

I can't have my family living next to this type of threat to our health, and there are several other families who live in our area, along with those who are a bit older and more susceptible to health issues.

It doesn't make any sense to me that a crematorium would be located in a residential area that is experiencing and will continue to experience an increase in population density. Crematorium use also appears to be on the rise recently as well.

Can we please instead have this crematorium located in a more industrial area, that is not located in such a densely populated location?

-Sean Gates



Sent Via E-Mail

February 4, 2022

Josh Kinkade, Associate Planner
City of Folsom
Community Development Department
50 Natoma Street
Folsom, CA 95630
jkinkade@folsom.ca.us

Subject: Lakeside Memorial Lawn Crematorium / MND / 2022010039

Dear Mr. Banks:

The Sacramento Municipal Utility District (SMUD) appreciates the opportunity to provide comments on the Mitigated Negative Declaration (MND) for the Lakeside Memorial Lawn Crematorium (Project, SCH 202010039). SMUD is the primary energy provider for Sacramento County and a portion of the proposed Project area. SMUD's vision is to empower our customers with solutions and options that increase energy efficiency, protect the environment, reduce global warming, and lower the cost to serve our region. As a Responsible Agency, SMUD aims to ensure that the proposed Project limits the potential for significant environmental effects on SMUD facilities, employees, and customers.

We have no comments to offer at this time but would appreciate if the City of Folsom would continue to keep SMUD facilities in mind as environmental review of the Project moves forward. Please reroute the Project analysis for SMUD's review if there are any changes to the scope of the Project.

If you have any questions regarding this letter, please do not hesitate to contact me at 916.732.7466, or by email at Ammon.Rice@smud.org.

Sincerely,

A handwritten signature in black ink, appearing to read "Ammon Rice", is written over a light blue horizontal line.

Ammon Rice
Environmental Services Supervisor
Sacramento Municipal Utility District
6201 S Street
Sacramento, CA 95817

cc: Entitlements



Sent Via E-Mail

February 4, 2022

Josh Kinkade, Associate Planner
City of Folsom
Community Development Department
50 Natoma Street
Folsom, CA 95630
jkinkade@folsom.ca.us

Subject: Lakeside Memorial Lawn Crematorium / MND / 2022010039

Dear Mr. Banks:

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We have no comments to offer at this time but would appreciate if the City of Folsom would continue to keep SMUD facilities in mind as environmental review of the Project moves forward. Please reroute the Project analysis for SMUD's review if there are any changes to the scope of the Project.

If you have any questions regarding this letter, please do not hesitate to contact me at 916.732.7466, or by email at Ammon.Rice@smud.org.

Sincerely,

A handwritten signature in black ink, appearing to read "Ammon Rice", is written over a light blue horizontal line.

Ammon Rice
Environmental Services Supervisor
Sacramento Municipal Utility District
6201 S Street
Sacramento, CA 95817

cc: Entitlements

Steve Wetzel
[REDACTED] Young Wo Circle
Folsom, CA 95630

February 6, 2022

City of Folsom Historic District Commission.

RE: opposition to the proposed Lakeside Memorial Cemetery crematorium.

Dear Planning Commissioners;

I am a resident living next to the Lakeside Memorial Cemetery where the owner wants to add a crematorium.

I am writing to ask you, as Planning Commission, to act to protect residents from a business requesting to change the business from burial to cremation and in doing so will add a new revenue stream and increase profits. The business has spent an amount of money considered significant to any one resident to write up a proposal that supports the business' desire to add to their revenue and profits.

The Planning Commission needs to act in a way to level the playing field and to stand up for our Residents, young and old. The residents do not have the financial resources available to the business, to defend our position with lawyers and agencies to write counter-proposals. We have our voices and have done our best to communicate our positions.

A crematorium should not be allowed to be added to a residential neighborhood. It should only be allowed to be added to a industrial or business area, such as a warehouse or non-retail area.

I personally do not understand how creating a new revenue stream for a business could outweigh the request for residents to maintain our existing lifestyle without pollutants (mercury for one), or odors, or smoke, added to the air we breathe every day of our lives. The residents should not have to be exposed to the odor, smoke, or pollutants, from a crematorium just so this business owner can increase his profits. Dead bodies come with implants and joint replacements, along with personal items the family wants burned with the bodies. All these along with the body and the box will create odors and pollutants regardless of what the owner may have paid for to be written up in his proposal.

Please do the right thing and use common sense. If all you can do is act according to reports created by private consultants then I do not understand what purpose you, the Commission, serve other than to give a stamp of approval to businesses to do whatever they want regardless of the effect on nearby residents.

Please decline this business owner his request for a conditional permit. Please deny his request so that we can live in peace in our neighborhood without fear of having the little cemetery next door to start emitting odor, smoke, and pollutants from burning hundreds of cremations each month.

As you may or may not be aware, cremations account for about 50% of all deaths currently and it is expected to increase to 75%-80% in the next 10 years. This business owner wants in on that revenue. He should happily do it in another location and not in a residential neighborhood but in a business or warehouse area where he could easily make money. He is only proposing it in the little shack at Lakeside Memorial Cemetery because he will have larger profits using a cheap existing structure. There are many options to place a crematorium elsewhere, not adjacent to a residential neighborhood, and still make good profits. Please deny the business owner the ability to cremate at this location.

Thank you for reading my beliefs.

Steve Wetzel
[REDACTED] Young Wo
Folsom, CA 95630

Josh Kinkade

From: Kelly Mullett
Sent: Tuesday, September 7, 2021 11:08 AM
To: Josh Kinkade
Subject: FW: Another Crematorium Letter

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



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From: Daron Bracht <daronbr@pacbell.net>
Sent: Tuesday, September 7, 2021 11:52 AM
To: Kelly Mullett <kmullett@folsom.ca.us>
Subject: Another Crematorium Letter

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----- Forwarded Message -----

Subject: proposed Crematorium
Date: Mon, 6 Sep 2021 19:14:30 +0000 (UTC)
From: Steve [REDACTED]
To: danwestmit@yahoo.com <danwestmit@yahoo.com>, kevin.duewel@gmail.com <kevin.duewel@gmail.com>, daronbr@pacbell.net <daronbr@pacbell.net>

Hello Daniel West, Kevin Duewel, and Daron Bracht (Historic District Commissioners);

I am writing you to register my protest to the Lakeside Cemetery requesting to put in a crematorium in the city of folsom - specifically the Lakeside cemetery along the Preserve neighborhood and State Parks open space recreation area also adjacent to Forrest Street.

I am writing you three as I believe Daniel and Kevin are on the Planning Commission and Daron on the Historic Preservation. Should I have sent it to all 7 on the Historic District Commission? I don't know how these things work but hope I can get this to the right people representing the city residents.

I don't believe or trust that a crematorium can be operated without putting pollutants into the air and affecting residents and the recreation area nearby. I don't care what the specifications are the Lakeside Cemetery provides you. The air emitted from burning a human being, clothing, chemicals injected into the dead body, and cardboard box will contain smoke, odor, and chemicals. From what I understand the USA does not require a crematorium to have filters that would remove all this. Additionally, if Lakeside says they would put them in voluntarily, I still don't trust the cemetery to do it because they are not "required" to put them in and therefore at the last minute can omit them.

Please join me in siding with the residents of Folsom and rejecting this request from the Lakeside Cemetery to put in a crematorium. And again, I would suggest you don't approve a crematorium any where in our city limits. A crematorium should be located in an area far from residents and is more suited for a warehouse area (like Rancho Cordova south of 50) than an area next to homes and recreation.

Thank you for reading my request.

Steve Wetzel
Folsom Resident

Josh Kinkade

From: Scott Johnson
Sent: Monday, September 6, 2021 7:58 PM
To: Josh Kinkade
Subject: Fwd: No Crematorium

Sent from my T-Mobile 5G Device
Get [Outlook for Android](#)

From: SUSAN LUBIENS [REDACTED]
Sent: Monday, September 6, 2021 7:54:26 PM
To: Scott Johnson <sjohnson@folsom.ca.us>
Subject: No Crematorium

You don't often get email from susan_lubiens@yahoo.com. [Learn why this is important](#)

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Stop the crematorium at Lakeside!

Susan Lubiens
Resident since 1971

[Sent from Yahoo Mail on Android](#)

Terry Sorensen
 ██████████ Forrest Street
 Folsom, CA 95630
 ██████████

February 7, 2022

Historic District Commission
 City of Folsom
 50 Natoma Street
 Folsom, CA 95630

RE: Lakeside Memorial Lawn Crematorium Proposal (PN-19-182)
 Hearing Date: February 16, 2022

Dear Commissioners:

The same crematorium proposal herein under consideration was advanced by Lakeside nearly 19 years ago as PN02-258 and scheduled for hearing before this Commission on January 15, 2003. However that hearing was canceled and did not go forward when applicant Lakeside withdrew the proposal, apparently in light of a Staff Report prepared by the City recommending the denial of a Conditional Use Permit (CUP) to operate a crematorium on the property. A copy of that Staff Report (with voluminous attachments) is attached as Exhibit "A."

The findings advanced in the Staff Report as the basis for its recommendation of denial can be summarized as follows:

- a. The use applied for is detrimental to the health, safety or general welfare of persons residing or working in the neighborhood, and detrimental or injurious to property and improvements in the neighborhood and the general welfare of the City because the introduction of this use will impact the historical character of the existing cemetery and historical use of the area.
- b. The conglomeration of historic cemeteries, combined with the California State Dredger Tailings dating back to the 1850's, create a rare combination of unique cultural resources that will be impacted by this proposal.
- c. The use of the proposed project is not consistent with Goal 2 of the City's Historic District Design and Development Guidelines in that it does not maintain the historic use of the site and, in addition, does not further Design and Development Guideline Policies 2.1, 2.2, and 2.3 in that:
 1. The Sacramento County Historic Cemetery Commission has identified this site as locally significant and intends to present the Lakeside Cemetery to the Board of Supervisors for designation as a historic pioneer cemetery;
 2. Approval of a CUP for a contemporary use that will jeopardize the eligibility status of the site would work to discourage, rather than encourage, national register nomination; and
 3. According to the Sacramento County Historic Cemetery Commission, a crematorium is not a consistent use with a historic cemetery.

In summary, the CUP sought by the applicant in this instance is identical to the CUP sought back in early 2003, and the applicant has presented no facts to the contrary. The proposal for the issuance of a

CUP allowing the construction and operation of a crematorium in the historic area in question was a bad idea back in 2003 and remains a bad idea today. The factual basis and logic behind the 2003 Staff Report remain valid, and the current request for approval of a CUP for the proposed crematory should be denied for the reasons expressed therein. Such a resolution of this matter would be simple and straightforward and consistent with the City's advertised goal of promoting historic preservation.

In closing, one is left to wonder why the City fails to mention anywhere in its papers submitted on this matter the applicant's prior efforts nearly 19 years ago to obtain a CUP for a crematorium on the Lakeside property and the City's opposition thereto. Seemingly, full disclosure by the City of those facts should have been forthcoming. The citizenry of Folsom deserve no less.

Very truly yours,



Terry L. Sorensen

TS/dg

attachment: 2003 Staff Report PN02-258

cc by email: kcolepolicy@gmail.com
justin@revolutionsdocs.com
danwestmit@yahoo.com
ankhelyi@comcast.net
johnfelts@e55tech.com
m.dascallos@yahoo.com

jkincade@folsom.ca.us
kmullett@folsom.ca.us

Agenda Item No. 1
 PN02-258
 HDC Mtg. 1-15-03

Withdrawn

HISTORIC DISTRICT COMMISSION STAFF REPORT

PROJECT TITLE:	Lakeside Cemetery Crematorium
PROPOSAL:	Request for approval of a Conditional Use Permit to allow for the operation of crematorium
RECOMMENDATION:	Denial
APPLICANT AND OWNER:	Lorin Claney
LOCATION:	1201 Forrest Street
ASSESSOR'S PARCEL NO.:	070-0082-014, 070-0130-002, 070-0130-004
ATTACHED REFERENCE MATERIAL:	<ol style="list-style-type: none"> 1. Vicinity Map 2. Site Plan, dated 12/19/97 3. Project Description and Crematorium Illustrations 4. Documentation from the Sacramento County Historic Cemetery Commission 5. Lakeside Cemetery Research Paper written by Sue Silver 6. Letters from the Public 7. Site Photographs
PROJECT PLANNER:	Jane Talbot, Assistant Planner

BACKGROUND

The Planning Commission approved a Use Permit and Variance for Mausoleums at Lakeside Memorial Lawn in 1991 (PC91-042). An amendment to the approval was granted in 1995 (PC95-033). That approval allowed for the construction of twelve mausoleums. To date, one mausoleum has been built and one additional mausoleum is under construction. An existing maintenance building, approximately 975 square feet in area, is located along the south border of the cemetery. The front of the project site is mostly level with a slight to moderate downward slope towards the rear of the site. Lakeside Cemetery has a variety of mature deciduous and evergreen trees. The front of the cemetery, along Forrest Street, is bounded by a brick wall capped with wrought iron fencing.

The project site consists of three parcels totaling 10 acres within the Open Space/Public Primary Area of the Historic District. It is located on the west side of Folsom Boulevard at 1201 Forrest Street. The property is bounded by the Chung Wah Cemetery and the American River Recreation Area to the west, single-family residences to the north, Forrest Street to the east, and mine tailings to the south. Across Forrest Street are single-family residences.

PROJECT DESCRIPTION

The applicant is requesting approval of a Conditional Use Permit to install and operate a crematorium at the existing Lakeside Cemetery at 1201 Forrest Street. The applicant proposes to install the cremation equipment inside an existing maintenance building as indicated on the attached site plan. The maintenance building is made of corrugated metal and currently is used for the storage of grounds maintenance equipment. The applicant proposes to install a 10-foot by 30-foot "Tuff Shed" adjacent to the existing maintenance building to store the displaced maintenance equipment. No new utilities are proposed to be connected to the shed.

GENERAL PLAN/ZONING CONFORMANCE

The General Plan land use designation for the project site is OS (Open Space). The zoning designation for the project site is OS/PB (Open Space/Public Primary Area of the Historic District). The zoning district is consistent with the General Plan designation. Cemeteries are a permitted use within the OS/PB zoning upon approval of a Conditional Use Permit. The subject cemetery has been in operation since the 1800's and is a permitted use. Crematoriums are not specifically addressed in the Folsom Municipal Code. Such uses that are not specifically addressed within the Folsom Municipal Code require the approval of a Conditional Use Permit.

LAND USE COMPATIBILITY/CONDITIONAL USE PERMIT

When approving a Conditional Use Permit, the Historic District Commission must make a finding that a project's impact on health, safety and welfare will not be detrimental to the health, safety, peace, morals, comfort and general welfare of a proposed use. Some of the comments received suggest that the proposed project may impact health, safety, and welfare. Without additional environmental studies, staff cannot support or refute any of these assertions (FMC 17.60). As staff is recommending denial, based upon the appropriateness of the land use, these necessary additional environmental studies have not been prepared. Should it be the Historic District Commission's desire to proceed with this application, staff will commence with the additional environmental studies.

Historical Significance

Information provided to staff illustrates that Lakeside Cemetery is actually a conglomeration of several cemeteries that have merged over the years. This conglomeration includes the Citizen's, Jewish, Masonic, Negro Bar, Cook's/American Legion, and Odd Fellows cemeteries that date back to around 1850. Adjacent to this site is the Chung Wah Chinese Cemetery. These cemetery sites, along with the neighboring California State Dredger Tailings Preserve, create a combination of rare cultural resources grouped into one small area. This combination, in itself, is an extremely unique resource according to Commissioners with the Sacramento County Historic Cemetery Commission.

Goal 2 of the Design and Development Guidelines defines the City's objective for the Preservation of Historic Sites as:

'To maintain, restore, and reconstruct sites which represent the history of the Folsom area.'

The supporting policies indicate how the City can accomplish this goal. Policy 2.1 states that locally significant structures and sites should be identified and documented to facilitate their preservation or restoration. To date, Commissioners from Sacramento County Historic Cemetery Commission have identified this site as locally significant, and the Commissioners intend to present Lakeside Cemetery to the Sacramento Board of Supervisors for designation as an historic cemetery.

Policy 2.2 advocates that the City should encourage National Register nomination of historic buildings, as well as other historical designations by state or local agencies. Approval of a Conditional Use Permit for a contemporary land use (i.e., the crematorium) may jeopardize the eligibility status of the site and might work to discourage rather than to encourage National Register nomination.

Policy 2.3 explains the priorities with which the City should evaluate proposed projects. The preference should be given to authentic restoration of historical buildings and sites. Based on the information provided to staff by the Sacramento County Historic Cemetery Commission, a crematorium is not a consistent, or authentic, use with a historic cemetery. Therefore, staff has determined that the proposed project is not consistent with Policies 2.1, 2.2, 2.3, and Goal 2 of the Design and Development Guidelines.

PUBLIC INPUT

The applicant held a community meeting to inform the public about this project on November 19, 2002. There was much public opposition to the project at that meeting. At the meeting, issues related to property values, air quality, traffic, and cultural resources were raised. During the public comment period, staff received comments from numerous individuals and organizations. (The written comments received are attached to this report.) The information received indicates that a crematorium is not a use that is consistent in historic cemeteries.

ENVIRONMENTAL ANALYSIS

An Initial Study and Mitigated Negative Declaration were originally published on August 29, 2002. Staff received feedback regarding the Initial Study. A number of issues relating to the proposed use were raised including land use and planning, noise, air quality, hazardous materials, and cultural resources. As stated previously, staff has determined that the proposal is not compatible with the existing historical character of the cemetery, and staff is recommending denial of the request. According to the California Environmental Quality Act, "CEQA does not apply to projects which a public agency rejects or disapproves." Should the Historic District Commission conclude that the proposal is compatible with the existing historic cemetery use, the Commission will need to direct staff to conduct further research on the issues raised on this project and return to the Commission with the appropriate environmental document.

STAFF RECOMMENDATION

Staff recommends denial of a Conditional Use Permit to operate a crematorium at an existing cemetery at 1201 Forrest Street in the Historic District, based on the following findings.

HISTORIC DISTRICT COMMISSION ACTION

MOVE TO DENY THE CONDITIONAL USE PERMIT TO OPERATE A CREMATORIUM AT AN EXISTING CEMETERY AT 1201 FORREST STREET IN THE HISTORIC DISTRICT.

FINDINGS FOR DENIAL







- A. NOTICE HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE USE APPLIED FOR IS DETRIMENTAL TO THE HEALTH, SAFETY, OR GENERAL WELFARE OF PERSONS RESIDING OR WORKING IN THE NEIGHBORHOOD, AND DETRIMENTAL OR INJURIOUS TO PROPERTY AND IMPROVEMENTS IN THE NEIGHBORHOOD AND THE GENERAL WELFARE OF THE CITY BECAUSE THE INTRODUCTION OF THIS USE WILL IMPACT THE HISTORICAL CHARACTER OF THE EXISTING CEMETERY AND HISTORICAL USE OF THE AREA.
- C. THE CONGLOMERATION OF HISTORIC CEMETERIES, COMBINED WITH THE CALIFORNIA STATE DREDGER TAILINGS DATING BACK TO THE 1850'S, CREATE A RARE COMBINATION OF UNIQUE CULTURAL RESOURCES THAT WILL BE IMPACTED BY THIS PROPOSAL.
- D. THE USE OF THE PROPOSED PROJECT IS NOT CONSISTENT WITH GOAL 2 OF THE DESIGN AND DEVELOPMENT GUIDELINES IN THAT IT DOES NOT MAINTAIN THE HISTORIC USE OF THE SITE. IN ADDITION, THE PROJECT DOES NOT FURTHER DESIGN AND DEVELOPMENT GUIDELINE POLICIES 2.1, 2.2, AND 2.3 IN THAT:
 1. COMMISSIONERS FROM SACRAMENTO COUNTY HISTORIC CEMETERY COMMISSION HAVE IDENTIFIED THIS SITE AS LOCALLY SIGNIFICANT, AND COMMISSIONERS INTEND TO PRESENT LAKESIDE CEMETERY TO THE SACRAMENTO BOARD OF SUPERVISORS FOR DESIGNATION AS AN HISTORIC CEMETERY.
 2. APPROVAL OF A CONDITIONAL USE PERMIT FOR A CONTEMPORARY USE THAT WILL JEOPARDIZE THE ELIGIBILITY STATUS OF THE SITE WOULD WORK TO DISCOURAGE, RATHER THAN TO ENCOURAGE, NATIONAL REGISTER NOMINATION.
 3. BASED ON A LETTER FROM JAMES A. PURCELL, CHAIRMAN OF THE SACRAMENTO COUNTY HISTORIC CEMETERY COMMISSION DATED JANUARY 2, 2003, A CREMATORIUM IS NOT A CONSISTENT USE WITH A HISTORIC CEMETERY.

Attachment 1

Vicinity Map

Attachment 2
Site Plan, dated 12/19/97

Legend

-  Property Line
-  Trees
-  Paved Road
-  Unpaved Road
-  Existing Building
-  Fence

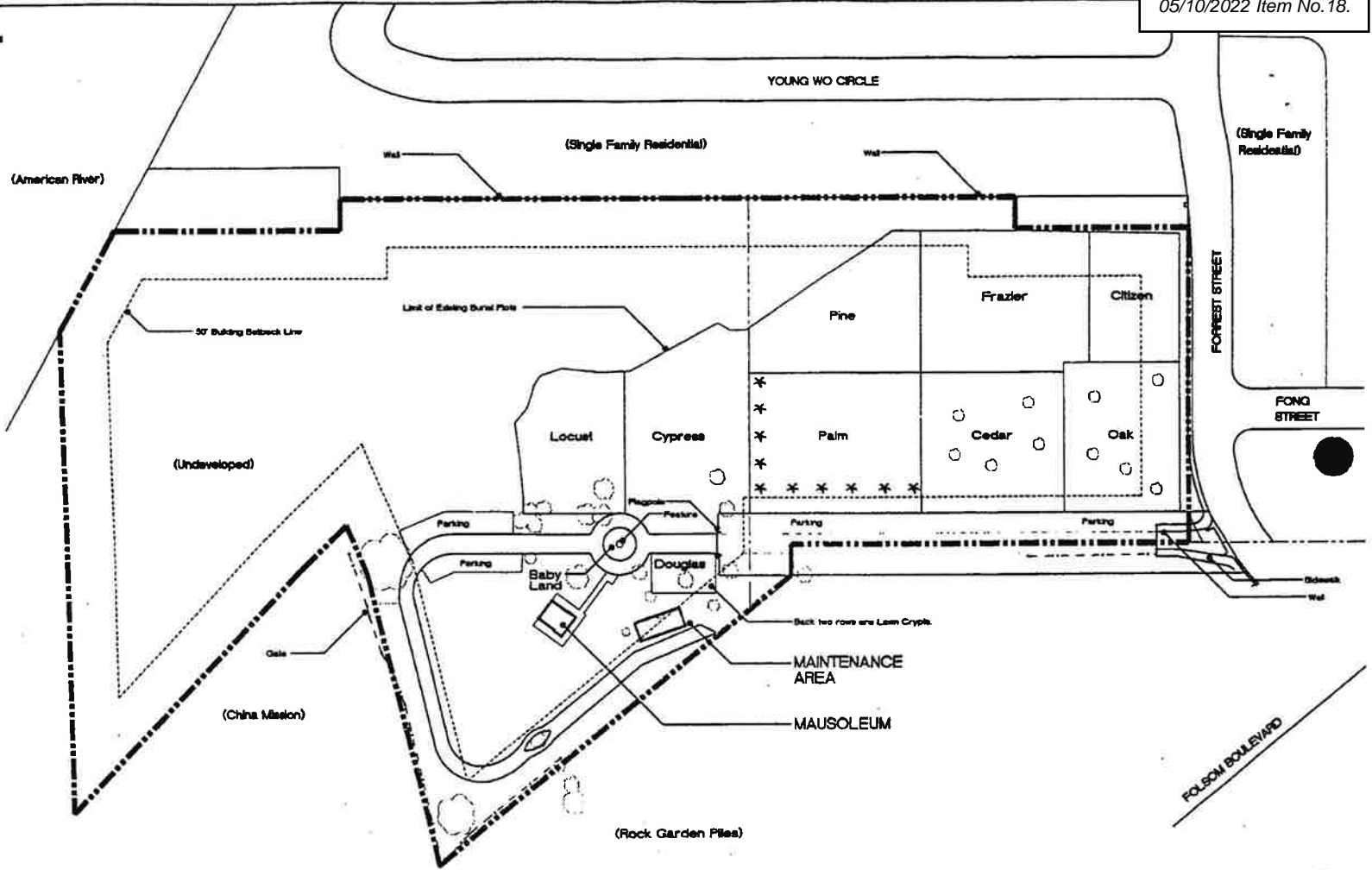


Figure 2 12/19/97
Existing Development

Lakeside Memorial Lawn

folsom, california

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Attachment 3
Project Description and Crematorium Illustrations

Form G101- General Information**1. Equipment Location Drawing:**

Attached

2. Description of Equipment:

Manufacturer: Industrial Equipment & Engineering Co. dba All Crematory

Model: 2500 Elite (identical to IEE Power-Pak II)

Description: Multiple chamber, gas fired, 100 lb/hr burn rate cremator

3. Description of Process:

The All Model 2500 Elite is designed to complete a typical cremation case in 2 hours. This time does not include the preheat of the secondary chamber, which is typically 1/2 hour or the cool-down period before the removal of the remains (1/2 hour). The cremator has a nominal burn rate of 100 lb/hr of remains and the associated containers, based on the entire cremation period. The cremator is a multiple chamber design and is fired with natural or LP gas as an auxiliary fuel. It is designed to be manually loaded in batches.

Typically, the remains are loaded in the primary chamber and then the secondary chamber is preheated by the secondary burner for 30 minutes or until the required minimum temperature is reached. The primary burner is then ignited to begin the cremation cycle. A cool-down period of 30 minutes or more is recommended at the end of the cremation cycle before removing the cremated remains and loading the next batch of remains.

The secondary chamber has a volume of 70 ft³. It has one secondary burner that is adjusted to a maximum high fire setting of 1.4 MM Btu/hr.

The secondary chamber temperature is monitored by a digital controller which adjusts the gas flow to the secondary burner to maintain the desired temperature. The cremator performs best and is most fuel efficient with a secondary chamber temperature of 1400 to 1600 °F.

The primary chamber volume has a volume of 69 ft³. It has one primary burner that is adjusted to a maximum of 0.6 MM Btu/hr. The chamber temperature ranges from 500 °F at the beginning of the first cremation of the day to 1600 °F or more during successive cremations.

The opacity alarm monitor system automatically takes action to eliminate visible emissions. If the opacity of the flue gases exceeds number 1 on the Ringelmann scale the controls will make sure the secondary air supply (throat air) is on and also turn off the primary burner to slow down the burning rate.

The All Crematory Model 2500 Elite is identical to the IEE Power-Pak II. A Power-Pak II unit was installed at North Sacramento Funeral Home in 1995.

4. Facility Description:

Funeral home.

5. Operating Schedule:

Up to 10 hours per day; up to 7 days per week; 52 weeks per year. Maximum usage expected to be less than 2000 hours per year (800 cremations).

6. Process Weight:

The nominal burn rate is 100 lb/hr.

7. Fuels and Burners Used:

Fuel is natural gas. Usage is 2000 scf/hr max.

8. Flow Diagram

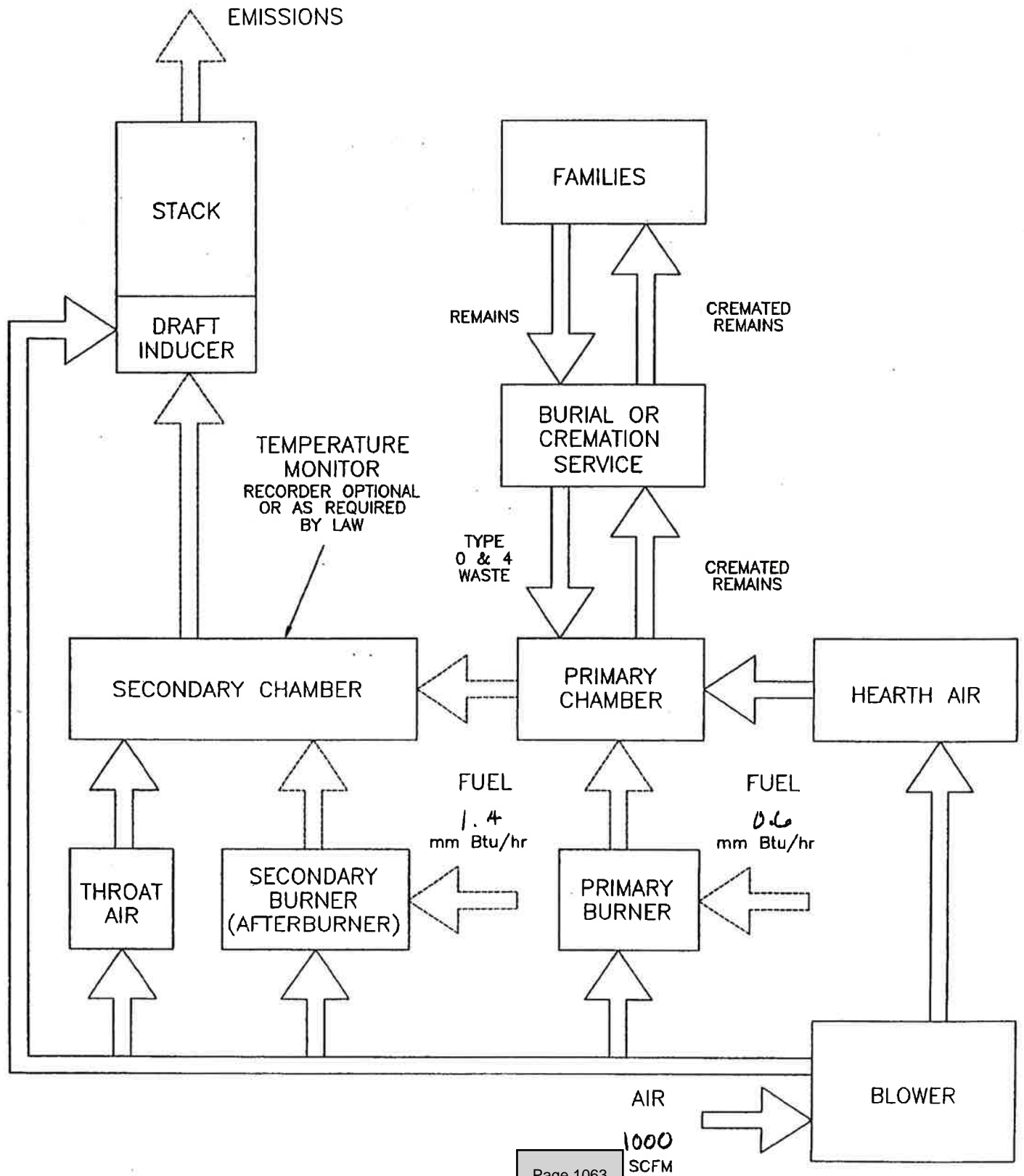
Attached.

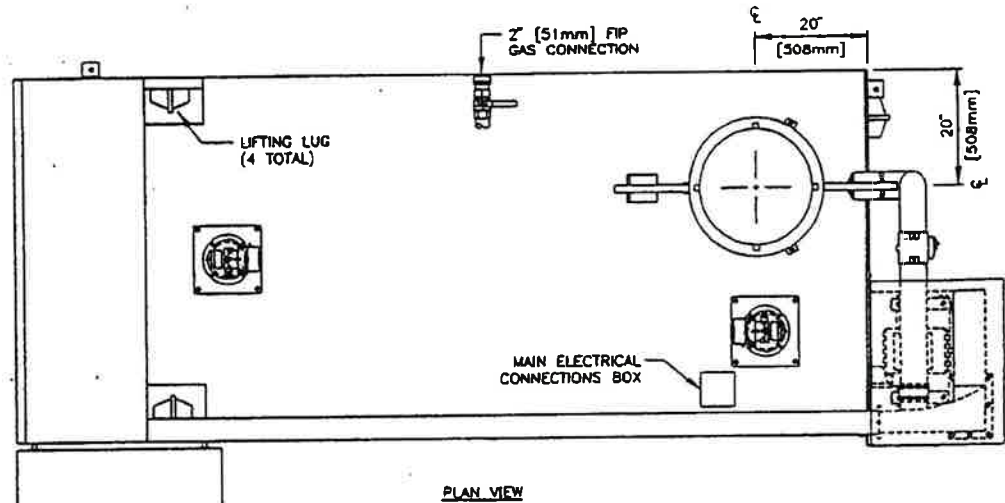
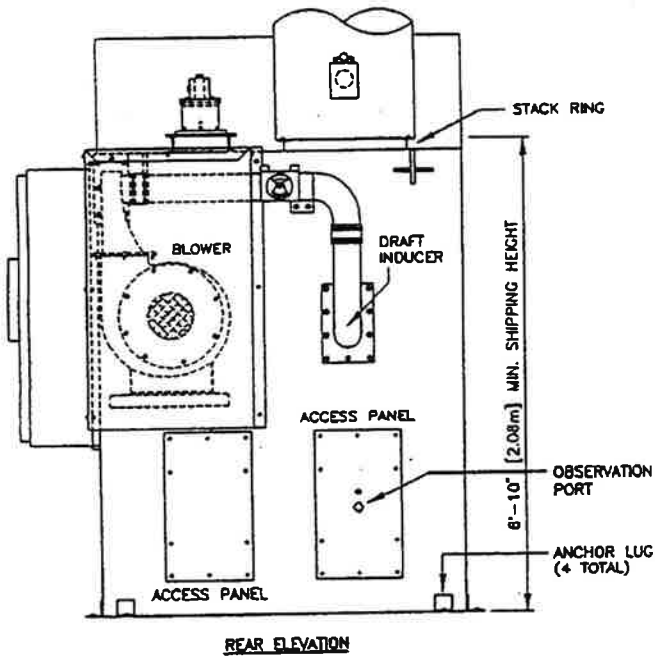
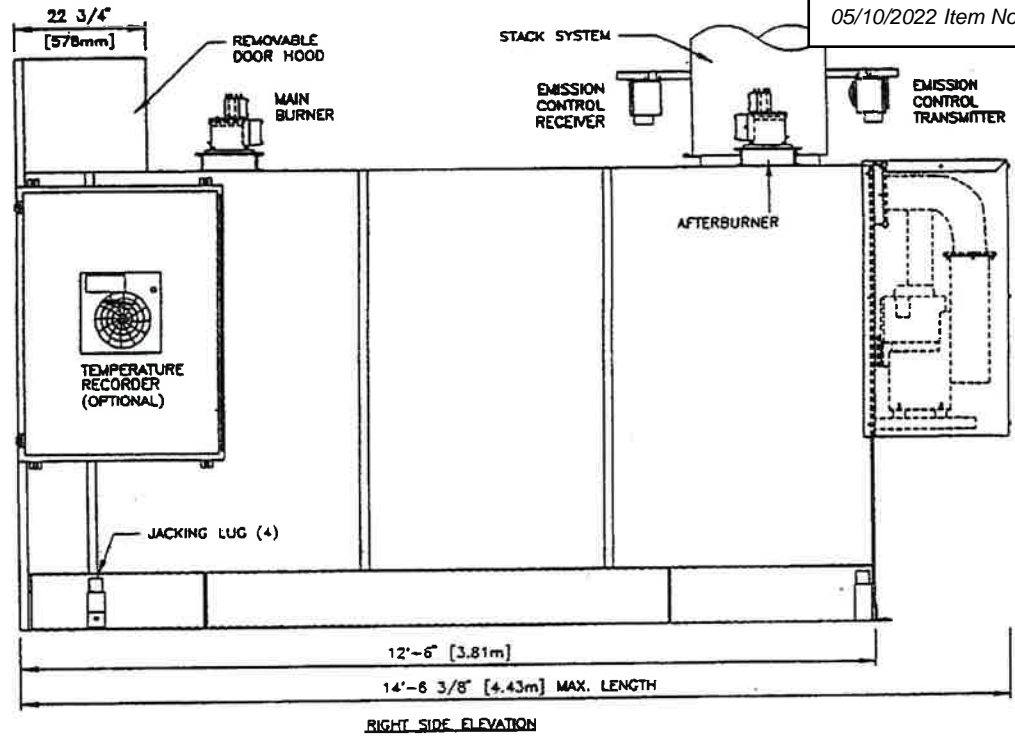
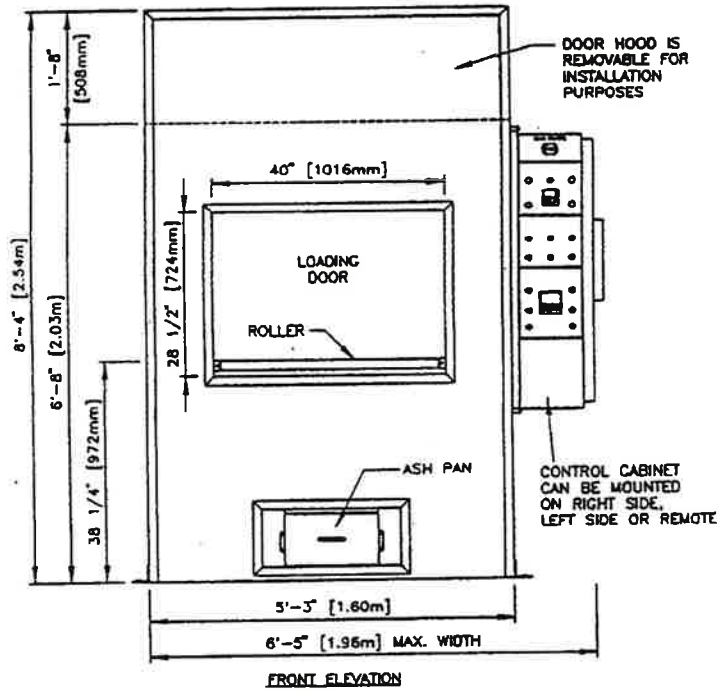
9. Equipment Drawings

A, B Attached.

C Blower motor is 5 hp.

PROCESS FLOW DIAGRAM CREMATOR





ALL CREMATORY
 P.O. BOX 547248
 ORLANDO, FLORIDA

2500 Elite

Elevations

DRAWN	C. BEAGRE	FILE	25-ELEV	SHEET NO.	1
CHECKED	T. LANGRISH	DATE	7/7/98		
APPROVED	L. THOMAS	SCALE	3/8" = 1"	OF	2 SHEETS

Attachment 4

**Documentation from the
Sacramento County Historic Cemetery Commission**

Sacramento County Cemetery Advisory Commission
4800 Broadway, Suite 100
Sacramento CA 95820

City of Folsom Historic District Commission
50 Natoma Street
Folsom, CA 95630

Attn: Jane Talbot

Dear Ms Talbot::

The Sacramento County Cemetery Advisory Commission is in receipt of your Notice of Public Hearing regarding PN 02-258 Conditional Use Permit and Mitigated Negative Declaration 1201 Forrest Street.

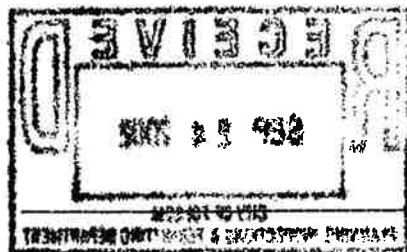
The commission is charged with the duty to encourage the preservation and designation of historical cemeteries. We are currently preparing a list of those cemeteries in Sacramento County, which should be considered historic. It is my opinion that Lakeview Cemetery will be one of the cemeteries in Sacramento County that will be on the list that is to be presented to the Board of Supervisors for designation as an historic cemetery.

While no official action has yet been taken by either The Cemetery Advisory Commission or The Board of Supervisors regarding Lakeview Cemetery we ask that you consider the above mentioned conditional use permit in the context of Lakeview's historic significance and endeavor to preserve its historic elements.

Sincerely,



James A. Purcell, Chairman
Cemetery Advisory Commission



Chair, James A. Purcell

Vice Chair, Dr. Robert La Perriere

Sacramento County Cemetery Advisory Commission
4800 Broadway, Suite 100
Sacramento CA 95820

January 2, 2003

Ms. Jane Talbot
City of Folsom Planning, Inspections and Permitting Department
50 Natoma Street
Folsom, CA 95630

Dear Ms Talbot:

The Sacramento County Board of Supervisors on June 12, 2001 approved Ordinance No. SCC-1193, which established The Sacramento County Cemetery Advisory Commission (The Commission).

The purpose of the advisory commission is:

1. To advise the Sacramento County Board of Supervisors on citizen's concerns and issues related to cemeteries;
2. To provide recommendations to preserve, protect, and maintain cemeteries;
3. To make recommendations to the Board of Supervisors regarding mechanisms for funding the preservation, protection and maintenance of cemeteries and the appropriation of funds so raised; and
4. To encourage the preservation and designation of historical cemeteries.

With regard to number 4, above, and in reference to your request for comments on the proposal for a crematorium at Lakeside Cemetery, The Commission would like to submit the following:

The area incorporating Lakeside Cemetery, the Chung Wah Historic Cemetery, and the California State Dredging Tailings Park is possibly the only site in the State of California that combines these important aspects of our heritage in one small area. Lakeside Cemetery, in fact, may be a grouping of 19th century cemeteries, which would most likely fit the criteria currently being developed to define a historic cemetery. It is the opinion of The Commission that the addition of a crematorium on that site would have a negative impact on the historical significance of the area. It is believed that a crematorium could be placed in an industrial area within Folsom to avoid the impact on the history of our countywide community.

Chair, James A. Purcell

Vice Chair, Dr. Robert La Perriere

Ms. Jane Talbot
January 2, 2003
Page 2

Area residents have notified our Commission that they oppose the placement of the crematorium on the site.

Research that has been provided to our Commission indicates that there have been instances in which monuments, and copings have been moved or destroyed, plot maps of burial locations are misplaced and that legal questions exist regarding the ownership of portions of Lakeside Cemetery. Further degradation of the historic value of the cemetery by the addition of a crematorium would only serve to lessen its cultural importance to the City of Folsom and Sacramento County.

Please contact us if we can provide any other information or support.

Sincerely,



James A. Purcell, Chairman
Sacramento County Cemetery Advisory Commission

cc Historic District Commission
50 Natoma Street
Folsom CA 95630

Dr. Bob LaPerriere

POB 255345

Sacramento CA 95865-5345

Phone (916) 481-4525

Fax (916) 481-8229

Email: drbob@inreach.com

Jane Talbot

Planning, Inspections and Permitting Department

City of Folsom

50 Natoma Street

Folsom CA 95630

Dear Jane:

In reference to your request for comments on the proposal for a crematorium at Lakeside Cemetery, I am happy to submit the following as an individual. I have been involved in cemetery preservation for almost 20 years.

I have great concern about the plans for a crematorium at Lakeside for the following reasons.

1) The area incorporating Lakeside Cemetery, the Historic Chung Wah Chinese Cemetery, and the California State Dredger Tailings Preserve is probably the only site in our State that combines these aspects of our heritage in one small area. To add a crematorium, with such limited acreage, it would not only be very close to all three of these sites, but would have an extremely negative impact on the setting, continuity, aesthetics, and historical significance of this area. . The idea that State Parks may someday develop an instructional trail by or through the tailings preserve is a distinct reality. The view of activities at the presently proposed crematorium location would be clearly evident to visitors on the trail. A crematorium could be placed in many other areas, such as an industrial area, without the impact it would have on both these

historic sites and the residential neighborhood adjoining.

2) There are also non historic concerns. Because of the many recent reports of problems at crematories, more and more families are requesting to view the cremation process. The planned location, in a rather crude metal building, is certainly not a respectful or comforting setting for this.

3) I also have concern regarding the operation of a crematorium in a cemetery that has apparently been plagued by multiple and unfortunate problems over many decades. A local newspaper article about five decades ago stated there was no further space for burials in Folsom, yet burials have continued in Lakeside. There are many stories of copings, and possibly monuments, having been moved or destroyed, a lack of adequate plat maps in some areas, and even questions regarding the ownership of certain areas of Lakeside. The fact that SCI turned the cemetery back to its current owners might suggest that they encountered such concerns.

Lakeside Cemetery, as I am sure you are aware, is really a grouping of primarily 19th century cemeteries (Citizen's, Jewish, Masonic and Odd Fellows, dates as early as 1850), all of which I consider historic. It would be extremely unfortunate if additional negative factors, such as the installation of a crematorium, further impacted these historic cemeteries...the final resting place of so many of our pioneers and early settlers who were responsible for creating the great region that we all live in and enjoy today.

Please contact me if I can provide any other information or support.

Sincerely,


Dr. Bob LaPerriere

cc: Historic District Commission

Attachment 5
Lakeside Cemetery Research Paper
Written by Sue Silver

LAKESIDE CEMETERY
Folsom, Sacramento County, California

Also known as the
Negro Bar Mining Camp Cemetery,
Old Masonic Cemetery, New Masonic Cemetery,
Odd Fellows Cemetery, Cook's / American Legion Cemetery,
Citizens', Folsom City, and Folsom Cemetery
and
Folsom Jewish Cemetery

Researched, Compiled and Written
by
Sue Silver
(First Draft published: June 27, 1999)
Updated: December 8, 2002

WRITER'S PREFACE

This was not an easy study for me nor was there any joy in locating the information about those who were buried in the cemetery and can never be found again. I have lived through nightmares of the worst kind and have spent hours contemplating how I might help make what has happened to this cemetery understandable.

I have learned to live with the knowledge that I cannot correct the years of abuse this cemetery has suffered. I knew I could not live without telling its true story.

The research performed for this study was begun in late 1994 and was continued until December of 1997, and is not yet considered completed. In researching, I found that many years of death notices and obituaries will need to be located utilizing the Sacramento papers as many editions are not available from the local Folsom Telegraph during those early years. For the most part, I have supplemented the very earliest years, those which encompass information written at the time of the Negro Bar mining camp (1850 - 1856) through those first pivotal years of the birth of the Town of Folsom (1856 - 1868). It was mainly these years which so very much information was lacking.

The compiled listing which accompanies this study report, shows there are **over 262 documented burials within this cemetery which cannot be located today**. There are 18 burials of unknown persons which are located in unmarked and unknown grave sites. There are **approximately 290 other probable unmarked and unknown graves located somewhere** in the cemetery in areas which today appear to be virgin, vacant ground. Where any of these missing, unmarked and unknown graves are is anybody's guess. Lakeside Cemetery has no historic records of burials, per their own admission.

The study report and the compiled listing paint a portrait of an historic cemetery which has been raped, mutilated and desecrated by the very authority licensed by the State of California to operate and protect it. Part of the responsibility for what has occurred at Lakeside Cemetery also rests on the shoulders of the Board of Supervisors of Sacramento County, and the Folsom City Council. Each of these two civil authorities have, at different times, been the legal authority for the Citizen's Cemetery, in all its progressive configurations.

The private, licensed operators of Lakeside Cemetery do not have any legal title to the old Citizen's Cemetery, nor do they likely have a legal title to what is known as the Cook's/American Legion Cemetery. Lee Miller knew this and when he sold Lakeside Cemetery to Robert Claney in 1963, he sold only the New Masonic Cemetery and the Jewish Cemetery, believing he had legal title to that religious ground. The subsequent acquisitions of portions of the original Negro Bar (aka: Citizen's) Cemetery, the Cook's/American Legion Cemetery, and the 1904 addition (Lots 1 through 3 of Block 44) to the Citizen's Cemetery, were not legal transactions.

Further, under the ownership of Robert Claney, much of the cemetery ornamentation which has disappeared was wilfully and maliciously removed by he and his staff. Whatever the motivation or intent of doing so, it apparently was not enough that, under his ownership, Claney had all the lone, historic tombstones laid flat in the ground to crumble away over time, but he obviously was desperate for additional burial space, so that in removing family plot coping he was able to gain access to what he thought were "unused" grave plots.

The fact that the Department of Consumer Affairs, which investigated charges against the cemetery operators in 1996-97, was unable to obtain sufficient, hard evidence (according to our weak and ineffectual cemetery laws) to make felony arrests, does not preclude that felonies were committed at this cemetery. Not just once, but time and time again. And no one - not one resident of the city of Folsom, not one city official, not one law enforcement official - made any effort to stop the desecration which he wrought upon this historic burial ground.

What once may have been one of the most unique, historic, gold rush-era, pioneer cemeteries to be found in the lower foothills of the Mother Lode, is now just a lawn full of destroyed and decaying historic tombstones and monuments to the early residents of Negro Bar and the Town of Folsom and vicinity. The evidence of this is contained in the compiled listing with it's source references, and with the photographic evidence, taken over various years, which show a long-ago cemetery that bears no resemblance to the "nice, memorial lawn" disastrously created by Robert Claney and Lakeside Cemetery.

If the State of California will not act to help this pioneer-era burying ground, who may the people trust in another hundred years with the remains of their loved ones or themselves?

I think about that a great deal. Perhaps so should you.

INTRODUCTION

The City of Folsom, in Sacramento County, was incorporated in 1947. Prior to that, this historic California town was first the site of the Negro Bar mining camp from as early as 1849, later becoming the Town of Folsom, as surveyed and platted for Capt. Joseph L. Folsom by Theodore T. Judah in 1855.

In February 1856, the town of Folsom became the terminus of the Sacramento Valley Railroad, the first railroad in the west. Negro Bar, for the most part still in place until the purchase of the town lots and blocks, was located between the American River and what is now Block 12 of the official plat of the town.¹

At all early mining communities, the one sure thing that faced the men who flocked to the gold fields of California, was that death was a part of life. Gold rush-era cemeteries dot the roadways and byways of California's Mother Lode. Some are well known, but some have become lost to the past. The mining camp at Negro Bar, with its surrounding camps of Texas Hill, Alder Creek, Rebel Hill, Ashland, Mississippi Bar and others, would be no exception in the need to find a place to bury the dead.

Studies conducted by the El Dorado County Pioneer Cemeteries Commission, at pioneer-era cemeteries throughout the Mother Lode, reveal that, wherever possible, the miner's cemeteries were established on small hills or knolls. Out of the reach of the miners' pans and sluice boxes along the river banks, these early citizens of the state felt certain they were laying their dead to rest in peace.

Some communities succeeded to that end. Some did not. In 1855 the cemetery at Mormon Island, east of Folsom, was in danger of being sluiced away by three mining companies. It was written that some bodies had already been washed away. In March of 1856, the editor of *The Granite Journal*, the first newspaper in Folsom, advised friends and loved ones that they had best see to the removal of their deceased before they were mined away.

In other instances, special Acts of the California State Legislature were passed to approve the relocation of cemeteries. This is known to have occurred at the Bidwell's Bar Cemetery, the Cold Springs Cemetery in El Dorado County, and to a Catholic cemetery in Stockton.

California does, indeed, have an appalling history of its treatment of the resting places of the dead. In that no records of burials were kept at most, those cemeteries still in use today, which date to the state's earliest times, are known to contain the unmarked remains of the state's pioneers, the graves of which are being impacted by current burials.

At the original Negro Bar Cemetery, now considered a part of Lakeside Cemetery, there is but one tombstone dated 1850. Yet the camp was said to have been populated by up to 700 miners between 1849 and 1851. In the Fall of 1850, Sacramento City experienced a terrible Cholera epidemic

¹ A copy of the townsite map may be found at the Folsom History Museum, the Sacramento County Recorder's Office, and the Sacramento Archives and Museum Collection Center (SAMCC).

which began in October and did not abate until sometime in December². Over 800 deaths occurred in the city alone, and thousands fled to outlying areas. The tombstone dating to the Negro Bar Cemetery is that of one Cholera victim whom the 1850 U.S. Census enumerated as a miner at Negro Bar in September of that year. He was Henry Probasco and he died on October 29, 1850. His tombstone identifies his enemy - "Died of Cholera".

How many human remains of other early pioneers of this vicinity who died and were buried in this cemetery, may never be known for certain. Reviewing studies of other cemeteries of similar size and era³, reveals that there are many more marked graves of the earliest era within those cemeteries than there are remaining at Lakeside Cemetery today. Empirical evidence of this type is an indicator that there must be a large number of unmarked and unknown graves within Lakeside Cemetery which may now be being impacted by current burials. Based upon this correlation between towns of similar size and population, the lack of historic tombstones at Lakeside is, at best, unnerving.

BACKGROUND

While researching the history of Folsom, it became necessary to visit the cemeteries located in the city. Historic cemeteries are generally filled with old marble and granite tombstones inscribed with various dates and sayings, and they stand prominently within the grounds. At Lakeside Cemetery, the majority of the remaining historic tombstones were found to have been laid flat in the ground where they are disintegrating and being destroyed not only from natural elements, but from the abuse of being driven over by lawn mowers and tractors.

Additionally, in 1994, historic tombstones, and remnants of historic tombstones and granite bases, were found hidden in Oleander bushes by the cemetery's storage sheds. This discovery eventually led to the study of the cemetery from which this document resulted.

During the summer of 1995, there appeared to be an increased number of burials occurring at the cemetery. A check of Disposition Permits filed with the County Office of Vital Statistics⁴, proved

² MEMORIES, MEN AND MEDICINE, A History of Medicine in Sacramento, California with Biographies of the Founders of the Sacramento Society for Medical Improvement and a few Contemporaries; J. Roy Jones, M.D.; The Sacramento Society for Medical Improvement, 1950; Premier Publications, Ltd., Sacramento, California

³ For example, a study of the 3.19 acre El Dorado Cemetery, in the town of El Dorado in El Dorado County, indicates the probability of over 500 unmarked graves, yet still it bears many more early markers than Lakeside.

⁴ A permit must be obtained for the interment and "disposition" of all human remains in the State of California. These permits, required by State law, are only required to be kept on file in the county's offices for a period of 12 months. Thereafter, they may be disposed. Sacramento County throws these documents away, and so it is impossible, without the name and date of death of the deceased, to verify interments in any cemetery.

that Lakeside Cemetery, through Miller Funeral Home, had not obtained as many disposition permits as the number of graves they had opened and closed in the ground that summer.

This discovery led to the theory that they were perhaps "prospecting" to find vacant ground. And that theory led to the realization that if they were seeking open ground and found historic remains, would they dare to remove those remains to cleanse the grave hole for future use? As ugly as this seems, it has happened all over California cemeteries and elsewhere.

A citizen complaint was filed with the State Cemetery Board in August of 1995. That Board, which disbanded in disgrace some thirty days after inspecting the cemetery in September, ruled that no improprieties had occurred at Lakeside. Sixty days after the Cemetery Board's duties were relinquished to the Division of Investigation of the Department of Consumer Affairs (DCA), the Board's director was under investigation of alleged misuse of trust funds from the Arlington/Bellview Cemetery in Sacramento County.

In late January of 1996, after the news of the investigation of the Board's director, a request was made to the director of the Department of Consumer Affairs that the department re-open the complaint and re-investigate the allegations made therein. In March 1996, a meeting was held with the new investigator for DCA which reviewed the facts and allegations of the complaint.

In late October and early November of 1996, the California Department of Consumer Affairs (DCA), in cooperation with the California Department of Parks and Recreation (DPR), excavated and removed approximately thirty family plot coping pieces from State park land adjacent to the cemetery⁵. It is believed they were removed by the cemetery operator between 1993 and 1995. Only one name was inscribed on any of the pieces of ornamentation found - **KNIGHT**⁶. None of the other ornamentation or tombstone bases found were inscribed to enable us to identify from which dismantled family plot it had been removed.

During the course of the excavation many citizens came forward to say they had had suspicions about what was going on at the cemetery. No one wanted to be identified and no one gave much

Early El Dorado County "Burial Permits" were retrieved and saved from 1910 through 1995 (1994 is the only exception) and these documents have been a tremendous help in the study of that county's pioneer cemeteries and burials. In Sacramento County, one would have to review all the death certificate microfilm rolls at the County Recorders Office in order to obtain much of the information obtained from the burial permits in El Dorado.

⁵ Color copies of photographs taken at the time of the excavation accompany this report. Video tape of some of the excavation is also available at the Folsom History Museum upon special request.

⁶ This piece of cut granite is believed to have been removed from the family plot of Henry T. Knight, an early trustee of the Natoma Water & Mining Company, and later the superintendent of the Folsom Water Power Company. His grave is located within the "Cook's/American Legion Cemetery" near the graves of many of the people with whom he was inter-related. The grave of his brother, Nathaniel Knight, is believed to be located near Henry's and is without marker today, unless his name may be found on the underside of Henry's marker which has now been laid flat in the ground.

detail by way of particulars. Except, that is, the former caretaker of the cemetery, who spoke with one of the investigators while in his car in the parking lot. It is not known if the investigator included in his reports what he was told by the former caretaker that day, but it is known that DCA never followed up in discussions with him throughout the remainder of the investigation. What he had to say was pivotal to the investigation and would have enabled DCA to file criminal charges against the operators.

To the date of this report, no charges have been filed by either DCA for this illegal removal of these items from the cemetery, or by DPR for the dumping of them onto state lands. The owner/operator of the cemetery sold the cemetery and his mortuary to a large corporation within 8 months of this investigative excavation. The cemetery continues to be used with present interments occurring within the most historic sectors of the graveyard - those representing the burial grounds dating to 1850, 1860 and 1870. In all likelihood, historic graves are either being buried over or, very likely, those human remains were removed by the former operator.

This document is being published so that sometime, somewhere in the future, there will be a record of what has occurred at this cemetery. Those in search of their family histories, those who plan to inter their deceased loved ones at this cemetery in the future, need to know that this cemetery, as of this date, is estimated to contain 575 unmarked and unknown grave sites. In more specific terms, this number of graves represents 12,075 square feet of ground if buried side-by-side and end-to-end. It totals roughly 1/4 of an acre of land or more if they were all buried in the same location.

What appears as "open space" within the lawn section of this cemetery, does not mean that there are no graves in the ground. Photographs taken in 1889, 1908 and in the 1940's and 1960's bear this out⁷, showing wooden headboards and other markers in areas now visible as vacant areas. Fires, which are documented to have swept through the cemetery, also took their toll on the wooden markers. The family plots containing the graves of members of numerous families which have been documented to be buried here, can no longer be found. Some of the markers which have been laid flat in the ground are believed to contain additional names of deceased on the unexposed sides of the monuments.

HISTORY OF CEMETERY SECTIONS

Lakeside Cemetery, is a combination of five historic burial grounds (Masonic, Odd Fellows, Jewish, Negro Bar [aka: Citizens'] and Cook's, and New Masonic), and has been actively used from as early as 1849 with the emergence of the mining camp called Negro Bar.

⁷ Early photographs may be found at the Folsom History Museum and at the Sacramento Archive and Museum Collection Center (SAMCC) in the Hepting Collection (photos by Thomas Betz and Len Kidder, 1940's).

Negro Bar Cemetery

Of the various and separate sections, the Negro Bar Cemetery⁸ was the first location to be used. It was comprised of Lots 4 through 8 of Block 44 (Figure 1.). These lots were sold at the auction of the town lots and blocks in Sacramento City in 1856⁹. According to church records and the newspaper obituaries, these lots would later be referred to as the Folsom cemetery, the City Cemetery, and the Citizen's Cemetery, from as early as 1861 until at least 1942¹⁰.

According to California law, the "Citizen's Cemetery" was a public cemetery. That is to say, that in 1872, under Political Code section 3105, title to this cemetery vested to the County of Sacramento by benefit of the provisions of the statute, which stated title to a cemetery, then in continuous use by the community for a period of five years, vested to the community. If not located within an incorporated city, authority for the cemetery would fall to the county in which the cemetery was situated¹¹.

Section 3105 further provided that once used as a public cemetery those lands could never be used for any other purpose whatsoever. Therefore, Lakeside Cemetery's claim of ownership of these lots is invalid, since title vested to Sacramento County in 1872, and they could never be used as anything but a public cemetery. Lakeside Cemetery is a licensed, private cemetery, operating an historic public cemetery outside the provisions of State law.

Lots 1 through 3 of Block 44 were owned by Joseph McDonald and his wife, Mary, during the 1860's. McDonald was killed in a railroad accident in December of 1867. According to his probate records, his property adjacent to the public cemetery, was never sold from the estate. This was probably because the "Citizen's Cemetery" had already encroached onto the lots owned by McDonald, and they were, therefore, not an attractive purchase for any buyer.

Graves within Lots 1 through 3, formerly belonging to McDonald, are known to have occurred as early as the 1920's. According to the County Tax Assessor's records, these lots were transferred to

⁸ The Negro Bar Cemetery was originally located on Lots 4 through 8 of Block 44. It was later referred to as the Citizen's Cemetery contained within only those lots. It appears that sometime after 1880, what is now called "Cook's" or "American Legion" Cemetery (Lots 14 through 16 of Block 44), was also referred to as the Citizen's Cemetery. Sometime after the turn of the century, and well into the 1940's, Lots 1 through 3 of Block 44, also became a known as a part of the Citizen's Cemetery.

⁹ No evidence has been found that the original purchaser ever transferred his title to the lots at anytime thereafter.

¹⁰ The Trinity Church records list a burial in the "Folsom Cemetery" in 1861. This would have been previous to the purchase of the Jewish cemetery lots in late 1861, and a year earlier than the purchase of the Odd Fellows and Masonic cemeteries in 1862. The Folsom Telegraph first advises of a burial in the "Citizen's cemetery" in 1887, and it continued to use that name as late as 1942. Previous to 1887, the paper referred to it only as "the grave yard."

¹¹ This per Political Code section 3109 of the California statutes enacted in 1872.

the State of California in 1935, presumably for back property taxes. Lakeside Cemetery supposedly purchased these (already) used burial parcels from the Sacramento County Tax Collector in 1976. Yet, according to the former caretaker of Lakeside Cemetery in a 1992 audiotaped, tour given to members of the Folsom Historical Society, Lee Miller supposedly "opened" this area up for interments in the mid-1960's.

The sale of these lots by the County Tax Collector was not lawful in that the grounds had been used by the public as a public cemetery from at least the 1920's. Again, according to State law, once it had been used as a cemetery by the public, it became a public cemetery never to be used for any other purpose whatever. How this property came to be sold for taxes when cemeteries have been exempted from property taxation since 1859, is not known. What is known, upon review of the state's laws, is that these lots are public property which are being unlawfully operated as private cemetery grounds.

Cook's/American Legion Cemetery

Cook's Cemetery (Lots 14-16, Block 44) (Figure 2.) has been historically known by several references. It is believed it was first called Murray's Cemetery for William Murray who died in 1870. It was later called Meredith's Cemetery for James S. Meredith whose daughter Laura Belle was supposed to have been interred in the Masonic Cemetery and whose grave somehow was placed in Murray's Cemetery. Later it was referred to as the Blower Cemetery for David Blower, the husband of Maggie Cook Blower and brother-in-law of Frank Cook, for whom it was later called Cook's Cemetery. Shortly after World War I, Frank Cook (purportedly) deeded the cemetery over to the Lewis J. Blodgett American Legion Post, and it became known as the American Legion Cemetery.¹²

Similar to the Citizen's Cemetery, this too is a public cemetery under the law. When William Murray died in 1870 and was buried in his "own cemetery," the ground began to be actively used by the community. No transfer of Murray's title has been located to date, though the county tax assessor does list James S. Meredith as owning an undivided 1/2 interest in the lots in 1873, one year after the enactment of the law which vested title to the public. It is likely the assessor listed Meredith since Meredith's daughter's grave is located within Lot 14. And, despite the Cook's claim of title to this cemetery, no deed has been located to prove the Cook family ever held legal title to the lots.

SPECIAL NOTE: Regarding Cook's, Citizen's, "City" and "Folsom" Cemetery References

¹² FN dated 2/22/1999: The following newspaper article was located after the original writing of this report:

"Lewis J. Blodgett Post wishes to announce the transfer of Cook's Cemetery to the Post. The cemetery will now be known as Lewis J. Blodgett Memorial Cemetery and any person owning plots wishing to make changes, please consult Anthony Castro, service officer of the Post."
(Folsom Telegraph, 5/26/1939, 1-3.)

This research has revealed that Lots 14 to 16 of Block 44, commonly referred to as Cook's/American Legion Cemetery, was also referred to as the Citizen's Cemetery, City Cemetery and Folsom Cemetery based upon the finding of tombstones of deceased of whom these various cemeteries were noted in obituary references.

This will create a great deal of confusion in determining the location of unmarked graves for they could all be anywhere within Lots 1 to 8 and 14 to 16 of Block 44. Because of these references, it is suspected that the residents of the area used Lots 14 to 16 of Block 44 at will, just as they did the Citizen's Cemetery. There was never any advertisement for plot sales found in the newspapers which would indicate that Murray, Meredith, Blower, or Cook ever really administered this cemetery. It is suspected that the community viewed it, and used it, as a **public cemetery**, just as they did with Citizen's Cemetery.

Citizen's Cemetery has been found to be referenced alternately over time as Citizen's, City or Folsom Cemetery. Because this was the **public cemetery**, during various points in time, the residents referred to it by the name used within that specific period of time.

The use of the term "City Cemetery" is especially confusing because the Sacramento City Cemetery is so heavily identified within the county. Almost always, in researching cemeteries located within townsites, this term arises periodically. This has been found in places such as Diamond Springs and Placerville in El Dorado County. The people who lived in the towns (versus an incorporated city) still referred to their locales as a "city" and their cemeteries were, naturally, "City" cemeteries.

Jewish Cemetery

The Jewish Cemetery, originally lots 13 through 16 of Block 43 (Figure 3.), was purchased by Jewish merchant, Joseph Fiel, in late 1861. Title to the property was later held in the name of E.R. Levy, and finally rested in the hands of Simon Cohn. The title to these lots was never transferred from Cohn's estate, and his estate documents officially established it as the "Folsom Jewish Cemetery" and not as the "Cohn Family Jewish Cemetery" as it would have to have been called if the grounds belonged to the family personally.

Title to the Jewish Cemetery was held in these individual's names probably because the Hebrew Benevolent Society, established at Folsom in 1857, had formally disbanded by the time the property was purchased. Fiel was a cousin through marriage of E.R. Levy, and it would have been understandable for Levy to take title in trust for the Jewish residents of Folsom after Fiel's death. Cohn was one of the most prominent Jewish men in Folsom, and it would also be understandable for him to assume title in trust after Levy.¹³

¹³ A chain of title to locate the transfer of this property has not been performed. This information has been taken from the County Tax Assessor's map book information. It is not yet known if the deed formally passed from Fiel to Levy, or from Levy to Cohn.

In spite of the fact that title to the Folsom Jewish Cemetery never legally passed from Simon Cohn's estate to any individual or group of individuals, the descendants of Cohn quit claimed Lots 13 and 14 to Lakeside Cemetery in the 1960's, when Lakeside was then under the ownership of Lee Miller¹⁴. That (illegal) quit claim deed, however, conditioned the transaction to the maintenance of a fence and road to segregate the Jewish cemetery ground from the cemeteries of the gentiles. The fence and roadway have both been removed since the execution of that deed.

Odd Fellows and Masonic Cemeteries

The Granite Lodge No. 62 of the Independent Order of Odd Fellows (IOOF) and the Natoma Lodge No. 64 of the Free and Accepted Masons (Masons), co-jointly purchased lots 9 through 13 of Block 44 in 1862. The property was divided so that the Odd Fellows would use lots 9 through 11 for their cemetery and the Masons would use lots 12 and 13 for their organization's cemetery. The IOOF and the Masons each sold their cemeteries to Lakeside Cemetery in 1969 and 1970, respectively.

Interestingly, since Lots 4 through 8 of Block 44 had been identified to be a "Public Cemetery" by the County Tax Assessor as early as 1873, by 1911 or 1912, the assessor's records reflected that these lots belonged to the Odd Fellows. When or how the Odd Fellows may have laid claim to the public cemetery is not known. Nevertheless, when they sold their burial grounds to Lakeside Cemetery in 1969, these four previously identified public cemetery lots were included in the deed.¹⁵

HISTORY OF FUNERAL HOME OPERATORS

In order to understand who was handling the burials within the cemetery throughout time, a review of the undertakers of Folsom is necessary. In the first few years of the Negro Bar mining camp, of course, we can assume that the miners took care of the burial of their associates and friends.

Generally speaking, this would entail wrapping the body in a blanket and depositing it into the ground. Lumber was scarce, there were few sawmills in operation in the beginning, and the cost of wood for a coffin was prohibitive. They would have much rather spent money on lumber for building sluice boxes and long toms to aid in their mining operations. Later, of course, as the cost of wood and lumber came down, the dead were buried in common pine boxes. The more affluent

¹⁴ Lakeside at that time was comprised of only the New Masonic Cemetery located on what was supposed to be Lots 15 and 16 of Block 43, but which, in reality, was located within the 66 feet of Sacramento Street with only 44 feet of Block 16 being taken for actual use. It would appear that the residents, the Masons, and the Cohn Family (P.C. and Alice Cohn and Rosa (nee Cohn) and Nathan Kohn), forgot that the cemetery parcels were laid out on the original lots and blocks, containing streets and alleys separating them, at the time the Masons obtained the ground for the New Masonic Cemetery from the Cohn heirs.

¹⁵ Since title to Lots 4 through 8 of Block 44 vested to Sacramento County with the enactment of Political Code section 3105 of the California statutes, the transfer of this public cemetery to private ownership was unlawful. The Odd Fellows, having no true title to them, could not have legally sold them to Lakeside Cemetery.

the deceased was the more likelihood his coffin would have metal handles. The less influential person's coffin probably had rope handles.

As communities developed over time, it is seen by advertisements in the newspapers that undertaking was only a side business to one that brought a regular clientele. In Folsom, William Murray appears to be the first merchant to supply coffins and undertaking services. His furniture house advertisement found in the Folsom Telegraph of October 2, 1862, was dated August 1861. The advertisement, however, touted his furniture business, and said nothing of the furnishing of coffins or undertaking services.

In May of 1869, Murray sold his business to Jacob Miller of Sacramento. Miller's first advertisement included, "(P.S. Undertaker and hearse furnished at reasonable rates.)" Miller's "Furniture House" was first located in the building on Sutter Street which was built by Murray and which still stands on the south side of Sutter Street between Riley and Wool streets bearing the name "MURRAY" on its edifice.

Thereafter, Jacob Miller attended the needs of the dead, providing both coffins and caskets and funeral services as well. He often served as the coroner for the eastern boundary of the county and was responsible for the burial of the unknown or indigent deceased in the vicinity. By the turn of the century, Miller's son Oscar, had gone to undertaking school (a trade his father is not known to have formally studied) and was active in the family business. Jacob died in 1905, leaving the business to his son, Oscar.

Oscar Miller continued the business until sometime in the early 1940's, at which time his son, Leland (Lee), also trained in the field of undertaking and embalming, had already joined the family enterprise. By this time, the only business being conducted by the Miller family was the funeral home. Oscar died in 1946, leaving the business to his son Lee and his wife, Elizabeth. Elizabeth died in 1954, leaving the business to Lee.

Approximately two years later, Lee saw the future in owning his own cemetery. Folsom Dam and Lake had recently been completed and the future of Folsom probably seemed brighter than it had in years. A recreational lake near the City of Folsom would draw new "settlers" to the area. Aerojet General was in operation and brought many new residents to the town. The cemeteries of the town were less than well kept, and Lee could see that it would need to be regularly attended and maintained.

This is when he obtained the New Masonic Lawn from the Natoma Lodge No. 64, which was struggling under financial constraints to maintain both its new and old cemetery sections. The transfer of ownership was made and Lee Miller formed Lakeside Memorial Lawn. Shortly thereafter, through his long association with Folsom Prison and his contract to undertake the deceased convicts, Lee persuaded a close associate and relative of the Cohn family to relinquish the Jewish Cemetery so that he could "better maintain it."

After much discussion, the family relented and quit claimed what they may have thought was their interest in the two lots comprising the Folsom Jewish Cemetery to Lee Miller. Miller, in turn,

incorporated the two lots into Lakeside Memorial Lawn, removed plot coping and ornamentation, and began to inter gentiles in the previously sacred, Jewish burial ground.

Miller sold Lakeside and the funeral home to Robert Claney, a long-time employee, in 1963. Thereafter, Claney proceeded to "upgrade" the cemetery to bring it more into conformance with the concept of the modern, "memorial lawn burial park." He bought the historic Odd Fellows Cemetery (unlawfully including Lots 4 through 8 of the public Citizen's Cemetery), and followed this purchase with obtaining the old, historic Masonic Cemetery.¹⁶

As part of Claney's program to enhance the cemetery grounds, he began to lay the historic tombstone and monuments in the ground, supposedly for ease of maintenance. Later, this appears not to have been enough to modernize the grounds, and he appears to have removed tons of granite, placerite and formed concrete coping pieces from inalienable family plots. In doing so, he was able to remove the original boundary lines of family burying space and was then able to utilize whatever "open" space seemed available. According to the former caretaker of 22 years employment, he removed for Claney all of the oldest and most unsightly plot outlinings and coping¹⁷, as well as tombstone bases and other ornamentation.¹⁸

Robert Claney transferred ownership of the funeral home and cemetery to his son, Loren, and his daughter in early to mid-1995. It was under the ownership of his offspring that the Department of Consumer Affairs investigation began, and which continued until both the cemetery and funeral home were sold in mid-1997 to SCI, Inc., a conglomerate corporation specializing in the funeral and cemetery industries.

For the first time since 1869, Miller Funeral Home is no longer run as a family enterprise. Since taking ownership of Lakeside Cemetery, SCI has increased the number of interments within the

¹⁶ The Masons built a new hall for their organization around 1870. It soon, thereafter, was a victim of fire and was burned to the ground. All of the association's records and documents were lost, including the record of deaths and burials at their cemetery. The first documented burial in the Masonic Cemetery was that of Capt. A.M. Hammond who was murdered at Smith's Flat, El Dorado County, and was brought to Folsom and buried by the Natoma Lodge of Masons.[Mountain Democrat, April 13, 1861, pg. 3, col. 1; Sacramento Daily Union, April 12, 1861, pg. 3, col. 2]. Therefore, the Masons had used their cemetery even before the formal purchase in 1862. By 1871, with the loss of the records by fire, any number of interments could have been made in the cemetery. There appears to have been no attempt by the Lodge to recreate their records in those early years:

¹⁷ Photographs on file at the Folsom History Museum show many "unsightly" grave sites situated at the rear (north) of the cemetery. One photo is believed to have been taken viewing east from the Jewish Cemetery and shows crude brick outlined plots and wooden markers.

¹⁸ Most of what the caretaker removed is located in an old ravine situated at approximately the north end of what was Sacramento Street (New Masonic Cemetery section) and extends easterly behind the Negro Bar (aka: Citizen's) Cemetery section. Since mid-1995, Lakeside maintenance workers have been filling in this ravine to conceal the materials hidden there. (Also found in this ravine in January of 1995, were pieces of granite coping.)

historic sections of the cemetery grounds, even though the records of interments which they received from the Claney family is known not to be accurate or anywhere near complete.

INTERMENT AND IDENTIFICATION LISTING (not included in 12/6/2002 update)

The attached listing was compiled using tombstone inventory lists previously published such as a 1935 Daughters of the American Revolution (DAR) list and Rest in Peace (1981, Bayless, et al.). Death notices, obituaries and articles about deaths taken from the Folsom Telegraph (1865 - 1943), Mountain Democrat (1855-1905, 1911-1913) and Sacramento Daily Union (1852-1868¹⁹) newspapers were abstracted from microfilm copies on file at the State Library.

The records of St. John the Baptist Catholic Church and Trinity Church were reviewed, as were El Dorado County Burial Permits. Abstracts of death certificates from Sacramento County deaths²⁰, a record of the Deaths and Interments In and From the City of Sacramento (provided by the Sacramento City Cemetery Archives), the New Masonic Cemetery²¹ Plot Sales records, and other miscellaneous documents derived from various other sources, have also been reviewed for this research.

Where necessary to identify familial relationships, the U.S. Census indexes and records for 1860, 1870 and 1880 have been referenced. A more complete record of families from census information would provide additional information for this study, as would the creation of genealogical family group records.

METHOD OF COMPILATION

The 1935 and 1981 tombstone inventory lists were first been combined alphabetically and, when obituary information has been available about these persons, it has been referenced and added. Newspaper source reference citations provide the newspaper title, the issue date, and the page and column numbers of where the reference was found.

Where the names from obituaries did not appear in the previously published listings, the information contained in the abstracted obituaries have been added to the list as separate entries. Some of these entries cite specific locations of interment as stated in the obituary notice, such as

¹⁹ As research continues, additional abstracts may be taken from later issues of the Sacramento Union.

²⁰ As abstracted by Dennis McCargar (1996-1998), Librarian at the California State Prison, Sacramento, for his study of the historic Folsom Prison Cemetery.

²¹ The New Masonic Cemetery purchased in 1916, but was not platted until 1924. A copy of the plat map was filed with the Sacramento County Recorder. A copy of the map, with some of the names associated with the grave plots is available through the Lodge Historian. The plot sales register is archived at the Folsom History Museum.

Citizen's, Cook's, Masonic, etc. The names provided by the obituary information that cite the location of burial and which cannot today be located at Lakeside, are indicative of the number of **known and unmarked graves** which exist at Lakeside Cemetery today.

Other entries represent those who died at or near Folsom and surrounding communities. These names have been added and are considered probable burials within one of the cemeteries at Folsom, other than St. John the Baptist Catholic Cemetery. Tombstone inventories for all known cemeteries of the area, have been referenced in order to eliminate the possibility of these burials having occurred elsewhere. The remains of the persons identified from these references, should be considered to **probably be in unmarked and unknown graves**.

The earliest marker (remaining) at Lakeside is dated 1850. Folsom Telegraph obituary information is not available on Folsom burials previous to 1865 as there are no copies available for referencing. Additionally, a total of 327 issues of the Telegraph are missing between the years 1865 and 1888. There is a great deal of information about Folsom burials which may never be known. From 1850 to 1865, and including the 327 missing issues, a total of 21 years of obituary information has not been recorded.

To supplement some of the missing Telegraph information, the Sacramento Transcript and the Sacramento Daily Union were referenced for the period of 1850 to 1868²². Still, it would be an intelligent presumption to believe that there are far more unmarked graves located within these historic cemetery sections than is reflected in this listing.

The previously unrecorded names on the list do not include names determined to have been buried at the Catholic, Eagles, Knights of Pythias, Mormon Island or Matthew Kilgore cemeteries, all of which are the closest burials places to Folsom. All names which may have had the appearance of being Irish, or closely related to other known Catholic recognized surnames (such as French, Portuguese, German, etc.), have not been included on this listing. However, it is recognized that not all Irish surnames are representative of the Catholic religion, though it would be difficult to differentiate between Irish Catholics and Irish Protestant names. There are a vast number of deaths of persons with Irish surnames which have been recorded in my previous research, but which have not been included in this study.²³

It is important to note that there were no "Unknown" graves recorded in either the 1935 or 1981 listings, nor were there estimates given of the number of unknown graves visible at those times. There are also no wooden headboards or common stone markers such as are found in nearly all pioneer cemeteries in surrounding communities. More telling is the lack of temporary metal

²² While it is entirely possible that additional editions of the Sacramento newspapers will yield further death and obituary information not found in the Folsom Telegraph editions, they have not at this time been referenced or abstracted.

²³ Refer to the report and study for the Historic St. John the Baptist Catholic Cemetery for abstracted deaths associated with suspected Catholic parishioners. It is possible that some of those included on St. John's listing were not Catholics and were buried at what is now Lakeside.

markers anywhere within the grounds. This is highly unusual, for many other pioneer cemeteries contain these temporary markers even though the name and death information has long since disappeared from the face plate.

It should be noted that addendum listings have been produced from the main listing for names of deceased known to be interred in a specific cemetery (by reference), so that there is a separate listing for unmarked/unknown graves within Citizen's Cemetery, Cook's Cemetery, Old Masonic, and Odd Fellows cemeteries. No attempt has been made to identify interments within the New Masonic Cemetery, with the exception of including the Masonic Association's plot sales records.

LISTING LEGEND:

A first listing which was compiled in 1995, and included color coding with a legend that identified what each colored entry represented in terms of possible interments, previously recorded tombstones no longer found in the cemetery, interments from obituaries which cited specific locations but for which no tombstones were located, tombstones dated 1900 and prior, tombstones located and found on previous inventories, etc. This was found to be too confusing to follow in a listing of this size. (The original listing and all updated listings have been archived for back referencing.)

The new legend has been designed for simplicity.

Bolded entries now represent known interments based on the recordation from previous tombstone listings or from obituary information found in newspaper accounts which cite an actual location of burial (ie. Citizen's, Cook's, etc.)

Underlined entries now represent probable or possible interments. In cemetery studies conducted in El Dorado County over the last 20 months (from the date of this printing), it has been found that where one died and there was a cemetery, one was buried in that local cemetery. If one was taken to another community for burial, the newspaper would almost always provide that information. Transporting the deceased to another locale was an unusual occurrence, especially in the earliest years, and we must therefore presume that those who died in and near Folsom were probably buried in the Folsom cemeteries.

THE UNKNOWN, UNMARKED AND MISSING GRAVES

In reviewing the compiled listing, it is clear to see that many of the burials which occurred at the Folsom cemeteries are now either unknown, unmarked or missing when compared with the previous tombstone listings and references cited. Of those who cannot be located within the grounds today are many family groups, as well as individuals who died and were buried here.

Only one grave representing Folsom's African American residents is marked in the cemetery and that is the grave of James H. "Honey" Cook. All of the other graves of African Americans noted to

be buried or suspected of being buried at this cemetery, are unmarked. Odd that at a cemetery which was first used by the "Negro" Bar mining camp, that none of the pioneer citizens of that race are able to be located in this cemetery.

It is estimated that between 20 and 30 family plots of coping materials were removed in 1996 from the State park land adjacent to the cemetery. With the "dismantling" of the family plots and the plot coping, a cemetery operator may seek to take advantage of "unused" plots within legally inalienable plot space. The motivation for removal of the early family plots may also have been to "re-use" grave sites for current burials, since in digging the new graves, only the equipment operator would know whether an old grave pre-existed.

Regardless of the motivation, one fact remains - there are a great number of old, historic graves which are unmarked within the cemetery which have already been, or are in jeopardy of being, buried over through current burials. This is not only illegal, but immoral. A society, upon discovering such activity, which would allow such activity to continue, evidences itself as having the most frayed of moral fabric. If we cannot protect the dead, those who lie in sacred peace, who can?

The following 61 family plots or grave sites have been identified as either missing, significantly altered by dismantling, or as being in jeopardy:

Adolph Alexander Family Plot

1 unmarked

The brother-in-law of Simon Cohn, Alexander's tombstone stands to the northwest of the Simon Cohn grave site in the Jewish Cemetery. In February of 1867, Alexander's daughter, Esther, died at the age of 18 months. Her grave cannot be located today. It is not known whether Mrs. Bertha Alexander, Adolph's wife, was buried here.

David Bennett Plot

2 unmarked

Update, 5/1/1998: According to Karen Rader (see Tyler J. Hill Plot), David Bennett was the second husband of Mrs. Lucy Ann Saunders, Marion Caroline Saunders Hill's mother. Both were buried in the Folsom Masonic cemetery. David Bennett was a pioneer of El Dorado County and was an early vintner of the area just east of Marble Valley in the vicinity of present day Cameron Park. During his ownership of that ranch, Bennett would have been a contemporary of Benjamin N. Bugbey of the Natoma Vineyard near Mormon Island east of Folsom. Bugbey also had vineyard land near Bennett's vineyard off of present day Durock [sic Duroc] Road in Cameron Park.

Charles Bishop Family Plot

3 unmarked

Two of the children of Charles and Sarah C. Bishop were buried in Folsom. According to Trinity Church records, daughter Edith, who died in January of 1868, and son Freddie, who died in September of 1868, were buried in the Odd Fellows Cemetery. Charles Bishop died in July 1876

and was brought to Folsom for burial. His interment would have been made with the graves of his deceased children in the Odd Fellows Cemetery.

Peter Bush Family Plot

1 unmarked

Located within what is now called the Cook's/American Legion Cemetery section, is a granite coped plot with the name "BUSH" chiseled into the front facing of the step which leads into the plot. Interred within this plot are members of the Fleckenstein family, who arrived in Folsom much later than Peter Bush, who died in February of 1889. According to the 1880 U.S. Census index, Bush, a 50 year old native of Germany, had a wife and five children. Only Peter's death notice has been located. His grave is unmarked at the cemetery today and his plot has been alienated²⁴ by the interment of the Fleckenstein family members.

Campbell Family Plot

4 unmarked

There is no evidence of the graves of the four members of this early African American family who are known to have been interred at this cemetery. The first of the family to die was Amelia Campbell, the 16 year old daughter of Squire Campbell, who died in August of 1895. The next member was Toy Campbell, the daughter of Squire Campbell, and the sister of "Sandford" Campbell, who died in February of 1904 and was buried in the "Citizen's Cemetery".²⁵

In March of 1907 came the death of little Sanford Edward Campbell, the seven month old son of Mr. and Mrs. Squire Campbell, who died March 11, 1907, his "interment being in the Citizen's Cemetery."²⁶ Sanford Campbell, the 33 year old son of Squire Campbell of Placerville, died at Sacramento in June of 1911, and the obituary stated the "funeral takes place in Citizen's Cemetery."²⁷ He was the brother of Toy Campbell.

None of the Campbell family member's graves are marked in the cemetery today. Whether, being African Americans, they may not have any grave markers originally, is not known. Squire Campbell died in El Dorado County and was buried in the Placerville City Cemetery.

Michael Carr Family Plot

No graves are believed to be unmarked or missing from this plot, if the plot is, indeed, where the marker is found today. However, the three names of the Carr family members, father Michael (died

²⁴ California Health and Safety Codes clearly state that family burial plots purchased and used by at least one member of the family are inalienable and may not be used or sold by any other parties.

²⁵ Folsom Telegraph, March 3, 1900, page 3, column 1.

²⁶ Folsom Telegraph, March 16, 1907, page 3, column 1.

²⁷ Folsom Telegraph, June 23, 1911, page 1, column 7.

1863), mother Ursula (died 1858), and daughter Mary Jane (died 1857), are all inscribed on the same marble tombstone. That tombstone sits adjacent to a tree and is thoroughly surrounded by more recent markers, many of which may be cremains burials. Regardless of the type of the more recent burials, the family plot became inalienable under California law. Whether the Carr graves are situated in this location is not really known as the marker could have been relocated from somewhere else in the cemetery. It must be noted, however, that in 1857 when Mary Jane died, there was but one cemetery, besides the Catholic cemetery, in use by the residents of Folsom and vicinity - the Negro Bar (aka: Citizen's) Cemetery. The tombstone is within what was called the Citizen's Cemetery.

Simon Cohn Family Plot

1 unmarked

The descendants of Simon Cohn, upon transferring the Jewish Cemetery to Lee Miller for use as Lakeside Cemetery ground, reluctantly granted Miller permission to remove the coping and ornamentation from the family plot²⁸. Miller had stated it would be easier to maintain the grounds if these items were removed. Since that time, the grave marker for Henrietta Cohn can no longer be found. (According to Henrietta Cohn Whelan, daughter of Philip C. Cohn, the family is curious as to what happened to this tombstone.) Included in the Cohn family plot is the grave of Senator Philip C. Cohn, the only Folsom resident in the 20th century to serve the state in that capacity.

James M. Cook Family Plot

1 unmarked

This man, one of Folsom's African American residents, also known as "Honey Cook", is buried in the southerly end of Lot 1 of Block 44, under a concrete topped grave with a military marker which has been laid flat and embedded in the concrete. This research revealed that Cook was hospitalized at the Veteran's Hospital in Boise, Idaho in 1942. It has not been identified when his death occurred. Cook's wife, Julia, who he once attempted to kill while living in Folsom, died in Folsom in 1936. Her grave is unmarked and is believed to be next to or near that of her husband²⁹.

William Cook Family Plot

2 unmarked

A large granite monument stands prominent within the William Cook family plot located in what is known as the Cook's/American Legion Cemetery section. The only name which appears on the monument is that of Ellen Cook, wife of William, who died in January of 1875. Of that family, William, the father and William E., the son, both died and were buried at Folsom. The senior Mr. Cook's obituary from June of 1905, states that he was buried in Cook's Cemetery³⁰. Son, William

²⁸ See description of the Cohn plot ornamentation and coping under Simon Cohn in the listing.

²⁹ This is not the only missing grave of an African American resident buried at Lakeside Cemetery.

³⁰ It should be noted that William and Ellen Cook's daughter, Maggie, married David Blower. Blower's name appears on the County Tax Assessor's maps as being the owner of Lots 14 through 16 of Block 44 sometime in the 1890's. Maggie's brother, Frank W. Cook, is the man from which the name "Cook's" cemetery originated.

E., was killed trying to board a moving train in Stockton in September of 1901. Both men are believed to be in unmarked graves within the William Cook family plot.

B.W. Cornell Family Plot

1-2 unmarked

The name "Brob. W. Cornell" is found on the 1935 DAR tombstone listing. Research which located the 1906 obituary of Mrs. A.M. Cornell revealed that she was the wife of B.W. Cornell, they having come to California in 1854. The couple is listed in the 1860 U.S. Census. There were no children living with them at the time. Mrs. Cornell's grave is not marked today. It has not yet been ascertained whether Mr. Cornell's grave remains marked as it was in 1935. [SS note, 5/11/2002: on Peter Kroff's 1999 inventory.]

Jesse Couch Family Plot

3 unmarked

Only the tombstone of little Margaret Jane Couch, who died in April of 1861, at the age of 9 months, can be located at Lakeside today. Couch's wife, Margaret, and two other children also died while he lived in the area³¹. The graves of wife Margaret, son B.S. Love, and daughter, Jessie Jane, are not found. The area surrounding baby Margaret Jane's grave has been extensively used in later times. Jesse Couch once served as the county supervisor of the district in which Granite Township was located.

Crawford/Tuck/Rightmeyer Family Plot

1 unmarked

As featured in the book *Folsom Fables ~ Pieces of the Past* (Sue Silver, 1995), in the story titled "A Widowed Woman," the graves of the first two husbands of Mary Tuck Rightmeyer Crawford, are located in the Odd Fellows cemetery, just east of the Little Family plot. Samuel Tuck, Mary's first husband, died in March of 1867 and she provided him with a beautiful marble tombstone in the Odd Fellows Cemetery. In October of 1889, Mary buried her second husband, Henry Rightmeyer, a woodchopper for the railroad company. She had Henry's name and dates inscribed into the same tombstone as Samuel's, and the two men now lay side by side in the cemetery. Mary died in February of 1898. Her obituary states that she was buried in the Odd Fellows Cemetery, though there is no marker there today. She is believed to be buried next to her first two husbands. A date of death or burial location for her third husband, Alphonse Crawford, is unknown.

E.L. Currier Family Plot

3 unmarked

Located toward the rear of the Cook's/American Legion Cemetery section, is the tombstone of Addie S. Currier, who died in May of 1880. The tombstone has been laid flat in the ground and is in the shape of an upright obelisk. A photograph taken of the Windmill Family plot around 1889, shows this tombstone standing erect, behind and to the northwest of the Windmill plot. The

³¹ Jesse Couch was one of Folsom's earliest pioneer residents. For a time, however, he did live at Latrobe, El Dorado County, after the extension of the railroad to that town. In his life in the area, Couch once served as a Supervisor for Sacramento County.

grave plot is surrounded by picket fencing and is estimated to have been approximately 10 feet by 20 feet in size.

E.L. Currier, the patriarch of the family, is known to have been a produce grower who was quite admired in Folsom. He is believed to have died between February 1887 and February of 1892. The last mention of him in the newspaper appeared in the former month and year and the latter occurred at the time his wife, Arvilla, died a widow in 1892. Their son, Ralph Currier, sometimes referred to as "Pet" in newspaper references, died in May of 1936 and his "interment was in the family plot in Cook's Cemetery here."³² Ralph served as the Agricultural Commissioner for the Granite Township district of Sacramento County in the early 20th century.

Today there are markers for many people who are unrelated to this family on every side of Addie's tombstone. It is not known if these later graves were buried over the missing Currier family members³³, or if these later markers were simply moved to this location. Since many are dated during the remainder of Ralph's lifetime, it is certain that no burials occurred in the family plot while he lived.

George Custer Family Plot

2 unmarked

The graves of George and Annie Custer cannot be located at Lakeside Cemetery. George died in 1895, a native of Pennsylvania and a 40 year resident of the area. Wife, Annie, died in 1896 and was buried in the "Folsom City Cemetery."³⁴ Understanding the history of the cemeteries now included in what is Lakeside, this could be either the Negro Bar (aka: Citizen's) Cemetery or what is now called the Cook's/American Legion Cemetery.

H.M. and B.J. Dains Family Plots

9 missing and unmarked

The elder patriarch of these two pioneer families was Henry M. Dains, an early pioneer of Georgetown in El Dorado County. His son, Benjamin J. Dains, and his family were also buried at this cemetery. There is no trace whatever of any member of the Dains family graves at Lakeside today.

Among the known interred of the two families are: Benjamin Dains (died 1929) buried in the "Masonic" cemetery at Folsom; Clarence G. Dains (died 1913), son of B.J. Dains, who was buried at "Cook's Cemetery"; Elton Dains (died 1902), son of Benjamin Dains, who was buried in the Odd Fellows cemetery; Elwood Dains (died 1908), son of B.J. Dains, who was buried in the Odd Fellows cemetery; Henry Montgomery Dains (died 1900), husband of Susan and father of B.J. Dains, who was buried in the Odd Fellows cemetery; Maggie (nee Robson) Dains (died 1903), wife

³² Folsom Telegraph, May 22, 1936, page 2, column 3.

³³ Since Ralph died in 1936, it is difficult to believe that the family plot could have been dismantled before the time of his death, since he was fully aware of his own family's burials.

³⁴ Folsom Telegraph, October 3, 1896, page 3, column 2.

of B.J. Dains, who was buried in the Odd Fellows cemetery; Susan Dains (died 1907), wife of H.M. Dains, whose "remains were brought to Folsom for interment at the Odd Fellows cemetery"³⁵; and Almira Dains (died 1897), the sister of H.M. Dains, whose obituary did not state which section of the cemetery she was interred.

In the mid-1880s, H.M. Dains served as the undertaker for the people of Georgetown. As part of his endeavors he also became the first monument dealer in that county to offer Zinc grave monuments. The Zinc monuments were introduced and highly touted as lasting longer than many other materials used for such purposes. Unfortunately for Dains, this new product does not appear to have been very successful.

SS Note, 5/11/2002: Also among the missing is Sabria or Sabrina Dains Johnson, sister of H.M. Dains, who died in 1897.

Michael Doll Family Plot

1 unmarked

The tombstone of Michael Doll (died 1886) also bears the name Phillip Henneman (died 1879) and is located in the Odd Fellows Cemetery section. The grave of Michael's widow, Emma Louise Doll (died 1920; formerly Henneman), is unmarked today, and there are many recent interments which have been made all around the Doll/Henneman monument. Phillip Henneman was Emma's first husband with whom she bore their daughter, Elizabeth (Lizzie).

Dresser Family Plot

4 unmarked

This missing and unmarked plot contains the graves of William W. Dresser and his family, and the grave of his brother, Jesse Dresser. Early pioneer citizens of Folsom and vicinity (including Prairie City), none of the known Dresser graves can be located today. Among the family who died and were, or are believed to be, buried in the cemetery are: Edmond R. Dresser (died 1867), son of William W. and Mary J. Dresser; Jesse Dresser (died 1874), brother of William Dresser, who the Trinity Church records state was buried in the Masonic Cemetery; Sherman Dresser (died 1906), the son of William and Mary Dresser, who died at Auburn, Placer County, and was "buried in the family plot in the Masonic Cemetery" at Folsom³⁶; William W. Dresser (died 1870), father of Sherman; Edmond, brother of Jesse, who died in Folsom at the age of 52 years; and Rebecca, aged 16 months and 13 days, the daughter of W.W. Dresser, who died at Folsom on December 16, 1869³⁷

³⁵ Folsom Telegraph, February 16, 1907, page 3, column 4.

³⁶ Folsom Telegraph June 20, 1906, page 3, column 4.

³⁷ Sacramento Union, 12/20/1869; page 2, column 4. 1870 U.S. Census Mortality Schedule for Granite Township, Sacramento County.

Daniel Eisner Family Plot

1 unmarked

This family plot is located in the Jewish Cemetery just behind the Jacob Hyman family plot. Today only the tombstone of little Sophia Eisner (died 1862), daughter of Daniel and M. Eisner, is found. The tombstone of her brother, Jacob (aged 12 years in 1860, date of death unknown), was vandalized in August of 1891 when it was overturned and broken. It is no longer in the cemetery in any condition. Next to Sophia's tombstone is an empty space which may represent Jacob's grave. More recent burials have occurred all around Sophia's stone. The 1860 U.S. Census lists the Eisner family as being parents Daniel and "M." with five sons - Jacob, David, Charles, Marcus, and Milton. No trace of the family was found in the U.S. Census for Sacramento County in either 1870 or 1880. This may indicate that son, Jacob, died previous to 1870. It is not known if any of the other Eisner sons may have died and been buried in the Jewish Cemetery with their sister and brother. (Daniel Eisner's eldest son went on to become one of the first regents of the University of California at Berkeley.)

W.H. Farmer Family Plot

2* unmarked

Located next to a tree, behind the Thad McFarland family plot in the Masonic Cemetery, is the tombstone of Margaret F. Farmer (died 1869), the wife of William H. Farmer. William Farmer died in 1884 at Oakland and his "remains were brought to Folsom" on the evening of November 28, 1884, for interment. He was buried in the Masonic Cemetery³⁸. William's grave cannot be located today.

* Between the U.S. Census of 1870 and the U.S. Census of 1880, baby Margaret Farmer (11 months old in 1870) seems to have become absent from the family. It may be that she also died and was buried in the Masonic Cemetery next to her mother, or possibly in the same grave with her mother.

Joseph Fiel Family Plot

5 missing and unmarked

Originally located near the Levy family plot in the Jewish Cemetery, the entire Fiel family plot is missing today. Known to be interred in the plot were: Jewish pioneer and family patriarch, Joseph Fiel (died 1876); his wife, Rosa (nee Kirsky) Fiel (died 1901); son, Israel³⁹ (died 1867); son, Isaac (died 1916), who was buried in the Jewish Cemetery; Isaac's wife, Gertrude (nee Sartain) Fiel, who died in December of 1899 of injuries sustained in a fall from a two story window in Oakland and was brought to Folsom and buried in the Jewish Cemetery.

According to Professor Austin Wahrhaftig, the son of Irma Levy Wahrhaftig, his mother used to tend the graves of her relatives, the Fiel family, when he was a child. His recollection was that the Fiel plot was near the Levy plot toward the outside edge of the cemetery. His aunt, Lotta Levy,

³⁸ Folsom Telegraph, November 29, 1884, page 3, column 5, and December 6, 1884, page 3, column 4.

³⁹ Israel has also been referred to as "Irwin" in some history accounts of Folsom. According to his death notice, however, his name was Israel, not Irwin.

later had her gardener tend to the graves in this plot. Professor Wahrhaftig only remembers wooden markers on these graves. (Whether the "outside edge" of the Jewish cemetery means to the west, south or north, was not ascertained.)

In the vandalism which occurred at the Jewish and Odd Fellows cemeteries in 1891, two tombstones of the Fiel family were broken off, indicating that at that time, both burials to that time had been marked with permanent markers. Whether the markers were totally destroyed and never replaced, is not known. The surviving family, though, is known to have been financially capable of paying for their replacement.

David Finch Family Plot

3 unmarked

There are two David Finch family plots. One is located near the rear of the Negro Bar (aka: Citizen's) Cemetery and contains the grave of David's first wife, Cyrilla, and that of his mother, Frances A. Finch, which is not marked today. David's father(?) William (died 1882), and Willie E. Finch (died 1877), possibly the son of David and Cyrilla, are also located in this section.

The second family plot, located on the western end and to the front of the Odd Fellows Cemetery, has had it's surname marker and coping removed. Today all that remains of the plot are markers bearing the given names of the deceased and vital statistics dates. Without study or prior knowledge of this family, one would be unable to determine their last name.

Of the known deceased Finch family members, the following graves cannot be located today: Daisy Belle (died 1896), the 20-year old daughter of David Finch and Lucille Finch (died 1900), the infant daughter of David Finch, who was "laid to rest (yesterday) in the Odd Fellows Cemetery."⁴⁰ The tombstone of Henry Finch (died 1889), son of David Finch, over whose grave J.L. House erected a "pretty little tombstone....in the Odd Fellows Cemetery"⁴¹ in November of 1889, is now located near the rear of the Negro Bar (aka: Citizen's) Cemetery.

David Finch was the Superintendent of the Natoma Company Vineyard before retiring to Verona, California. Because of his stature in the community, his family plot would have been handsomely arrayed.

J.W. Gains Family Plot

1 unmarked

Located in the Masonic Cemetery, only the tombstone of John William Gains (died 1885) remains in place. His wife, Mary Ann (nee Scurlock) Gains (died 1895) is noted as having a tombstone by the 1935 DAR tombstone listing. DAR noted that the marker was inscribed with "Neylaid South Wales." Mary Ann's name may be inscribed on the underside of the stone which now bears John's name only. The Gains' had two daughters who survived childhood. One became Mrs. Minor Miller and is interred at the Mormon Island Relocated Cemetery in El Dorado Hills. The other,

⁴⁰ Folsom Telegraph, April 21, 1900, page 3, column 1.

⁴¹ Folsom Telegraph, November 30, 1889, page 3, column 3.

Elizabeth Beatrice, died sometime around 1914 and may be buried with her parents or may be one of the "Unknown" graves relocated to the Mormon Island Relocated Cemetery by the U.S. Army Corp of Engineers in 1954 for the construction of Folsom Dam and Lake.

R.H. Gegax Family Plot

1 unmarked

The children of Folsom building contractor Richard H. Gegax were buried in this cemetery. The concrete markers for Charlie, Emma and two unnamed infant sons, are located in the Odd Fellows Cemetery. Absent is a marker for Anna Elizabeth (died 1891), who died of diphtheria in April of that year. Records from the O'Keefe Funeral Home in Placerville, indicate the death and burial of Arthur F. Gegax, the son of R.H. Gegax in August of 1893. Those records indicate that Arthur was buried in "Johnson's Cemetery" in El Dorado County. Johnson's Cemetery is located off North Canyon and Cleese roads below Camino. With other of his children buried at Folsom, is it likely he would have had Arthur buried in such an out of the way cemetery?

Edward Hicks Family Plot

5 unmarked

There is not one tombstone or marker for any of the members of this pioneer African American family known to have been buried at this cemetery. While patriarch, Edward Hicks, is known to have been buried in the National Cemetery at the Presidio in San Francisco, the graves of the members of his family who were buried at the Folsom cemetery cannot be located today.

Among them are: Alice Hicks (died 1915), daughter of Mrs. George Hicks, granddaughter of Edward Hicks; Garfield Hicks (died 1935), son of Mr. and Mrs. Edward Hicks, who grew up in Folsom; George Hicks (died 1940), the son of Mr. and Mrs. Edward Hicks, who was interred in the "Citizen's Cemetery"⁴²; Mrs. E. (Eliza) Hicks (died 1901), wife of Edward Hicks, and Mrs. Isabel Hicks (died 1933), the wife of George B. Hicks, and daughter-in-law of Edward Hicks, mother of the late Alice Hicks, was a "member of a former Folsom colored family"⁴³, and buried in the Citizen's Cemetery.

Tyler J. Hill Family Plot

3 missing and unmarked

Pioneer Folsom blacksmith and hotel keeper, Tyler Joseph Hill (died 1875), was a member of the Masonic fraternity and is believed to have been buried in the Masonic Cemetery. His daughter, Ada Marian (died 1874) was buried in the Masonic Cemetery according to Trinity Church records. The tombstones of T.J. Hill, Ada Marian Hill, and Mrs. Marion Caroline Hill (died 1922), Tyler's wife, were recorded by the 1935 DAR tombstone inventory. All three graves are unmarked and their location unknown today. T.J. Hill served as the Constable for the town of Folsom and later served as a Deputy Sheriff of Sacramento County.

⁴² Folsom Telegraph, March 1, 1940, page 1, column 6.

⁴³ Folsom Telegraph, April 14, 1933, page 1, column 7.

Update, 5/1/1998: Met with Karen Rader, a family descendant, who has visited the graves since she was a young girl. She has not been back for several years, since about the time she got married in 1975. In addition to the three Hill family members mentioned above, the son of Tyler and Marion, Charles Hill, and his wife, Augusta, were also buried at Lakeside.

A second meeting with Karen, accompanied by her mother, Marion Rader, was held at the cemetery. Mrs. Rader was unable to locate the Tyler Hill plot and stated that the last time she visited the plot with her mother (who has since deceased) in the mid-1960's, the wooden markers which had always been in place at the graves, were still at the cemetery. Despite the many drastic changes to the cemetery, including the removal of the fencing along the present driveway/parking area, Mrs. Rader was able to show me the location of where the Hill graves should be.

J.L. Holmes Family Plot

3 unmarked

John L. Holmes was a painter when he lived in Folsom around the turn of the century. The grave of his wife, Cora, who died in Folsom at the age of 23 (probably of complications of childbirth) and was buried in the Citizen's Cemetery, is unmarked today. The couple's infant daughter, Lizzie, died in June 1905 at the age of 2 months 9 days old. According to the records of the Sacramento City Cemetery Archives, Lizzie was buried in Folsom. John died in Sacramento in 1910 and is believed to have been brought to Folsom and buried next to his wife. Mrs. Holmes assisted Constable Donnelly by identifying that some of the convicts who violently escaped Folsom Prison in July of 1903 had passed by her home on the way to Sacramento. Her observation led to the capture of two of the convicts in that city just a few days later.

John Holmes may have been an African American citizen or possibly a mulatto.

William Jarvis Family Plot

No one is believed to be missing or unmarked in the William Jarvis plot. However, the removal of the ornate iron fencing and granite coping which once outlined this plot is nearly as criminal as would be the loss of their graves. Jarvis and his wife, Rebecca Russell Vanlandingham, were early pioneers of both El Dorado and Sacramento County. In the mid-1850s they were the owners and proprietors of the Natoma Valley House, a road house on the Sacramento to Coloma Road, just west of the El Dorado and Sacramento county line. They also owned what was called the Valley Home, another property located on the Sacramento Road in El Dorado county, approximately one mile east of the Natoma Valley House. The Valley Home appears to have been the Jarvis' residence at the time.

A trustee of the Natoma Water and Mining Company from its very earliest years, Jarvis alternated residency between Natoma, Folsom and Sacramento. He is also believed to have lived in San Francisco, probably after the Company's headquarters were moved to that city. The photograph of the Windmiller family plot shows the Jarvis plot behind and diagonally to the right of that family plot. Today the joint headstone of William and Rebecca Vanlandingham Russell Jarvis, his wife, has been laid flat in the ground, its layered granite bases are nowhere to be found.

W.D. Johnson Family Plot

3 unmarked

Only the grave of Charles Edward Johnson (died 1885), the son of W.D. and Philenia Johnson, is marked in the cemetery today. His monument, presumably located in the Odd Fellows Cemetery, was vandalized in 1891 and was "smashed to pieces."⁴⁴ [SS Note, 5/11/2002: Peter Kroff's inventory **does not list** Charles Edward Johnson.] Another son of William and Philenia, Emmett Oliver, committed suicide in 1917 and was buried at the Odd Fellows Cemetery. He was a six year Army veteran who had enlisted in Portland, Oregon. William D. Johnson (died 1920), aged 64 years and a native of California, was also buried in the Odd Fellows Cemetery, his grave bearing no marker today. Mrs. Philenia Johnson died at Sacramento in 1925 at age 66 years, and was buried in the Odd Fellows Cemetery at Folsom. Her grave is also unmarked.

Zachariah Jones Family Plot

3 unmarked

Three of the members of this family, patriarch Zachariah, and his twin infant children, are buried in unmarked graves at this cemetery. The twins, who were born and died on October 18, 1867, were the infants of "Z. and H.A." Jones. The 1870 U.S. Census lists a Harriet Jones as head of her household in Granite Township. According to the records of Trinity Church, Zachariah Jones (died 1868), was buried in the Citizen's Cemetery on December 4, 1868.

H.T. Knight Family Plot

While the grave of Henry T. Knight supposedly remains marked by his tombstone, there is evidence from the investigative excavation that his granite coped plot was dismantled and the ornamentation dumped onto the adjacent State park land. Nathaniel Knight, the father of Henry T., John (buried Mormon Island Relocated Cemetery), and Thomas Knight (buried El Dorado Cemetery, El Dorado county), died at Folsom in 1871. He was a native of Vermont, as were Henry, John and Thomas.

The location of Henry T. Knight's tombstone to others known to have had a familial relationship with the Knight family, brings to question whether their graves were also enclosed in the "Knight Family Plot." There were pieces of coping ranging up to 20 feet in length that were removed during the 1996 excavation. This may have been long enough to have also enclosed the grave sites of F.A. Blanchard and family, Elvira Bradner, and W.H. Comstock and family, who were all inter-related by marriage to the Knight's.

Knight/Shumway Family Plot

Located at the western edge near the front of the Odd Fellows Cemetery, the Knight/Shumway family plot represents the burials of members of the Knight family who inter-married with the Shumway family. While no missing graves have been noted, the military tombstone of K. Loren Knight has been laid flat in the ground. Because of this, it is slowly being destroyed by the equipment which runs over it during maintenance activities.

⁴⁴ Folsom Telegraph, August 22, 1891, "A Ghoul's Work", page 3, column 3, in which the Jewish and Odd Fellows cemeteries were vandalized.

Samuel Kyburz Family Plot

2 unmarked

The graves of Samuel Kyburz (died 1898) and his wife, Rebecca (died 1903), are unmarked in the Old Masonic cemetery today. Each of their obituaries stated they were buried in the Masonic cemetery, though no markers can be found. Some of their children and grandchildren's graves are found in the New Masonic Cemetery.

Samuel and Rebecca were early California pioneers, and are known to have operated a hotel at Sutter's Fort at the advent of the gold rush. Samuel was one of the men to first discover the Coloma Valley where James Marshall and Capt. John Sutter built their sawmill, where shortly after Marshall discovered gold in the mill's tailrace.

Edward R. Levy Family Plot

1 unmarked

Only one Levy grave is believed to have been unmarked in the Jewish Cemetery. This was the grave of the infant son of Edward Levy and wife, who died in July 1870, three days after his birth. He was the only son born to the couple. The remainder of the family plot has been significantly altered by the removal of coping (material unknown) and the replacement of placerite tombstones with more modern flat markers. The headstones, originally facing east, have been turned so that they face the drive straight on. The graves would have been east facing at an angle to the present day markers. There are many, many more recent burials which have occurred all around this two-row family plot and a likelihood that some of the Levy graves have been disturbed by the more recent interments.

O.C. Lewis Family Plot

There are no burials believed unmarked or missing from this plot. However, there are scratches to the marble obelisk monument which bears the names of the four Lewis family members buried here. It is believed the granite base which once adorned this monument was removed (hence the scratches on the marble obelisk), and dumped into the State park lands. Surrounding these graves which date from 1879 to 1954, are more recent burials and markers.

George Little Family Plot

3 unmarked

The Little family plot is located near the east edge and at the southern front of the Odd Fellows Cemetery. It is coped and concrete capped, and bears the name "Little" on the face of the step. In the mid to late-1970's, the operators of Lakeside Cemetery erected three cremains niches around west, north and east sides of this plot. During his 1992 tour of the grounds with members of the Folsom Historical Society, the former caretaker stated that there were no burials in the Little plot. Evidence bears otherwise.

Ashland pioneer George Little raised his family on his ranch across the river from Folsom. His wife, Elizabeth (died 1895), was a native of Scotland and a resident of the Folsom vicinity from as early as 1851. George Little's obituary information (died 1908) placed him in San Francisco in 1851, and at Negro Bar in 1852. He was a member of the Granite Lodge No. 62 IOOF. One of the

couples' sons, William (died 1930), died there at the age of 77 years, having lived at Ashland since 1853. He was buried in the Odd Fellows Cemetery. It is not known what may have happened to the other son, John Little, William's younger brother.

The placement of the cremains niches around this family plot is at best rude, since people visiting the cremated remains of their loved ones there, are now walking on top of the Little family member's graves. Under Californian law, the plot purchased by George Little garnered him interment rights and rights to erect an appropriate monument on the surface of the graves. The erection of the cremains niches, under California law, would therefore violate the contract between Little and the Odd Fellows. Every cremated remains niche sold atop the Little plot is tantamount to double selling (or more) the interment rights for that space in the cemetery.

Walter Lovelady Family Plot

According to Dorothy Rollins of Stockton, the granddaughter of Walter Lovelady, this plot was once concrete capped and bore a black granite marker with four names inscribed. Three of the names represent actual burials; the fourth name being for memorial purposes only. In 1980, Mrs. Rollins visited the cemetery and was unable to locate her family's graves. When she contacted the cemetery operator she was told that the concrete was removed because it had become cracked and dangerous and that the marker was placed directly onto the ground and was stolen from the cemetery sometime thereafter. Granite weighs 175 pounds per cubic foot. Mrs. Rollins estimated the marker to have been approximately 2 feet high by 4 feet wide - a rather large marker. The weight of such a marker would prohibit its having been taken from the cemetery without benefit of equipment such as a tractor or backhoe. The grave site, according to the cemetery operator, is registered in their interment book. [SS Note, 5/11/2002: Peter Kroff's inventory lists Regina Lovelady b. 5/3/1962, d. 5/19/1962; don't know if related.]

Isaac Maltby Family Plot

1 unmarked

Located near the rear of Lot 14 in Block 44 (Cook's/American Legion Cemetery), the family plot of Isaac Maltby (died 1887) was originally used for the burial of little Elizabeth Maltby, the couple's infant daughter who died in 1859. (In 1859, this portion of the cemetery, Lots 14-16 of Block 44, was owned by William Murray.) After Isaac Maltby committed suicide in 1887, his probate case file in El Dorado County indicates that the family purchased a "grave yard lot" for \$25.00 as shown on the bill from Jacob Miller, undertaker. The price for a grave at most other cemeteries studied during that time ranged from \$2.00 per grave to \$75.00 per lot, depending on the lot size. A "grave yard lot" for \$25.00 in 1887, therefore, might represent a six person burial lot.

In a photograph taken by Mary Bowen, and printed in the Folsom Telegraph in the mid-1960's, the tombstone of little Elizabeth is shown upright and to the east of a larger, three-tiered monument. To the right of Elizabeth's marker is what appears to be concrete plot lining. Today the large monument is no where to be found, and to Elizabeth's right, is a marble obelisk monument bearing only Isaac's name. Eveline Maltby, Isaac's wife, died in 1899 and was buried next to her husband and child. There is no grave stone for Eveline at the cemetery today and it is believed that the large monument shown in Bowen's photograph represented both Eveline and Isaac, and that the obelisk

marker now bearing only Isaac's name probably bears Eveline's name on one of the unexposed sides. More recent burials are encroaching on this "grave yard lot" which has so obviously been dismantled.

SS Update, July 1998: On a recent visit to the Chung Wah Cemetery, adjacent to Lakeside, I visited several of the tombstones I believed bear additional names than those which are presently showing. Using a piece of twig, I scraped the mud pack on the sides of a few, one of which was the Isaac Maltby marker. On the west side of the marker, I found the letters "BY" could be found. This is where Eveline's name is going to be found.

J.K. McComber Family Plot

This plot is located at the rear of the Masonic Cemetery and contains the graves of seven and possibly eight of the John McComber family. One unknown grave, that of little Marion McComber (died 1907), the infant daughter of Fred McComber, may exist within the plot. It appears that the plot coping has been removed based on the lack of dressed granite corner pieces on the western edge of the adjacent coped plot. Ordinarily, corner pieces of stand-alone plots which are coped, bear finished or dressed corner pieces with smooth surfaces which provide a nice visual effect. The present corner pieces on the plot to the immediate east of the McComber plot shows evidence that other pieces of granite originally abutted it. Further, the upright marble obelisk bearing the names of John and his wife, Mattie, has been removed from its granite base and placed directly on the ground. More recent burials appear to have encroached within this inalienable plot and may have impacted the older graves.

Dugand McMillan Family Plot

3 missing and unmarked

Little historically is known of this family. The first death recorded is that of George McMillan (died 1900), the 15 year old son of "Mr. and Mrs. Dougall McMillan", whose "remains were interred in the City Cemetery near Folsom."⁴⁵ Daughter Emma (died 1901), whose death resulted from a pine nut lodged in her throat, was "buried at Folsom." The family patriarch, Dugand McMillan (died 1912), a native of Scotland and 65 years old at the time of his death, was interred in "Cook's Cemetery"⁴⁶. Mr. McMillan left a wife and several children. None of the graves of this family can be located today.

J.E. Moon Family Plot

2 unmarked

In 1935, the DAR tombstone listing recorded the markers of infant Edna Lorraine Moon (died 1916) and her brother, infant Kack Moon, the children of J.E. and Hazel Moon. The 1981 listing produced in Rest In Peace also picked up the marker of Edna Lorraine, but not that of Kack. Neither of these markers was located in 1995, when a brief inventory was taken at the onset of this

⁴⁵ Folsom Telegraph, January 13, 1900, page 3, column 1.

⁴⁶ Folsom Telegraph, February 23, 1912, page 1, column 7.

research. No location of the parents graves was noted and they will have to be relocated upon additional inventorying.

SS Note, 5/11/2002: Peter Kroff's 1999 inventory lists Edna Lorraine Moon (d. 1916), Hazel M. Moon (d. 1958), J. Edward Moon (d. 1958) and "Jack" Moon (d. 1911).

Peter Morris Family Plot

2 unmarked

The British born Morris family came to California in 1849, landing in San Francisco. By the time of the Cholera epidemic of 1850, at least one of the sons was living in Sacramento. They eventually settled near Alder Creek and Mississippi Bar and remained there for the rest of their lives. Only their daughter, Louisa, who married Don Juan M. Winder and died at Lima, Peru in 1868, left the family fold.

Patriarch, Peter Morris (died 1882), had been married to wife, Eliza, for 55 years at the time of his death. Eliza (died 1888), was 81 years old at the time of her death. Their sons, Richard and Robert, both died in 1894, Richard being the first of the two to de cease in March of that year. He was "buried near his father, mother and sister"⁴⁷, which would have been near the rear of Lot 16 in Block 44 (Cook's Cemetery), where the headstones of Peter and Eliza are found today. Robert Morris, died in November of that year and was interred in the Masonic Cemetery⁴⁸. Neither Richard nor Robert's graves are marked today.

It is uncertain whether sister, Louisa Winder, was returned to California from Peru for burial. At the time, embalming compounds utilized arsenic, which arrested decomposition and allowed for long distance transport of human remains. Certainly, if the body had been returned for interment at Folsom, one of the local newspapers would have found space to mention it. This becomes a question of whether Richard's obituary, which stated he was buried near his father, mother, and sister, was speaking of Louisa or another sister, possibly unknown to us.

Only one article has been noted linking any other female offspring to the Morris family and that mention stated that Mrs. Morris had come to town with her daughter, Mrs. Lewis. The only Mrs. Lewis this researcher is aware of living in Folsom and vicinity was Mrs. Margaret Lewis. She, however, was a native of New York, while all the other Morris family members were natives of England.

Musso Family Plot

Two graves of Musso family members exist at the cemetery and are located within the Odd Fellows section. The grave of mother, Annie M. Musso (died 1862), wife of Laurence Musso, is marked with a large marble tombstone which has been laid flat in the ground. The grave of her son, Frank (Francis) H. Musso (died 1870), who was 12 years old at the time of his death, is located within the

⁴⁷ Folsom Telegraph, March 10, 1894, page 2, column 4.

⁴⁸ Folsom Telegraph November 10, 1894, page 2, column 1.

Peter Imhof family plot, some two grave sites west of that of his mother. According to the records of Trinity Church, Francis Musso was interred in the Odd Fellows Cemetery. It is puzzling that Frank's tombstone is not located next to his mother's and that no familial relationship has been located linking the Imhof(f)'s and the Musso's. It is far more likely that Frank's tombstone has been removed from its original location and deliberately placed inside the coped Imhof plot. [SS Note, 5/11/2002: Peter Kroff's inventory does not appear to have included the Imhof plot, and he does not list Frank H. Musso.]

William H. Nichols Family Plot

There are no known missing or unmarked graves within this plot. However, according to family descendants, this concrete topped plot was once adorned with granite coping and iron railing and chain all around it. That ornamentation is missing today revealing that portions of this plot have been dismantled and without permission from the family.

William Nutter Family Plot

The marker of the grave of Elsie Olive Nutter (died 1928) was inventoried for the first time in 1995. A death notice for her husband, William Nutter (died 1936), was found in the pages of the Folsom Telegraph. The 1995 inventory did not note a marker for William, but it is uncertain if one does not exist or if, because the inventory only sought to identify markers through 1930, his was deliberately not noted. The obituary stated that he was laid to rest beside his late wife in the Citizen's Cemetery. [SS Note, 5/11/2002: Peter Kroff's 1999 inventory **does not include** William Nutter.]

John Odell Family Plot

This concrete capped plot is located in the Masonic Cemetery. The tombstone which bore the names of John Sr. (died 1881), John Jr. (died 1901) and wife and mother, Mrs. R.S. (nee Berry) Odell Freese, was found in the Oleander bushes behind the maintenance sheds in 1994. The front of the plot is marked "ODELL" and there are two small marble plates bearing the initials R.S. and J.A. embedded in the concrete topping, presumably marking the location of the graves of John Sr. and his wife. A concrete monument base is also embedded in the topping and remains in place.

After complaint about the tombstones being found in the bushes, the Odell monument was moved back into the cemetery but was placed near the tombstones of Rosalie and Camille Lamblett, behind the Benjamin F. Bates plot under the Cedar of Lebanon trees.⁴⁹

Thomas Orr Sr. Family Plot

2 unmarked

California pioneer Thomas Orr and his family, travelled to the new state with a contingent of Mormon immigrants in 1848. The family initially settled in the Salmon Falls area of El Dorado

⁴⁹ At the same time, the monument of John Woods was also placed in this location.

County, but was well known in Folsom and its surrounding communities. The grave of Catherine Orr (died 1879), the wife of Thomas, is located near the front of Lot 15 of Block 44 (Cook's Cemetery). This large, once ornate marble tombstone has been removed from its granite base⁵⁰ and laid flat in the ground. In July of 1891, the Orr's son, James, died at Rolling Hill (near present day El Dorado Hills at Salmon Falls Road) and was buried in the "Folsom Cemetery"⁵¹. Presumably he would have been buried near his mother's grave.

Family patriarch and one of the "oldest pioneer inhabitants of the county"⁵² (El Dorado), Thomas Orr died at Shingle Springs in 1893 at an advanced age and was buried at Folsom⁵³. There is no marker at the cemetery today to indicate that Thomas' remains were interred next to those of his wife. Current interments are coming dangerously close to this location which is believed to contain the two unmarked, east facing graves of James and Thomas Orr.

Hiram Parker Family Plot

A resident near the Fifteen Mile House on the old Placerville Road, Hiram Parker (died 1885), a thirty year resident of the vicinity, and was buried "in the Odd Fellows plot in the Citizen's Cemetery at this place"⁵⁴. Parker's wife, Marie, who remarried after his death and became Marie Hall (died 1901), was buried beside "her first husband in the Odd Fellows Cemetery"⁵⁵.

The blue and gray veined marble obelisk monument (supposedly) marking Parker's grave bears chain marks and scratches, and is located near the front of the Odd Fellows Cemetery (no where near what could be considered within the "Citizen's Cemetery"), next to the coped "Patterson" plot. It appears to have been moved in toward the abutted corner of that plot and the one directly behind the Patterson plot.

A video tape of this plot taken in 1990 by historian Armand Kimball (now deceased), shows a small pine tree growing in the location of where the Parker monument is now found. There is no sign of the Parker monument behind the pine tree or anywhere near the location where it is today. It is highly questionable whether the Parker graves are where the monument is today or whether the monument has been moved from another location in the cemetery.

⁵⁰ Reference photograph taken by Mary Bowen (circa 1965) which appeared in the Folsom Telegraph. This photo is on file at the Folsom History Museum.

⁵¹ Folsom Telegraph, July 25, 1891, page 3, column 4.

⁵² Mountain Democrat, November 18, 1893, page 1, column 3.

⁵³ This per the journal of El Dorado County pioneer, James S. Russell, who, ironically, was the grave digger for the communities of Rescue and Green Valley from as early as 1861 until the turn of the century. Russell's son, Frank, and his grandson, are buried in the coped Russell plot at Lakeside Cemetery.

⁵⁴ Folsom Telegraph, August 29, 1885, page 3, column 3.

⁵⁵ Folsom Telegraph, May 4, 1901, page 3, column 1.

Franklin Riggins Family Plot

3 unmarked

Three members of this family were interred at Folsom. Son Owen (died 1911), was twelve years old at the time of his death and was buried at "Citizen's Cemetery." His father, Franklin B. Riggins (died 1912), a fifteen year resident of the vicinity, and the husband of Emily (nee Saul) Riggins, was buried at the "Citizen's Cemetery". Emily Riggins (died 1932), the sister of Ed Saul and the daughter of Charles Saul, was buried in the "Citizen's Cemetery." None of these graves are marked in the cemetery today. There is a Saul Family plot located in the Cook's/American Legion Cemetery, and it should be reviewed to determine if the Riggins graves may be located within it. [SS Note, 12/6/2002: Peter Kroff did not record grave markers for the Riggins family in 1998.]

Caroline Rose Family Plot

2 unmarked

Caroline (nee Riley) Rose (died 1887) was the wife of Milton Rose, one of Folsom's African American pioneers. Her death resulted from burns received when the Rose's home burned to the ground. The funeral services for Mrs. Rose were held at the Episcopal Church (Trinity) and she was later buried "by the side of her son, in the Citizen's Cemetery.⁵⁶" No death notice has been found to indicate the name of the son next to whom she was buried, or how old the child may have been at the time of his death.

Her husband, Milton, who is featured in the book, *Folsom Fables ~ Pieces of the Past*, in the story titled, "Three Gentlemen of Color," died in Sacramento in 1890 and was buried at New Helvetia Cemetery⁵⁷ where he was likely removed and re-interred in the mass grave now located at East Lawn Cemetery. Mr. Rose's name appears on very early county tax assessor maps as owning property in Folsom.

Frank Russell Family Plot

2 unmarked

The son of El Dorado County pioneer James S. Russell, Frank Russell was killed in a dredge accident in September of 1912. His infant son (name unknown) died in February of that same year. The coped Russell family plot is located in Lot 8 of Block 44 (Negro Bar [aka: Citizen's] Cemetery), but the two markers once located here can no longer be found⁵⁸. Only the military marker of Lawrence P. Paulsen, Frank's brother-in-law, is in place in the plot. According to the Folsom Telegraph obituary for Frank Russell, he was buried in the "Citizen's Cemetery."

J.P. Schaffer Family Plot

2 unmarked

⁵⁶ Folsom Telegraph, September 10, 1887, page 3, column 2.

⁵⁷ This per the records on file at the Sacramento City Cemetery archives.

⁵⁸ Per Russell descendant Linda (nee McBeath) Van Gundy, daughter of Elmer McBeath, there were two markers for Frank and his son at one time.

Historical background on Mr. and Mrs. Schaffer has not been recorded in this research. Caroline M. Schaffer (died 1891) was a Folsom resident at the time of her death. J.P. Schaffer, died in Sacramento in 1900 and his "remains brought to Folsom and interred beside those of his wife who preceded him to the grave..."⁵⁹. The location of the graves is not given in either obituary, nor were markers recorded in previous tombstone inventories.

Melchoir Schlittler Family Plot

1-2 missing and unmarked

Another review of the cemetery grounds may be in order before it can be factually determined that this observation is accurate. In 1994, while wandering the cemetery in search of the grave stones of people identified in earlier research, the marker of Melchoir Schlittler was noted. The 1935 DAR inventory listed both Melchoir and Mary Schlittler, but the marker for Mary was not noted in 1994, nor was it found in 1995.

Melchoir Schlittler (died 1890), a native of Switzerland, is buried in the Odd Fellows Cemetery section. His son, Melchoir Jr. (died 1905), a native of Folsom, was buried in the Odd Fellows Cemetery⁶⁰, though no marker has been found to identify his grave. Melchoir Sr.'s wife, Mary F. (died 1914), a native of Germany, was interred in the "family plot in the Odd Fellows Cemetery"⁶¹, though no marker is noted as having been found. Therefore, the "family plot" is not evident and the graves of Mrs. Schlittler and son, Melchoir, probably located next to Mr. Schlittler, are unmarked today. [SS Note, 5/11/2002: Peter Kroff's inventory lists Melchoir Schlittler b. 1827, d. 1890 AND Mary E. Schlittler (no dates). Mary's marker was not next to Melchoir's marker in 1995!]

Crozier Slayback Family Plot

2 unmarked

Born October 13, 1819, Crozier Slayback, who resided at 501 Figueroa Street, died at Folsom on October 13, 1889, his 70th birthday. His wife, Minerva, died in Sacramento in 1903 and was "buried in the Folsom cemetery"⁶². (This should be interpreted to mean the Citizen's Cemetery, though Crozier's tombstone is located next to the coped Lawson plot in the Cook's/American Legion Cemetery.) There is no tombstone for Minerva at the cemetery today, but it is believed that if Crozier's stone which has been laid flat in the ground, were excavated, Minerva's name will be found on one of the unexposed sides.

The Slayback's son, Dr. Charles M. Slayback, who once owned the home at 606 Figueroa Street and practiced medicine in Folsom, died in San Francisco in 1910. "His remains were brought to

⁵⁹ Folsom Telegraph, December 15, 1900, page 3, column 2.

⁶⁰ Folsom Telegraph, April 29, 1905, page 3, column 4.

⁶¹ Folsom Telegraph, January 30, 1914, page 1, column 6.

⁶² Folsom Telegraph, November 7, 1903, page 3, column 1.

Folsom...⁶³". There is no tombstone or grave marker for Dr. Slayback in the cemetery. A married man, no information has been found about the death or demise of Mrs. Ella Slayback, his wife. Many recent burials have begun to encroach toward the location of Crozier Slayback's tombstone and there is a fear that the unmarked graves of Minerva and Charles will be eventually be impacted.

Ira Sowles Family Plot

2 unmarked

Located toward the rear of Cook's/American Legion Cemetery (within Lot 16 of Block 44), close to the tombstones of the Jarvis and Morris families, is the marker for the "Infant son of Ira & Ella Sowles" who died in 1887. Ella Sowles, wife of Ira and mother of the infant, died in 1889, though no location within the cemetery was mentioned in her obituary. Ira Sowles died in Sacramento in 1909 and was interred in "Cook's Cemetery"⁶⁴." He was survived by three children - Etta, Chester and Bert. Etta Sowles died at San Francisco in 1918, but there is no indication that she was brought to Folsom for burial. At the time of her death, she was survived by both her brothers.

It should be noted that the tombstone for the Sowles' infant son is now surrounded by other, non-family related tombstones. It is not known whether these stones represent burials (unlikely) or if the markers there now were removed from other locations in the cemetery.

SS Note, 5/11/2002: Another review of the cemetery might be warranted. While cleaning the Chung Wah Chinese Cemetery in April, I took a walk around Lakeside. I believe I noted a Sowles grave marker at the rear of the Citizen's cemetery along the line of bushes. A re-check is recommended.

Edward Stockton Family Plot

5 unmarked

In 1935, the DAR tombstone inventory listed "Stockton, Edward No dates visible.", and "Stockton, wife No dates visible." There are no markers with the name Stockton located at the cemetery today. Research, however, has revealed that there were likely five members of the Stockton family interred at the cemetery.

Edward Stockton, famous as the industrious man who built the Stockton flour mill on the American River above Folsom, and his wife, Emily, lost two children in December of 1856. A son, Edward Coover⁶⁵, died December 13, 1856, and a daughter, Emily, died December 22, 1856. The couple lost another son, John Thornton, in March of 1862.

⁶³ Folsom Telegraph, April 15, 1910, page 1, column 4.

⁶⁴ Folsom Telegraph, February 19, 1909, page 1, column 2.

⁶⁵ Coover was the surname of Stockton's partner in his flour mill.

Emily Stockton died at Folsom in November of 1867. Her funeral was held November 11, from Trinity Church and she was buried in the Odd Fellows Cemetery⁶⁶. Edward Stockton, having lost his mill in a lawsuit involving his partner, died at Truckee in June of 1868, where he was working for the railroad. His remains were shipped to Sacramento from Reno and forwarded to Folsom where he was buried by the Committee of the Sacramento Pioneers "by the side of his wife in the Mason's plot."⁶⁷

Having found the death notice for John Thornton Stockton in the Sacramento Daily Union of 1862, a review of the 1935 DAR tombstone inventory has revealed that DAR also picked up the name "THORNTON." The 1935 list includes: Edward Thornton, Mrs. Emily Thornton, Emily Thornton, and John Thornton, with the notation "No dates visible on marker." There is too close a similarity between these given names and the given names of the Stockton family members to ignore. It is believed that the "Thornton" family recorded by DAR was, in actuality, the Stockton family members. Still, no markers with the name Stockton or Thornton are found at the cemetery today.

James H. Sturges Family Plot

5 missing and unmarked

James H. Sturges, a pioneer of Folsom and vicinity and jewelry merchant on Sutter Street, was buried in the Masonic Cemetery in 1904⁶⁸. His wife, Henrietta (died 1902), who preceded him in death and was also buried in the Masonic Cemetery. Sturges also became the first weather recorder for Granite Township under the original U.S. Signal Corps.

According to the 1870 U.S. Census, the couple had three children - Charles, Mary L., and James. The first of these children to de cease was Mary Louetta (nee Sturges) Martine. The 1935 DAR tombstone inventory lists Mary Louetta Martine, age 24 years, died July 30, 1888. No tombstone for Mary was located in 1995 and a 1997 telephone inquiry to the cemetery operator was unsuccessful in finding the location of the grave.

The second of the couple's offspring to die was Charles H. Sturges who died at Folsom on August 30, 1900. He was buried in the Citizen's Cemetery⁶⁹. At the time of his death, Charles left a wife and five small children. The son of Charles H. and Margaret Sturges, Hugh Charles, the grandson of James H. Sturges, died in Folsom December 23, 1892, though the obituary did not indicate his place of burial.

⁶⁶ As per the records of Trinity Church as transcribed by Lois A. Dove, docent of Sacramento City Cemetery Committee.

⁶⁷ Sacramento Daily Union, July 13, 1868, page 3, column 2.

⁶⁸ Folsom Telegraph October 22, 1904, page 3, column 2.

⁶⁹ Folsom Telegraph, September 1, 1900, page 3, column 4.

In October of 1940, Mr. and Mrs. Edgar Martine of Nevada City, California, visited the graves of his grandparents, Judge and Mrs. J.H. Sturges and his mother, Mrs. Martine⁷⁰. Today there are no marked graves bearing the name Sturges or Martine in either the Masonic cemetery or the Citizen's Cemetery.

⁷⁰ Folsom Telegraph, October 25, 1940, page 1, column 4.

John White Family Plot

5 unmarked

Between 1856 and 1862, four of the children of John White died at Folsom and were attended by the minister of Trinity Church. They were: Anna Belle (died 1862), Charles Alexander (died 1856), Charles Henry (died 1862) and Clara Louisa (died 1861). The 1860 U.S. Census lists John White (age 30 yrs), Ann White (age 26 yrs), John White (age 9 yrs), Clara White (age 3 yrs) and Emma White (age 9 mos.).

John White, listed on the transcribed records of Trinity Church as John "Waite", died June 3, 1861 and was buried in the Citizen's Cemetery. His estate was probated in Sacramento County⁷¹, with one of the estate appraisers being W.W. Dresser. There are no grave markers for John White or any of his children at the cemetery today.

Carl Windmiller Family Plot

5 unmarked

Photographs of the Windmiller family plot were taken circa 1889 and 1908. The earlier photograph shows the graves of Rosa (died 1889) and Charles Martin (no date available but shown in photo), each mounded in the ground. The 1908 photograph shows the family plot as it was when it was coped with granite with the name "Windmiller" inscribed into the face of the top step into the plot. Today the coping has been removed and there are no markers in the plot to identify the names of those buried there.

Charles Martin Windmiller, the son of Carl and Wilhelmina Windmiller, was the first to be buried in the plot, since his grave existed at the time Rosa Windmiller was buried in 1889. According to family records, Aurora Anne Windmiller, another daughter of Carl and Wilhelmina, is also buried in the family plot, though no record of her death has been located in this research. She would have had to have died after Rosa's death in 1889 as there are only two graves visible in the plot in that year.

Carl Windmiller died in 1897 and was buried in the "Folsom City Cemetery"⁷², although the remaining face plate from the top step of the coped plot has been left in the ground within the Cook's/American Legion Cemetery, just east of the J.B. Schmidt coped plot. Wilhelmina Windmiller died in 1908 (the year the granite coped plot was captured on film) and was buried at "Cook's Cemetery."⁷³ This would be consistent with the present location of the name plate and is also an example of how the section known as "Cook's Cemetery" had previously been referred to as the "Citizen's Cemetery" or "Folsom City" and "City" Cemetery.

This family plot has been severely desecrated with its dismantling. With five unmarked burials within the (unknown) boundaries of the plot, current burials which are presently encroaching within

⁷¹ Sacramento Daily Union, June 25, 1861, page 3, column 1: Ann White appointed administrator.

⁷² Folsom Telegraph, November 20, 1897, page 3, column 4.

⁷³ Folsom Telegraph, November 14, 1908, page 1, column 6.

this area and are of great concern. Portions of the granite coping can be identified as those taken from the State park lands during the 1996 investigative excavation.

SS Update, July 1998: Ric Windmiller, a descendant family member, obtained a Ground Penetrating Radar survey through NorCal Geophysical Consultants. According to Ric Windmiller, and Jerry Nelson, NorCal's consultant, the mapping of this survey clearly shows where overburials have occurred at or near the Windmiller plot. In viewing a copy of the mapping, one of the overburials may be of a Slayback family member.

John G. Woods Family Plot

1 unmarked

Located at about the half-way mark of Lot 15/16 of Block 44 (Cook's/American Legion Cemetery) is the marker of Mary Woods, the wife of John G. Woods, who died in 1888. In February of 1889, Folsom monument dealer, J.L. House, put up granite coping around the Woods grave in the Citizen's Cemetery. In April of that year, House erected "one of the finest monuments in our cemetery..." at the grave of the wife of J.G. Woods.⁷⁴

Mary's marble tombstone has been laid flat in the ground at its present location. The large, ornate marble marker bearing the name J.G. Woods, was found in the Oleander bushes behind the maintenance shed in 1994.⁷⁵ With the information provided by the Telegraph, we know that the plot was coped with granite and that a "handsome" monument was erected over Mary's grave in 1889.

John Woods died in Placerville in May of 1923. He was buried in "Cook's Cemetery."⁷⁶ He was over 80 years old at the time of his death and was survived by his daughter, Mrs. Roy Celio of Oakland, California.

Today, in the grave to the east of Mary Woods, is a 1974 burial. It is not known to which side of Mary's grave John was interred. If he was buried to the east of her present marker, he has now been buried over by the more current burial. If he was buried to the west of Mary's marker, he is in danger of being buried over by a future interment. In either case, the granite coped and enclosed Woods plot, has been desecrated.

SS Update, 12/5/2002: From the GPR map provided during the Windmiller family plot survey, it appears that the grave of John Woods has been impacted.

⁷⁴ Folsom Telegraph, February 16, 1889, page 3, column 1, and April 13, 1889, page 3, column 1.

⁷⁵ Like the Odell tombstone, John Woods marker was moved back into the cemetery and placed in the Lamblett family plot behind the Benjamin Bates family plot in the Masonic Cemetery.

⁷⁶ Folsom Telegraph, March 9, 1923, page 1, column 4.

Peter Yager Family Plot

8 unmarked

The first burial believed to have occurred in the Peter Yager family plot in the "Citizen's Cemetery," was that of William Hudson Thomas, the grandson of Peter Yager. According to the transcribed records of Trinity Church, this two year old was interred in the "Folsom Cemetery" (read Citizen's Cemetery) in February of 1870. He was the son of Thomas and Elizabeth Thomas. This burial was followed by the burial of little Mabel Thomas in July of 1881. The infant daughter of Mr. and Mrs. David (nee Emma Yager) Thomas, and the granddaughter of Peter Yager, little Mabel died at Oakland and her remains brought to Folsom by train.⁷⁷

Peter Yager's wife, Elizabeth (nee Ruman of the Ruman family of Michigan Bar, Sacramento County) was the next to be buried in the family plot in 1885. She was buried in the Citizen's Cemetery near a nephew and two grandchildren.⁷⁸ Yager's step-son, John O. Brown, the son of Peter's second wife, Sarah (nee King) Brown Yager, died in 1897 and was "interred in the family plot in the City Cemetery."⁷⁹

Peter Yager, the renowned brewer who moved to Folsom around 1871, and whose building remains on Sutter Street, died December 31, 1898 at Folsom. He was buried in the "Blower Cemetery near Folsom."⁸⁰ He was survived by his wife, Sarah, and son, L.T. (Louis) Yager.

Elizabeth (nee Emma Yager) Thomas, wife of Thomas Thomas and daughter of Peter and Elizabeth Yager, died in 1904 and may also be buried in the family plot. Her husband, Thomas, died in Los Angeles in 1894, though the only notice of his death came from Elizabeth's obituary.

Louis T. Yager, son of Peter and first wife, Elizabeth Yager, died in 1907 in a Sacramento hospital. "His remains were brought to Folsom where he was buried."⁸¹

Of the eight unmarked graves noted as being part of the Peter Yager family plot, only Elizabeth and Thomas Thomas' graves are questionable as to city of interment. The remainder of the family, Yager's and Thomas', were either buried in the same plot or very near each other. None of the graves are marked today.

⁷⁷ Folsom Telegraph, July 23, 1881, page 3, column 1.

⁷⁸ Folsom Telegraph, March 14, 1885, page 3, column 7.

⁷⁹ Folsom Telegraph, November 20, 1897, page 3, column 3.

⁸⁰ Note that in the discussion of the Cook's/American Legion Cemetery, Maggie Cook, daughter of William Cook and sister of Frank Cook, had married David Blower. There was a time on the assessor's records that David Blower was listed as owning these lots or a portion of them.

⁸¹ Folsom Telegraph, January 4, 1908, page 1, column 5.

Charles Zimmerman Family Plot

2 unmarked

The only marked Zimmerman grave in the cemetery today is that of Bertha Zimmerman who died in August of 1897. She was interred in the "City Cemetery..." "the remains consigned to their last resting place in the family plot."⁸²

Previous to Bertha's death, however, her sister, Ethel Zimmerman, died in 1893 at the family home in Clarksville, El Dorado County. Charles Zimmerman was the proprietor of the Western Hotel at the corner of Riley and Sutter Street, the building of which remains today. There is no record of Ethel being buried at Clarksville, and with the business being in Folsom, it is believed she was buried in the Citizen's Cemetery, where the tombstone of her sister is located today.

Charles Zimmerman's sister-in-law, Mary Anderson, died at Sacramento in 1911, and was buried in the "Folsom City Cemetery."⁸³ There are no markers in the cemetery for Ethel Zimmerman or her aunt, Mary Anderson.

KNOWN DISINTERMENTS AND RELOCATED REMAINS

Of all the research performed for this study, only two instances of disinterments were encountered. The first occurred in February of 1924, when the granddaughter of Folsom railroad pioneer, John Kinney (died 1864), and the daughter of lifelong Folsom station master, Joseph Kinney, had the remains of her grandfather, father and mother disinterred and relocated to a cemetery in San Diego. Her brother, Ned Kinney, lived there and they wished the graves to be closer to family.

The second occasion of disinterment came after the death of Katherine Blanchard, wife of Folsom's beloved school professor, J.E. Blanchard. Mrs. Blanchard was interred at East Lawn Cemetery in Sacramento and the surviving family apparently had the remains of Professor Blanchard (died 1914) and their son, Leland (died 1903), removed from the cemetery at Folsom and transferred to East Lawn for re-interment.

CONDITION OF THE CEMETERIES

At the end of the compiled listing may be found accounts of the condition of the cemeteries as found in the newspaper throughout the years. The first article noted occurred in March of 1870 when a thief or thieves desecrated the graves in the Masonic Cemetery by removing plants, flowers and shrubs. A \$25 reward was put up by the Masonic Lodge.

With the exception of the vandalism and desecration which occurred in the Jewish and Odd Fellows cemeteries in 1891, no other instance of deliberate damage to grave markers has been

⁸² Folsom Telegraph, August 7, 1897, page 3, column 1.

⁸³ Folsom Telegraph, May 11, 1911, page 3, column 4.

found. In June of 1890, "A Destructive Fire" occurred within the cemetery which spread over the Masonic and Citizen's cemeteries. "A great deal of damage was done to the fences enclosing the graves and nearly all the tombstones were blackened or badly cracked up."

The dismal condition of the cemetery was remarked on in June of 1891. The grounds needed mowing and there was fear another fire would spread through the grave yard as had occurred the previous year. "The remains of a great many people who have no relative here repose in the cemetery, and only a wooden board marks their last resting places. A fire acquiring any headway at all would burn them up and no way to determine the burial would remain.", the paper wrote.

Another fire occurred in 1904, this time at the Jewish Cemetery. It was reported to have done a great deal of damage to the fences.

For many years, committees comprised of members of the Odd Fellows and Masonic lodges attended to the needs of those cemeteries and the Citizen's Cemetery. Later, County Road Crews would undertake to provide clean-up of the grounds. In April of 1939, the paper headlined an article about one such clean-up, "Oldest Tombstone Marks Cholera Victim's Grave." The article informed that the tombstone of Henry Probasco had been found during the clean-up by the county road crews. It related that Mrs. O.J. Miller recalled her father telling of hearing his uncle say that a colony of Germans who located near Folsom in early days, suffered an epidemic of Cholera. "It seems likely that Henry Probasco (sic) may have been a victim of that epidemic.", the paper wrote, having drawn its conclusion on erroneous historic information.

In 1942, in preparation for the "Annual Road Day" during which the cemeteries would again be cleaned, the paper noted that "an additional piece of ground" had been taken up and added to the Citizen's Cemetery. This was likely Lots 1 through 3 of Block 44 (which was later sold to Lakeside Cemetery by the County Tax Collector in 1976).

POSTSCRIPT

This report contains many facts which are difficult to fathom or believe. It is incomprehensible to most of us that such activity can and does occur. What we all must reconcile ourselves to is that those who participate in the "death industry" are in it strictly for one thing - profit. When a cemetery fills to capacity and beyond, there is little profit to be gained in the future.

When those interred in a cemetery have no one to visit them, no one to watch over them, the cemetery operators know this. They can track visitations by the flowers left at the graves. They are also aware, on filling out death certificates for families or friends, if there is anyone left in a given family who will be around in 10 or 20 years.

Funeral directors and employees all act as if they feel your pain on losing a loved one. They become a close confidant because they can perform the "ugly" business you cannot. You become dependent upon their expertise and knowledge and you feel comforted by their caring attitudes. Do not be fooled. They teach all this in undertaking classes.

The best reference regarding the modern funeral home and cemetery industry was written by Jessica Mittford in the mid-1960's. "The American Way of Death" is an excellent resource from which to educate and understand how this most necessary of all industries has evolved over time. How they have a built in business that is never going to go away, how you are dependent on their services and that, in the numbness of loss and grief, you and your pocketbook are vulnerable.

The cemetery industry in California is an extremely corrupt and dirty business. In 1995 alone two cemeteries in southern California were closed by the State and taken into receivership. One, Lincoln Memorial Park in Carson, California, was found to have utilized the same grave and coffin for up to eight burials a day, discarding the remains in a mass grave at the rear of their grounds. They had removed grave markers and utilized them for street curbing. Distraught families who broke into mausoleum crypts thinking they would find the coffins of their loved ones, found empty space instead.

Other cemeteries are presently under investigation for similar practices. The unfortunate part about these matters is that, after years of being under the control of those employed in the cemetery industry, State laws enacted to regulate the industry have been diluted to the point that there is no enforcement or punishment capability. A law may say "Don't do this.", but it gives no one a way to stop a corrupt operator from committing the act.

As it stands now, the Department of Consumer Affairs seems to be actively pursuing those cemetery operators who have raided their "endowment" accounts - stolen from the monies intended to be there when a cemetery could no longer be actively used. DCA's focus does not seem to be geared toward correcting abuses of older graves or the re-selling of pre-owned plots. And until laws are enacted which provide enforcement and punishment, these unlawful activities will continue to occur.

At Lakeside Cemetery, it continued to occur even while the operators were being investigated. One of the comments made by Robert Claney to the DCA inspector, Ron DeMaderios, was that in the early years the graves were much deeper and over time the coffins and remains have collapsed and condensed to a thickness of approximately 4 to 6 inches. To paraphrase Mr. Claney based on what Mr. DeMaderios told me, "We don't bury that deep any longer. If we bury over an older grave, who cares?"

Bury your loved ones here, if you like. But keep in mind, **it would be nice to know whose grave you will be re-using or atop which pioneer citizen of Folsom your loved one will be interred.**

ADDENDUM, December 6, 2002:

A new pattern of illicit behavior has begun to be discovered by the State Cemetery and Funeral Bureau in California. For many years crematory operators went unregulated. Although the State has recently enacted legislation which occurred in reaction to the Georgia incident in which a crematorium operator left hundreds of corpses lying about his property, crematories in California have also been discovered to have their problems.

As one example, in 1998 the Rogers Family Funeral Home operated the crematorium at the Pacheco Cemetery in Contra Costa County. The principal players in that scenario were Christopher and Laurel Rogers, husband and wife. The state discovered that Rogers had left corpses and cremated remains unattended and improperly handled. Though the couple was prosecuted, the Cemetery and Funeral Bureau instead settled the case out of court by revoking their licenses to operate and attaching a condition that if the couple ever again attempted to obtain licensure in California the State would require them to reimburse the \$32,000 in investigation costs it had incurred.

Some penalty.

Attachment 6
Letters from the Public

Dr. Terry G. Murphy

September 11, 2002

Dear Commissioners of Folsom Historic District:

The crematorium proposal for Lakeside Cemetery must not be approved!

Aside from the obvious zoning violation of putting industry in a residential area, there are compelling health concerns that should absolutely prevent this proposal from further consideration.

As a physician and resident of the Preserve, I have researched the health related impact a crematorium would have on the people who live here. I am appalled by the deleterious effects I and my neighbors would suffer by living next to such a toxic polluter.

Inhaled mercury vapors can cause chemical pneumonitis and may result even in pulmonary edema (swelling of the lungs). Mouth sores extending down the gastrointestinal tract may occur. Chronic exposure is even worse as the central and peripheral nervous systems are affected with brain and nerve damage which may be irreversible. Effects on children are more pronounced as their smaller bodies can't handle as much poison. Mercury is a well-known teratogen which leads to miscarriages and birth defects.

The smoke, ash, and odor, even without the mercury, pose a pulmonary risk in acute and chronic exposure to basically, anyone with lungs. This risk is greater for those who are medically fragile or have pre-existing lung conditions such as asthma or emphysema.

Once these particles find their way into swimming pools, and our treasured Lake Natoma, more health hazards abound, and they are nearly impossible to clean up. Our health, lives, and futures depend on each of you voting your conscious, and your intellect. Given the facts, you can't approve such a harmful proposal.

Sincerely,

Terry G. Murphy

Terry G. Murphy MD, MS, FACEP

& Friends & Neighbors of the Preserve

**A. LAURENT COMPLAINT
CREMATORIUM C.U.P APPLICATION**

I reviewed the Folsom application PN 02-258, (published August 2002, for September 18, 2002 hearing) on August 26, 2002. I documented that the Application File at the Folsom Planning Dept. did NOT have the required documents. August 26, 2002, Public Records Request for copies attached. (The city refused to fulfill my 8/26 request.) There was NO Initial Study, no copies of NOI, NOP, NOD; no Proofs of Posting. Arbitrarily, the public hearing was continued to October. Perhaps because we objected to the city council.

PN 02-258: application for conditional use permit to construct and operate a crematorium in Lakeside Cemetery, despite the following:

1. Crematorium is NOT a permitted use in any Folsom zoning district. (FMC Title 17)
2. The cemetery contains many old trees, and is part of the forest at Lake Natoma -- the same forest that is in a State Park on the American River.
3. The proposed site for a crematorium abuts single family homes on two sides.
4. The proposed site abuts Federal Waters and Federal land operated at a State Park on two sides.
5. The land is zoned "Open Space Conservation." (FMC Title 17)
6. The property lacks adequate access for fire and emergency protection. There is no access to the State Park, should a crematorium fire accident occur!
7. The application file is incomplete: no title.
8. There are multiple CEQA violations: Notices not filed. NO Initial Study in file 8/26/02. Owner said he did not do one.
9. The file contains absolutely no input from oversight Trustee agencies, because proper, legally mandated, Notices were never filed with the State and County agencies.
10. The cemetery ownership has a checkered history, which includes multiple selling of the same plots, disappearance of graves and markers, marijuana cultivation, burial outside boundaries.

In an ordinary city, such facts would prevent such a proposal from being put forward. I spoke with the cemetery owner August 26, 2002. I informed him of the CEQA deficiencies in his application. I also informed him it is necessary to attach a copy of the title to the property because he sold it recently and rebought it last year. The purchaser contacted me after discovering the reselling of plots, grave removals, pot garden, encroachments, and more.

I request your office investigate this case. This is not the only bogus rezoning/use request facing me. The city Rezone Committee recommended rezoning a State-regulated toxic site for Affordable Housing high density zoning. Planning Staff allowed an application to make a public presentation before the Historic District Commission without making an application, without giving any notices, and without paying required fees. This is typical Folsom operation.

Submitted 9/17/02 by Laurette J. Laurent

CC: LNS, media

140 Fargo Way
Folsom, Ca., 95630
September 27, 2002

*Jeff Ferreira-Pro, Chairman
Historic District Commissioners
Planning Inspection & Permitting Dept.
Folsom City Hall
50 Natoma Street
Folsom, Ca., 95630*

This letter is being written to protest the Lakeside Memorial Cemetery and Mausoleum Crematorium's impact on the National and California Registered Landmark Chung Wah Memorial Cemetery and State Historical Point of Interest Young Wo Memorial Cemetery in order to maintain preservation of the circa Chinese Gold Rush cultural, religious and archaeological special features.

These circa Chinese Gold Rush cemeteries for their descendants today continue to be held in solemn spiritual reverence and profound sacred esteem for those interned eternally.

The existing metal onsite maintenance building at Lakeside Cemetery and Mausoleum has 2 garage rollup doors. Because of cremated ashes in the air – either from the crematorium chimney; when ashes are put/poured into a container; through clean-up of ash residue waste disposal or cleaning up of interior crematorium – the potential impact over time would be significant to both Chung Wah Memorial Cemetery and Young Wo Memorial Cemetery. Remnants ashes that are at the crematorium will be in the air and with winds will have to land someplace such as the Chinese burial mounds.

If the crematorium conditional use permit is approved, the most responsible conditions should be mandated to whomever owns, operates, cleans and removes the crematorium ashes. It is requested that this letter be included in tonight's minutes of the Historic District Commission Meeting.

Respectfully,



JUNE C. CHAN

cc:

PETITION TO PROTEST BUILDING A CREMATORIUM

We protest the Lakeside Memorial Cemetery and Mausoleum building an crematorium at Natoma Street and Forrest Street. We feel that this crematorium would impact the Historic District and have an devastating/bad effect on air, environment, health, humans, animals, life, cultural, and archeological landmarks, residents etc. We request that both an Environmental Study and Environmental Impact Report be done.

Date	Signature	Name (Print)	Address (Print)	Comment (Print)
* D) 9/11/02	James L. Ming, Jr.	James L. Ming, Jr	140 Fargo way Folsom, Ca. 95630	
* 2) 9/11/02	Richard L. Ming	RICHARD L. MING	140 Fargo way Folsom, Ca. 95630	
* 3) 9/16/02	Jane Carolyn Chan	Jane Carolyn Chan	140 Fargo way Folsom, Ca. 95630	see letter enclosed
4) 9/23/02				

PETITION TO PROTEST BUILDING A CREMATORIUM

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Date	Signature	Name(Print)	Address(Print)	Comment (Print)
* 1) 9/21/2002	<i>Randall N. Marks</i>	Randall N. Marks	140 Fargo Way Folsom, Ca. 95630	Cremating Bodies at the Cemetery will put Ash + Smoke All Over the Cemetery, also the
2) 9/21	<i>Brian S. David</i>	Brian S. David	202 Fargo Way	Smell of Burning Body allow the Housing Development,
3) 9/21/02	<i>Gladys David</i>	Gladys David	459 Clearstone PLACC	↑ Not in our town This is too close residences
* 4) 9/21/02	<i>Howard Citran</i>	HOWARD CITRAN	203 FARGO WAY FOLSOM CA 95630	

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	Date	Signature	Name	Address	Comment
1)	9-5-02	<i>M. Butler</i>	5244 Butler	1002 Persifer St	
2)	9-5-02	<i>Debbie Butler</i>	Debbie Butler	1002 Persifer St. Folsom	
3)	9-5-02	<i>Everett Rendon</i>	Everett Rendon	902 Natoma St	
4)	9-5-02	<i>Clair Schrank</i>	Clair Schrank	606 Reading St Folsom 95630	
5)	9-5-02	<i>Dinella Covert</i>	Dinella Covert	604 Reading St. Folsom 95630	
6)	9-5-02	<i>Pasquala Rendon</i>	RENDON Pasquala	902 Natoma St.	
7)	9-5-02	<i>Guadalupe</i>	Guadalupe	902-FIGUEROA ST. A	

PETITION TO PROTEST BUILDING A CREMATORIUM

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	Date	Signature	Name	Address	Comment
8)	9/5/02	Carol Howard	Carol Howard	507 Reading St.	
9)	9/5/02	Mike Howard	MIKE HOWARD	507 READING ST.	
10)	9/5/02	Bob Bunker	BOB BUNKER	815 MARMON ST.	
11)	9/5/02	Dixie Bunker	DIXIE BUNKER	815 Marmon Tolsomc.	
12)	9/5/02	Colleen Bohall	COLLEEN BOHALL	509 Natoma	
13)	9/5/02	Jeffrey J. Miller	Jeffrey J. Miller	906 Natoma	

September 27, 2002

Mr. David Storer, Director
Planning, Permitting & Inspections
City of Folsom
50 Natoma Street
Folsom, CA 95630

RECEIVED
SEP 27 2002
PIP DEPT.

RE: Comments on the Initial Study/Draft Mitigated Negative Declaration for
Lakeside Cemetery Conditional Use Permit, 1201 Forrest Street (PN02-258)

Dear Mr. Storer:

I have reviewed the above-referenced CEQA documents and I have the following
comments:

Description of Project

The project description does not provide adequate information to assess the potential
environmental impacts. While this section does describe where the crematorium will be
installed, it does not include information on the crematorium structure itself. CEQA
Guidelines state that the description should include "the whole action involved" to help
the public understand the whole project. In addition, I believe that the following
information should be provided:

1. What are the proposed hours of operation of the project?
2. How long will this proposed project be allowed to operate under a conditional
use permit? Is there a time limit, i.e. 2 years, 20 years?

Environmental Factors Potentially Affected

It states that the proposed project is consistent with the General Plan and with applicable
zoning. According to Section 17.39.020 of the City Municipal Code, permitted uses in
Open Space and Conservation Districts do not include crematoriums.

In addition, the City is relying on a 14-year old Environmental Impact Report (EIR) and
General Plan for assessing potential significant effects for this project. As many changes
and much growth have occurred in the City since 1988, I believe that information in these
documents is too outdated to be relied upon for this proposed project.

I. Land Use and Planning

As stated above, this proposed project is not consistent with existing zoning and the
General Plan, so "b", should be checked either "potentially or "less that with mitigation"
rather than "no impact."

Under the discussion section, it states that this proposed project is consistent with the
General Plan Designation of Open Space, based on the assumption that a conditional use
permit is required for a cemetery. This analysis is confusing and misleading.

As the proposed project is located in the Historic District, reference to any conflict with the Historic District Specific Plan should be discussed. Also, according to the City's Municipal Code Section 17.52.360, Conditional Use Permit review, it states that the Historic District Commission has the final authority relating to the issuance of conditional use permits for projects located in the Historic District. This information should be included in the discussion and the analysis of potential impacts.

VII. Noise

As the hours of operation for this proposed project are not included in this initial study, I do not believe that a determination on whether this proposed project complies with the City's Noise Ordinance is possible.

Also, the discussion section states that a "standard condition regarding hours of operation will be included with the conditions of approval of this project." It is difficult for the public to assess possible noise impacts without having a description of these conditions. As such, I believe that "potentially significant" is more appropriate.

IX. Air Quality

This section does not adequately assess potential environmental impacts from this proposed project. This section includes a comprehensive discussion of the Sacramento Metropolitan Air Quality Management District standards, but it does not include specific information on potential air quality impacts from this proposed project. For example:

1. What are the pollutants from the crematorium?
2. What will the levels of pollutants be for the crematorium?
3. What are the prevailing winds for the area? What is the projected trajectory for air pollutants?
4. What will the odor levels be?
5. What about air emissions from metals such as mercury, lead, etc?

As there is no specific information and analysis on potential air pollutants from this proposed project, an analysis and determination of potential impacts cannot be completed. As such, I believe that "potentially significant" should be checked instead.

XII. Hazards and Hazardous Materials

The discussion section states that no health hazards are associated with crematory use. Other than Sacramento County, has the City researched or consulted with other health and research entities to make this determination? Has the County conducted health and risk assessments on crematoriums to be able to make this statement? If not, it appears to me that this statement is not appropriate unless substantiated by experts. What about potential health risks from mercury?

In addition, how will residual cremation remains be disposed of?

XV. Cultural Resources

As the proposed project is adjacent to an historical landmark (Chung Wah Chinese Cemetery), and is located within a cemetery with historical significance of its own, I don't believe that there will be "no impact" on a historic resource. The Lakeside Cemetery contains many graves of California Pioneers.

XVIII. Earlier Analyses

As stated previously, relying upon an out-dated 1988 EIR for the General Plan is not adequate, and thus the impacts are not adequately addressed. Further, mitigation measures incorporated into the proposed project are also inadequately analyzed, and site-specific information is not provided.

While earlier analysis may be used, CEQA requires that mitigation measures be described which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

Other Comments

Mitigation Monitoring or Reporting Plan

What mitigation monitoring or reporting program will the City establish for this mitigated negative declaration? CEQA requires that the City adopt a program of monitoring or reporting to insure that mitigation measures are complied with (Public Resources Code, Section 21081.6).

Approval of Other Public Agencies

The CEQA Environmental Checklist Form included in the CEQA Guidelines includes a question on other public agency approvals. Doesn't the Historic District Commission have to approve this project, and the State Cemetery and Funeral Board approve the license? Also, the Sacramento Metropolitan Air Quality Management District should be included.

Public Noticing and Comment Period

The public comment period was not provided on the CEQA package signed August 29, 2002. Also, when and what media was the public notice made for these CEQA documents?

Environmental Impact Report

Due to the reliance on an out-dated EIR, and due to potentially significant impacts, I believe an EIR is more appropriate than a mitigated negative declaration for this proposed project. In addition, as I believe that this is a controversial project it warrants an EIR.

Sincerely,



Nancy Carroll
1348 Young Wo Circle
Folsom, CA 95630

cc: Jeff Starsky, Folsom Mayor
cc: Kerri Howell, Folsom Vice-Mayor
cc: Cyndi Dow, City Council Member
cc: Eric King, City Council Member

cc: Steve Miklos, City Council Member
cc: Jeff Ferreira-Pro, Chair, Historic District Commission
cc: Dan Burgoyne, Member, Historic District Commission
cc: Candy Miller, Member, Historic District Commission
cc: Mary Hegarty, Member, Historic District Commission
cc: Dan McNeil, Member, Historic District Commission
cc: John Messner, Member, Historic District Commission
cc: Jerry Fry, Member, Historic District Commission
cc: Martha Clark Lofgren, City Manager
cc: Mary Rigney, The Preserves/Natoma Shores
 Neighborhood Alliance

Laurette J. Laurent
Land Use Consultant
1212 Forrest Street
Folsom, CA 95630-2468
November 23, 2002

Members, Folsom Historic District Commission
50 Natoma Street
Folsom CA 95630

Request for official notice: 1200 block of Mormon Street, public street in the city of Folsom

Dear HDC Members:

With the assistance of Folsom staff, I was able to obtain records for property which is the subject of a Conditional Use Permit by owners, Messrs. Claney. Pursuant to FMC 17.52.360 the Historic District Committee has jurisdiction.

Using legal records, it was possible to identify Mormon Street as a dedicated Folsom street and Right of Way existing for over 100 years. City staff were kind enough to show me that Mormon Street southwest of Forrest Street is the dedicated city street providing access to APN 070 0130 002 000, commonly known as 1221 Mormon Street, Folsom CA,

ILLEGAL TO CREATE A LAND-LOCKED PARCEL: City staff reminded me that under California law, Mormon Street COULD NOT be abandoned because it is ILLEGAL to create a legal parcel that has no public access. 1221 Mormon is accessed/served by Mormon Street.

RECORDS SEARCH: a search of records indicated that Mormon Street southwest of Forrest Street WAS NOT ABANDONED by the city.

STREET SIGNAGE: The 1200 block of Mormon Street southwest of Forrest Street is posted with an official Folsom street sign.

ATTACHMENTS: Metroscan APN records from Sacramento County Clerk Recorders' Office:

1. 1201 Forrest Street, Lakeside Memorial Lawn
2. 1221 Mormon Street, Chung Wah

ACTION REQUEST: I am requesting the Members of HDC take official notice of fact that Mormon Street southwest of Forrest Street is a dedicated Folsom street and Public Right of Way, which has existed continuously since Theodore Judah drew the city subdivision in the nineteenth century; and that 1221 Mormon Street is a Lot/parcel of Record in Sacramento County,

Sincerely,


Laurette J. Laurent

Att:

Cc: Preserves Natoma Shores Neighborhood Alliance

Laurette J. Laurent
Land Use Consultant
1212 Forrest Street
Folsom, CA 95630-2468

November 24, 2002

Members, Folsom Historic District Commission
50 Natoma Street
Folsom CA 95630

Re: DEED Required in Application for Conditional Use Permit 1201 Forrest Street

Dear HDC Members:

In reviewing this application file, I noted that if the property has changed hands within the year prior to the initial application, the owners are required to submit a copy of the deed. This requirement is stated on the city of Folsom Application, It is my understanding that the Application must be complete in order to proceed to a Public Hearing and Finding of Fact Hearing.

I believe Lakeside Memorial Lawn was purchased by Messrs. Claney from a large corporation during 2002, I believe this because several weeks ago Mr. Loren Claney told me this personally. He stated he repurchased Lakeside Memorial from the corporation to which it was sold the previous year,

Since the property in question changed ownership with one year, the new owners are required to file a copy of the Deed with the Application for Conditional Use.

If I were a member of the HDC I would desire to know the facts and circumstances surrounding the sale and quick repurchase of Lakeside Memorial by Messrs. Claney, because they pertinent,

When the Deed is obtained, I request to be informed so that I may obtain a copy of the ~~complete~~ Condition Use Application File.

Thank you for your attention to this matter.

Sincerely,



Laurette J. Laurent

Cc: Preserves Natoma Shores Neighborhood Alliance

Laurette J. Laurent
Land Use Consultant
1212 Forrest Street
Folsom, CA 95630-2468

November 25, 2002

Members, Folsom Historic District Commission
50 Natoma Street
Folsom CA 95630

Re: STANDARDS for granting Conditional Use Permits

Dear HDC Members:

I would like to obtain a copy of the STANDARDS utilized when evaluating a conditional use permit pursuant to Folsom Municipal Code Section 17.52.360.

What I am seeking is the set of rules, guidelines, formal criteria, which the Historic District Commission is required to use when evaluating an application received for a CUP, as well as any supporting material contained in FMC and enabling legislation.

I have an immediate need for this information. Please ask your city support staff to forward this information to me.

Thank you for your attention to this matter.

Sincerely,



Laurette J. Laurent

Cc: Preserves Natoma Shores Neighborhood Alliance

ATT: FMC 17.52.360

Chapter 17.52 H-D. HISTORICAL DISTRICT

Section 17.52.360 *Conditional use permit review.*

A. The historic district commission shall have final authority relating to the issuance of *conditional use permits* for any of the uses or purposes for which such permits are required or permitted by the terms of this title, within the boundaries of the historic district.

B. In acting upon applications for *conditional use permits*, the historic district commission shall adhere to the procedural requirements set forth in Chapter 17.60, except for matters of appeal, which shall be governed by this chapter. (Ord. 890 § 2 (part), 1998)

HISTORIC DISTRICT
COMMISSION

Laurette J. Laurent
Land Use Consultant
1212 Forrest Street
Folsom, CA 95630-2468
November 27, 2002

05/10/2022 Item No. 18.

Eric Dutton, Chief, Fire Department
City of Folsom
50 Natoma Street
Folsom, CA 95630

Re: LAKESIDE CREMATORIUM PROPOSAL

Dear Chief Dutton:

In decades of practice, I have always had an excellent rapport with fire professionals and regulators. I am a March 2002 Regional Water Quality Control Board Designated Party. At this moment I am smelling the smoke from the wildfires up the hill that started in slash after inches of rain. The fires underscore serious problems with this application. I spoke with owner Loren Claney, who said he was unable to afford experts because of his financial condition since he had to repurchase Lakeside.

HIGH TEMPERATURE FURNACE IN A FOREST??

I am seeking Fire Marshall's expertise on this application for what is a **gigantic furnace, in what is in fact a forest**. I attach documents and photos. Did Planning Department, Michael Johnson, or anyone seek your input when this proposal came in last summer? I believe not, because it was brought to the Historic District Commission by staff without discussion of fire danger, emergency access, and misuse of public land. There is no legislation even allowing consideration of this usage, let alone in a forest.

- FACTS:**
1. A crematorium is NOT a use eligible for a Conditional Use Permit within the Historic District, nor any zoning district in Folsom. The site is zoned Open Space Conservation.
 2. The proposed tin shed to house the furnace abuts Federal lands operated as a State Park.
 3. Lakeside Memorial Lawn cemetery is a wooded site within the larger Park forest.
 4. There is absolutely no fire or emergency access to the rear of this site.
 5. The owners have a checkered legal compliance history and were forced to repurchase it.
 6. The owners make a commercial profit by utilizing city land. The Encroachments are numerous, and harm the public and adjacent property owners.
 7. One encroachment is the obstruction of Mormon Street pavement and Right of Way with burial sites. This is a very serious issue.
 8. The Mormon and other city street encroachments have a direct impact on fire & emergency access to other properties.
 9. The Mormon Street encroachment restricts the use and access to abutting Chinese Cemetery property which contains official California Historical Sites of Interest.
 10. The owners block and lock public street at times.
 11. There are brand new preconditions imposed by the Governor upon operators of crematories (which resulted from unscrupulous acts by operators). Mr. Claney has not shown he meets all the newly legislated requirements for crematory owners/operators.
 12. Lakeside owners have cut down large historic oak and pine trees and left the materials which are tinder-dry fire hazards. Attached is a photograph of such an instance of cut trees dumped over the ravine at the edge of the public forest.

I request a violation notice be issued to the owner, and fire hazards be immediately abated. I request the city enforce the NPDES Permit, Tree Ordinance, Subdivision Code, and Health & Safety Codes.

13. For unknown reasons, a full Environmental Impact Report has not been done, despite the huge implications for Lake Natoma/American River and Federal Lands, State Park. (PIP staff apparently have been directed to rely upon verbiage in the 1988 General Plan to justify Negative Declarations. However, the California Attorney General has declared the General Plan INVALID.)

Given the facts, and the attached information about pollution issues, it is apparent a full investigation of the application was not performed. Issues for Fire Marshall include:

1. Illegal encroachments on fire access streets and rights of way,
2. Potential desecration of graves during legal Fire Department access to 1221 Mormon Street for fire protection or other emergency,
3. Cutting of protected trees, then dumping and leaving them as fire hazards,
4. Impact of encroachments and obstruction of public streets on cooperative agreements under which Folsom Fire Department provides emergency assistance to other agencies,
5. Impact of encroachments on FFD ability to serve Young Wo Circle properties,
6. Permitting a gas furnace in Open Space Conservation zone,
7. Permitting a gas furnace in forested unimproved location (State Park) habitat for many forest creatures and protected species,
8. (Faulty) process, by which this application reached the Public Hearing stage without formal Findings of Fact by Folsom Fire Department regarding safety issues,
9. Preparation of a full CUP Application, including ALL required documents, and payment of all fees necessary to cover the costs of FFD investigating the application,
10. Instituting changes necessary to ensure Folsom Fire Department reviews ALL applications for development, improvement, encroachment impacting fire and safety access. Checklist.

This last point is very important. My home is located within a few hundred feet of the proposed furnace. A wild fire could destroy abutting homes as a result of the misuse of public lands and willful obstruction of public streets and ROWs. I have documented moving of markers, and grading changes in public ROWs in the cemetery, which make me suspect they might bury more bodies in dedicated street ROWs. ("Baby Land" is in Mormon St.") Note in the attached Cella Barr Plat the "Encroachments" is blacked out. The original document must be produced. I would like to see a copy of the original.

Why is the city not protecting public streets and rights of way for FFD access? Why is the city not enforcing against the tree destruction and dumping of them to create fire hazards? Why is the city not enforcing the State Codes governing proper use of land and protection of the health, safety, and welfare?? Why doesn't the city require full street improvements to Code? Why is public land allowed to be used for private gain? Why is this property owner permitted to endanger us and hinder our Fire Fighters from doing their job?

Because of the gravity of the situation, I am copying this letter to impacted entities. I am sure you will address these serious questions in order to protect us. I request a copy of your report. If you have questions, please contact me at 985-4488.

Sincerely,



Laurette J. Laurent



CREMATORIUM
CONSTRUCTION

FEDERAL
LAND

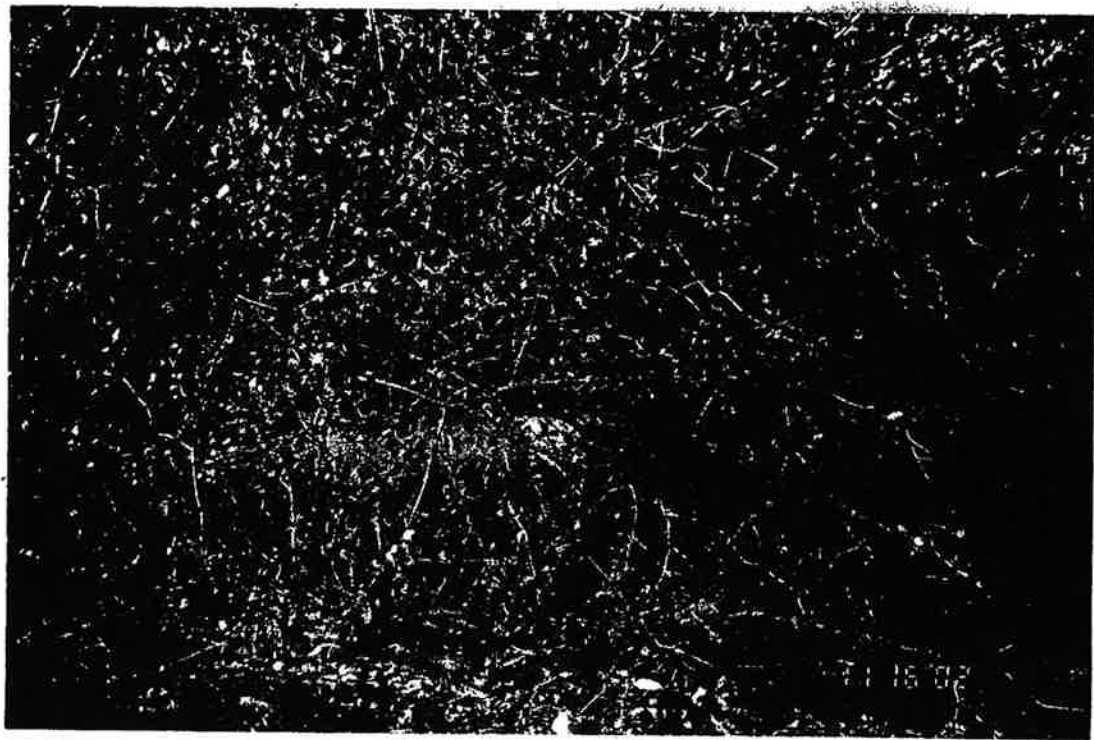


ROAD

← UNCONTROLLED
MUD + SOIL

REAR OF
1201 FORREST
LAKESIDE
MEMORIAL
LAWN,
FOLSOM BLVD.

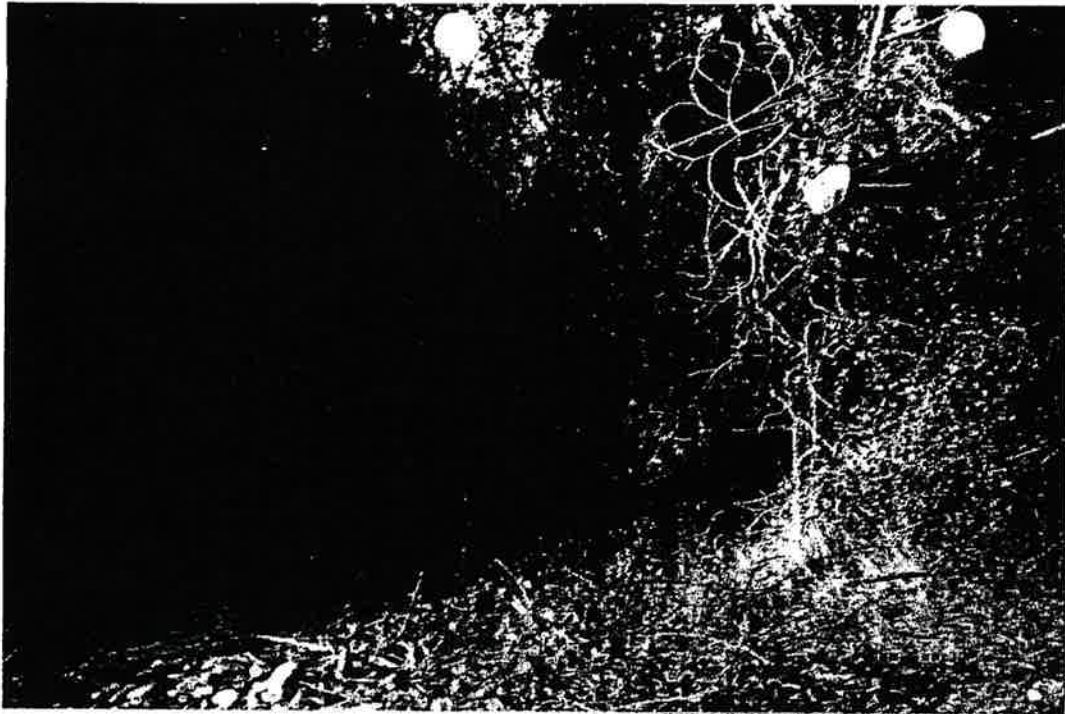
↪ DRAIN INLET



LOCATION
1221
MORMON ST.

DRAIN PIPE FROM AREA OF CONSTRUCTION,
DUMPING ONTO STATE PARK (FED. LAND)
BELOW.

LAKESIDE MEM. LAWN CEMETERY
1 OF 4



TIN

← REMAINS OF CUT DOWN LIVE OAK TREE, DUMPED OFF 'LAKESIDE' DOWN RAVINE.

← STUMPS

FEDERAL LAND AT AMERICAN RIVER IN REAR OF PHOTO

↳ STUMP OF ~27-32" PINE TREE



SIGN POINTING TO MORMON STREET INSIDE LAKESIDE MEM. LAWN GATE

LAKESIDE MEM. LAWN

2 OF 4



STUMP/REMAINS
OF PINE TREE

REAR OF
1221 MORMON

LAKESIDE
MEMORIAL
LAWN

TINDER DRY



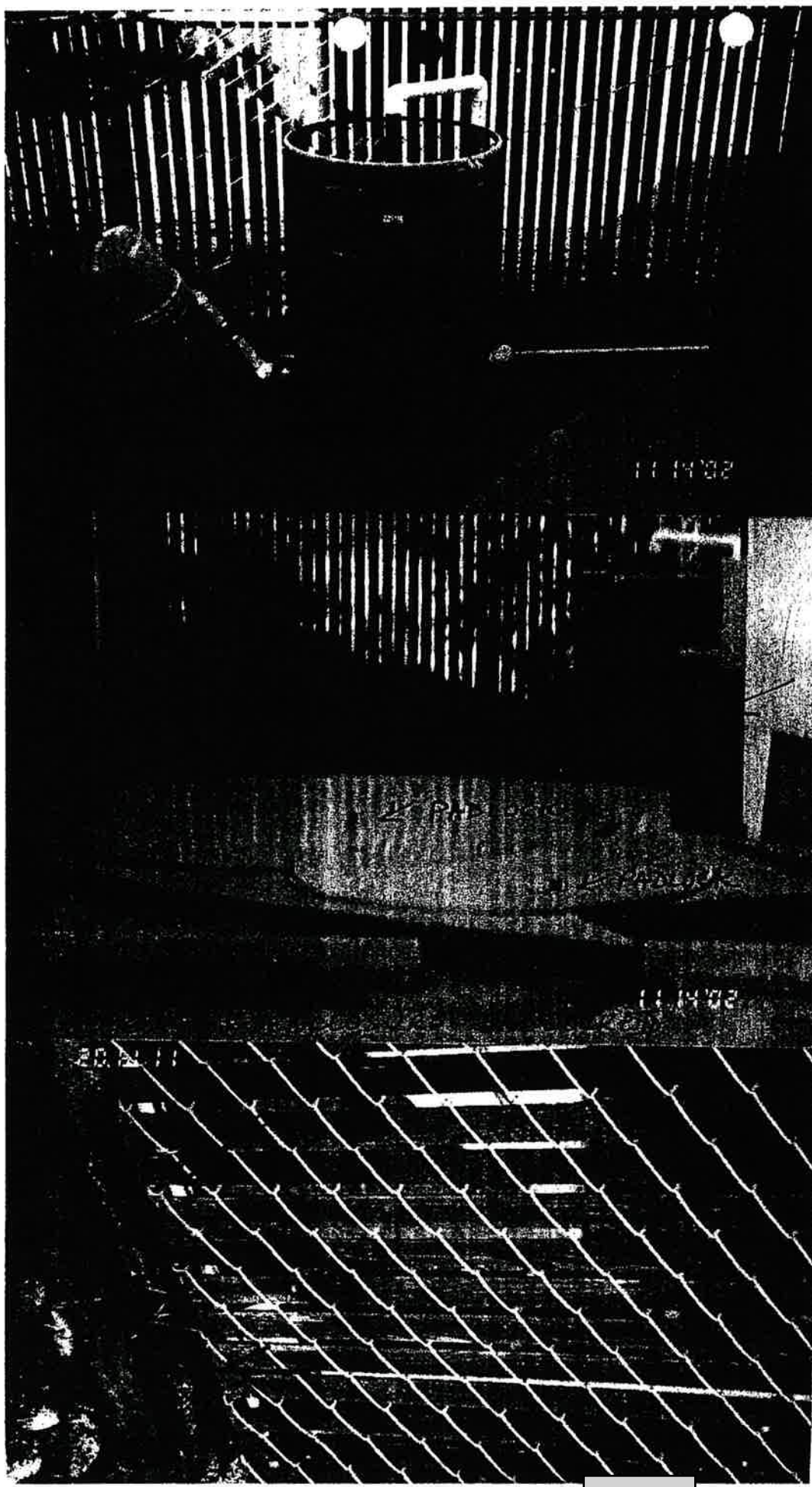
← CEMETERY



DRY FALLEN TREES
FED. LAND + LAKE NATOMA - AMERICAN RIVER

3 OF 4

LAKESIDE MEM. LAWN



05/10/2022 Item No. 18.

LIFT
1330 YOUNG WO
LAKE NATOMA
SHORES SUBDIV.
FOLSOM

DRUM #1
RECEIVING
"VENT" GASES,
LEAKING
LIQUID ONTO
STATE PARK

#2
DRUM WITH
FEED TUBE
INTO WET
WELL
CHLORINE??
← LOCKED
DOWN
WELLS
AT SS
LIFT STATION
1330 YOUNG WO
CIRCLE, FOLSOM

SPILLED LIQUID
FROM 55 GAL.
CONTAINER +
PIPE.

WET AREA,
PHOTO ON
DRY DAY.
4 OF 4

Folsom HISTORIC
DISTRICT
COMMISSION

Laurette J. Laurent
Land Use Consultant
1212 Forrest Street
Folsom, CA 95630-2468

05/10/2022 Item No. 18.

December 6, 2002

To: Interested Party, Organization, Agency

LAKESIDE CREMATORY IN THE FOREST

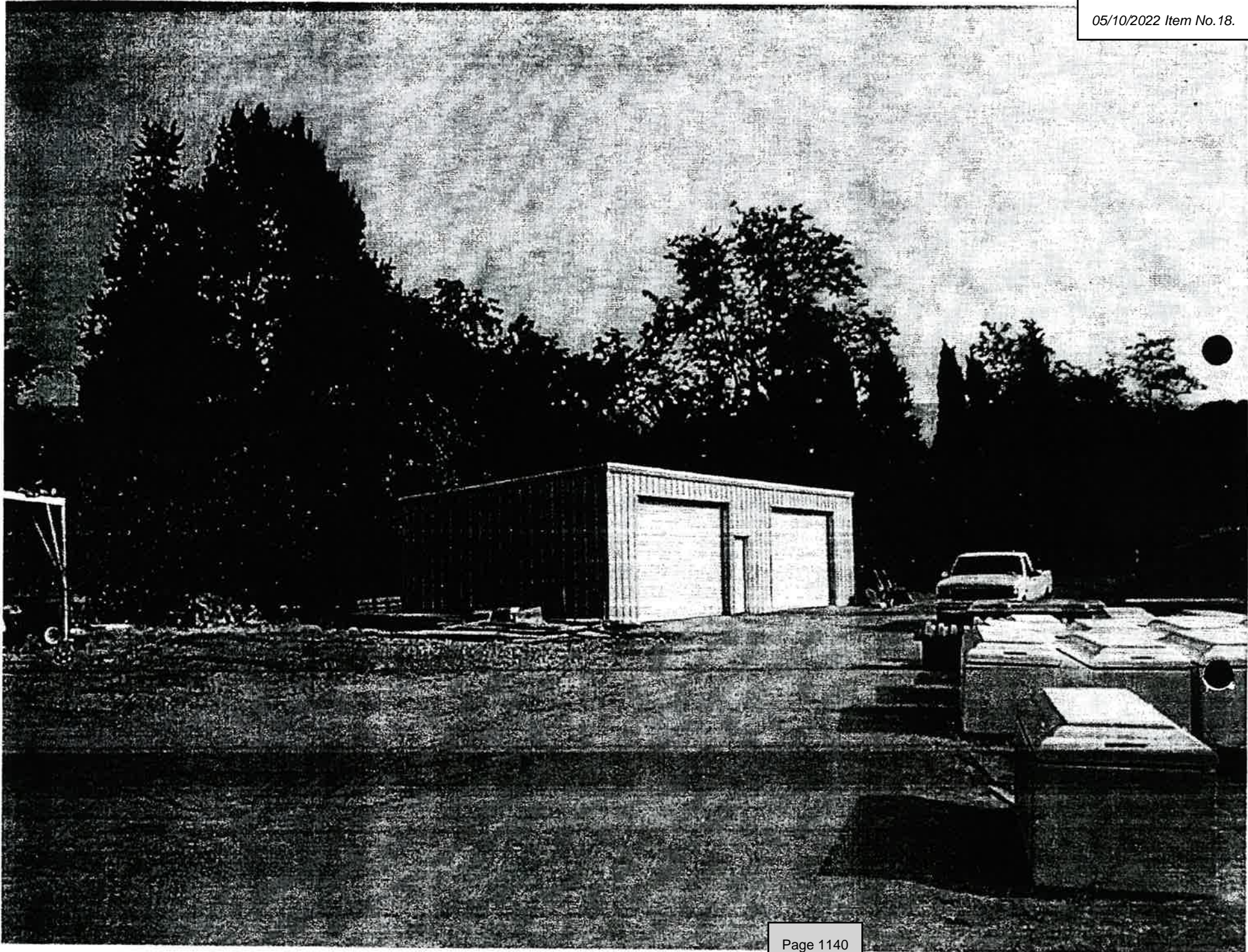
As you know, November 27, 2002, I wrote a detailed letter to Folsom Fire Chief Eric Dutton. I hand-carried it to Fire Marshall Ron Phillips and had a good discussion with him and two others. December 2, 2002, I had a telephone conversation with Chief Dutton. I called him because I did not receive the return call promised to me the 27th. He told me that everything is "great" in Folsom and he saw all development and improvement proposals. I reminded him that he never saw the incinerator in a forest proposal made August 2002, until I brought the records to him on Nov. 27th. I informed him of another huge local development proposal that has never been submitted to him even though it was discussed at Public Hearing.

I realize that the development interests micro-manage the entire city of Folsom, but it is outrageous to endanger the health and safety of a city for the sake of aiding private interests. In this case, I refer specifically to the potential for a 2000 degree inferno machine in a forest where there is NO ACCESS!! There is no access to the garage where the Claney's want to put an incinerator. He expects to have natural gas or PROPANE in a location unreachable in an emergency. I documented the forest nature of the location, the lack of access for emergency vehicles. I documented the misuse of dedicated public streets, and inappropriate re-grading.

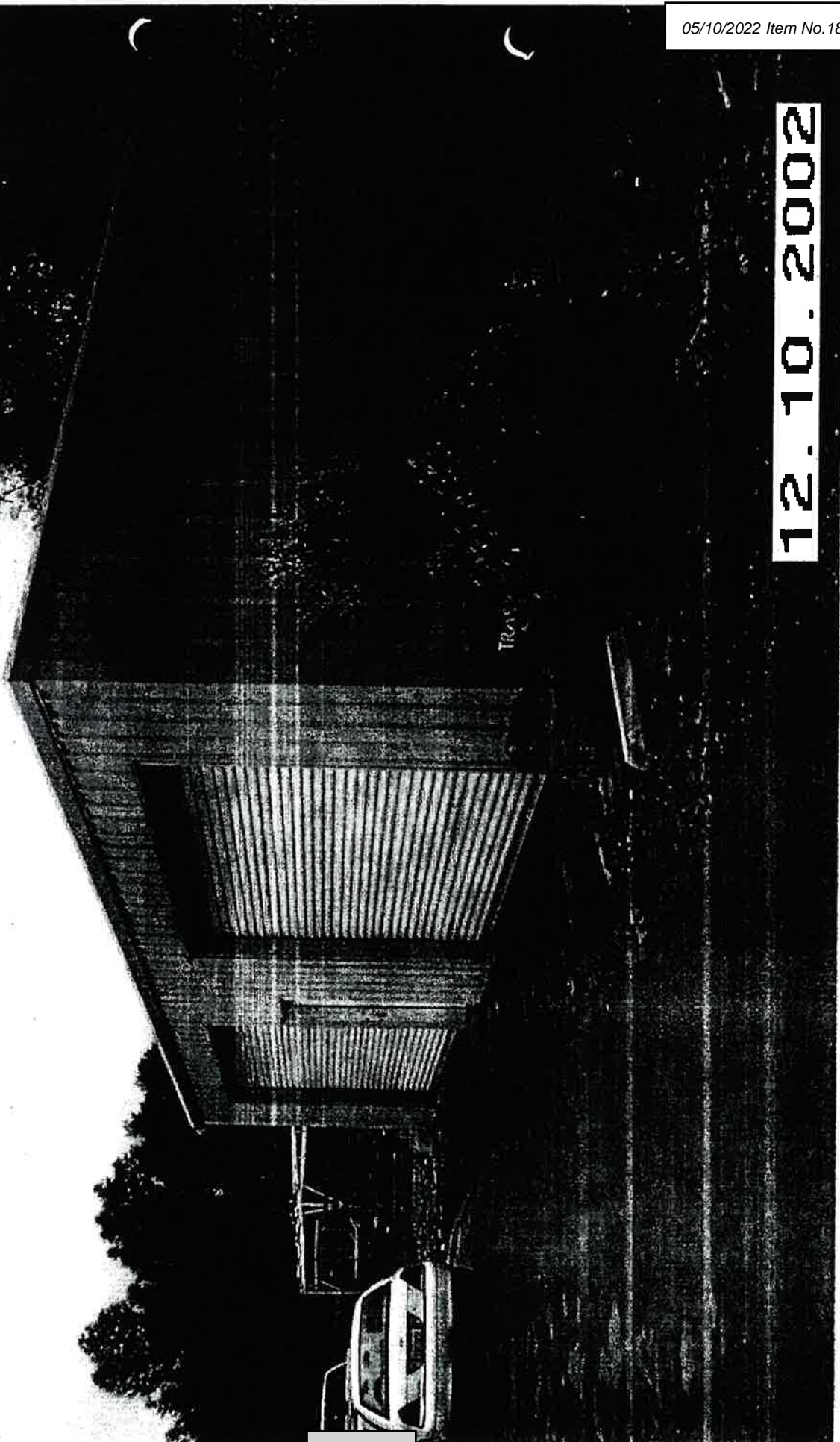
I appealed to Chief Dutton's sense of decency in protecting the lives of his fire-fighters who are called upon to lay their lives on the line for these property owners. I gave him color photos of the trees illegally chopped down and dumped on the cemetery and adjacent lands - creating fuel for an inferno. I gave him the news article detailing the health objections of a physician. I told him I learned that a gas explosion could blow a fire engine 300 feet away. I told him I learned a gas fire could spread through the entire forest along the American River, and destroy our homes easily. I described the smell of the Plum wildfire taking place as I wrote my 27th letter. I told him he may think the city is OK, but I don't want innocent people endangered.

UPDATE: There is none from Fire Chief Dutton. He asked me when the application was filed, and when it goes to the Historic District (Plan) Commission for a Conditional Use Permit hearing. Rumor has it, that date is 12/18/02. Who knows around here?

Attachment 7
Site Photographs



12.10.2002



**CITY OF FOLSOM
HISTORIC DISTRICT COMMISSION AGENDA
January 15, 2003
City Hall, 50 Natoma Street
Planning, Inspections and Permitting Conference Room
Folsom, California 95630
5:00 p.m.**

CALL TO ORDER HISTORIC DISTRICT COMMISSION: Chair Jeff Ferreira-Pro; Vice-Chair Dan McNeil;
Commissioners: Jerry Fry, Candy Miller, Mark Roberts; Commissioner-Elect Susan Mehring

CITIZEN COMMUNICATION: The Historic District Commission welcomes and encourages participation in City Historic District Commission meetings, and will allow up to 5 minutes for expression on a non-agenda item. Matters under the jurisdiction of the Commission, and not on the posted agenda, may be addressed by the general public; however, California law prohibits the Commission from taking action on any matter which is not on the posted agenda unless it is determined to be an emergency by the Commission.

OATH OF OFFICE – FOLSOM HISTORIC DISTRICT COMMISSION

1. Susan Mehring

MINUTES: Minutes of December 18, 2002 stand approved unless there are corrections.

NEW BUSINESS:

1. PN 02-258, 1201 Forrest Street, Conditional Use Permit to Operate a Crematorium

Lorin Claney
% Miller Funeral Home
507 Scott Street
Folsom, CA 95630

A Public Hearing to consider a request from Lakeside Cemetery for a Conditional Use Permit to operate a Crematorium located at Lakeside Cemetery at an existing cemetery at 1201 Forrest Street. The site is designated Open Space (OS) in the General Plan and zoned Open Space Conservation District (OSC) and is located within the Open Space/Public Primary Area of the Historic Commercial Primary Area of the Historic District. (Project Planner: Assistant Planner Jane Talbot)

Committee/Commissions:

Historic District Commission:

Director:

The next Historic District Commission meeting will be held on January 15, 2003. Additional non-public hearing items may be added to the agenda; any such additions will be posted on the bulletin board in the foyer at City Hall at least 72 hours prior to the meeting. Persons having questions on any of these items can visit the Planning, Inspections, & Permitting Department during normal business hours (8:00 a.m. to 5:00 p.m.) at City Hall, 2nd Floor, 50 Natoma Street, Folsom, California, prior to the meeting. The phone number is 355-7222 and FAX number is 355-7274.

NOTICE REGARDING CHALLENGES TO DECISIONS

The appeal period for Historic District Commission Action: Pursuant to all applicable laws and regulations, including without limitation, California Government Code, Section 65009 and/or California Public Resources Code, Section 21177, if you wish to challenge in court any of the above decisions (regarding planning, zoning, and/or environmental decisions), you may be limited to raising only those issues you or someone else raised at the public hearing(s) described in this notice/agenda, or in written correspondence delivered to the City at, or prior to, this public hearing. Any appeal of a Historic District Commission action must be filed, in writing with the City Clerk's Office no later than ten (10) days from the date of the action. A fee of \$150 shall accompany any appeal, payable at such time the appeal is filed.

Terry Sorensen
 ██████████ Forrest Street
 Folsom, CA 95630
 916 ██████████

September 9, 2021

Historic District Commission
 City of Folsom
 50 Natoma Street
 Folsom, CA 95630

RE: Lakeside Memorial Lawn Crematorium Proposal
 Hearing Date September 15, 2021

Dear Commissioners:

The same crematorium proposal herein under consideration was advanced by Lakeside nearly 19 years ago as PN02-258 and scheduled for hearing before this Commission on January 15, 2003. However, that hearing was canceled and did not go forward when applicant Lakeside withdrew the proposal, apparently in light of the Staff Report prepared by the City recommending the denial of a Conditional Use Permit (CUP) to operate a crematorium on the property. Copies of that Staff Report (minus voluminous attachments) and the "canceled" meeting notification are attached as Exhibits "A" and "B," respectively.

The reasons advanced in the Staff Report as the basis for its recommendation of denial can be summarized as follows:

- a. The use applied for was detrimental to the health, safety, or general welfare of the community in that the use would impact the historical character of the existing cemetery and the historical use of the surrounding area;
- b. The conglomeration of historic cemeteries in the area combined with the California State Dredger Tailings Preserve dating back to the 1850s creates a rare combination of unique cultural resources that will be impacted negatively by the proposal; and
- c. The use of the proposed project is not consistent with Goal 2 of the City's Historic District Design and Development Guidelines in that it did not maintain the historic use of the site and, in addition, did not further Design and Development Guideline Policies 2.1, 2.2, and 2.3 (as detailed at the bottom of page 4. of Exhibit "A," attached).

In conclusion, the CUP sought by the applicant in this instance is identical to the CUP sought back in early 2003, and the applicant has presented no facts to the contrary. The proposal for the issuance of a CUP allowing the construction and operation of a crematorium in the historic area in question was a bad idea back in 2003 and remains a bad idea today. The factual basis and logic behind the 2003 staff Report remains valid, and the current request for approval of a CUP for the proposed crematory should be denied for the reasons expressed therein.

Very Truly Yours,


 Terry L. Sorensen

TS/dg

attachments: "A" and "B"

EXHIBIT "A"

Agenda Item # 1
 PN02-258
 HDC Mtg. 1-15-03

Withdrawn

HISTORIC DISTRICT COMMISSION STAFF REPORT

PROJECT TITLE: Lakeside Cemetery Crematorium

PROPOSAL: Request for approval of a Conditional Use Permit to allow for the operation of crematorium

RECOMMENDATION: Denial

APPLICANT AND OWNER: Lorin Claney

LOCATION: 1201 Forrest Street

ASSESSOR'S PARCEL NO.: 070-0082-014, 070-0130-002, 070-0130-004

ATTACHED REFERENCE MATERIAL:

1. Vicinity Map
2. Site Plan, dated 12/19/97
3. Project Description and Crematorium Illustrations
4. Documentation from the Sacramento County Historic Cemetery Commission
5. Lakeside Cemetery Research Paper written by Sue Silver
6. Letters from the Public
7. Site Photographs

PROJECT PLANNER: Jane Talbot, Assistant Planner

BACKGROUND

The Planning Commission approved a Use Permit and Variance for Mausoleums at Lakeside Memorial Lawn in 1991 (PC91-042). An amendment to the approval was granted in 1995 (PC95-033). That approval allowed for the construction of twelve mausoleums. To date, one mausoleum has been built and one additional mausoleum is under construction. An existing maintenance building, approximately 975 square feet in area, is located along the south border of the cemetery. The front of the project site is mostly level with a slight to moderate downward slope towards the rear of the site. Lakeside Cemetery has a variety of mature deciduous and evergreen trees. The front of the cemetery, along Forrest Street, is bounded by a brick wall capped with wrought iron fencing.

The project site consists of three parcels totaling 10 acres within the Open Space Public Primary Area of the Historic District. It is located on the west side of Folsom Boulevard at 1201 Forrest Street. The property is bounded by the Chung Wah Cemetery and the American River Recreation Area to the west, single-family residences to the north, Forrest Street to the east, and single-family residences to the south. Across Forrest Street are single-family residences.

PROJECT DESCRIPTION

The applicant is requesting approval of a Conditional Use Permit to install and operate a crematorium at the existing Lakeside Cemetery at 1201 Forrest Street. The applicant proposes to install the cremation equipment inside an existing maintenance building as indicated on the attached site plan. The maintenance building is made of corrugated metal and currently is used for the storage of grounds maintenance equipment. The applicant proposes to install a 10-foot by 30-foot "Tuff Shed" adjacent to the existing maintenance building to store the displaced maintenance equipment. No new utilities are proposed to be connected to the shed.

GENERAL PLAN/ZONING CONFORMANCE

The General Plan land use designation for the project site is OS (Open Space). The zoning designation for the project site is OS/PB (Open Space/Public Primary Area of the Historic District). The zoning district is consistent with the General Plan designation. Cemeteries are a permitted use within the OS/PB zoning upon approval of a Conditional Use Permit. The subject cemetery has been in operation since the 1800's and is a permitted use. Crematoriums are not specifically addressed in the Folsom Municipal Code. Such uses that are not specifically addressed within the Folsom Municipal Code require the approval of a Conditional Use Permit.

LAND USE COMPATIBILITY/CONDITIONAL USE PERMIT

When approving a Conditional Use Permit, the Historic District Commission must make a finding that a project's impact on health, safety, and welfare will not be detrimental to the health, safety, peace, morals, comfort and general welfare of a proposed use. Some of the comments received suggest that the proposed project may impact health, safety, and welfare. Without additional environmental studies, staff cannot support or refute any of these assertions (FMC 17.09). As staff is recommending denial, based upon the appropriateness of the land use, these necessary additional environmental studies have not been prepared. Should it be the Historic District Commission's desire to proceed with this application, staff will commence with the additional environmental studies.

Historical Significance

Information provided to staff illustrates that Lakeside Cemetery is actually a conglomeration of several cemeteries that have merged over the years. This conglomeration includes the Citizen's, Jewish, Masonic, Negro Bar, Cook's/American Legion, and Odd Fellows cemeteries that date back to around 1850. Adjacent to this site is the Chung Wah Chinese Cemetery. These cemetery sites, along with the neighboring California State Dredger Tailings Preserve, create a combination of rare cultural resources grouped into one small area. This combination, in itself, is an extremely unique resource according to Commissioners with the Sacramento County Historic Cemetery Commission.

Goal 2 of the Design and Development Guidelines defines the City's objective for the Preservation of Historic Sites as:

'To maintain, restore, and reconstruct sites which represent the history of the Folsom area.'

The supporting policies indicate how the City can accomplish this goal. Policy 2.1 states that locally significant structures and sites should be identified and documented to facilitate their preservation or restoration. To date, Commissioners from Sacramento County Historic Cemetery Commission have identified this site as locally significant, and the Commissioners intend to present Lakeside Cemetery to the Sacramento Board of Supervisors for designation as an historic cemetery.

Policy 2.2 advocates that the City should encourage National Register nomination of historic buildings, as well as other historical designations by state or local agencies. Approval of a Conditional Use Permit for a contemporary land use (i.e., the crematorium) may jeopardize the eligibility status of the site and might work to discourage rather than to encourage National Register nomination.

Policy 2.3 explains the priorities with which the City should evaluate proposed projects. The preference should be given to authentic restoration of historical buildings and sites. Based on the information provided to staff by the Sacramento County Historic Cemetery Commission, a crematorium is not a consistent or authentic use with a historic cemetery. Therefore, staff has determined that the proposed project is not consistent with Policies 2.1, 2.2, 2.3, and Goal 2 of the Design and Development Guidelines.

PUBLIC INPUT

The applicant held a community meeting to inform the public about this project on December 19, 2002. There was much public opposition to the project at that meeting. At the meeting, issues related to property values, air quality, traffic, and cultural resources were raised. During the public comment period, staff received comments from numerous individuals and organizations. (The written comments received are attached to this report.) The information received indicates that a crematorium is not a use that is consistent in historic cemeteries.

ENVIRONMENTAL ANALYSIS

An Initial Study and Mitigated Negative Declaration were originally published on August 29, 2002. Staff received feedback regarding the Initial Study. A number of issues relating to the proposed use were raised including land use and planning, noise, air quality, hazardous materials, and cultural resources. As stated previously, staff has determined that the proposal is not compatible with the existing historical character of the cemetery, and staff is recommending denial of the request. According to the California Environmental Quality Act, "CEQA does not apply to projects which a public agency rejects or disapproves." Should the Historic District Commission conclude that the proposal is compatible with the existing historic cemetery use, the Commission will need to direct staff to conduct further research on the issues raised on this project and return the Commission with the appropriate environmental document.

~~Staff~~ ~~comments~~ ~~on~~ ~~the~~ ~~Conditional~~ ~~Use~~ ~~Permit~~ to operate a crematorium at an existing cemetery at 1201 Forrest Street in the Historic District, based on the following findings.

HISTORIC DISTRICT COMMISSION ACTION
MOVE TO DENY THE CONDITIONAL USE PERMIT TO OPERATE A CREMATORIUM AT AN EXISTING CEMETERY AT 1201 FORREST STREET IN THE HISTORIC DISTRICT.

FINDINGS FOR DENIAL

- A. NOTICE HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. ~~THE USE APPLIED FOR IS DETRIMENTAL TO THE HEALTH, SAFETY, OR GENERAL WELFARE OF PERSONS RESIDING OR WORKING IN THE NEIGHBORHOOD, AND DETRIMENTAL OR INJURIOUS TO PROPERTY AND IMPROVEMENTS IN THE NEIGHBORHOOD AND THE GENERAL WELFARE OF THE CITY BECAUSE THE INTRODUCTION OF THIS USE WILL IMPACT THE HISTORICAL CHARACTER OF THE EXISTING CEMETERY AND HISTORICAL USE OF THE AREA.~~
- C. ~~THE CONGLOMERATION OF HISTORIC CEMETERIES, COMBINED WITH THE CALIFORNIA STATE DREDGER TAILINGS DATING BACK TO THE 1800S, CREATE A RARE COMBINATION OF UNIQUE CULTURAL RESOURCES THAT WILL BE IMPACTED BY THIS PROPOSAL.~~
- D. THE USE OF THE PROPOSED PROJECT IS NOT CONSISTENT WITH GOAL 2 OF THE DESIGN AND DEVELOPMENT GUIDELINES IN THAT IT DOES NOT MAINTAIN THE HISTORIC USE OF THE SITE. IN ADDITION, THE PROJECT DOES NOT FURTHER DESIGN AND DEVELOPMENT GUIDELINE POLICIES 2.1, 2.2, AND 2.3 IN THAT:
 1. ~~COMMISSIONERS FROM SACRAMENTO COUNTY HISTORIC CEMETERY COMMISSION HAVE IDENTIFIED THIS SITE AS LOCALLY SIGNIFICANT AND COMMISSIONERS INTEND TO PRESENT LAKESIDE CEMETERY TO THE SACRAMENTO BOARD OF SUPERVISORS FOR DESIGNATION AS AN HISTORIC CEMETERY.~~
 2. ~~APPROVAL OF A CONDITIONAL USE PERMIT FOR A CONTEMPORARY USE THAT WILL JEOPARDIZE THE ELIGIBILITY STATUS OF THE SITE WOULD WORK TO DISCOURAGE, RATHER THAN TO ENCOURAGE, NATIONAL REGISTER NOMINATION.~~
 3. ~~BASED ON A LETTER FROM JAMES A. PURCELL, CHAIRMAN OF THE SACRAMENTO COUNTY HISTORIC CEMETERY COMMISSION DATED JANUARY 2, 2003, A CREMATORIUM IS NOT A CONSISTENT USE WITH A HISTORIC CEMETERY.~~

EXHIBIT "B"

**** CANCELLED ****

**Historic District
Commission meeting
(January 15, 2003)**

**The Proposed
Crematorium Project
has been withdrawn
by the Applicant.**

February 3, 2021

Dear Historic District Commission,

I am writing to you today in opposition of the conditional use permit set forth and applied by Miller's Funeral Home at Lakeside Memorial Lawn, which is owned and operated by Caring Services Group. I live in the preserve neighborhood and have for the past 3 years and counting, I hope.

As I sit here and write this letter to you, I think about the reasons my wife and I decided to move into this neighborhood and start our family. First, we were excited it was so close to downtown Historic Folsom. We could walk to all the great restaurants and bars to meet friends and family for gatherings or just a casual date night. Next it was the amazing trail head down the street and the river. We have used the amazing trails for years now with it being so close to our home. The river is right here at our doorstep and who doesn't enjoy a day floating down the river or jumping in during the hot June, July and August days. Lastly it is a quiet and quaint little neighborhood that has little traffic and people who all look out for each other and truly give you a sense of community.

We have now grown our family and have a dog and an infant child. We have a swing in our front yard that hangs off of a very nice oak tree. I push our daughter in it very often as she enjoys being outdoors. Our home sits directly across from Lakeside Memorial Lawn which we really enjoy. The amount of wildlife we are able to introduce to our daughter as she sits in her swing is second to none. We have turkeys, deer, geese, owls, foxes and the bald eagles that fly over. You really can't ask for anything more. So it saddened me when I heard about Caring Services Groups plan from a neighbor to put in a crematorium across the street. We live just over 600 feet away from where they plan to cremate bodies five to seven days a week. I still have not received a single notice from Caring Services Group about the planned proposal. I thought they were great neighbors but now I see they care more about their bottom line than the community they are so eager to serve in their proposal to you all. The City of Folsom already benefits from the revenue the funeral home derives, even though they have to use an out of city crematorium. Caring Services/Miller Funeral Home simply just doesn't want to drive anymore to do it.

Once I found out about the crematorium potentially entering our residential neighborhood and listening to Igor Semenyuk speak, I did a little research. It still boggles my mind. Why in a residential neighborhood next to the trails and river? Why was it even a thought to put it in any neighborhood let alone a high fire zone area? In looking up what Semenyuk stated about the device, "[It] would be a puff of smoke and then it would only be heat waves after that." I found his statement hard to believe. The COO of Messinger Mortuaries (James Ahearne) stated "Every crematory will smoke; if they tell you they don't, they're lying. It's just the way it is." When talking about neighbors complaining about the amount of black smoke that is released from the companies crematoriums. The neighbors made several complaints with the county

regarding the smoke and the horrible odor that was released. Why would we want this so close to our homes or downtown Folsom?

Semenyuk stated their company wants to be able to serve the larger Indian and Sikh community. I do understand and I believe it is needed with the growing population in Folsom. In speaking to many different people who are Sikhs, they asked why would they put it in a neighborhood, and secondly, they shared there are a lot of good existing options a few minutes away. I do not believe Semenyuk wants to really serve any population in the City of Folsom except his own pocket. You can look it up and see how much of a financial gain they get off having the cemetery zoned in open space but he wants to put a commercial incinerator in an open spaced area of our city. In the city plan it states, "The City of Folsom Plan Area has been designed to showcase the best of life in Folsom. More than 30% - over 1000 acres - of the total Plan Area is maintained as **permanently protected open space to preserve sensitive habitat areas.**" This was voted on and approved in 2004, so help me understand how commercial incinerator in an open space zoned area which is permanently protected is even on your desk and the city's own planners are helping bring it to your ears? Seems ludicrous to me. I'm sure Ernie Sheldon, who is buried across the street, would be rather upset if you are considering this in a zoned open space preserve. It is mind boggling to think about

One of my other concerns besides the smells and heavy metal emissions which cannot be measured since they fall down so quickly is how the fire and police department are supposed to evacuate us in an emergency situation across the street with added ignition sources and high fire risk areas. They added fencing along the shed—yes sheds not a building built for a crematorium— but a shed like one you have in your backyard for mowers and shovels. There isn't a lock box to even get in at night let alone the EMS personnel would block our only exit out. No police consideration has even been given to this project and the increase demand which would reduce resources for the rest of the city with Caring Services wanting to allow in person cremations which according to Semenyuk the Sikh require without creating calls for services for fighting, drunk in public, DUI's and domestic violence issues which can and have occurred. All of these requirements are clearly outlined in CEQA guidelines and ignored by Caring Services Group.

I hope you really look at the motive behind this proposal and what you will be doing other zoned open space areas to in the long run, and consider the bad precedent you will set with this proposed project. This neighborhood is like a family to many of us. We ask, if it was your neighborhood would you want it with the unknowns presented to you? Is it right for a residential neighborhood? Is it right for an open spaced area? Is it right for Folsom? Ask yourself why wouldn't this be in a commercially zoned area?

Please, please vote "No" on this proposal.

Tim McGarry
1204 Forrest St.
Registered voter

February 3, 2021

Dear Historic District Commission,

I am writing to you today in opposition of the conditional use permit set forth and applied by Miller's Funeral Home at Lakeside Memorial Lawn, which is owned and operated by Caring Services Group. I live in the preserve neighborhood and have for the past 3 years and counting, I hope.

As I sit here and write this letter to you, I think about the reasons my wife and I decided to move into this neighborhood and start our family. First, we were excited it was so close to downtown Historic Folsom. We could walk to all the great restaurants and bars to meet friends and family for gatherings or just a casual date night. Next it was the amazing trail head down the street and the river. We have used the amazing trails for years now with it being so close to our home. The river is right here at our doorstep and who doesn't enjoy a day floating down the river or jumping in during the hot June, July and August days. Lastly it is a quiet and quaint little neighborhood that has little traffic and people who all look out for each other and truly give you a sense of community.

We have now grown our family and have a dog and an infant child. We have a swing in our front yard that hangs off of a very nice oak tree. I push our daughter in it very often as she enjoys being outdoors. Our home sits directly across from Lakeside Memorial Lawn which we really enjoy. The amount of wildlife we are able to introduce to our daughter as she sits in her swing is second to none. We have turkeys, deer, geese, owls, foxes and the bald eagles that fly over. You really can't ask for anything more. So it saddened me when I heard about Caring Services Groups plan from a neighbor to put in a crematorium across the street. We live just over 600 feet away from where they plan to cremate bodies five to seven days a week. I still have not received a single notice from Caring Services Group about the planned proposal. I thought they were great neighbors but now I see they care more about their bottom line than the community they are so eager to serve in their proposal to you all. The City of Folsom already benefits from the revenue the funeral home derives, even though they have to use an out of city crematorium. Caring Services/Miller Funeral Home simply just doesn't want to drive anymore to do it.

Once I found out about the crematorium potentially entering our residential neighborhood and listening to Igor Semenyuk speak, I did a little research. It still boggles my mind. Why in a residential neighborhood next to the trails and river? Why was it even a thought to put it in any neighborhood let alone a high fire zone area? In looking up what Semenyuk stated about the device, "[It] would be a puff of smoke and then it would only be heat waves after that." I found his statement hard to believe. The COO of Messinger Mortuaries (James Ahearn) stated "Every crematory will smoke; if they tell you they don't, they're lying. It's just the way it is." When talking about neighbors complaining about the amount of black smoke that is released from the companies crematoriums. The neighbors made several complaints with the county

regarding the smoke and the horrible odor that was released. Why would we want this so close to our homes or downtown Folsom?

Semenyuk stated their company wants to be able to serve the larger Indian and Sikh community. I do understand and I believe it is needed with the growing population in Folsom. In speaking to many different people who are Sikhs, they asked why would they put it in a neighborhood, and secondly, they shared there are a lot of good existing options a few minutes away. I do not believe Semenyuk wants to really serve any population in the City of Folsom except his own pocket. You can look it up and see how much of a financial gain they get off having the cemetery zoned in open space but he wants to put a commercial incinerator in an open spaced area of our city. In the city plan it states, "The City of Folsom Plan Area has been designed to showcase the best of life in Folsom. More than 30% - over 1000 acres - of the total Plan Area is maintained as **permanently protected open space to preserve sensitive habitat areas.**" This was voted on and approved in 2004, so help me understand how commercial incinerator in an open space zoned area which is permanently protected is even on your desk and the city's own planners are helping bring it to your ears? Seems ludicrous to me. I'm sure Ernie Sheldon, who is buried across the street, would be rather upset if you are considering this in a zoned open space preserve. It is mind boggling to think about

One of my other concerns besides the smells and heavy metal emissions which cannot be measured since they fall down so quickly is how the fire and police department are supposed to evacuate us in an emergency situation across the street with added ignition sources and high fire risk areas. They added fencing along the shed—yes sheds not a building built for a crematorium— but a shed like one you have in your backyard for mowers and shovels. There isn't a lock box to even get in at night let alone the EMS personnel would block our only exit out. No police consideration has even been given to this project and the increase demand which would reduce resources for the rest of the city with Caring Services wanting to allow in person cremations which according to Semenyuk the Sikh require without creating calls for services for fighting, drunk in public, DUI's and domestic violence issues which can and have occurred. All of these requirements are clearly outlined in CEQA guidelines and ignored by Caring Services Group.

I hope you really look at the motive behind this proposal and what you will be doing other zoned open space areas to in the long run, and consider the bad precedent you will set with this proposed project. This neighborhood is like a family to many of us. We ask, if it was your neighborhood would you want it with the unknowns presented to you? Is it right for a residential neighborhood? Is it right for an open spaced area? Is it right for Folsom? Ask yourself why wouldn't this be in a commercially zoned area?

Please, please vote "No" on this proposal.

Tim McGarry
 [REDACTED] Forrest St.
 Registered voter

Josh Kinkade

From: Kelly Mullett
Sent: Thursday, July 8, 2021 4:31 PM
To: Josh Kinkade
Subject: FW: Dave Higgins & crematorium plans

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



**CITY OF
FOLSOM**
DISTINCTIVE BY NATURE



www.folsom.ca.us

From: Tim Milne [REDACTED]
Sent: Thursday, July 8, 2021 11:53 AM
To: Kelly Mullett <kmullett@folsom.ca.us>
Cc: [REDACTED]
Subject: Dave Higgins & crematorium plans

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A crematorium should not be located within 1 mile of human habitat!

Josh Kinkade

From: Kelly Mullett
Sent: Thursday, July 8, 2021 10:58 AM
To: Josh Kinkade
Subject: FW: Lakeside Memorial Lawn crematorium concerns.

FYI

Kelly Mullett

Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



CITY OF FOLSOM
DISTINCTIVE BY NATURE



www.folsom.ca.us

From: TIMOTHY THOMAS [REDACTED]
Sent: Thursday, July 8, 2021 10:54 AM
To: Kelly Mullett <kmullett@folsom.ca.us>
Subject: Fw: Lakeside Memorial Lawn crematorium concerns.

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Sent from [Outlook](#)

From: TIMOTHY THOMAS
Sent: Thursday, July 8, 2021 10:49 AM
To: mkozlowski@folsom.ca.us <mkozlowski@folsom.ca.us>; saguino@folsom.ca.us <saguino@folsom.ca.us>; 1156ykc@folsom.ca.us <1156ykc@folsom.ca.us>; kerri@atlanticcorrosionengineers.com <kerri@atlanticcorrosionengineers.com>; rrodriguez@folsom.ca.us <rrodriguez@folsom.ca.us>; thefra@gmail.com <thefra@gmail.com>; sjohnson@folsom.ca.us <sjohnson@folsom.ca.us>
Subject: Lakeside Memorial Lawn crematorium concerns.

Dear Council members and City Representatives,

I have recently been informed of the Lakeside Memorial Lawn's proposal for building a crematorium onsite and I am extremely concerned.

My fiance and our 9 week old son, plus a 12 year old son live just a stones throw away on Young Wo Circle. I myself have respiratory issues, and that along with having a newborn leaves me with great concern. Not to mention the chilling psychological effects of knowing that bodies are being cremated just a block away. It will have a lasting effect on our piece of mind, quality of life, health and property values.

With enough concern already regarding the annual wildfire smoke issues, please spare us another health and psychological factor in this beautiful city that we call home.

Warm regards,
Timothy Thomas
Young Wo Circle
Folsom, Ca

Sent from [Outlook](#)

Sent from [Outlook](#)

From: [REDACTED]
To: [Josh Kinkade](#)
Subject: Lakeside Memorial Lawn Crematorium
Date: Saturday, February 5, 2022 1:53:39 PM

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February 5, 2022

Mr. Josh Kinkade
Associate Planner
City of Folsom Historic District Commission

Re: Lakeside Memorial Lawn Crematorium
Planning No. PN-19-182

Dear Mr. Kinkade:

I am writing in opposition to the proposed Lakeside Memorial Lawn Crematorium project. The project site is zoned OS/P (Open Space/Public Primary Area) with an underlying zoning of OSC (Open Space and Conservation). The request of a Conditional Use Permit should be denied because the emissions will degrade the visual character of the area and mercury in the TAC have bioaccumulative and biomagnification effects on the adjacent waterway and its organisms.

The site is located adjacent to Jedidiah Smith Memorial Trail (American River Bike Trail), Lake Natoma, and a residential area. The American River Bike Trail runs adjacent to the American River and Lake Natoma. The trail and lake are used and frequented by families, tourists, and recreation visitors such as bikers, hikers, nature groups, picnickers, kayakers, and other water sport enthusiasts. The adjacent residential area is comprised of senior citizens and young families who recreate outside and have gardens and fruit trees to add to their food supplies.

I disagree with the applicant's findings in the Initial Study and Mitigated Negative Declaration, Section 9, Environmental Initial Study Checklist, Section 1 Aesthetics), the applicant determined a "Less Than Significant Impact" for letter c. In rebuttal, the proposed project does pose a "Potentially Significant Impact".

The proposed project does pose a "Potentially Significant Impact" under Appendix G, CEQA, 1 Aesthetics, letter c. Appendix G, CEQA states: "c) Substantially degrade the existing visual character or quality of the site and its surroundings." The emissions of a crematorium will

significantly degrade the visual character of the area in respect to both the recreation users/visitors of the American River Bike Trail, Lake Natoma and the adjacent residential area.

The recreation users of the American River Bike Trail and Lake Natoma are humans who are visiting and using the area to experience the natural surroundings of a riparian habitat, which include the aesthetics of the tree lines and skyline. These “viewers” are specifically seeking a view of nature and the surrounding riparian habitat. Emissions from a crematorium will “substantially degrade” the visual quality of the area. Humans seeking scenic views and experiences with nature are especially sensitive to man-created emissions. They are even more sensitive knowing the emissions are the resulting emissions from cremating human remains.

The “visual character and quality of the site” will also be substantially degraded for the residents in the adjacent area. The neighboring residents enjoy the outdoors of their backyards and enjoy the foods from their outdoor gardens. These groups of viewers will be substantially affected by seeing emissions and their awareness that they are from an adjacent crematorium while eating food from their gardens, having an outdoor dinner party, or having their children playing outdoors. The emissions from the proposed project will substantially affect their visual quality.

In Section III, Air Quality, letter c of the Initial Study and Mitigated Negative Declaration, found that “the project would not result in a cumulatively considerable increase of any criteria pollutant.” “Crematoriums are sources of air pollution including mercury emission and may cause plausibly subtle chronic health effects due to long-term low-dose exposure” (US National Library of Medicine, National Institutes of Health). More concerning than mercury emissions on chronic health of humans, is the bioaccumulation and biomagnification of mercury. As previously mentioned, the proposed site is adjacent to the American River/Lake Natoma which is the habitat for fish, bald eagles, deer, foxes, skunks, and many other wildlife. The waterway is also used for fishing. Mercury accumulates in organisms, increasing levels, which then is transferred to higher-level organisms such as fish, bald eagles, and humans. The Initial Study and Mitigated Negative Declaration did not address the bioaccumulation and biomagnification effects of mercury emissions, which is crucial since the site is next to a State riparian parkway.

Clearly, a crematorium should not be permitted next to a recreational riparian habitat and residential area. Its emissions will substantially degrade the visual quality of the area and will be a source of mercury bioaccumulation in the riparian organisms. I implore you to deny the conditional use permit for the above project.

Sincerely,

Tracy Wetzel

██████ Young Wo Circle
Folsom, CA 95630

From: [Pam Johns](#)
To: [Josh Kinkade](#)
Subject: Fwd: Lakeside Memorial Lawn Crematorium
Date: Monday, February 7, 2022 12:32:47 PM

Hi Josh. Please see attached comment.

From: City of Folsom - Website Admin <webmaster@folsom.ca.us>
Sent: Saturday, February 5, 2022, 2:19 PM
To: Pam Johns
Subject: Lakeside Memorial Lawn Crematorium

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Message submitted from the <Folsom, CA> website.

Site Visitor Name: Tracy Wetzel
Site Visitor Email: [REDACTED]

I am writing in opposition to the proposed Lakeside Memorial Lawn Crematorium project. The request of a Conditional Use Permit should be denied because the emissions will degrade the visual character of the area and mercury in the TAC have bioaccumulative and biomagnification effects on the adjacent waterway and its organisms.

The site is located adjacent to Jedidiah Smith Memorial Trail (American River Bike Trail), Lake Natoma, and a residential area. The American River Bike Trail runs adjacent to the American River and Lake Natoma. The trail and lake are used and frequented by families, tourists, and recreation visitors such as bikers, hikers, nature groups, picnickers, kayakers, and other water sport enthusiasts. The adjacent residential area is comprised of senior citizens and young families who recreate outside and have gardens and fruit trees to add to their food supplies.

August 10, 2021

Historic District Commission

Re: Proposed crematory at Lakeside Memorial Lawn

Dear Historic District Commission,

I am writing to request for your opposition to the proposed conditional use permit to install and operate a crematorium at Lakeside Memorial Lawn.

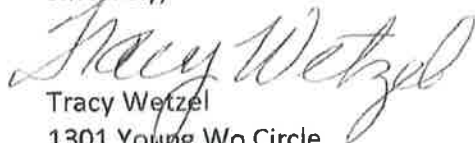
The crematorium will emit air pollutants that include toxic air contaminants such as trace metals and organic compounds. Emission and deposition of these air pollutants (even at low levels) can have accumulative adverse effects. They have the potential to cause cancer and other deleterious effects to both the human and wildlife populations and the land and water in the American River Parkway.

Lakeside Memorial Lawn is located next to a residential area and the American River Parkway. The residential area that is next to the proposed site contains families who are elderly, with young children, and pregnant women. These families are particularly vulnerable to pollutants. The American River Parkway that is adjacent to the site is used for outdoor recreation requiring physical exertion such as biking, running, hiking, kayaking, paddle boarding, etc. These activities require an increased respiration rate.

A crematorium belongs in an industrial area away from residential and recreational areas and riparian habitats. I implore you to oppose the approval of the crematorium conditional use permit.

Thank you for your consideration.

Sincerely,



Tracy Wetzel
1301 Young Wo Circle
Folsom, CA. 95630
tracymex@hotmail.com

August 10, 2021

Historic District Commission

Re: Proposed crematory at Lakeside Memorial Lawn

Dear Historic District Commission,

I am writing to request for your opposition to the proposed conditional use permit to install and operate a crematorium at Lakeside Memorial Lawn.

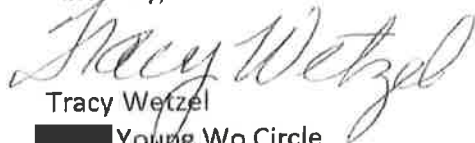
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Lakeside Memorial Lawn is located next to a residential area and the American River Parkway. The residential area that is next to the proposed site contains families who are elderly, with young children, and pregnant women. These families are particularly vulnerable to pollutants. The American River Parkway that is adjacent to the site is used for outdoor recreation requiring physical exertion such as biking, running, hiking, kayaking, paddle boarding, etc. These activities require an increased respiration rate.

A crematorium belongs in an industrial area away from residential and recreational areas and riparian habitats. I implore you to oppose the approval of the crematorium conditional use permit.

Thank you for your consideration.

Sincerely,



Tracy Wetzel

██████ Young Wo Circle
Folsom, CA. 95630
████████████████████

Josh Kinkade

From: Kelly Mullett
Sent: Monday, September 27, 2021 8:32 AM
To: Josh Kinkade
Subject: FW: proposed cremation facilities in Folsom

Kelly Mullett

Administrative Assistant

Community Development Department
50 Natoma Street, Folsom, CA 95630
O: 916.461.6231
F: 916.355.7274



www.folsom.ca.us

From: Marilu Craig [REDACTED]
Sent: Sunday, September 26, 2021 1:00 PM
To: Kelly Mullett <kmullett@folsom.ca.us>
Subject: proposed cremation facilities in Folsom

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Historic District Commission Members,
Thank you for your time in considering my email to you.

I am writing in regard to the,
Noburningbodies.com campaign.

Evidently it is clear evident that no one on this committee has bothered to avail themselves in going to a cremation site to educate themselves about the process and what it involves.

One only has to ask for an appointment to go through their facility:

Affordable Cremation & Funeral Center

4750 Beloit Drive Sacramento

CA 95838

916-432-8443

To see clearly that the horrific picture captured at the top of a flyer is ludicrous and misleading. It resembles more of a Auschwitz picture than what is. I realize that to gain a point through fear, loathing and preying on the lack of knowledge in cremation, one would perhaps buy into their tactics to gain favor and a vote against the proposed facility.

The facility that I enclosed is one of the most clean, no odor, no smoke, respectful facilities in the chain of their business'.

First and foremost in cremating the deceased, there are nine different levels of filters one must go through. There is nothing that tosses any ash from the remain, any odor, any smoke, anything that could pollute the surrounding community at large.

It is clear that the " noburningbodies" campaign is one sided and a detriment to the community at large. It is a personal point of view that is uneducated, judgmental, and a prevarication.

Dental amalgam is a concern for those who may not know that the dental fillings can and will be removed before cremation at the request of who ever it is that is responsible for the deceased.

All avenues that are of a concern can be addressed with some inquiry to properly trained persons in a position to educate and answer all concerns.

It is my personal opinion that the plans for a crematorium proceed and be of benefit for the community at large.

Thank you for your time in reading this.

Appreciated

Jobekah and Deino Trotta

[REDACTED]

Sent from [Mail](#) for Windows

From: [Steven Banks](#)
To: [Josh Kinkade](#)
Subject: FW: PLEASE VOTE NO ON THE LAKESIDE CREMATORIUM
Date: Friday, February 4, 2022 10:51:39 AM
Attachments: [Outlook-qj05gxdb.jpg](#)

FYI

From: Victoria Foster <vfoster@interorealestate.com>
Sent: Friday, February 4, 2022 10:51 AM
To: Steven Banks <sbanks@folsom.ca.us>
Cc: Victoria Foster [REDACTED]
Subject: PLEASE VOTE NO ON THE LAKESIDE CREMATORIUM

You don't often get email from [REDACTED] [Learn why this is important](#)

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Dear Steve,

I am writing to you to get your SUPPORT to DECLINE the proposed Lakeside crematorium in the Historic Folsom district.

As the cemetery name eludes...it's lakeside and not the area to put a crematorium. I strongly oppose this crematorium being built inside such a small neighborhood, next to historic landmarks, in and around adjoining open space, bike trails and the lake. **Please!!! We need your support on stopping this crematorium in HISTORIC FOLSOM.**

I live at [REDACTED] Young Wo circle and am very concerned about the potential FIRE HAZARD propane tanks can cause if there was an explosion. Getting out of this neighborhood in an emergency situation would be horrific. The roads are narrow on Forrest Street and fire engines would have trouble coming through with all residences and the Vets from the Veteran's Hall trying to escape any fire. The light rail at the intersection of Forrest Street and Folsom Blvd runs every 15 minutes and backs up traffic leaving the neighborhood as it is and that alone often causes congestion. The trees and brush along the river gets very dry and we already have had extreme scares of fires from careless people starting fires along the river. There is NO GUARANTEE these propane tanks can be safe and never have an explosion and start a fire.

I'm concerned for HEALTH reasons of the dangerous particular matter that would be released into the air, plus the smell and the atrocious look. I have severe asthma and i don't want further respiratory issues, I also have a very strong sense of smell and i don't want to feel trapped in my home. I have children living with me in their twenties, plus i also watch my 2 year old grandson and newborn grandson. i fear for lasting known and unknown side effects that they could be exposed to, besides my fellow neighbors and anyone else enjoying the trails and lake. Several running clubs and bike clubs and use the bike trail consistently, plus the river is filled with other sport enthusiast like fisherman, paddle boarders, kayakers and that would be unsafe for their health also.

We also have the affect it can have on WATER and WILDLIFE.
Our PROTECTED BALD EAGLES HAVE A NEST directly across the river from the proposed crematorium.

I'm concerned for the REGULATION of how often they would cremate. The studies were done for a few cremations a day. You know they would need to up that number to make a profit and

that's what this business is about. Who would monitor them daily to make sure they aren't going above the regulated amount? You know they will- it's about making a profit to a business!

This area is designated OPEN SPACE and in the Historic district. If a crematorium is needed, this company needs to find a suitable "industrial area " away homes & schools where they can run a business efficiently without harming the residences and wildlife of Folsom. They applied for this permit and were denied and the homeowners here had to take time and resources to fight this proposal before and it was declined. How are they allowed to keep coming back- the Historic Area & Open space has not moved

Lastly, I am a local Realtor and it will DECREASE OUR HOME VALUES in the Preserves/Lake Natoma Shores. Property prices will be affected which in turn means LOWER TAX REVENUE for the city. Is there a crematorium tax on each cremation? and if so, would it even match the difference in property taxes for 150 plus immediate surrounding homes?? No one wants to live by a crematorium so close by where you can smell, see and feel its presence. It will definitely need to be disclosed on a seller's property disclosure forms for any home sales and will cut out a large number of potential buyers for this neighborhood. I have 3 clients that want to buy in this neighborhood- two of them will not

reside here if a crematorium is built for reasons listed above and the my 3rd client Liz Chighizola has stated to me, she would want the crematorium to be put in because home prices would than drop here and then she would finally be able to afford this neighborhood. Really?? How is it ok for this business to decrease our home values?? Residences bought homes knowing a historic cemetery was here- we did not agree to hurt our health, environment and property values when purchasing our HOMES next to a crematorium that was not even in existence and might have swayed our investment in a home here.

Please be our voice and VOTE NO, STOP and OPPOSE the Lakeside crematorium. Voting No and stopping this is what is right for the families in the City of Folsom.

WE THE LIVING RESIDENCES ARE WHO MAKE UP FOLSOM TODAY AND WE SHOULD HAVE PRIORITY, NOT THE DECEASED.

Lakeside needs to find an industrial area not in historic and open space to expand their business and profits. This will make it a win win for everyone in Folsom.

Thanks for your time and hoping your assistance.

Best regards,
Victoria Foster. [REDACTED]

[Click here to see what my clients have to say about me!](#)



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From: [Elaine Andersen](#)
To: [Josh Kinkade](#)
Subject: FW: Please stop the proposed Lakeside crematorium
Date: Thursday, October 14, 2021 4:29:32 PM
Attachments: [Outlook-5fwqerzn.jpg](#)

From: Victoria Foster [REDACTED]
Sent: Thursday, October 14, 2021 4:15 PM
To: Elaine Andersen <eandersen@folsom.ca.us>
Cc: Victoria Foster [REDACTED]
Subject: Please stop the proposed Lakeside crematorium

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Dear Elaine,

I am writing to you to get your support to decline the proposed Lakeside crematorium in the Historic Folsom district.

As the cemetery name eludes...it's lakeside and not the area to put a crematorium. I strongly oppose this crematorium being built inside such a small neighborhood, historic landmarks, adjoining open space, bike trails and the lake. We need your support of stopping this bad decision.

I live at [REDACTED] Young Wo circle and am very concerned about the potential fire hazard propane tanks can cause if there was an explosion. Getting out of this neighborhood in an emergency situation would be horrific. The roads are narrow on Forrest Street and fire engines would have trouble coming through with all residences and the Vets from the Veteran's Hall trying to escape any fire. The light rail at the intersection of Forrest Street and Folsom Blvd runs every 15 minutes and backs up traffic leaving the neighborhood as it is and often causes congestion.

I'm concerned for health reasons of the dangerous particular matter that would be released into the air, plus the smell and the atrocious look. I have severe asthma and i don't want further respiratory issues, I also have a very strong sense of smell and i don't want to feel trapped in my home. I have children living with me in their twenties, plus i also watch my 2 year old grandson and i fear for lasting known and unknown side effects that they could be exposed to besides my fellow neighbors and anyone else enjoying the trails and lake. Several running clubs and bike clubs use the bike trail consistently and that would be unsafe for their health also.

I'm concerned for the regulation of who and how often they would cremate. The studies were done for a few cremations a day. You know they would need to up that number to make a profit and that's what this business is about. Who would monitor them daily to make sure they aren't going above the regulated amount?

This area is designated open space and in the Historic district. if a crematorium is needed, this company needs to find a suitable "industrial area " away homes & schools where they can run a business efficiently with harming the residences of Folsom.

Lastly, I am a Realtor and I realize it might not be any concern of yours or anyone's unless they live in the Lake Natoma Shores, but it will affect our home prices. No one wants to live by a crematorium so close by where you can smell, see and feel its presence. It will definitely need to be disclosed on a sellers property disclosure form for any home sales and will cut out a large

number of potential buyers for this neighborhood.

Please be our voice and vote NO, STOP and OPPOSE the Lakeside crematorium. Voting No and stopping this is what is right for the city of Folsom. Lakeside needs to find an industrial area to expand their business and profits and make it a win win for everyone in Folsom.

Thanks for your time and hoping your assistance.

Best regards, Victoria Foster. [REDACTED]

[Click here to see what my clients have to say about me!](#)



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To the Folsom Historic Commission,

This letter is in regards to the vote towards the application for a Crematory being built at the Lakeside Memorial Lawn Cemetery. I am asking you to protect our community with your vote of no, against having a crematory in our residential neighborhood.

Here are a few examples of what will happen to our air quality to our neighborhood and communities in our area when burning human remains. There will be toxic emissions associated with contaminants such as mercury amalgum dental fillings, organohalogens and other toxins which may also come from breast implants that contains toxic chemicals like PVC, Methylene Chloride. Let's not forget the other types of medical implants that may be left behind before cremation.

Studies has shown the risks of still birth was 4% higher as well as the high risk of a brain abnormality anencephalus among babies whose mothers lived near a crematorium. Fumes from a crematorium are potentially harmful and should not be located so close to residential neighborhood's where harmful toxins will be inhaled deep into our lungs tissues on a daily basis. We ask that you protect our families and our community and vote no to the Crematorium being built at Lakeside Memorial Lawn Cemetery.

Respectfully,



Wendy Yezzi 08/11/21

1102 Fong Court

Folsom, CA 95610

To the Folsom Historic Commission,

This letter is in regards to the vote towards the application for a Crematory being built at the Lakeside Memorial Lawn Cemetery. I am asking you to protect our community with your vote of no, against having a crematory in our residential neighborhood.

Here are a few examples of what will happen to our air quality to our neighborhood and communities in our area when burning human remains. There will be toxic emissions associated with contaminants such as mercury amalgum dental fillings, organohalogens and other toxins which may also come from breast implants that contains toxic chemicals like PVC, Methylene Chloride. Let's not forget the other types of medical implants that may be left behind before cremation.

Studies has shown the risks of still birth was 4% higher as well as the high risk of a brain abnormality anencephalus among babies whose mothers lived near a crematorium. Fumes from a crematorium are potentially harmful and should not be located so close to residential neighborhood's where harmful toxins will be inhaled deep into our lungs tissues on a daily basis. We ask that you protect our families and our community and vote no to the Crematorium being built at Lakeside Memorial Lawn Cemetery.

Respectfully,



Wendy Yezzi 08/11/21

██████ Fong Court

Folsom, CA 95610

Petition summary and background: Lakeside Memorial Lawn has applied for a conditional use permit to install and operate a crematorium on designated open space. This has significant negative implications for the physical, environmental and fiscal health of our community. **Action petitioned for:** We, the undersigned, are highly concerned citizens who urge our leaders to act now to vote 'NO' on the permit application.

Date Submitted	Name.first	Name.last	Contact Information	Comment
08/20/2021 09:14pm	Tim	McGarry		Let's keep open space open and put a crematorium in an Industrial area where it belongs.
08/21/2021 05:50am	david	higgins		Attempted in 2002 - 2003, then withdrawn. What's different now??? Still not a good idea,
08/21/2021 11:32am	Dan	Sugars		No Crematorium
08/21/2021 02:42pm	Emily	Romine		No on the crematorium
				<p>prepared by the applicant, not the Lead Agency (City of Folsom) Title 14 CCR Section 15365. The applicant's IS/MND does not consider all phases of project planning, implementation and operation, Title 14 CCR Section 15063 (a) (1). The following were not properly identified or evaluated:</p> <p>The applicant has applied for a Conditional Use Permit (CUP) to service new religious funerary needs that are in conflict with the established historicity of some of the religions and funerary rights of those already interred. The earlier Lakeside Memorial's 2002-2003 CUP crematorium application was responded to by the Chinese community in regard to cremation ashes and smoke stack effluent being allowed to escape into the air and settle on nearby graves of those interred at the Chung Wah and Young Wo cemeteries, 01.15.03 Historic District Staff Report: letter and petition by June Chan, pg 68, "In order to maintain preservation of the circa Gold Rush cultural, religious, and archaeological special features."</p> <p>The applicant's IS and MND do not identify significant Chinese cultural resources on the National Register of Historic Places, the CA Registry of Historic Points of Interest and the Sac County Cemetery Commission's Registry of Pioneer Cemeteries, Title 14 CCR 15125 (c). The integrity of these culturally unique, historic cemeteries are of importance to a specific time period during the building of Folsom, the State of California, and the opening up of the West, Title 14 CCR 15064.5 (a) (1-3 (A-D)) The addition of new religious practices can directly alter historicity, and indirectly alter historical perceptions of spiritual worship - forcing an unwanted social change in present day spiritual and religious practices, Title 14 CCR 15131 (b) and (c).</p> <p>The IS/MND missed the 2013 Folsom Community Wildfire Protection Plan signed off on by the CA State Dept of Forestry, the City of Folsom and the Folsom Fire Safe Council that identify the crematorium area as a High Risk Wildfire hazard probability, CWPP, pg 27, Title 14 CCR 15126.2 (a); the IS/MND have no water utility lines, or hydrant installations planned to the crematorium site or cemeteries; the crematorium's two 250 gal propane design is not fire hardened in the event of wildfire, Title 14 CCR 15126.2 (d); the dirt road into the crematorium site does not meet Folsom Fire regulation width in case of emergency, Title 14 CCR 15359, and I vote no on the permit application.</p>
08/21/2021 05:51pm	Deborah	Grassl		
08/21/2021 08:30pm	Megan	Bunce		

				<p>This is NOT the right location for a facility like this !</p> <p>There is a reason this neighborhood is not zoned for Commercial use - of any kind. It should definitely not be used for commercial use that is potentially hazardous !</p>
08/22/2021 11:45am	Darrell	Jenkins		Let's simple ask these people to move the crematorium.
08/22/2021 01:25pm	Stephanie	Nelson		We are residents of Historic Folsom and are concerned with the crematorium.
08/23/2021 12:02pm	Katie	Musfelt		Vote No on the historic Folsom crematorium at Lakeside Memorial.
08/23/2021 12:03pm	Adam	Musfelt		Vote No on the historic Folsom crematorium at Lakeside Memorial.
08/23/2021 12:38pm	Barbara	French		Strongly recommend this not be allowed to happen.
08/23/2021 12:40pm	Jack	French		Recommend a NO PASS of this permit
08/23/2021 01:41pm	Donna	Campagna		vote no please
08/23/2021 01:42pm	Mark	Campagna		vote no please.
08/23/2021 03:33pm	Andrea	Norris		No crematory
08/24/2021 10:09am	Leland	Wilson		No way !
				I have lived in Historic Folsom for 10 years and my son has severe asthma. I DO NOT want him to be breathing in polluted air. NO crematory should be allowed in our backyards of Historic Folsom. Our community has worked hard to persevere this unique area as a draw for social community gatherings.
08/24/2021 10:27am	Jessie	Kasko		No crematorium at Lakeside Memorial
08/24/2021 11:40am	Jean	Wiggin		We may need one BUT NOT in historical downtown Folsom.
08/24/2021 11:49am	Robert	Wiggin		NO HISTORIC FOLSOM CREMATORIUM AT LAKESIDE MEMORIAL.
08/24/2021 12:38pm	Mike	Cuevas		I vote NO on this permit application. I live two streets away and do not want this in our neighborhood.
08/24/2021 12:46pm	Liane	Powell		contact me
08/25/2021 12:17pm	gary	mcfarland		Voting NO ?
08/25/2021 03:04pm	Regina	Charette		We don't want a disgusting crematorium in the heart of beautiful Folsom!
08/25/2021 03:33pm	Tara	Munier		I would like to share my support in the opposition of the Lakeside Crematorium application and development. The Lake Natoma, Murer House, trails, and neighborhood is impacted by this decision. With the drought and risk of losing precious nature preserve areas, I would like to implore local government officials to oppose the crematorium at this location while leveraging alternative solutions. There are alternative retail sites/ sites that are vacant with no nature preserve, athletic, or residential impact. Please consider the alternative sites with no negative impact.
08/25/2021 09:25pm	Anne	Bentz		I am a resident of Sibley Street and the thought of living so close to a crematorium is depressing. Please reconsider another area for the construction of a crematorium as with the vastness of California I'm sure there are other more viable, less populated options.
08/26/2021 01:51pm	Michelle	Domingo		Please keep Historic Folsom protected from this crematorium.
08/26/2021 01:52pm	Alexander	DiStefano		No more ash in the air. Especially with all the fires lately. The land that is zoned historic and should stay pristine.
08/26/2021 06:34pm	Troy	Drennon		Troy Drennon

08/26/2021 10:29pm	Carolyn	Barker		I have lived in Old Town Folsom for 40 years. I have lost my Son at 34. Lost my Husband of 52 years. Both were cremated. But I cannot imagine being able to smell the stuff here where I live. Please find another open space., like the old dump sites that were here. Might cost you more money. Please think about this. Thank you for your time. Carolyn
08/27/2021 10:17am	Jerald	Lewis		I do not want a crematorium only a few blocks from my house.
08/27/2021 07:36pm	Paul	Zenak		As I live just 2 blocks away on Figueroa Street, I'm appreciative of your activism.
08/28/2021 07:38am	Darla	Dunn		Please do not allow a crematorium in Historic Folsom. I live in the historic district. We have enough smoke from all the natural fires and don't need or want year round crematory smoke. This is detrimental to us personally and for the city of Folsom. Please stop additional pollutants to our town.
08/28/2021 11:49am	Jenelle	Hallock		Voting No!
08/28/2021 06:47pm	Dlane	Colgan		No to a crematorium in Folsom. Our air quality here is worse than other areas in Sacramento as smoke gets trapped here and cannot get over the hill. The same thing happens with the fog in the winter and it settles in Folsom and does not lift as it is trapped. Unhealthy air in Folsom and we do not need anything that will add to it.
08/28/2021 07:53pm	Amanda	Watson		No thank you!! Keep our air clean! We get enough smoke with the fires!
08/30/2021 09:39am	Benjamin	Gamache		To whom it may concern: As I sit here writing this, I look outside to smoky skies and an ash-covered deck. Adding more smoke and particulate matter to this already impacted community must not happen. Historic Folsom is a quaint, family-oriented community that must remain preserved at all costs. Please vote NO on the permit application.
08/30/2021 08:57pm	Lori	Molitor		Sincerely, A Concerned Neighbor
09/01/2021 12:14am	Denise	Shuker		It's bad enough with fire season. Don't add more pollution to the air year round. This is a residential area near nature trails. This does not belong here.
09/02/2021 04:35pm	Robert	Yost		I have lived in Folsom on Forrest Street for nearly 24 years and do not want a crematorium in our little neighborhood.
09/03/2021 10:31am	Jill	Huckaby		The reasons are many; possible harm to the air if there is a problem with the process. I have had asthma most of my life and fear the potential for harm to my already sensitive lungs; possible harm to the eco-system in Lake Natoma and the American River Parkway; and if a spark escapes from the chimney stack or the 500 gallon propane tank explodes, it would be
09/03/2021 11:16am	Angelica	Galeana		My vote is no.
09/03/2021 11:30am	Jeremy	McNeill		Please NO cremation should occur in a neighborhood like this. Take it outside of residential neighborhood please!!!!
09/03/2021 11:41am	Allison	Hull		I vote NO on the Folsom Crematorium!
09/03/2021 12:41pm	alexia	hughes		N/A
				I work across the street from this location and am not ok with the idea of breathing potentially toxic fumes let alone the smell. Please consider a commercial location away from businesses and residential areas.
				we want clean air

09/03/2021 01:43pm	Michelle	Lozano	NO
09/03/2021 02:25pm	Kathryn	Ruiz	no on crematorium
09/03/2021 02:26pm	Kimberly	Shepherd	I feel like it needs to be in a commercial area Not next to restaurants, and small businesses
09/04/2021 12:41am	Nicole	Alexander	No
09/04/2021 07:06pm	Steve	Croft	No on the permit application. Keep our air clean and avoid the eyesore of this proposed crematorium in the historic district.
09/06/2021 07:20pm	Cassandra	Olson	Highly object to the installation of a crematorium due to public health issues.
09/06/2021 07:58pm	Susan	Lubiens	Vote NO!!!
09/06/2021 09:02pm	Siue	Railsback	No way. I live right across the lake from this. Furthermore, we have nesting Bald eagles each year in this very vicinity. The toxicity is probably harmful to our national treasure.
09/06/2021 09:48pm	Stephanie	Chandler	Stop the pollution
09/06/2021 10:17pm	Denise	Mullenax	I oppose a Folsom Crematorium! City planning has made many mistakes in the last 20 years. This is one that needs to be stopped.
09/06/2021 10:28pm	Laura	Basini	I have serious concerns on health and economic grounds.
09/06/2021 10:45pm	John	Iniguez	I am totally against allowing a crematorium to be build at the Lakeside memorial. There is already one on Greenback Lane near near Fair Oaks Blvd, This business is one that needs to be removed from any neighborhoods or within wind drift of neighbors. We have had a bridge with much traffic, and light rail added, and a flight path change that puts endless planes overhead. This area has already sacrificed, we should not have this business added as well. It will create too much noise, when they are started, mostly likely would run when electricity is cheaper(night?), be bad for air quality, remember we are already adding exhaust from cars to our area, and the smell is not good when you are near. This is not a good fit to neighborhoods that have already sacrificed much in quality of life.
09/06/2021 10:52pm	Julle	West	
09/07/2021 05:16am	Clarence	Macaspac	No to pollution
09/07/2021 06:18am	Leslie	Wynn	Please do not add more particulate matter to our air by allowing a crematorium to be put in, anywhere in Folsom. They need to find a place that is farther away from developed areas. Let's put this in a less populated area!
09/07/2021 06:48am	Cecelia	Cutler	
09/07/2021 07:43am	Margaret	Weaver	I oppose a crematorium in the Folsom Historic District and would encourage Lakeside to promote legislation for human composting instead. This third option is environmentally
09/07/2021 07:47am	Michael	Poncin	No, not in historic Folsom
09/07/2021 07:52am	Patricia	Foulk	Air quality should not be polluted.
09/07/2021 07:55am	Lauren	Britt	I am against the Lakeside Memorial Crematory.
09/07/2021 09:37am	Kathleen	Kenna	With the poor air quality lately due to fires, a crematorium should not even be a consideration.
09/07/2021 10:17am	Joanna	Stanfield	ABSOLUTELY NO crematorium at Lakeside Memorial
09/07/2021 10:19am	Christine	Gorman	NO crematorium please!!!
09/07/2021 10:20am	Thomas	Rasmussen	Let the people speak - we do not need or want a crematorium in a residential area. Vote "NO" as your constituency want!!


			<p>Hello,</p> <p>This was brought to my attention and I'd like to comment on it.</p> <p>First of all I'd like to say that I am pro business but I'm also pro community. I have a huge concern about the vapors and exhausts being pumped into the air and likely settling into Lake Natoma. The location is so close to this precious lake, the paddle borders, Sacramento State Rowing team, and the wild life. I'm sure we have done the environmental impact report, but I'd make sure that report include to biological impact on the water ways; specifically on Lake Natoma, the over flow into the American River and the effects on the fish that swim up river to lay their eggs at the Hatchery (on Hazel).</p> <p>I think it's best that this special use permit be denied. At least until all of these factors can be thoroughly worked through.</p> <p>This is not the location for a Crematorium. The negative implications on the Physical, Environmental and Fiscal Health of our community cannot be properly accounted for at this time. Nor can the actual impact be mitigated where the it is less than 20%.</p> <p>The residual ashes and gases alone will impact not only the American River Parkway/Natomas-Negro Bar area but the homes located to the NW of the proposed site. It should be noted that the majority of these homes are located significantly higher than the site for planning or in lower impact areas when there are periods of inversion.</p> <p>This is not in the best interest of the community.</p> <p>Thank You</p> <p>David D Sirsi</p> <p>Please keep our air as clean as possible, it's already bad enough with fire season.</p> <p>No, No, No!! On the permit for a crematory at the preserve in Folsom!!</p> <p>voting no</p> <p>I vote NO on the Lakeside Memorial Lawn Crematorium.</p> <p>Please don't allow a crematorium at this site</p> <p>My home is in line as to where the smoke would probably go (American River Canyon) And I am VERY sensitive to air quality.</p> <p>Thank you in advance for your careful consideration in not allowing this to go forward.</p> <p>No for permit</p> <p>No for permit</p> <p>My family does not want this crematorium here in Folsom. Put it on the other side of the freeway where Aerojet was. Why have it so close to homes? What are you guys thinking?????</p> <p>VOTE "NO" ON THE PERMIT APPLICATION. It will cause our air quality to be worse than it already is and it will drive down our home values.</p> <p>Please no</p> <p>Please no</p>
09/07/2021 10:34am	Levin	Hughes	
09/07/2021 12:15pm	David	Sirsi	
09/07/2021 12:28pm	Kathleen	Gately	
09/07/2021 02:06pm	Mary C	Meden	
09/07/2021 02:39pm	Judith	Bates	
09/07/2021 03:46pm	Monique	Green	
09/07/2021 05:01pm	Debby	Alberti	
09/07/2021 05:47pm	Joni	McColloch	
09/07/2021 05:47pm	Joni	McColloch	
09/07/2021 06:17pm	Janis	Kuwamoto	
09/07/2021 07:29pm	Hannah	Sanders	
09/07/2021 09:39pm	Michelle	Lu	
09/07/2021 09:40pm	Harry	Lu	

			<p>In the ten years that the Lakeside Crematorium has been proposed, I have not seen nor heard one good reason to build or operate it next a designated open space preserve; especially when there are industrial areas within a few miles of the proposed crematorium.</p> <p>These other areas are well-suited for commercial activities and are zoned for businesses that could create potentially toxic emissions and waste, noise, and other distractions unsuited to residential and historic neighborhoods.</p> <p>In all seriousness, may I suggest a location near the Keifer landfill? Perhaps there's a place on or near it that would be a safe location away from population centers for a crematorium. The last time I was out that direction, there was plenty of open space and I see no reason that a crematorium could not discreetly fit into that area -- I know it sounds disrespectful at first thought but it's just a stop - not a long term stay... maybe it could be called the Sloughhouse Facility..</p> <p>Please carefully consider the ramifications of this decision. Putting a Crematorium so near to homes, schools and wild areas is a bad idea and it is much harder to undo than NOT do.</p> <p>Please say NO to the Crematorium as currently proposed.</p> <p>Regards, Charlotte Bryant</p>
09/09/2021 08:32pm	Charlotte	Bryant	
09/09/2021 09:46pm	Christine	Vangelatos	<p>Please vote 'no' on the permit application. PLEASE VOTE "NO" ON THE PERMIT APPLICATION.</p>
09/11/2021 09:48am	Barbara	Barnett	<p>Seeing smoke coming from a crematory when enjoying our bike trails, kayaking on Lake Natoma or just driving down Folsom Boulevard would be depressing, especially for us older people who have lost so many close friends and family and no that's going to be us in that</p>
09/11/2021 02:08pm	LAURA	BROWN	<p>This type of establishment belongs in an industrial area not across the street from the hub of Folsom street fairies, farmers markets, restaurants, stores concerts & more that welcome tourists to visit Folsom. To see smoke from the crematory up in the sky while enjoying the bike trail or shopping , eating on sutter street is an affront. Tourism is crucial to the city of Folsom. There is a housing development that lives this area. They already have the cemetery. To put a crematorium near those people who live across from the cemetery is absolutely horrible. This is a bad idea. Move somewhere else.</p>
09/11/2021 06:04pm	Andrew	Cherniski	<p>I depend on the trail system in this area to improve and maintain my health. The addition of this crematorium will destroy the recreational experience that Folsom has worked hard to develop over the years. If this happens, I will be spending my recreation dollars elsewhere.</p>
09/11/2021 10:55pm	Barbara	Storm	<p>Please reject this proposal.</p>
09/12/2021 11:33pm	Lisa	Bracero	<p>Please don't allow a crematory to operate here!</p>

09/07/2021 09:54pm	Isaac	Monical	do to profit over the safety and wishes of a community.
09/07/2021 10:15pm	Mary	Gates	oppose the crematorium!
09/07/2021 11:45pm	Sara	Adams	do historic folsom crematorium at lakeside memorial
09/08/2021 07:16am	Jeff	Cruit	VERY Strongly opposed to a crematorium at this location
09/08/2021 09:22am	Vicki	Romani	do crematorium here, please do not for all of the obvious reasons. Let them line their pockets elsewhere!
09/08/2021 11:48am	Bonnle	Sirsi	Please vote no
09/08/2021 12:36pm	Terence	Gates	oppose having a crematorium built
			Our deceased relatives (parents) have been placed at this cementing domain, and Er are both strongly against any crematorium in Folsom ca. Near the parkway is no suitable site, nor anywhere close to Folsom. There are enough existing crematoriums within driving distance for cremations, and under no circumstances should this additional Folsom area site be permitted. Thank you.
09/08/2021 03:05pm	William/Dree	Bleay	William B Bleakly and Dree R Bleakly
			NOTE: NO! ON THE PERMIT APPLICATION. It will make our air quality even worse and drive our home values down!
09/08/2021 05:28pm	Hannah	Sanders	reflections neighborhood and I say no I don't want it. There's already enough pollution in this
09/08/2021 05:31pm	Catherine	Tenner	Please do not build a crematorium at Lakeside Memorial.
09/08/2021 10:43pm	Kristine	Wells	
09/09/2021 07:47am	Kimberly	Johnson	
09/09/2021 08:45am	Aurelio	Merlos	NO crematorium in lakeside, close to living areas and natural reserves.
09/09/2021 08:59am	Steve	Holden	oppose this in a residential area.
09/09/2021 09:10am	Karen	Pardieck	would hate to be eating outside at Karen's Bakery it SCOTTS and smell the smoke as it wafts over from the crematorium. Folsom does not need this!!!
09/09/2021 09:14am	Genevieve	Bergman	Voting NO on the permit application for the Lakeside Memorial Lawn Crematorium.
09/09/2021 09:38am	kim	roberts	Do not pollute Folsom. Over building is causing enough problems.
09/09/2021 12:01pm	Octavio	Sanchez	NOTE NO
			This is not an acceptable place for a crematorium. I saw all the signs in the neighborhood today. can't even imagine how terrible the smell would be and knowing what it is from. How can Folsom think this is ok?
09/09/2021 07:41pm	jack	baker	

09/13/2021 09:15am	Linda	Johnson	a crematorium should be in an industrial area where there are no homes, children, aging adults, pets.
09/14/2021 12:36am	Rochelle	Jacob	No crematorium in a residential area
09/14/2021 12:27pm	Megan	Shill	It's absurd to put one near homes like this. Not only is it not healthy, the city is forcefully decreasing the value of residents homes.
09/15/2021 07:19pm	Linzl	Miner	Keep historic air clean!
09/16/2021 11:42am	Victoria	Brown	Preserve our clean air!
09/17/2021 02:06pm	Susan	Rivieccio	This is not the area to place a crematory.
09/17/2021 02:08pm	Nicki	Rivieccio	I am against having this done without community input.
09/18/2021 11:08am	Allison	Knight	I say no to the crematorium and YES to keeping our air clean.
09/19/2021 04:24pm	Christine	Pocock	Voting NO on the crematory!!!
09/19/2021 04:28pm	Tyler	Rawles	No on the crematory!!! This would ruin the historic district!
09/19/2021 04:29pm	Christine	Rawles	Voting NO on the crematory, this would ruin our neighborhood. It would be terrible for the river, trails, and lake! Not to mention a total fire hazard!
09/20/2021 09:20am	Jennifer	Kelly	I would like to see Folsom protected against the Lakeside Memorial Lawn Crematorium.
09/20/2021 09:23am	Ryan	Kelly	For the sake our community, please no Historic Folsom crematorium at Lakeside Memorial.
			I am voting NO on the crematory. I am in my 60s and wanted to retire here in our town of Folsom. This crematory would be very bad for people like myself who enjoy walking, SUP, biking, listening to music. Our community here is special. PLEASE do not destroy our area for older people and younger children who enjoy being outside. Go someplace else where it is accepted. Ruth McGuire
09/20/2021 06:20pm	Ruthmarie	McGuire	
09/20/2021 08:48pm	Bruce	Fields	I do not want a crematorium in this beautiful area.
			I have lived at Folsom Village @ [redacted] Oakdale St. for over 5 Years. I'm a senior as most of the other residents in the park. Many of whom have respiratory problems. I am very much against the property across the street building a crematorium.
09/25/2021 03:27pm	Nancy	Thornton	This site is too close to homes and recreational areas.
			This is gross. Please do not put that here as that would ruin the air quality and hurt the community around it. Propane tanks next to the river and trees is a horrendous idea. We love this area and do not want it ruined. Not safe at all
09/26/2021 01:08pm	Chris	Bernardi	This is proven to be unsafe near wooded area and irresponsible creating air pollution in a designated historic site in this town.
09/26/2021 01:17pm	Garret	Foster	For the reasons given above, I don't want a Crematorium in my neighborhood. It should be outside of town, a good distance from neighborhoods. I see home owner property values going down if a crematorium was to be put this open space, (which shouldn't be allowed in the first place) who would pay a premium price for a home if a crematorium was so close to their
09/27/2021 06:04pm	Jerry	Amaral	A crematorium in very close proximity both to a residential community and the tourist attraction of historic Folsom is a terrible idea.
10/01/2021 08:26am	Alan	Gonsalves	I live in Historic Folsom & have a son with asthma. This would harm the air quality in, Folsom which makes it a health concern for our community.
10/01/2021 09:51am	Ben	Kasko	

10/01/2021 09:59am	Amy	Kunst		This should not be located anywhere near a town, especially one with such an outdoor lifestyle. This is not ok!!
10/03/2021 08:47pm	natalia	merlos		no crematorium! breathing dead people isnt helathy
10/05/2021 08:56am	Jennifer	Grattan		I reside in Historic Folsom in close proximity to the proposed crematorium site. I am in opposition to the permit and the designated location of the crematorium. I believe a crematorium belongs in an industrial zoned space and is not meant to be in a residential
10/05/2021 09:16am	Edwin	Grattan		I am 75 years old and I live within a stones throw of the selected Crematorium site. The unhealthy air produced by burning bodies is a risk to my health. I feel the Crematorium should be constructed in an industrial area.
10/07/2021 09:57am	Jessica	Birch		I oppose this crematorium. As a Jewish person whose faith forbids cremation and as someone whose community includes people whose relatives were killed by the Nazis and then burned in Crematoriums, the sight and smell of a crematorium in the middle of our beautiful historic town is disturbing.
10/12/2021 08:30am	Richard	Perez		I respect that being cremated is the choice of many after death and is the norm in some cultures, but please place the crematorium outside town. Thank you.
10/12/2021 08:41am	Sandra	Perez		Keep historic Folsom beautiful! No burning bodes!!!!
10/12/2021 08:44am	Estela	Lewis		A crematorium should be away from residential area. Keep historic Folsom clean and beautiful. The crematorium will be too close to residential and historic Folsom . Our city and the businesses will be stigmatized losing our clean air where our children play and families come to be close to nature.
10/17/2021 04:09pm	Caell	Horwith		No crematorium!
10/20/2021 08:22am	Keith	Derry		Under no circumstances would I expect a new crematorium to be constructed so near residential housing. This endeavor to build one here has been floated before and thankfully it did not happen. If one is needed find a place in the recently annexed land south of Highway 50 that can be designated for such a use. Then let homeowners not bothered by this use buy in the vicinity. To allow the cemetery to build it alongside an established small neighborhood that is obviously concerned and almost uniformly opposed would be a callous disregard of those residentsâ€™ wishes to enjoy the neighborhood they bought into
10/26/2021 07:27pm	Steve	Wetzel		Nobody should be asked to live next to a place that burns 100's of dead bodies every year. Even if you don't see the smoke you just know the air outside contains the micro particles of dead, burned people. Everyone has smelled the awful smell of burning skin or hair at one point in your life. Can you image that every day outside your home?
11/23/2021 12:26pm	Juli	Neal		Please consider relocating the site of this crematorium away from populated areas and the waterways. Thank you
12/04/2021 01:28pm	Sherry	Barron		I want to breathe clean air. Regularly ride bikes and walk the trails with my family. Itâ€™s a big NO for this crematory!
01/03/2022 08:59am	Kara	Kreutz		Crematorium should be located away from residential and natural areas. They are more appropriate in an industrial area.
01/03/2022 10:01am	Adrienne	Simpson		Vote no on the permit application.
01/10/2022 09:29am	Catherine	Reinmiller		I donâ€™t want this next to the river or tails in Folsom Please no crematory in the neighborhood. Is there open land close by that could be used for

10/04/2021 08:21am	Victoria	Foster		<p>I strongly oppose the crematorium being built in the historic district & inside a small neighborhood. I live on [redacted] Young Wo Circle and extremely concerned on the air quality impact. The air quality studies are done for a lot fewer than you know they will do. I have asthma and I don't want my health impacted. This is also zoned open space if this crematorium is so needed than it should be put in a commercial zoned development away from homes. In an industrial area. This is too close to the bike path and river where so many people exercise and enjoy what we have to offer.</p> <p>Another huge concern is fire safety- if there was an explosion and fire with the propane tanks we are in a one way in and one way our neighborhood. This company needs to build the crematory in an industrial park area not near a children's & recreational park.</p> <p>Please vote no and protect us.</p> <p>Sincerely, Victoria Foster [redacted]</p>
01/29/2022 05:01pm	Laurie	Brown		<p>I vote NO on the permit application! Super sad to see it is even being considered. As a person with severe asthma this is extremely disheartening. In July 2020 California approved alkaline hydrolysis cremation with very low (if any) emissions because it doesn't heat up to the high heat as the other uses and is water based. Anything burned in the high heat will not be good for the community...Burial caskets coated in insecticides or preservatives, things people leave in the caskets, dental amalgam fillings, presence of radioactive substances within the remains, either from devices or as a result of radiotherapy, etc. YEP....I VOTE NO!</p>

			oppose locating this crematorium in this designated open space location. Not only is it out of character with our treasured historic Folsom nearby, it would create toxic air pollutants particularly by virtue of being self regulated), and those pollutants would not only affect humans in the area but would have an affect on Lake Natoma and all of the wildlife that we reasure and that use that little jewel of a lake. Not to mention downstream pollution of the American River.
01/11/2022 06:45pm	Christine	Holmes	A better location for this facility would be near the landfill on the other side of Highway 50. An additional benefit may be that, since the landfill is heavily regulated, that may tend to affect
01/12/2022 02:54pm	Jenifer	Orr	We want to live in a community with clean air. Stop the burn!
			Please vote "No" for the conditional use permit to install a crematory at Lakeside Memorial Lawn cemetery located at Folsom Blvd and Forrest St.
01/14/2022 07:50am	Bert	Pittari	
			Please vote "No" for the conditional use permit to install a crematory at the Lakeside Memorial Lawn cemetery located at Folsom Blvd. and Forrest St.
01/14/2022 08:00am	Patricia	Zuccaro	This crematory will have a negative impact on current and future generations .
01/15/2022 06:24pm	Loretta	Poland	Please do not allow this in Folsom.
			Do not allow a crematorium!!!
01/15/2022 06:40pm	cliff	mombourquette	
01/17/2022 10:09am	Todd	Dambly	NO HISTORIC FOLSOM CREMATORIUM AT LAKESIDE MEMORIAL
			one day people will come to folsom and say[Whats historic about this place?
01/18/2022 09:22am	edward	livesay	No crematorium
01/22/2022 08:54am	kathryne	Hauser	snt there a less populated place for this lol
01/22/2022 02:18pm	Rebecca	Mendell	stop the crematory.
01/23/2022 09:23am	Heidi	Scherer	NO NO NO NO NO No No NO No!!!!
01/27/2022 02:10pm	Trevor	Moran	NO HISTORIC FOLSOM CREMATORIUM AT LAKESIDE MEMORIAL.
01/28/2022 04:08pm	Liz	Byer	As a resident of old town Folsom we definitely object to the crematorium. Summers are already becoming unbearable due to unprecedented fires. We believe this is not the time or
			The proposal to put a crematorium in this location is shocking. The impact to air quality and smell will be very detrimental to the historic district of Folsom. The historic district will be joked about as being under a cloud of death.
01/28/2022 06:17pm	Alan	Gonsalves	The health impact to local residents within very close proximity is also very impactful. How can this even be considered a good idea?
01/31/2022 04:53pm	Stacey	Meredith	Doesn't belong in residential area
02/02/2022 12:54pm	ADRIANNE	GALE	I BELIEVE IT WOULD BE TRAGIC TO ALLOW THIS PROJECT TO PROCEED . FOR THE SAKE OF A HEALTHY LIVING RESIDENTIAL AND HISTORIC AREA IT MUST NOT BE APPROVED.

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Printed Name	Signature	Address	Comment	Date	
Yvonne Bagner	<i>[Signature]</i>	[REDACTED]		Aug 4, 2021	
Randall Stoker	<i>[Signature]</i>		No!!	8/5/21	
Emma Clapp	<i>[Signature]</i>		No thank you	8/5/21	
Suzanne Lundy	<i>[Signature]</i>			5 Aug 2021	
Cally Kabe	<i>[Signature]</i>			8-5-21	
Bert Lundy	<i>[Signature]</i>			8-5-21	
Anita Rincon	<i>[Signature]</i>			8-5-21	
Debbie Paritz	<i>[Signature]</i>			8-5-21	
DIANIE EVANS	<i>[Signature]</i>			STOP THE CREMATORIUM	8/8/21
JOAN BOYCE	<i>[Signature]</i>				8/13/21
BOB EVANS	<i>[Signature]</i>				8/21/21

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Printed Name	Signature	Address	Comment	Date
Cindy Florance	C. Florance	[REDACTED]	use the lake concern work nearby ^{EE} hazardous waste	8/12/21
Mackenzie Eells	Mackenzie Eells	[REDACTED]	not in residential area	8/12/21
Tiffany Giles	Tiffany Giles	[REDACTED]	fire risk + air quality	8/12/21
BEET CIARK	BT Clark	[REDACTED]	not in residential area	8/12/21
Lauren Anderson	Lauren Anderson	[REDACTED]	move to non-residential area	8/12/21
Holly Gallant	Holly Gallant	[REDACTED]	not in residential area	8/13/21
Mohini Farias	Mohini Farias	[REDACTED]	Fire Risk - Air quality	8/13/21
Jeanie Van Vorst	Jeanie Van Vorst	[REDACTED]	fire hazard air quality move to non-residential area	8/13/21
Micka Shelley	Micka Shelley	[REDACTED]	to close to residents	8-13-21
Erica ^{TYLER} Johnstone	Erica Johnstone	[REDACTED]	environmental risk	8/14/21
Leanne Shabon	Leanne Shabon	[REDACTED]		8/14/21
MARC MUR	MARC MUR	[REDACTED]		8/14/21
Carol Elaine Ferris Pro	Carol Elaine Ferris Pro	[REDACTED]	not in residential & recreation	8/14/21

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Helen Walsh	<i>Helen Walsh</i>	[REDACTED]	Air quality; Mercury	8/5/21
Mary To Cogburn	<i>Mary To Cogburn</i>		Air quality; Mercury Poor	8/8/21
Gary Cogburn	<i>Gary Cogburn</i>		Air quality for environment Poor	8/8/21
RICK CUSTER	<i>Rick Custer</i>		Air quality - environment	8/8/21
Cheryl TABISH	<i>Cheryl Tabish</i>		AIR QUALITY - ENVIRONMENT	8/8/21
JOEL MOTT	<i>Joel Mott</i>		AIR QUALITY	8/8/21
Melissa Ingle	<i>Melissa Ingle</i>		Sight/Environmental	8/8/21
Betty Reed	<i>Betty Reed</i>		Air & Environment	8/8/21
Joy Kewin	<i>Joy Kewin</i>		Air Environment	8/8/21
RICHARD McGEE	<i>Richard McGee</i>		SMELL & AIR QUALITY	8-8-21
Haley Harmon	<i>Haley Harmon</i>		air quality & safety	8-8-21
Gabrielle Romero	<i>Gabrielle Romero</i>		air quality & smell	8/8/21
Ine Perugini	<i>Ine Perugini</i>		smell & quality	8/8/21

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Printed Name	Signature	Address	Comment	Date
MATT RANSOM		[REDACTED]	BAD IDEA!!	9/23/21
Russ Mebane		[REDACTED]	BAD	9/23/21
Billie Pekarok		[REDACTED]	NO Bad	9-25-21
Heather Grubb		[REDACTED]	no.	9-26-21
Gloria Hansen		[REDACTED]	Someone does Not have common sense	9-28-21
L Selig		[REDACTED]		9/28/21
Jonah Skellum		[REDACTED]		10/1/21
Lynza Skellum		[REDACTED]		10/1/21
Robert Whitaker		[REDACTED]		10/2/21
Christina Coppola		[REDACTED]		10/2/21
JES DRAPER		[REDACTED]	No!	10/2/21
Megan Bune		[REDACTED]	NO	10/2/21
Madhavi		[REDACTED]	NO!	10/2/21

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Printed Name	Signature	Address	Comment	Date	
Jim Detwiler				9/10/21	
Alexandra Detwiler				11	
Stephen Austin					9/14/21
Victoria Smyk					9/14/21
Conner Denny					9/19/21
BRENDA DERRY					9/19/21
TRICIA STOWEN					9-19/21
Colleen Baker				Residential!	9/19/21
JAMES HENDERSON					9/19/21
Sarah Swenson					9/19/21
Lou Ann Pittler-Swenson					9/19/21
ERIC LONEY					10/31/21
NATHAN SALEM					11/2/21

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Printed Name	Signature	Address/Contact Information	Comment	Date
Joanna Stanfield	<i>Joanna Stanfield</i>	[REDACTED]	agree - NO Crematorium	10/23/21
Thomas Rasmussen	<i>Thomas Rasmussen</i>	[REDACTED]	" "	10/23/21
Jackie Selliti	<i>Jackie Selliti</i>	[REDACTED]	NO!	10/27/21
MIKE SELLITI	<i>Mike Selliti</i>	[REDACTED]	NO!	10/27/21
Susan		[REDACTED]	YES!!!	
Debbie Jones	<i>Debbie Jones</i>	[REDACTED]	Agreed!	10/27/21
Thomas Shimozuka	<i>TS</i>	[REDACTED]	No crematorium!	10/29/21
MICHELLE ROBERTS	<i>Michelle Roberts</i>	[REDACTED]	AGREE	10/29/2021
RON FALLON	<i>Ron Fallon</i>	[REDACTED]	NO CREMATORIUM	10/29-21
STEVE KAUC	<i>Steve Kauc</i>	[REDACTED]	No CREMATORIUM	10/29/21
Hannah Forrest	<i>Hannah Forrest</i>	[REDACTED]	Bad Idea. Residential	11/1/21
CHRISTINE COE	<i>Christine Coe</i>	[REDACTED]	NO CREMATORIUM	11/7/21

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Printed Name	Signature	Address	Comment	Date	
BRIANNA Anderson	<i>[Signature]</i>	[REDACTED]		8/14/21	
Caron Anderson	<i>[Signature]</i>			8/18/21	
Keryn McGuffin	<i>[Signature]</i>		Dangerous Chemicals	8/21/21	
Mimi Hafeman	<i>[Signature]</i>		Dangerous Chemicals	8/21/21	
Donna Houle	<i>[Signature]</i>		Harmful Chemical & Air Quality	8/21/21	
Aliya Kasabian	<i>[Signature]</i>			8/21/2021	
Chelsey	<i>[Signature]</i>			8/21/21	
Brigit Lh	<i>[Signature]</i>			8/24/2021	
FONG-SHEK LAM	<i>[Signature]</i>			8/25/21	
Susan Mitchell	<i>[Signature]</i>			8/25/21	
Dorothy Pugh	<i>[Signature]</i>			Bad Chemicals	8/30/21
CAERRYLL RODNEY	<i>[Signature]</i>				8/30/21
MARY Kelley Colton	<i>[Signature]</i>			Air Quality	9/3/21

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Printed Name	Signature	Address	Comment	Email	Date
Lynn Medley	<i>[Signature]</i>	[Redacted]	NO		9/1/21
Joyce Kramer	<i>[Signature]</i>	[Redacted]	NO		9/14/21
Christa Mathera	<i>[Signature]</i>	[Redacted]			9/15/21
Debra Williams	<i>[Signature]</i>	[Redacted]			8/15/21
Rich Kretzmer	<i>[Signature]</i>	[Redacted]			8/15/21
DANIEL WINKELMAN	<i>[Signature]</i>	[Redacted]			8/15/21
KAREN WEST	<i>[Signature]</i>	[Redacted]			8-15-21
Dann Shivek	<i>[Signature]</i>	[Redacted]			8/16/21
Staci Shivek	<i>[Signature]</i>	[Redacted]			8/16/21
✓ Joseph Lyons	<i>[Signature]</i>	[Redacted]			8/16/21
✓ Latika Cardray	<i>[Signature]</i>	[Redacted]			8/16/21
Deborah Grassl	<i>[Signature]</i>	[Redacted]			8/16/21
NANCY FALLAN	<i>[Signature]</i>	[Redacted]			08/17/2021
		[Redacted]			8-17-21

✓ may be a dup

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Printed Name	Signature	Address	Comment	Date	
Kristen Anderson		[REDACTED]	Poor Air Quality	8/4/21	
Myrna Lindajon			re lakeside	8/5/21	
Lynn Parker				8/9/21	
Karen Hoff				Air Quality	8/9/21
Tylee Bailey				Air Quality	8/12/21
Paul Tyler				Air Quality	8/12/21
Chantalle Richard				Air Quality	8/13/21
JASIT Kaur				Air Quality	8/13/21
Anna Baburkina				Air Quality	8/13/21
Vitalia Kobelova				Air Quality	8/13/21
Tom Kummel				Air Quality	8/13/21
Sandy Economme				Air Quality	8/13/21
Sofia Schittman				Air Quality	8/13/21

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Printed Name	Signature	Address	Comment	Date
Randy	[Signature]	[Redacted]	health risks and environment	8/5/2021
Carl Dewey	[Signature]	[Redacted]	Zone for open space	8-6-21
Randall Thompson	[Signature]	[Redacted]	Health Risk/Not in neighborhoods	8-8-21
Robert S. Allen	[Signature]	[Redacted]	Health Risk	8/8/21
M. Ke Leverett	[Signature]	[Redacted]	Not in neighborhoods	8/8/21
Emily Beaton	[Signature]	[Redacted]	Health Risk	8/8/21
Amy Thompson	[Signature]	[Redacted]	Health Risk	8/8/21
Ariel Tibbs	[Signature]	[Redacted]	Health risks/smells	8/8/21
Andrew Beckley	[Signature]	[Redacted]	Health Risks	8/8/21
Tonya Lucas	[Signature]	[Redacted]	health risks not in neighborhoods	8/8/21
Jo Carol [unclear]	[Signature]	[Redacted]	Health Risk	8/9/21
Cynthia Bauer	[Signature]	[Redacted]	Health Risk	8/9/21
Cynthia Veiros	[Signature]	[Redacted]	Health Risk	8/9/21

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Holly Luvell	Holly Luvell	Fair Oaks, CA		3AUG21
Andy W. Tsamir	Andy W. Tsamir	Fair Oaks, CA		8/3/21
Rebecca Clark	Rebecca Clark	Concord, CA		8-3-21
Diana Agay	Diana Agay	[REDACTED]		8-4-21
Ruth Hudson	Ruth Hudson	Folsom, Ca 95630		8-4-21
Lindsay Jesser	Lindsay Jesser	Citrus Heights, CA		8-4-21
KAREN BRANSON	Karen Branson	Roseville, CA	concerned about air quality	8-8-21
Sheila Zurich	Sheila Zurich	Folsom, CA		8-8-21
Teri Sautler	Teri Sautler	Folsom, CA	concerned about air quality	8-8-21
DENNIS FAKS	Dennis Faks	Folsom		8-8-21
HARVEY	Harvey	Folsom	air quality	8-8-21
Shirley	Shirley	Folsom	air quality	8/8/21
David Bringer	David Bringer	Folsom	air quality	8/8/21

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Printed Name	Signature	Address	Comment	Date
GINA LEEDS	<i>[Signature]</i>			8/10/21
MARK E BERTHA MITCHEL	<i>[Signature]</i>		Preserve Tolson	8/10/21
MARK MITCHEL	<i>[Signature]</i>		Preserve Tolson	8/11/21
BERTHA MITCHEL	<i>[Signature]</i>		Preserve Tolson	8/11/21
Larry Templin	<i>[Signature]</i>			8-12-21
STEPHANIE KERNS	<i>[Signature]</i>			8/12/21
ZAVINE YOUSSEF	<i>[Signature]</i>			8/14/21
Delaney Polidori	<i>[Signature]</i>			8/14/21
Sophann Yu	<i>[Signature]</i>			8-14-21
Stephanie Lano	<i>[Signature]</i>		fire concern	8-14-21
Dick Fortier	<i>[Signature]</i>		Fire Fire	8/14/21
Paul Bireault	<i>[Signature]</i>			8/14/21
Becca White	<i>[Signature]</i>		Air quality!	8/14/21

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Printed Name	Signature	Address	Comment	Date
Breanne Higgins	Breanne Higgins	[REDACTED]	Shouldn't be in a neighborhood. Bad Air!	8/3/21
Clyde Hagood	Clyde Hagood	[REDACTED]	NOT good idea	8-5-21
Laura Cass	DAWN CASS	[REDACTED]	BAD IDEA	8-6-21
Penny Pomodoro	Penny Pomodoro	[REDACTED]	Bad IDEA	8-6-21
CHIN ANDERSON	Chin Anderson	[REDACTED]	Not good to have	8-6-21
Laura Fujitsubo	Laura Fujitsubo	[REDACTED]	AIR, Smell, Fire risk	8/6/21
Veronica Homich	Veronica Homich	[REDACTED]		8/6/21
Robert Baker	Robert Baker	[REDACTED]	pollution - Hazard waste - smell -	8/7/2021
HARON Moore	Haron Moore	[REDACTED]	Bad IDEA	8/7/2021
Wendy Yezzi	Wendy Yezzi	[REDACTED]	Personality worked for a cemetery. Crematory would have	8/8/21
Marko McEwen	Marko McEwen	[REDACTED]	ashes floating in air dangerous for children	8/12/21
TIM M'GARR	Tim McGarr	[REDACTED]	NOT IN A RESIDENTIAL NEIGHBORHOOD.	8/12/21
Kim Copeshon	Kim Copeshon	[REDACTED]	Person with Disabilities	8/14/21

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Printed Name	Signature	Address	Comment <i>EMAIL</i>	Date
STEVEN JENSBY	<i>Steven Jensen</i>			8/21/21
MARIE RICE	<i>Marie Rice</i>			8-21-21
ILAN RICE	<i>Ilan Rice</i>			8/21/21
Pat Zuccaro	<i>Pat Zuccaro</i>			8/22/21
Bert Pittari	<i>Bert Pittari</i>			8/22/21
Christine George	<i>C. George</i>			8/22/21
Melissa McCann	<i>Melissa McCann</i>			8/22/21
CHRISTOPHER NAIL	<i>Christopher G. Nail</i>			8/22/21
Vivian Olson	<i>Vivian Olson</i>			8/22/21
TIM MILNE	<i>Tim Milne</i>			8/22/21
Helen Milne	<i>Helen Milne</i>			8/22/21
Ernie Nibiolini	<i>Ernie Nibiolini</i>			8-22-21
Corinne Lykins	<i>Corinne Lykins</i>			8-22-21

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Printed Name	Signature	Address	Comment	Email	Date
Mary Johnson	<i>Mary Johnson</i>	[Redacted]	NO crematorium!		8/17/21
Megan McClure	<i>M McClure</i>	[Redacted]			8/17
Robin Bassell	<i>Robin Bassell</i>	[Redacted]			8/17/21
PETER LUCYGA	<i>Peter Lucyga</i>	[Redacted]			8/17/21
LISA DITTOLICO	<i>Lisa Dittolico</i>	[Redacted]			8/17/21
Maryellen Blackburn	<i>Maryellen Blackburn</i>	[Redacted]			
DYLAN MAURO	<i>Dylan Mauro</i>	[Redacted]			
Kelly Mauro	<i>Kelly Mauro</i>	[Redacted]			8/17/21
Grace Mauro	<i>Grace Mauro</i>	[Redacted]			8/17/21
DAVID ALDHAM	<i>David Aldham</i>	[Redacted]	N/A		8/17/21
Mary Boylen	<i>Mary Boylen</i>	[Redacted]			8/17/21
Cesar Vaz	<i>Cesar Vaz</i>	[Redacted]			8-17-21
Angel [Redacted]	<i>Angel [Redacted]</i>	[Redacted]			8-17-21

8/17/21

NO LAKESIDE MEMORIAL LAWN CREMATORIUM!

Petition summary and background	Lakeside Memorial Lawn has applied for a conditional use permit to install and operate a crematorium on designated open space. This has significant negative implications for the physical, environmental and fiscal health of our community.
Action petitioned for	We, the undersigned, are highly concerned citizens who urge our leaders to act now to vote 'NO' on the permit application.

Printed Name	Signature	Address	Comment	Date
Angelina Kazinec	<i>angelina Kazinec</i>	[REDACTED]	No crematory	08/03
ERICA Kazinec	<i>Erica Kazinec</i>		No crematory	8/03
Siena Kazinec	<i>Siena Kazinec</i>		no crematory	8/03
Deb Ozdinski	<i>Deb Ozdinski</i>		NO	8/03
GREG OZDINSKI	<i>Greg Ozdinski</i>		DOES NOT BELONG IN HISTORIC DISTRICT	8/03
DAN GRIFFIN	<i>Dan Griffin</i>		Poor Air Quality bad!	8/03
Timothy Farley	<i>Timothy Farley</i>		poor a q, near river	8/03
Alexandra Edwards	<i>Alexandra Edwards</i>		POOR air quality	8/03
Lynsi Busch	<i>Lynsi Busch</i>		poor air quality	8/4
Andrey Anaya	<i>Andrey Anaya</i>		poor air quality	8/4
Kyle Terry	<i>Kyle Terry</i>		Poor air quality	8/4
Sasha Lucyga	<i>Sasha Lucyga</i>		no crematory - poor air quality	8/4
Erica Graham	<i>Erica Graham</i>		NO crematorium	8/4/22







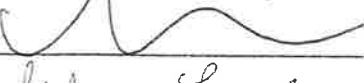





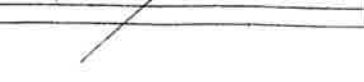

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Printed Name	Signature	Address	Comment	Date
DAVE HIGGINS		[REDACTED]	NOT GOOD!	8-5-21
MARIE RICE		[REDACTED]	FIRE DANGER!	8-5-21
Emily Bertelli		[REDACTED]	Health issue	8-5-21
Daniel Ruv		[REDACTED]		8-5-21
Dann Shively		[REDACTED]		8-5-21
Heather Hayes		[REDACTED]	small children	8/5/21
Christy [REDACTED]		[REDACTED]		8/5/21
Dennis [REDACTED]		[REDACTED]	Pollution	8/5/21
Stan DeLima		[REDACTED]	Bad!	8/5/21
Rosemarie DeLima		[REDACTED]	Bad idea	8/5/21
Jessica [REDACTED]		[REDACTED]	Health/pollution	8/5/21
Ryan [REDACTED]		[REDACTED]	Health / Pollution	8/5/21
Pamela Ceccarelli		[REDACTED]	Air/Fire/odor HEALTH	8/7/21

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Printed Name	Signature	Address	Comment	Date	
Matt Eastman		[REDACTED]	Pollution	8-8/21	
Jeanne Duarte			"	8-8-21	
Kevin Hill				8/8/21	
				Pollution	8/8/21
Joe CAVANAUGH				No More Pollution!	8/8/21
CAROL CAVANAUGH				" "	8/8/21
Megan Caesar				" "	8/8/21
Melissa Loaschelder				Pollution	8/8/21
KEVIN Loaschelder			" "	" "	8/8/21
Sebastian Chihuahua			" Folsom "	Pollution	8/8/21
José Chihuahua		Folsom	Pollution	8/8/21	
TREVOR WEHNER		FOLSOM	Pollution	8/8/21	
JANIE NAGLEY		FOLSOM	POLLUTION	8/8/21	

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Printed Name	Signature	Address	Comment	Date
Aano Duarb		[REDACTED]		8/8/20
Nicole Wells		[REDACTED]		8/8/2021
LARRY MARQUEZ		[REDACTED]		8/8/2021
Koyshu Trask		Solsom		8/8/21
MAR TRASH		ROSEVILLE		8/8/21
Josiah Trask		Roseville		8/8/21
Elijan Trask		Roseville		8/8/21
Ashley Martin		[REDACTED]	and is an issue already	8/8/21
Jesus Cruz		[REDACTED]		8/8/21
Tianna Lee		[REDACTED]		8/8/21
Lauren M		[REDACTED]		8/8/21
Lauren R		[REDACTED]		8/8/21
Hope Fenske		[REDACTED]		8/8/21

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Printed Name	Signature	Address	Comment	Date
Jill Hamer		[REDACTED]	Clean air for walking + the eagles I like the trails	08/02/21
Erika Hamer			r. clean air on the lake	8/3/21
James M. Fox			530 to close to residential property	8/3/21
MARY MATTHEW			Long term air quality - Eagles	9/3/21
MATHE RICE			Scary Fire Hazard!!	8-3-21
Alysse Rocha			Lots of time outdoors in yards + street - disturbed smoke, odors	8/3/21
Johnson			Healthy Kids !!!	8/3/21
MIKE OCAMPO			Not needed in Residential Hwy	8/3/21
Laetitia Godray			Very Concern about toxic fumes and fire Risk	08/03/21
Josiah Gill			toxic fumes + not needed in residential area	8-3-21
Josiah Gill			Toxic air	8/3/21
Kyral VanGasteren			CO ² - Global Warming	9/3/21
Victor Gonzalez		not in my backyard	8/5/21	

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Printed Name	Signature	Address	Comment	Date
Paul Taybr	<i>Paul Taybr</i>		Inappropriate Location	8/3/21
JANINE FOX	<i>Janine Fox</i>		Bad Air Quality	8/3/21
ELIZABETH SOTO	<i>Esoto</i>		Health Impact, Air Quality,	8/3/21
Rodain Soto	<i>Rodain Soto</i>		environmental impact, safety,	8/3/21
TATIANA OSORN	<i>Tatiana Osorn</i>		Bad Air Quality	8/3/21
Thomas Osborn	<i>Thomas Osborn</i>		Environmental Impact	8/3/21
Brian Paciotti	<i>Brian Paciotti</i>		Bad Location	8/3/21
John Capburn	<i>John Capburn</i>		Environment	8/3/21
Marie Gonzales	<i>Marie Gonzales</i>		Fire Hazard & ^{Bad Air} Quality	8/5/21
Frank Alois	<i>Frank Alois</i>		Bad idea	8/5/21
Elizabeth Aloisa	<i>Elizabeth Aloisa</i>		Inappropriate location	8/5/21
Joe P...	<i>Joe P...</i>		" "	8/5/21
Kentri W...	<i>Kentri W...</i>		No Good AC	8.5.21

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Printed Name	Signature	Address	Comment	Date
Nicole Gates	<i>Nicole Gates</i>		asthmatic	8/01/2021
Kim Higgins	<i>Kim Higgins</i>		Asthma	8/1/21
Sean Gates	<i>Sean Gates</i>		pollutants	8/1/21
Jan Eastman	<i>Jan Eastman</i>		we walk the trails	8/1/21
<i>M. Costello</i>	<i>M. Costello</i>		pollution	8/1/21
DELTA ANATAKIO	<i>Delta Anatakio</i>			8/1/21
JER MAHER	<i>Jer Maher</i>			8/2/21
Lia Maher	<i>Lia Maher</i>		Folsom CA	8/2/21
Karen Williams	<i>Karen Williams</i>		Folsom, CA	8/2/21
	<i>Karen Williams</i>		Folsom CA	8/2/21
Margaret Kmetz	<i>Margaret Kmetz</i>		Folsom CA	8/3/21
Brooke Drewing	<i>Brooke Drewing</i>		Rocklin CA	8/3/21
Carlin Bowman	<i>Carlin Bowman</i>		Rocklin CA	8/3/21
Janet West	<i>Janet West</i>		Folsom, CA	8/3/21

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Printed Name	Signature	Address	Comment	Date
Lara Bullantine	<i>Lara Bullantine</i>	[REDACTED]	<i>na</i>	8/14/21
GIANNI VILPROI	<i>Gianni Vilproi</i>	[REDACTED]		8.14.21
Ainslee Voss	<i>Ainslee Voss</i>	[REDACTED]		8/14/21
Ilya Cetin	<i>Ilya Cetin</i>	[REDACTED]		8/14/21
Christian Ramos	<i>Christian Ramos</i>	[REDACTED]		8-14-21
TRACI BRKEND	<i>Traci Brkend</i>	[REDACTED]	AIR QUALITY/POLLUTION	8-14-21
David Rowe	<i>David Rowe</i>	[REDACTED]	POOR AQ	8/14/21
William Rowe	<i>WR</i>	[REDACTED]	Poor air	8/14/21
JUDITH DECKER	<i>Judith Decker</i>	[REDACTED]	^{+SMELL (TRAFFIC)} BAN AIR QUALITY	8/14/21
Kelly BARAGA	<i>Kelly Baraga</i>	[REDACTED]	5746 GT 6B CA 95746	8/16/21
Stephen Larson	<i>Stephen Larson</i>	[REDACTED]	Air Quality impacting Folsom Historic District	8/17/21
Beth Zucson	<i>Beth Zucson</i>	[REDACTED]	Folsom Historic District Air Quality	8/17/21
Heather Vu	<i>Heather Vu</i>	[REDACTED]	Very bad for AQ	8/18/21

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Printed Name	Signature	Address	Comment	Date
Lance Binley	<i>Lance Binley</i>	[REDACTED]	Who wants burning bodies in their neighborhood?	8/8/21
Kari Seward	<i>Kari Seward</i>	[REDACTED]	Air Quality Concerns	8/8/21
Amanda Painter	<i>Amanda Painter</i>	[REDACTED]	Smoke from burning bodies is harmful to most people	8/9/21
Pet Binley	<i>Pet Binley</i>	[REDACTED]	many reasons	8-20-21
[REDACTED]	<i>Jenna Fujitsuho</i>	[REDACTED]	• zoning • health	8-21/21
Curtis	<i>Jenna Fujitsuho</i>	[REDACTED]	health air quality	8/21/21
Vanessa Fujitsuho	<i>Vanessa Fujitsuho</i>	[REDACTED]	Health / children concerns	8/21/2021
Neki Monical	<i>Neki Monical</i>	[REDACTED]	My children need clean air!	8/21/21
Max Bering	<i>Max Bering</i>	[REDACTED]	Air quality	8/21/21
Deb Swanson	<i>Deb Swanson</i>	[REDACTED]	air quality ^{home} value	8/21/21
Perry Best	<i>Perry Best</i>	[REDACTED]	NO crematorium	8/21/22
Salwa Kasabian	<i>Salwa Kasabian</i>	[REDACTED]	fire hazard	8/21/21
Dennis Kasabian	<i>Dennis Kasabian</i>	[REDACTED]	fire hazard I don't want to breathe other people's health	8/21/21

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Printed Name	Signature	Address/Contact Information	Comment	Date
Susan Hufford	<i>S Hufford</i>	[REDACTED]		9/25/21
Maria Perez	<i>M. Perez</i>			25 Sept 2021
Jenny C.	<i>Jenny C.</i>			9/25/21
Dan Fikes	<i>Dan Fikes</i>			9-25-21

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Printed Name	Signature	Address	Comment	Date
Susan Houston	<i>Susan Houston</i>	[REDACTED]		8/26/21
Patrice Surratt	<i>Patrice Surratt</i>		we already have poor Air Quality	8/28/21
Katy Surratt	<i>Katy Surratt</i>		Absolutely against	8/28/21
Kristin Rodgers	<i>Kristin Rodgers</i>			9/3/21

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Printed Name	Signature	Address	Comment	Date
Brittany Griffin	<i>Brittany Griffin</i>	[REDACTED]	This is not an appropriate place to put this!	9/16/2021
Martin Stack	<i>Martin Stack</i>	[REDACTED]	Too much smoke	11/20/21
Amanda Escobedo	<i>Amanda Escobedo</i>	[REDACTED]	Showing support for our neighbors	11/22/21
Ashley Moore	<i>Ashley Moore</i>	[REDACTED]	Not appropriate. Showing support	11/22/21
Alan Roberts	<i>Alan Roberts</i>	[REDACTED]	I BREATHE FRESH AIR	11/20/21

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Printed Name	Signature	Address	Comment	Date
Tanice Briat	Tanice Briat	[REDACTED]	air quality ^{TRAFFIC} fire concerns	8/14/21
Don Donovan	[Signature]	[REDACTED]		8/14/21
Andrea Donovan	[Signature]	[REDACTED]		8/14/21
Michael Castle	[Signature]	[REDACTED]	Practical cycle	8/14/21
David Rolin	David Rolin	[REDACTED]	Practical cycle work in area	8/14/21
Jeff Hogg	Jeff Hogg	[REDACTED]	Traffic & Emergency egress	8/14/21
Macie Walsh	Macie Walsh	[REDACTED]	Fire Hazard	8/15/21

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Printed Name	Signature	Address	Comment	Date
Barbara Harris		[REDACTED]	NO Crematorium in our neighborhood	8/19/2021
Paul Best			NOPE!!	8/20/2021
MATS DANSSON			Compost instead!	8/20/21
Joanne Reik			No!!	8/21/21
Louise Barnett			NO!!	8/20/21
Carol Hart			No!	8/20/21
Elaine Moura			No!!	8/20/21
Rhonda DesVoignes			No vote	8/20/21

NO LAKESIDE MEMORIAL LAWN CREMATORIUM!

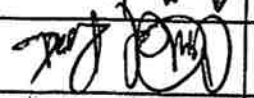

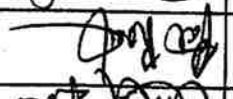
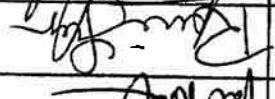



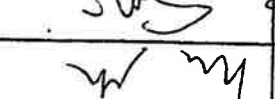

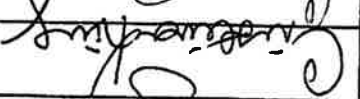
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Printed Name	Signature	Address	Comment	Date
Claire	<i>Claire</i>	Eldorado Hills CA		9/11
Jamie Plank	<i>Jamie Plank</i>	Folsom CA		9/12
Don Elser	<i>Don Elser</i>	[REDACTED]	We exercise here and I never walk	9/12
Lorie Jo Paulina D'Donnell	<i>Lorie Jo Paulina D'Donnell</i>	[REDACTED]	add this emissions close others it's toxic!	9/12
JOHN PATRICK O'DONNELL	<i>John Patz O'Donnell</i>	[REDACTED]	MURKBY WILL GO INTO EAGLE FEEDIN AREA	9/12
Laura Wiese	<i>Laura Wiese</i>	[REDACTED]		9/12
Kathleen Noble	<i>Kathleen Noble</i>	[REDACTED]	people enjoying the bike trail not good for residents or	9/13
CHARLES NOBLE	<i>Charles Noble</i>	[REDACTED]	MERCURY HEAVY METAL OM 95630 REUSE	9/13/21

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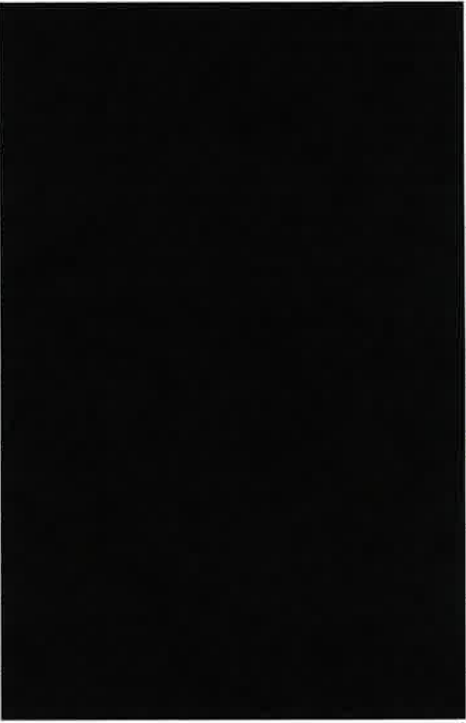









Petition summary and background
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Action petitioned for
 We, the undersigned, are highly concerned citizens who urge our leaders to act now to vote 'NO' on the permit application.

Printed Name	Signature	Address	Comment	Date
Crystal Roark				8/9/21
Patrick Roark				8/9/2021
Renee Lupien				8/16/2021
Hannah Christensen				8/16/2021
Michelle Lopez				8/16/21
Matt Greulich				8/19/21
Suzanne Ayle Sage				8/22/21
Cristina King				8/22/21
Shannon Rippe				8/24/21
				Folsom, CA

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Printed Name	Signature	Address	Comment	Date	
Aime Bentz			email received to petition @ Historic Folsom	9/9/21	
Norma Petta			Don't do it!	9/9/21	
Jonnie Cosimo			unsafe!	9/9/21	
Chela Cecilia Ann Garcia			No!	9/10/21	
Frank Garcia			Not an option!	9/10/21	
Teresa Stark				9-10-21	
				STOP it !!	9-10-21
Elie Perez				"	"
EMMA HIGGINS				NO THANKS	9/11/21

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Printed Name	Signature	Address	Comment	Date
MARLOVES	<i>[Handwritten Signature]</i>	[REDACTED]	visiting here w/ daughter	
Dore Baker	<i>[Handwritten Signature]</i>		8/21/21	
Annaliese Rodego	<i>[Handwritten Signature]</i>		8/21/21	
Dylan Mervell	<i>[Handwritten Signature]</i>		8/21/21	
Monalorenzo	<i>[Handwritten Signature]</i>		No crematorium	8/21/21
Chris Kilts	<i>[Handwritten Signature]</i>			8/22/21
Alan McHoyan	<i>[Handwritten Signature]</i>		NO crematorium	8/22/21
Dominique M.	<i>[Handwritten Signature]</i>		↓	8/22/21
Isaac Monical	<i>[Handwritten Signature]</i>		Not safe or appropriate No crematorium!	8/22/21

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Printed Name	Signature	Address	Comment	Date	
Shay Maunders		[REDACTED]		8/12/21	
Claudia Kapinski				8/12/21	
Carolina Gutierrez				8/12/21	
Jake Gsell				8/12/21	
Jessica					
JONATHAN SMALL					08/14/21
Rachel Verdino					08/15/21
Eugenia Moeszinger	Eugenia Moeszinger		[REDACTED]	BAO IDEA	08/15/2021
Nancy Talamasca	Nancy Talamasca			"	08/19/2021

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Printed Name	Signature	Address	Comment	Date
JULIE ANN SOUTHERN	<i>[Signature]</i>	[REDACTED]	NO!! SO WRONG!!	10/03/2021
Michelle Reisinger	<i>[Signature]</i>	[REDACTED]	NO	10/8/21
Leona Duff	<i>[Signature]</i>	[REDACTED]	No	10/8/21
Kapri Pelle	<i>[Signature]</i>	[REDACTED]	NO!!!	10/8/21
Brittiany Fitzgerald	<i>[Signature]</i>	[REDACTED]	NO	10/8/21
Christina Napier	<i>[Signature]</i>	[REDACTED]	No! Protect our vulnerable	10/15/21
Angelique Horess	<i>[Signature]</i>	[REDACTED]	NOOOO!!	10/15/21
Celi Hornish	<i>[Signature]</i>	[REDACTED]	NO! [REDACTED]	10/17/21
Janet Gill	<i>[Signature]</i>	[REDACTED]	No Industrial area <i>Needs to be in</i>	10/17/21

NO LAKESIDE MEMORIAL LAWN CREMATORIUM!

Petition summary and background	Lakeside Memorial Lawn has applied for a conditional use permit to install and operate a crematorium on designated open space. This has significant negative implications for the physical, environmental and fiscal health of our community.
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Printed Name	Signature	Address	Comment	Date
Kiana Sherburn		[REDACTED]	should stay healthy for open space!	8/8/21
Heather Clark		[REDACTED]	air quality and fire danger concerns	8/9/21
MARNA WIGGEN		[REDACTED]		8/10/2021
Jeanne Heifer		[REDACTED]		8/10/21
Diane Moore Rippey		[REDACTED]	everything	8/19/21
Susan Alexander		[REDACTED]		8-20-21
Melissa Day		[REDACTED]		8-20-21
Chris Taylor		[REDACTED]	concerned for my kids health	8-20-21
Danielle Taylor		[REDACTED]	concerned for everyone's health	8/20/21
MARJORIE WELLS		[REDACTED]	Smell air quality. Strong fumes of fuel in neighborhood.	8/20/21

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Printed Name	Signature	Address/Contact Information	Comment	Date
Tony COE			NO CREMATORIUM ⁰⁰⁰ ₀₀₀	10/30/2021
Thomas Bothwell			10/2/21	
Ramesh Rathana			NO crematorium	10/2/21
Senthil Kumar N			NO crematorium	10/2/21
Danny Gill			"TRUMP 2024"	10/2/21
Jim Rossi			NO CREMATORIUM	10/2/21
Selva Prasad			NO	10/2/2021
GABRIELA GERLACH			NO crematorium!	10/2/21
William Carpenter			10/2/21	
Joan Auer			OK NOT OK	10.7.21
Tony Campos				
Disha Nambiar				

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Printed Name	Signature	Address	Comment	Date
Bill St. Marie	<i>Bill St. Marie</i>			8-21-21
Joyce Roderick	<i>Joyce Roderick</i>		lson	8-21-21
Jason Yom	<i>Jason Yom</i>		24	9-3-21
Sarah Tyler	<i>Sarah Tyler</i>		air quality	9-9-21
Spencer Enochian	<i>Spencer En</i>		Air Quality	9-9-21
Sharon Harder	<i>Sharon Harder</i>		air Quality	9/4/21
Nichol Hadfield	<i>Nichol Hadfield</i>		lie Quality	9/15/21
Alison Berry	<i>Alison Berry</i>		air quality	9/19/21
J. DEACON	<i>J. Deacon</i>		air Quality	9/19/21
DEE BRIGHT	<i>Dee Bright</i>		AIR QUALITY	9/20/21

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Printed Name	Signature	Address	Comment	Date
Tracy Wetzel	<i>Tracy Wetzel</i>	[REDACTED]	[REDACTED]	08/19/2021
Steve Wetzel	<i>Steve Wetzel</i>	[REDACTED]	[REDACTED]	1.com
Ryan Wells	<i>Ryan Wells</i>	[REDACTED]	[REDACTED]	8-19-21
Dawn Paciotti	<i>Dawn Paciotti</i>	[REDACTED]	[REDACTED]	8-21-21
Penny Young	<i>Penny Young</i>	[REDACTED]	[REDACTED]	8/21/21
Sharon Barnes	<i>Sharon Barnes</i>	[REDACTED]	[REDACTED]	8/21/21
Tom Barnes	<i>Tom Barnes</i>	[REDACTED]	[REDACTED]	8/22/21
Kyle Harding	<i>Kyle Harding</i>	[REDACTED]	[REDACTED]	8-22-21
Trevor Harding	<i>Trevor Harding</i>	[REDACTED]	[REDACTED]	8/23/21
Barbara Krieger	<i>Barbara Krieger</i>	[REDACTED]	[REDACTED]	8/23/21
		[REDACTED]	[REDACTED]	9/11/21
		[REDACTED]	[REDACTED]	
		[REDACTED]	[REDACTED]	
		[REDACTED]	[REDACTED]	

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Printed Name	Signature	Address	Comment	Date	
Catherine Mauder		[REDACTED]		8/8/21	
Jeff Maulkin				8/8/21	
Andrea Gomez				8/8/21	
Natalie Smith	Natalie Smith				8/8/21
Rae Anderson					8/8/21
Madison M					8/8/21
Jacelyn Stekelberg					8/8/21
Dawn Swinger	Dawn Sw			Sac Cath	8/11/21
Wyatt P. Swinger	Wyatt P. Swinger			Sac	8/11/21
Hailey Spruitenberg	Hailey Spruitenberg				8/12/21
Graham Booklin					8/12/21

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Printed Name	Signature	Address	Comment	Date
Cathy Owen	<i>Cathy Owen</i>	[REDACTED]		8/11/21
Brian Adams	<i>Brian Adams</i>			8/11/21
TOM LANGFORD	<i>Tom Langford</i>			11
Tom GLENSKY	<i>Tom Glensky</i>			8/11/21
Robert Lee	<i>Robert Lee</i>			8/19/21
RORY HANLEY	<i>Rory Hanley</i>			8/19/21
Heather Hanley	<i>Heather Hanley</i>			8/19/21
Jose Lopez-Alvares	<i>Jose Lopez-Alvares</i>			8/19/21
ELYSE HUNT MACKENZIE	<i>Elyse Hunt Mackenzie</i>			8/19/2021
Bob Mackenzie	<i>Bob Mackenzie</i>			8/19/2021
DAN SCHOLZ	<i>Dan Scholz</i>			8/19/2021

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Printed Name	Signature	Address	Comment	Date
Chris Shimozaki		[REDACTED]		9/6/21
KRISTIN KESSLER				9/6/21
JUSTIN KESSLER				9/6/2021
Jacob Davis				9/6/21
Ashley Davis				
Sheila Ortega				9/6/21
Bess Shapiro				9/6/21
Grace Westlake				9/6/21
VICKIE TOWE				9/7/21
Jessica Hodge				9/7/21
Loyce Roderick				9/7/21

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Printed Name	Signature	Address	Comment	Date
Bryan Givens	<i>Bryan Givens</i>	[REDACTED]		8/4/21
Margaret Crawford	<i>Margaret Crawford</i>	[REDACTED]	Business owner of Pediatric Therapy business. Natoma Professional Center	8/4/21
Rachel Hoy	<i>Rachel Hoy</i>	[REDACTED]	My son works on breathing here at this office, I do not want him breathing this.	8/5/21
JOHN CARRIGER	<i>John Carriger</i>	[REDACTED]	THIS MY DAUGHTER HAS BREATHING/SPENCER THERAPY PER THE STREET.	8/5/21
Viktoriya Ivanova	<i>Viktoriya Ivanova</i>	[REDACTED]	Too close to children and children breathing.	08/06/21
Alyssa Chapman	<i>Alyssa Chapman</i>	[REDACTED]	Asthma - concerns w/ possible airway issues	8/10/21
DENA CANNON	<i>Dena Cannon</i>	[REDACTED]	Air pollution, climate toxics	9/1/21
Andrea Norris	<i>Andrea Norris</i>	[REDACTED]	unnecessary, air pollution, clean air	9/1/21
DAVID KRIG	<i>David Krig</i>	[REDACTED]		9/3/21
Autumn Herrington	<i>Autumn Herrington</i>	[REDACTED]	pollution, unhealthy	9/3/21
Bella Simabedi	<i>Bella Simabedi</i>	[REDACTED]	pollution, unhealthy	9/3/21

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Printed Name	Signature	Address	Comment	Date	
Seth Johnson	<i>Seth Johnson</i>			9/19/21	
RICHARD BYRD	<i>R. Byrd</i>			9/20/21	
Jennifer Lane	<i>Jennifer Lane</i>	[REDACTED]	Poor zoning	9/20/21	
Diana Rodriguez	<i>Diana Rodriguez</i>		Health Risk	9/21/21	
Ricardo Rodriguez	<i>Ricardo Rodriguez</i>		Health Risk	9/21/21	
Mohini Prasad	<i>Mohini Prasad</i>			9/22/21	
Debra Real	<i>Debra Real</i>			9/23/21	
Debra Field	<i>Debra Field</i>			no smell will go down & up stream	9/23/21
Angus Jones	<i>Angus Jones</i>			Not able to breathe in Athassia	9/23/21
Christina Bratton	<i>Christina Bratton</i>			Poor zoning	9/23/21
Kathryn Bratton	<i>Kathryn Bratton</i>			health risk	9/23/21

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Printed Name	Signature	Address	Comment	Date
Joy Hays	<i>Joy Hays</i>	[REDACTED]	Allergic to smoke. Protect our vulnerable	8/6/2021
Ivy Hollingsworth	<i>Ivy Hollingsworth</i>	[REDACTED]	clean air PLEASE!	8/6/2021
Melody Poley	<i>Melody Poley</i>	[REDACTED]	Not healthy for the babies & elderly!	8/6/2021
Crystal Cerrillo	<i>Crystal Cerrillo</i>	[REDACTED]	Not healthy	8/6/21
Marilyn Peterson	<i>Marilyn Peterson</i>	[REDACTED]	definitely <u>not</u> healthy	8/6/21
Frances Servente	<i>Frances Servente</i>	[REDACTED]		8-6-21
Austin P.	<i>Austin P.</i>	[REDACTED]		8-6-21
CHAD WASIAK	<i>WASIAK</i>	[REDACTED]	NOT HEALTHY	8-6-21
Kailce Hays	<i>Kailce Hays</i>	[REDACTED]	Vaporized chemicals from crematorium smoke dangerous to all people in area and bald eagle family (endangered species) MAJOR FIRE DANGER!!!	8/6/21
Rob Busher	<i>Rob Busher</i>	[REDACTED]	Air quality, fire, not healthy	8/8/21
Larry Appelbaum	<i>Larry Appelbaum</i>	[REDACTED]	NOT an Appropriate Place	8/14/21

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Printed Name	Signature	Address	Comment	Date	
Walter Brennan	<i>Walter Brennan</i>	[REDACTED]		8/8/21	
Kim Brennan	<i>Kim Brennan</i>			8/8/21	
Domine Davis	<i>Domine Davis</i>			08/08/21	
Stephen Warren	<i>Stephen Warren</i>			8/8/21	
Denise Estridge	<i>Denise Estridge</i>			not against a crematorium, but needs a better location	8/16/21
Chelsea Bozick	<i>Chelsea Bozick</i>				8/16/21
Cheryl Bethke	<i>Cheryl Bethke</i>				8/16/21
Chelsea Keys	<i>Chelsea Keys</i>				9/14/21
Griffin Fuller	<i>Griffin Fuller</i>				9/7/21
Karen Montoya	<i>Karen Montoya</i>				9/19/21
Diana Smith	<i>Diana Smith</i>			Fire hazard Environmental toxins Air quality	9/17/21
Michelle Montoya	<i>Michelle Montoya</i>				9/17/21

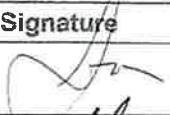

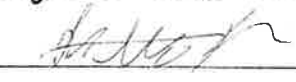


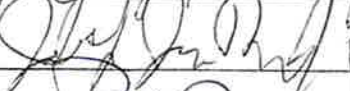

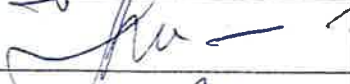




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Printed Name	Signature	Address	Comment	Date	
Shannon Gassman		[REDACTED]		8/23/21	
Gil Gassman				8/23/21	
Janet Wright					8/24/21
MIKE WRIGHT					8/24/21
Rosemary ...					8/24/21
Gina Hasler				2 Polson	8/25/21
STEPHEN WALSH					8/25/21
Craig Babcock				BAC	8/25/21
TIMOTHY NAMM				Rancho Cordova	8/26/21
Winona McLeod				sim	8-27-2021
ELIZABETH SOTO					8/27/21
Tommy Konwinski					8/27/21

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Printed Name	Signature	Address	Comment	Date	
Electeric		[REDACTED]	NO crematory	8-9-21	
Christine Catelli			NO crematorium!! DON'T WANT BUILDING IN OUR AREA!!!	8/9/21	
L. LITA ANON			NOT GOOD	8/19/21	
PATRICIA NAPPY			ANOTHER LOCATION	8/19/21	
+ Georgia Karam Catmeyer			NOT IN ZONE	8/22/21	
+ John James Paise					
Paul Earl Tomkinson					8/28/21
MICKEL LEIFER				NOT CREMATORY	8/28/21
Anaëlia Mathenax				Not Crematory	8/28/21
Paul Best				no crematory	8/29/21
Paula Sanchez				" "	8-31-21
Mike Sanchez				no crematory	8-31-21

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Printed Name	Signature	Address	Comment	Date
MARK PACE	<i>Mark Pace</i>	[REDACTED]	NO	08-21-21
DIANNE BEASLEY	<i>Dianne Beasley</i>	[REDACTED]	NO	08-21-21
BRETT ALLEN	<i>Brett Allen</i>	[REDACTED]	NO	8/23/21
Mike Gaide	<i>Mike Gaide</i>	[REDACTED]	NO	8/23/21
Mary Rigney	<i>Mary Rigney</i>	[REDACTED]	No Crematorium	8/23/21
Elizabeth New	<i>Elizabeth New</i>	[REDACTED]	NO	8/23/21
Regan Orilla	<i>Regan Orilla</i>	[REDACTED]	NO	8/23/21
Brett Robertson	<i>Brett Robertson</i>	[REDACTED]	no	8/23/21
KATHARINE PELLER	<i>Katharine Peller</i>	[REDACTED]	NO	9-3-21
Tanner Peller	<i>Tanner Peller</i>	[REDACTED]	NO!	9-3-21
JERI MORRIS	<i>Jeri Morris</i>	[REDACTED]	NO	9-3-21
Richard Matz	<i>Richard Matz</i>	[REDACTED]	No	9-3-21

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Printed Name	Signature	Address	Comment	Date
Debra K Howard	<i>[Handwritten Signature]</i>	[REDACTED]	<i>[Handwritten Comment]</i>	8/17/21
Dunise M Shaker	<i>[Handwritten Signature]</i>	[REDACTED]	This doesn't belong in a residential neighborhood - PERIOD!	8/17/2021
Megan Stockard	<i>[Handwritten Signature]</i>	[REDACTED]	<i>[Handwritten Comment]</i>	8/17/21
Jarrod Angeja	<i>[Handwritten Signature]</i>	[REDACTED]	I don't want any more smoke in the air than there already is. I have asthma.	8/21/21
Chad Shaker	<i>[Handwritten Signature]</i>	[REDACTED]		8/22/21
Sharon Kinzel	<i>[Handwritten Signature]</i>	[REDACTED]		9-15-21
Caron Farrell	<i>[Handwritten Signature]</i>	[REDACTED]	Weird	9-15-21
Sharon Frost	<i>[Handwritten Signature]</i>	[REDACTED]	gases both stored & emitted/unhealthy in residential area	9-16-21
Veronica Hamick	<i>[Handwritten Signature]</i>	[REDACTED]		9-17-21
Louise Mast	<i>[Handwritten Signature]</i>	[REDACTED]		9-17-21
Pamela Sowers	<i>[Handwritten Signature]</i>	[REDACTED]		9/18/21
Rob Busher	<i>[Handwritten Signature]</i>	[REDACTED]		9/18/21

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Printed Name	Signature	Address	Comment	Date
Debra Melzer	<i>[Signature]</i>	[REDACTED]	inappropriate location	10/20/21
Jillianne Allen	<i>[Signature]</i>	[REDACTED]		10/22/21
Donovan Devine	<i>[Signature]</i>	[REDACTED]		10/23/21
DENNIS DEVINE	<i>[Signature]</i>	[REDACTED]		10-23-21
CHRIS MAHONEY	<i>[Signature]</i>	[REDACTED]	Bad location choice for close to residential	11/3/21
Bobby McSherry	<i>[Signature]</i>	[REDACTED]	Resident don't want smoke/smell around water creation is more natural	11/6/21
Teff Martin	<i>[Signature]</i>	[REDACTED]		
Red Robinson	<i>[Signature]</i>	[REDACTED]	nope	11/12/21
Heather Briggs	<i>[Signature]</i>	[REDACTED]		11/12/21
Kari Barton	<i>[Signature]</i>	[REDACTED]	least	11/14/21
William Benton	<i>[Signature]</i>	[REDACTED]	bro people live here.	
Kelsie Kieffer	<i>[Signature]</i>	[REDACTED]		11/15/21

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Printed Name	Signature	Address/Contact Information	Comment	Date
Debbie Hamer	<i>Debbie Hamer</i>	[REDACTED]	to close to PARKS	12/19/21

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Printed Name	Signature	Address/Contact Information	Comment	Date
Elizabeth Velona	<i>Elizew</i>	[REDACTED]	not in resident area!	1/15/2022
JIM TIBERTI	<i>Jim Tiberti</i>	[REDACTED]	NOT IN RESIDENTIAL AREA. THIS IS RIDICULOUS!	1/16/2022
Jim Krentz	<i>JK</i>	[REDACTED]	Not here please!	1/17/22
Kara Krentz	<i>KK</i>	[REDACTED]	Not in residential area * Needs to be INDUSTRIAL	1/17/22

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-26-2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- A small business in Aptoma Professional Center across the street.*

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 08/26/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ✓ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- ___ OVER THE AGE OF 65 AND VULNERABLE.
- ✓ HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notes levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Sella Toyon

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/20/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

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NONCONFORMING USE OF OPEN SPACE

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Alyssa Chapman

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Date: 8/28/21

Dear Historic District Commissioners,

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/28/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Doris K Souza

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8.26.2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/26/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

 Jordan W. Pacy

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/27/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Gracie Joy Frey

Contact Information:

[Redacted Contact Information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/29/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LARGE PROPANE TANKS IN OPEN SPACE.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate an industrial crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for unborn children, developing children, elderly, and those with existing health conditions in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial or industrial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by *nonconforming use* of the land. This will harm the living to profit off of the dead. With only one way in and out for emergency vehicles, One fire will harm thousands. Such an industrial incinerator does not belong in open space.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

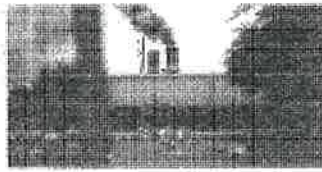
The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause *lasting damage on the physical, environmental and fiscal health* of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

FOLSOM HISTORIC DISTRICT



VOTE NO. 00
Lakeside N 05/10/2022 Item No. 18.
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/27/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study *fails* to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:
ELIZABETH SOTO



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Date: 8/9/21

Dear Historic District Commissioners,
I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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NONCONFORMING USE OF OPEN SPACE

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Myra Lindgren Huskel

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 5-30-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely, *Alec Foster*

Contact Information: 

Alec Foster

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-15-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Troy Bencuer

Contact Information:

[Redacted Contact Information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-12-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Kenton Foster
Kenton Foster

Contact information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/20/21

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Sincerely,

Garret Foster

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/23/21

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Sincerely,

Ciara Martinez

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/23/21

I AM:

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Sincerely,

Anne Martinez

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 5/20/21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

ROBERT BAILEY NORTH

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 08/20/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Michaela Balzer

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/17/21

I AM:

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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Ryan Grob 

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 9/9/21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.**

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Karen Hogg

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 08/09/21

- ___ **HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- ___ **A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Mya Adkinsworth

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Date: 8/9/2021

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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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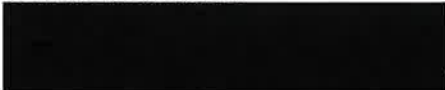
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Sincerely,


Contact Information:


PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Date: 8/8/21

Dear Historic District Commissioners,
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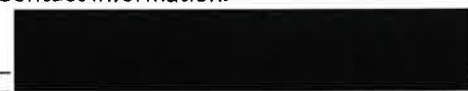
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Date: 8-8-21

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The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. With only one way in and out for emergency vehicles, One fire will harm thousands. Such an operation does not belong in *any child's backyard*.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

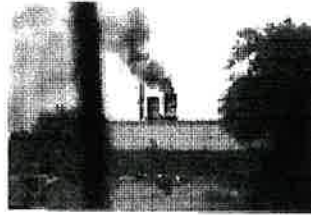
The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
**Lakeside Memorial
Lawn Crematorium**

Dear Historic District Commissioners,
I AM:

Date: 8-12-21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.**

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
**Lakeside Memorial
Lawn Crematorium**

Dear Historic District Commissioners,
I AM:

Date: 8-21-21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

W. Mitchell

Contact Information:

[Redacted]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/11/21

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- ___ OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Brett Mitchell

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/10/21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 08-08-2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-8-21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- ___ OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-27-2022

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- ___ OVER THE AGE OF 65 AND VULNERABLE.
- ___ VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- ___ HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LARGE PROPANE TANKS IN OPEN SPACE.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate an industrial crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study *fails* to report significant and potentially deadly levels for *unborn children, developing children, elderly, and those with existing health conditions* in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial or industrial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by *nonconforming use* of the land. This will harm the living to profit off of the dead. With only one way in and out for emergency vehicles, One fire will harm thousands. Such an industrial incinerator does not belong in open space.

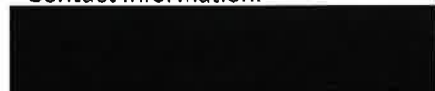
AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/30/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Robin Strunk
Robin Strunk

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/17/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Jessica Grebo

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-15-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

VICTORIA FOSTER

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/3/2021

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Kathy Kellogg

Contact Information:

[Redacted]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/3/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Date: 8-4-21

Dear Historic District Commissioners,

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
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Date: 4 AUG 21

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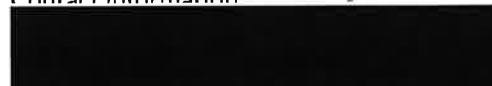
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PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 5/18/21

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Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/8/21

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Sincerely,

Contact Information:

Tonya Lucas

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-6-21

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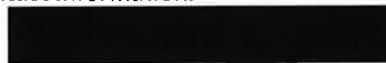
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Sincerely,

Carl D. Ervey

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/8/21

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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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Sincerely,

Anna Trayer

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/18/21

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Sincerely,


Randall Timmerman

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Date: 8/8/21

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Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 5-8-21

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Dear Historic District Commissioners,

Date: 5/15/22

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Robert S. Allen

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8-8-21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.**

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/23/22

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Date: 8/14/21

Dear Historic District Commissioners,

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Colleen Mesa

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-13-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/13/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/10/2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Mikaela Mulligan

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/7/21

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

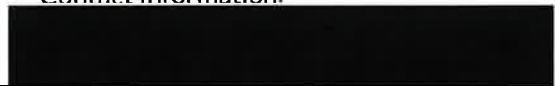
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- ___ **HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- ___ **A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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Sincerely,



Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 08/07/21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,


Celine Costello

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: August 12, 2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Barbara Mansell

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/12/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-8-21

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Sincerely,



Contact Information:

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Dear Historic District Commissioners,

Date: 8-8-21

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- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/4/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Deanne Giox

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Date: 8/3/21

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I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Date: 8/4/2021

Dear Historic District Commissioners,

I AM:

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

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- ___ **A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/9/2021

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Leslie Beach

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-10-2021

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Jan Foster

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/10/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Pamela Haupt

Contact Information:

[Redacted Contact Information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/8/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- ___ OVER THE AGE OF 65 AND VULNERABLE.

___ HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM. *thank*

X High fire risk area w/ crematorium propane
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Sincerely,

Mary Rigney

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Date: 8-9-21

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I AM:

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/14/21

- ___ **HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/12/21

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 08/13/21

I AM:

- ✓ **HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 5/12/21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
 A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
 AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
 OVER THE AGE OF 65 AND VULNERABLE.
 VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
 HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: Aug 8th 2024

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Carron Anderson

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Date: 8/14/21

Dear Historic District Commissioners,
I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-8-21

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/13/21

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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Sincerely,

S. Swannome

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/4/21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/1/2021

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Kathryn Corbett

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/7/21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Nancy Stream

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/10/2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 5/10/2021

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Sincerely,



Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Date: 8/14/2021

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I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Maria Gonzales

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/10/2021

I AM:

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

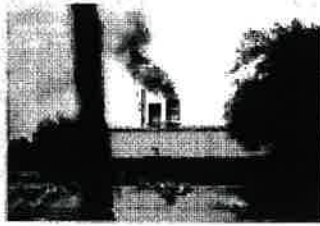
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/13/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Sasha Lueyer (Sasha Lueyer)

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/12/21

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:

[REDACTED]

KYLE
TERRY

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/14/21

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

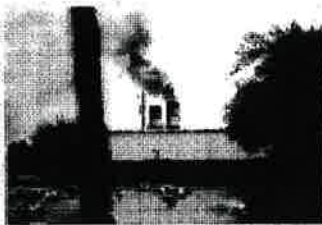
Sincerely,

 LISA DATTOLICO

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/15/21

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Sincerely,

 (PETER LUCYGA)

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/9/21

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Amanda Painter

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/6/21

I AM:

HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.

A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.

AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.

OVER THE AGE OF 65 AND VULNERABLE.

HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

Highly worried about the extreme fire risk caused by LP tanks in open space.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

 AUREE ROMER

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/6/21

I AM:

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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Kevin Romer

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/6/21

I AM:

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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Vicki Olson Romer

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/9/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 08/14/2021

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.**

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Sincerely,

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/15/2021

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also concerned about the fact this is in neighborhood

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Nicole Wells

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/15/2021

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Jolynn Marquez

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/15/2021

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Lasar Marquez

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/17/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Lynn Wickham

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/15/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Victor C. Gonzales

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/4/21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

ANDREW JOHNSON

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/4/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Faclyn Jomsan

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/7/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
 A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
 AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
 OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:
Jarrod Angele

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/7/2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Chad Shuker

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



**Vote NO on
Lakeside Memorial
Lawn Crematorium**

Dear Historic District Commissioners,
I AM:

Date: 8/8/21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.** *elderly*
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.**

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Kari Sewan

Contact Information:



**PROTECT
FOLSOM
HISTORIC
DISTRICT**



**Vote NO on
Lakeside Memorial
Lawn Crematorium**

Dear Historic District Commissioners,

Date: 8/8/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



**Vote NO on
Lakeside Memorial
Lawn Crematorium**

Dear Historic District Commissioners,

Date: 8-7-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/7/2021

I AM:

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Sincerely,

Contact Information:

Denise Shuker

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/10/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/4/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

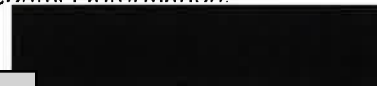
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Wendy y [signature]

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/4/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Jeff Martin

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/2/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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Sincerely,

Kathleen Noble

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

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Date: 8/4/21

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Sincerely,

 CHARLES NOBLE

Contact Information:

FOLSOM

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/14/21

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Sincerely,

Ainslee Voss

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



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Lakeside Memorial
Lawn Crematorium

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Date: 8/13/21

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8-14-21

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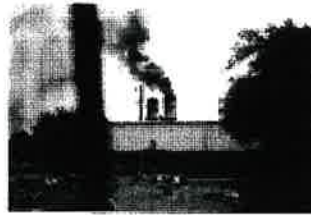
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Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



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Sincerely,

Joetta Schiffman

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

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Date: 08/14/21

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Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
**Lakeside Memorial
Lawn Crematorium**

Dear Historic District Commissioners,
I AM:

Date: 8/14/21

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Sincerely,

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



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Lakeside Memorial
Lawn Crematorium

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



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Lakeside Memorial
Lawn Crematorium

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Date: 8/8/21

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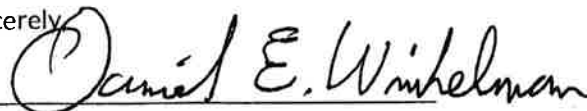
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Sincerely,



Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/3/2022

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Sincerely,

Kelley Swanson

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



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Lakeside Memorial
Lawn Crematorium

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Date: 8/3/2021

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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

95630

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-13-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

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NONCONFORMING USE OF OPEN SPACE

Not Zoned for industrial use 17.30.020(B)(6)

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Nancy J. Allan Adams
"Residential Receptor"

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/9/2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/9/2021

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Marjorie Wells

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-5-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,


Peter J. Sorensen

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-1-21

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Louise Barnett

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: Aug 4, 2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Shari Shively

Contact Information:

Folsom,

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: Aug. 4, 2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Daniel Shively

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 08-05-21

I AM: Alisha Moore

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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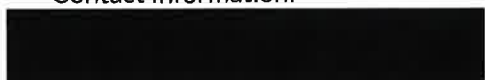
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Alisha Moore

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: ~~8/3~~
August 05, 2022

I AM: Diane Moore

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Diane Moore

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date 05-05-21

I AM: Jonathan Van Dyke

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Jonathan Van Dyke

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/2/21

I AM: *worried about the air quality and my daughter breathing the air.*

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.** *(A one year old)*
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

I AM: Strongly against this crematorium!

Date: 8-1-2021

I am 82 yrs. of age
I have COPD
This should not
be near homes
and children!

- ___ **HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- ___ **A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- ___ **AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- ___ **OVER THE AGE OF 65 AND VULNERABLE.**
- ___ **HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Carol J. Hart

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-1-2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

I am 85 years old and have lung issues. Please do not build this crematorium.

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Elaine M. Mouna

Contact Information



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/4/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN, *ON OCCASIONS*
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- " " LACK OF OPEN GOVT. ; DISREGARD OF FMC.*

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:



↓
CONSULT MY FORMAL COMPLAINTS

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/3/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 08 04 21

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- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-4-2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 14 Aug 2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
 A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
 AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
 OVER THE AGE OF 65 AND VULNERABLE.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

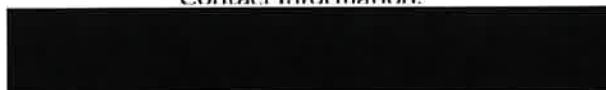
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Mari Beth Sexton

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: Aug 3, 2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.**

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Patricia Zuccaro

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: Aug 3, 2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Luz C. Ocampo

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-1-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

91 years old.
On behalf of Joan Sexton

Contact Information: *Joan Sexton*



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/9/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

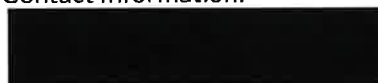
The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Thomas Osburn 

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 08/09/2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ✓ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- ___ OVER THE AGE OF 65 AND VULNERABLE.
- ✓ HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Tatiana Osborn
Osborn

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Date: 8/3/21

Dear Historic District Commissioners,

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Date: 8/3/21

Dear Historic District Commissioners,

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for unborn children, developing children, elderly, and those with existing health conditions in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by *nonconforming use* of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause *lasting damage on the physical, environmental and fiscal health* of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Michelle Carr

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-4-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
 A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
 AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
 OVER THE AGE OF 65 AND VULNERABLE.
 HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 05/03/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT HISTORIC FOLSOM DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Folsom Historic District Commissioners,

Date: 8-4-21

I AM: Fire

HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.

A CAREGIVER OF A DEVELOPING CHILD.

AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.

OVER THE AGE OF 65 AND VULNERABLE.

HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. As you know, Lakeside Memorial Lawn project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a conditional use permit, the following concerns require your attention and action on our behalf:

TOXICITY LEVELS FOR ADULTS V. CHILDREN

The Initial Study prepared by HELIX Environmental Planning, Inc. notes toxicity levels for many harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The report designates the levels as "not significant. This applies only to average, healthy adults. The Study fails to acknowledge and report the levels as significant and potentially deadly for **developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. The thousands of families, children, park visitors, trail and lake users shall not be inequitably impacted by a **nonconforming use** of the land that will harm the living to profit off of the dead. Such a business does not belong in *any child's backyard*.

AIR QUALITY, SMELL, PARTICULATE MATTER

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Air quality, smell, and particulate matter implications can have **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat is incredibly out of character for the District. Scent will be detected for miles, even if not visible. The smaller particulate matter and smoke created will be deadly to hundreds.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. VOTE "NO" CREMATORIUM.

Sincerely,

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-14-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

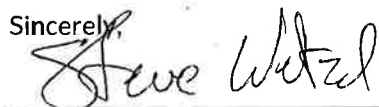
The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9-27-21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- ___ OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

The Initial Study by HELIX Environmental Planning, Inc. notates levels for harmful toxins including chromium, mercury, and organics. Such toxins become the *most dangerous when vaporized*. The smaller the particulate matter, the more dangerous it becomes. The report designates the levels as "not significant." This applies *only* to average, healthy adults. The Study fails to report significant and potentially deadly levels for **unborn children, developing children, elderly, and those with existing health conditions** in the neighboring community.

NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

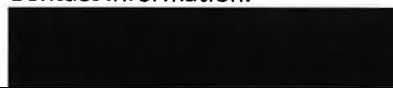
AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: Oct 11, 2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- ___ OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Laura Luke

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-8-2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

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NONCONFORMING USE OF OPEN SPACE

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-8-2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

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NONCONFORMING USE OF OPEN SPACE

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause *lasting damage on the physical, environmental and fiscal health* of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



**Vote NO on
Lakeside Memorial
Lawn Crematorium**

Dear Historic District Commissioners,

Date: 8/13/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.**

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

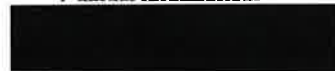
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

J. Van Voris

Contact Information



**PROTECT
FOLSOM
HISTORIC
DISTRICT**



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/13/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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Sincerely,

Holly Baller

Contact Information:

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/12/21

I AM:

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Sincerely,

Tiffany Guler

Contact Information: Tiffany Guler

[Redacted]

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



**Vote NO on
Lakeside Memorial
Lawn Crematorium**

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Date: 8-12-21

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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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Sincerely,

Mackenzie Falls

Contact Information:

Mackenzie Falls

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 5/12/21

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Sincerely,

Contact Information:

LARKIN ANDERSON



**PROTECT
FOLSOM
HISTORIC
DISTRICT**



**Vote NO on
Lakeside Memorial
Lawn Crematorium**

Dear Historic District Commissioners,
I AM:

Date: 9/14/21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information


**PROTECT
FOLSOM
HISTORIC
DISTRICT**



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 9/15/21

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Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/19/2021

I AM:

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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Alison Berry

Contact Information:

[REDACTED]

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



**Vote NO on
Lakeside Memorial
Lawn Crematorium**

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Date: 09/20/21

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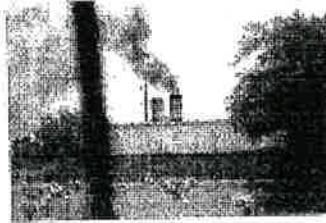
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Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Date: 10/14/21

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Sincerely,

Debra Heber Stanley

Contact Information:

[Redacted Contact Information]

PROTECT FOLSOM HISTORIC DISTRICT



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**Lakeside Memorial
Lawn Crematorium**

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Date: 9/19/21

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Sincerely,

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



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Lawn Crematorium

Dear Historic District Commissioners,

Date: 10/15/21

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Sincerely,

Christina Napier

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

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- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 04/25/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Phoebe Moralde

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 10/29/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

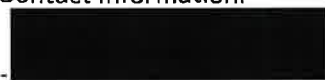
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Thomas Shimozaka

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 11-5-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Bobby McSherry

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/19/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Kim Ocampo

Contact Information:

Kim Ocampo

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/14/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

LEVI OCAMPO

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8-16-21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Date: 8/14/24

Dear Historic District Commissioners,

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Justin C. Keenan

Contact Information:



com

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/13/21

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

R.S. KRAEGEN

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/17/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-2-21

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Shen Erachian

Contact Information: _____

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 08/02/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-3-21

I AM: Joseph Lyons

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information: _____



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: August 3, 2021

I AM:

- Concerned about fire risk
- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:
Brian Paciotti

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-21

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-2-2021

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Mary Matthews

Contact Information: [REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/2/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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Sincerely,

John Ogburn

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/2/2021

I AM:

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-1-21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

M McClone

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-9-21

I AM:

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- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Date: 8-7-21

Dear Historic District Commissioners,

I AM:

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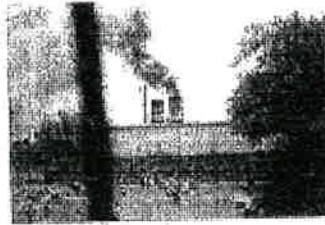
PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Date: 8/7/21

Dear Historic District Commissioners,

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8.6.22

I AM:

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Sincerely,

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Vote **NO** on
Lakeside Memorial
Lawn Crematorium

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Date: 08/01/2021

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-3-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Timothy Thomas

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 0-3-21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

N/A

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-3-21

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

S/A

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/4/21

I AM:

df
df
df
df
df
df

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Joy Hays

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 3 AUG 2021

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

 CHRISTOPHER HAYS

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-4-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Frances Servente

Contact Information:

[Redacted]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/4/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- ___ OVER THE AGE OF 65 AND VULNERABLE.

HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

Highly concerned about the extreme fire risks of liquid propane tanks in open space!

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Kaitee Hays

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/4/21

- I AM: **SPEAKING FOR MY FAMILY: CHILDREN, GRANDCHILDREN**
- ___ **HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- ___ **A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- ___ **AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- ___ **OVER THE AGE OF 65 AND VULNERABLE.**
- ___ **HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

PLEASE MAIL CITY ENGINEER SEALED/SIGNED REPORT

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FIRE MARSHAL REPORT TO OFF. CA ST. FIRE MARSHAL

TOXICITY LEVELS UNSAFE FOR CHILDREN, VULNERABLE

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/6/2021

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Date: 8/2/21

Dear Historic District Commissioners,

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-1-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM

Sincerely,

Cynthia Delaney

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-1-21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Lindsey Harbison

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/2/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely, 

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/2/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Michelle Gates

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/2/21

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Cathy Owen

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-3-2021

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Rally Hayes

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/3/2021

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 2 Aug. 2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: Aug 2, 2021

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Helen Melus

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-5-21

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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Sincerely, *Jennifer K. Sorensen*
Jennifer K. Sorensen

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-6-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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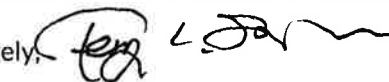
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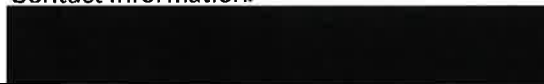
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,


Terry L. Sorensen

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/1/2021

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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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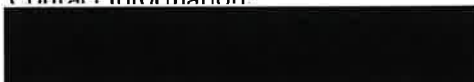
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Cindy Baker

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Robert E Baker

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Date: 8/6/21

Dear Historic District Commissioners,

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- Highly worried about the extreme fire risk caused by LP tanks in open space.*

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Raumer

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

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- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-11-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Morgan Day 

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Date: 8/9/21

Dear Historic District Commissioners,

I AM:

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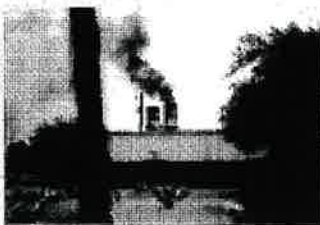
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/8/21

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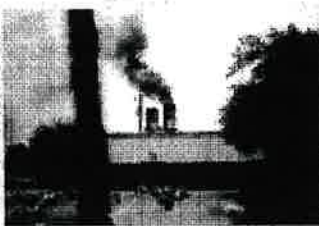
Sincerely,

Susan J.F. Lueyga *Susan J.F. Lueyga*

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Date: 8/8/21

Dear Historic District Commissioners,

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



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Lakeside Memorial
Lawn Crematorium

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Joyce L. Kramer

Contact Information: _____

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-1-2021

I AM:

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Sincerely,

Wance Somerville

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8.2.2021

I AM:

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Sincerely,

PAUL A. TYLER

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: August 1st 2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
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Sincerely,

Cherelle Peihun

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

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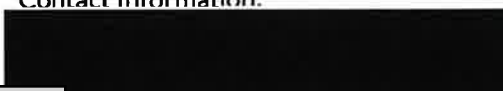
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Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



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Lakeside Memorial
Lawn Crematorium

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Sincerely,

Contact Information:

William Rowe

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

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- ___ **HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Evelyn gates

N/A

PROTECT FOLSOM HISTORIC DISTRICT



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Lakeside Memorial
Lawn Crematorium

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Sincerely,

Contact Information:

Jackson Gates

N/A

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 08/03/21

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Edwin Sanchez

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/3/21

I AM: Very worried on the days we have an unsafe Air Index Quality, the Crematorium would add to very high toxicity levels in the air.

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Sheila Krager

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 7/31/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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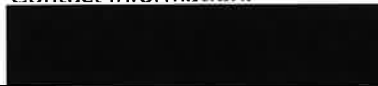
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/8/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/8/21

I AM:

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/8/21

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Katie Hoag

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/17/2021

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Beth Larson

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/18/21

- ___ **HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- ___ **A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- ___ **AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/21/21

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Sincerely,

Mirra Hafeman

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-21-21

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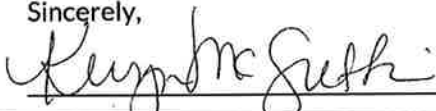
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



Vote No on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/21/21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Kevin Fox

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Date: 8/21/21

Dear Historic District Commissioners,
I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Dinae Houck

Contact Information:

[REDACTED]

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/21/21

I AM:

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

James M. Fox

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 5/21/21

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Date: 9/21/2021

Dear Historic District Commissioners,

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/21/21

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Sincerely,

Contact Information:



**PROTECT
FOLSOM
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Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8.21.21

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Sincerely,

Contact Information



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Date: 8/21/21

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

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I AM:

Date: 5/10/22

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Sincerely,

Max Boling

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/24/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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Sincerely,

Contact Information

PROTECT FOLSOM HISTORIC DISTRICT



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Lakeside Memorial
Lawn Crematorium

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

*Should have been put in **PRIOR** to houses being built,*

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: Sept. 20, 21

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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Sincerely,

Jennifer Lane

Contact Information:



Formerly HDC Commissioner.
Formerly Planning Commissioner
City of Folsom
for 10 years

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-15-21

I AM:

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Sincerely,



Doug Barton

Contact Information:

[Redacted contact information]

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Lakeside Memorial
Lawn Crematorium

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-15-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



05/10/2022 Item No. 18.

Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 5-17-22

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



05/10/2022 Item No. 18.

V
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9-11-2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



V

05/10/2022 Item No. 18.

Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/16/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

ROCHELLE MORALDE / JOHN HORRELL

Contact Information:

**PROTECT
FOLSOM
HISTORIC
DISTRICT**



**Vote NO on
Lakeside Memorial
Lawn Crematorium**

Dear Historic District Commissioners,

Date: 9-14-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

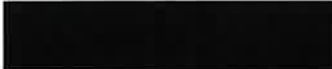
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Victoria Smyke

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/12/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Louise & Patrick O'Donnell

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9-11-21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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Sincerely,



Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/13/2021

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Salwa Kasobian

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/15/21

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

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- ___ **A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Karen Davies

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-3-21

I AM:

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/1/21

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- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LP TANKS IN OPEN SPACE.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate a crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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NONCONFORMING USE OF OPEN SPACE

The proposed crematorium would be installed and operated in designated open space that is *not zoned for commercial use*. Thousands of families, children, park visitors, trail and lake users will be inequitably impacted by **nonconforming use** of the land. This will harm the living to profit off of the dead. With only one way in and out for emergency vehicles, One fire will harm thousands. Such an operation does not belong in *any* child's backyard.

AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

The Folsom Historic District is treasured for its legacy of community, architecture, and natural appeal. Poor air quality, smell, and toxic particulate matter will cause **lasting damage on the physical, environmental and fiscal health** of our community. Visually, the shed, smoke, and heat waves are incredibly out of character for the District. Scent will be detected for miles, even if not visible. A permanent pollutant, foul smell and toxins will leave a damaged legacy.

PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Patricia Nappi

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/5/2021

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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It will affect you economically because of lack of tourism.
PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Cheryl Adler

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-10-21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Kyle Terry 

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-10-21

I AM:

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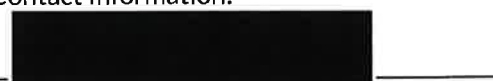
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

MATTHEW MCBRIDE 

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/10/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:

sasha lucyga

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- ___ **HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- ___ **A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- ___ **AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Karen Ames

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 3 Sept 21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

Paul Amos

PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/3/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM

Sincerely,

Jeri Mom

Contact Information [Redacted]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/3/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- VERY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.
- HIGHLY CONCERNED ABOUT EXTREME FIRE RISK CAUSED BY LARGE PROPANE TANKS IN OPEN SPACE.

I write to you with great concern about Lakeside Memorial Lawn's application for a conditional use permit to install and operate an industrial crematorium. Lakeside Memorial Lawn's project site is designated as Open Space and includes historical burial grounds. In reviewing Lakeside Memorial's application for a condition use permit, the following concerns require your attention and action on the community's behalf:

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

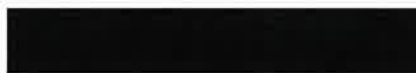
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Tanner Pelle

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/24/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- ___ AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,



Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/23/21

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8.23.21

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AIR QUALITY, SMELL, PARTICULATE MATTER MAKING HISTORY

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Carla Hame

Contact Information:

[REDACTED]

FOLSOM HISTORIC DISTRICT



VOT
Lakeside M
Lawn Crematorium

05/10/2022 Item No. 18.

Dear Historic District Commissioners,

Date: 8/23/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.
- OVER THE AGE OF 65 AND VULNERABLE.
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.

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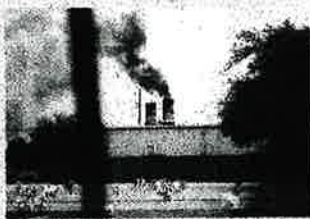
PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-20-21

I AM:

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- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
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Sincerely,

Contact Information:



Dear Historic District Commissioners,

Date: 08-21-21

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Monty Rose

Contact Information:





Dear Historic District Commissioners,

Date: 08.21.21

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Sincerely,

DIANNE BEASLEY

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-20-21

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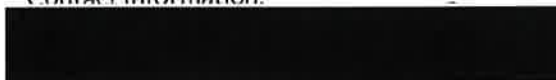
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Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/21/21

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Sincerely,



Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote NO on Lakeside Memorial Lawn Crematorium

Date: 8/21/21

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Sincerely,

Paul Erickson 1104 Fowey Ct

Contact Information:

[Redacted contact information]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/15/2021

I AM:

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Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/4/21

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Karen Sepulveda

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9.6.21

I AM:

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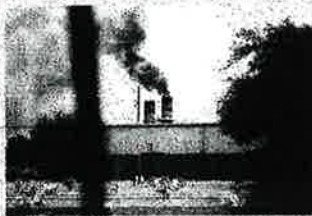
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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: _____

I AM:

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
- ___ A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.
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Sincerely,

Contact Information:

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Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/3/21

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PROTECT HISTORIC FOLSOM. PROTECT THE VULNERABLE. NO CREMATORIUM.

Sincerely,

Autumn Hemington

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 9/10/21

I AM:

- HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.**
- A PARENT OR CAREGIVER OF A CHILD OR CHILDREN.**
- AN INDIVIDUAL WHO INTENDS TO HAVE CHILDREN.**
- OVER THE AGE OF 65 AND VULNERABLE.**
- HIGHLY WORRIED ABOUT THE SAFETY & LEGACY OF HISTORIC FOLSOM.**

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Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: Sept 9, 2021

I AM:

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Sincerely,

Jonnie Colosimo

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



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Date: 9-3-21

I AM:

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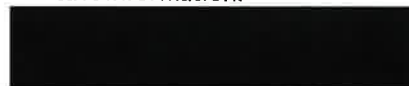
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Sincerely,

Mary Kelley Colla

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/30/21

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Sincerely,





PROTECT FOLSOM HISTORIC DISTRICT



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Lakeside Memorial
Lawn Crematorium

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I AM:

Date: 8/30/21

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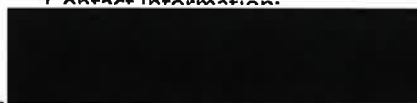
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Sincerely,

Contact Information:



PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 8/28/21

I AM:

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Sincerely,

S. M. F. Chel

Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,
I AM:

Date: 8/24/21

- ___ HIGH RISK FOR AIR QUALITY HEALTH IMPLICATIONS.
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Sincerely,



Contact Information:

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on Lakeside Memorial Lawn Crematorium

Dear Historic District Commissioners,

Date: 8-23-21

I AM:

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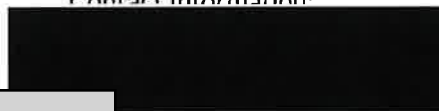
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Lakeside Memorial
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Sincerely,

Maissa A. Fenton

Contact Information:

[REDACTED]

PROTECT FOLSOM HISTORIC DISTRICT



Vote **NO** on
Lakeside Memorial
Lawn Crematorium

Dear Historic District Commissioners,

Date: 09-08-2021

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Contact Information:

[Redacted contact information]

Attachment 3

Public Comment Letters received after the February 16, 2022 Historic District Commission Meeting

Terry Sorensen

Folsom, CA 95630

February 15, 2022

Historic District Commission
City of Folsom
50 Natoma Street
Folsom, CA 95630

RE: Lakeside Memorial Lawn Crematorium Proposal (PN-19-182)
Hearing Date: February 16, 2022

Dear Commissioners:

In my correspondence to you of February 7, 2022, I pointed out that an identical request for a CUP authorizing the construction and operation of a crematory at the Lakeside Cemetery was made back in 2003 and resulted in a Staff Report recommendation of denial. In brief, the Findings for Denial in support of that recommendation were as follows:

1. Lakeside is a historic cemetery and the introduction of a crematory on the site was inconsistent with such historic cemetery use;
2. Lakeside is, in fact, a conglomeration of historic cemeteries dating back to around 1850 which, when considered in conjunction with the adjacent Chung Wah Chinese Cemetery and the adjacent California State Dredger Tailings Preserve, creates a rare combination of unique cultural resources in one small area;
3. The crematory use applied for will be detrimental to the health, safety, or general welfare of the neighborhood and the surrounding City as a whole in that the introduction of such use would impact the historical character of the cemetery as well as the historical use of the area; and
4. The use of the proposed project is inconsistent with Goal 2 of the City's Historic District Design and Development Guidelines in that it did not maintain the historic use of the site and, in addition, did not further Design and Development Guideline policies 2.1, 2.2, and 2.3. Specifically, the County Historic Cemetery Commission has stated that a crematory is not a consistent use with a historic cemetery and has identified the site as locally significant and intends to present the cemetery to the Board of Supervisors for designation as a historic pioneer cemetery. Furthermore, approval of a CUP for such a contemporary use would jeopardize the eligibility status of the site and discourage, rather than encourage, national register nomination for the cemetery.

Based on the foregoing it was the opinion of this writer that the logic and reasoning employed by the City in its 2003 Staff Report would be followed by the City on this current iteration of the crematory proposal. But that trust in logic and reason proved ill-founded when the City recommended approval of a CUP for the crematory in its Staff Report released February 10. However, in doing so, the City set forth only two factors on which it relied in attempting to justify its departure from its 2003 decision on the exact same proposal. Those two factors, expressed on pages 29-30 of the Staff Report, are as follows:

1. That cremation technology "has improved significantly since 2003"; and
2. That the Sacramento County Cemetery Commission did not provide any comment regarding the current iteration of the proposal as it did in reference to the 2003 version.

In the opinion of this writer, this attempt by the City to distinguish its position on the 2003 crematory project from its current contrary position is incredibly weak and almost laughable. As to the first comment by the City (regarding cremation technology), the comment must be dismissed as irrelevant to the discussion at hand. The 2003 recommendation for denial was *not* based on the state of crematory technology (good or bad), at all, but rather on the fact that the presence of a crematory on the property was not compatible with the historical character of the cemetery.

Similarly, the second comment (regarding the lack of any expression of concern from the Sacramento County Cemetery Commission in reference to this renewed crematory effort) must be dismissed, as well. If the cemetery was deemed a historic cemetery of local significance and worthy of County designation as a historic pioneer cemetery with potential national recognition back in 2003, it certainly does so today absent some convincing evidence to the contrary. Furthermore, the conclusion expressed by the Cemetery Commission that the presence of a crematorium is not consistent with a historic cemetery remains valid, as well.

In conclusion, the reasoning behind the City's 2003 recommendation for denial of a CUP for the construction and operation of a crematorium on the grounds of the Lakeside Cemetery remain valid. The Findings expressed by the City in its 2003 Staff Report in support of that denial are compelling, have not been addressed, at all, by the City in its current Staff Report, and therefore remain conclusive on the issue at hand and mandate that this CUP request be denied.

Very truly yours,

/s/

Terry L. Sorensen

dg/TS

cc: Historic District Commissioners and City of Folsom Staff
kcolepolicy@gmail.com; justin@revolutionsdocs.com; danwestmit@yahoo.com;
ankhelyi@comcast.net; johnfelts@e55tech.com; m.dascallos@yahoo.com;
info@johnlanephoto.com; kmullett@folsom.ca.us; jkinkade@folsom.ca.us;
sbanks@folsom.ca.us; sjohnson@folsom.ca.us; pjohnson@folsom.ca.us

February 16, 2022

City of Folsom
 Historic District Commission
 50 Natoma Street
 Folsom, CA 95630
 via email to: kmullett@folsom.ca.us for distribution to HDC

SUBJECT: Lakeside Crematorium - Comments for February 16, 2022, HDC Hearing

Dear HDC Commissioners:

This letter is to express my objection to the proposed Lakeside Crematorium Lakeside Memorial Lawn Crematorium as currently described and evaluated in the January 2022 Initial Study/Mitigated Negative Declaration (“IS/MND”); the staff report and attachments made available on February 10, 2022 (dated February 16, 2022) and included in the HDC’s February 16, 2022, meeting packet (“Staff Report”); and February 15, 2022, Topical Responses to Comments memorandum made available sometime after 4 p.m. on February 15, 2022 (“Topical Responses Memo”) for reasons including the following and as discussed in more detail in the remainder of this letter.

1. The Historic District Commission does not have authority to approve the project.
2. The proposed crematorium exhaust stack is not sufficiently described or illustrated to provide the necessary understanding of its design and appearance.
3. Design Review in compliance with the Folsom Municipal Code is required for the proposed shed modification.
4. The General Plan has no land use designation for either a cemetery or a crematorium, therefore the analysis cannot tier from the General Plan EIR.
5. The project description must identify whether the project would involve public attendance at services at the Lakeside Memorial cemetery and/or other locations within the City and, if so, define the parameters and evaluate impacts associated with such services.
6. The Staff Report and IS/MND fail to recognize the visibility of the existing shed and proposed modifications from public view locations (including Folsom Boulevard) and the impacts of such visibility on visual quality and locally designated historic resources.
7. The IS/MND does not adequately evaluate potential impacts on nesting and foraging bald eagles and other special-status bird and bat species.
8. The Staff Report’s consideration of fire risk is frighteningly dismissive and warrants a full evaluation and definitive determination by the City Fire Department and California State Parks.

1. The Historic District Commission (HCD) does not have authority to approve the project.

The City of Folsom Charter at Section 4.07, “Boards and Commissions,” establishes the City Council’s authority to create Boards and Commissions and to prescribe the powers and duties of such Boards and Commissions. However, Section 4.07 of the City Charter expressly states that “[a]ll boards and commissions only shall be advisory to the Council.” The City Charter

February 16, 2022

may be amended only by a vote of the citizens of the City of Folsom and the citizens of Folsom have not delegated approval authority to the HDC.

Neither City staff, the HDC, nor the City Council has the authority to amend or disregard this limitation on the HDC's authority. Therefore, to function within the limitations prescribed by the citizens of the City of Folsom in the City Charter, HDC decisions may not constitute final approvals. Instead, HDC decisions must be treated as advisory recommendations to the City Council for the City Council's final consideration and decision of whether to approve or otherwise take final action on a project.

2. The proposed crematorium exhaust stack is not sufficiently described or illustrated to provide the necessary understanding of its design and appearance.

Staff report Attachment 8 (meeting packet pages 62 and 63) illustrate a blurred and disproportionate black square that apparently is intended as the applicant's rendering of the proposed crematorium stack (inserted as Figure 1 below). The so-called rendering looks akin to a plastic garbage bag covering a rooftop air conditioner and is meaningless for demonstrating the actual visual appearance and height of the project exhaust stack. The applicant's rendering fails to demonstrate the actual height (which would extend to over 10 feet above the shed rooftop) and looks nothing like any of the five exhaust stacks illustrated in the representative crematorium photographs included in meeting packet pages 298 through 303. The representative photographs (discussed further below) illustrate at least five different crematorium exhaust stack designs, demonstrating the variation and diversity in design options for an exhaust stack, yet, it appears that no real effort has been made to consider and present an actual design and visual appearance of the exhaust stack that would be installed for the project. The stack's design and visual appearance is critical for the required design review and to the CEQA analysis of impacts associated with visual character, historical resources, and fire risk.

Figure 1. Applicant's Rendering of Exhaust Stack (from meeting packet pg. 62)



February 16, 2022

3. Design Review in compliance with the Folsom Municipal Code is required for the proposed shed modification.

The proposed shed modification with addition of the exhaust stack requires design review pursuant to Folsom Municipal Code (FMC) section 17.52.300, "Design Review," which requires design review for, "*B. All exterior renovations, remodeling, modification or addition to existing structures.*" FMC Section 17.52.310 provides design review application submittal (including design review application fee) requirements. The Community Development Department has erred by not identifying *design review* as a necessary entitlement and, therefore, by not requiring the project applicant to submit an application for design review. The Staff Report notes that comments on the IS/MND raised the issue of "whether a design review application is warranted" (meeting packet page 28) but the Staff Report provides no explanation of why a design review application has not yet been required for this project.

An application for design review containing the required submittals must be submitted and design review application fees paid before a decision regarding the CUP can be made. The design modification addition of a 10-foot exhaust stack above the roof of the existing shed is an integral component of the proposed use. Importantly, design review would consider whether the structure modification is consistent with Historic District design standards and guidelines or if the design of that structure requires additional modifications to comply with Historic District design standards and guidelines. The design review would necessarily consider the actual proposed design and visual character of the project exhaust stack which is currently unspecified.

As noted at item 2, above, the Staff Report (meeting packet pages 298-303) clearly demonstrates that various diverse designs for an exhaust stack are possible; but the design must be vetted through a public review and decision-making process, not ignored or addressed as an afterthought. Two examples from the Staff Report are presented in Figure 2 below and demonstrate the variation and importance of selecting an appropriate design for the exhaust stack. Without design review, the exhaust stack could be constructed with an intrusive oversized vertical pipe appearance (left photo) whereas with design review the HDC could ensure that the exhaust stack is thoughtfully designed in consideration of its context which is the very reason for design review for structure modifications in the Historic District.

Figure 2. Exhaust Stack Design Variability Examples



February 16, 2022

4. The General Plan has no land use designation for either a cemetery or a crematorium, therefore the analysis cannot tier from the General Plan EIR.

The IS/MND incorrectly states that the project would be consistent with the General Plan land use designation of Open Space. A review of the General Plan intent for the Open Space land use designation reveals that there is no basis for concluding that a crematorium is consistent with the General Plan Open Space land use designation. The General Plan mentions "cemeteries" just one time and crematoriums not at all.¹ Table LU-5 of the General Plan (page 2-7) defines the Open Space designation as, "The Open Space land use designation encompasses the preserved natural open space areas of Folsom." Throughout the General Plan, policies encourage that development incorporate areas of open space. It is unreasonable to suggest that the intent of those policies is that such open space areas could or might be used for siting a cemetery and a crematorium.²

The IS/MND's incorrect interpretation and the subsequent analysis which *tiers* from the General Plan EIR are fundamentally flawed. The IS/MND must be revised to eliminate the erroneous approach to General Plan consistency, and the impact analyses must fully evaluate the project without attempting to tier from the General Plan EIR.

Further, while FMC section 17.52.550 identifies "cemeteries" as a permitted use in the Open Space/Public primary area of the Historic District (subject to a conditional use permit when proposed by a private entity), the FMC does not extend the definition of cemetery to a crematorium. Additionally, the FMC is subordinate to, and may not conflict with, the General Plan. Therefore, attempting to expand the unspecified FMC definition of a cemetery to include a crematorium (and especially as an "accessory use" as asserted in the February 15 Topical Responses Memo; pg. 3 of 4) even further strays from the General Plan's Open Space definition and is impermissible.

5. The project description must identify whether the project would involve public attendance at services at the Lakeside Memorial cemetery and/or other locations within the City and, if so, define the parameters and evaluate impacts associated with such services.

Neither the Staff Report nor the IS/MND provide information on whether cremations (up to 4 per day and 500 per year) would or could be attended by family, friends, or other members of the public. If no such attendance will be permitted, a condition of any use permit for this project must specifically state that such attendance is prohibited and must include a mechanism to ensure the prohibition is enforced. Alternatively, if such attendance will be permitted, the IS/MND must be revised to discuss the maximum anticipated attendance at each cremation and evaluate the impacts associated with vehicle trips, noise, parking capacity, neighborhood vehicle circulation and pedestrian safety, effects on other services and activities at the cemetery, and other factors associated with public attendance.

¹ The single General Plan cemetery reference pertains to Noise Compatibility Standards (Table SN-1) which is unrelated to establishing land use designations and uses

² Example: Policy LU 3.1.1 - "Encourage mixed-use development in nodes located at major intersections that include housing, open space, and offices." The IS/MND's interpretation would suggest that the expectation of that policy is for those open space areas to be eligible for siting a crematorium.

February 16, 2022

Furthermore, neither the Staff Report nor the IS/MND discuss whether cremations at the proposed Lakeside Crematorium would result in an increase in memorial services either at Lakeside cemetery or elsewhere in Folsom. If cremations at Lakeside cemetery would result in an increase in services at other locations in Folsom (e.g., the funeral home on Scott Street), similar evaluations of potential impacts associated with vehicle trips, noise, parking capacity, neighborhood circulation and pedestrian safety and other factors associated with those services must be addressed.

6. The Staff Report and IS/MND fail to recognize the visibility of the existing shed and proposed modifications from public view locations (Folsom Boulevard) and the impacts of such visibility on visual quality and locally designated historic resources.

The IS/MND aesthetics and cultural resources analyses are fundamentally flawed by failing to recognize that the existing structure is visible from public viewpoints including Folsom Boulevard and the bluffs on the north side of Lake Natoma. Page 2 of the IS/MND incorrectly states that “[t]ailing piles between the site and Folsom Boulevard prevent the site from being visible from that street.” This is incorrect. The roof and upper portion of the structure in which the crematorium is proposed to be located and on which an exhaust stack would be installed is clearly visible from Folsom Boulevard near the intersection of Natoma Street. The February 15 Topical Responses Memo discusses that the “site of proposed modifications is already largely shielded by tailings piles and concludes that the site of proposed modifications is already largely shielded from public view due to the presence of dredge tailing piles and would remain so” and then asserts with regard to the exhaust stack that “[t]his physical improvement to an already existing metal structure would not significantly impact the visual character of the project setting” but without discussing locations from where the shed *is* visible, providing no discussion of the visual appearance of the exhaust stack or analysis of how the stack’s would visually intrude on the quality of existing views, and no basis for a concludes that the impact would not be significant.

Figure 3 on the following page is a photograph of the viewshed toward the site taken Sunday, February 6, 2022, from the south side of Folsom Boulevard just east of the Natoma Street intersection and facing northwest toward the project site. The project shed as well as a recently constructed larger shed are both clearly visible from this segment of Folsom Boulevard. Folsom Boulevard is a heavily traveled public road with and adjacent public light-rail line and bicycle path, all from which the existing structures are clearly visible and from which the crematorium exhaust stack would be visible extending 10 feet or more above the heights of the existing structures.

Between Folsom Boulevard and the structure are cobble mine tailings that are identified in the City of Folsom Cultural Resources Inventory as import local historic resources. This section of tailings is one of the most prominent locations of representative historic mine tailings visible to the largest number of viewers anywhere in the City. The impact of the project’s structural modification with the addition of the exhaust stack would be visible in public views from and near segments of Folsom Boulevard, light-rail passengers, and bicycle path users (cyclists and pedestrians) and must be identified and evaluated in terms of impacts to the quality of views of the tailings and impacts to the historic quality of the tailings viewshed. The visual intrusiveness and inconsistent character of an exhaust stack of any design would be visible to a large number of viewers and would substantially and adversely affect the quality of the viewshed, and would result in a significant projects specific impact and cumulative impact in consideration of the substantial view modification caused by the

February 16, 2022

recently constructed shed. Mitigation, including alternatives to the proposed project, must be considered for this significant impact.

Figure 3. Viewshed from Folsom Boulevard



7. The IS/MND does not adequately evaluate potential impacts on nesting and foraging bald eagles and other special-status bird and bat species.

The Staff Report, IS/MND, and Topical Responses Memo fail to provide any discussion or analysis of impacts to the annually active bald eagle nest located just 0.5 mile north of the project site and the potential effects of the project and exhaust stack on foraging behavior of the eagles or other protected bird and bat species. While the IS/MND discusses that effects of vehicles and workers at the site would not adversely affect migratory birds, the analysis does not address the potential effects on foraging activity of the furnace exhaust heat blast with an assumed exhaust gas temperature of over 1,000 degrees Fahrenheit (°F) and a gas exit velocity of 14.7 feet per second that would occur for up to 90 minutes up to four times a day. The furnace exhaust blast would have the potential to adversely affect foraging behavior and could also result in direct injury or death of individual birds, including bald eagles and other protected species.

8. The Staff Report and IS/MND's consideration of fire risk is frighteningly dismissive and warrants a full evaluation and definitive determination by the City Fire Department and California State Parks.

The Staff Report and IS/MND consideration of potential fire risk associated with the project is insufficient. The discussion of potential exposure to wildland fire risks downplays and fails to provide a meaningful analysis of the project's potential fire risk. The IS/MND discusses

February 16, 2022

that the “project site is located in an urbanized area in the City of Folsom.” In fact, the project site contains and is located immediately adjacent to substantial oak woodland areas and oak canopy adjacent to the building proposed to house an exhaust flume with an assumed exhaust gas temperature of 1,080 degrees Fahrenheit (°F) and a gas exit velocity of 14.7 feet per second.

Within and adjacent to the site are oak canopy linkages to the large oak woodland open spaces to the north, west, and south of the project. The applicant’s rendering shown above, clearly shows tree canopy near the proposed exhaust stack location. The IS/MND states that, “the project is not likely to cause any ignition, given that the crematory will not emit sparks.” This conclusion is frighteningly dismissive. Evidence providing a definitive conclusion that the crematory – a facility designed for burning and with an exhaust stack emitting temperatures of over 1,000 °F – will not create an eminent fire risk

Furthermore, the IS/MND discusses that the City Fire Department reviewed the project and did not raise any concerns regarding *water supply or site access*. This fails to address whether the Fire Department raised other concerns and even suggests some uncertainty of whether the Fire Department reviewed and considered the project at all. The Folsom Fire Department’s specific consideration of the potential fire risk associated with the project must be provided and with assurances that the Fire Department has considered *actual existing site conditions* including the large new structure blocking emergency vehicle maneuverability near the project shed that was not identified in project drawings until just a few days ago.

The proposal to install and operate a large furnace in an open space area adjacent to oak woodlands with residences beyond warrants specific review and documented feedback from the Folsom Fire Department specifically confirming that the Fire Department has carefully reviewed the project and all potential fire risk issues. Also, because the project site is immediately adjacent to lands managed by State Parks, similar definitive review and input from State Parks wildland fire experts should be documented and included in the analysis.

Thank you for considering my comments.

Sincerely,



Bob Delp
Historic District Resident
Folsom, CA 95630

Josh Kinkade

From: LJ Laurent [REDACTED]
Sent: Tuesday, February 15, 2022 3:35 PM
To: Kelly Mullett; Rosario Rodriguez; Sarah Aquino; YK Chalamcherla; Mike Kozlowski; Kerri Howell; Josh Kinkade; Pam Johns
Cc: ernest.conant@usbr.gov; Drew Lessard; Elisabeth G. Lucas; blm_ca_web_re@blm.gov; Dale Kasler; daoffice@sacda.org; Rhonda Lamoureux; john.baum@waterboards.ca.gov; Eileen Sobeck; Lydia Konopka; Steve Krahn; Ken Cusano; Lauren Ono; kcra_news_tips; DESK
Subject: Crematory PUBLIC COMMENT: pics PROVE deceit furnace issue

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To: Folsom: whoever is in charge, if anyone
 From: LJ Laurent, LNS Resident abutting furnace/LPG site
 February 15, 2022

Re: HDC Staff APPROVAL recommendation to APPROVE this falsehood-laden application

Context: Neighbor took photos revealing fully the fraudulent documents filed by owners and filed by city with higher officials.

Clearly Health, Safety, and Fire Regulations mean Nothing to this city of "approve everything whether illegal & fraudulent in process."

COMMENTS in re pre-approved LPG tanks and furnace or multiple furnaces with Zero Access and Zero Water for explosions/fires.

Does our silent/ inactive City Engineer S. Krahn know the background and expertise of this Commenter?

Does S. Krahn anticipate Complaints against his License which requires him to Certify/Seal/Sign all approvals for New Projects?

Does it bother our silent engineer that there IS NOT ROAD ACCESS sufficient to handle Explosions of LPG which has huge blast range?

What does our City Engineer say about this Parcel having only a tiny 3" water supply line, with a 2" meter restriction?

As Sac Bee said on its first new style Front Page:
 "Folsom what are you thinking?"

Folsom has NO Liability Insurance, and no Oversight agency since Northern CA self-insured cities bumped city out.

What did this Igor applicant offer to the city for this PRE-Approval?

How can the private citizens on the "judgment panel" think they have no personal exposure to potential Liability issues?

City has no Liability Insurance to cover them. Why we wish to know, did the Staff in Development Pre-Approve this project?

Add to this another neighbor who is PhD in environmental issues, and clearly opposed. Add to this the city actual/current members of this alleged 2nd Plan Commission or false Zoning Appeals Board are NOT posted at city website as this is written.

We know nothing about who is doing What [in Truth/reality] and what actual Imminent Physical Dangers they pose for entire city, Federal American River and federal Forests and Natl Historic Site.

How many humans can this city's pre-approved applicants kill with impunity? How much of Federal Assets can they destroy, pollute, and harm with impunity?

I thank our neighbors for standing up against Secrecy, "scoff law" Folsom Officials, employees, and "consultants". FYI, city had a consultant file at CEQA SCH, an NOC Notice of Completion. Folsom CA never filed and Circulated properly a Notice of Intention to file Negative environmental impact Declaration.

We know what the world thinks about these behaviors, but why are those who profit so arrogant and insisting they are Above the Law.

Concerned abutting neighbor.

Our neighbor who supplied these Comments and Photos Knows the Issues & dangers. I thank him and his extended family. Our Firefighters should be thankful and proud of this wonderful new neighbor, Dave.

He's right: these pictures tell the entire filthy, dangerous, story.

This picture was taken January 13th, 2022. This is what the maintenance ground is currently. The new storage shed is in the background and the existing shed metal security fencing surrounds the grounds and has a locked security gate. Tailing Pile condition. It has been disturbed over the years.



This picture was taken January 13th, 2022. This picture illustrates the current maintenance grounds, sheds and security fencing. The metal shed on the right proposed crematory is to be installed.



This picture shows the propane tank pad right of the building under the large photograph was taken January 13th, 2022. The application site plan confirms propane pad. In addition, the site plan calls for 2 x 500 gallon tanks, not the tanks in the Negative Declaration text.



This photograph taken January 13th 2021 displays another angle of the local tanks pad. Blocked by a wooden fence, metal security fencing, a large red v metal shed.



This picture was taken on August 4th, 2021. This is Lakeside Memorial Lav Valdimir Semenyuk, driving out of the cemetery with a 250 gallon propane ta disturbing on many levels. It is my opinion that the Caring Service is very cc outcome of the Conditional Use Permit. If indeed this tank is for the Cremati arrogant for the Caring Service Group to show no discretion in their activities



February 4, 2022

Historic District Commissioners
City Council Members
City Manager
Kelly Mullett

My name is Dave Higgins, I live across the street from Lakeside Memorial L proposed crematorium is to be built. Over the course of summer to the pres taken pictures of activity at the cemetery. I wish to share.

This picture was submitted by the Caring Service Group and Miller Funeral I application for the Conditional Use Permit to install a crematory. Two years the metal storage shed and surrounding grounds looked like in 2-27-2020.



Josh Kinkade

From: Kelly Mullett
Sent: Tuesday, February 15, 2022 9:35 AM
To: Josh Kinkade
Subject: FW: CREMATORIUM

From: Richard Perez [REDACTED]
Sent: Tuesday, February 15, 2022 9:29 AM
To: Kelly Mullett <kmullett@folsom.ca.us>
Subject: CREMATORIUM

You don't often get email from [REDACTED]. [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Commissioner,

Please **Vote NO** on a Crematorium in the Historic Folsom neighborhood. This is a historic area, around residents, beautiful trails, and a great touristic attraction due to Folsom's rich Nature. A crematorium should not be part of such beautiful scenery.

Sincerely,

Richard and Sandra Perez

[REDACTED]
Folsom, CA 95630

Josh Kinkade

From: Bob LaPerriere <[REDACTED]>
Sent: Monday, February 14, 2022 3:23 PM
To: Josh Kinkade
Subject: LAKESIDE
Attachments: CREMATORIUM PN 02-258 Staff Reports-Minutes.pdf; FOLSOM LAKESIDE.docx; Historic Cemetery Designation.docx; CREMATORIUM Excerpts.pdf

You don't often get email from [REDACTED]. [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Here is what I can submit at this time...if more current info is needed would need to schedule a Commission meeting which could take about a week.

Excerpts are from the Staff Report, page one attached below, about 2003

Bob LaPerriere

Bob LaPerriere
Chair, Sacramento County Cemetery Advisory Commission

[REDACTED]
POB 255345, Sacramento CA 95865-5345

URL: <http://www.coroner.saccounty.net/sccac/Pages/default.aspx>



To: Folsom Historic District Commissioners
City of Folsom Planning Dept

Date: February 15, 2022

Re: Lakeside Cemetery

From: Dr. Bob LaPerriere
Chair, Sacramento County Cemetery Advisory Commission

“Lakeside Memorial Lawn” has not existed since 1850, but the multiple historic cemeteries that became merged as Lakeside did. Unfortunately, much of the history related to those cemeteries and burials has been lost over the past 1 ½ centuries.

I am writing regarding the plans to build a crematorium at Lakeside Cemetery. Due to COVID our Commission has not been meeting regularly, but I am attaching comments from 2003 that have represented the feeling of the Commission that such construction would be inappropriate so closely related to multiple cemeteries of historic importance, including one of our rare remaining Chinese Cemeteries. Also attached is documentation of our designation for Lakeside as a Historic Cemetery. I am not aware of the signage with that designation, which we provided, ever being erected on the site.

Please consider our concerns about the inappropriate location for a crematory.

Thank you

Bob LaPerriere
Chair, Sacramento County Cemetery Advisory Commission



URL: <http://www.coroner.saccounty.net/sccac/Pages/default.aspx>

Historical Designation On the recommendation of the Commission, the Board of Supervisors has designated 21 cemeteries as “historic”. This is phase I of the project, as there are many other historic cemeteries in Sacramento County. Cemeteries designated were:

- | | |
|--|-------------------|
| 1.Bellview Cemetery | Sacramento County |
| 2.Union Cemetery | Sacramento County |
| 3.Lakeside Cemetery | Folsom |
| 4.Matthew Kilgore Cemetery | Rancho Cordova |
| 5.24th & Meadowview Cemetery | Sacramento City |
| 6.Chung Wah Cemetery | Folsom |
| 7.Elder Creek Cemetery | Sacramento |
| 8.Rancho Murieta Cemetery (North & South) (2) | Rancho Murieta |
| 9.Sacramento Historic City Cemetery | Sacramento City |
| 10.Michigan-Bar Cemetery (Ione Road) | Sacramento County |
| 11.Sloughouse Cemetery | Sloughouse |
| 1.Sylvan District Cemetery | Citrus Heights |
| 12.Galt /Arno District Cemeteries (2) | Galt |
| 13.Elk Grove Consumnes District Cemeteries (5) | Elk Grove |
| 14.Fair Oaks District Cemetery | Fair Oaks |

Plaques were provided for each cemetery, and we still have several to distribute. The text of the plaque is as below:

***THIS SITE HAS BEEN DESIGNATED BY
SACRAMENTO COUNTY AS A
HISTORIC CEMETERY.***

HERE REST MANY OF THE MEN, WOMEN AND CHILDREN WHO SAW THE BEAUTY
AND VALUE OF THIS LAND, CHOOSING TO SETTLE HERE
AND BUILD THE COUNTY WE CHERISH TODAY.

PLEASE HELP US PRESERVE THESE GRAVES, MARKERS,
AND LANDSCAPING FOR FUTURE GENERATIONS.

SACRAMENTO COUNTY BOARD OF SUPERVISORS
AND CEMETERY ADVISORY COMMISSION

Agenda Item No. 1
 PN02-258
 HDC Mtg. 1-15-03

Withdrawn

HISTORIC DISTRICT COMMISSION STAFF REPORT

PROJECT TITLE: Lakeside Cemetery Crematorium

PROPOSAL: Request for approval of a Conditional Use Permit to allow for the operation of crematorium

RECOMMENDATION: Denial

APPLICANT AND OWNER: Lorin Claney

LOCATION: 1201 Forrest Street

ASSESSOR'S PARCEL NO.: 070-0082-014, 070-0130-002, 070-0130-004

ATTACHED REFERENCE MATERIAL:

1. Vicinity Map
2. Site Plan, dated 12/19/97
3. Project Description and Crematorium Illustrations
4. Documentation from the Sacramento County Historic Cemetery Commission
5. Lakeside Cemetery Research Paper written by Sue Silver
6. Letters from the Public
7. Site Photographs

PROJECT PLANNER: Jane Talbot, Assistant Planner

BACKGROUND

The Planning Commission approved a Use Permit and Variance for Mausoleums at Lakeside Memorial Lawn in 1991 (PC91-042). An amendment to the approval was granted in 1995 (PC95-033). That approval allowed for the construction of twelve mausoleums. To date, one mausoleum has been built and one additional mausoleum is under construction. An existing maintenance building, approximately 975 square feet in area, is located along the south border of the cemetery. The front of the project site is mostly level with a slight to moderate downward slope towards the rear of the site. Lakeside Cemetery has a variety of mature deciduous and evergreen trees. The front of the cemetery, along Forrest Street, is bounded by a brick wall capped with wrought iron fencing.

Attachment 4

**Documentation from the
Sacramento County Historic Cemetery Commission**

Sacramento County Cemetery Advisory Commission
4800 Broadway, Suite 100
Sacramento CA 95820

City of Folsom Historic District Commission
50 Natoma Street
Folsom, CA 95630

Attn: Jane Talbot

Dear Ms Talbot::

The Sacramento County Cemetery Advisory Commission is in receipt of your Notice of Public Hearing regarding PN 02-258 Conditional Use Permit and Mitigated Negative Declaration 1201 Forrest Street.

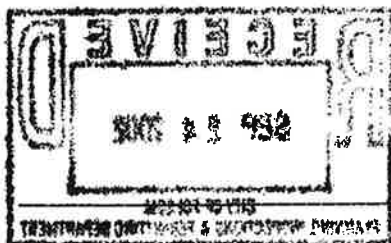
The commission is charged with the duty to encourage the preservation and designation of historical cemeteries. We are currently preparing a list of those cemeteries in Sacramento County, which should be considered historic. It is my opinion that Lakeview Cemetery will be one of the cemeteries in Sacramento County that will be on the list that is to be presented to the Board of Supervisors for designation as an historic cemetery.

While no official action has yet been taken by either The Cemetery Advisory Commission or The Board of Supervisors regarding Lakeview Cemetery we ask that you consider the above mentioned conditional use permit in the context of Lakeview's historic significance and endeavor to preserve its historic elements.

Sincerely,



James A. Purcell, Chairman
Cemetery Advisory Commission



Chair, James A. Purcell

Vice Chair, Dr. Robert La Perriere

Sacramento County Cemetery Advisory Commission
4800 Broadway, Suite 100
Sacramento CA 95820

January 2, 2003

Ms. Jane Talbot
City of Folsom Planning, Inspections and Permitting Department
50 Natoma Street
Folsom, CA 95630

Dear Ms Talbot:

The Sacramento County Board of Supervisors on June 12, 2001 approved Ordinance No. SCC-1193, which established The Sacramento County Cemetery Advisory Commission (The Commission).

The purpose of the advisory commission is:

1. To advise the Sacramento County Board of Supervisors on citizen's concerns and issues related to cemeteries;
2. To provide recommendations to preserve, protect, and maintain cemeteries;
3. To make recommendations to the Board of Supervisors regarding mechanisms for funding the preservation, protection and maintenance of cemeteries and the appropriation of funds so raised; and
4. To encourage the preservation and designation of historical cemeteries.

With regard to number 4, above, and in reference to your request for comments on the proposal for a crematorium at Lakeside Cemetery, The Commission would like to submit the following:

The area incorporating Lakeside Cemetery, the Chung Wah Historic Cemetery, and the California State Dredging Tailings Park is possibly the only site in the State of California that combines these important aspects of our heritage in one small area. Lakeside Cemetery, in fact, may be a grouping of 19th century cemeteries, which would most likely fit the criteria currently being developed to define a historic cemetery. It is the opinion of The Commission that the addition of a crematorium on that site would have a negative impact on the historical significance of the area. It is believed that a crematorium could be placed in an industrial area within Folsom to avoid the impact on the history of our countywide community.

Chair, James A. Purcell

Vice Chair, Dr. Robert La Perriere

Ms. Jane Talbot
January 2, 2003
Page 2

Area residents have notified our Commission that they oppose the placement of the crematorium on the site.

Research that has been provided to our Commission indicates that there have been instances in which monuments, and copings have been moved or destroyed, plot maps of burial locations are misplaced and that legal questions exist regarding the ownership of portions of Lakeside Cemetery. Further degradation of the historic value of the cemetery by the addition of a crematorium would only serve to lessen its cultural importance to the City of Folsom and Sacramento County.

Please contact us if we can provide any other information or support.

Sincerely,



James A. Purcell, Chairman
Sacramento County Cemetery Advisory Commission

cc Historic District Commission
50 Natoma Street
Folsom CA 95630

From: [Bob LaPerriere](#)
To: [Josh Kinkade](#)
Subject: Lakeside
Date: Tuesday, February 15, 2022 11:01:10 PM
Attachments: [Cemetery by SK2.docx.docx](#)

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Here is an additional (individual) letter from one of our Commissioners. Thanks.

Bob LaPerriere

Bob LaPerriere
Chair, Sacramento County Cemetery Advisory Commission

[REDACTED]
POB 255345, Sacramento CA 95865-5345

URL: <http://www.coroner.saccounty.net/sccac/Pages/default.aspx>

February 15, 2002

Folsom Historical Commission

To Whom it May Concern:

Back in 2003 there was a plan to originally build a Crematorium at Lakeside Cemetery. Lakeside Cemetery is not 100 years old but was formed from smaller cemeteries dating back to the Gold Rush. As such, the current cemetery borders another cemetery occupied by the Chinese and is currently administered by the Bureau of Land Management and the Chan Trust on a previously arranged agreement. The Chinese cemetery is a registered National Landmark and another close by has potential for a National Landmark nomination. The proposal could push nomination into oblivion.

With this going on, there has been no consultation with the Bureau of Land Management and the Chan Trust on how it will affect the National Landmark nomination. Lakeside Cemetery is an amalgamation of several cemeteries, their records are rather vague, and there are issues of this proposal being built on other existing burial sites that are difficult to pinpoint. There could be a destruction of different cultural groups that are historic in nature without the Chinese community or others being able to give their input. This proposal would adversely affect the historic features and burials of the Chinese, Euro American and other interested parties that may be buried there. This could potentially be a violation of the National Historic Preservation Act, as well as the California Graves Protection Act, in which six or more burial sites is considered an official cemetery.

This late notification of this meeting has not allowed the Sacramento County Cemetery Advisory Board due diligence to respond in a timely manner to the meeting taking place on February 16, 2022. In the past we were against this same proposal taking place at this cemetery. I consider this is a historic cemetery and the records for this location are poor in nature and the possibility of disturbing the graves of the dead is highly irregular. In the past graves have been destroyed during construction phases.

Yours,

StephAnie Kadle
District 2
Sacramento County Cemetery
Advisory Commission

Attachment 4

HELIX Topical Responses to IS/MND Comments

Memorandum

HELIX Environmental Planning, Inc.
11 Natoma Street, Suite 155
Folsom, CA 95630
916.365.8700
www.helixepi.com



Date: February 15, 2022

Project: Lakeside Memorial Lawn Crematorium

RE: Topical Responses to Comments (public review draft Initial Study/Mitigated Negative Declaration)

Aesthetics

Neither the project site nor the surrounding areas are scenic vistas due to the presence of existing nearby commercial and residential developments. Further, neither the project site, nor views to or from the project site, have been designated as important scenic resources by the City of Folsom or any other public agency. Additionally, the site of proposed modifications is already largely shielded from public view due to the presence of dredge tailing piles and would remain so. Therefore, the proposed development would not interfere with or degrade a scenic vista, and no impact would occur.

The crematory would be placed inside a metal structure that already exists on the property and is already mostly shielded from public view. The only external modifications would be the addition of two 250-gallon propane tanks on a concrete pad near the edge of the building and the addition of a small exhaust stack to the roof of the shed. This stack would be approximately 19.5 feet above grade and would project approximately 10 feet above the existing roof of the shed. This physical improvement to an already existing metal structure would not significantly impact the visual character of the project setting. An existing wooden fence would shield the propane tanks from view from the publicly used areas of the cemetery.

Air Quality

Criteria pollutant and precursor emissions for long-term operation of the proposed crematory were calculated using propane combustion emissions factors from the USEPA AP-42 Compilation of Emissions Factors Chapter 1.5, and crematory emissions factors provided by the Sacramento Metropolitan Air Quality Management District (SMAQMD), which combined USEPA AP-42 data and the USEPA Factor Information Retrieval Program.

Potential health risks to nearby sensitive receptors from the emission of toxic air contaminants (TACs) during operation of the proposed crematory were analyzed after consultation with the SMAQMD and in accordance with the Office of Environmental Health Hazard Assessment (OEHHA) *Air Toxics Hot Spots Program Guidance Manual for Preparation of Health Risk Assessments*.

Localized concentrations of TACs were modeled using Lakes AERMOD View version 9.8.3 and the California Air Resources Board's (CARB's) Hotspots Analysis and Reporting Program (HARP), Air Dispersion Modeling and Risk Tool (ADMRT) version 19121. SMAQMD provides two sets of

Lakeside Memorial Lawn Crematorium
February 15, 2022

Page 2 of 4

meteorological data files for use with AERMOD: one for the Sacramento International Airport and one for the Sacramento Executive Airport. Data for the Executive Airport was recommended for use by SMAQMD staff (provided by Venk Reddy on 8/28/2019).

Though it is uncertain whether the exhaust stack will include a rain cover, for the purposes of dispersion modeling, assuming a rain cover is installed on the crematorium exhaust stack is the more conservative approach. The rain cover would limit the initial dispersion of the exhaust gases, thereby resulting in increased concentrations near the source. Without a rain cover, the exhaust may travel farther, but would result in decreased concentrations in any given volume of air. These decreased concentrations would result in decreased exposure and health risks.

HELIX's coordination with Sacramento Metropolitan Air Quality Management District (SMAQMD) began in August 2019 when HELIX's Senior Air Quality Specialist, Victor Ortiz, reached out to SMAQMD Air Quality Engineer, Venk Reddy, via phone. Ongoing coordination via phone and email with SMAQMD staff, including Venk Reddy, Karen Huss, Steve Mosunic, and Brian Krebs, continued through the end of October 2021. Initial coordination included discussion of SMAQMD approved methodologies, models, and emission factors for use in quantifying emissions and risks associated with crematory operations. Mr. Reddy provided Mr. Ortiz with the SMAQMD approved toxic air contaminant (TAC) emission rates and the recommended meteorological data for use in the AERMOD dispersion model. In the late spring of 2021, Mr. Reddy and his team conducted a review of HELIX'S AERMOD and HARP modeling files used in the health risk assessment (HRA). All comments made by Mr. Reddy following his review dealt with confirming manufacturer specs included in the modeling. Items specifically called out, including exhaust flow rate, physical dimensions of the equipment, and hourly burn rate, were provided by Hartwick Combustion Technologies, Inc.

The HRA examined risks to the human population as required by CEQA. Both inhalation of gaseous TACs and oral consumption of deposited TACs were examined. The exposure duration was set to 30 years beginning with infants in utero in the third trimester of pregnancy, in accordance with OEHHA guidelines. All risks were found to be below the CEQA significance thresholds.

Criteria pollutant emissions are compared to the SMAQMD thresholds of significance, which are established with the goal of helping the SMAQMD attain the ambient air quality standards. These standards are designed to protect people most sensitive to respiratory distress, such as asthmatics, the elderly, very young children, people already weakened by other disease or illness, and persons engaged in strenuous work or exercise. By resulting in emissions less than the thresholds developed to attain the standards aimed at protecting the most sensitive populations, the project's criteria pollutant emissions are not expected to result in adverse health effects on said populations.

Cultural and Tribal Cultural Resources

The presence of the nearby historic Chinese cemeteries was acknowledged in Section 5.1 of the required confidential cultural resources technical study prepared for the project, which was used to support the IS/MND. However, these cemeteries are not located within the project area. At their closest points, the Chung Wah Cemetery is located approximately 263 feet southwest of the project area, and the Young Wo Cemetery is located approximately 847 feet north of the project area. The local historical significance of the Lakeside Cemetery itself and its origins and historic use extending back to 1846 were addressed in Section 3.5.4 of the confidential cultural resources study prepared for the project.

Lakeside Memorial Lawn Crematorium
February 15, 2022

Page 3 of 4

The use of a crematorium would be exclusively within the boundaries of an existing facility in the modern portion of the cemetery. The cemetery complex in the immediate area already reflects several different religious or cultural funerary practices, including Chinese, Jewish, Masonic, Odd Fellows, and others. Introducing a crematorium is not the first time a new funerary practice was introduced to the cemetery area.

The project would not create an adverse effect on significant historical cemeteries and resources. The project area does not contain any historical graves or interments and the confidential cultural resources survey of the project area failed to identify any historic or cultural resources within its footprint. No changes to the existing adjacent or nearby cemeteries or mine tailings will occur as a result of the project. The crematorium equipment will be housed within an existing shed and there will be no visible or physical change to the surrounding area. None of the features of the historic or modern portions of the cemetery will be affected.

The visibility of the proposed stack does not have a direct effect on the historical significance of the historic cemeteries, especially as there is no visible exhaust and no deposition of cremated remains. The qualities that make the cemeteries significant are the aspects of integrity of setting, feeling, and association (according to the National Registration Form for the Chung Wah cemetery). The footprint of the proposed project is minor and would not result in an impact on the project site's integrity, setting, and feeling.

Hazards and Emergency Evacuation

The City of Folsom Fire Department provides fire protection services. There are four fire stations providing fire/rescue and emergency medical services within the City of Folsom with a fifth station planned near the eastern city limits. Station 35 is the nearest station to the project site and is located at 535 Glenn Drive, approximately 1.5 miles east of the project site. Station 36 is second nearest to the project site and is located at 9700 Oak Avenue, approximately 2.3 miles north of the project site. The project site is easily accessible to fire service personnel. Consistent with the City's Multi-Hazard Emergency Management Plan, the City of Folsom maintains pre-designated emergency evacuation routes along major streets and thoroughfares.

The project is not located in or near a State Responsibility Area or in a Very High Fire Hazard Severity Zone. Vegetation on the property is irrigated and includes maintained lawns and well-spaced trees with a generally open canopy and limbs pruned near ground level. Furthermore, the project is subject to standard structural separation requirements from the Fire Department with regards to the crematorium's distance to the propane tanks and potentially flammable material.

Land Use and Planning

Cemeteries are a permitted use within the OS/P Primary Area upon approval of a Conditional Use Permit (CUP) per Section 17.52.550 of the FMC. The subject cemetery has been in operation since the 1800's and pre-dates the requirement for a CUP. The cemetery did receive a CUP for operation of a mausoleum in 1995. The proposed crematorium would be operating as an accessory use to the existing cemetery, not as a stand-alone business.

In this case, the cemetery is the primary or principal use and the applicant is proposing a crematorium as an accessory use to the existing cemetery. As proposed, the crematorium would be subordinate in area, extent, and purpose to that of the existing cemetery. It would provide a service related to and

Lakeside Memorial Lawn Crematorium
February 15, 2022

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supportive of the service already provided by the cemetery and mausoleum. It would be located on the same lot and in the same zoning district as the principal use. It would be owned and operated by the same people who own and operate the existing cemetery and mausoleum. As such, a crematory can be considered as an accessory use subject to a CUP.

Attachment 5

Draft Minutes from February 16, 2022 Historic District Commission Meeting



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

DRAFT
HISTORIC DISTRICT COMMISSION MINUTES
February 16, 2022
ZOOM VIRTUAL MEETING
5:00 p.m.
50 Natoma Street
Folsom, California 95630

CALL TO ORDER HISTORIC DISTRICT COMMISSION: Mark Dascallos, Daniel West, John Lane, Mickey Ankhelyi, Justin Raithel, John Felts, Kathy Cole

ABSENT: None

PLEDGE OF ALLEGIANCE

CITIZEN COMMUNICATION: Loretta Hettinger addressed the Historic District Commission regarding the Zoning Code Update Home Occupations item going forward to the City Council on March 8, 2022.

MINUTES: The amended minutes of the January 19, 2022 meeting were approved.

Oath of Office Administered to John Lane

NEW BUSINESS

1. PN 19-182, Lakeside Memorial Lawn Crematorium Conditional Use Permit, Mitigated Negative Declaration, and Mitigation Monitoring and Reporting Program

A Public Hearing to consider a request from Igor Semenyuk for approval of a Conditional Use Permit to allow for a crematory to operate in an existing metal structure situated within the Lakeside Memorial Lawn cemetery located at 1201 Forrest Street. The zoning classification for the site is OS/P, while the General Plan land-use designation is OS. An Initial Study and Mitigated Negative Declaration have been prepared in accordance with the requirements of the California Environmental Quality Act. **(Project Planner: Josh Kinkade / Applicant: Igor Semenyuk)**

1. Beth Kelly addressed the Historic District Commission in opposition to the proposed project.
2. Steve addressed the Historic District Commission in opposition to the proposed project.
3. Loretta Hettinger addressed the Historic District Commission in opposition to the proposed project.
4. Janice B. addressed the Historic District Commission in opposition to the proposed project.
5. Deborah Grassl addressed the Historic District Commission in opposition to the proposed project.
6. Nicole Gates addressed the Historic District Commission in opposition to the proposed project.
7. Mariko McGarry addressed the Historic District Commission in opposition to the proposed project.
8. Sharon Kindel addressed the Historic District Commission in opposition to the proposed project.

9. Daniel & Ashley Martinez addressed the Historic District Commission in opposition to the proposed project.
10. Tracy Wetzel addressed the Historic District Commission in opposition to the proposed project.
11. Helen Walsh addressed the Historic District Commission in opposition to the proposed project.
12. Stephanie Kadle addressed the Historic District Commission in opposition to the proposed project.
13. Marie Gonzales addressed the Historic District Commission in opposition to the proposed project.
14. Sean Gates addressed the Historic District Commission in opposition to the proposed project.
15. Marie Sims Rice addressed the Historic District Commission in opposition to the proposed project.
16. Mary addressed the Historic District Commission in opposition to the proposed project.
17. Greg addressed the Historic District Commission in opposition to the proposed project.
18. Victoria Foster addressed the Historic District Commission in opposition to the proposed project.
19. Dennis Kasbian addressed the Historic District Commission in opposition to the proposed project.
20. Joy addressed the Historic District Commission in opposition to the proposed project.
21. Laura Fisher addressed the Historic District Commission in opposition to the proposed project.
22. Kim Higgins addressed the Historic District Commission in opposition to the proposed project.
23. Kyle & Breanne Higgins addressed the Historic District Commission in opposition to the proposed project.
24. Peter Lucyga addressed the Historic District Commission in opposition to the proposed project.
25. Tim McGarry addressed the Historic District Commission in opposition to the proposed project.
26. Isaac addressed the Historic District Commission in opposition to the proposed project.
27. Owen addressed the Historic District Commission in opposition to the proposed project.
28. Erika Hamer addressed the Historic District Commission in opposition to the proposed project.
29. Jennifer Lane addressed the Historic District Commission in opposition to the proposed project.
30. June Chan addressed the Historic District Commission in opposition to the proposed project.
31. Charles Noble addressed the Historic District Commission in opposition to the proposed project.
32. Daron Bracht addressed the Historic District Commission commending Commissioners and City Staff.
33. Steve Walsh addressed the Historic District Commission in opposition to the proposed project.
34. Brian Pacciotti addressed the Historic District Commission in opposition to the proposed project.
35. Jill Hamer addressed the Historic District Commission in opposition to the proposed project.
36. Pat Binley addressed the Historic District Commission in opposition to the proposed project.

COMMISSIONER RAITHEL MOVED TO ADOPT THE MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING AND REPORTING PROGRAM FOR THE LAKESIDE MEMORIAL LAWN CREMATORIUM, PER ATTACHMENT 11; AND MOVED TO APPROVE THE CONDITIONAL USE PERMIT (PN 19-182) FOR OPERATION OF A CREMATORIUM WITHIN AN EXISTING 1,071-SQUARE FOOT METAL STRUCTURE LOCATED AT 1201 FORREST STREET, WITHIN THE LAKESIDE MEMORIAL LAWN CEMETARY AS ILLUSTRATED IN ATTACHMENTS 5 AND 6, WITH THE FINDINGS (A-K) AND CONDITIONS (NOS. 1-30)

COMMISSIONER ANKHELYI SECONDED THE MOTION.

COMMISSIONER LANE RECOMMENDED A FRIENDLY AMENDMENT TO INCLUDE THE FOLLOWING CONDITION UNDER "MISCELLANEOUS CONDITIONS":

"31. A Davis Instruments Vantage Vue, Vantage Pro2 or similar weather station shall be installed on the shed on which the crematory machine is proposed prior to installation of the crematorium to the satisfaction of the Community Development Department."

COMMISSIONER WEST RECOMMENDED A FRIENDLY AMENDMENT TO INCLUDE THE FOLLOWING CONDITION UNDER "MISCELLANEOUS CONDITIONS":

"32. The proposed stack shall be subject to Design Review approval subsequent to obtaining a permit from the Sacramento Metropolitan Air Quality Management District (SMAQMD)."

COMMISSIONER RAITHEL AND COMMISSIONER ANKHELYI ACCEPTED THE FRIENDLY AMENDMENTS TO THE MOTION, WHICH LEAD TO THE FOLLOWING VOTE:

AYES: RAITHEL
NOES: DASCALLOS, WEST, LANE, ANKHELYI, FELTS, COLE
RECUSED: NONE
ABSENT: NONE

MOTION FAILED.

COMMISSIONER WEST MOVED TO DENY THE PROJECT WITH THE FOLLOWING FINDING:

"The use applied for is detrimental to the health, safety, or general welfare of persons residing or working in the neighborhood, detrimental or injurious to property and improvements in the neighborhood and the general welfare of the City because introduction of this use will impact the historical character of the existing cemetery and historical use of the area."

COMMISSIONER LANE SECONDED THE MOTION WHICH CARRIED THE FOLLOWING VOTE:

AYES: DASCALLOS, WEST, LANE, ANKHELYI, FELTS, COLE
NOES: RAITHEL
RECUSED: NONE
ABSENT: NONE

MOTION PASSED.

PRINCIPAL PLANNER REPORT

The next Historic District Commission meeting is tentatively scheduled for March 16, 2022.

RESPECTFULLY SUBMITTED,

Kelly Mullett, ADMINISTRATIVE ASSISTANT

APPROVED:

Kathy Cole, CHAIR

Attachment 6

Public Comment Letters Regarding the Applicant's Appeal

Josh Kinkade

From: Elaine Andersen
Sent: Wednesday, March 23, 2022 11:20 AM
To: Josh Kinkade
Subject: FW: Lakeside Memorial Lawn Crematorium - City Council Meeting 4/12/22

-----Original Message-----

From: Adam and Katie Musfelt <[REDACTED]@yahoo.com>
Sent: Wednesday, March 23, 2022 11:19 AM
To: Elaine Andersen <eandersen@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>
Subject: Lakeside Memorial Lawn Crematorium - City Council Meeting 4/12/22

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City Council Members,

We, Adam and Katie Musfelt, are unable to attend the Lakeside Memorial Lawn Crematorium meeting on April 12, 2022. We would like to go on the record and say that we strongly oppose the building of this crematorium.

Our quality of life will have a significant negative impact if this crematorium is allowed to be built; families and businesses will suffer repercussions for years to come. Please consider the future of this neighborhood by preserving the historical and cultural integrity of it and voting no on the crematorium.

Thank you,

Adam and Katie Musfelt

Josh Kinkade

From: Elaine Andersen
Sent: Friday, April 1, 2022 3:31 PM
To: Josh Kinkade
Subject: FW: Proposed Crematorium at Lakeside Cemetery

From: Bert p [REDACTED]
Sent: Friday, April 1, 2022 3:15 PM
To: Elaine Andersen <eandersen@folsom.ca.us>
Subject: Proposed Crematorium at Lakeside Cemetery

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Dear Elaine Anderson,

I, together with my neighbors at Lake Natoma Shores, a community next to the Lakeside Memorial Lawn Cemetery am asking you to halt the proposed construction of a crematorium there. The crematorium will be located adjacent to a residential neighborhood, the Historic District, and the American River Parkway. This area resides near many outdoor activities promoted by our city as family-friendly, safe, and “distinctive by nature”. It entails a farmers market, outdoor concerts; city-sponsored festivals, and sits adjacent to the American River Parkway that is actively used by many walkers, runners, and bicyclists daily. The Parkway serves as a window to the natural world, for all those that live and visit Folsom.

In researching the toxicity impacts of a crematorium I went to the National Collaborating Center for Environmental Health and found the following information, as per an article written in the [US National Library of Medicine](#):

“Cremation is a combustion process whereby a casket and human remains are incinerated at a high temperature in a closed chamber. The process of corpse cremation generates numerous harmful air pollutants, including particulate matter, sulfur dioxide, nitrogen oxides, volatile organic compounds, and heavy metals. These pollutants are carcinogenic and could have **severe effects** on human health and the surrounding environment.” (*)

It is of great concern to me that scientific papers have been written proving that the emissions from crematorium incineration are toxic. And that the level of toxicity is a danger to people’s health and well-being, has destroyed surrounding watersheds & water quality, and is of great harm to the natural environment. So improvements in the burner’s design do not mitigate the fact that it does emit pollutants, and if installed, will negatively impact this area for generations.

The proposed crematorium owners may see a viable commercial need for such an operation, but I portend, it is not in the Historic District, adjacent to a residential neighborhood and a State Parkway. It is best suited for a heavy industrial park where the zoning and utility systems can better accommodate any toxic air emissions, toxic water runoff, and any potential fire danger that may transpire by such an operation.

Please consider our concerns and let that serve as your guide forward.

Sincerely,

Bert Pittari,
Resident Lake Natoma Shores

Patricia Zuccaro
Resident Lake Natoma Shores

(*)"Toxic atmospheric pollutants from crematoria ovens: characterization, emission factors, and modeling"

>*Environ Sci Pollut Res Int. 2020 Dec*

Josh Kinkade

From: Elaine Andersen
Sent: Friday, April 1, 2022 2:32 PM
To: Josh Kinkade
Subject: FW: Crematorium proposal

From: breanne higgins [REDACTED]
Sent: Friday, April 1, 2022 2:31 PM
To: Elaine Andersen <eandersen@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; rrodriquez@folsom.ca.us; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>
Subject: Crematorium proposal

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Hello,

My name is Breanne Higgins and I am opposing the proposal for The Lakeside Memorial Lawn Crematorium. I am a resident of the Preserves Neighborhood and have many family members and friends within this small community. So this is very important to me. I am opposing it for many reasons: quality of life, cultural insensitivities, and not belonging in a historical district.

This has already affected my quality of life and consumed my time. It has been difficult, as many of us have careers, young children and households to maintain. I have a 21 month old and am 36 weeks pregnant. I should be looking up baby names, but instead I find myself researching crematoriums and city council meeting dates. Our neighborhood is a tight knit community with many young children. Should our kids have to see white puffs of smoke and know that a body is burning in a storage shed. At the age of 7 should you really know what a crematorium is? Is that age appropriate?

There is a big difference between living near a cemetery and living next to a crematorium. Being next to a cemetery is peaceful, somewhere you show respect, it's quiet and green. It's wonderful to see family's come to visit buried loved ones. A crematorium is loud with the noise of propane tanks being transported in and out. The sight of smoke indication a body being burned. Although the propane tank is "not anticipated" to be seen, there is no guarantee. As a neighbor I don't want to hear this, see this or smell this. Do you think someone visiting a deceased loved one at the cemetery does? Can YOU gaurtenee me that there won't be any smell? Can Igor gaurtenee this, would he put it in writing?

My husband, Kyal Vongunten spoke during the Historical Commissioners meeting suggesting the environmentally friendly aquamation instead of cremation. We are offering different alternatives and locations to his crematorium. It just doesn't belong in a neighborhood, especially one within a historical district.

The crematorium hasn't even been out in yet and it is distributing our lives. If this is approved, I can guarantee once I smell the scent of burning bodies, I will call and report it. Every time. In the Historic District meeting, it was reported that residents living next to crematoriums did call to report "smells." Nothing is guaranteed about not having a smell associated with the burning of a body. Another resident of the Preserves neighborhood, Sean Gates, mentioned during the Historic District meeting that we can smell the Kikkoman's food plant on certain days. This is true. The Kikkoman's Food, Inc. building is less than 1 mile away from my house. One can only assume, with the right wind, that the smell of burning bodies from the proposed crematorium would be present at not only my house, but on Sutter Street, which is

within a mile radius of the proposed crematory. Do we really want our visitors who are enjoying the business on Sutter Street saying "What is that smell?"

The Chung Wah Cemetery, is listed on the National Register of Historic Places and is in the center of our neighborhood. The Chung Wah cemetery is less than a .2 miles away from the proposed crematorium. As a historic/sacred site, it should be preserved and protected. Recently, The Murer House was also listed as a National Register of Historic Places. We as a city should respect and protect these unique additions to our neighborhood.

I have thought about the pros of putting this crematorium in our neighborhood. It is really just convenient for Igor. If Folsom needs a crematorium, place it in a properly zoned area. Not in open space within a historical district. Do we really bend for one person? What does the city get out of placing a crematorium in a historic district within a residential area so close to Sutter Street? Sure, there are other crematoriums in residential areas, but are they in a historical district? No, they are not. Why? Because they don't belong there. It's not appropriate.

Please vote NO to the proposed crematorium being placed in the preserves neighborhood. Protect our historic district.

Thank you for your time,

Breanne Higgins

March 22, 2022

To Whom it May Concern,

I live with the Preserve neighborhood, and I recently learned from my neighbors that the Lakeside Memorial Lawn (owned by the Caring Service Group) has applied for a conditional use permit to install a crematory on the grounds.

I live at 1357 Young Wo Circle. The region where the crematory will sit (currently a shed) is within about 400-500 feet from our home. My wife Dawn, and our twelve-year-old son Austin, have lived here for 12 years—we enjoy the lake, our wonderful neighbors, and the Folsom historical areas nearby. The Chinese cemetery in our neighborhood is an awesome tribute to the past Chinese immigrants. Unsurprisingly, we prefer our current situation, and we prefer to not smell or breath the output from the crematorium. Although our personal situation is obviously import to our family (me), the message of this letter concerns the greater public good of conserving history and ecological landscapes (us).

I understand the societal need for crematoriums—we need them. However, it makes more sense as a community to keep industrial operations separate from neighborhoods, historical regions, and precious ecological reserves. There are societies that have chosen a hands-off approach to regulation. In one large US city, I recall seeing a brothel, chemical plant, churches, and residences all in the same neighborhood. Folsom is not like this at all—we have a planned community that includes some of the best parks, bike paths, historical districts, and neighborhoods in the country. Thus, people move to Folsom and will continue to move here in the future due to our desirable planned community.

Yet what will residents think if they internalize the brand of “distinctive by nature”, come to enjoy our historical district, and then learn that the city allowed an unattractive industrial process to occur directly in the heart of the most precious area of the city?

I strongly believe that the Folsom Historical District and others need to work together to ensure that our city follows its brand—we are distinctive by nature. Moreover, we are distinctive by our historical treasures. Let us work together to live our distinctive and precious brand.

Best,

Brian Paciotti

Ph.D. Ecology, UC Davis. M.S. Healthcare Informatics, UC Davis Medical Center

██████ Young Wo Circle

Folsom, CA 95630

February 22, 2022

Folsom City Council Members

Re: Lakeside Crematorium

As you are aware, The Proposed Lakeside Crematorium is being appealed by the applicant. You are also aware that this is a sensitive issue for the Historic District, especially the Preserve/Natoma Shores Neighborhood.

The Preserve Neighborhood is a community bound together by strong relationships and common interests. This proposed crematorium has in the last year and a half put our small community on edge and diminished our quality of life.

If you haven't been in the neighborhood for awhile I suggest you come and visit. Walk around and reacquaint yourself with the Lakeside Cemetery, Chung Wah Cemetery, Young Wo cemetery, Dredger Diggings Preserve, Veterans Hall, Murer House, Lake Natomas, the bald eagles, our small park and of course, the Residents.

I am available anytime to act as your tour guide if you so choose. I am acutely aware of the Brown Act and will not put you in a compromising position. Email, text, phone or just come by and ring the bell.

Thanks for your time

Dave Higgins
[REDACTED] Fong St.
[REDACTED]

Cc: Mari Peshon

March 24, 2022

Extension of Appeal Hearing for Lakeside Memorial Cemetery.

Ms. Anderson,

I was recently made aware of the rescheduling of the appeal hearing for the proposed Lakeside Memorial crematorium. It is my understanding you granted the rescheduling to April 26, 2022. This is in violation of FMC 17.52.710 titled Appeal Hearings.

According to FMC 17.52.710, this appeal hearing needs to be heard on April 12, 2022.

On February 22, 2022 the applicant, Igor Semenyuk submitted a handwritten request for appeal well within the 10 day appeal period. After the 10 day appeal period Igor Sementyuk was allowed to resubmit a more detailed application for appeal. This too is a violation of the appeal process.

The ignoring of Codes and Ordinances and Procedures is disturbing. The special treatment Igor Semenyuk and the Miller Funeral home is receiving is blatant.

Please rescind the rescheduling and reset the Hearing to April 12, 2022.

David Higgins
Folsom resident

Josh Kinkade

From: Elaine Andersen
Sent: Monday, April 4, 2022 7:26 AM
To: Josh Kinkade
Subject: FW: Please vote NO on the Lakeside Crematorium Proposal

From: Isaac Monical [REDACTED]
Sent: Sunday, April 3, 2022 9:47 PM
To: Elaine Andersen <eandersen@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>
Subject: Please vote NO on the Lakeside Crematorium Proposal

Some people who received this message don't often get email from [REDACTED] [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To whom it may concern with the City of Folsom leadership,

The Lakeside Memorial Lawn crematorium proposal is deeply troubling to me and my family. The Caring Service Group is not a small business and states outright on their website that they're in the business of buying up and aggregating small funeral home businesses. This model further distances the business from its community's concerns and it shows with the "Initial Study/Mitigated Negative Declaration" dated April 2021.

The TAC (Toxic Air Contaminants) assessment in Appendix B of the "Lakeside Memorial Lawn Crematorium Initial Study/Mitigated Negative Declaration" dated April 2021 is flawed and irresponsible. Most specifically in terms of the evaluation of the impact to "Sensitive Receptors". The report by Helix Environmental Planning notes the fact that the adjacent residential houses are as close as 450 feet which is not that far! That's about the width of 5 or 6 lots in our neighborhood. 5 houses down the street. The report makes it sound like the combustion stack is remotely located away from our neighborhood which couldn't be farther from the truth. I walk the neighborhood regularly with my 1 and 3 year old children and it's a short walk from the closest house to the proposed site which is where my son likes to occasionally ride his balance bike. The Historic District Historical Society write-up of the cemetery (<https://www.folsomhistoricalsociety.org/post/lakeside-cemetery>) concludes with "The next time you are in town I highly suggest you take a visit to the cemetery; the older burial sites are beautiful, and its quiet location makes a perfect Fall afternoon walk." This will certainly change when there are constant emissions adjacent to the cemetery and increased vehicular activity carting the deceased to and from the crematorium "shed".

The report also neglects to recognize the American River Bike Trail that is adjacent to the proposed site where numerous people of all ages including sensitive groups, i.e. elderly and families with young children religiously use the trail. The proposed industrial process does not fit within the open space plan of the adjacent area which should not be used for a pollution buffer zone, it's a recreation area. While using the trail, the folks that are exercising are going to be subject to toxic emissions while breathing heavily which is an even more sensitive time to be outside which is why the Air Quality Board recommends restricted exertion levels of people depending on levels of air quality, especially sensitive groups.

The assumption that the meteorological data used from the Sacramento Executive Airport station, almost 20 miles away, which regularly gets the delta breeze where the planned site does not, appears to be laughably unethical. The canyon near the proposed site has significantly different geography and often has stagnated air that collects along the river trail which can be confirmed when exercising along the trail in the summer and winter months where the air is perfectly still and the combustion gas from leaf blowers from the adjacent business parking lots simply stagnates right on the trail. In addition to the overuse of leaf blowers, the trail users will now get to look forward to breathing in toxic hydrocarbon and heavy metal combustion products combined with new unpleasant smells. The near-zero initial vertical gas velocity assumption of the stack configuration in the source parameters paragraph will only exacerbate the emissions settling issue on the trail and adjacent neighborhood. The 500 meter radius geography sample used in the analysis is not enough to capture the adjacent yet substantial cliffs in the region and could be interpreted to be an attempt to replicate geography near the airport which would also be unethical in terms of being non-representative.

For a city that wants to define itself as distinctive by nature, this is a far cry from the current mission statement. The benefits to the community are substantially outweighed by the safety risk and miss-use of the planned site.

It's been noted by the Folsom Historic Commission that nearby crematoriums exist and they have been shown to have issues from time to time as shown by the historic commissions own research. Equipment breaks down and we don't want to be subject to issues when they inevitably rise. I ask why would the commission assume this project would be any different?

The first time a request for a Crematorium came up many years ago, the Historic District did not believe the historic site was appropriate for this kind of activity and nothing has changed except for the aggressiveness of the applicant.

This project was chosen to evaluate the CUP based on essentially the general welfare of the neighborhood or city but the charter of the commission is first and foremost to protect the historic and cultural character of the city's Historic District. This is a distinct industrialization of the area which will degrade the character of the district.

It already appears that the applicant has created the pad for the propane tanks as if this has already been approved. The application is based on equipment in a metal shed but it has been communicated by the applicant at the last public comment session with the Historic District Commission that the intent is actually to improve the structure as well for inevitable viewings. It appears that wool is being pulled over the eyes of the approving boards and this is not the end of the development.

The fencing has been improved and gates have been constructed in preparation for installation. There is now a fence that has been improved that further restricts access to the Chung Wah historical cemetery. It would seem logical that the commissions' energy would be better spent enhancing access to Chung Wah and Showing a better connection with our past rather than allowing further restricting of access for an industrial process.

The crematorium will only increase the toxins that already surround us, we don't need more. The world needs less CO2 emissions and with low to zero emissions technology available, this project doesn't make sense for the community. This is further industrialization of a quaint location. This will not be limited cremations in a shed.

The project site is currently within the Open Space/Public Primary Area of the Historic District (OS/P), with an underlying zoning of Open Space and Conservation (OSC). Does adding industrial equipment, of which is an air emitter, to the area continue to comply with the idea of open space and conservation?

I encourage the commissioners of the board to reconsider and follow the precedent of the previous rejection of the same proposal. Do you care if you get cremated right here in Folsom or not? There are several local crematoriums to

choose from and the carbon footprint to get there is in the noise so there is no environmental benefit of having one in the requested location.

The Historic Commission evaluation criteria was as follows: " The establishment, maintenance, or operation of the use or building applied for will or will not, under the circumstances of the particular case, be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood of such proposed use, or be detrimental or injurious to property and improvements in the neighborhood, or to the general welfare of the City"

- The peace and comfort of the neighborhood will be violated with inevitable smell especially when equipment breaks down.
- The industrialization of the site will be injurious to the properties cultural/historical significance going back to the violation of the Chung Wah cemetery in the first place - time to stop that cycle
- The need to disclose the industrial site near a residential neighborhood will undoubtedly hurt property values thus be injurious to property and the neighborhood.

I strongly encourage the Folsom City Leadership to reevaluate the applicant's intentions to monetize a currently quaint property with a toxic industrial process directly adjacent to a family oriented neighborhood and world class nature trail.

Sincerely,

Isaac Monical

██████████ Fong Ct
Folsom, CA 95630
████████████████████

February 23, 2022

Council Members

Toxic Air Contaminants (TAC's)

Attached is an excellent article concerning the toxicity associated with cremations. During the Historic District Commission hearing comments were made concerning the Helix Report and Air Quality District standards for emissions resulting in a less than significant impact. The findings were accepted as they should be. The issue of Toxic Air Contaminants (TAC) in the IS/MND was covered by Josh Kinkade in his presentation. However, I believe the topic was dismissed. The TAC's metals and inorganics listed in the IS/MND;

Metals, (mercury, arsenic,beryllium, cadmium, chromium, copper, hydrogen fluoride, lead,nickel' selenium)

VOC's(benzene, toluene, xylenes, vinyl chlorides).

Aldehydes,

Polyaromatic Hydrocarbons, (PAHS)

Polychlorinated Dibenzodioxins

Polychlorinated Dibenzofurans

These TAC's are not a Air Quality Board concern. These TAC's fall under the EPA. The Sac County Air Board's mission is carbon monoxide, nitrogen dioxide, lead, sulfur dioxide, sulfates, hydrogen sulfide, and visibility reducing particle emissions. The IS/MND defined mercury in a generic form, naturally occurring. The IS/MND does not discuss vaporized mercury. Mercury when vaporized becomes a deadly concern along with the other TAC's listed and should not be released in a residential neighborhood. This health risk is unexceptable.

Please read this document.

Thank You
Kim Higgins

At least please review page 6 of this document

JOHN MUIR GREEN BURIAL SANCTUARY

a beautiful green burial in a spectacular location

WHY CREMATION IS NOT A GREEN CHOICE

10 Green Reasons To Avoid the Toxicity of Cremation

1. **Mercury Vapors.** This is the #1 lethal toxin in crematory vapors. Currently, no crematories have adequate filters for mercury or the many other heavy metals, plastics, and dioxins that are emitted in the vaporization of a body. Most mercury vapors are due to the mercury-silver dental fillings (eight on average in each body) that are liquified and vaporized into the air from each cremation. With over a million bodies cremated each year, that's a lot of toxic mercury in the air that we breathe.
2. **Alzheimer's Disease (#3 cause of death in the U.S.)** and other neurological diseases such as **MS, ALS, Parkinson's Disease, Depression**, etc are caused largely by mercury build-up in the body over 20-40 years. Unlike cyanide poison which has an immediate effect, mercury has a long half-life in the brain. Mercury

contamination is due to mercury dental fillings, coal mining, and... you guessed it, cremation vapors. The United Nations 2019 estimates show that 680,000 pounds of mercury is emitted from dental amalgams into the wastewater and air annually.

Even if all mercury fillings were removed prior to cremation, most human bodies, including children's, have unsafe levels of mercury in the liver and other organs. According to science, no amount of mercury is considered safe in any amount or form. With the rise of cremation in America we have seen an epidemic in neurological illnesses. (Watch the Youtube video "Evidence of Harm" by Dr. Boyd Haley, who originated the chelation medication Emeramide to pull mercury safely out of the body of those suffering from neurological diseases.)

3. **Climate Change:** Cremation is a huge climate change contributor: each cremation uses about 28 gallons of fuel and releases about 540 lbs. of carbon dioxide into the atmosphere. Estimates from the UK say their cremations contribute about 16% of total climate emissions. There are no statistics by the EPA, though 70% of westerners unknowingly choose cremation, mis-informed by the industry to believe it is "green" because of decreased land use. About 1.7 Billion pounds of CO² are emitted every year in the US alone from about a million cremated bodies. Planting thousands of Legacy Trees at the site of a plot helps to reduce our carbon footprint and sequester carbon dioxide from the atmosphere back into the earth.
4. **Mercury contamination of our global environment.** Mercury is known to travel long distances, and biologists are finding mercury in alarming levels in far-reaching places around the planet. Polar bears in the arctic now have peak bioaccumulation of mercury. Mountain lions and deer are drinking mercury-contaminated fog droplets, and showing high levels in their fatty tissues in coastal areas of California, and mammals in the eastern U.S. and Europe also have high levels in their blood and fur.
5. **Industry diseases related to mercury use** - Crematory workers, biologists and other handlers of mercury tainted mammals, dental workers, mortuary and morgue workers are all showing evidence of chronic illness from mercury vapors

that are rising in air pollution, through skin absorption, in office vapors and within the air surrounding the site of mercury contamination. They suffer from higher rates of neurological disease as well as rates of acute and chronic respiratory disease.

6. A positive reason to choose an alternative like Green Burial is **Restoration of the Earth's Soil**. Giving your body back to the earth after a lifetime of food, water, sustenance and enjoyment of nature's bounty helps restore the rich agricultural biome that America has enjoyed over three centuries. Scientists predict that the topsoil across the U.S. will be depleted by 2060 at the rate we continue to use it. Even though bodies contain contaminants, experts maintain that mercury and other heavy metals will trickle down into the soil to form deep mines from where they originated. The safest place for mercury is actually back into the soil to be returned deep into the earth.
7. **Your Wallet** – Green burial has the least cost of all funeral choices at **\$1k to \$3k on average**, compared with cremation costs of \$6,000 and conventional cemetery costs of \$7,000 to \$20,000 or more.
8. **Conservation and Restoration of Nature – A legal and beautiful sense of place** for your body to be held in a natural reserve that your descendants can visit. Location of your plot is through GPS, and Conservation certified cemeteries have a long-term easement for nature to return to its beautiful state. Planting a Legacy tree or other native tree of your choice can help restore native forests to be enjoyed by many future generations. Conservation burial meadows can help restore pollinator meadows, wildflower meadows, and habitat and food for a wide variety of wildlife. Many certified green burials are open to the public on weekends for quiet enjoyment of the natural surroundings and a new appreciation of cemetery use. Since there is no use of pesticides, herbicides like RoundUp, or embalming fluids, the local watershed is free of toxin runoff as found in conventional cemeteries. No concrete or steel vaults are used for the sole purpose of lawn care as in conventional cemeteries. Saving these resources could

assist with rebuilding failing infrastructures across the U.S.

9. **The only non-toxic, truly green choice in deathcare** is green burial. So-called “alternatives” of Alkaline Hydrolysis (“water cremation”), Human Composting, and Conventional Cremation all are extremely polluting in their lack of responsible mercury handling, hazardous waste disposal, and housing of hazardous waste sites in residential neighborhoods and industrial sites. These expose the public to dangerous availability of mercury in the form of vapor, contaminated wastewater, and natural disaster unpreparedness in the event of wildfire, earthquake, or other environmental disasters.

10. **Taking time for natural grief, enjoyment of a beautiful funeral in nature.** Most green cemeteries don't put a limit on how long you and your family can take alongside a green grave-side burial. Memorial Halls with spectacular views of nature can remind us of its eternal aspect and the natural cycle of life and death.

As John Muir wrote in 1869, in “My First Summer in the Sierras”, “Another glorious Sierra day in which one seems to be dissolved and absorbed and sent pulsing onward we know not where. Life seems neither long nor short, and we take no more heed to save time or make haste than do the trees and stars. This is true freedom, a good practical sort of immortality... One is constantly reminded of the infinite lavishness and fertility of Nature ... no particle of her material is wasted or worn out. It is eternally flowing from use to use, beauty to yet higher beauty; and we soon cease to lament waste and death, and rather rejoice and exult in the imperishable, unspendable wealth of the universe...[We] faithfully watch and wait the reappearance of everything that melts and fades and dies about us, feeling sure that its next appearance will be better and more beautiful than the last.”

Additional statistics relating to cremation pollution:

- The Environmental Protection Agency estimates crematoriums emit 320 pounds of mercury per year, while activists say the real figure could be as high as three tons in 2007. A review of a study done by the EPA that estimated emissions from dental amalgam has since been underestimated. The United Nations Environmental Programme current (2019) accounts

indicate that 340 tons (680,000 pounds) of mercury is discharged into the environment from dental amalgam, 100 tons of which enters the waste stream. From cremation, tooth loss, human waste and infectious waste are released significant releases, and it was determined that cremation is the most critical because of the invisibility of vapors into the air without adequate or appropriate filters.

- Mercury in dental amalgams has been banned in Denmark, Norway and Sweden. However it is estimated by the EU EPA that 1,500 tonnes (200,000 pounds) of mercury is held in human bodies and will be released in cremation, with 75% of 500 million EU residents having had mercury fillings in their older generations, 1,500 tons (3 million pounds) total mercury in their bodies to become cremated. [Http://www.eea.europa.eu/publications](http://www.eea.europa.eu/publications)
- Overall the US has a 51% cremation rate, while Oregon and Washington have 70% rates mostly due to the myth in advertising that cremation saves on cemetery land use and is therefore “green”. However, Neptune Society, the largest funeral monopoly in the US, will not comment about high fossil fuel use or about mercury vapor emissions, claiming instead that cremation is “green”.
- 340 tons of dental mercury in the world is dumped directly into waste water systems, 34 tons at minimum in the U.S. In 2008, the average European held 2-5 grams of mercury in their bodies, while the allowable amounts are zero grams.
- For an excellent discussion of the link between mercury vapors in the environment and the link with Alzheimer’s Disease (the 3rd largest cause of death in the US), watch the documentary “Evidence of Harm” a youtube video by AD researcher Dr Boyd Haley, PhD. <https://www.youtube.com/watch?v=Wqb4fDSODiQ> and <http://Evidence-of-Harm.com>

This is a documentation of Dr Haley’s 26 year NIH career linking mercury toxicity with Lewy bodies in Alzheimer’s Disease and other neurological diseases.

It is commonly thought that cremation is more environmentally-friendly because it seems to simplify the funeral/burial process and minimizes land use for cemeteries and their inherent pollutants. However, research shows the facts which reveal disturbing problems for the environment. In our research into the effects of cremation on the environment, **there was a strange lack of studies, particularly by the EPA and U.S. government.**

"In 2012, the EPA Crematorium Working Group reported that crematoria are significant sources of mercury, dioxin, and particulate matter. Incineration of bodies, body parts, and infectious and chemotherapeutic wastes collectively represent the second largest known source of dioxin and mercury pollution in the US. The World Health Organization, the US EPA and other public health experts consider any level, no matter how low, of emissions of mercury, dioxins, furans, and particulate matter from incineration to be a threat to human health. Vulnerable populations such as babies, children, women of childbearing age, and the elderly are particularly at risk from exposure to these toxins. Employees who work in these environments, as well as those populations who live near the source are exposed to higher levels of these pollutants. The effects of mercury vapor exposure can last long after the exposure has ended. While typical symptoms and signs, such as tremors, gingivitis and salivation may quickly disappear after exposure has stopped, mechanisms of long-lasting or remote effects have not been investigated. This is possibly due to the damage caused by mercury vapor exposure remaining for a long period of time, or by mercury remaining in the body and continuing to cause adverse effects, or to the prior exposure somehow stimulating aging, resulting in poorer neurobehavioral performance.

The final report of the Senate Crematoria Study Committee was prepared in 2012. This report noted that while there are emissions of other chemicals during the cremation process, mercury is of the most concern to communities near crematoriums. When mercury is burned, it becomes a colorless and odorless gas that can travel long distances. While mercury exposure has the potential to cause a variety of health problems, the brain and kidneys are especially vulnerable. According to Dr. Anne Summers of the University of Georgia, there is no known lower level for toxicity of mercury, and scientists clearly agree that mercury

toxicity can have serious consequences on human health.” (from *Mercury Contamination from Dental Amalgam*, 2019)

Amy Cunningham, a “green” funeral director of Fitting Tribute Funeral Services (and Crematory) in the Greater New York City area writes in her well-known blog:

“Cremation takes up less land and might save some money, but here’s the downside with some crematories: it takes a lot of fossil fuel to heat that retort (or cremation chamber) to 1800 degrees F and keep it heated for two to three hours... Then perhaps, if you are not satisfied with the answers you’re getting and your family is open to changing plans quite dramatically, consider the love of my life (sorry Steve)–Green Burial. Pine box. Or simple shroud. Drive out of the city and convene in a green cemetery. Let your loved one descend into the soil naturally–without chemicals or vaults or barriers to Mother Nature.”

More studies and research have been done in Europe in recent decades as the rate of cremation has increased slowly over the last century. Several articles reveal periodic surveys of literature over twenty years that showed a largely unregulated industry by the US Environmental Protection Agency. At the grassroots level, citizens in both Canada and in over 35 U.S. states have set up blocks and ordinances, built a library of research for other states to refer to and assisted in local initiatives to deconstruct or prevent the further building of crematories.

Several studies in the last two decades have shown a correlation between local crematories and stillbirth, anencephaly, and increasingly widespread air pollution containing toxic gases. Finally, a visit to the Crematorium will show you that both the time a family can say goodbye to their loved one’s body and naturally move through the letting-go process is very minimized and tends to make the grief process interrupted.

Cremation involves a box or casket containing the body to be placed in a steel incinerator and heated to temperatures from 7600 to 21000 F. At the highest temperature, most of the body is vaporized and oxidized as water within about

two hours. However, gases released are then temporarily held in a second metal chamber or “filter” and then released to the outside air through an exhaust system.

It is commonly thought that crematories have “filters” –adjacent storage tanks that are supposed to catch and “hold” toxins such as mercury. The EPA's answer to this has been to add a second “chimney” in effort to somehow “catch” some of the toxic vapors.

“Gaseous emissions are by far the greatest source of cremation pollution and thus far the only crematorium waste that is regulated. In addition to harmless compounds such as water vapor, emissions include:

- the greenhouse gas carbon dioxide
- extremely toxic mercury vapors
- toxins and carcinogens of carbon monoxide, nitrogen oxide, and sulfur oxide;
- volatile acids such as hydrogen chloride and hydrogen fluoride, both of which form during vaporization of plastics and insulation
- compounds such as benzenes, furans and acetone are also emitted and react with HCl and HF under combustion conditions to form polychlorinated dibenzodioxins (PCDDs) and polychlorinated dibenzofurans (PCDFs), both of which cause cancer.

“These and mercury are of special concern because they are susceptible to bioaccumulation.”

An estimated one-third of all air-borne mercury pollution is due to the cremation of bodies containing mercury from dental amalgams in the deceased person's mouth. In a “Summary of References on Mercury Emissions from Crematoria, September 25, 2012”, Jon Reindl, P.E. investigated studies in both the U.S. and Europe for three aspects of cremation: mercury emissions, deposits in filters and chimneys, and mercury found in cremains (cremation ash):

“Crematoria represent a significant source of mercury emissions to the

environment. While estimates of the quantities vary significantly, it appears that each cremation releases between 2 and 4 grams, with the maximum seen by this reviewer at 8.6 grams in an individual cremation in Switzerland. There has been an increase in the number of cremations annually and forecasts include both a further increase in the number of cremations over time and an increase in the amount of mercury released in the next few decades due to an increase in the number of the deceased having a larger number of their own teeth with amalgam restorations. This increase is expected to be followed by a decrease in mercury emissions from industrialized countries as the next generation of people has both fewer cavities and an increased substitution of amalgam restorations with restorations that do not use mercury.”

“In the US, a mercury flow worksheet developed for Region V of the EPA estimates that in 2005, just under 3,000 kilograms of mercury were released to the environment from cremation to the US. Bender estimates that this will increase to 7,700 kilograms by 2020.”

“Most of the mercury from crematoria is released into the air, although some may collect on the walls of the oven and chimney. Soil surveys have shown that while there is often an elevation of mercury in the topsoils near crematoria, most (over 99%) of the mercury emitted to the air does not settle to the soil in the nearby area, but is instead added to the general atmosphere. Mercury levels in the ash have been only rarely tested, and have been shown to be negligible in those tests.” One wondered what the blood and tissue levels of air-borne mercury is in crematory industry workers who breathe in mercury fumes every day.

“Mercury emissions from crematoria are regulated in few places in the world, although the amount of regulation is slowly growing. Possible control of mercury from crematoria includes the removal of teeth with amalgam restorations before cremation, the use of selenium capsules to bind up the mercury and exhaust gas capture systems. The effectiveness of the selenium capsules is controversial and the effectiveness of the exhaust gas capture systems is not well documented.”

Although laws now require crematoriums to place mercury storage tanks on their incinerators, most of the toxic residues are released. These also include toxic metals or plastics that can leach into the air and then water, causing a public health concern. One study of the Cremation Association of North America found that “filtering crematorium fumes has little effect on the toxins released.” In India, where outdoor cremation has been the norm for thousands of years, air pollution is in the top five highest percent in the world. Meanwhile, more research needs to be done in the U.S. to assess these very real effects of crematory air pollution.

In addition, there is the issue of cremation remains and their dispersal into the air. “Cremains are often sprinkled somewhere in memorial, releasing whatever compounds and toxins found in them back into the environment in a form that is easily picked up by wind or water,” writes Huffman. A scientific method for analyzing cremains is X-ray diffraction and has found that “calcified compounds within cremains can contain metals such as **lead, boron, cadmium, chromium, cobalt, copper, tin, lithium, magnesium, manganese, nickel, and strontium.** Metals such as arsenic and selenium, though present in a live human body, are volatile and decompose quickly upon burning... I have found no studies of whether or not sprinkling cremation remains could have a significant impact on the levels of metals in the soil.” Often the ashes are then stored in metal urns or other non-biodegradable receptacles, and then buried in cemeteries which are already over-filled. Many cemeteries, particularly in larger US cities, as well as in Japan and Europe have reached maximum use. In London, a space crisis led to proposals to reopen old graves to create more space for the burial of cremains and the deceased.

“Not all that remains is bone. There may be melted metal lumps from missed jewelry, casket furniture, dental fillings, and surgical implants, such as hip replacements. Breast implants do not have to be removed before cremation. Large items such as titanium hip replacements (which tarnish but do not melt) or casket hinges are usually removed before processing, as they may damage the processor. (If they are missed at first, they must ultimately be removed before

processing is complete, as items such as titanium joint replacements are far too durable to be ground.) Implants may be returned to the family, but are more commonly sold as ferrous/non-ferrous scrap metal. After the remains are processed, smaller bits of metal such as tooth fillings, and rings (commonly known as gleanings) are sieved out and may later be interred in common 'consecrated' ground in a remote area of the cemetery."

While cemeteries may have the illusion of holding consecrated ground, they are actually sites of heavy metal waste that accumulate over time and which cause leaching downstream, especially when located adjacent to natural water sources such as creeks, rivers, and oceans as is more common in older cemeteries.

On a more positive note, Community Awareness Network (CAN!) is an informal grassroots organization that advocates on the local, state and national levels for change in the way the crematory industry in America is being operated and regulated. It "educates communities about the real nature of toxins in crematory emissions and what they can do to succeed when faced with the challenge of preventing or stopping a crematory from operating in a residential area or near schools and daycare facilities."

As of 2015, CAN has grown to 55 individual communities in 35 states. Originally, it started as a small group of volunteers and then grew to 700 residents who organized a protest that successfully proved to their county planning department that their town was too densely populated to accommodate a crematorium. They believe no more communities should have to absorb another crematory that is unsafe for public health and the environment.

The CAN Website reports:

"When first faced with this daunting task, it was noticed that there are communities who had challenged crematories near their residential areas ... but with varied results. Wanting to learn from the success stories, and the failures, many hours were spent online searching blogs and forums of newspapers around

the country. The successful communities all had one thing in common: someone in that community was willing and able to stop everything else they were doing and devote their energy to finding the data, and these voluntary warriors motivated their neighbors to act. These communities fought back and won – but at a huge cost. The cost was so great to most of these “activists/advocates” that once the fight was over, they mostly just wanted to go back to their normal lives. Who could blame them? The fight is exhausting. The only reward is winning – preventing or stopping or even closing down a crematory. There is no financial gain, only the stop-loss prevention of property devaluation and keeping one more pollution source away. For the communities that have lost – the cost to their health, homes, and happiness has no measure. How can you measure that? It was decided that no community should ever have to re-invent the wheel when faced with a crematory near their homes. No community should win or lose based on whether or not there is a volunteer activist among them. No more communities should have to spend months of research just to determine if the crematory is going to be unsafe and then prove that to their local government authority.”

Is it not strange that a government agency such as the EPA would not preventatively or even extensively study the toxic emissions of cremation? Why is the cremation industry largely unregulated when there clearly are toxic gas emissions? With cremation reaching an all-time high of nearly 50-70%, and with humanity’s huge impact on the environment worldwide over the last two centuries, the mercury and gaseous emissions of our cremains must now be extensively studied, and existing crematoria must be regulated by local, state or federal agencies. Further building of crematoria should be halted while alternatives for our deceased and their descendants and environment should be put first. Alternatives include green burial which allows for the natural return of our bodies to the earth.

Finally, cremation does not allow for the necessary time essential for the natural letting-go and grief process that is made “real” for people with burial. People tend to “send away” the body, or if they actually visit the Crematory, there is a short

amount of time to “say goodbye” to their loved one’s body. Numerous experiences and videos show how there is limited time at a crematory. The “industrial” environment of cement walls and steel ovens has little ambiance of emotional safety for the grieving person or family. The grief process then tends to be aborted or put off for some other time when it is more “convenient”. Although some families have a memorial prior to cremation there is often still a lingering feeling of difficulty accepting that a loved one has died. A grief that is complicated from a sudden loss, traumatic accident or suicide becomes even more difficult when the body is boxed away and cremated before a person can fully accept it and come to terms with the surreal feeling, numbness and other feelings specific to these types of loss.

With memorialization and burial, there is much more time to see the body, tend to it, and bury a beloved in a final goodbye with an attitude of acceptance and in a timely way. Grave-side funerals also allow for the influence of nature, where we can see that everyone is given the gift of both birth and death in the natural life cycle. With the twenty-year-old natural burial movement, which is really a return to ancient million-year-old traditions, there is much more involvement by the family to be involved in natural deathcare, even if a funeral home is involved. The movement invites people to spend up to 3 days being with their loved one in a home vigil, home funeral, and natural rites of passage that ease and more quickly heal the grief process.

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[“The book can be recommended as a suitable complement of the general practitioners library...the information, provided in this book will help physicians to realize that those important first steps in lifestyle counseling—helping patients to get rid of heavy metal excess—are of a greater importance than has generally been assumed.” — *V. Bencko in the Journal of Hygiene, Epidemiology, Microbiology and Immunology, 34, 1990, No. 1*].

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Josh Kinkade

From: Elaine Andersen
Sent: Monday, March 21, 2022 10:34 AM
To: Josh Kinkade
Subject: FW: Lakeside crematorium

-----Original Message-----

From: Marie [REDACTED]
Sent: Monday, March 21, 2022 10:34 AM
To: Elaine Andersen <eandersen@folsom.ca.us>
Subject: Lakeside crematorium

[You don't often get email from [REDACTED] Learn why this is important at <http://aka.ms/LearnAboutSenderIdentification.>]

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am Marie Gonzales and my husband and I have been living in the Preserves for over 10 years. We have a lovely neighborhood and I love my small community. I have asthma and I had thyroid cancer 4 years ago so I do not have the best respiratory system. I love the clean air that we breathe and love to keep it that way. I know Igor has presented his views on how there's no concern for us as to air quality but cannot believe there will be 0% toxic emissions. Just like the HCD has voted no to this project, please let me have trust in that you'd do the same. In a poll that somebody did on FB chat, a large percentage of our local residents do not place a lot of emphasis on being cremated here in Folsom, a lot answered they did not care where they were cremated. I know a lot of people may think what's the big deal, but I think they are thinking, "oh I'm glad it's not in my neighborhood!". Nobody is going to say, "Oh how lucky are those people living next to the crematorium! I wished I lived there!" Will you? Thank you for being so understanding. Marie Gonzales

Marie Sent from my iPhone

Close

Send

To:  kerri@atlanticcorrosionengineers.com  rrodriguez@folsom.ca.us
 Sarah Aquino  ykc@folsom.ca.us  mkozlowski@folsom.ca.us

Cc/Bcc:

Subject: **Crematorium****Council Members,**

After reaching out to Dr. Bob Laperriere, of the of the Sacramento County Cemetery Advisory Commission, he answered a few of my questions after the Historic Commission meeting regarding the historical site designation of the Lakeside Memorial Cemetery. Here is our email question and answer text (received 2/19/22 at 10:52pm):

Me: Do you know the date the Lakeside Memorial Cemetery received its historic designation?

Bob: Do no have the date handy but it was over 10 years ago.

Me: I feel that the owner/applicant, Igor Semenyuk, is trying to downplay the sites historical significance. During the Commission meeting he said to the best of his knowledge it was not a registered historical site, and that it didn't have historical significance.

Bob: I can see where he is not clear on "registered" as it is not on the State or National historic designation...only the designation that our Commission requested of the board of supervisors. However it is difficult to not believe or deny the historic significance of the cemeteries that "became" Lakeside and date back to the 1800's in addition to the adjacent Chinese Cemetery. We did give a large aluminum sign to the Funeral Home to post at the cemetery with its historic designation on it but do not recall ever seeing it posted.

Bob LaPerriere
Chair, Sacramento County Cemetery Advisory Commission
chca@winfirst.com
(916) 481-4525 (voice)
(916) 712-8991 (text)
POB 255345, Sacramento CA 95865-5345

During the 2/16/22 Historic District Commission meeting (at the 4:46 mark) the applicant, Igor Semenyuk, states, "this is not an historical cemetery." Clearly, he is mistaken. His partner Lorin Claney's father purchased the funeral home in 1962, so Lorin Claney had been a part of the family business when the funeral home received their designation and plaque.

The "Complete Sacramento County Cemetery List" from the Sacramento County Advisory Commission states there are many historical parts of Lakeside Cemetery. These historic portions of Lakeside Memorial Cemetery include: Babyland, Citizens, Cook, Jewish, Masonic, and Oddfellows cemeteries. There's also a question of historic Negro Bar Cemetery being a part of Lakeside.

Also, according the The Miller Funeral Home website they state: " Miller is also proud to own and operate Lakeside Memorial Lawn, Folsom's only active historic cemetery. With headstones and burials dating back to 1846, Lakeside remains a beautiful memorial to Folsom's citizens both past and future."

More so, in 1995 the United States Department of the Interior National Park Service also entered Chung Wah Cemetery in the National Register.

I have contacted the list of historic cemeteries that Dr. Bob LaPerriere included in his letter to the Historic District Commission. I reached out to all of them, and none have a crematory on their historical site. I have included the email correspondence. Why put a crematory on this historical site? Does Folsom really want to have the only crematory on historic grounds? Once it's in, it's forever.

The Lakeside Memorial Cemetery has Historic Designation! Let's protect it!

Thank you for your time,

Nicole Gates

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Historical Designation On the recommendation of the Commission, the Board of Supervisors has designated 21 cemeteries as "historic". This is phase I of the project, as there are many other historic cemeteries in Sacramento County. Cemeteries designated were:

1. Bellview Cemetery
2. Union Cemetery
3. Lakeside Cemetery
4. Matthew Kilgore Cemetery
5. 24th & Meadowview Cemetery
6. Chung Wah Cemetery
7. Elder Creek Cemetery
8. Rancho Marieta Cemetery (North & South) (2)
9. Sacramento Historic City Cemetery
10. Michigan-Bar Cemetery (Ione Road)
11. Sloughhouse Cemetery
1. Sylvan District Cemetery
12. Galt /Arno District Cemeteries (2)
13. Elk Grove Consumnes District Cemeteries (5)
14. Fair Oaks District Cemetery

double checked with address and Federal reference.
 Sacramento County No crematory - satellite view
 Sacramento County No - only 10 gravestones
 Folsom \emptyset
 Rancho Cordova - No crematory (email 2/22)
 Sacramento City - NO crematory - DEFUNCT cemetery
 Folsom \emptyset
 Sacramento - No crematory - checked on Fed. list
 Rancho Marieta - No crematory (email 2/25)
 Sacramento City - No crematory (email 2/23)
 Sacramento County - No crematory (email 2/25)
 Sloughhouse - No crematory (email 2/25)
 Citrus Heights - No crematory (email 2/22)
 Galt - No crematory (email 2/22)
 Elk Grove - No crematory (email 2/25)
 Fair Oaks - No crematory (email 2/22)

Plaques were provided for each cemetery, and we still have several to distribute. The text of the plaque is as below:

***THIS SITE HAS BEEN DESIGNATED BY
 SACRAMENTO COUNTY AS A
 HISTORIC CEMETERY.***

***HERE REST MANY OF THE MEN, WOMEN AND CHILDREN WHO SAW THE BEAUTY
 AND VALUE OF THIS LAND, CHOOSING TO SETTLE HERE
 AND BUILD THE COUNTY WE CHERISH TODAY.***

***PLEASE HELP US PRESERVE THESE GRAVES, MARKERS,
 AND LANDSCAPING FOR FUTURE GENERATIONS.***

***SACRAMENTO COUNTY BOARD OF SUPERVISORS
 AND CEMETERY ADVISORY COMMISSION***

#4

Matthew Kilgore Cemetery - No crematorium

05/10/2022 Item No. 18.

From: **Steve Harriman** sharriman@cityofranchocordova.org
Subject: **RE: Crematory question**
Date: **Feb 22, 2022 at 10:40:59 AM**
To: **nicole higgins** niki_higgins@yahoo.com

Hi Nicole:

There is not a crematorium at **Kilgore Cemetery**. The City contracts with Green Valley Cemetery and Mortuary and they have a crematorium. They can be reached at [916 985-8844](tel:9169858844).

Please feel free to contact me if you have additional questions. Thanks!

Steve

Steve Harriman, Operations and Maintenance Division Manager
City of Rancho Cordova Department of Public Works
[\(916\) 851-8716](tel:9168518716)

-----Original Message-----

From: nicole higgins [REDACTED]
Sent: Tuesday, February 22, 2022 9:06 AM
To: Steve Harriman <sharriman@cityofranchocordova.org>
Subject: Crematory question

Hello,

I was wondering if your cemetery has a crematory on the premises?

Thanks,

Nicole

Sent from my iPhone

From: Dennis Buscher [REDACTED]
Subject: Re: Sloughhouse Cemetery
Date: Feb 25, 2022 at 6:49:55 PM
To: niki_higgins@yahoo.com
Cc: eghs@elkgrovehistoricalsociety.com

Hi Nicole

I am a Board member of the Elk Grove Historical Society and also a Trustee for the Elk Grove Cemetery District.

In answer to your question, Sloughhouse Cemetery does not have a crematorium in their cemetery, nor do any of the 5 cemeteries in the Elk Grove Cemetery District. While the Elk Grove cemeteries are not designated as historic yet, they do date back to the 1870's. The Rancho Muieta cemeteries also do not have a crematorium.

I would think that the zoning laws would be very restrictive for the placement of a crematorium for the cremating of bodies. Check with the County of Folsom to see what restrictions there are for a crematorium. It sounds like the cemetery your concerned about is privately owned, so they may have rights that public cemeteries do not.

In the Elk Grove Cemetery District, we do have Niche Banks for the placement of ashes in 4 of our 5 cemeteries. These niche banks are about 5 feet high. Sloughhouse does not have a niche bank.

If you have any other questions, please let me know.

Dennis Buscher
2nd VP, EG Historical Society



- #8 Rancho Murieta (N+S)
- #10 Michigan Bar (Part of Rancho Muieta cemeteries)
- #11 Sloughhouse
- #13 Elk Grove Consumnes

From: nicole higgins [REDACTED]
Date: Fri, Feb 25, 2022 at 10:03 AM
Subject: Sloughhouse Cemetery
To: <eghs@elkgrovehistoricalsociety.com>

\
All have no crematories

Hi,

#9 Sac City Cemetery - No crematory

From: **Lori Bauder** LBauder@cityofsacramento.org
Subject: **RE: City Cemetery Information**
Date: **Feb 23, 2022 at 8:04:36 AM**
To: nicole higgins [REDACTED]

Hi, Nicole;
I'm sorry we do not have a crematory on site. Lori

Lori Bauder
Cemetery Manager
1000 Broadway
Sacramento, CA 95818
Cell: [916-201-6254](tel:916-201-6254)

-----Original Message-----

From: nicole higgins [REDACTED]
Sent: Tuesday, February 22, 2022 9:49 AM
To: Lori Bauder <LBauder@cityofsacramento.org>
Subject: City Cemetery Information

Hello,

I was wondering if the **Sacramento Historic City Cemetery** had a crematory on-site?

Thanks,

Nicole

Sent from my iPhone

From: **Sylvan Cemetery** office@sylvancemetery.com
Subject: **Re: Crematory**
Date: **Feb 22, 2022 at 9:55:37 AM**
To: **nicole higgins** [REDACTED]

No, we are the cemetery only.

Sylvan Cemetery District
[\(916\) 725-3406](tel:(916)725-3406)

On Tuesday, February 22, 2022, 09:39:38 AM PST, nicole higgins <niki_higgins@yahoo.com> wrote:

Hello,

I was wondering if you have an on-site crematory at the **Sylvan District Cemetery?**

Thanks,

Nicole
Sent from my iPhone

#1 (2)
Sylvan Cemetery - No crematory

From: galarn@softcom.net
Subject: RE: Crematory
Date: Feb 22, 2022 at 1:50:15 PM
To: nicole higgins [REDACTED]

Good afternoon Nicole,

We are solely a cemetery. There is no crematory on site.

Have a wonderful afternoon. We are here if you need any more assistance.

Kristi

-----Original Message-----

From: "nicole higgins" [REDACTED]
Sent: Tuesday, February 22, 2022 9:37am
To: galarn@softcom.net
Subject: Crematory

Hello,

I was wondering if there is a crematory on the premises of the Galt/Arno district cemeteries?

Thanks,

Nicole

Sent from my iPhone

12

Galt/Arno District Cemeteries

- No crematory

From: info.focd@gmail.com
Subject: Fair Oaks Cemetery Inquiry
Date: Feb 22, 2022 at 9:41:06 AM
To: [REDACTED]

We do not have a crematory on site.

Fair Oaks Cemetery District
[916-966-1613](tel:916-966-1613)
7780 Olive Street
Fair Oaks, CA 95628

#14

Fair Oaks District Cemetery - No crematory

Coroner

Complete Sacramento County Cemetery List

This list of Sacramento County Cemeteries is a project in progress/development. Data on many is limited or non-existent. Accuracy of entries may need to be verified. We will appreciate any comments, additions, corrections, additional information, documents or photographs relating to these cemeteries.

Please respond to the Sacramento County Cemetery Advisory Commission at:

Dr. Bob LaPerriere, Vice-Chair
 Sacramento County Cemetery Advisory Commission
 Phone: (916) 874-9103
 Email: cemeterycommission@saccounty.net
 (please include your email address for correspondence)

<u>Cemetery</u>	<u>Category†</u>	<u>Notes</u>	<u>More Information</u>
24th & Meadowview Cemetery	No longer exists - no remains		Cemetery History
Alder Creek Cemetery	No longer exists - no remains	see Prairie City Cemetery	Cemetery History
Arlington Cemetery	No longer exists - no remains	see Quiet Haven	
Kinney School Cemetery	No longer exists - no remains		
Prairie City Cemetery Monument	No longer exists - no remains		
B'nai Israel Cemetery	No longer exists - possibly or partially relocated		

<u>Cemetery</u>	<u>Category†</u>	<u>Notes</u>	<u>More Information</u>
Brenton Burials	No longer exists - possibly		

	or partially relocated	
Buckeye Knoll Cemetery	No longer exists - possibly or partially relocated	
Mormon Island Cemetery	No longer exists - possibly or partially relocated	
Negro Bar Cemetery	No longer exists - possibly or partially relocated	(?now Lakeside???)
New Helvetia Cemetery	No longer exists - possibly or partially relocated	
St. Rose's Cemetery	No longer exists - possibly or partially relocated	
Cook's Bar Cemetery	Possible other alias or unknown site	
Daylor's Ranch Cemetery	Possible other alias or unknown site	
Dry Creek District Cemetery	Possible other alias or unknown site	
Georgetown Cemetery	Possible other alias or unknown site	
Grand Island Chinese Cemetery	Possible other alias or unknown site	
Granger Cemetery	Possible other alias or unknown site	
Hoit Ranch Cemetery	Possible other alias or unknown site	
Nathan Ranch Cemetery	Possible other alias or unknown site	
see 24th and Meadowview Cemetery??	Possible other alias or unknown site	
see Michigan Bar Cemetery	Possible other alias or unknown site	
Sloughouse Cemetery # 2	Possible other alias or unknown site	(?)
Stage Station Burying Ground	Possible other alias or unknown site	
Strait Family Cemetery	Possible other alias or unknown site	
Sutterville Cemetery	Possible other alias or unknown site	



	name	Cemetery
San Juan Cemetery	With an alternate name	see Sylvan Cemetery
Sheldon Cemetery	With an alternate name	see Pleasant Grove Cemetery
Spooner Family Cemetery	With an alternate name	see Michigan Bar Cemetery
Walltown Cemetery	With an alternate name	see Wilson Cemetery?
Babyland	Within another cemetery	see Lakeside
Bellview Cemetery	Within another cemetery	see Quiet Haven
Citizens	Within another cemetery	see Lakeside
Cook	Within another cemetery	see Lakeside
Eagles	Within another cemetery	see Eagles & Knights of Pythias
Elk Grove IOOF Cemetery	Within another cemetery	see Hilltop Cemetery
Elk Grove Masonic Lawn	Within another cemetery	see Elk Grove Cemetery
Grand Army of the Republic (Civil War)	Within another cemetery	see Sacramento Historic City Cemetery
Jewish Cemetery	Within another cemetery	see Lakeside
Knights of Pythias	Within another cemetery	see Eagles & Knights of Pythias
Knights of Pythias	Within another cemetery	
Masonic	Within another cemetery	see Lakeside
Masonic	Within another cemetery	see Elk Grove
Masonic (Old)	Within another cemetery	see Sacramento Historic City Cemetery
Oddfellows Cemetery	Within another cemetery	see Lakeside

Notes

[More Information](#)

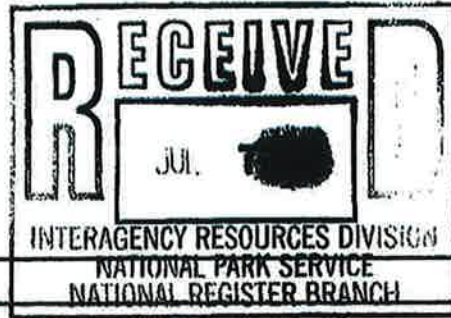
Walnut Grove Chinese Cemetery	Possible other alias or unknown site
Barton Family Cemetery	Site in Sacramento County
Chinese Cemetery-Folsom (Young Wo) Cemetery	Site in Sacramento County
Chinese Cemetery-Folsom Chung Wah Memorial Site	Site in Sacramento County

◀ 31 - 60 ▶



- > [Cemetery Advisory Commission](#)
- > [Brochures](#)
- > [Cemetery Districts](#)
- > [Sacramento County Indigent Burial Sites](#)
- > [Sacramento County Deaths 1850-1933](#)
- > [Burial Database, Sacramento County Cemetery](#)
- > [Sacramento County Cemetery List](#)
- > [Commission Contact Information](#)
- > [Useful Links](#)

United States Department of the Interior
National Park Service



**NATIONAL REGISTER OF HISTORIC PLACES
REGISTRATION FORM**

1. Name of Property

historic name: Chung Wah Cemetery
other name/site number: Sze Yup/Sam Yup Cemetery

2. Location

street & number: Momon Street vicinity not for publication
city/town: Folsom vicinity
state: California code: CA county: Sacramento code: 067 zip code: 95

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act of 1986, as amended, I hereby certify that this nomination request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60. In my opinion, the property meets does not meet the National Register criteria. I recommend that this property be considered significant nationally statewide locally. (see continuation sheet for additional comments.)

[Signature] 6/6/95
Signature of certifying official Date

California Office of Historic Preservation
State or Federal agency and bureau

In my opinion, the property meets does not meet the National Register criteria. (See continuation sheet.)

Signature of commenting or other official Date

State or Federal agency and bureau

4. National Park Service Certification

I hereby certify that this property is:

- entered in the National Register See continuation sheet.
- determined eligible for the National Register See continuation sheet.
- determined not eligible for National Register
- removed from the National Register
- other (explain): _____

[Signature] 3/21/95
Signature of Keeper of Action Date

From: [REDACTED]
To: [Elaine Andersen](#); kerri@atlanticcorrosionengineers.com; [Sarah Aquino](#); [Rosario Rodriguez](#); [YK Chalamcherla](#); [Mike Kozlowski](#); [Josh Kinkade](#)
Subject: Crematorium Council Meeting Change
Date: Friday, March 25, 2022 4:46:23 AM

Some people who received this message don't often get email from [REDACTED]
[Learn why this is important](#)

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Members of the Council,

Our lives have been on hold and turbulent for one greedy business and their “business opportunity.”

This has cost us time, money, sleepless nights, our real estate, and our health. In fact, a neighbor had a health event while in a conversation about the crematorium! Our neighbor has no fight left, and is very upset over that fact. This is a neighborhood in turmoil!

“The Folsom City Council is dedicated to ensuring Folsom's high standards for public health, safety, and quality of life.”

Why is The Caring Services Group allowed to add to their appeal after the deadline? Why are they allowed to extend the hearing date when the municipal code says the meeting has to be within a specific time frame? Why is the applicant getting extra hand holding? What’s the point of municipal codes if they aren’t followed?

Why weren’t the citizens notified of the meeting date change? We have been in contact with the Council, Clerk, and Planning Department this whole process. Why aren’t the residents being kept up to date...transparency?

Please move the hearing date back to April 12. That’s our kids spring break at school. When we heard that the crematorium meeting date was over spring break we cancelled our vacation to attend the meeting. It’s too expensive to purchase tickets again. We stopped our lives for this. Please consider the residents as well in this process.

Thank you for your time.

Nicole Gates

Josh Kinkade

From: Elaine Andersen
Sent: Wednesday, March 30, 2022 10:19 AM
To: Josh Kinkade
Subject: FW: Lakeside Memorial Crematory: Please Vote No

-----Original Message-----

From: Patrick Nooren [REDACTED]
Sent: Wednesday, March 30, 2022 9:21 AM
To: Elaine Andersen <eandersen@folsom.ca.us>
Cc: Patrick Nooren <pnooren@biddle.com>
Subject: Lakeside Memorial Crematory: Please Vote No

[You don't often get email from [REDACTED] Learn why this is important at <http://aka.ms/LearnAboutSenderIdentification.>]

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To City Manager Anderson.

Hello. My name is Patrick Nooren and I am a longtime resident of El Dorado Hills. My wife and I are currently in the escrow process on a house in Folsom directly across from Lakeside Memorial Lawn Cemetery [REDACTED] Fong St.). Our intent is to live there with our disabled daughter, Dutch, who has Pallister-Killian Syndrome (PKS) and is compromised in a number of ways, including respiratory issues and being wheelchair bound.

This leads us to "why" we are moving. We are looking to downsize into a single-story house with a short, flat, walk to Sutter Street...a rarity in the adjoining neighborhoods...and [REDACTED] Fong is perfect!

While performing my due diligence I came across the upcoming vote on the crematorium and, as a result, I have read each of the "Helix" studies, including the Greenhouse Gas and Emissions study (and amendments). The results of these studies indicate the environmental impact would be statistically insignificant, but fall short of indicating there would be "no impact." In fact, my particular home at [REDACTED] Fong would be within the zone of elevated emissions.

Should this vote approve the crematorium, I will, unfortunately, not be able to continue with the purchase of this, the perfect home. I simply cannot risk the elevated emissions and the potential for a negative impact on my extremely vulnerable daughter.

Please vote no.

While there may be some potential for additional Folsom tax revenue, it is far outweighed by the potential negative ramifications to nearby property values, the Sutter Street experience (and potential odor) and, unfortunately, the health and safety of those who would live nearby with compromised health.

Thank you.

Patrick M. Nooren
President
Biddle Consulting Group, Inc.

[REDACTED]
<https://gcc02.safelinks.protection.outlook.com/?url=http%3A%2F%2Fwww.biddle.com%2F&data=04%7C01%7Cjkin%2Fkade%40folsom.ca.us%7C90d79ed9c9dc46fd805e08da12715c29%7C1cfb4b4a254c47b48448af71335fd6c0%7C0%7C0%7C637842575334134134%7CUnknown%7CTWFpbGZsb3d8eyJWIjoiMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTiI6IjEhaWwiLCJXVCi6Mn0%3D%7C3000&sd=Wo4G0xXIbmIENyfrIMVtxUc%2BJVcFCN083jVzdyDzwYo%3D&reserved=0>

|
<https://gcc02.safelinks.protection.outlook.com/?url=http%3A%2F%2Fwww.bcginstitute.org%2F&data=04%7C01%7Cjkin%2Fkade%40folsom.ca.us%7C90d79ed9c9dc46fd805e08da12715c29%7C1cfb4b4a254c47b48448af71335fd6c0%7C0%7C0%7C637842575334134134%7CUnknown%7CTWFpbGZsb3d8eyJWIjoiMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTiI6IjEhaWwiLCJXVCi6Mn0%3D%7C3000&sd=88mRA3jYFmzqGppqGA7oZTmDjepmc5drp5%2FbASVFjz8%3D&reserved=0>

<https://gcc02.safelinks.protection.outlook.com/?url=http%3A%2F%2Fwww.testgenius.com%2F&data=04%7C01%7Cjkin%2Fkade%40folsom.ca.us%7C90d79ed9c9dc46fd805e08da12715c29%7C1cfb4b4a254c47b48448af71335fd6c0%7C0%7C0%7C637842575334134134%7CUnknown%7CTWFpbGZsb3d8eyJWIjoiMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTiI6IjEhaWwiLCJXVCi6Mn0%3D%7C3000&sd=g2TYrT85J8gRDXyh4hpxYnGtiKPNrahBTOPqcwWYUSE%3D&reserved=0>

[REDACTED] Fong St. (hopefully)
Folsom, Ca 95630

March 23, 2022

Elaine Anderson, City Manager
 Kerri Howell, Mayor
 Rosario Rodriguez, Vice Mayor
 Sarah Aquino, Council Member
 YK Chalamcherla, Council Member
 Mike Kozlowski, Council Member

RE: Lakeside Crematorium Application

To the Folsom City Manager and Folsom Council Members:

I am a resident in The Preserve neighborhood, which is directly across the street from Lakeside Memorial Lawn. I am writing this letter to request that you deny Caring Services Group's appeal to build a crematorium at Lakeside Memorial Lawn, and uphold the decision of the Historic District Commission that was reached on February 16, 2022, denying the Applicant's conditional use permit.

There are several details regarding the appellant's Initial Study and proposed Mitigated Negative Declaration (IS/MND) that do not comply with the California Environmental Quality Act (CEQA). Among the shortcomings, the IS/MND does not address key environmental safety and health issues that may significantly affect the surrounding community, despite that fact that there is substantial evidence indicating the potential for such significant effects. It would be very important to achieve a well-informed project review by preparing an environmental impact report (EIR) with all the relevant environmental topics where potential impacts could occur, sufficient substantial evidence to describe the nature and magnitude of potentially significant effects, and feasible alternatives and mitigation measures that could reduce or avoid potentially significant impacts.

A summary of the CEQA inadequacies is presented below:

1. Hazards and Hazardous Materials: The explosion risk and consequences in the event of explosion from two 250-gallon propane tanks are not analyzed. The potential for an explosion is dismissed as not likely without evaluation or evidence. Although there is a low probability of an accident, there would be high impact if it in fact occurred. As an example, the following story is from the website news feed of MTI Industries, a manufacturer of fire alarms, regarding the explosion of a 250-gallon propane tank in Prunedale, Monterey County.

Propane Tank Explodes in Prunedale, 2 Injured (Jul 28, 2011)

Prunedale, CA. Two people are injured after a propane tank exploded in Prunedale Thursday morning, said the North County Fire Department. The propane tank held 250

gallons of propane. The explosion happened around 11 am on the 500 block of Strawberry Road. Both of the victims were taken to the hospital with minor to moderate injuries. Fire officials say that there was a leak in the propane tank that ignited and caused the explosion. Witnesses say that they could feel the explosion all the way across the street. According to the fire department, the explosion sent a shock wave through the house that blew out the windows and shifted the walls of the house.

The concussion wave of explosions can be modeled to address how far damage would occur. The concussion may affect the homes in our neighborhood and travelers on Folsom Boulevard. Also, there is no analysis of exacerbation of wildfire risk if an explosion occurred. All these topics should be addressed in an EIR, because impacts may be significant. The analysis in the environmental study needs to be backed by evidence, analysis, and modeling. If a significant hazard is identified, feasible mitigation must be implemented.

2. Inaccurate/Unstable Project Description. The project description appears unstable and potentially flawed. For CEQA compliance, a project description must be accurate, and it needs to be stable during the course of environmental review.

The IS/MND says no family gathering will occur as a reason to not provide facilities for gathering. This premise is flawed, because it conflicts with the stated objective that the crematorium is designed to serve the customs of the families seeking cremation. Several cultures seek to have family members gather at the cremation site. Facilities would be needed to accommodate them (e.g., a turnaround for a hearse and dropping off family, parking including handicapped spaces, broad walkways to the crematory, seating on the grounds around the crematory).

Also, the IS/MND says no sewer is needed but does not comment on the sanitary needs of the employees working there. Where are comfort stations nearby for employees? Recognizing the likelihood of demand for family gathering, how will the sanitary needs of visitors be accommodated? This is a shortcoming of the project description that could overlook potentially significant environmental impacts related to sewer construction, such as increased tree removal.

The IS/MND states a fixed estimate of the rate of cremations (1 or 2 per day, 500 per year), which is misleading because it does not assume growth in demand. It is reasonable to conclude that the stated rate of cremations is likely underestimated in the future, based on the evidence that there are no local, nearby alternative crematories and the Folsom/eastern Sacramento County/southern Placer County region is projected to grow substantially. In fact, the document takes credit for the lack of nearby facilities through an estimate of reduced vehicle miles traveled to more distant sites, because the surrounding communities and funeral homes would logically use this closer facility. The document acknowledges growing demand with nearby populations over time, but does not account for the growth in demand in the environmental analysis by failing to provide future projections of cremations per day or per year based on that growing demand. Without such a demand-driven estimate, the analysis is short-sighted, inaccurate, and potentially well understated for 5 years, 10 years, or more, in the future.

A revised accurate, credible, and stable project description is needed to address these issues and provide adequate environmental review. With an accurate project description, environmental analyses will likely need to be revised and corrected.

3. Air Quality/Toxic Air Contaminants: If the demand is substantially higher than 500 cremations per year in the future, as questioned in the previous item, the air quality and toxic air contaminant estimates would also be underestimated. Higher emissions may result in significant health impacts, and it is important for neighbors to be able understand the nature and magnitude of potential health impacts. A more detailed study and non-technical explanation of potential health impacts, and if needed feasible mitigation, should be prepared and included in an EIR.

4. Noise: The analysis of noise impacts is without evidence substantiating the noise level generated by the crematory. There are no facts confirming "roughly" estimated noise generations, no evidence from other similar facilities, no document cited in the text, nor any description of noise reduction features. The document says the estimate is "rough", which means it appears to be qualitative, maybe even arbitrary. Noise measurements from other crematories would be easy and cost-effective to obtain as the basis for accurate noise modeling; such measurements are standard practice in CEQA noise studies. Recognizing the early (7:00 am) and late (10:00 pm) operation, the noise impacts during otherwise quiet times of day may be significant, especially single-event noise, or short-term noise during the hour or so of burning. Noise impacts, particularly during quieter times and reflecting growing demand, should be provided in an EIR with feasible mitigation for potentially significant noise effects.

These are the details regarding the appellant's Initial Study and proposed Mitigated Negative Declaration that I believe do not comply with the California Environmental Quality Act. Based on this noncompliance, I would ask that the Applicant's conditional use permit to install a crematorium be denied, upholding the Historic District Commission's vote on February 16.

Sincerely,

Salwa Kasabian
[REDACTED] Forrest Street
Folsom, CA 95630
[REDACTED]

Josh Kinkade

From: Elaine Andersen
Sent: Monday, February 28, 2022 7:44 AM
To: Josh Kinkade
Subject: FW: No Lakeside Memorial Crematorium

From: Sean Gates [REDACTED]
Sent: Sunday, February 27, 2022 3:26 PM
To: Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Elaine Andersen <eandersen@folsom.ca.us>
Subject: No Lakeside Memorial Crematorium

Some people who received this message don't often get email from [REDACTED] [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Council Members,

I live in the Lake Natoma Shores neighborhood with my wife and two young children. This is a wonderful neighborhood. Each house shows pride of ownership. I can look out my front windows and see slivers of Lake Natoma. We have neighborhood access to the trails and lake. The bald eagles fly over my house and perch at the Lakeside Cemetery.

Putting a crematory in the historic cemetery will change all this. I don't want to look out my back windows and see "puffs of smoke" per Igor's words. I want to enjoy the beauty of the cemetery out my back windows. Lake view out the front windows and smoke puffs out the back window. What a contradiction.

I don't want to have to close my windows while 90 minute cremations are happening. I don't want to tell my kids to come in the house and stop playing with the neighborhood kids because the crematory is running.

This effects our quality of life.

During the 1/11/22 city council meeting there was much concern for the preservation of the oak trees with the Toll Brothers at Folsom Ranch Phase 2 Subdivision Project. Ten minutes was spent discussing grading concerns, buffering, and the probability of survival of one oak tree. I hope the Council gives the history at the cemetery, Historic District, open space, and residents of The Preserves/Lake Natoma Shores community as much time, concern, and thoughtfulness as the oak trees received.

Thank you,

Sean Gates

Terry Sorensen
 [REDACTED] Forrest Street
 Folsom, CA 95630
 [REDACTED]

April 3, 2022

City Council
 City of Folsom
 50 Natoma Street
 Folsom, CA 95630

RE: Lakeside Memorial Lawn
 Crematorium Conditional Use Permit
 (PN-19-182)
 Hearing Date: April 26, 2022

Dear Councilpersons:

This letter is submitted in opposition to the request of applicant Igor Semenyuk (hereinafter, Applicant) for a conditional use permit (hereinafter, C.U.P.) allowing the operation of a crematorium on the premises of Lakeside Memorial Lawn, a historic cemetery located in the City. Numerous grounds have been raised in opposition to Applicant's request by various members of the community, but this letter will focus on only one ground; to wit: that Applicant's assertion that "the crematorium, as proposed, is an appropriate accessory use to the existing cemetery" is without merit and therefore cannot provide the basis for the issuance of the C.U.P. sought by Applicant. (emphasis added.)

Applicant's argument that the proposed crematorium should be allowed based solely on its "accessory use" status is set forth on pages 9-10 of the Historic District Commission Staff Report issued by the City. Basically, the argument advanced is that the existing cemetery is the principal (or primary) use of the property whereas "the proposed crematorium would be operating as an accessory use to the existing cemetery, not as a stand-alone business." (emphasis added.)

In support of this argument, the Staff Report relies on Section 4.46 of California Land Use Practice (2021) "Primary and Accessory Uses" by Adam U. Lindgren & Steven T. Mattas which reads as follows: "A primary or principal use is the main use to which the premises are devoted and the primary purpose for which the premises exist. Primary uses may be permitted by right or may be conditional uses subject to a CUP. Accessory uses are structures or activities that are subordinate in area, extent, and purpose to the primary use; contribute to the comfort, convenience, or necessity of the principal use; and are located on the same lot and the same zoning district as the principal use.... By definition, an accessory use must be associated with a principal use and cannot be established on a property without a principal use." (emphasis added.)

Obviously, reliance on Section 4.46 in support of the argument advanced in the Staff Report is misplaced in the fact setting a hand, and fatally so. Clearly, the proposed crematory operation could be established and function fully on a non-cemetery property, its existence and operation not in any way dependent on an underlying cemetery business. This is made clear by the City's finding on page 22 of the Staff Report that, of the 16 crematoriums currently operating in Sacramento County, only five are located within cemeteries.

Accordingly, and to quote the Lindgren & Mattas work relied on in the Staff Report, the operation of a crematorium on the cemetery property in question would not, "by definition," constitute an accessory use. To the contrary it would be operating as a stand-alone business. As such, Applicant's attempt to piggy-back its way to C.U.P. status by way of the proposed crematorium's "accessory use" status must be rejected, and the request for a C.U.P. denied.

Respectfully submitted,

/s/

Terry L. Sorensen
dg/TS

email: City Councilpersons and City of Folsom Staff
kerri@atlanticcorrosionengineers.com; rrodriguez@folsom.ca.us; saquino@folsom.ca.us;
ykc@folsom.ca.us; mkozlowski@folsom.ca.us; kmullett@folsom.ca.us; jkinkade@folsom.ca.us;
sbanks@folsom.ca.us; sjohnson@folsom.ca.us; pjohnson@folsom.ca.us

Josh Kinkade

From: Nicole Gates [REDACTED]
Sent: Sunday, April 17, 2022 5:43 AM
To: Elaine Andersen; Sarah Aquino; kerri@atlanticcorrosionengineers.com; Mike Kozlowski; YK Chalamcherla; Rosario Rodriguez; Josh Kinkade
Subject: No crematorium

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Council Members,

I have sent previous emails but none have touched on the emotional toll this has taken on me and my family. This proposed crematorium is effecting our sleep and mental health. I used to be a sound sleeper, but recently I am such an insomniac over thoughts and nightmares of the crematorium coming in. I write and research when I should be sleeping. I've been getting migraines for the first time in my life. I realize this sounds dramatic, but it's very true. This is the definition of stress.

I am so proud of my neighbors and family who are fighting so hard against this. I am not trained and educated to fight a city and business. I am, however, a mother, wife, daughter, sister and auntie to other families that live in the Preserves. I want my children, niece, and future niece/nephew living in a neighborhood where propane trucks aren't constantly rattling in. I don't want them knowing that when they see a puff of smoke or smell the smell of cremation they need to stop playing basketball or riding their bikes to come home and close the windows. I tried to shield my young kids about what we were fighting against in the neighborhood. This didn't last long with all the protest in the neighborhood. This has effected our quality of life already!

I drive around with "No Crematorium" magnets on my car. Everyday I take them off when I'm picking my kids up from school. I don't want to traumatize other kids the same way mine are being traumatized. They shouldn't have to know at an elementary school what a crematorium is.

I love the peaceful cemetery with animals. I have nothing against cemeteries or cremation, but this is not the right location for it. The cemetery was here before the neighborhood, but the neighborhood was here before the proposed crematorium.

Should this crematorium proposal pass, I will call to report every time there is odor. I will call every time there's smoke. I will call if there is after hours burning. I will call if ANYTHING from Lakeside disrupts the neighborhood.

I would like to add that I personally collected numerous signatures on the petition from people inside the Lake Natoma Shores neighborhood who use the trail to Lake Natoma that's located on Young Wo Circle. It's not just the people in this neighborhood who don't want the crematorium here, it's people from the greater Folsom area that use the neighborhood for recreation. Please listen the the public outcry!

Folsom's City Council motto, " The Folsom City Council is dedicated to ensuring Folsom's high standards for public health, safety, and quality of life."

I will protect my children's quality of life. Please help me protect it. I hope the council keeps their motto in mind during their vote.

Thank you,

Nicole Gates

Attachment 7

Public Comment Letters Received After Publication of the April 26, 2022 Staff Report

Peter Lucyga
[REDACTED] Young Wo Circle
Folsom, CA 95630
[REDACTED]

April 27, 2022

Mayor Kerri Howell
Vice-Mayor Rosario Rodriguez
Sarah Aquino
YK Chalamcherla
Mike Kozlowski
Christa Freemantle, City Clerk
City Council
50 Natoma Street
Folsom, CA 95630

Via E-mail

Pam Johns, Community
Director
Josh Kinkade, Associate Planner
Community Development
City of Folsom
50 Natoma Street
Folsom, CA 95630

**RE: UPDATE FROM COUNCIL MEETING OF APRIL 26, 2022 – LAKESIDE MEMORIAL LAWN
CREMATORIUM (PN 19-182)**

**Dear Mayor Howell, Vice-Mayor Rodriguez, Councilmembers Aquino, Chalamcherla and Kozlowski,
Director Johns, Ms. Freemantle, and Mr. Kinkade,**

I really appreciated the opportunity to provide citizen input at yesterday's Council Meeting, it was my first time in the chambers. I was the speaker who suggested that the city consider providing an incentive (tax or otherwise) to facilitate the siting of a crematorium at an alternate location further away from residential and especially the Historic District.

I have two additional insights after this meeting which I'd like to share with all.

#1 – Why the focus on needing a crematorium with Folsom city limits? The city and residents of Folsom leverage numerous other key services from Sacramento County instead of having it provided by the city itself or by businesses within city limits. A few examples:

- **Weekly residential and commercial trash disposal** – Folsom doesn't insist on it's own landfill, we use Kiefer landfill for this important **service function**. I know because I've had occasion to use Kiefer and always seem to see Folsom "Distinctive by Nature" painted dump trucks there.
- **Electric utility** - Folsom doesn't insist on having its own electric utility, it uses SMUD for this important **service function**.
- **Folsom Police Department** - Folsom doesn't insist on having its own "Police Academy" to train new police officers, it relies on other larger police departments like the City of Sacramento to

conduct Police Academy training for this important service function. I know this because a relative is attending the Sacramento Police Academy next month.

#2 – Cremation as a Service function. I took a fresh look at the funeral process to connect the dots a bit better, at least for myself. The base components are:

- **Funeral home – a service function** not usually located on cemetery property. It's a business housed in a business-zoned area that provides families with body preparation and funeral services such as viewing.
- **Cemetery – a physical plot of land** for the express purpose of burying or housing the remains of the deceased. Traditionally cemeteries have cultural or religious significance and are considered sacred by many people, not to mention having historic significance as in this case. *There is a service ceremony with burial but primarily it's a sacred resting place for the deceased.*
- **Crematory – a service function** usually not located on cemetery sites themselves. It's a business housed in a business-zoned area. As explained by Mr. Semenyuk, the cremation process is typically done by an attendant and can be facilitated by a live video feed if desired by the relatives or friends of the deceased. This provides great flexibility on location, since a crematory is basically a giant industrial oven.

In reflecting on the wishes of my own parents before they passed, both asked to be buried in family plots. I have relatives who chose cremation and not one stated *"I wish my cremation process takes place in Auburn, Folsom, San Diego"*. What you do hear is that *"Please scatter my ashes in the Pacific Ocean, at Lake Tahoe, the mountains, or keep them at home with family"*.

My main point is that the physical cremation facility is really suited to a flexible location as a service function, even more so than a funeral home. Folsom is within Sacramento County and we leverage numerous services from the county as stated above. Why struggle to find a suitable Folsom location of greater than 500 feet from residential areas when Sacramento County offers locations with 15-20 minutes' drive that easily exceed the 500 or even 5,000 feet boundary? I see a great business opportunity for a Sacramento County sited location that can provide high volume services to multiple cities throughout the Sacramento area.

As a minor point with the stated quantity of possible cremations, it was claimed that a maximum of 4 per day could take place daily with a 500 annual limit. Given there are 365 days in the year and regardless if cremation days are 5,6 or 7 days a week, the math always exceeds 500 annually.

Please consider my additional insights and do not allow crematory operations to take place in our historic area cemetery.

Sincerely,


Peter Lucyga

Terry L. Sorensen
 [REDACTED] Forrest Street
 Folsom, CA 95630

May 02, 2022

City Council
 City of Folsom
 50 Natoma Street
 Folsom, CA 95630

RE: Lakeside memorial Lawn
 Crematorium Conditional Use Permit
 (PN-19-182)
 Hearing Date: May 10, 2022

Dear Councilpersons:

Please accept this correspondence as my attempt to summarize the written submissions that have been made to date by concerned citizens on the "loss in property value" issue that has been raised in this matter.

The issue is addressed forcefully in a 4-20-22 email to the City from Victoria Foster, a resident of The Preserve and a realtor with Interø Real Estate Services (page 9 of 4-26-22 Additional Information Transmittal). She has been a realtor for over 30 years and works full-time at Interø. She asserts that "being next to a crematorium will absolutely affect our property values.... Some homeowners stated they will feel compelled to move. This absolutely will affect our home values and eliminate a large number of buyers not only for health concerns, but yes, the creep factor." She further opines that all of this will cause a loss of home values of "tens of thousands of dollars and possibly even more...compared to other areas in Folsom because we have a crematorium dropped in our neighborhood."

Ms. Foster states that the California Association of Realtors requires that a seller of real property fill out a Sellers Property Questionnaire which constitutes a mandatory disclosure document that must be included in every sales transaction. She points out seven difference paragraphs on page 4 thereof which would require the seller to make disclosure of the existence of a crematorium in the area. In her words: "It's not something to be taken lightly." She adds that "No buyer coming to Folsom has ever asked me, 'how is your crematorium her?'" They come...and they stay for the quality of life – not death."

Nearly identical in tone and substance is an email dated 9-13-21 from RE/MAX Gold real estate agent Barbara Krieger (Staff Report Part 1 of 2, page 330). She recounts her inability to sell a property that she recently listed "in the beautiful Historical District of Lake Natoma Shores because of buyer concerns about the crematorium going in." The third paragraph of her email merits a verbatim quotation:

"I am not a Folsom resident, but am writing this letter out of concern for the neighborhood. Having personally seen every single buyer that walked through this beautiful home get turned off from the highly desirable location because of an undesirable crematorium should concern every single representative of the people, the nearby homes and the neighborhood itself as an entity. I wonder why the City has allowed the cemetery owner to get this far in the planning phase of the project, despite the multiple and loud cries from the Folsom residents and groups, who should not

have to trouble themselves with such a threat while living in such a popular, sophisticated and historically protected area. I am absolutely astonished this is occurring at all.”

Another “decrease in property value” comment is found at pages 456-7 of the Staff Report, Part 1 of 2, a 7-20-21 email from Preserve resident Kim Higgins. On the second page of that email in the next-to-the last paragraph, she references an article by Mark Agree dated June 19, 2008 from “Applied Economics Magazine” that analyzed 27 months of home sale data (7 months before and 20 months after the startup of crematory operations) in which it was concluded “that proximity measured in terms of direction and distance from the crematory, imparts a statistically significant negative impact on average home sale prices,” the prices increasing the further the home was from the crematory.

Ms. Higgins concludes her email with these comments: “California disclosure law requires us to disclose. Not many people desire to live near one. Do you?...Our quality of life will be affected if the crematorium goes forward. Honestly, would you want this in your backyard?”

At pages 144-5 of the Staff Report, Part 2 of 2, we find another email from realtor Victoria Foster who identifies herself as a resident of the Preserve living on Young Wo Circle. She voices numerous objections to the proposed crematory. Finally she focuses on decreased property values on the second page of her email, as follows:

“Lastly, I am a local Realtor and it will DECREASE OUR HOME VALUES in the Preserves/Lake Lake Natoma Shores if a crematorium is built...I have 3 clients that want to buy in this neighborhood – two of them will not reside here if a crematorium is built and my 3rd client Liz Chighizola has stated to me, she would want the crematorium to be put in because home prices would then drop here and then she would finally be able to afford this neighborhood. Really?? How is it OK for this business to decrease our home values??”

Last, I would invite the attention of the members of the Council to an email dated March 30, 2022, from Patrick Nooren of El Dorado Hills (Staff Report, Part 2 of 2, page 618). I would classify this email as a “cross-over” between the issue of property values and the issue of health concerns raised by the proposed crematory. In any event, I found the email heart-wrenching, dealing as it does with the proposed crematory crushing the hopes of Mr. Nooren to obtain the “perfect” home on Fong Street for his disabled daughter (wheelchair-bound with respiratory issues and compromised in a number of other ways due to PKS). As Mr. Nooren puts it: “Should this vote approve the crematorium, I will, unfortunately, not be able to continue with the purchase of this, the perfect home. I simply cannot risk the elevated emissions and the potential for a negative impact on my extremely vulnerable daughter.”

In closing, I would like to raise a personal concern/irritation in regard to the manner in which many of the issues on the crematory question have been framed. To put it bluntly, I am sick and tired of hearing it implied (if not flat-out asserted) that the residents of the Preserve are motivated by a selfish, “not-in-my-backyard” (NIMBY) attitude, using that attitude to deprive the Applicant of his property rights. Nothing could be further from the truth.

No one is claiming that the Cemetery does not have the right to exist and do business as a cemetery. The Cemetery has preexisted the Preserve residential development by many, many years. However, that certainly is not the case with the crematory. With the crematory, the situation is reversed with the Preserve preexisting even a claim by the Applicant of any right to establish a crematory on the Cemetery property. To put the issue in “territorial” terms, the Applicant is the “intruder” on the crematory issue, not the residents of the Preserve. It is the Applicant who is taking the role of the

aggressor here, not the residents. The residents are OK with the Applicant pursuing his cemetery business, but not OK with the Applicant opening a new business enterprise (i.e., a crematory) that interferes with and diminishes the preexisting private property rights of his neighbors.

Respectfully submitted,

/s/

Terry L. Sorensen

email: City Councilpersons and City of Folsom Staff

kerri@atlanticcorrosionengineers.com; rrodriguez@folsom.ca.us; saquino@folsom.ca.us;
ykc@folsom.ca.us; mkozlowski@folsom.ca.us; kmullett@folsom.ca.us; jkinkade@folsom.ca.us;
sbanks@folsom.ca.us; sjohnson@folsom.ca.us; pjohns@folsom.ca.us

From: [REDACTED]
To: kerri@atlanticcorrosionengineers.com; [Rosario Rodriguez](#); [Sarah Aquino](#); [YK Chalamcherla](#); [Mike Kozlowski](#); [Kelly Mullett](#); [Josh Kinkade](#); [Steven Banks](#); [Scott Johnson](#); [Pam Johns](#)
Cc: [REDACTED]
Subject: Lakeside Cemetery Project RE: Chung Wah Cemetery National Register of Historic Places
Date: Wednesday, April 27, 2022 3:04:56 PM
Attachments: [ChungWahCemetery_Natnl_RegHistPl.pdf](#)
[Historic Preservation Master Plan plus Cultural Resources Inventory](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Council Members and Staff,

There was a brief discussion at last night's City Council Meeting regarding the significance of the Chung Wah Cemetery and its National Register of Historic Places status. Attached please find the national application finalized for Folsom's Chung Wah Cemetery to the National Register of Historic Places on March 21, 1995. This application was submitted by Mary L. Maniery, Historian and Cindy Baker, Historian. The national importance of this Chinese Cemetery to the nation's founding, contains valuable information about the Chinese pioneers and events they participated in critical to the founding of the American West, the City Folsom, and the State of California. The Chung Wah is of national importance.

The authentication of the Chung Wah Chinese Cemetery was archived at:

- the Folsom Historical Society and History Museum in Folsom,
- the Chinese Historical Society of America in San Francisco,
- the Sacramento County Archives and Museum Collection Center in Sacramento,
- and the California State Historic Preservation Office.

The Chung Wah Cemetery, the Young Wo Cemetery, along with all eight of the small Lakeside Cemeteries, together, were included in the City of Folsom's Historic Preservation Master Plan and Cultural Resources Inventory List, adopted by the City of Folsom on November 5, 1998. The authentication of these cultural resources and sites on the Cultural Resources List was archived at:

- The City of Folsom;
- The Folsom Historical Society and History Museum;
- referenced in Folsom Zoning Code 17-57 Historic District;
- and referenced in Folsom's Historic District Design and Development Guidelines.

How could ECorp's Cultural Resources report, contained in the 2022 Lakeside Memorial Lawn Crematorium Project, have missed all of these publicly available environmental documents in its Cultural Resources report for inclusion in the Initial Study and the C.U.P. Mitigated negative Declaration?

Indeed, how could the Community Services Director have missed cultural significance of these environmental documents for inclusion in the Staff Report's Initial Study and Mitigated Negative Declaration?

This information is critical to the Findings that the City Council will make on the Lakeside

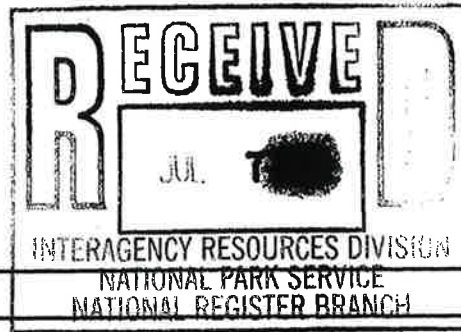
Memorial Lawn Crematorium Project proposal and C.U.P.

Sincerely,

Deborah Grassl

05/10/2022 Item No. 18.

United States Department of the Interior
National Park Service



**NATIONAL REGISTER OF HISTORIC PLACES
REGISTRATION FORM**

1. Name of Property

historic name: Chung Wah Cemetery
other name/site number: Sze Yup/Sam Yup Cemetery

2. Location

street & number: Mormon Street vicinity not for publication
city/town: Folsom vicinity
state: California code: CA county: Sacramento code: 067 zip code: 95

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act of 1986, as amended, I hereby certify that this nomination request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60. In my opinion, the property meets does not meet the National Register-criteria. I recommend that this property be considered significant nationally statewide locally. (see continuation sheet for additional comments.)

[Signature] 6/6/95
Signature of certifying official Date

California Office of Historic Preservation
State or Federal agency and bureau

In my opinion, the property meets does not meet the National Register criteria. (See continuation sheet.)

Signature of commenting or other official Date

State or Federal agency and bureau

4. National Park Service Certification

I hereby certify that this property is:

- entered in the National Register
 See continuation sheet.
- determined eligible for the National Register
 See continuation sheet.
- determined not eligible for National Register
- removed from the National Register
- other (explain): _____

[Signature] 3/21/95
Signature of Keeper of Action Date

5. Classification

05/10/2022 Item No. 18.

Ownership of Property
(check as many boxes as may apply)

- private
- public-local
- public-State
- public-Federal

Category of Property
(check only one box)

- building(s)
- district
- site
- structure
- object

Number of Resources within Property
Contributing Noncontributing

_____	_____	buildings
1	_____	sites
_____	_____	structures
_____	_____	objects
1	0	Total

Number of related multiple property listing
(Enter "N/A" if property is not part of a multiple property listing)

N/A

Number of contributing resources previously listed in
the National Register

None

6. Function or Use

Historic Functions
(Enter categories from instructions)

FUNERARY/cemetery
FUNERARY/graves/burials

Current Functions
(Enter categories from instructions)

FUNERARY/cemetery

7. Description

Architectural Classification
(Enter categories from instructions)

OTHER/shrine
OTHER/vault

Materials
(Enter categories from instructions)

foundation CONCRETE, BRICK
roof
walls BRICK, STONE/cobbles
other

Narrative Description

(Describe the historic and current condition of the property on one or more continuation sheets.)

Applicable National Register Criteria

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing.)

- A** Property is associated with events that have made a significant contribution to the broad patterns of our history.
- B** Property is associated with the lives of persons significant in our past.
- C** Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- D** Property has yielded, or is likely to yield, information important in prehistory or history.

Criteria Considerations

(Mark "x" in all the boxes that apply)

- A** owned by a religious institution or used for religious purposes.
- B** removed from its original location.
- C** a birthplace or grave.
- D** a cemetery.
- E** a reconstructed building, object, or structure.
- F** a commemorative property.
- G** less than 50 years of age or achieved significance within the past 50 years.

Areas of Significance

(Enter categories from instructions)

ETHNIC HERITAGE/Asian

RELIGION

Period of Significance

1906 - 1946

Significant Dates

Significant Person

(Complete if Criterion B is marked above)

Cultural Affiliation

Architect/Builder

Narrative Statement of Significance

(Explain the significance of the property on one or more continuation sheets.)

9. Major Bibliographical References

05/10/2022 Item No. 18.

Bibliography

(Cite the books, articles, and other sources used in preparing this form on one or more continuation sheets.)

Previous documentation on file (NPS):

- preliminary determination of individual listing (36 CFR 67) has been requested
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey # _____
- recorded by Historic American Engineering Record # _____

Primary Location of Additional Data:

- State historic preservation office
- Other state agency
- Federal agency
- Local government
- University
- Other

Name of Repository:

Sacramento Archives and Museum Collection Center
Chinese Historical Society, San Francisco,
Folsom Historical Society, Folsom

10. Geographical Data

Acreeage of Property 2.616 acres

UTM References

(Place additional UTM references on a continuation sheet.)

	Zone	Easting	Northing	Zone	Easting	Northing
1	10	657000	4281540	3		
2				4		

See continuation sheet

Verbal Boundary Description

(Describe the boundaries of the property on a continuation sheet.)

Boundary Justification

(Explain why the boundaries were selected on a continuation sheet.)

11. Form Prepared By

Name/Title: Mary L. Maniery, Historian/Cindy Baker, Historian

Organization: PAR ENVIRONMENTAL SERVICES, INC./City of Folsom Date: February 21, 1995

Street & Number: P.O. Box 160756 Telephone: (916)739-8356

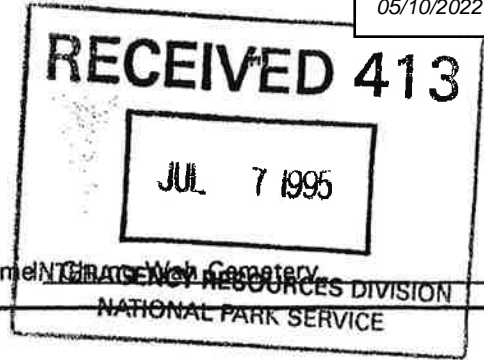
City or Town: Sacramento State: CA ZIP: 95816

United States Department of the Interior
National Park Service

**NATIONAL REGISTER OF HISTORIC PLACES
CONTINUATION SHEET**

Section number 7 Page # 1

Property Name Chung Wah Cemetery
AGENCY RESOURCES DIVISION
NATIONAL PARK SERVICE



SUMMARY

Folsom's Chung Wah cemetery encompasses 2.616 acres of land south and west of Block 70 and on parts of Lots 7, 8, 9, and 10. Located on a bluff near today's Lake Natoma, the cemetery is surrounded by dredge tailings, native oaks, and mature grey pines. Its features include burial mounds, exhumation depressions, brick-lined vaults, a burning pit, and the remains of a shrine. Although subject to vandalism in the 1960s, this site retains a high level of integrity. Its physical separation from the main Folsom cemetery, haphazard arrangement of burial mounds and pits, lack of formal landscaping, and rural setting increases the integrity of setting, feeling, and association that cloaks the site.

DESCRIPTION

The town of Folsom, located in eastern Sacramento County, once housed a flourishing Chinese community. Two cemeteries were associated with the community and were owned and maintained by different associations. The Yeong Wo Association's plot currently is landscaped and partially obscured by a building and retains no surface reminders of its use as a burial ground for members of the Yeong Wo association. The Chung Wah cemetery, used by members of the Sze Yup and Sam Yup associations, is the larger and probably older of the two cemeteries and retains numerous features and burials.

The present 2.616-acre site includes parts of Lots 7, 8, 9, and 10 of Block 70, then extends south and west to the Bureau of Reclamation property line. The larger portion of this cemetery lies outside the original 1855 town limits depicted on Theodore Judah's plat map for the City of Folsom (see Maps 1 and 2). The Chung Wah cemetery is near Folsom's non-Asian cemetery, which dates to the 1850s. Chinese burials in the motherlode region were typically located in segregated sections of non-Asian cemeteries. The Chung Wah is close to the other cemetery, but is a separate, remote site.

The site's irregular shape suggests it was haphazardly planned, perhaps beginning with a few Chinese burials and then spreading out as more Chinese died in Folsom over the years. The lack of historical photographs and maps for the cemetery precludes an accurate description of its early physical appearance. Oral testimony presents an image of the cemetery in the 1920s to the 1940s, allowing for a reconstruction of the cemetery's physical appearance. Physically, the site was located on the outskirts of town. Dredging occurred around the perimeters of the site in 1907 and 1908 (Map 3), but the cemetery plot was left undisturbed. Throughout its period of use it nestled on the bluff near the American River, surrounded by dredge tailings or steep escarpments leading to the American River.

The Chung Wah plot was accessed from the north by passing through a gate. A wagon road led east, then south and west, traversing the perimeter of the cemetery (Map 4). The road exited out the southwest side of the cemetery, allowing mourners to enter at the gate, travel around the cemetery, and leave without backtracking or crossing over their entry path. Graves were dug wherever there was room, with no specific orientation or layout. People were buried in shallow graves about two feet deep and were covered with a mound of earth. The earthen mounds were often touching or at angles to each other and protruded about two to three feet above the ground surface. Occasionally wooden markers or inscribed bricks marked the surface of graves. More often than not, however, graves were unmarked. As one Chinese-American related, it was better to be "unmarked and undisturbed" (G. Chan, Jr. 1994). While the deceased were often not identified by surface grave markers, other markers of bricks, stone, or wood inscribed with name, date, association affiliation, and ancestral village or province were placed in the graves to assist in identification of remains during exhumation.

United States Department of the Interior
National Park Service

NATIONAL REGISTER OF HISTORIC PLACES CONTINUATION SHEET

Section number 7 Page # 2

Property Name Chung Wah Cemetery

Scattered in and around the mounds were depressions from the exhumation of bodies. While bodies were carefully exhumed, the pits were not backfilled (and could have been used for future burials) and the pits were interspersed with the mounds. While the majority of exhumed remains were sent to China for reburial, others were placed in urns and reburied at the Chung Wah cemetery. Graves were not well-tended, aside from yearly ancestral ceremonies. The Chung Wah cemetery was not landscaped with lawns, benches, domestic trees or shrubs, or other landscaping elements typical in a non-Chinese burial site. Instead, the cemetery was left in a natural state and its appearance matched that of the surrounding vacant fields, seasonally covered with grasses with occasional scattered native oaks or grey pines. The road was sometimes kept clear by mowing, but the mounds were often overgrown.

Apart from the mounds and pits several burials were placed in brick-lined, rectangular-shaped vaults that were laid into the ground. A large depression was dug just inside and to the south of the entry gate and served as a burning pit where the belongings of the deceased were set afire. A rectangular brick, cobble, and concrete shrine, used during the *Ch'ing Ming* ceremony, was in place on the west side of the cemetery by 1940 and was likely constructed by the 1920s or before. This shrine stood approximately four feet high, and was eight feet wide and 13 feet long.

By the 1930s, the Chung Wah cemetery was still in use, although declining, as only a few families remained in the area to tend to the graves of their deceased relatives and friends. In the early 1940s, brick markers still remained on some of the grave mounds, and some lay scattered around. In addition, the burning pit and shrine were still in use into the 1960s.

In 1967 vandals desecrated the cemetery. The graves of Chin Oak, his wife, Ping Woo Choy, and Chin Lai Shee were uncovered and their coffins smashed. The Chan family discovered bones left scattered on the ground (H. Chan, Sr. 1981). The vandals also destroyed the shrine, believing it to be the grave of a wealthy person (*Sacramento Bee* n.d.). A deep hole extending below the shrine foundation attests to this vandalism. Jewelry stolen from the graves was sold and was later traced to San Francisco pawn shops. Those responsible for the desecration were only charged with public health laws concerning unlawful digging in a cemetery (G. Chan, Jr. 1991). The Chans reburied their family's remains on site and covered the graves with a concrete slab. A brass grave marker was set flush into the concrete at this time and is the only marker currently visible at the cemetery.

Today the cemetery retains much of its original appearance (see Map 4). The wagon road, while overgrown, can still be traced around the perimeter of the site. The cemetery contains between 75 and 100 mounds, many barely discernible due to settling. The mounds vary from only a few inches to about two feet in height. Their location is haphazard, with many perpendicular to each other and arranged in no set pattern. Exhumation pits, ranging in depth from six inches to several feet, are interspersed among the mounds. The entire burial site is overgrown with tall grasses, thistles, and encroaching vegetation and appears hummocky. This untended appearance, however, is in keeping with the historical appearance of the site as related by numerous people in Folsom. The graves of Chin Oak and his kin are clearly marked, covered by a large concrete slab with a flat brass monument.

The burning pit is evident inside the gate, although immature oaks and vegetation are growing inside it. An ash lens attests to its past use. While the shrine was dismantled by the vandals, its foundation and part of its cobble and brick walls are still intact (Map 5). One rectangular brick-lined vault, now empty, is exposed and evident along the west edge of the fence line. The vault is four feet wide, eight feet long, and about four feet deep. Four courses of brick are present around the top of the vault. In addition to its physical features, the site remains secluded, tucked between dredger tailings and an undeveloped area along Lake Natoma and physically separated from the Folsom cemetery by a vacant lot. Its seclusion, hummocky appearance, and intact features all contribute to the sense of peace and timelessness that pervades the site, adding to the integrity of feeling and association so apparent at the cemetery.

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SUMMARY

The Chung Wah cemetery in Folsom is unusual for its strong association ties and ownership, for its continued existence in an area that has been extensively mined, and for the intact associated features. Folsom's Chinese community once contained the homes, businesses, and gardens of hundreds of people. Today, only this cemetery remains as the last visual reminder of a rich heritage and a viable Chinese population that once flourished in town. Acquired in 1906, the cemetery also represents strong district association relationships that were maintained in Folsom well into the twentieth century. It has numerous features associated with historical use, including burial mounds and vaults, exhumation depressions, a burning pit, and a shrine foundation, all attesting to the overall physical integrity of the cemetery and reflecting the spiritual beliefs and practices of the association members. The sense of time and place evident when standing in the cemetery grounds is strong. The historical and ethnic associations of the cemetery, combined with its high level of physical integrity, contribute to its importance under Criterion A, Consideration D and G. The last burial occurred in 1946, ending a 40-year period of use and representing the decline of the Chinese community in Folsom.

HISTORICAL CONTEXT

Folsom's Chinese Heritage

Chinese men and women have lived in Folsom since the California gold rush. Arriving as miners, laborers and merchants, their numbers gradually climbed through the 1850s and 1860s, sharply rose during the 1870s and peaked in the 1880s. Exclusionary immigration laws, dwindling mining and labor opportunities, and racial tension precipitated a steep population decline during the 1890s and 1900s. By the early twentieth century, only a handful of the original Chinese community remained. Today the Chan family is the last remaining fragment of what was once a large and flourishing community.

Gold Rush Era

The start of large-scale Chinese immigration to California dates to the early months of the gold rush. News of the discovery of gold drew thousands of Chinese to California from throughout the Pearl River delta in South China, particularly the City of Canton. Given the political unrest, worsening economic conditions, and the repressive Manchu rule in China in the mid-nineteenth century, reports of the fabulous *Gum Shan* (Mountain of Gold) became even more appealing, resulting in a mass immigration to California, primarily by people from the Kwangtung Province (cf. Chinn 1969; Chiu 1967; Hoexter 1976; Lai and Choy 1972; Sung 1967). Immigration records from the Customs House in San Francisco attest to this migration: 325 immigrants recorded in 1849, 450 in 1850, and 2,716 in 1851. This number jumped to 20,000 in 1852 (Chiu 1967:12; Hoexter 1976:10; Lai and Choy 1972:45).

Some scholars estimate that one in ten newly arriving Chinese remained in San Francisco, while the others headed out to either the northern or southern mining regions (Chinn 1969; Chiu 1967; Williams 1930). In the first few years of the gold rush, mining in the northern region focused on the American River because of its proximity to Sacramento (Williams 1930). It is probable that by late 1849 Chinese arrived at Negro Bar, predecessor to the town of Folsom, where they either stopped to mine or continued up the river.

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Typically, a Chinese merchant would set up shop in a central location in a mining region to sell food, supplies, and clothing to both his countrymen and Anglo miners. Often the store owner would start serving tea to his kinsmen in the back of the store and, if interest was high, might expand into a restaurant business, selling Chinese delicacies as well as beef steaks to American miners (Hoexter 1976:61-62). If business was steady and profitable, a back room might have housed a few fan-tan or domino tables, or other diversions. Sometimes, a woman might be brought in to entertain the miners. As one merchant thrived, others might join him and a Chinatown would grow. In larger mining towns, one or more temples would be erected by local Chinese leaders or benevolent associations to allow immigrants to offer prayers to the gods (Hoexter 1976:62).

From such humble beginnings a Chinatown would emerge, as was probably the case at Folsom. While there is some indication that at least one Chinese merchant was operating at Negro Bar in 1852, the location of this center is uncertain (J. Chan 1992; *The Telegraph* 1966). It is probable that the Chinese center of Negro Bar in 1852 was situated upstream and separated from the Anglo camp, at or near the area known as Chinatown today. The first evidence of a Chinese presence centered in Folsom is found in the first assessment plat of the newly formed town of Folsom, completed in 1855. According to the 1855 Folsom Map Book, Chinese owned lots or owned improvements on lots on Blocks 8, 9, 10, and 19. In addition, they had a church located in Block 11 (Sacramento County 1855).

In the late 1850s, as the initial gold rush ended and claims were abandoned, many Chinese miners arrived in the region to rework old claims or work as laborers for the Natoma Company. While usually residing at their mining claims, these men contributed to the growth of the local Chinatown, frequenting it on their day of rest. The local benevolent association hall (You See Tong) probably served as a social center for the men, providing mail from home, news of friends, gambling diversions, and tea or food. Barbers and doctors took care of personal hygiene and health needs, while the many stores offered a variety of supplies (United States Bureau of the Census 1860).

A Flourishing Community - 1860 to 1900

Folsom's Chinatown continued to expand rapidly in the 1860s, fed in part by miners retreating to the town to escape escalating racial violence. Trouble continued in the region in the 1860s, but Chinese miners found other ways to earn a living. While independent miners and companies were driven off their claims and harassed, hundreds of others were being employed to work on Anglo-owned claims, particularly in hydraulic and ground sluicing operations. The Natoma Company also hired Asian laborers to work their property, especially after 1864 when they acquired legal title to the eastern half of the historic Rancho de los Americanos (Castaneda et al. 1984:83).

As gold deposits dwindled and Anglo miners left the region, independent Chinese miners and companies once again began working old tailings and claims and the population increased in number. This general pattern is evident in the census records. In 1860, 94 miners were enumerated within the Chinese district of Folsom. In 1870 this number increased to 187. In addition, at least 370 miners were living in camps outside Folsom, but likely visited town occasionally to obtain supplies (United States Bureau of the Census 1860, 1870). By 1879 some 3,000 Chinese were said to be mining in the Folsom area (Askin et al. 1980:11; Plimpton n.d.). It is probable that many of these miners lived at least seasonally in Folsom and used it as a residential base.

During the 1880s, Chinatown continued to grow. This growth was related to anti-Chinese sentiment throughout the region in the late 1870s. As racial violence forced miners and laborers out of the countryside and surrounding communities, they often retreated to Folsom. The San Francisco *Alta* reported trouble in the region in 1878, noting that

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Chinese expelled from Rocklin, Roseville, and Penryn had gone to Folsom "where a camp of several hundred strong has been formed" (*Alta* 20 September 1878; Bancroft n.d.:37). This pattern continued for years.

In the midst of this period of economic prosperity and peak population, two Chinese associations purchased land for Chinese cemeteries. Yeong Wo & Company purchased Lots 3 through 8 of Block 40 from James and Mary Harris for their cemetery in 1883 (Sacramento County 1883). Two years later, Chung Wah Company purchased Lots 6 through 11 of Block 71 from James S. Meredith for \$190 (Sacramento County 1885b).

The Chinese living in town who did not own stores typically formed small companies to mine claims, or went to work for others. The Natoma Company continued to be the major source of employment during the 1880s. The company hired Chinese as employees and paid them \$1.00 a day, or rented land to them for mining. Company records note several mining companies buying water from the Natoma Company in 1882 (Plimpton n.d.). As employees, Chinese worked building, maintaining and clearing water ditches, as well as on company agricultural operations.

While manuscript census records are not available for 1890, Folsom's Chinatown appears to have continued to flourish during the late 1880s and 1890s, despite tepid local efforts to eliminate the Chinese from the city. In fact, Chinese advertisements in the local paper indicate a growing interaction with the non-Asian community. Three companies placed business ads in the *Folsom Telegraph* beginning in 1889: Fong Lee Lung, whose store carried groceries and miners' supplies; Cop Kee, a grocer; and Wing Sing Wah, a grocery store owned by merchant Chin Oak.

In 1893 an undetermined epidemic killed many Chinese in Folsom. The local paper reported that "Chinatown is full of sick heathens . . . and the number of deaths is greater than ever before known in history" (*Folsom Telegraph* 4 November 1893). Perhaps this epidemic, striking near the peak of Chinatown's population, marked the beginning of the community's gradual decline. The decline was also aided by the development of dredging in the region, an activity that effectively ended mining by small, independent companies around Folsom (Askin et al. 1980:13).

Decline - 1900 to 1946

As a new century dawned, Chinatown was holding its own. The 1900 census records depict a decrease in population within Chinatown since 1880, but only by 30 people. The district still had 13 merchants or grocers (three less than 1880), shoemakers, barbers, clerks, gardeners, cooks, butchers, launderers, restaurant owners, and gamblers (United States Bureau of the Census 1900). The main difference in 20 years is the numbers of Chinese within Granite Township but living outside of Folsom. This number fell from about 300 in 1880 to only 27 in 1900. Without the hundreds of miners and laborers frequenting Chinatown on days off, the economy within the district began to fail (Castaneda et al. 1984).

Ideally the decline of Chinatown would have occurred gradually, but was unfortunately hurried by fire. In August 1901 a big fire broke out in Chinatown at 3:00 a.m. According to the *Folsom Telegraph*, the fire started near the Tong Hing store (southwest corner of Block 9) and ended up consuming half of Chinatown. Firecrackers, bombs, and an oil tank helped spread the fire (*Folsom Telegraph* 17 August 1901).

By 1910, census records indicate only five merchants remained in town, compared to the 13 listed in 1900 (United States Bureau of the Census 1910). A few Chinese-operated laundries, some of whom had relocated on Sutter Street after a major fire, were still open (Sanborn Company 1899, 1910) and several Chinese cooks were privately employed (United States Bureau of the Census 1910).

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Over the next ten years Chinatown declined rapidly. In 1913 the old Chinese temple located on a knoll in Block 11 burned down. This building had stood on the same spot since the early 1850s and was a fixture in the district. By the time it burned, however, the paper noted that it was "little used of late due to the small number of Chinese remaining in this community" (*Folsom Telegraph* 20 June 1913; Plimpton n.d.a). By 1920 two Asian merchants remained in town, among them Chin Oak and his family store on Sutter Street, along with the three laundries and a few cooks. Only 26 Chinese were enumerated in Granite Township that year, most of whom lived on Block 10 in Folsom or on Sutter Street (Sacramento County 1921; United States Bureau of the Census 1920).

When Chin Oak died in 1924, the last pioneer Chinese in Folsom was laid to rest, signaling the end of the original gold rush Chinese community. His family continued to live and work in town, although the other few remaining Chinese families moved away looking for opportunity elsewhere over the next two decades. By 1925, non-Chinese residents were building new residences on abandoned sections of Folsom's historic Chinese district (Sanborn 1925). Chinatown dwindled to a couple of old stores, which were all gone by the late 1930s (Fong 1994). Some Chinese bachelors remained in Folsom until their death.

During the 1920s and 1930s, other Chinese families who had left Folsom occasionally returned to bury family members and to perform the annual ceremonies at their family grave sites. The shrine was intact and in use during the early 1940s (Puffer 1994). After her death in 1946, Chin Lai Shee became the last Chinese person interred in the cemetery (G. Chan, Jr. 1991).

After Chin Oak's death, his eldest son, George, Sr., ran the family store, the Chan and Chan Market on Sutter Street. Until his death in 1959, George, Sr., led his family as they observed the annual cemetery ceremonies. These rituals were modified during the 1930s and 1940s when food became precious during the Great Depression, followed by World War II rationing (G. Chan, Jr., 1994). All of Chin Oak's children in Folsom continued to observe these ceremonies, although more sporadically as the decades slipped by.

Today, only Chin Oak's grandchildren remain in town as active representatives of Folsom's early Chinese heritage. His grandson, George, Jr., continues to run the family store on Sutter Street and is the guardian of the Chung Wah Cemetery. His granddaughter, June Chan, continues her parents' efforts to honor and preserve the Chinese legacy in Folsom.

District Associations in Folsom

The Chinese who came to California during the mid-1800s came from 24 districts in Kwangtung Province (Map 6). The overwhelming majority was comprised of three major dialect groups: the Sam Yup (a.k.a. Sam Yip), Sze Yup (a.k.a. See Yup, Sze Yap), and Heungshan (a.k.a. Chungshan) people (Chinn 1969:4; Leung 1984:15, 28; Minnick 1988:6-7). Sam Yup-speaking people came from the districts of Namhoi, Punyu, and Shuntak. Sze Yup-speakers arrived from Sunwui, Hoiping, Yanping, and Toishan districts. Heungshan people immigrated from the district of Chungshan. The Hakka, a fourth smaller dialect group, came from a scattering of districts throughout Kwangtung Province (Chinn 1969:4).

The Sam Yup, Sze Yup, and Heungshan formed separate district associations to assist their members during their time abroad. All three associations offered temporary housing for their new arrivals disembarking in San Francisco, as well as in smaller Chinese communities near mining and agricultural centers. The Heungshan people named their association Yeong Wo (a.k.a. Young Wah, Yong Wa), while the Sam Yup and Sze Yup used their district name. These

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associations provided employment and legal assistance, recreational and spiritual considerations, advice on dealing with non-Asians, and access to familiar foods, medicines, and clothing (Minnick 1988:9-10).

Chinese usually maintained their distance from other Chinese from different districts or associations. According to Chinese-American historian Sylvia Minnick, "Those from Toishan encamped away from the Heungshan diggings, and both kept their distance from the Hakkas" (Minnick 1988:13). This was due to personal preference as well as distinct dialectic differences; people from one district often could not understand those from another district. In addition, Chinese from these two districts felt a great deal of animosity towards each other and brought their long-standing feud with them to America (Leung 1984:7).

The feuding between the districts, especially the Sze Yup and/or Sam Yup versus the Yeong Wos, resulted in "wars" or small-scale battles that occurred in the 1850s in several of the mining camps in California. In Weaverville, disputes over mining claims led to a full scale battle between the "See-Yups" and the "Yong Wahs" that culminated in a series of fights and numerous deaths between 1853 and 1859 (Brott 1982:13-17; Minke 1960:14-15). Chinese Camp, in Tuolumne County, was the site of another physical battle between numerous "Sam Yups" and "Yan Wos" over mining claim disputes. This battle took place in 1856 and ended in four dead and four wounded (Minke 1960:18-19; Paden and Schlichtmann 1959:71-72). While no known battles occurred at Folsom, these incidents point to the level of antagonism that existed between the various factions.

The Sam Yup Association quickly established branches in Sacramento and Stockton to meet the needs of the large number of Sam Yup immigrants heading for the gold fields. During the 1860s Chinese from some non-Sze Yup districts previously under the jurisdiction of the Sze Yup applied to the Sam Yup for membership (Lai n.d.:16). By 1878 Sam Yup membership peaked at 12,000 members statewide (Lai n.d.:17). Unfortunately a feud, climaxing in the 1890s, created tension between Sam Yup and Sze Yup people (Lai n.d.:18), although there is no evidence to indicate how this affected the Chinese community in Folsom.

The separation between the Yeong Wo and Sze Yup/Sam Yup lasted in Folsom into the 1920s. The Yeong Wo Association members, while doing business with other Chinese, socialized separately from members of other district associations in Folsom's Chinese community. Yeong Wo members traveled into Sacramento to visit other Yeong Wo rather than socialize with the Folsom Sze Yup or Sam Yup (G. Chan, Jr. 1994). This, in part, explains why two different groups of Chinese in Folsom bought land for cemeteries instead of sharing one cemetery.

Members of all three major district associations resided in Folsom. Their association buildings were prominent fixtures in Folsom's Chinatown (Minnick 1988:22). The Sam Yup Association established their Folsom branch during the 1850s (Lai n.d.:15). The association rented land on Block 19 for their operations into the twentieth century, including a store, social hall, and housing for the sick and aging (Minnick 1988:22; Sacramento County, 1893a, 1893b, 1895a, 1895b, 1900). In the 1880s, the branch corresponded with their headquarters in San Francisco regarding funding for repairs to their association hall building (Lai 1994). This perhaps indicates the vitality of the association and its positive outlook for continued membership in Folsom.

The Yeong Wo Association, comprised of Heungshan people, owned property for its operations by 1883, although it probably rented for many years before buying (Map 7). The association built their benevolent hall and shrine on their property on Lots 1 through 4 of Block 17, on the southwest corner of the intersection of Leidesdorff and Burnett Streets (Sacramento County 1883, 1885a, 1912). Their shrine remained in the 1930s, although the structure, with its black doors and peeling red paint, was almost abandoned (G. Chan, Jr. 1994). All that remained of their benevolent hall at that

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time was a small "shack" where a Chinese caretaker tended the gardens and fruit trees surrounding the site (Fong 1994). Yeong Wo also owned and maintained a cemetery on Lots 3 through 8 on Block 40, several blocks from the Chung Wah burial site (see Maps 3 and 7). No evidence of the Yeong Wo cemetery remains today.

Spiritual Life

Associations provided for the spiritual and religious needs of their members. As early as 1855 Folsom Chinatown had two "churches," one on Block 10 and one on Block 11 (Sacramento County 1855). In December, 1865 the *Folsom Telegraph* reported:

The Celestial portion of our community, dedicated their new temple on last Thursday -- Many strange looking images were placed upon their altars. On one side of the altar, there was a picture representing a dragon, on the other side was a tiger. At the entrance there was a representation of his Plutonic Majesty leading a tiger. Goats, pigs, chickens and numerous other edibles were placed upon the altar for their hungry gods to feast upon.

Despite frequent fires, the Chinese rebuilt and maintained their churches, or joss houses, into the twentieth century.

Concern for the Spirit After Death

Receiving proper care after death was a major concern to Chinese sojourners living far from their native land and family. Accounts in miners' diaries and newspapers detailing Chinese funerals, care of the dead, and burial practices began appearing as early as 1849. For example, one Euroamerican miner working in the Folsom region observed that after a Chinese miner drowned on the North Fork of the American River in 1850, his countrymen put gold dust in his mouth and hands and buried him. Four weeks after the burial the remaining members of the dead man's company came down to the grave, bringing boiled beef, pork, a dozen oranges, raisins and some brandy. They placed the food on the grave, burned cakes beside it, and shared the brandy with the Euroamerican on-lookers (Forbes 1850).

This attention and respect for the dead is deeply rooted in Chinese culture. Two traditional Chinese ceremonies annually honor and tend to relatives' spiritual afterlife. During the Pure Brightness Festival (*Ch'ing Ming* or Chinese Memorial Day) the family elder ritually sweeps the graves of relatives with a willow branch, believed to repel evil spirits. Once the grave is swept, the family cleans and removes weeds growing on the grave mound. Dishes of cooked food, such as roast pork, are placed before the grave, then wine is poured over the grave. As incense sticks, red candles, paper money and paper clothing are burned to send to the deceased in the spirit world, exploding firecrackers create confusion to hopefully stop evil spirits from pursuing the deceased. Following the ceremony the food is removed and eaten at home. This festival usually occurs during the spring, in the third month of the Chinese calendar (Chinn 1969:76; Culin 1887:195).

The second festival is celebrated on the fifteenth day of the seventh moon as relatives make their second visit to their family graves. This Spirits' Festival (*Shao'!* or "burning paper clothing") is described by Chinese historian, Thomas Chinn as:

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the day the dead were believed to return to roam at will, or to visit living relatives. And since the spirits must have money to travel, more paper money and paper clothing was transmitted by burnt offerings than at the *Ch'ing Ming* festival [Chinn 1969:77].

Associations usually provided for the burial and later return of their members' remains to China, where their families would attend to their needs in the afterlife. Associations also frequently erected shrines within their cemeteries in America to meet the spiritual needs of the deceased before their remains were hopefully one day returned to China. These shrines were used extensively during *Ch'ing Ming* for burning incense and other offerings (Chinn 1969:77).

The patriarch of Folsom's Chan family, Chin Tock Oak, came from Toishan district in China, a member of the Sze Yup Association, and therefore he and his descendants used the Chung Wah cemetery. The Chan family celebrated *Haung Tien*, loosely translated as "Climb to Heaven" in the spring and autumn of each year. In this ceremony, the family would boil a whole chicken, with head and feet still attached, and take the chicken with boiled eggs, Chinese whisky and beer to the cemetery. Occasionally the family would travel into Sacramento or San Francisco to buy special Chinese foods, like leechee nuts, unavailable in Folsom. In later years, they also brought non-Chinese foods. The elder male would put the food near the head of the grave mounds of family members, pour whisky or beer over the grave, and talk to the deceased. He would also burn paper money and punks, lighting them three at a time, to send as offerings to the deceased. Some punks were thick, some thin, some red and some waxy, each conveying a different kind of message (G. Chan, Jr. 1994).

By custom, the deceased was buried in a wooden coffin in a shallow grave, about two feet deep. An identification marker was often placed in the coffin, and sometimes on the grave mound itself. This marker provided verification of the identity of the remains when the body was exhumed after a period of five or more years for return to China. Identification markers consisted of either a brick painted or etched with the deceased's name, a marked slip of paper in a bottle, or an above-ground wooden marker (Askin et al. 1980:11-12; Minnick 1988:291-292, 1994).

Professional exhumers from association headquarters in San Francisco periodically traveled to outlying Chinese communities to conduct exhumations. After ensuring all the deceased's bones had been properly cleaned and accounted for, the skeleton was reassembled in a crouching position and placed in an urn or small box. Sometime after the turn of the century, remains were only returned to China if relatives existed to pay the high costs of exhumation permits, shipping, and reburial (H. Chan, Sr. 1981; Minnick 1988:291-292). Otherwise, the urns were reburied in the cemetery.

One account of a nineteenth-century exhumation at Chinese Camp in Tuolumne County serves to illustrate the ceremony that surrounded this procedure. Paden and Schlichtmann (1959:137-138) quote Mr. Edwin Harper's story as follows:

I remember when a boy, that a group of us used to watch a certain Chinaman when he came here from San Francisco. He was a priest or some important official. We would hide in the bushes to watch the priest with several others as they walked slowly, in single file, to the graveyard. They wore fine Chinese clothing and hung bright-colored banners on the shrubs around the grave they had come to open. Then they chanted and gestured for a time. The officials brought Chinese laborers to do the actual digging but they were most particular to see that every tiny bone was gathered. A piece of silk was spread at one side of the grave and bones placed on that. When every single one was found and accounted for they were placed in a small wooden box which was given to the Chinese priest with a good deal of ceremony and they all went back to Chinatown.

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Paden and Schlicktmann continue to relate that the box was always the length of a human thigh bone and that the bones were scraped clean and sealed, with the queue carefully placed on top of the bones.

The Sam Yup Association began exhuming and returning remains from California to China in 1858, again in 1863, and then every ten years thereafter until the 1910s (Lai n.d.). No records exist for exhumations in Folsom, although apparently they did occur until the late 1910s (Askin et al. 1980:12; G. Chan, Jr. 1994; H. Chan, Sr. 1981; Lai 1994). By periodically removing remains, hundreds of Chinese could be buried in a relatively small cemetery without running out of space.

Accounts of Funerals in Folsom

Accounts of Chinese funerals in Folsom offer a vivid image of the spiritual vitality of the Chinese community. Limited funds made most funerals simple affairs. A few friends of a miner or laborer would follow his coffin to the grave, scattering paper money along the way in the hopes of distracting evil spirits from bothering the dead. The noise from firecrackers, a fiddle or a gong would fill the air to scare away those spirits (Minnick 1988:288). Such was probably the case with Ah Tan, a miner who died in Folsom after a brief illness in 1889. He was buried "with the characteristic ceremonies of his race" in a fine coffin purchased from the Folsom undertaker, Jacob Miller. The coffin had been purchased by a Chinese merchant, presumably acting on behalf of the deceased's association (*Folsom Telegraph* 30 November 1889).

The elaborate funeral of Ah Chung, a merchant with Num Sing and Company, was described in the *Folsom Telegraph* on September 23, 1871 as follows:

His remains were followed to the grave by a large number of his countrymen in carriages and on foot. A son of the deceased who seemed almost overwhelmed with grief, was dressed in white, with a white cloth over his head, and carried a pan of ashes in front of him, and was supported in a bent position by a Chinaman on each side of him and presented a singular appearance.

The Chinese community conducted traditional funerals at least as late as 1910. The coffin was transported in a horse-drawn hearse from the 900 block of Sutter Street to one of the Chinese cemeteries. Some mourners walked along with the wagon, while others rode with the coffin. As the procession moved west down Sutter Street, the Chinese tossed food, small change, and red paper with holes in it along the way (F. Hill 1994). Strewing perforated strips of red paper was a common element of Chinese funerals (Minke 1960:10).

When Chin Oak, a well-known merchant and resident of Folsom for 65 years, died in 1924, his eldest son, George, washed his body and placed him in a coffin. The ceremony and casket were both simple. A Chinese priest, who tended the local joss house, conducted the ceremony dressed in a black gown and black hat. Presiding over Chinese funerals provided the priest's sole income, amounting to two or three dollars for Chin Oak's service in 1924. Many local residents, Asian and non-Asian alike, came to pay their last respects to this pioneer of Folsom's Chinese community (G. Chan, Jr. 1994; H. Chan, Sr. 1981; J. Chan 1994).

The Chinese used two locations in Folsom for burning the deceased's belongings needed for their journey in the afterlife, such as their clothes, shoes, and blankets. A finely-built brick oven with steel doors stood near the temple on the Yeong Wo property on Block 17. As the belongings burned, smoke rose out of a large exhaust pipe to accompany

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the deceased's spirit (H. Chan, Sr. 1981). At the Chung Wah Cemetery, relatives or friends of the dead person burned their belongings in a large depression located just inside the main entrance (G. Chan, Jr. 1994).

Identification of Chinese Burials

It is unlikely that the names and number of all Chinese and Chinese-Americans buried in Folsom can ever be discovered. The community of Folsom kept no death records, unlike Sacramento where city officials and cemetery owners maintained comprehensive records of deaths and burials dating back to 1849.

Jacob Miller, the founder of Folsom's sole funeral business from the 1860s to the present, retrieved those Chinese discovered dead or those who died from unnatural causes. He also signed most coroner's inquest reports. The Miller family funeral business sold coffins from their store on Sutter Street to the Chinese and let their hearse for conveyance to the cemetery. Unfortunately the Miller family kept no written business records (Askin et al. 1980:10; Claney 1994).

The *Folsom Telegraph* published notices of Chinese deaths, but rarely included given names prior to the 1890s, unless the deceased was well-known by the non-Asian community. The *Folsom Telegraph* notices indicate that burials were occurring in the Chinese cemeteries at least by 1871, although interment probably began in the 1850s. The Sacramento County Recorder only began issuing death certificates for Folsom after the Chinese community had dwindled to a mere handful. Three death certificates for Chinese burials in Folsom cited in Askin et al.'s report on Folsom's Chinese cemeteries are for Chin Him, 9-11-1916, Wing Sing Wo, 11-20-1923, and Charlie Heoung, 2-28-1935 (Askin et al. 1980:10). The death certificate of Wing Sing Wo, also known as Chin Oak, gives the date of death as November, 1923, his grave marker notes that he passed away in 1924. The majority of deaths in the Chinese community after 1893 were the result of old age.

As Chinese immigration and population in California declined, the original Chinese Six Companies (a composite of district associations and guilds) were succeeded by the Consolidated Chinese Benevolent Association or *Chung Wah Wui Kun* in Chinese (Minnick 1988:273). While the Chung Wah Association remained strong in California until the 1950s, no membership records have been found for Folsom. The Sam Yup Association does not know when its membership in Folsom ended, although Sam Yup historian, Him Mark Lai, believes it likely ended in the 1920s as the aging bachelor community died off or moved away (Lai 1994).

Chung Wah Cemetery

Records of property tax assessments and deeds usually provide concrete evidence of land ownership and use, but this is not the case with the Chinese. Due to prevailing prejudice, the transient nature of the Chinese sojourner in California, and lack of funds, among other reasons, it was common for Chinese associations and business people to rent property from non-Asians rather than purchase it themselves, or to rent for a while and then purchase the property. It is reasonable to assume that lots purchased for cemeteries were already being rented for that purpose prior to acquisition of title. After passage of the Alien Land Acts in 1913 and 1921, Chinese were no longer able to purchase land, but retained title to land acquired before 1913.

The first recorded Chinese burial in 1871 occurred 14 years before the Chung Wah Association acquired their land. There were certainly Chinese deaths in or near Folsom as early as 1850 (Forbes 1850). While some deceased may have

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Property Name Chung Wah Cemetery

been transported by their association to burial sites in the New Helvetia or City cemeteries in Sacramento, the majority were likely buried in Folsom. Considering the intensive mining occurring in this area, as well as the great importance Chinese spirituality places on the afterlife, it is unlikely that Chinese were buried in isolated graves.

Comparative analysis of other Chinese burial sites further supports the use of the plot as a cemetery long before its purchase for that use. Throughout Sacramento County, Chinese were buried in segregated sections of non-Asian cemeteries. This did not occur in Folsom. The Chinese who died in Folsom before 1883 were not buried in other Folsom cemeteries, were probably not interred in isolated locations, and most likely were buried in devalued land already mined out well above seasonal river flooding levels, precisely the location of the Chung Wah site.

The Chung Wah cemetery is the larger and probably older of Folsom's Chinese cemeteries. Evidence suggests the present cemetery, along with another adjoining six lots, comprised a large Chinese cemetery dating from the gold rush. Initially, the Natoma Water and Mining Company, and its descendant, the Natoma Vineyard Company, owned the land, a tiny segment of the company's tens of thousands of acres of property. Although there appear to be no records of the Natoma Company leasing the land to the Chinese for a cemetery, several factors suggest that this may have been the case. First, the Natoma Company relied heavily on Chinese labor for its canal and ditch operations, as well as its ranch and vineyard businesses. Chinese use of two acres for burials, out of the company's more than 32,000 acres, would probably have been ignored, especially on land mined out early during the gold rush. Secondly, the Chung Wah cemetery is near Folsom's non-Asian cemeteries, which date to the 1850s. Chinese burials in the motherlode region were typically located in a segregated remote section of non-Asian cemeteries. The Chung Wah is close to the other cemeteries, but more remote.

The site's irregular shape suggests it was haphazardly planned, perhaps beginning with a few Chinese burials and then spreading out as more Chinese died in Folsom over the years. When the Folsom Development Company deeded the present site to the Chung Wah Company in 1906, the site was referred to on the deed as the China Mission cemetery, suggesting its pre-existence as a cemetery. When fire consumed the last Chinese temple, located on Block 11, in Folsom in 1913, the *Folsom Telegraph* referred to the structure as the "China Mission #1," stating it had stood since the 1850s (*Folsom Telegraph* 20 June 1913; Plimpton n.d.). The similarities in their names suggests they may have been related.

Concrete evidence for Chung Wah Company ownership of a cemetery begins in 1885. On October 6th, James S. Meredith deeded Lots 6 through 11 of Block 71 to Chung Wah for \$190 (Sacramento County 1885b). Previously, the only recorded owner was Charles G. W. French, an attorney who left Folsom to set up a practice in Sacramento after the death of his wife, Abby Hewes (Sacramento County 1875). French owned numerous lots in Folsom as investment property until his death in 1892 (Sacramento County 1892).

The Chung Wah Company owned no other land in Folsom. During this period the Sam Yup Company rented property for a store and possibly a hall, while other buildings identified as tongs (or associations) were prominent fixtures in Chinatown (Sacramento County 1893a, 1893b, 1900). It is probable the Sam Yup and other associations, such as the Sze Yup, combined resources to purchase the Chung Wah site as a cemetery for joint use.

In 1906 the Chung Wah Company exchanged Lots 6 through 11 of Block 71 for the present 2.616-acre site, then owned by the Folsom Development Company. C. G. Lang, listed as president of Chung Wah Company, acted as agent for the association. Presumably provisions were made for the removal of any remains in Lots 6 through 11 before the company dredged Block 71 in 1907 and 1908 (Sacramento County 1906a, 1906b).

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As association membership was eclipsed by family and community identification, Chinese-American families in Folsom continued to bury and honor their relatives in the Chung Wah Cemetery. The last known burial in the site occurred in 1946 when Chin Lai Shee was interred there. The mother of Marie Chan, George Chan, Sr.'s wife, had lived for years in San Francisco. After she died there, the Chan family brought her to Folsom for burial (G. Chan, Jr. 1994; H. Chan, Sr. 1981).

For decades the Chan family and other families with friends or relatives buried in the Chung Wah Cemetery have celebrated holidays that honor and tend to the needs of the dead; each generation learning from the last, passing on and adapting the rituals to their times. Today, George Chan, Jr. is the trustee and guardian for the cemetery, a natural extension of the family association passed down through the Chung Wah Company lineage.

Comparative Analysis

Folsom's Chinese cemetery is unique in Sacramento County. All other known Chinese burial sites lie within community cemeteries shared by Asian and non-Asian alike. There appear to be no other separately owned and operated Chinese cemeteries in the county. Of particular note is the proximity of two Chinese cemeteries in Folsom, indicating the strong identification with district associations from China, as well as the vitality of the associations.

In Sacramento, the New Helvetia and City cemeteries both contained Chinese sections dating back to the gold rush. The Record of Deaths book for Sacramento City, 1850 to 1870, lists the first Chinese death in Sacramento occurring on September 25, 1851. This entry lists this individual as "Chinaman," native of China, no age, no former residence, no cause of death, no attending physician, and cites a burial location in either the New Helvetia or City Cemetery that can no longer be traced due to plot renumbering.

Chinese district and family associations bought large "family" lots for their member interment. For example, in 1891, Sow Yuen Tong & Co. purchased a 24-foot by 60-foot lot for \$270. Burial registers list Chinese burials in the City Cemetery as early as 1865. Chinese associations that purchased lots between 1867 and 1874 alone include Ming Yueng Co., See Yup Co., Sam Yup Co., Foulk Hing Tong Co., Hop Wo Co., Coy Chew Co., Chong Chaw Co., Hong Chew Co., Young Chow Co., and Quong Chew Co. (Sacramento, City of 1847-1955, 1850-1870, 1871-1874).

District associations periodically exhumed remains for return to China beginning in the 1850s. In 1955, Chinese remains were disinterred from the New Helvetia Cemetery when that cemetery was de-activated. Those remains were either returned to China or reburied in East Lawn Cemetery and marked with wooden markers. (A. Lee 1994).

The earliest Chinese burial sites in the City Cemetery, the "tiers," have been modified and the bodies moved to the southern corner of the cemetery. The Chinese association lot purchase records indicate that the Southside section of the City Cemetery became the Chinese section of the City Cemetery after 1900. No monument, altar or shrine was ever erected in this section. The Odd Fellows purchased the adjacent land from the City Cemetery in 1905 and retain its ownership to the present day. The Masonic Order bought more adjacent land in 1906. Together they presently maintain the appearance of the Chinese section (Bettencourt 1994).

In communities outside Sacramento, this pattern of burial in a section of the larger community cemetery continued. Elsewhere in the foothills, communities actively excluded the Chinese from the main cemeteries. Chinese can be found in separate areas set aside outside the main confines of the cemeteries (Bettencourt 1994).

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The Elder Creek District Cemetery, between 65th Street and Power Inn road on Elder Creek Road, contained a Chinese section (Hayse 1994). Franklin had a Chinese section in their larger cemetery (L. Chan 1994; Hayse 1994; P. Lee 1994). The large Chinese community of Locke never had its own Chinese cemetery (P. Lee 1994). There are also no Chinese/Chinese-American burials in Courtland (L. Chan 1994).

In the nearby communities of Rio Vista, Franklin, and Stockton, all outside of Sacramento County, most Chinese burials exist only in the regular cemetery, not in exclusively Chinese cemeteries (P. Lee 1994). However, at least two exclusively-Chinese cemeteries outside of Sacramento County are known to exist. One is located on Matthews Road in French Camp. This private cemetery was founded in the late 1920s by Stockton's Chung Wah group and is still in use. The leaders of Chung Wah in that area conduct "tomb sweeping" rites twice every year (Minnick 1988:290). Another is located in Auburn. This cemetery, located in behind a gravel plant off Highway 49, still contains the oven used for burning belongs and offerings for the deceased (Costello 1994; Minnick 1994). In addition, there were three small Chinese cemetery sites at Virginiatown in Placer County. Two were exclusively Chinese and are on two separate hills. The third is smaller and an oral history map refers to this third cemetery as "the woman's Chinese cemetery" and also notes that the Sickles, a Euro-American family, were buried there. Apparently, all bodies were removed from all three cemeteries (exhumation pits are evident) and no associated features remain (Farnscomb 1994).

PERIOD OF SIGNIFICANCE

While it is likely that the cemetery was in use in the nineteenth century, there are no documents to support this supposition. Therefore, the period of significance begins in 1906 when Chung Wah Company acquired legal ownership of the plot in a trade with the Folsom Development Company. The last burial occurred in 1946 with the interment of Chin Lai Shee, ending the period of significance. Annual ceremonies, like *Ch'ing Ming*, continued long after 1946, but no burials or planned exhumations (other than those associated with the 1967 vandalism) occurred after this date.

STATEMENT OF SIGNIFICANCE

The Chung Wah Cemetery in the City of Folsom is eligible for the National Register of Historic Places under Criterion A, Consideration D and G. As the last visible reminder of the viable Chinese population that once inhabited Folsom, the cemetery acts as a link to the rich ethnic heritage of the town. The burning pit, shrine remains, and other features also attest to the spiritual value Chinese place on the afterlife and the permeation of religious beliefs into all aspects of their culture. One of two district cemeteries in town, the Chung Wah is unusual for its strong association ties and actual Chinese ownership. Elsewhere, Chinese shared a portion of Anglo cemeteries and other cemeteries in the county no longer contain traditional elements like the Chung Wah, increasing its importance on a local level.

Folsom's Chinese community consisted of stores, restaurants, barber shops, gaming houses, gardens, homes, churches, and association halls that serviced the hundreds of miners working around Folsom. As a commercial transportation center for a vast mining area, Folsom played an important role in the life of Chinese miners or laborers from the 1850s until around 1910, when the population rapidly declined. District associations were extremely important, providing a link to the traditional culture and to relatives back home in China, a social center for workers far from home, and a guarantee that one's remains would be properly taken care of after death. Regular exhumations, with remains sent home for burial and care by relatives, was an important role fulfilled by the association.

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The last burial occurred 49 years ago, in 1946, marking the end of the period of significance and qualifying the cemetery under Consideration G. The significance placed on the burial place of relatives in Chinese culture is seen in the observation of yearly *Ch'ing Ming* ceremonies by Folsom's Chinese-American residents for many years after the termination of the Chinatown. The cemetery remains in the care of the Chan family, the last of Folsom's original Chinese families still residing in town. Its natural setting, its location in a remote area on the outskirts of town, the intact features, and the sense of time and place that pervade the site mark a high level of integrity and increase the historical value of the site.

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Property Name Chung Wah Cemetery

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Property Name Chung Wah Cemetery

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Property Name Chung Wah Cemetery

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Personal Communications

1992	Chan, June	Folsom resident
1994	Bettencourt, James	Cemetery historian, Sacramento
	Chace, Paul	Historical archaeologist/Chinese historian
	Chan, George, Jr.	Folsom resident and Chung Way cemetery trustee and guardian
	Chan, June	Folsom resident
	Chan, Lincoln	Courtland resident
	Claney, Robert	Folsom resident
	Costello, Julia	Historical archaeologist/Chinese sites specialist
	Farnscomb, Melissa	Historical archaeologist working at Virginiatown
	Fong, Robert	Former Folsom resident
	Hayes, John	Cemetery historian, Sacramento County
	Hill, Fern	Folsom resident
	Lai, Him Mark	Chinese-American historian, Chinese Historical Society, Sam Yup Association
	Lee, Anna	Chinese-American historian, Sacramento
	Lee, Ping	Locke/Walnut Grove resident

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Property Name Chung Wah Cemetery

Minnick, Sylvia Sun
Wegars, Priscilla

Chinese-American historian
Director, Asian Collection Center, University of Idaho, Moscow

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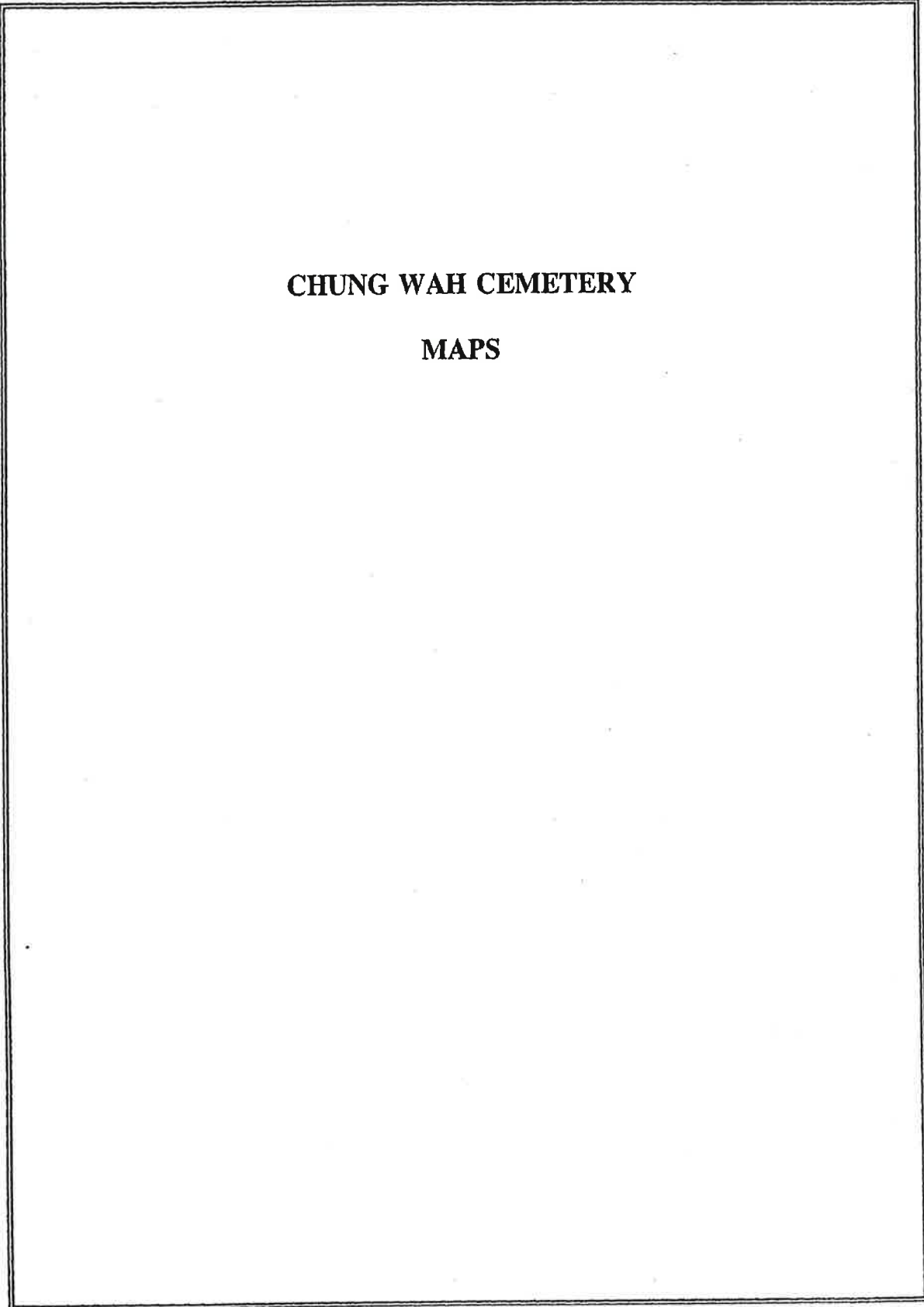
Property Name Chung Wah Cemetery

VERBAL BOUNDARY DESCRIPTION

The cemetery encompasses 2.616 acres contained within an irregularly-shaped parcel. The parcel includes a portion of Lots 7 and 8 in Block 70 and continues south and west on unparceled land to the boundary of the Bureau of Reclamation's Lake Natoma holdings. The boundaries are depicted on attached Map 8, labeled "Chung Wah Cemetery Boundaries."

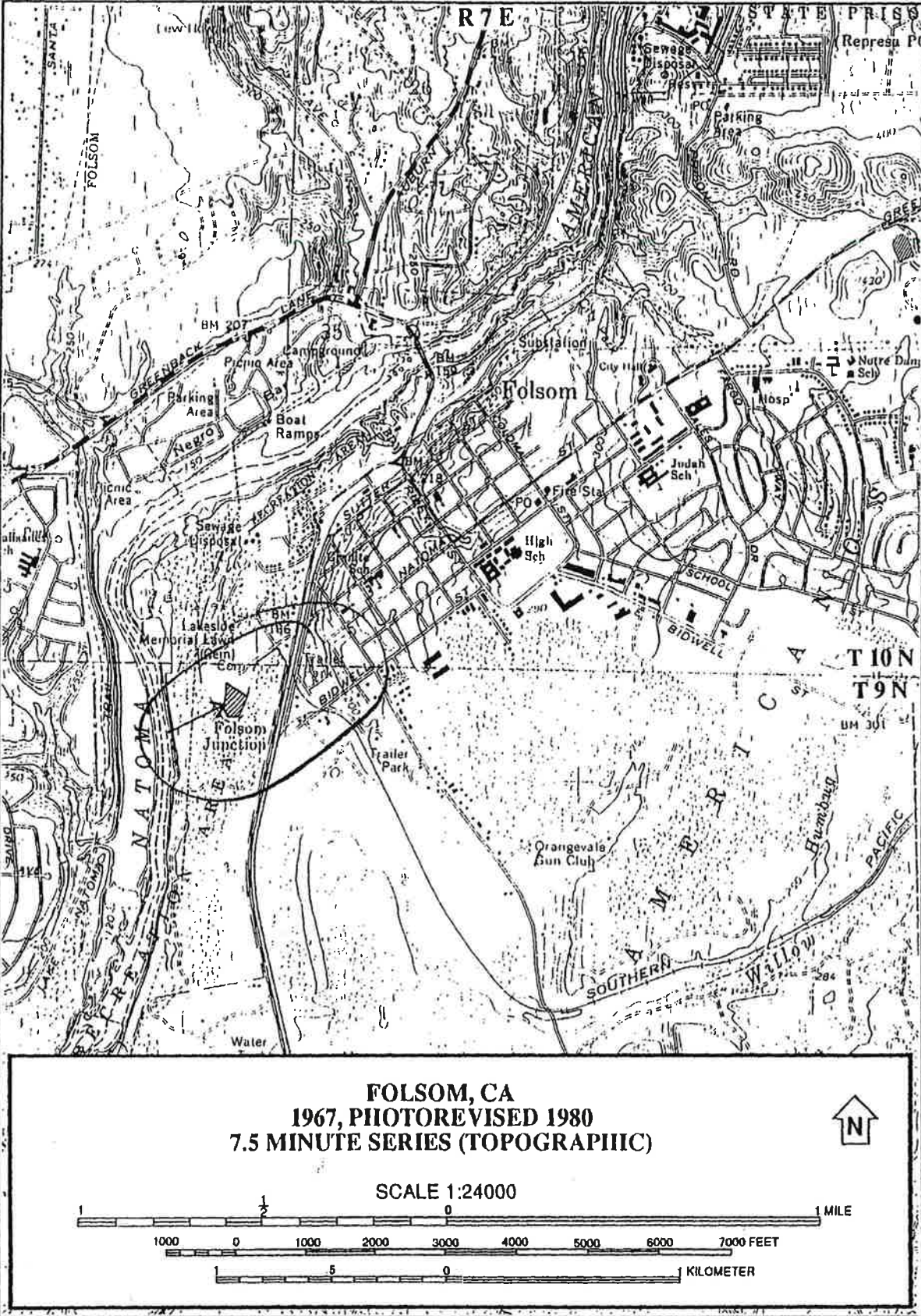
BOUNDARY JUSTIFICATION

The boundaries delineated for the Chung Wah cemetery include the area outlined on the 1906 land deed transferring this property to the Chung Wah company for use as a cemetery. The boundaries encompass the area currently held in trust under the guardianship of George Chan. All related cemetery features are contained within the fenced boundaries.

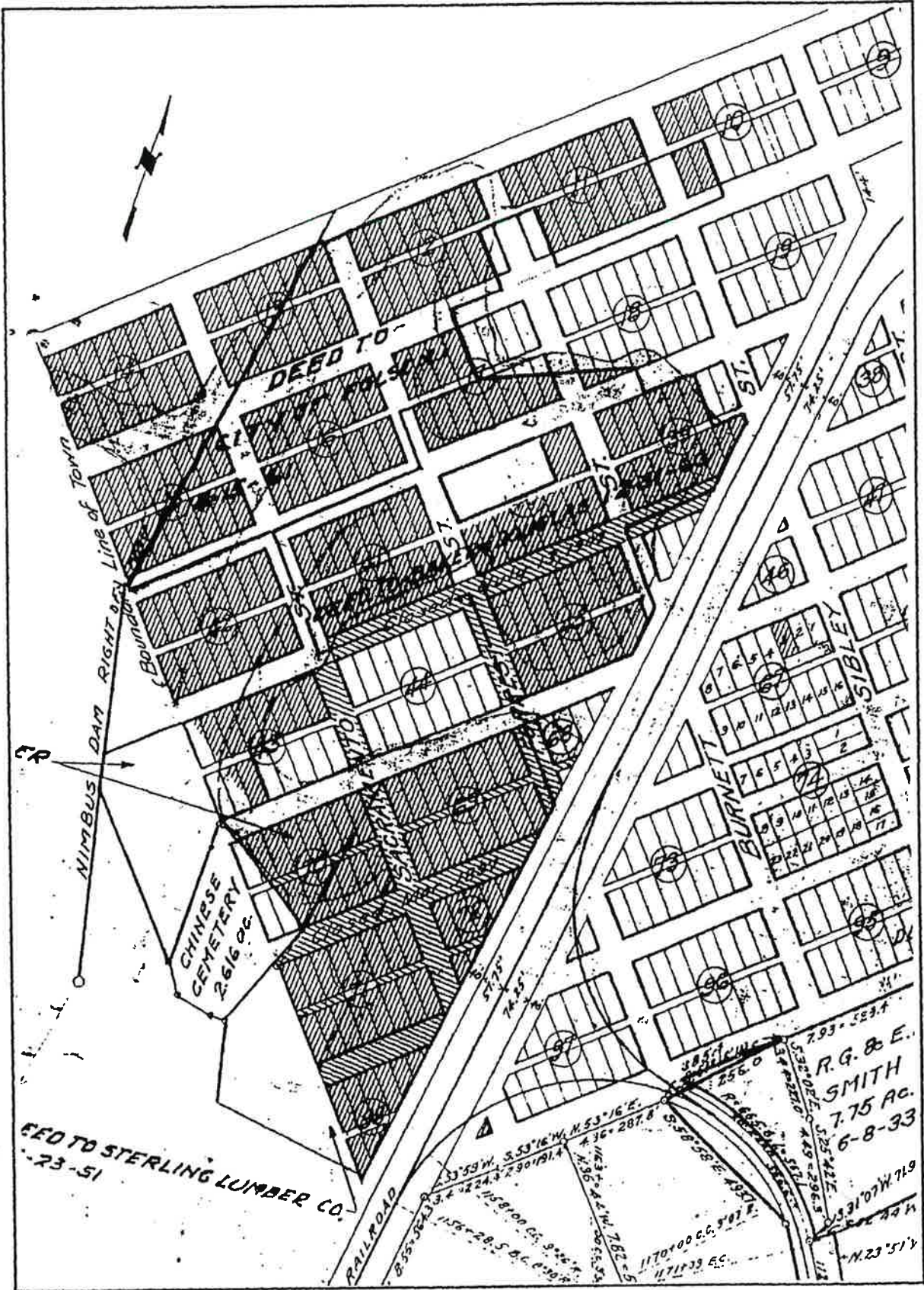


CHUNG WAH CEMETERY

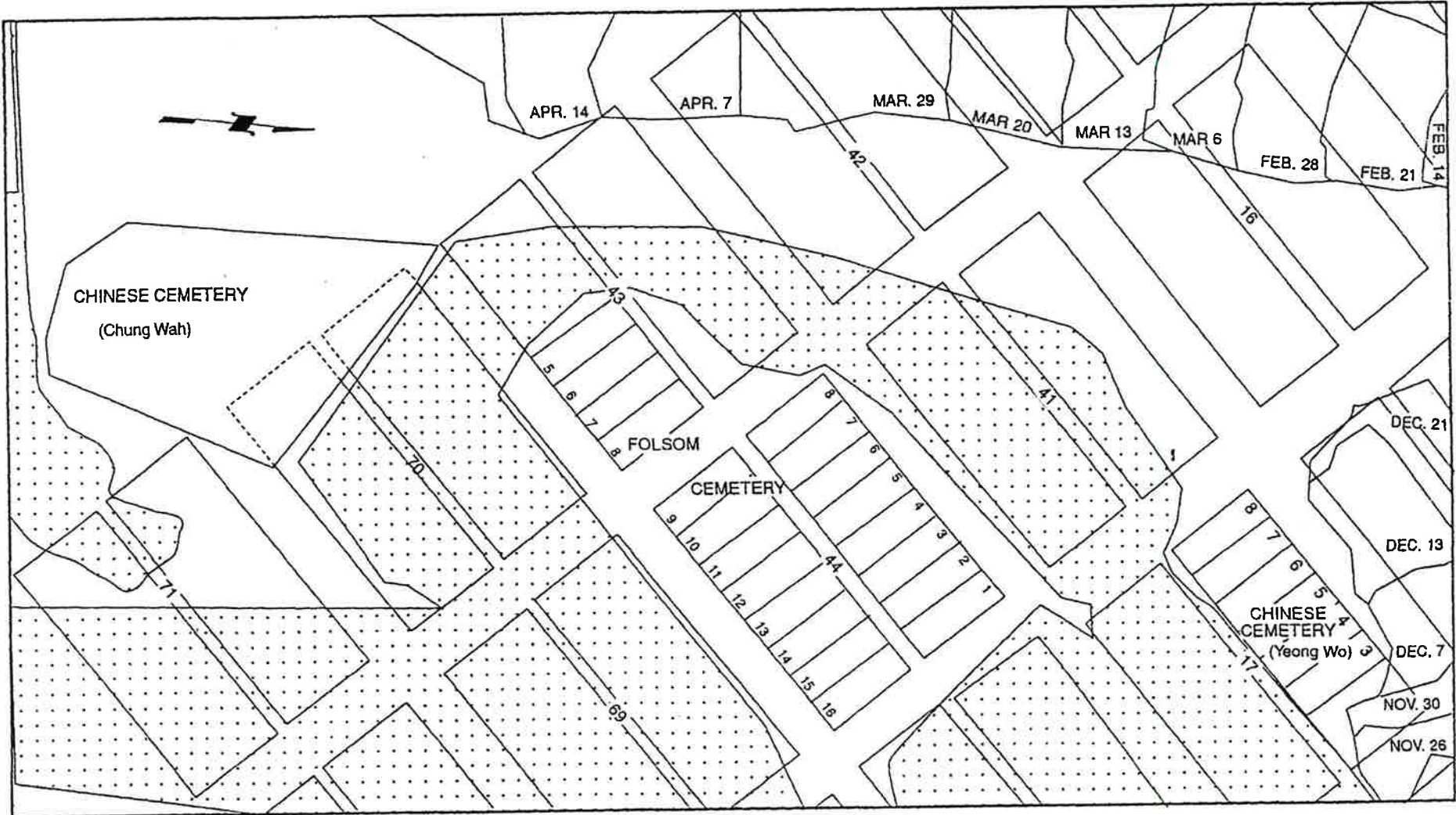
MAPS



Map 1. Chung Wah Cemetery

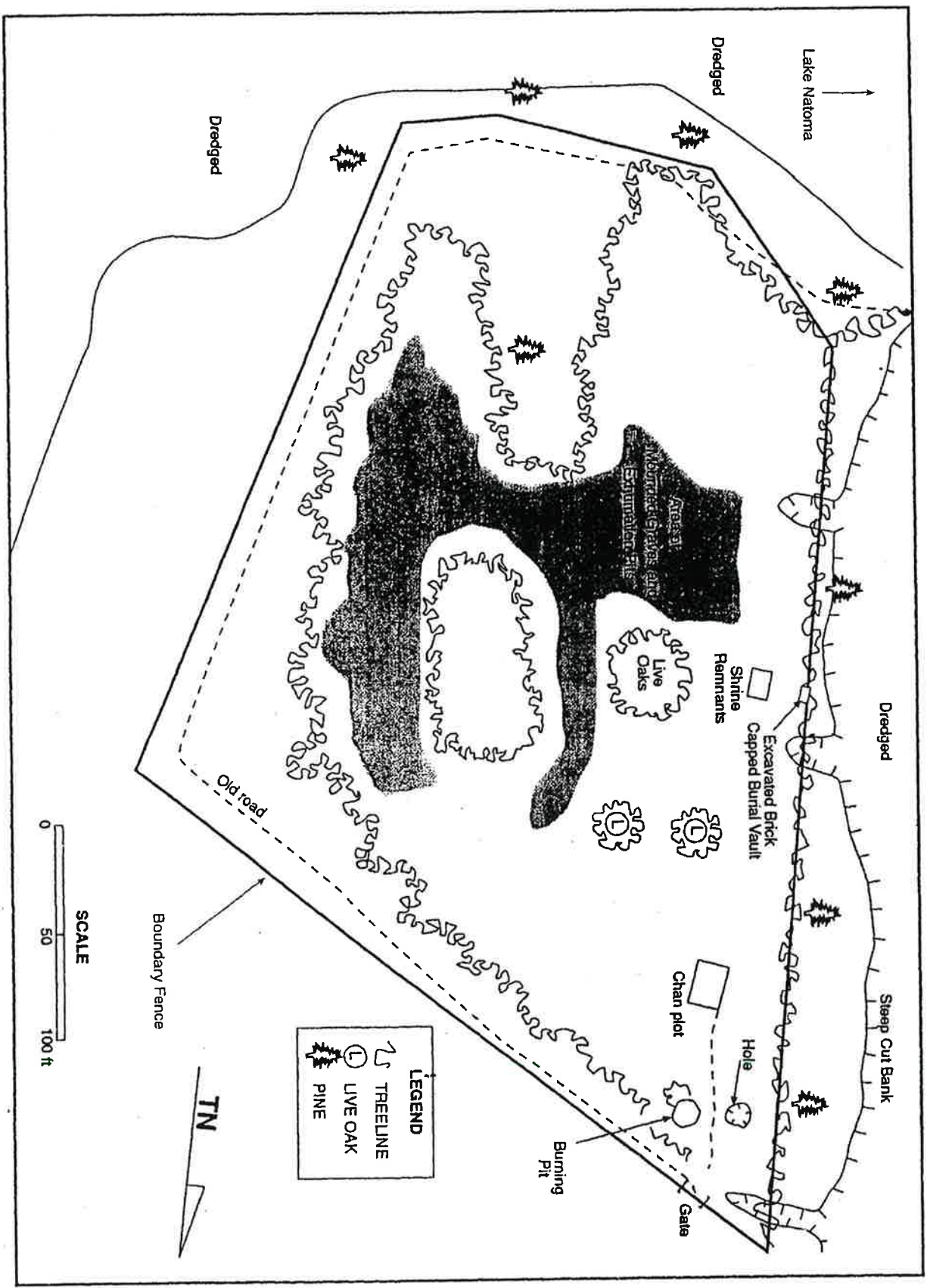


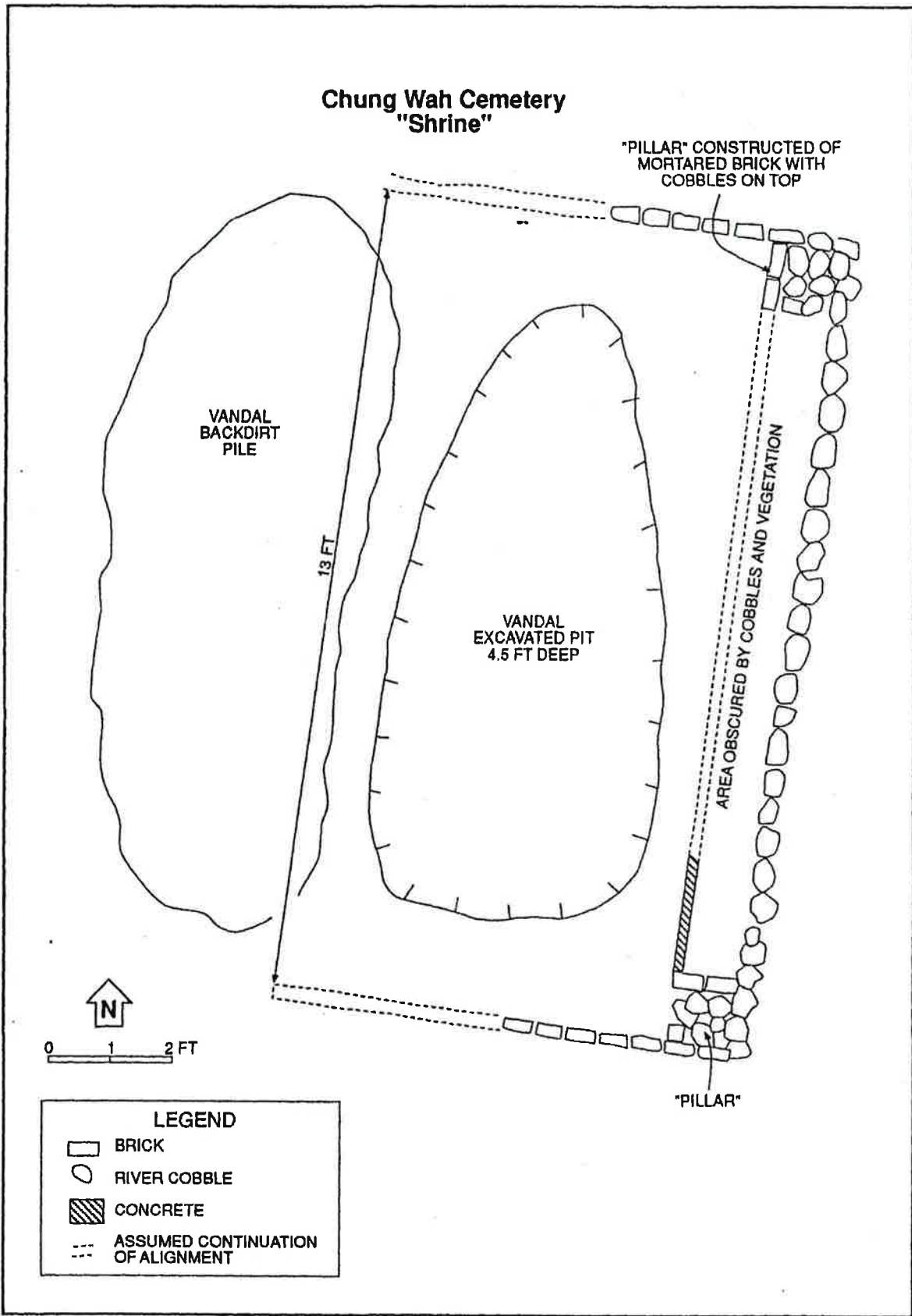
Map 2. Location of the Chung Wah Cemetery within the City of Folsom



Map 3. Map of Dredged Land Depicting Chinese Cemeteries (Adapted from Natomas Company 1907-1909; Screened Areas and Dates Represent Dredged Lands)

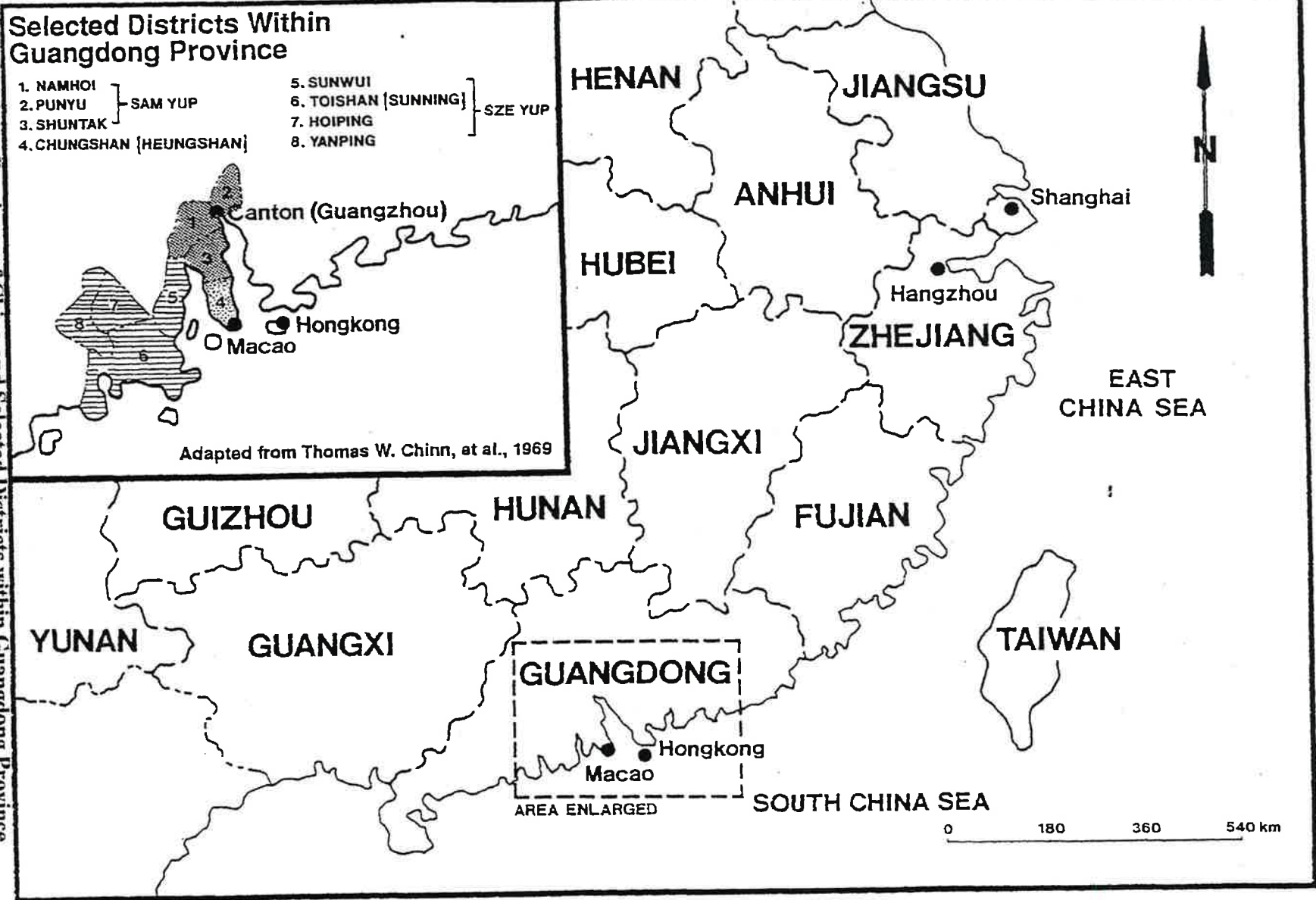
Map 4. Map of Chung Wah Cemetery Site, 1994

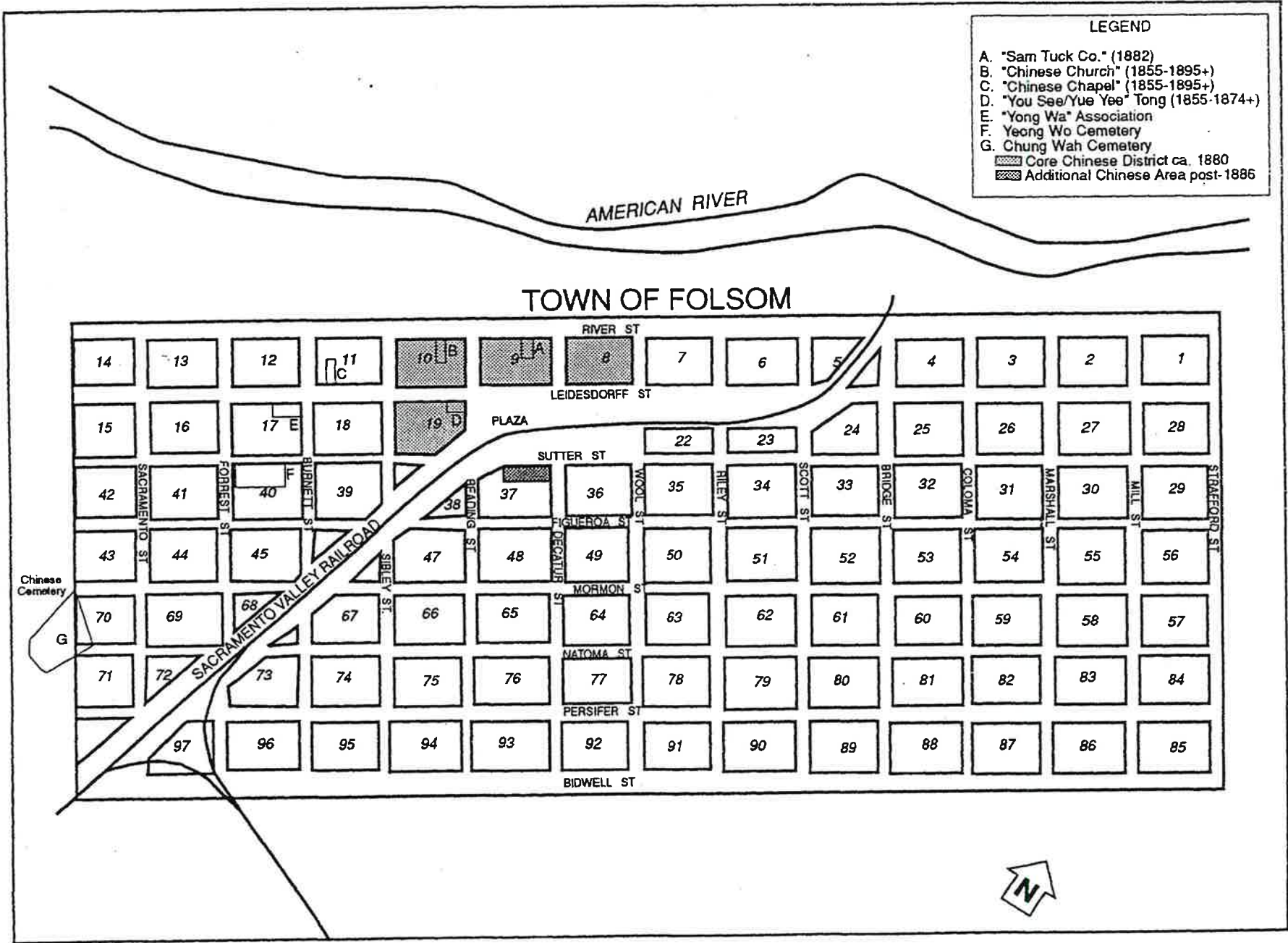




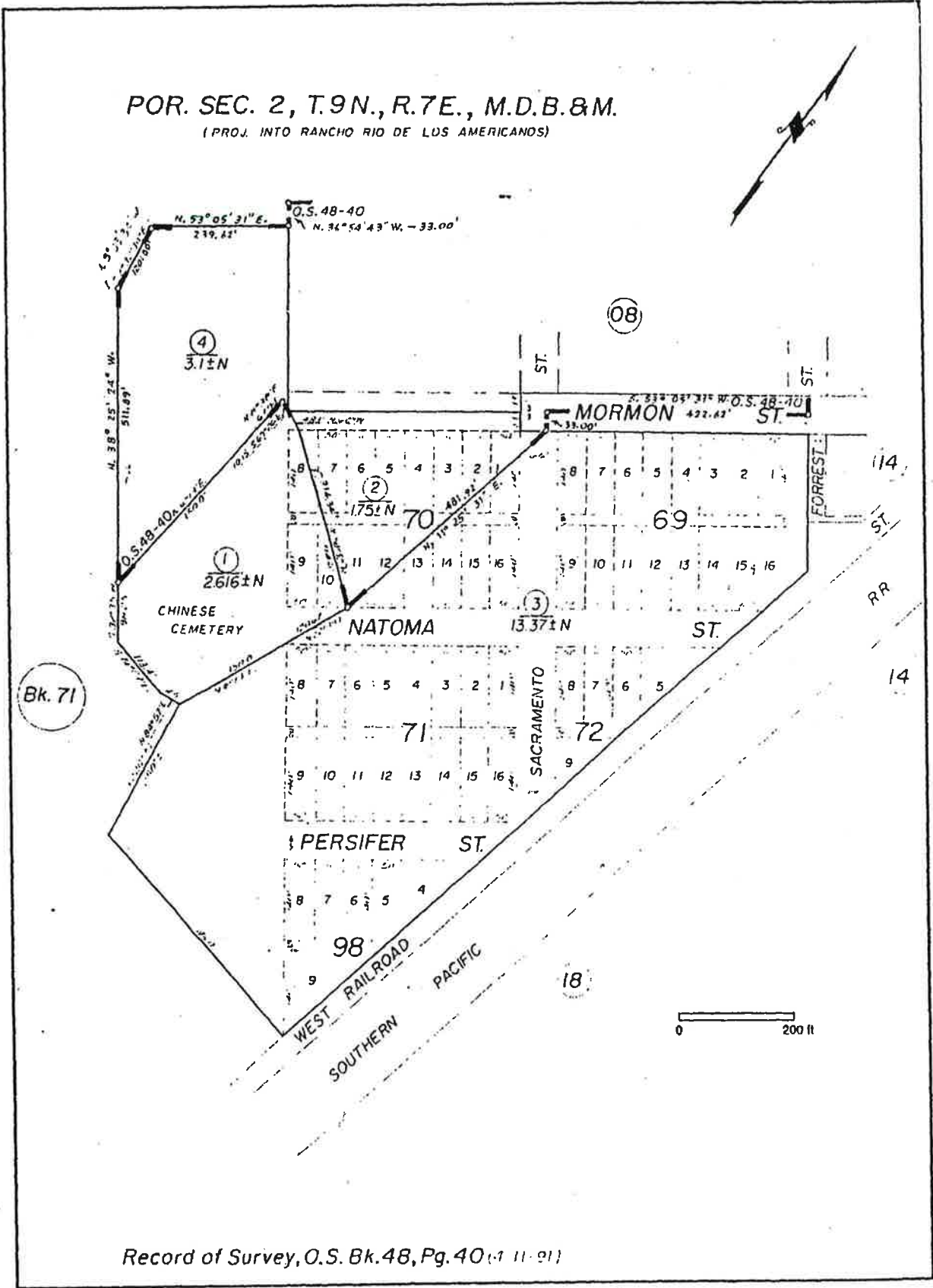
Map 5. Sketch of Chung Wah Cemetery Shrine, 1994

Map 6. Major Provinces of China and Selected Districts within Guangdong Province
(after Minnick 1988:3)





Map 7. Areas of Chinese Use in Folsom and Association-Owned Property



Map 8. Chung Wah Cemetery Boundaries

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National Park Service

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Continuation Sheet

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SUPPLEMENTARY LISTING RECORD

NRIS Reference Number: 95000999

Date Listed: 8/21/95

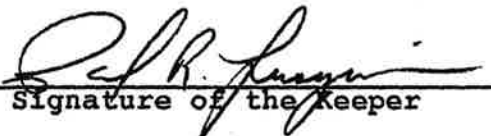
Chung Wah Cemetery
Property Name

Sacramento
County

CA
State

N/A
Multiple Name

This property is listed in the National Register of Historic Places in accordance with the attached nomination documentation subject to the following exceptions, exclusions, or amendments, notwithstanding the National Park Service certification included in the nomination documentation.


Signature of the Keeper

8/21/95
Date of Action

=====
Amended Items in Nomination:

Significance:

The areas of significance are revised to read: Ethnic Heritage-Asian and Social History in order to correspond with the site's well-documented traditional cultural use by local Chinese residents over an extended period.

This information was confirmed with Marilyn Lortie of the California SHPO.

DISTRIBUTION:

- National Register property file
- Nominating Authority (without nomination attachment)



CITY OF FOLSOM

HISTORIC PRESERVATION MASTER PLAN

November 5, 1998

Draft October 20, 1998

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Appendix B	Information Packet for General Distribution
Appendix C	City of Folsom, Cultural Resource Inventory and Registration Form
Appendix D	List of Cultural Resources
Appendix E	Map of Cultural Resources

Acknowledgements

The Historic Preservation Master Plan committee as listed below prepared this document. The committee wishes to thank all of the individuals who assisted in the development and review of this document, especially David Storer, Director of the Department of Planning, Inspection, and Permitting, who provided City staff support to the committee. The committee further wishes to thank the City Council and former Mayor Glenn Fait for their efforts toward protecting and preserving the rich history of our community.

- Cindy Baker: Historic Residences
- Stan Gisler: Folsom Historical Society
- Patrick Maxfield: Planning Commission
- Candy Miller: Historical District Commission and Historical Area Architectural Review Commission
- Chuck Thurman: Gold Mining History
- Dallas Grenley: Friends of the Folsom Powerhouse Association
- Sue Mehrten: Cattle Ranching History
- Tom Hickey: Folsom Prison Museum
- Bob Minshew: Folsom, El Dorado and Sacramento Historical Railroad Association, Pacific Coast Chapter, Railway and Locomotive Historical Society

Introduction

During the State of the City Report delivered at the January 14, 1997 City Council meeting, Mayor Glenn Fait proposed that Council convene an ad hoc committee of representatives from historic preservation groups and others with similar interests in City of Folsom. The purpose of this committee was to develop a comprehensive Historic Preservation Master Plan to ensure that historic preservation is given proper consideration as the City continues to grow.

On March 25, 1997, the City Council passed and adopted Resolution No. 5346, a resolution establishing an ad hoc committee for the preservation of Folsom's history to develop a comprehensive historic preservation master plan. Mayor Fait appointed 12 committee members representing different aspects of Folsom's history.

This committee began meeting on November 19, 1997 in order to establish the plan, along with an inventory of cultural resources to present to the City Council. The following is that plan and inventory, along with recommendations for preservation action.

The committee's intention was to create an inventory that shall be updated over time. Listing on the inventory shall be accessible to all members of the community using an inventory nomination or registration form. Standardized forms will be available through the City of Folsom Planning, Inspections and Permitting Department (PIP) and accessible to the general public. Completed forms will then be submitted for review by PIP staff and then to the Historic District Commission for determination of eligibility.

The current Cultural Resources Inventory and a complete set of registration forms, including maps and photographs, shall be kept by City of Folsom PIP Department. Archaeologically sensitive or historically sensitive information will be considered as such and held back from public distribution if that distribution might endanger the resource through vandalism or theft, or for privacy considerations.

Goals and Objectives

- *Mission Statement*

The City of Folsom by this Master Plan seeks to preserve and promote its rich and colorful historical heritage by protecting its cultural and historical resources and educating the citizenry and general public of its past.

- *Ongoing Objectives*

1. To **identify** historical resources, including sites, structures, archaeological resources, documents, and artifacts,
2. To **protect and preserve** these resources for future generations,
3. To **educate** the public about these resources through interpretation and exhibition in the hopes that knowledge will result in a greater respect for and appreciation of these resources,
4. To **promote** Folsom's history through tourism and local programs in the hopes of enriching the city through cultural and economic development.

Area of Preservation

The City of Folsom shall extend this preservation policy throughout the City of Folsom limits and other areas outside the city limits that were part of Folsom's historical sphere of influence, including areas such as Mormon Island, Prairie City, and Aerojet.

Objective Actions

- *To identify historical and cultural resources, including sites, structures, archaeological resources, documents, and artifacts,*

An inventory of cultural resources shall be maintained and available to the public by the City through the PIP Department. This list shall be derived from the inventory submitted herein, as well as extant publications and reports, and updated systematically with the receipt of new and ongoing environmental documentation and historical publication. A registration or nomination form for applying and adding to the inventory shall be adopted so that the City, individuals, and organizations will be able to apply for listing of a resource with the City of Folsom.

- *To protect and preserve these resources for future generations,*

The City of Folsom shall consider the protection, preservation, documentation and/or recordation of cultural resources on the inventory when decisions affecting these resources come before the City. This includes notification of the Planning Commission, or any other decision-making body, of a resource's inclusion in the inventory as part of that body's staff report. The City, when possible, should consider the acquisition and storage or deposition of artifacts or resources when feasible or practicable.

The City of Folsom, in seeking to preserve the history and development of the municipality, shall maintain its system of records management whereby City documents are regularly cycled from active use, to inactive storage, to archival storage. The City shall designate an archival storage facility for said purpose.

These documents should at a minimum include the records of the City decision-making bodies, including the City Council and all commissions and public meetings or hearings. Other records should include the logs of the Police and Fire Departments, the PIP Department maps, plats, and other documents that will help plot the history and development of Folsom. Copies of the Department of Parks and Recreation flyers, brochures, and memorabilia should also be archived.

- *To educate the public about these resources through interpretation and exhibition in the expectation that knowledge will result in a greater respect for, recognition of and appreciation of these resources*

The City of Folsom shall recognize and support the important role of education and promotion in meeting its goals of preservation and protection. Further, the City shall recognize and support the importance of our heritage in the economic and cultural development of the community. The City shall suggest ways to make information about the resource interesting and readily available to the public and to promote tourism.

- *To promote Folsom's history through tourism and local programs in the hopes of enriching the city through cultural and economic development.*

The City shall continue to promote its cultural resources through tourism, events, and special programs. The City shall also cooperate with local historical interest groups to meet all the goals of the Historic Preservation Master Plan.

Cultural Resources Inventory

- *Processes for Applying to and Maintaining the Inventory*

A Cultural Resources Registration or Nomination Form shall be completed and submitted to the PIP Department staff for consideration. The PIP Department shall apply the criteria above in determining whether or not the resource appears to be eligible for the Inventory. The PIP Department shall then submit all applications with their recommendations to the Historic District Commission.

The Historic District Commission shall then consider the application and determine if it is eligible for listing on the Inventory. If an application is determined ineligible by the committee, the applicant may appeal to the City Council for consideration.

Responsibility for maintaining the inventory will be assigned to the Historic District Commission. PIP staff shall provide staff support to this committee. New additions to the Cultural Resources Inventory shall be forwarded to the Planning Commission for consideration in actions before that commission. PIP shall be responsible for notifying the Planning Commission of any cultural inventory listings in active project areas as part of its normal staff report to commissioners.

Listing on the City of Folsom Cultural Resources Inventory does not grant any special privileges or impose any restrictions on private property rights. Rather it recognizes the resource as significant in Folsom's cultural and historical heritage and may assist the property owner in obtaining awards or financial benefits from outside agencies. Listing may also grant a priority to a resource in applying for City of Folsom Redevelopment Agency funding for preservation or promotion. Listing may also provide for application of the more flexible Historic District Building Code to structures outside the Historic District, if the owner of the property so desires.

PIP shall notify the owner of the resource that it is being considered for nomination to the Inventory. The owner shall be provided with a copy of the application and given 90 days to comment on the nomination. No property shall be listed on the Inventory over the notarized objection of the owner. However, properties may still be registered as eligible for the Inventory.

Public Education

The City shall make available to the public free of charge a general information handout on the City of Folsom Cultural Resource Inventory, its impacts, and procedures for distribution to the general public (see Appendix B for example). The City shall also make available free of charge an instructions packet for individuals interested in nominating a resource to the inventory.

Mapping

A map indicating the location of the resource shall be submitted with the nomination or registration form. In order to establish standardization of the inventory and to avoid confusion, the applicant shall use the most current USGS 7.5' (minute) series topographical quadrangle map with the resource indicated in pencil, not ink. Coordinates of the resource shall be written in the margin of the map.

Identification

Identification of the resource shall be accomplished using the nomination form, inventory, USGS map, and photographic documentation. Black and white photographs and negatives shall be submitted to the PIP once a resource has been accepted to the inventory.

ADDITIONAL RECOMMENDATIONS FOR IMPLEMENTING THE GOALS AND OBJECTIVES

General Public Education

- A coordinating council/committee could be established to follow upon the implementation of these recommendations. Said council/committee would be representatives from the various historical groups and possibly city staff. This body would seek to coordinate the efforts of their representative groups by reporting upcoming events, issues and concerns and hopefully coordinating event planning and mutual support.
- **Initiate Historic Tours.** The Department of Parks and Recreation could institute periodic tours of the following: historic district residences, historic cemeteries, river-front nature trails in the following segments (1) from the zoo to the rainbow bridge focusing on the geologic history of the ravine, the natural botanical array, and the ditch and canal; (2) from the powerhouse to Lake Forest Café or Willow Creek Park access focusing on the Nisenan history, Chinatown history, Negro Bar mining town, Texas Hill mining town, placer bedrock sluicing and Natoma Company dredging tailings; (3) from Rainbow Bridge east along the bike trail on the north bank discussing the original toll bridges, the Stockton Flour Mill, Folsom Prison, and Folsom Dam. Similar tours conducted in kayaks or canoes could be developed on Lake Natoma in the same general configurations in conjunction with kayak rental facilities already in operation at Negro Bar State Park.
- Install a Historical Display Area in public waiting areas or lobbies of City Hall for changing historical exhibitions and notice of upcoming historical promotion events.

- Organize Folsom History events commemorating significant periods in Folsom's history (i.e. inauguration of the SVRR, etc.). Themes could change annually (i.e. Chinese history year, railroad history year, etc.) or be fixed (founding of Folsom City) and could include an historical theme parade, like the former Pioneer Days parade. Schools, community groups, and residential subdivisions would be encouraged to enter floats and entries with a theme relevant to the history of Folsom.
- Encourage local merchants and restaurants to name products and menu items after specific or general aspects of Folsom's past (i.e.: the Leidesdorff Burger, etc.)
- Install vista points at various overlooks in Folsom such as at top of Folsom Point (formerly Dyke 8) entrance looking south and southeast, another at the Folsom Dam Overlook, another near the new high school site, also at the Rainbow Bridge/ Historic Trestle Bridge site.
- Create and distribute public brochures, signage, and other materials for public information.
- New development street and project names should, if possible, have an historical linkage to the project area under development. These could be ranch names, local ditch or mining site names, historical personages, or general historical land use of the area.
- Establish Historic District Signage, such as a standardized program for residents of the historic district to apply to the City for a bronze or some other kind of sign identifying the history of their house. These would be keyed into a walking tour brochure, self-guided.
- Develop a Ranch Center at the Broder Ranch property. Move historical buildings from the Wilson Ranch to this center for historical interpretation/living history of Folsom's cattle ranching, farming/agricultural, and rodeo history. Site could also develop equine center for recreation and historical interpretation.
- Establish appropriate archival storage facility for record storage

Youth Programs

- Establish essay contests, like a local history day program, judged by City officials and awarded through a monetary scholarship or award and plaque (\$100 or less). The winning essay could then be published in the Folsom Telegraph, the City of Folsom Newsletter, or some other local periodical.

- Consider creating an Official Folsomite Card program: This program is designed to engage families (the greater majority of new Folsom residents) in actively learning about Folsom's history together. Children participating would ask the Department of Parks and Recreation for an application to become an official Folsomite. The application would include a general knowledge test and a list of historical sites in Folsom with a check-off list. The child would be required to pass the general knowledge test and visit three local sites of historical interest. The signature of an adult, either a parent, museum personnel, etc would verify visits. The test and visit list would then be returned to the Department of Parks and Recreation. The child would receive a business card-size "Official Folsomite" card and thereby become card-carrying Folsomite. This activity would encourage parents to spend their limited leisure time actively learning about Folsom.

APPENDIX A

DETERMINING ELIGIBILITY FOR CITY OF FOLSOM- CULTURAL RESOURCES INVENTORY

DETERMINING ELIGIBILITY

In order to be listed on the inventory, resources need to meet a level of significance within Folsom's history. By adopting the Secretary of the Interior's standards for the National Register of Historic Places to a local level, it is possible to establish a set of criteria for inclusion on the inventory.

Cultural resource significance is evaluated in terms of a resource's eligibility for listing in the National Register of Historic Places (36 CFR 60.6 [48 R 46306]) as outlined below.

The quality of significance in American history, architecture, archaeology, and culture is present in districts, sites, buildings, structures, and objects of state and local importance that possess integrity of location, design, setting, materials, workmanship, feeling, and association; and,

- That are associated with events that have made a significant contribution to the broad pattern of our history; or
- That are associated with the lives of persons significant in our past; or
- That embody the distinct characteristics of a type, period, method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction, or;
- That has yielded, or may be likely to yield, information important in prehistory or history.

Sites younger than 50 years, unless of exceptional importance, are not eligible for the National Register.

An integral part of assessing cultural resource significance, aside from applying the above criteria, is the physical integrity of the resource. Prior to assessing a resource's potential for listing on the National Register, it is important to understand the subtleties of the seven kinds of integrity mentioned above. To summarize a National Park Service (NPS) bulletin, entitled *How to Apply the National Register Criteria for Evaluation* (1991:44-48), the types of integrity are defined as:

- **Location** is the place where the historic property was constructed or the place where the historic event occurred;
- **Design** is the combination of elements that create the form, plan, space, structure and style of a property;
- **Setting** is the physical environment of a historic property;

- *Materials* are the physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property;
- *Workmanship* is the physical evidence of the crafts of a particular culture or people during any given period in history or prehistory;
- *Feeling* is a property's expression of the aesthetic or historic sense of a particular period of time; and
- *Association* is the direct link between an important historic event or person and a historic property.

Integrity is based on significance: why, where, and when a property is important. Only after significance is fully established is the issue of integrity addressed. Ultimately, the question of integrity is answered by whether or not the property retains the identity for which it is significant. A resource must have at least two types of integrity and meet one of the four criteria listed above in order to qualify for the Cultural Resources Inventory.

NOTE:

Guidelines and further information for determining what is or is not culturally significant are available through the National Park Service in Washington, D. C. and the State Office of Historic Preservation in Sacramento. Using these guidelines, the Commission shall seek to apply the standards at the local level, not at the national or state level, of significance. The applicant should be encouraged to seek state or national recognition if they feel the property merits that consideration.

APPENDIX B

**INFORMATION PACKET
FOR GENERAL DISTRIBUTION**

**THE CITY OF FOLSOM
CULTURAL RESOURCE INVENTORY
PURPOSE AND PROCEDURES**

TABLE OF CONTENTS

Introduction

- What is the Cultural Resource Inventory?
- Questions and Answers

The Nomination Process

- Procedures
- Appeals and Removals

Introduction

What is the Cultural Resources Inventory?

- The Cultural Resources Inventory is a guide to the City of Folsom's significant historical and archaeological resources.
- The City of Folsom has designed this program for use by private groups and citizens to identify, evaluate, register and protect Folsom's cultural resources.
- The Cultural Resources Inventory program encourages public recognition and protection of resources of architectural, historical, archaeological and cultural significance, identifies historical resources for City planning purposes and determines eligibility for Redevelopment Agency preservation grant funding.

Questions and Answers

What protection does listing in the Cultural Resources Inventory offer?

- Listing in the Inventory does not guarantee that a resource will not be preserved. Projects that will affect resources may be subject to review pursuant to the California Environmental Quality Act (CEQA). All projects carried out by public agencies are subject to environmental review (require preparation of an environmental impact report) as are projects involving privately owned properties when a permit or review is required. For questions related to a specific resource and CEQA review, please contact the local government or state agency with jurisdiction over the project.

How does the Inventory define a cultural resource?

- For the purposes of listing, a “cultural resource” includes, but is not limited to, any object, building, structure, site, area or place which is historically or archaeologically significant, or is significant in the architectural, engineering, scientific, economic, agricultural, education, social, political, military or cultural annals of Folsom. Historical resources may be eligible for listing in the inventory if they satisfy the criteria established by the City of Folsom for this program.

What are the criteria for listing a resource in the Inventory?

- While the significance criteria for the Inventory is similar to that used by the National Register of Historic Places and the California Register of Historical Resources, this inventory will document the unique history of Folsom. The resource must:
 - 1) Be associated with events contributing to the broad patterns of Folsom’s history and culture;
 - 2) Be associated with historically important people;
 - 3) Embody distinctive characteristics of a type, period, region or construction method, or represent the work of a creative individual; or
 - 4) Have the potential for yielding important information in Folsom’s history or prehistory.

How does a resource become listed in the Inventory?

Resources may be added in two ways:

- 1) Some are automatically eligible, including those determined eligible for the National Register of Historic Places, the California Register of Historical

Resources, and California State Historical Landmarks pertaining to Folsom and its historical sphere of influence.

- 2) Other resources can be nominated to the Inventory by private organizations or citizens.

Does listing on the Inventory affect property rights?

- Listing on the City of Folsom Cultural Resources Inventory does not grant any special privileges or impose any restrictions on private property rights. Rather it recognizes the resource as significant in Folsom's cultural and historical heritage and may assist the property owner in obtaining awards or financial benefits from outside agencies. Listing may also grant a priority to a resource in applying for City of Folsom Redevelopment Agency funding for preservation or promotion. Listing may also provide for application of the more flexible Historic District Building Code to structures outside the Historic District, if the owner of the property so desires.
- Cultural resources nominated to the Inventory may not be listed over the written notarized objection of the property owner. However, a resource whose owner objects to the listing may still be formally determined eligible for the Inventory. Property owners must be notified, sent a copy of the nomination and provided the opportunity to comment on the nomination. Property owners may specify that specific records of their property be confidential and not for public distribution.

What are the benefits of being listed in the Inventory?

- The Historic Building Code may be applied when the use of the Uniform Building Code threatens the historical integrity of a resource. Also, resources included on the Inventory will receive priority consideration for any funds available from the City of Folsom for historic preservation. Further, City of Folsom staff shall include a resource's eligibility to the inventory in its reports to City Council and all other decision-making bodies.

Where do I get more information on historical resources?

Further information about historical resources in Folsom can be obtained from the Folsom History Museum and Folsom Historical Society, the Folsom Chamber of Commerce, the Folsom Railroad Museum, the Folsom Prison Museum, and the Folsom Power House. These resources can also direct you to other agencies, museums, and repositories available in the region.

The Nomination Process

PROCEDURES

Cover Letter

- A cover letter must be submitted with the nomination and must include the names and addresses of the applicant and the resource owner, along with a concise statement of significance. The statement of significance should clearly list the justification for the importance of the historical resource.

Nomination Submission

- The application shall be submitted to the City of Folsom Department of Planning, Inspections, and Permitting (PIP), who will then review the documents for completion and apply the criteria to the application. PIP will also notify the resource owner within 30 days of receipt of the application. After the 90-day waiting period for owner comment, or as soon as comment is received, PIP will then forward the application and comments, along with its recommendations, to the Historic District Commission.
- The Historic District Commission will then consider and determine the resource's eligibility for inclusion on the Inventory.

City of Folsom Review

- The applicant, owner, interested parties, and the general public will be notified by PIP of time, date and location where the Historic District Commission will consider and determine the resource's eligibility. Notification will be sent at least 21 days prior to the hearing and decision. Following the hearing and decision, notification letters will be sent within 45 days to the applicant and owner(s) informing all entities of the Commission's decision and of procedures for requesting an appeal.
- PIP will notify all resource owners of the nomination within 30 days of receipt of the nomination. If the resource owner objects by notarized letter to the nomination, the resource cannot be listed in the Inventory, but can still be formally determined eligible for listing by the Historic District Commission.

Appeals and Removals

Request for Appeal

- Request for an appeal must be made to PIP in writing within 30 days of the Commission's decision. Only an error in the facts presented or an error in judgement by the Commission may be grounds for an appeal. Within 60 days of receipt of a request for an appeal, the Commission will advise the applicant if it will consider the request and, if so, the time and place where the hearing will take place. If the request for an appeal is denied, the applicant may make one final appeal in public hearing before the City Council.

Removal from the Inventory

- The Historic District Commission or its successor may remove a resource from the Inventory if the resource, through demolition, alteration or loss of integrity, has lost its historic qualities or potential to yield information or if new information or analysis shows that the resource was not eligible at the time of its listing. Requests for removal must be made to the Commission in writing and must include a detailed justification for removal, photographs, other documentation regarding the current condition of the resource, and complete ownership information. Requests for removal are subject to the same notification and Commission hearing process as those for nomination.

APPENDIXC

**CITY OF FOLSOM
CULTURAL RESOURCES INVENTORY
REGISTRATION FORM**

CITY OF FOLSOM CULTURAL RESOURCES INVENTORY REGISTRATION FORM

1. Name of Property

historic name: _____
 other name/site number: _____

2. Location

street & number: _____ not for publication
 city/town: Folsom vicinity
 state: California code: CA county: Sacramento zip code: 95630

3. City Agency Certification

As the designated authority under the City of Folsom Master Historic Preservation Plan of 1998, as amended, I hereby certify that this nomination request for determination of eligibility meets the documentation standards for registering properties in the City of Folsom Master Historic Preservation Plan and meets the procedural and professional requirements set forth in _____. In my opinion, the property meets does not meet the City of Folsom criteria. † recommend that this property be considered significant nationally statewide locally. (see continuation sheet for additional comments.)

 Signature of certifying official

 Date

 City agency and commission

In my opinion, the property meets does not meet the City of Folsom criteria. (See continuation sheet.)

 Signature of commenting or other official

 Date

 City of Folsom agency and commission

4. City of Folsom Certification

I hereby certify that this property is:

entered in the Cultural Resources Inventory

See continuation sheet.

determined eligible for the Cultural Resources Inventory

See continuation sheet.

determined not eligible for Cultural Resources Inventory

removed from the Cultural Resources Inventory

other (explain): _____

 Signature of Keeper of the Inventory

 Date

5. Classification

Ownership of Property

(check as many boxes as may apply)

- private
- public-local
- public-State
- public-Federal

Category of Property

(check only one box)

- building(s)
- district
- site
- structure
- object

Number of Resources within Property

Contributing

Noncontributing

_____	_____	buildings
_____	_____	sites
_____	_____	structures
_____	_____	objects
_____	_____	Total

Number of related multiple property listing

(Enter "N/A" if property is not part of a multiple property listing)

Number of contributing resources previously listed in the Cultural Resources Inventory

6. Function or Use

Historic Functions

(Enter categories from instructions)

Current Functions

(Enter categories from instructions)

7. Description

Architectural Classification

(Enter categories from instructions)

Materials

(Enter categories from instructions)

foundation _____

roof _____

walls _____

other _____

Narrative Description

(Describe the historic and current condition of the property on one or more continuation sheets.)

8. Statement of Significance

Applicable Cultural Resources Inventory Criteria

Mark "x" in one or more boxes for the criteria qualifying the property for Cultural Resources listing.)

- A** Property is associated with events that have made a significant contribution to the broad patterns of our history.
- B** Property is associated with the lives of persons significant in our past.
- C** Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- D** Property has yielded, or is likely to yield, information important in prehistory or history.

Criteria Considerations

(Mark "x" in all the boxes that apply)

- A** owned by a religious institution or used for religious purposes.
- B** removed from its original location.
- C** a birthplace or grave.
- D** a cemetery.
- E** a reconstructed building, object, or structure.
- F** a commemorative property.
- G** less than 50 years of age or achieved significance within the past 50 years.

Areas of Significance

(Enter categories from instructions)

Period of Significance

Significant Dates

Cultural Affiliation

Architect/Builder

Narrative Statement of Significance

(Explain the significance of the property on one or more continuation sheets.)

9. Major Bibliographical References

Bibliography

(Write the books, articles, and other sources used in preparing this form on one or more continuation sheets.)

Previous documentation on file (City):

Primary Location of Additional Data:

- preliminary determination of individual listing has been requested
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey # _____
- recorded by Historic American Engineering Record # _____

- State historic preservation office
- Other state agency
- Federal agency
- Local government
- University
- Other

Name of Repository: _____

10. Geographical Data

Acreage of Property _____

UTM References

(Place additional UTM references on a continuation sheet.)

Zone	Easting	Northing	Zone	Easting	Northing
------	---------	----------	------	---------	----------

See continuation sheet

Verbal Boundary Description

(Describe the boundaries of the property on a continuation sheet.)

Boundary Justification

(Explain why the boundaries were selected on a continuation sheet.)

11. Form Prepared By

Name/Title: _____

Organization: _____

Date: _____

Street & Number: _____

Telephone: _____

City or Town: _____

State: _____

ZIP: _____

**CITY OF FOLSOM
CULTURAL RESOURCES INVENTORY
CONTINUATION SHEET FOR ADDITIONAL INFORMATION**

Section number ____ Page# ____ Property Name _____

SAMPLE

APPENDIX D

**CITY OF FOLSOM
PRELIMINARY CULTURAL RESOURCES
INVENTORY**

CITY OF FOLSOM
PRELIMINARY CULTURAL RESOURCES INVENTORY
 (Numerical Index to Cultural Resources Map)

Ethnographic Features – Native American

1. BRM locations along American River below Rainbow Bridge

Historical Buildings/ Structures/ Features – Transportation-Related

2. Sacramento Valley Railroad Grade, factual date 1855
3. Granite Block Culvert beneath Folsom Boulevard near Willow Creek State Park, factual 1855
4. Alder Creek Trestle
5. SVRR/CPRR turntable site on Railroad Block, National Register Property, factual dates 1856, 1867, 1900.
Archaeological deposits on Railroad Block, circa 1856-1870
6. Alder Creek Depot Building, circa 1890s
7. Station Master's House near Wye Junction, circa 1920s
8. Ashland Depot, National Register Property, circa 1860s
9. Folsom Depot, National Register Property, factual 1906
10. Kinsey Bridge Abutments, circa 1850s
11. Rainbow Bridge, NRHP eligible, factual 1917
Steel Truss Bridge, factual 1983-1930
12. Sacramento, Placer and Nevada Railroad ROW, factual 1862
Railroad grade along Oak Avenue Parkway near Cascade Falls
13. California Central ROW, Folsom to Lincoln Railroad grade
Wye junction at Bidwell and Folsom Boulevard
14. Ashland townsite
15. Placerville and Sacramento Valley Railroad ROW
16. Folsom Dam
17. Stone building remnants

Historical District Cultural /Architectural Resources

18. Granite pillars from State Capitol grounds
19. Granite School, circa 1900
20. Figueroa Street Bridge, between Riley and Wool, factual 1916
21. Sutter Street Historic Commercial District, 600-900 blocks of Sutter Street
Historic Residential Area
22. Emma's

Historical Cemeteries and Churches:

23. St. John's Catholic Church, est. circa 1855
24. Trinity Episcopal Church, est. circa 1860
25. Landmark Baptist Church, est. circa 1855
26. St. John's Catholic Cemetery, established circa 1855
27. Odd Fellows and Mason's Cemeteries, est. circa 1856
Remainder of Lakeside Cemetery, est. circa 1850s
28. Chung Wah Cemetery, NRHP property, est. circa 1850s
29. Young Wo Cemetery, CHL, est. circa 1870s
Mormon Island Cemetery

Previously surveyed Structures:

30. a) 305 Scott Street, Cohn House, NRHP property, factual 1860, alt. 1895
b) 607 Sutter Street, original library, circa 1915
c) 701 Sutter Street, Murer Gas Station, circa 1920
d) 707, 709, 711, 713 Sutter Street, Commercial buildings, circa 1860
e) 917, 921, 923 Sutter Street, Chinese Laundries and residences
31. Stockton Flour Mill site and remnant foundations, circa 1856
32. Giuseppe Murer House

Historic Structures, Industrial/Energy

33. Folsom Hydroelectric National Historic Landmark, CA-Sac-429H
Powerhouse 1, NRHP Property, CHL, est. 1895
Powerhouse 2, NRHP Property, CHL
Twin Mines/ Gray Eagle Mine
34. Livermore sawmill foundation remnants and mill pond
35. Diversion Dam and Powerhouse, Folsom Prison
Canal (1.5 miles) and main Gates, Livermore operation
Gas plant archaeological remains, circa 1860
Granite Quarry, Folsom Prison
Other granite quarry sites
36. Aerojet and aerospace industrial operation

Historic Features, Mining-related Resources

37. Walltown gold mines and ditch network
38. Natoma Ground Sluice diggings, Hwy. 50
39. Placer Sluicing pits, tailing piles, ditches and drains, Lake Natoma
40. Dredger Tailing Piles representative of different dredging technology episodes
41. Natoma Water and Mining Company ditches and reservoirs
42. Mining adits and tunnel portals, Lake Natoma
43. Tate's (aka Teat's) Flat Ditch
44. Alder Creek Pump House remains

45. Negro Bar townsite, 1849-1856
46. Texas Hill townsite, 1849-1856
47. Prairie City townsite, 1850-1856
48. Mormon Island townsite, 1850-1945
49. Pratt Rock narrow-gauge railroad grade
50. Eucalyptus and olive grove experimental reclamation project property
51. Willow Spring Hill Diggings
52. Humbug and Willow Creeks Mining Corridors
53. Hydraulic mining sites American River bike trail across from City Park
Hydraulic mining areas
54. Negro Hill
55. Chinatown Site
56. Chinese mining site

Historic Structures, Sites – Agricultural/Ranching-related

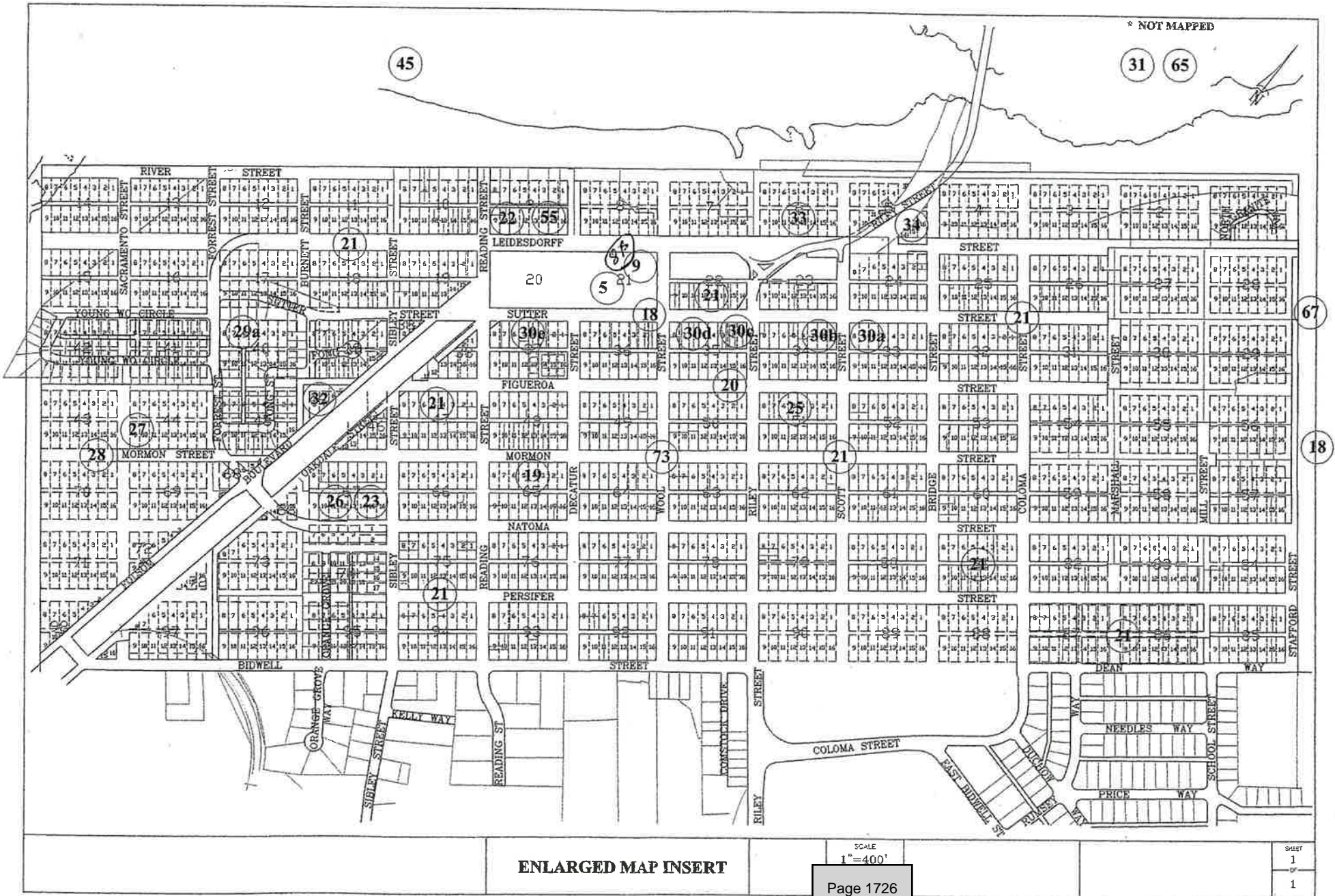
57. Broder Ranch Complex
58. Russell Ranch Complex(with old horse barn)
59. Smith Ranch
60. Wilson Ranch (1850s house and barn)
61. Olive Orchard east of Folsom-Auburn Road north of Oak Avenue
62. Salmon Falls townsite

Points of Local Interest

63. Natoma Grove
64. Dredge/Natoma townsite
65. Folsom Institute Site
66. Folsom High School (original Hall/wing)
67. Rodeo Arena site
68. John Kemp House
69. Clarksville

Views, Viewsheds, and Landscapes

70. Oak Canopy on Folsom Boulevard between Blue Ravine and Factory outlets
Folsom Historic District from Greenback looking southeast from northwest corner
of Negro Bar State Park.
71. River and gorge looking upstream from Rainbow Bridge
72. River and bluffs looking downstream from new bridge
American River drainage from new high school site looking west.
73. Shoot-out site at Wool and Mormon Streets



* NOT MAPPED

45

31

65

67

18

ENLARGED MAP INSERT

SCALE
1"=400'

Page 1726

SHEET
1
OF
1

Attachment 8

Public Comment Letters Received After the April 26, 2022 Hearing



Folsom City Council Meeting

Additional Information Transmittal

MEETING DATE:	4/26/2022
AGENDA SECTION:	Public Hearing
STAFF REPORT TITLE	Agenda Item # 8- Appeal by Igor Semenyuk of a Decision by the Historic District Commission Denying a Conditional Use Permit for the Lakeside Memorial Lawn Crematorium project (PN 19-182) located at 1201 Forrest Street
FROM:	Community Development Department

Staff is providing the attached additional information for the above-referenced agenda item.

- Public comment letters received by Staff since the publication of the City Council staff report.

Instructions to staff: Deliver original and 30 stapled/double-sided copies to the City Clerk's Department; City Clerk's Department will distribute via email and hardcopy to City Council, City Manager, City Attorney, and City Clerk.

Updated: Jan 2022

April 20, 2022

Council Members

Opposition to the Lakeside Crematorium

Disagreement with Primary Use

My name is Dave Higgins and I oppose the crematorium.

During the Historic District Commission hearing, 02-16-2022 Igor Semenyuk stated the crematorium is an accessory to the existing cemetery, as does the memo from Helix dated 02-15-2022.

In the Helix Memo dated 02-15-2022, page 3-4 states:

“In this case, the cemetery is the primary or principal and the applicant is proposing a crematorium as an accessory use to the existing cemetery. As proposed, the crematorium would be subordinate to area, extent and purpose to that of the existing cemetery.

It would provide a service related to and supportive to the service already provided by the cemetery and the mausoleum. It would be located on the same lot and in the same zoning district as the principal use. It would be owned and operated by the same people who own and operate the existing cemetery and mausoleum. As such, a crematory can be considered as an accessory use subject to a CUP”

So when the cemetery reaches capacity for in ground burials and the mausoleum niches are full is the crematorium still accessory to the cemetery? It is my opinion the crematorium will be the primary business immediately after approval. After more than 150 years of ground burials one would think the cemetery is full now.

I disagree. In my opinion the crematorium becomes the primary or principal use.

Using Igor Semenyuk’s data from his powerpoint presentation during the HDC hearing he states that:

70% of California’s population currently choose cremation as an disposition choice

54% of Folsom residents choose cremation.

In a letter **“Scope of Work - Installing a crematory”** dated 02-27-2020, Igor Semenyuk states:

“Since 2019, the City of Folsom has an estimated 450 deaths a year and rising as the population increases. That is an estimated 300 cremation needed to be performed every year”

What these statements tell me is that cremations are increasing. Over 50% of Folsom residents are choosing cremations. 2019 data, 450 Folsom deaths a year with 54% (243) Folsom residents choosing cremations. Igor's estimate is 300 needed to be performed. That leaves roughly 250 cremations from outside Folsom. Miller Funeral needs to import 250 corpses to the Historic District to fulfill the 500 cremations a year. Does Folsom want or need that kind of distinction?

Living directly across from the cemetery I witness an estimated one funeral a week, 52 per year. Some of the funerals are ground burials, the others are placements in the mausoleum. I speculate all the burials and mausoleum placements are not Folsom residents. However, this is still a far lower number than 500 cremations per year.

If the number of cremations exceeds the number of burials and mausoleum placements I believe the primary use becomes cremations. The cemetery is not the primary or principal entity anymore. Lakeside Cemetery becomes the accessory to the Crematory. As such, the crematory can not be considered an accessory; it becomes the principal and is not subject to a CUP. The Lakeside Cemetery does not have the ability or capacity to provide 500 burials or mausoleum placements a year. With the crematorium now becoming the new primary business I believe an environmental impact report (EIR) must be conducted.

Igor said they are running at a capacity of 500+ cremations a year now at some of their facilities. They are taking human remains from all over Northern California to their other facilities for processing. During the peak of Covid they received a temporary exception from Air Quality and were allowed to exceed the 800lbs a day, 500 per year numbers and burn more human remains.

Igor said Folsom needs a crematory so Miller Funeral Home can provide a necessary service to the community. What service and benefit is there when Miller Funeral Home begins importing human remains from the greater Sacramento Region to Folsom for processing? **How does the City of Folsom and the residents benefit?** The cemetery is tax exempt. I see no monetary gain for the City of Folsom. The only benefit is to Miller Funeral Home. There

are 16 other crematory businesses between Sacramento and Folsom. This is a very competitive industry. Igor is seeking that competitive advantage at the risk of Folsom residents.

Miller Funeral Home can find other locations outside the City of Folsom such as the commercial areas off Sunrise Blvd in Rancho Cordova, The Industrial Park off Latrobe Rd in El Dorado Hills, and Aerojet Industrial Park. There are other options. Miller Funeral Home does not want to invest in a practical manner. Miller Funeral home wants to go to the cheapest option, a metal tool shed.

Conditional Use Permit FMC 17.60.040

The establishment, maintenance, or the operation of the use or building applied for will or will not, under the circumstance of the peculiar case, be detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing or working in the neighborhood of such proposed use, or be detrimental or injurious to property and improvement in the neighborhood, or to the general welfare of the City. (prior code 3122.04)

Thank You for your time and consideration

David Higgins

April 22, 2022

Council Members

Opposition to crematorium

On March 14, 2022 on I-80 near Applegate a propane delivery truck was involved in an accident which resulted in I - 80 closure and nearby evacuations for 9 hours.

My Name Is Dave Higgins. I live in the Preserves neighborhood. I'm a retired Fremont fire captain with 30 years experience, 10 of which I was assigned to the HazMat Response Team. Three years as a Training Officer.

I have a serious concern about the fire safety and egress in the neighborhood.

Miller Funeral Home wants to store 500 gallons of highly flammable propane on property. In two separate tanks. Within 13 -14 feet of a 19 ft. exhaust stack operating at over 1000 degrees.

The Fire Chief stated he didn't see a problem with this.

The problem I see with this is access and visibility

In the **Hazards Section** of the Negative Declaration it states:

“As an existing facility Lakeside Memorial Lawn maintains adequate fire response infrastructure for both current operations and the proposed project.”

I am curious as to what fire response infrastructure there is to control a fire involving 500 gallons of propane.

There are no fire hydrants on the property.

The closest fire hydrant is 650 feet away on Forrest St.

The old metal shed is there to store tools and equipment.

What is the fire Infrastructure? Is it a fire extinguisher??

In section F **Emergency Response or Evacuation Plan;**

States:

“Project site meets the fire department standards for fire engine maneuvering, location for fire engine to fight fire, rescue access to the units ???, and fire hose placement to all sides of the building.”

I don't know what the statement **“rescue access to the units” means.** Storage units, apartment units????
Maybe staff or Helix can explain.

This statement was obviously written before all the construction at the site.

This statement does not take into account the stored 500 gallons of highly flammable propane.

It does not take into account the automatic security gate at the entrance of the cemetery.

It does not take into account the metal security fence and locked gate surrounding the maintenance grounds.

It does not take into account the wooden fence hiding the propane tank pad.

It does not take into account the trees and shrubbery surrounding maintenance grounds.

It does not take into account the Dredger Tailing pile.

There is no emergency access to the propane tanks

In my opinion, Emergency response and mitigation will be delayed. Site access has become very difficult because of the obstacles created. Fire personnel will have difficulty identifying the nature of the incident because of hampered visibility.

Due to the difficulty in ACCESS and VISIBILITY fire apparatus and other emergency vehicles will be parked or staged in the street.

This would effectively block egress out of the Preserves Neighborhood trapping the community.

The difference between the Lakeside Cemetery propane tanks and the propane tanks you see throughout the City is access.

The other tanks are approachable from 360 degrees and visible from far better distances.

An Incident involving one of these propane tanks would have early detection and timely 911 notification.

People can self evacuate.

These tanks are not within 13-14 feet of an 1000 degree ignition source.

These other locations create a far safer planning and firefighting environment.

My suggestion, should this proposed project get approved, would be to require the energy source be **natural gas**. It would be extremely safer. The fuel is delivered underground and on demand. Removing the need to store 500 gallons of highly flammable propane gas near a 1000 degree exhaust stack. Natural gas would eliminate the need to have weekly propane deliveries in an already busy area, reducing the hazard further.

In the “**Environmental Setting**” section

There is a statement:

The project site not located in or near a State Responsibility area or lands classified a “Very High Fire Hazard Zone”

This is misleading. The project's environmental setting is located right next to a State Responsibility area.

Though the State Responsibility area is not classified as a **Very** High Fire hazard it is next to the recognized City of Folsom Community Wildfire Area which makes it a fire exposure.

The Community Wildfire Protection Plan, dated 2013, states the area around the City Corporation Yard and the East Natoma Lake Trail is a high fire priority for fuel management. This was proven true a couple of years ago with a wildfire at the entrance to the East Lake Natoma trail and Young Wo Circle. All we needed was a north wind (red flag weather) to make that situation worse.

So it's true it is not in a **Very** High Hazard Zone. It is near the City of Folsom High Fire Priority area. The threat of a wildfire is real.

In the webcast from the HDC hearing (at 1:34 minutes) The Fire Chief stated he had a hard time linking the evacuation wildland fire scenario and the Crematorium propane hazard scenario together.

The link is the location of the incident.

In the wildfire scenario, the incident is in the back of the neighborhood and the evacuation is directed away from the incident, past the cemetery. With the propane scenario the incident is at the evacuation exit, and will be likely blocked by emergency vehicles.

There are serious issues related to fire and life safety.

Miller Funeral Home needs to review their business plan. It would have been far cheaper to have originally invested in a zoned commercial site. Possibly in a neighboring community, close the Folsom. It would have protected the Miller Funeral Home Brand and Reputation from the frustration and anger fostered within this community and beyond.

Thank you for your time and consideration

Dave Higgins

From: Elaine Andersen
To: Josh Kinkade
Subject: FW: Please VOTE NO for the Lakeside Crematorium
Date: Thursday, April 21, 2022 7:34:32 AM
Attachments: SPO.pdf

From: Victoria Foster [REDACTED]
Sent: Wednesday, April 20, 2022 6:55 PM
To: Elaine Andersen <eandersen@folsom.ca.us>
Cc: Victoria Foster [REDACTED]
Subject: Please VOTE NO for the Lakeside Crematorium

You don't often get email from [REDACTED]. [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Elaine Andersen,

April 19, 2022

When the applicant first applied for the conditional use permit to start this new business – it was to burn 2 bodies a day- now it's already up to 4 bodies a day and the business hasn't even started. This application states it is putting the crematorium in an existing metal structure... a metal shed. They also are stating this is to help the growing demand for the community and of a religion where they need to be present during the cremation. I don't see how anyone can feel at peace standing inside a metal shed like this during the burning of their family member. I've seen nicer Rubbermaid sheds in backyards. So what would be next with the increasing demand?? A bigger and newer building to be put in this historic cemetery? You give an inch - they take a mile -It's the wrong location - it needs to be in an industrial area. So how big will that number grow too? A business needs to make money to sustain and like with any business, what does their business plan look like? What are their 3-5-10 -20 -year forecasts and projections? This business needs to be taken seriously and put in the proper place not just conveniently placed within a business he conveniently owns inside the **historic district**. With this demand growing how would anyone know if extra bodies aren't being cremated well past their allowed limit? You really think he's going to ask the city again for a license to increase the number of bodies to burn knowing what he's up against now? I don't think so! This business needs to be put in an industrial area that can increase with the demand and if Folsom doesn't have the industrial area, well than the applicant needs to find the next best location for it.

Also, I don't think anyone can guarantee that the emissions from mercury and unknown metals from the burning bodies won't affect the air we breathe. The air quality tests & data were done from the vicinity of the 2 Sacramento airport's- We are in a different geographical zone than that. I'm not an expert but common sense needs to come into play -the homes in The Preserves/ Lake Natoma Shores are on the American River/ Lake Natoma which is up against the canyon bluffs. This waterway is filled with wild- life, kayakers, paddle boarders and the trails filled with runners, bikers and families on strolls and into the cliffs, our protected bald eagles with 2 baby hatchlings. These massive sheer walls hold in the fog in the winter and smoke during fire season. How can you tell me it won't be any different for each burning body that's be expelled into the air with poisonous mercury? Some days during winter when it's foggy- we will be fogged in our neighborhood most the day - yet you drive out of our development and 1/4 mile away it's clear and sunny.

I and the neighbors are not coming to you as whiners and not just with the 'not in my neighborhood' mentality- we also come to you with facts. I'm a realtor for over 30 years and full time at Intero Real Estate services. Being next to a crematorium will absolutely affect our property values. As you know owning a house is a large investment and even more so a HOME where we live, breathe & raise our families. We as homeowners have purchased homes knowing a couple historical cemeteries were here, but not a business such as a crematorium - that's totally different. Some homeowners stated that they would feel compelled to move. This absolutely will affect our home values and eliminate a large number of buyers not only for health concerns, but yes, the creep factor. Why should we have to uproot from our homes for fear of unknown health risks that no one can guarantee? And on top of that, lose tens of thousands of dollars and possibly even more with the way home prices are going now in California, potentially lose a hundred thousand dollars or more compared to other areas in Folsom because we have a crematorium dropped in our neighborhood.

The real estate phrase that's been heard thru the years of "Location, location, location" is trumped in this litigation filled business of real estate where now Realtors chant to their clients "Disclose, disclose, disclose".

The California Association of Realtor's have a mandatory disclosure form that must be included in every sale. It's the Sellers Property Questionnaire. I am attaching it to this email and I highlighted and indicated in 7 different paragraphs on this 4 page document. This is where a seller would have to disclose about this crematorium and its issues. It's not something to be taken lightly.

In the applicant's case study, they pointed out that there are other crematoriums next to residential homes, but none are in a historic district. Pictures of those homes shown in the study looked vacant and abandon, cars were on the lawns, they were next to run down apartment buildings and in 'mixed use' zoning areas. Nothing in comparison to the historic quality or pricing of the neighborhood here or up next to such beautiful trails and waterways.

We have two very important boards in Folsom-The City Council and The Historic District Commission. **The Historic District already researched, heard the request from Lakeside Cemetery, heard the City's response and turned down this request 6-1!!!!** **I'm asking there be unity with their decision** and for the City Council to all be on the same page and support each other. One of the reason's we have a Historic Committee is to help uphold the history that makes Folsom quaint and unique.

As a realtor I talk to Families that love living here and ones that want to move to here- all for such reasons for the outstanding schools, for the trails & recreation of both lakes, the restaurants, shopping, for the family feel of the community, for the quaint historic feeling of Folsom. No buyer coming to Folsom has ever asked me, 'how is your crematorium here?' They come.... and they stay

for the quality of life- not death.

Is a Crematorium needed? maybe ... but not in an area that will affect **hundreds of tax paying residents home values and more importantly their health values and of the community enjoying the downtown, river and trails.** A crematorium needs to be placed in the proper area and zoning to be capable of the ever -increasing need and growth to come and not sacrificing the historic district and the living citizens of Folsom over the deceased.

Please vote No against this proposal.

Warm Regards,

Victoria Foster

██████ Young Wo Circle. Folsom

██████

Reminder: email is not secure or confidential. Interco Real Estate Services will never request that you send funds or nonpublic personal information, such as credit card or debit card numbers or bank account and/or routing numbers, by email. If you receive an email message concerning any transaction involving Interco Real Estate Services and the email requests that you send funds or provide nonpublic personal information, do not respond to the email and immediately contact Interco Real Estate Services. To notify Interco Real Estate Services of suspected email fraud, contact: **fraudalert@interco.com**



SELLER PROPERTY QUESTIONNAIRE
(C.A.R. Form SPQ, Revised 12/21)

05/10/2022 Item No. 18.

This form is not a substitute for the Real Estate Transfer Disclosure Statement (TDS). It is used by the Seller to provide additional information when a TDS is completed. If Seller is exempt from completing a TDS, Seller should complete an Exempt Seller Disclosure (C.A.R. Form ESD) or may use this form instead:

NOTE TO SELLER: YOU ARE STRONGLY ADVISED TO CAREFULLY REVIEW THE DISCLOSURE INFORMATION ADVISORY (C.A.R. Form DIA) BEFORE YOU COMPLETE THIS SELLER PROPERTY QUESTIONNAIRE. ALL SELLERS OF CALIFORNIA REAL PROPERTY ARE REQUIRED TO PROVIDE VARIOUS DISCLOSURES, EITHER BY CONTRACT, OR BY STATUTE OR CASE LAW. MANY DISCLOSURES MUST BE MADE WITHIN CERTAIN TIME LIMITS. TIMELY AND THOROUGH DISCLOSURES HELP TO REDUCE DISPUTES AND FACILITATE A SMOOTH SALES TRANSACTION.

Seller makes the following disclosures with regard to the real property or manufactured home described as 123 Example Street, Assessor's Parcel No. _____, situated in _____ County of Sacramento California ("Property").

1. Disclosure Limitation: The following are representations made by the Seller and are not the representations of the Agent(s), if any. This disclosure statement is not a warranty of any kind by the Seller or any agents(s) and is not a substitute for any inspections or warranties the principal(s) may wish to obtain. This disclosure is not intended to be part of the contract between Buyer and Seller. Unless otherwise specified in writing, Broker and any real estate licensee or other person working with or through Broker has not verified information provided by Seller. A real estate broker is qualified to advise on real estate transactions. If Seller or Buyer desires legal advice, they should consult an attorney.

2. Note to Seller, PURPOSE: To tell the Buyer about known material or significant items affecting the value or desirability of the Property and help to eliminate misunderstandings about the condition of the Property.
- Answer based on actual knowledge and recollection at this time.
- Something that you do not consider material or significant may be perceived differently by a Buyer.
- Think about what you would want to know if you were buying the Property today.
- Read the questions carefully and take your time.
- If you do not understand how to answer a question, or what to disclose or how to make a disclosure in response to a question, whether on this form or a TDS, you should consult a real estate attorney in California of your choosing. A broker cannot answer the questions for you or advise you on the legal sufficiency of any answers or disclosures you provide.

3. Note to Buyer, PURPOSE: To give you more information about known material or significant items affecting the value or desirability of the Property and help to eliminate misunderstandings about the condition of the Property.
- Something that may be material or significant to you may not be perceived the same way by the Seller.
- If something is important to you, be sure to put your concerns and questions in writing (C.A.R. form BMI).
- Sellers can only disclose what they actually know. Seller may not know about all material or significant items.
- Seller's disclosures are not a substitute for your own investigations, personal judgments or common sense.

4. SELLER AWARENESS: For each statement below, answer the question "Are you (Seller) aware of..." by checking either "Yes" or "No." There is no time limitation unless otherwise specified. Explain any "Yes" answers in the space provided or attach additional comments and check section 19.

5. DOCUMENTS: ARE YOU (SELLER) AWARE OF...
Reports, inspections, disclosures, warranties, maintenance recommendations, estimates, studies, surveys or other documents (whether prepared in the past or present, including any previous transaction), pertaining to (i) the condition or repair of the Property or any improvement on this Property in the past, now or proposed; or (ii) easements, encroachments or boundary disputes affecting the Property whether oral or in writing and whether or not provided to the Seller.
[] Yes [] No
Note: If yes, provide any such documents in your possession to Buyer.
Explanation: _____

6. STATUTORILY OR CONTRACTUALLY REQUIRED OR RELATED: ARE YOU (SELLER) AWARE OF...
A. Within the last 3 years, the death of an occupant of the Property upon the Property [] Yes [] No
B. An Order from a government health official identifying the Property as being contaminated by methamphetamine. (If yes, attach a copy of the Order.) [] Yes [] No
C. The release of an illegal controlled substance on or beneath the Property [] Yes [] No
D. Whether the Property is located in or adjacent to an "industrial use" zone [X] Yes [] No
(In general, a zone or district allowing manufacturing, commercial or airport uses.)
E. Whether the Property is affected by a nuisance created by an "industrial use" zone [X] Yes [] No
F. Whether the Property is located within 1 mile of a former federal or state ordnance location (In general, an area once used for military training purposes that may contain potentially explosive munitions.) [] Yes [] No
G. Whether the Property is a condominium or located in a planned unit development or other common interest subdivision [] Yes [] No



SELLER PROPERTY QUESTIONNAIRE (SPQ PAGE 1 OF 4)

- H. Insurance claims affecting the Property within the past 5 years Yes No
 - I. Matters affecting title of the Property Yes No
 - J. Material facts or defects affecting the Property not otherwise disclosed to Buyer Yes No
 - K. Plumbing fixtures on the Property that are non-compliant plumbing fixtures as defined by Civil Code § 1101.3 Yes No
- Explanation, or (if checked) see attached; NEXT TO LAKEVIEW MEMORIAL
LOWN CEMETERY AND CREMATORY

- 7. REPAIRS AND ALTERATIONS: ARE YOU (SELLER) AWARE OF...**
- A. Any alterations, modifications, replacements, improvements, remodeling or material repairs on the Property (including those resulting from Home Warranty claims) Yes No
 - B. Any alterations, modifications, replacements, improvements, remodeling, or material repairs to the Property done for the purpose of energy or water efficiency improvement or renewable energy? Yes No
 - C. Ongoing or recurring maintenance on the Property (for example, drain or sewer clean-out, tree or pest control service) Yes No
 - D. Any part of the Property being painted within the past 12 months Yes No
 - E. Whether the Property was built before 1978 Yes No
 - (a) If yes, were any renovations (i.e., sanding, cutting, demolition) of lead-based paint surfaces started or completed Yes No
 - (b) If yes to (a), were such renovations done in compliance with the Environmental Protection Agency Lead-Based Paint Renovation Rule Yes No
- Explanation: _____

- 8. STRUCTURAL, SYSTEMS AND APPLIANCES: ARE YOU (SELLER) AWARE OF...**
- A. Defects in any of the following (including past defects that have been repaired): heating, air conditioning, electrical, plumbing (including the presence of polybutylene pipes), water, sewer, waste disposal or septic system, sump pumps, well, roof, gutters, chimney, fireplace foundation, crawl space, attic, soil, grading, drainage, retaining walls, interior or exterior doors, windows, walls, ceilings, floors or appliances Yes No
 - B. The leasing of any of the following on or serving the Property: solar system, water softener system, water purifier system, alarm system, or propane tank(s) Yes No
 - C. An alternative septic system on or serving the Property Yes No
- Explanation: _____

- 9. DISASTER RELIEF, INSURANCE OR CIVIL SETTLEMENT: ARE YOU (SELLER) AWARE OF...**
- Financial relief or assistance, insurance or settlement, sought or received, from any federal, state, local or private agency, insurer or private party, by past or present owners of the Property, due to any actual or alleged damage to the Property arising from a flood, earthquake, fire, other disaster, or occurrence or defect, whether or not any money received was actually used to make repairs Yes No
- Explanation: _____

- 10. WATER-RELATED AND MOLD ISSUES: ARE YOU (SELLER) AWARE OF...**
- A. Water intrusion, whether past or present, into any part of any physical structure on the Property; leaks from or in any appliance, pipe, slab or roof; standing water, drainage, flooding, underground water, moisture, water-related soil settling or slippage, on or affecting the Property Yes No
 - B. Any problem with or infestation of mold, mildew, fungus or spores, past or present, on or affecting the Property Yes No
 - C. Rivers, streams, flood channels, underground springs, high water table, floods, or tides, on or affecting the Property or neighborhood Yes No
- Explanation: _____

- 11. PETS, ANIMALS AND PESTS: ARE YOU (SELLER) AWARE OF...**
- A. Past or present pets on or in the Property Yes No
 - B. Past or present problems with livestock, wildlife, insects or pests on or in the Property Yes No
 - C. Past or present odors, urine, feces, discoloration, stains, spots or damage in the Property, due to any of the above Yes No
 - D. Past or present treatment or eradication of pests or odors, or repair of damage due to any of the above Yes No
- If so, when and by whom _____
- Explanation: _____

12. BOUNDARIES, ACCESS AND PROPERTY USE BY OTHERS:

ARE YOU (SE

05/10/2022 Item No. 18.

- A. Surveys, easements, encroachments or boundary disputes
B. Use or access to the Property, or any part of it, by anyone other than you, with or without permission, for any purpose, including but not limited to, using or maintaining roads, driveways or other forms of ingress or egress or other travel or drainage
C. Use of any neighboring property by you

Yes No
Yes No

Explanation: LAKE SIDE MEMORIAL LOWN CEMETERY AND

CREMATORY

13. LANDSCAPING, POOL AND SPA:

ARE YOU (SELLER) AWARE OF...

- A. Diseases or infestations affecting trees, plants or vegetation on or near the Property
B. Operational sprinklers on the Property
(a) If yes, are they automatic or manually operated.
(b) If yes, are there any areas with trees, plants or vegetation not covered by the sprinkler system
C. A pool heater on the Property
If yes, is it operational?
D. A spa heater on the Property
If yes, is it operational?
E. Past or present defects, leaks, cracks, repairs or other problems with the sprinklers, pool, spa, waterfall, pond, stream, drainage or other water-related decor including any ancillary equipment, including pumps, filters, heaters and cleaning systems, even if repaired

Yes No
Yes No
Yes No
Yes No
Yes No
Yes No
Yes No

Explanation:

14. CONDOMINIUMS, COMMON INTEREST DEVELOPMENTS AND OTHER SUBDIVISIONS: (IF APPLICABLE)

ARE YOU (SELLER) AWARE OF...

- A. Any pending or proposed dues increases, special assessments, rules changes, insurance availability issues, or litigation by or against or fines or violations issued by a Homeowner Association or Architectural Committee affecting the Property
B. Any declaration of restrictions or Architectural Committee that has authority over improvements made on or to the Property
C. Any improvements made on or to the Property without the required approval of an Architectural Committee or inconsistent with any declaration of restrictions or Architectural Committee requirement

Yes No
Yes No
Yes No

Explanation:

15. TITLE, OWNERSHIP LIENS, AND LEGAL CLAIMS:

ARE YOU (SELLER) AWARE OF...

- A. Any other person or entity on title other than Seller(s) signing this form
B. Leases, options or claims affecting or relating to title or use of the Property
C. Past, present, pending or threatened lawsuits, settlements, mediations, arbitrations, tax liens, mechanics' liens, notice of default, bankruptcy or other court filings, or government hearings affecting or relating to the Property, Homeowner Association or neighborhood
D. Any private transfer fees, triggered by a sale of the Property, in favor of private parties, charitable organizations, interest based groups or any other person or entity.
E. Any PACE lien (such as HERO or SCEIP) or other lien on your Property securing a loan to pay for an alteration, modification, replacement, improvement, remodel or material repair of the Property
F. The cost of any alteration, modification, replacement, improvement, remodel or material repair of the Property being paid by an assessment on the Property tax bill

Yes No
Yes No
Yes No
Yes No
Yes No
Yes No

Explanation:

16. NEIGHBORS/NEIGHBORHOOD:

ARE YOU (SELLER) AWARE OF...

- A. Neighborhood noise, nuisance or other problems from sources such as, but not limited to, the following: Neighbors, traffic, parking congestion, airplanes, trains, light rail, subway, trucks, freeways, buses, schools, parks, refuse storage or landfill processing, agricultural operations, business, odor, recreational facilities, restaurants, entertainment complexes or facilities, parades, sporting events, fairs, neighborhood parties, litter, construction, air conditioning equipment, air compressors, generators, pool equipment or appliances, underground gas pipelines, cell phone towers, high voltage transmission lines, or wildlife
B. Any past or present disputes or issues with a neighbor which could impact the use and enjoyment of the Property

Yes No
Yes No

Explanation: A) CREMATORY B) PRESERVE NEIGHBORHOOD OPPOSED THE MILLER / LAKE SIDE CREMATORY IN 2004 AND IN 2022.



17. GOVERNMENTAL:

- A. Ongoing or contemplated eminent domain, condemnation, annexation or change in zoning or general plan that applies to or could affect the Property
B. Existence or pendency of any rent control, occupancy restrictions, improvement restrictions or retrofit requirements that apply to or could affect the Property
C. Existing or contemplated building or use moratoria that apply to or could affect the Property
D. Current or proposed bonds, assessments, or fees that do not appear on the Property tax bill that apply to or could affect the Property
E. Proposed construction, reconfiguration, or closure of nearby Government facilities or amenities such as schools, parks, roadways and traffic signals
F. Existing or proposed Government requirements affecting the Property (i) that tall grass, brush or other vegetation be cleared; (ii) that restrict tree (or other landscaping) planting, removal or cutting or (iii) that flammable materials be removed
G. Any protected habitat for plants, trees, animals or insects that apply to or could affect the Property.
H. Whether the Property is historically designated or falls within an existing or proposed Historic District
I. Any water surcharges or penalties being imposed by a public or private water supplier, agency or utility; or restrictions or prohibitions on wells or other ground water supplies
J. Any differences between the name of the city in the postal/ mailing address and the city which has jurisdiction over the property

Explanation: WE WOULD HAVE TO NOTE HERE... 17A) ZONING OF OPEN SPACE ADJACENT TO THE PERFORMER CHANGED TO ACCOMMODATE THE LAKE-SIDE CREMATORY.

18. OTHER:

ARE YOU (SELLER) AWARE OF...

- A. Any occupant of the Property smoking or vaping any substance on or in the Property, whether past or present
B. Any use of the Property for, or any alterations, modifications, improvements, remodeling or material change to the Property due to, cannabis cultivation or growth
C. Any past or present known material facts or other significant items affecting the value or desirability of the Property not otherwise disclosed to Buyer

Explanation: LAKE-SIDE CREMATORY IN USE MON-SUNDAY.

19. (IF CHECKED) ADDITIONAL COMMENTS: The attached addendum contains an explanation or additional comments in response to specific questions answered "yes" above. Refer to line and question number in explanation.

Seller represents that Seller has provided the answers and, if any, explanations and comments on this form and any attached addenda and that such information is true and correct to the best of Seller's knowledge as of the date signed by Seller. Seller acknowledges (i) Seller's obligation to disclose information requested by this form is independent from any duty of disclosure that a real estate licensee may have in this transaction; and (ii) nothing that any such real estate licensee does or says to Seller relieves Seller from his/her own duty of disclosure.

Seller Date
Seller Date

By signing below, Buyer acknowledges that Buyer has read, understands and has received a copy of this Seller Property Questionnaire form.

Buyer Date
Buyer Date

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Terry L. Sorensen
 [REDACTED] Forrest Street
 Folsom, CA 95630

April 15, 2022

City Council
 City of Folsom
 50 Natoma Street
 Folsom, CA 95630

RE: Lakeside memorial Lawn
 Crematorium Conditional Use Permit
 (PN-19-182)
 Hearing Date: April 26, 2022

Dear Councilpersons:

Applicant's request for the issuance of a C.U.P. allowing the operation of a crematorium on the grounds of Lakeside Memorial Lawn, an historic cemetery located in the Historic District, should be denied in light of the fact that such an issuance would be violative of the provisions and intent of Folsom's General Plan, Historic Preservation Master Plan, and Design and Development Guidelines. The relevant provisions of these documents are summarized, below.

General Plan 2035

"At the heart of the General Plan are 'Guiding Principles' that express the key values and aspirations of Folsom's future and act as guideposts for the goals, policies, and implementation measures contained within the General Plan." (Folsom General Plan 2035 [hereinafter, G.P.] I-15)

The following Guiding Principles appear to be relevant and controlling in regard to the Lakeside C.U.P. issue herein under consideration:

1. Guiding Principle #17: Embrace the City's rich historic...heritage, preserving, restoring, maintaining, and enhancing heritage sites throughout the city;
2. Guiding Principle #18: Recognize and celebrate the cultural diversity and Folsom residents; and
3. Guiding Principle #19: Facilitate active and meaningful community participation...and actively seeking citizen input in the decision-making process, ...guided by the public interest and... in maintaining and improving quality of life in Folsom. (G.P. I-18).

To ensure that City staff and decision-makers systematically implement the policies and proposals of the General Plan, State law since the early 1970s has increasingly insisted that the actions and decisions of a local governmental entity concerning private projects it approves are consistent with the adopted general plan of that entity. Included in the list of those public entity actions which must be consistent with that entity's general plan are approval of development projects. G.P., IM-3,4).

Historic Preservation Master Plan

The Cultural Resources section of the General Plan (commencing at G.P., NCR-10) provides as follows: "The policies in this section strive to preserve and protect Folsom's historic character as well as Folsom's archaeological resources. Folsom has many cultural resources, most notably Historic Folsom...."

Following this introductory comment, the General Plan goes on to state (at NCR-11) under the heading Goal NCR 5.1 as follows: “Encourage the preservation, restoration, and maintenance of cultural resources, including buildings and site, to enrich our sense of place and our appreciation of the city’s history.” To assist in achieving this goal, the General Plan goes on to require that the City:

1. Whenever feasible, require historic buildings and sites to be preserved (NCR 5.1.1);
2. Maintain an inventory of prehistoric and historic resources (NCR 5.1.2);
3. Nominate additional buildings and sites to the City of Folsom Cultural Resources Inventory of locally significant cultural resources (NCR 5.1.3);
4. Ensure compliance with City, State, and Federal historic preservation laws, regulations, and codes to protect and assist in the preservation of historic and archaeological resources as listed in the City of Folsom Historic Preservation Master Plan (NCR5.1.4);
5. Strive to obtain Federal, State, and private funding incentives for maintaining and rehabilitating historic buildings and sites (NCR 5.1.5); and
6. Maintain and implement design and development standards for the Historic District (NCR 5.1.6).

The City of Folsom has adopted many master plans, strategies, and programs focusing City attention on various types... of development or geographic areas so as to provide... direction for City decision-makers... on how the General Plan will be implemented. Among such plans, strategies, and programs is the Historic Preservation Master Plan. (G.P., IM-5).

The Historic Preservation Master Plan (H.P.M.P.), though dated November 5, 1998, was adopted by the City on November 24, 1998. The Introduction on page 3 of the Plan indicates that on March 25, 1997, the City Council authorized an ad hoc committee to develop a comprehensive historic preservation master plan. That committee began meeting on November 19, 1997, and eventually created the Historic Preservation Master Plan together with a Preliminary Inventory of Cultural Resources (Appendix D thereto) to be presented to the City Council for approval and adoption.

As is relevant to the issues presently under consideration, the Preliminary Cultural Resources Inventory lists the following cultural resources:

- Item No. 27: Odd Fellows and Mason’s Cemeteries, est. circa 1856 Remainder of Lakeside Cemetery, est. circa 1850s;
- Item No. 28: Chung Wah Cemetery, NRHP property, est. circa 1850s;
- Item No. 29: Young Wo Cemetery, CHL, est. circa 18870s; and
- Item No. 40: Dredger Tailing Piles representative of different dredging episodes.

The H.P.M.P. (on page 2 of the Cultural Resources Inventory attached to the H.P.M.P.) states that the Cultural Resources Inventory is designed to serve as a guide to the City’s significant historical and archaeological resources; for use by private groups and citizens to identify, evaluate, register, and to encourage public recognition and protection of resources of architectural, historical, archaeological, and cultural significance; and to identify historical resources for City planning purposes.

Historic District Design and Development Guidelines

Further guidance on the issue of preservation of cultural resources within the City is provided in the Historic District Design and Development Guidelines. Specifically, Goal 2 of those Guidelines defines the City's objective for the preservation of historic sites as follows: "To maintain, restore, and reconstruct sites which represent the history of the Folsom area." Specific policies are provided to assist in achieving this objective, as follows:

Policy 2.1: Locally significant structures and sites should be identified and documented to facilitate their preservation or restoration;

Policy 2.2: The City should encourage National Register nomination for historic sites, as well as other historical designations by State or local agencies; and

Policy 2.3: In prioritizing proposed projects, preferences should be given to authentic restoration of historical buildings or sites.

Concluding Comments

Clearly, the City, by way of its creation of the above-described policies, regulations, and guidelines, has evidenced an intent to protect and preserve its historic cultural resources for the benefit of present and future citizens of the City, the County of Sacramento, the State of California and yes, even the nation as a whole. This is particularly true of those resources existing in the Historic District which include, of course, not only the Lakeside Memorial Lawn Cemetery but the immediately adjacent Chung Wah Chinese Cemetery and Dredger Tailing Piles and the nearby Young Wo Chinese Cemetery, as well.

These historic/cultural treasures have all achieved local recognition as assets deserving preservation and protection and several have received County, State or National recognition as well, (Lakeside Memorial Lawn, Young Wo, and Chung Wah, respectively).

James A. Purcell, then Chair of the Sacramento County Cemetery Commission, opined in his letter to the City dated January 2, 2003, that "the area incorporating Lakeside Cemetery, Chung Wah Historic Cemetery, and the California State Dredging Tailings Park is possibly the only site in the State of California that combines these important aspects of our heritage in one small area." He added that "Degradation of the historic value of the Cemetery by the addition of a crematorium would serve to lessen its cultural importance to the City of Folsom in Sacramento County."

A similar letter from Dr. Bob La Perrier, Vice Chair of the County Cemetery Commission, voiced nearly identical sentiments but added that the Lakeside Cemetery, Chung Wah Cemetery, and the Dredger Tailings preserve "is probably the only site in the State that combines these aspects of our heritage in one small area." (Emphasis added.) He added that it would be, "extremely unfortunate if additional negative factors, such as the installation of a crematorium, further impacted these historic cemeteries (i.e., the grouping of historic cemeteries presently comprising Lakeside)...the final resting place of so many of our pioneers and early settlers...." (Parenthetical added.)

Perhaps the best recitation of the arguments in opposition to the crematorium C.U.P. in question is set forth in the Historic District Commission Staff Report created by City staff back in 2003. On page 4 of that Report, City staff sets forth the following Findings in support of its recommendation for denial:

"B. THE USE APPLIED FOR IS DETRIMENTAL TO THE HEALTH, SAFETY, OR GENERAL WELFARE OF PERSONS RESIDING OR WORKING IN THE NEIGHBORHOOD, AND DETRIMENTAL OR INJURIOUS TO PROPERTY AND IMPROVEMENTS IN THE

NEIGHBORHOOD AND THE GENERAL WELFARE OF THE CITY BECAUSE THE INTRODUCTION OF THIS USE WILL IMPACT THE HISTORICAL CHARACTER OF THE EXISTING CEMETERY AND HISTORICAL USE OF THE AREA.

- C. THE CONGLOMERATION OF THE HISTORIC CEMETERIES, COMBINED WITH THE CALIFORNIA STATE DREDGER TAILINGS DATING BACK TO THE 1850'S, CREATE A RARE COMBINATION OF UNIQUE CULTURAL RESOURCES THAT WILL BE IMPACTED BY THIS PROPOSAL.
- D. THE USE OF THE PROPOSED PROJECT IS NOT CONSISTENT WITH GOAL 2 OF THE DESIGN AND DEVELOPMENT GUIDELINES IN THAT IT DOES NOT MAINTAIN THE HISTORIC USE OF THE THE SITE. IN ADDITION, THE PROJECT DOES NOT FURTHER DESIGN AND DEVELOPMENT GUIDELINE POLICIES 2.1, 2.2, AND 2.3 IN THAT:
1. COMMISSIONERS FROM SACRAMENTO COUNTY HISTORIC CEMETERY COMMISSION HAVE IDENTIFIED THIS SITE AS LOCALLY SIGNIFICANT, AND COMMISSIONERS INTEND TO PRESENT LAKESIDE CEMETERY TO THE SACRAMENTO BOARD OF SUPERVISORS FOR DESIGNATION AS AN HISTORIC CEMETERY.
 2. APPROVAL OF A CONDITIONAL USE PERMIT FOR A CONTEMPORARY USE THAT WILL JEOPARDIZE THE ELIGIBILITY STATUS OF THE SITE WOULD WORK TO DISCOURAGE, RATHER THAN TO ENCOURAGE, NATIONAL REGISTER NOMINATION.
 3. BASED ON A LETTER FROM JAMES A. PURCELL, CHAIRMAN OF THE SACRAMENTO COUNTY HISTORIC CEMETERY COMMISSION DATED JANUARY 2, 2003, A CREMATORIUM IS NOT A CONSISTENT USE WITH A HISTORIC CEMETERY."

In summary, both the law and the facts support a denial of the request for the C.U.P. sought herein. The City staff agreed with that resolution of this matter in its 2003 ideation, and the Historic District Commission agreed with that determination in its decision of denial on February 16, 2022. It is respectfully submitted that the Council should follow the lead of both City staff (back in 2003) and, more recently, the Historic District Commission on the issue.

Respectfully submitted,

/s/

Terry L. Sorensen

email: City Councilpersons and City of Folsom Staff

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Terry L. Sorensen
[REDACTED] Forrest Street
Folsom, CA 95630

April 12, 2022

City Council
City of Folsom
50 Natoma Street
Folsom, CA 95630

RE: Lakeside memorial Lawn
Crematorium Conditional Use Permit
(PN-19-182)
Hearing Date: April 26, 2022

Dear Councilpersons:

This memorandum is submitted in opposition to the request for the issuance of a C.U.P. allowing the construction and operation of a crematorium on the premises of Lakeside Memorial Lawn, a historic cemetery located in Folsom's Historic District.

This request should be denied in that the Historic District Commission, in conjunction with its denial of this request back on February 16, 2022, based that denial, in part, on evidence presented establishing that the proposed crematorium would significantly degrade the historical significance of the Cemetery and the immediate adjacent Chung Wah Cemetery and Dredger Tailing Piles, all of which have been listed in the City's Historic Preservation Master Plan as cultural resources requiring special protection/consideration. Specifically, the finding enunciated by the Historic District Commission in support of its denial reads:

"The use applied for is detrimental to the health, safety, or general welfare of persons residing or working in the neighborhood, detrimental or injurious to property and improvements in the neighborhood and the general welfare of the City because introduction of this use will impact the historical character of the existing cemetery and historical use of the area."

Accordingly, the situation presented by the subject C.U.P. request falls directly within the prohibition set forth in Folsom Municipal Code, Section 17.52.690(D) which reads as follows:

"Whether legally in place or not, a structure or feature which was not original may be deemed nonconforming if its existence..., in the opinion of the historic district commission, significantly degrades a locally significant structure or site."

That is exactly the determination the Historic District Commission has made in the situation presented by the C.U.P. request in question, and that determination should be final and binding.

Respectfully submitted,

/s/
Terry L. Sorensen
TS/dg

email: City Councilpersons and City of Folsom Staff

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Mary Johnson

██████████ Young Wo Circle Folsom CA
 ██████████
 ██████████

Dear Folsom City Counsel Members,

I am writing this letter to voice my concerns about the proposed construction of a crematorium behind my house, I am very much opposed to this construction. Per the study commissioned by the Cemetery owners I live in the area most affected by the potential emissions. My concerns are the devaluation of my property, the impact on air quality from toxins produced by burning bodies, a severe increase in fire hazards from propane tanks in a high fire risk area, impact on the surrounding wildlife and environment, and degradation of Cultural and Historic sites. Lastly the ability to enjoy my property and beautiful backyard without knowing I'm being exposed to cancer causing emissions such as Mercury and Hexavalent Chromium.

I was much encouraged when the Historic District voted 6 to 1 to stop the crematorium. These are the people who know our neighborhood and recognize the historic, cultural and natural resources present in this area. I do hope as City Counsel members you took the time to watch the hearing and consider the concerns presented at this meeting. I have not seen an issue galvanize our community in a unified opposition as the crematorium has done. The Historic District is the "Crown Jewel" of Folsom whose beautiful attributes - Lake Natoma, the State Park, the Bike trails and Sutter Street are featured prominently in all the brochures promoting Folsom. Do you think a crematorium that can be seen from Folsom Blvd should part of our "Distinctive by Nature Slogan"?

In my previous letters I did not address the Historic and Cultural aspects of our neighborhood but was exceedingly dismayed that Mr Semenyuk seemed to downplay the Historic significance of our area. One of his last comments during the Historic District was questioning if the cemetery even had a historic designation. I am concerned that he is the caretaker of this very historic place and He does not even recognize its significance. During the meeting much was made of the important Chinese artifacts and cultural sites located on the property. It's location adjacent to a State Park and its zoning as open space. There was much information presented about how the crematorium would be a major cultural affront to the Chinese ancestors buried there. But there was no mention of the significant number of Jewish graves present in the cemetery. From my research, cremation was traditionally prohibited by the Jewish faith, and only recently cremation has been accepted by a small minority within Reform Judaism. I doubt that the families of the Jews buried in the cemetery would be accepting of a crematorium so close to where their loved ones were buried according to Jewish Laws.

I am a retired RN who has worked in Oncology and Hospice. I have cared for patients prior to and during the death process. There are multiple medical implants, catheters, joint replacements and dental work that when cremated release toxic substances. Not to mention the toxic pharmaceuticals (chemotherapy, radioactive pellets and multiple drugs) that remain in human tissues. My concerns about the cremation process is that all of the above will vaporize and release toxins into the air in our neighborhood.

Toxic air contaminants are the greatest threat to the health of the neighborhood. Per the helix report they are a "diverse group of air pollutants that may cause or contribute to an increase in

deaths or serious illness. TACs can cause long term serious health effects such as cancer, birth defects, neurological damage, lung and respiratory issues. TACs can be carcinogenic or non carcinogenic". FOR CARCINOGENIC TACS THERE IS NO LEVEL OF EXPOSURE THAT IS CONSIDERED SAFE.

That is a direct quote from the Helix report. Yet they go on to state that there there will be two TACs that are of concern for health risks, Mercury and Hexavalent Chromium. These two substances are the "primary drivers of the health risks from crematory emissions". Mercury is considered a Non cancer chronic and acute health risk TACs. But mercury is known to be a potent neurotoxin and can cause developmental delays including decreased IQ in children. It can also cause kidney damage. A report from the NIH website state that "Crematoria represent a significant source of mercury emissions to the environment". The danger with mercury is long term indirect exposure to the environment and the food chain. This is because mercury can accumulate in our tissues over time. We live in a neighborhood where we eat fruits and vegetables from our yards and people and wildlife (including endangered Bald Eagles) catch and consume fish from lake Natoma. The Lake Natoma/ American River is a drinking water source for well over a million people.

Hexavalent Chromium is a known Cancer causing toxic air contaminant. Prolonged exposure has been known to cause lung cancer. Per their air quality report they know that this TAC will be released into our neighborhood even though per their report there are no safe levels.

In the Helix report they stated that our area is particularly prone to air stagnation due to the bluffs and hills that act as a barrier to airflow and can trap pollutants causing them to become more concentrated. In addition our neighborhood lies within the "Shultz Eddy". This eddy effect causes the wind pattern and pollutants to circle back southward further concentrating them. Please take this into consideration, not only will the crematorium produce Toxic Air Contaminants but the geographical location prevents the dispersement of these toxins.

Fire safety is my next area of concern. Our neighborhood is surrounded on three sides by woodlands and brush, a urban wild land interface. I was on the Folsom Fire safe council in 2012 because I was so concerned about the dry brush and vegetation build up behind my home. Because of our efforts, the area between our homes and cemetery was cleared. But unfortunately this brush clearing has not happened in 2 years. Not only that but the grounds keepers routinely toss trimmings and drag downed trees into that area. It has created a extreme fire hazard that I will be reporting to the fire department. I am worried that the people who routinely create a worse fire situation by piling brush will be the same workers in charge of maintaining the safety of the crematorium.

My last concern is the devaluation of my property. I am retired and a widow and I will need to sell my house in the future to downsize and access the equity for future expenses. There is a very real impact on the desire-ability and value of my home to potential sellers knowing that there are cremations occurring yards a few hundred yards away. Who wants to sit by the pool and eat outside knowing they a breathing residue of human bodies being burned?

Because of all the the above concerns please vote against building a crematorium in our neighborhood,

Thank you so much for your time,

Mary A Johnson



Photo of area behind my house at edge of Cemetery

From: Elaine Andersen
To: Josh Kinkade
Subject: FW: Vote No on Lakeside Memorial Crematorium Appeal on 4/26
Date: Thursday, April 21, 2022 7:34:24 AM

From: Mariko Peshon McGarry [REDACTED]
Sent: Wednesday, April 20, 2022 7:40 PM
To: Elaine Andersen <eandersen@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; Mike Kozlowski <mkozlowski@folsom.ca.us>; Sari Dierking <sdierking@folsom.ca.us>
Subject: Vote No on Lakeside Memorial Crematorium Appeal on 4/26

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Dear Members of the Council, City Manager Anderson, and Assistant City Attorney Dierklin,

I write this letter with great concern about the Caring Services Lakeside Memorial Crematorium appeal to be heard by this body on April 26, 2022. My concerns are rooted in the historical, racial, and cultural implications this appeal has on so many of us that already experience underrepresentation in this community.

In attending the Historic District Commission meeting on February 16, 2022 where the Commission voted to protect the historical significance and well-being of our unique community, I witnessed Caring Services Group's Igor Semenyuk comment on not wanting or asking to be recognized by regional, state, and national entities as a historical site. I have since read his written comment in his initial appeal document, later elaborated upon beyond the official timeline, about residents being "biased." As you are likely now aware, the proposed industrial crematorium is to be located in close proximity to several publicly recognized historical sites. Mr. Semenyuk's statements have sent a clear message to the Asian American, and Chinese American community specifically, as well as the larger community that our cultural heritage, our property values, and our health is meaningless in the face of an opportunity for profit.

Caring Services Group has been clear from the first virtual community meeting in 2021 that this effort is driven by the fact that cemeteries are a "dying business." What is missing in this business venture is the critical sensitivity and cultural competency that would reduce the consistent prejudice and minimization of racial history being enacted to support a business plan. Mr. Semenyuk can pretend the plaque provided by the Sacramento County Cemetery Advisory Commission to display Lakeside's historical designation does not exist, but I cannot pretend the shape of my eyes are not of Asian descent. I cannot choose to have different DNA simply because a local business chooses to not recognize unique elements essential to honoring our community's heritage. The fact that the historical designations of Lakeside Memorial Cemetery, the Chinese burial site adjacent to Lakeside Memorial Cemetery, and the

Chan Wah Historic Cemetery are completely disregarded by Caring Services Group's proposed industrial crematorium disrespects and devalues the cultural practices that honor these historic sites.

I deeply value the six Historic District Commissioner's firm stand on protecting the "historical character of the existing cemetery and historical use of the area," and recognition of the detrimental [impact the application has on] the health, safety, or general welfare of persons residing or working in the neighborhood." I respect the Commission's recognition of the historical evidence, documentation, and previous Planning Commission recommendations that the City has had access to for decades. As a pregnant Asian American woman who is raising a toddler in the Historic District, I felt we were seen. I felt that our concerns and fears about our history and future were heard. I felt represented.

Caring Services Group's appeal makes me feel discarded. It suggests that the sacrifices of our ancestors and founding community members are perceived to be irrelevant, as they do not garner a desirable profit margin for a private business. Overlooking the significance of the past in this way dehumanizes the present.

In a time of social and racial reckoning, the City of Folsom cannot afford to side with the face of cultural and historical disregard. Our City has much work to do to continue to recognize, repair, diversify, and create equitable opportunities for our residents to live in a community that values their unique cultural backgrounds. A decision to support Caring Services Group's appeal would serve as a loud, public step in the wrong direction toward cultural, and potentially racial, systemic discrimination. We cannot undo harm to history and to residents once executed and publicly supported. We cannot unknow the experience of being underrepresented by public figures once votes are finalized. The community will remember this.

Attending to a community's well-being as elected representatives involves more than ensuring natural and tangible resources are available and safe. As a governing body, there is a moral obligation to ensure that our marginalized communities are represented in the decisions that seek to protect where and who we came from, and what impacts our current quality of life in order to meet the future with a focus on diversity and equity.

As a tax-payer, I urge you to think beyond profit and local business relations on April 26th so that I may wake up my child on April 27th and tell them that being an Asian American in the City of Folsom matters. That they have a place in this community that is deeply respected and honored by the elected officials making decisions that impact their life course trajectory. I would like to give birth to another child later this year without fear of what heavy metals I may have ingested on days when several bodies have been cremated while sitting in our front yard. I want this child to come home to a community without fear that their racial identity will be minimized by a business decision in the Historic District.

Vote no on the Lakeside Memorial Crematorium appeal. Protect History. Honor cultural significance. Support your Historic District Commission. Represent *all* members of the community and their desire to belong.

Sincerely,
Dr. Mariko McGarry

Due to limitation of resources and time, we unfortunately cannot supply further comments on the concerns regarding the construction of a crematorium at Lakeside Memorial Park (Cemetery). You are welcome to use material we have submitted in the past, both recent and from several decades ago.

However, we would appreciate the following items be considered by Miller Funeral Home/Lakeside Memorial Park as a strong recommendation and a demonstration of responsibility for owning and maintaining a historic cemetery (cemeteries), well over 1½ centuries old.

1) Acceptance of the Historical Nature of Lakeside Cemetery (Memorial Park) and further documentation of its detailed history going back to the mid 1800's, both on its website and as a large interpretive sign on the Cemetery property, in addition to the placement of the aluminum signage we delivered years ago designating it as a Sacramento County Historic Cemetery.

2) Acceptance of the major significant of the Historic Chinese Cemetery in proximity to Lakeside.

3) If this has not been done, public access to an updated plot map of Lakeside, both historic and current.

4) Consideration of the construction of a retaining wall at the sloping area at the far back corner area both for stabilization and safety concerns.

Thank you for your assistance and support.

Dr. Bob LaPerriere
Chair, Sacramento County Cemetery Advisory Commission

 (voice)

(text)

URL: <http://www.coroner.saccounty.net/sccac/Pages/default.aspx>

From: [REDACTED]
To: dlaoffice@sacra.org; Jeannie Lee; Eileen Sobock; Caltrans.Director@dot.ca.gov
Cc: Sarah Aquino; Lydia Konopka; Josh Kinkade; The HFRA; assessor@sacounty.net; County of El Dorado Clerk of the Board; Rick Hillman; john.baum@waterboards.ca.gov; Patrick Pulupa; Drew Lessard; errest.conant@usbr.gov; wade.crowfoot@resources.ca.gov; karla.nemeth@dwr.ca.gov
Subject: Notice of incomplete legal compliance: PN 21-115, GP, Zone, Map changes in dark.
Date: Friday, April 22, 2022 3:14:49 PM
Attachments: [1650659536014blob.jpg](#)
[1650659606848blob.jpg](#)
[1650659755970blob.jpg](#)
[1650661047079blob.jpg](#)
[1650661136651blob.jpg](#)
[1650661351704blob.jpg](#)

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TO: Folsom City Council Via Clerk, for distribution;
and for referral and Expert Reports from City Engineer, Licensed City Legal staff, and for city licensed civil engineers
Impacted Region jurisdictions; State enforcers; impacted US agencies
From: LJ Laurent
April 22 2022

Re: PN 21-115; County Parcel Number NOT shown; Changes Not shown; MAP NOT Shown;
Proof of Compliance - none apparent; Surveyor/Engineer Cert of Compliance and Nature of change -- NOT shown

Folsom Comm. Dev. obviously does not make use of "city engineer" Krahn, who is subordinate to Ms. Johns, a non-licensed city employee. Who is conducting the Publics' Business at cityhall? Make that who is Mis-conducting and not being Open about our business?

Comm Dev. person signed off on Public Notice which is just about 100% Deficient in Legal Compliance.

Although it is mind-numbing, below is a conglommeration of just a FEW of State Laws which city staff have violated, ignored, omitted, or been ordered to disregard -- or whatever else you have as explanation.

This include Total Disregard of using our CA Licensed Civil Engineer in residence as the Definitive expert to draw, examine, inspect, certify, and Determine Which State & federal & county PROCESSES must be observed, with certified papers filed, in order to be legally compliants.

The below citations will be totally Clear to an experienced, trained, and hopefully state-licensed Expert.

Too bad if it's complicated. The city & or its officials, or agents, or employees, or "beneficiaries" deliberately and without Public Hearings altered FMC. Look at the Commercial Table. It is a mess of incomprehensible check marks and lengthy columns/tables. It can be Proved the city NEVER brought such nonsense to Public Scrutiny. We had a few council who served over 115 years total together. One still sits, but has relinquished CA state engineer license last year.

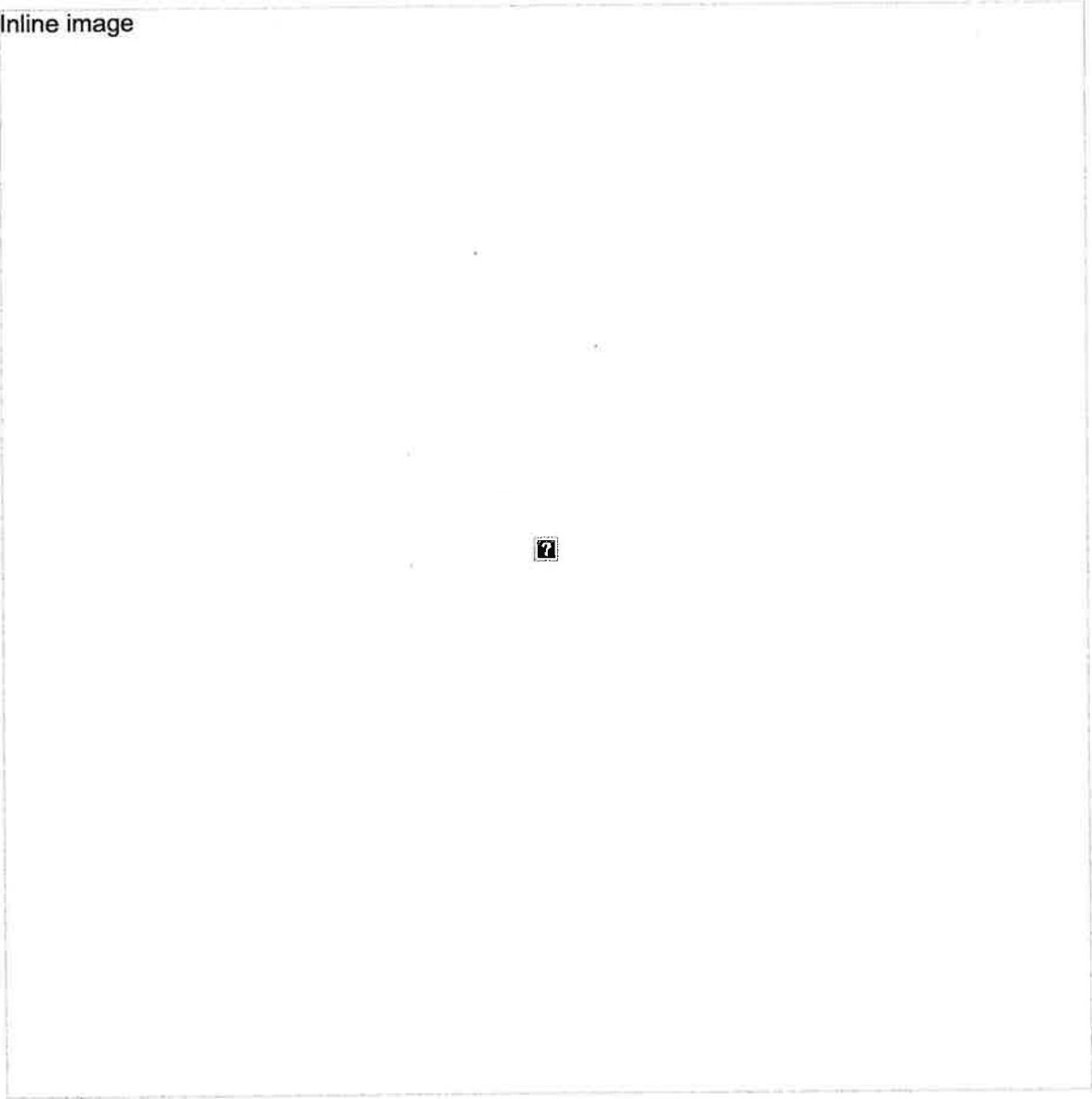
This is a pathetic mess, which appears designed to confuse, deceive, avoid Due Process, and achieve Land Usages which are NOT sufficiently SUPPORTED by Infrastructure, Water Supply, and Public Comments, as well as Regional agency comments.

Complaint:

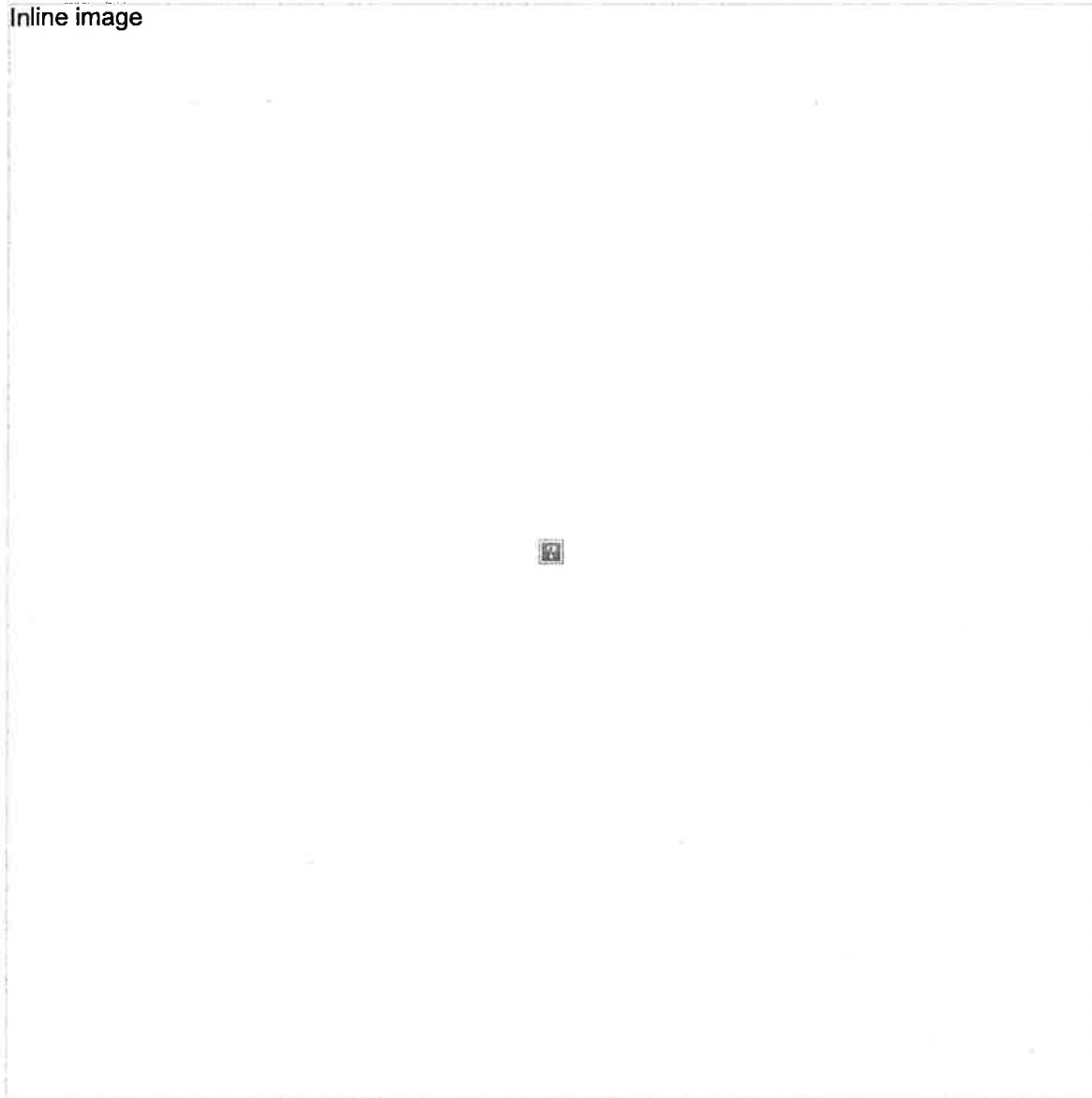
It is respectfully requested of Sacramento County District Attorney, County Assessor, and County

Recorder that they direct their fullest attention to the deficiencies noted below -- in no special order, since there are SO MANY.

Inline image



Inline image



First PROBLEM: This is **NOT CONSISTENT with 2018 GENERAL PLAN**, which shows "Regional Commercial" for the entire Parcel.

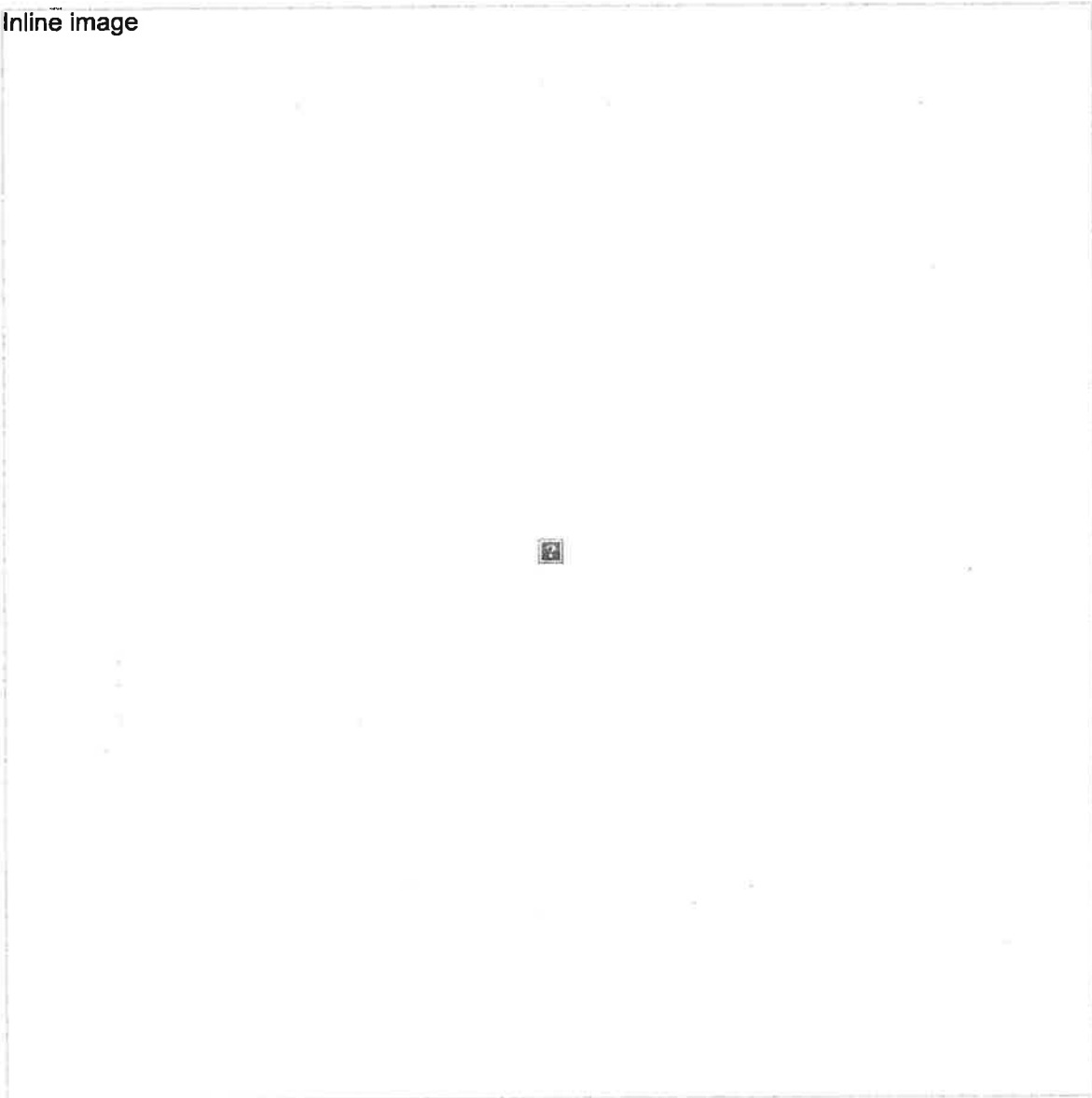
As REGIONAL, and at Hwy 50, HOW can this NOT be Subject to CEQA ??? Folsom is not legally defined as a "Region", but a "city."

Plan IS NOT consistent with Folsom GP Map 2018, and it is "Regional Commercial." abutting Hwy 50.... "regional" is NOT SAME as "city".

As for Planned Dev. Zone -- **without showing the PD Map and Engineer Assessment of IMPACTS on REGION**, how can you do a rezone which VIOLATES General Plan without advertising and giving Notices and doing the Entire Legal Process under CEQA and other laws?

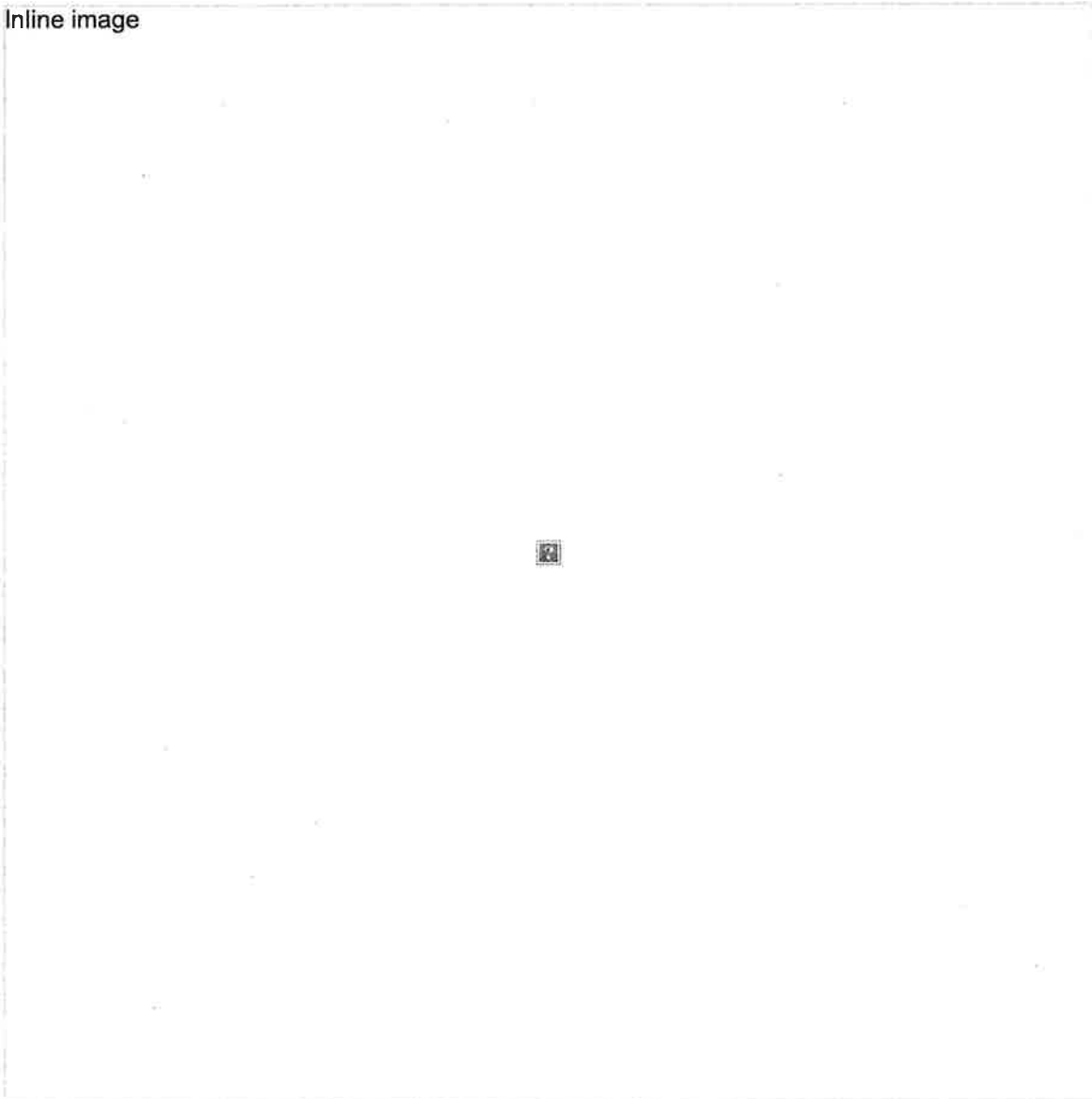
Folsom RCC on Map/Gen Plan

Inline image



**Note: there is NO Definition of RCC, just a jumble of inscrutable "stuff" which was obviously NEVER brought to Public Hearing NOR Public Scrutiny, Nor Sealed/signed with a Lic. Civil Engineers license.
Who is responsible for all this, aside from city council who appear to "go along with stuff?"**

Inline image



HUGE HUGE PROBLEM: According to FMC 17.22 et seq, the **TABLE does NOT AGREE with TEXT**. Note in FMC, as photographed below, that in C3 Commercial, very heavy usage zone, the Footnote #7 states the **STANDARDS DO NOT APPLY**.

A. Commercial Use Table. The purpose of the commercial use table is to designate the uses permitted within the buildable area and within the yard areas in each of the following zones.....

3. C-3, general commercial zone as further regulated in Section 17.22.050 of this chapter. The purpose of the C-3 zone is to designate areas appropriate for heavy commercial activities. While all types of commercial activities are permitted, the C-3 zone is intended for the highest-intensity commercial activities, which include **heavy auto and truck traffic. The C-3 zone should be located on major arteries and thoroughfares.**

Perhaps FMC needs to include what an Engineer would know is required by higher Laws: that

STUDIES are Mandatory to ensure the "major arteries and thoroughfares" are not at Level F, or gridlock.

This location is on E. Bidwell, right at US Hwy 50 area. What are the Implications of adding this new "PD, Planned Development" without even knowing the Subdivision Map itself, the uses, the IMPACTS on Region and neighboring jurisdictions??? What about Water Supply? A hotel uses far more water than a "big box retailer" who brings in tons of tax dollars.

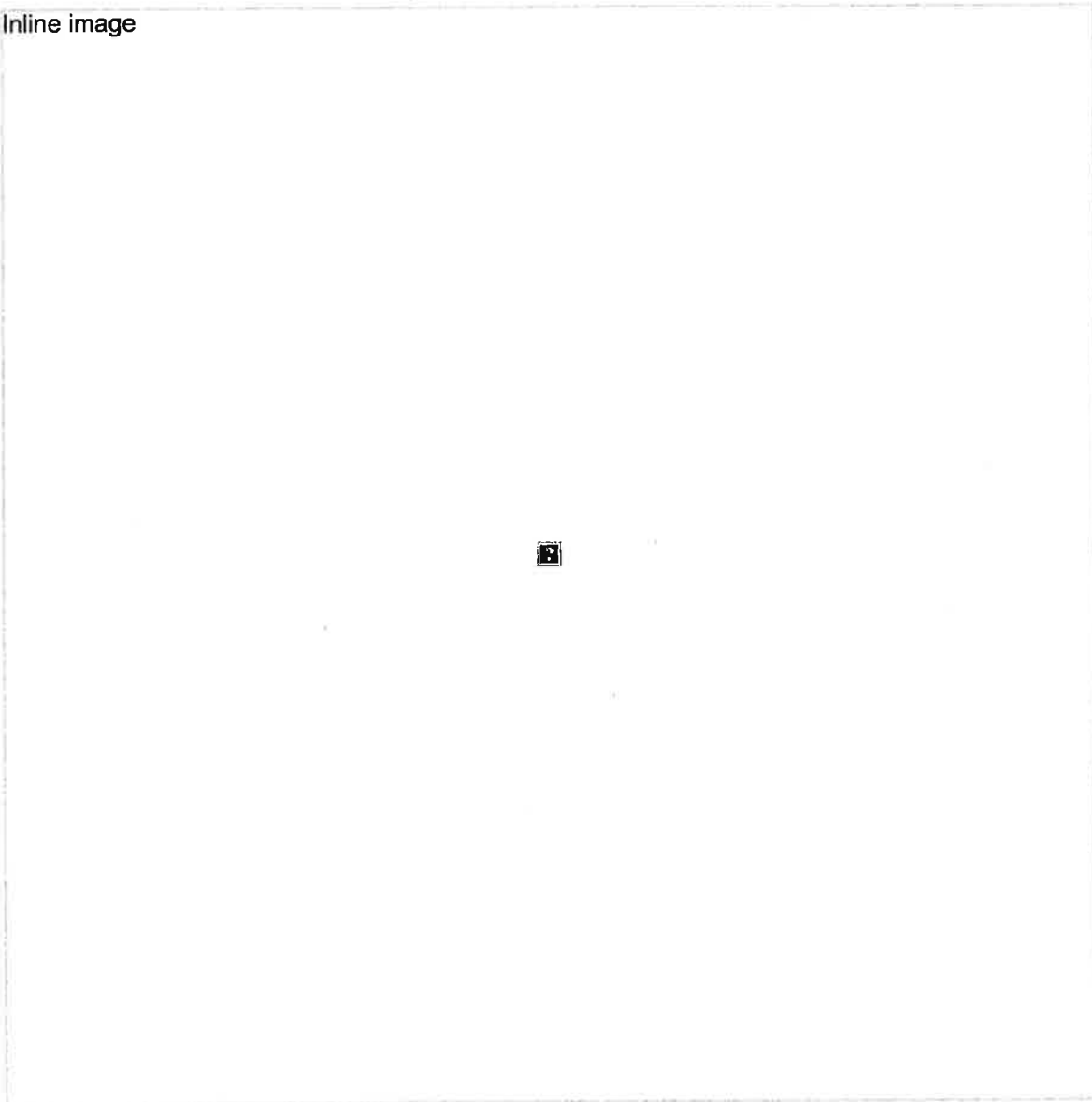
Folsom Municipal Code

Link to another dumb "table" which obscures analysis and understanding. No licensed Engineer would provide this as sufficient for legal enactment.

Check link: the table says one thing but Note 7 contradicts the table.

"not applicable" in fine print footnote.

Inline image



Commercial Use Table (17.22.030E)

	Zone
--	-------------

	Use, service or facility	C-1	C-2	C-3	BP
--	--------------------------	-----	-----	-----	----

119	Hotel	N			1
-----	-------	---	--	--	---

Would city engineer mind explaining what this Table really means, and exactly WHY is General Plan Inconsistency with Proposed Use NOT Subject to CEQA, with Notices, and Regional Impacts circulated for comments?
 After all, according to CA GC, a licensed Civil Engineer MUST, he/she "shall" do the proposed Subdivision Map or Supervise & approve its production in Compliant Form.

QUOTE: B. Permitted Uses Within the Buildable Area of Commercial Lots. A " " indicates that the described use is permitted in the zone represented by the described symbol appearing at the top of the column. An "N" indicates that the described use is not permitted in the zone represented by the symbol appearing at the top of the column. A number indicates that the described use is permitted in that zone upon continuing compliance of the special condition referenced by the corresponding number in Section 17.22.040 of this chapter. **The special condition requirements govern the described use.**

What "special condition requirements"??????
 Where are they hidden?

APN #: there is now response when you enter the APN in search. But, Folsom Comm Dev omitted the APN for this proposed "subdivision."
 Might city approve putting a 5 story hotel on just one acre? This city might.
 Is Water Supply certified adequate for such General Plan alterations without CEQA, and for Map Approvals without the Required Licensed Engineers' seal/signature?

Inline image



Who regulates subdivisions in California? The California Government Code – Subdivision Map Act (GC Section 66410 et seq.) regulates the subdivision of real property. The Revenue and Taxation Code (RTC Section 11511) also allows a County Assessor/ Tax Collector to subdivide a parcel and sell the “sufficient” part through a tax foreclosure auction.

19.76.010 - Purpose.

SHARPRINTEVENTUALDIRECTOR OF SECTIONS

This Chapter establishes requirements consistent with the Act for certificates of compliance, lot line adjustments, parcel mergers, and reversions to acreage.

19.76.020 - Certificates of compliance.

GOVERNMENT CODE - GOV

TITLE 7. PLANNING AND LAND USE [65000 - 66499.58] (*Heading of Title 7 amended by Stats. 1974, Ch. 1536.)*

DIVISION 2. SUBDIVISIONS [66410 - 66499.40] (*Division 2 added by Stats. 1974, Ch. 1536.)*

CHAPTER 2. Maps [66425 - 66450] (*Chapter 2 added by Stats. 1974, Ch. 1536.)*

ARTICLE 3. Parcel Maps [66444 - 66450] (Article 3 added by Stats. 1974, Ch. 1536.)

QUOTE --

The parcel map shall be prepared by, or under the direction of, a registered civil engineer or licensed land surveyor, shall show the location of streets and property lines bounding the property, and shall conform to all of the following provisions:

There are LOTS of "provisions", which are not same as City FMC and Standards "provisions." In other words, CA STATE LAW CONTROLS EVERY MOVE YOU MAKE, city staffers. Take Note City Council.

Perhaps you need to File this first with Sacramento County Recorder and RE ASSESSOR Office, ALONG with the Licensed Civil Engineers Seal/Signature, all CEQA compliance & Notices to regional jurisdictions. This PN is a travesty, NOT a compliant Notice of

- 1. General Plan alteration ignored -- but required.**
- 2. No Lic. Engineers seal/signature of oversight, enforcement, planning, and mandatory Notices.**
- 3. City is changing Zone District without using Mandatory public notice and Mandatory Processes.**
- 4. City has no Proof of water supply, Infrastructure, Impacts on Regional Services such as Fire control, evacuation routes, and**
- 5. NO comments from remainder of region and all higher or separate agencies serving Region, or Providing Utilities for this new use and Subdivision --**
- 6. of which we know NOTHING at all.**

Questions/comments welcomed. However, there is no way on earth I could explain how city gets away with this. There's water disaster afoot, 63 dead humans on city property, and the most secretive, jumbled set of "ordinances" enacted so secretly and without vetting, that one doubts anyone could explain it.

How does state administration and state governor tolerate this?

From: [Elaine Andersen](#)
To: [Josh Kinkade](#)
Subject: FW: Vote No to Save Folsom
Date: Monday, April 25, 2022 7:44:53 AM

From: [REDACTED]
Sent: Sunday, April 24, 2022 6:01 PM
To: Mike Kozlowski <mkozlowski@folsom.ca.us>; Sarah Aquino <saquino@folsom.ca.us>; YK Chalamcherla <ykchalamcherla@folsom.ca.us>; kerri@atlanticcorrosionengineers.com; Rosario Rodriguez <rrodriguez@folsom.ca.us>; jkincaid@folsom.ca.us; Elaine Andersen <eandersen@folsom.ca.us>
Subject: Vote No to Save Folsom

You don't often get email from [REDACTED] [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I urge you to vote NO on the proposed Lakeside Memorial Lawn crematorium. My reasons include heritage, the air we breathe, long term health problems, and fire danger.

The proposed site is zoned Open Space. Our city of Folsom is Distinctive by Nature. Let's work together to keep it that way.

I urge you vote to preserve this heritage, to preserve what sets our town apart from just another suburb of Sacramento. For ourselves, and for those who come after us. The following points are briefly noted; you most certainly will have the background information to which I refer in the Historic District Commission notes, or in other concerned citizens' letters.

The area immediately adjacent to the proposed crematorium is of historical note to our community. The Chinese community has been instrumental in the development of our town, and we as a community need to continue to acknowledge those contributions, and grow that appreciation. Our neighborhood is adjacent to ancient Chinese burial grounds. It is imperative that we respect our ancestors and our city's previous commitments.

The environmental reports presented earlier measured air quality near Sacramento Executive Airport, where almost every night the Delta breeze refreshes the air quality, and near Sacramento International Airport. By design that airport is on flat land in relatively wide -open spaces, and birds are actively discouraged.

The City of Folsom, and especially our neighborhood, rarely if ever gets the Delta breeze. The air in our neighborhood, adjacent to the cemetery, gets trapped between the foothills, our town and the bluffs across Lake Natoma from us.

In the summertime, we historically get forest fires in Northern California. It is predicted we will get more fires and more frequently. Fire smoke gets trapped, and the AQI is such that we are encouraged to stay in our homes. We have had neighbors move because of the summer trapped smoke, and their inability to be active outside. They moved so their kids could grow up where they can play outdoors.

Please consider that the proposed crematorium smoke and smell will contribute to this unhealthy atmosphere. And yes, there will be smoke, and there will be the smell of human flesh burning. This is the report of real people who have lived near crematoriums, not sales brochures. And there will be Toxic Air Contaminants (TACs) such as mercury vapor and hexavalent chromium, for which there is no level of exposure that is considered safe. According to the National Institute for Health "crematoria represent a significant source of mercury emissions to the environment." Mercury builds up in human tissue and causes developmental delays. There is a child care center five blocks from the entrance to our neighborhood. There are many young children playing in our neighborhood. We must protect them. Prolonged exposure to hexavalent chromium can cause lung cancer. Our community traps stagnant air; we must not risk contaminating the jewel of our Distinctive by Nature city by approving a crematorium. Once the crematorium would start burning bodies, the resulting air quality issues could and would make our air quality untenable. Please do not take that chance. Let's work together to keep Folsom Distinctive by Nature.

Perhaps you have seen the You Tube video demonstrating what happens when a propane tank malfunctions and blows up. (Search "propane tank explosion caught on camera.") If you have not seen it, I encourage you to do so and to consider your responsibility to vote NO to make sure that does not happen in Folsom.

We do not need a fire in our community, where there is one way in, and one way out. Yes, I heard the fire chief state they would open up the Leidesdorf access. I am not certain how fast that could happen and how quickly it could be communicated. I would like to avoid finding out by worst case scenario. Many if not most of us in this community have the bulk of our retirement savings in our homes. I would love to survive any fire, or course, but I am not willing to risk a fire that would destroy my life's possessions and memories.

In April the trees are all green, plants are budded out, and a walk or a bike ride on the American River bike trail has us believing that it will always be so. Please before you vote consider the tinderbox this whole area becomes when the seasons change and the ravaging effects of our multi-year drought make themselves known again. We must keep our City of Folsom safe and Distinctive by Nature.

With all these potential risks, what could be worth voting any way but NO? Respectively I urge you to support the City of Folsom Historic District Commission and their 6-1 vote against the proposed crematorium. I urge you to vote NO to the proposed crematorium and keep our city Distinctive by Nature.

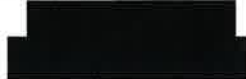
Thank you for your service and for your time.

Jov Hays

██████ Young Wo Circle, Folsom, CA 95630

████████████████████

Deborah Grassl
Young Wo Circle
Folsom, CA 95630



April 22, 2022

City Council
City of Folsom
50 Natoma Street
Folsom, CA 95630

RE: 2022 Lakeside Memorial Lawn
Crematorium's Appeal and this Appeal's
relationship to the withdrawn 2003 Lakeside
Cemetery Crematorium Project

Dear Council Members:

The Lakeside Memorial Lawn Crematorium Appeal should be denied based on the same Finding made by the Historic District Commission on February 16, 2022: "B. THE USE APPLIED FOR IS DETRIMENTAL TO THE HEALTH, SAFETY, OR GENERAL WELFARE OF PERSONS RESIDING OR WORKING IN THE NEIGHBORHOOD, AND DETRIMENTAL OR INJURIOUS TO PROPERTY AND IMPROVEMENTS IN THE NEIGHBORHOOD AND THE GENERAL WELFARE OF THE CITY BECAUSE THE INTRODUCTION OF THIS USE WILL IMPACT THE HISTORICAL CHARACTER OF THE EXISTING CEMETERY AND HISTORICAL USE OF THE AREA." (page 4). This same Finding was used as the basis for the Staff Report recommendation of denial in reference to the Lakeside Cemetery Crematorium Project (composed of the crematorium and a "Tuff Shed" construction) that was scheduled for hearing before the Commission back in 2003 before it was withdrawn.

The Appeal before the Council is, also, directly related to the 2020 approval by the Historic District Commission (HDC) of PN-20-160 Lakeside Memorial Lawn Storage Shed and Determination that the Project is Exempt from CEQA, which involved a storage shed that was cherry-picked as an individual project from the 2003 Lakeside Cemetery Crematorium Project, a project which clearly was not CEQA-exempt. As such, this 2020 Storage Shed* successfully flew under the environmental radar by way of a ministerial building use permit application which allowed the 40'X40' Storage Shed to be constructed without proper CEQA vetting.

CEQA GUIDELINES Section 15300.2 EXCEPTIONS

(f) Historical resources. A categorical exemption shall not be used for a project which may cause a substantial adverse change in the significance of a historical resource.

Staff was aware that the Lakeside Cemetery, already on the Sacramento County Historic Cemetery Commission's list of important pioneer cemeteries, needed protection in the estimation of the Sacramento County Cemetery Commissioners, to preserve the Cemetery's integrity should an application for listing on the National Register of Historic Places ever be applied for (Lakeside Cemetery Crematorium Project 01/15/2003 Staff Report, Attachment 4).

In addition, the City failed to apply certain provisions of the Folsom Municipal Code, as follows:

Folsom Municipal Code 17.52.690 Nonconforming structures.

D. Whether legally in place or not, a structure or feature which was not original may be deemed nonconforming if its existence prevents listing on the National Register of Historic Places of an otherwise eligible structure or site or, in the opinion of the historic district commission, significantly degrades a locally significant structure or site. (Ord. 890 § 2 (part), 1998)

Staff was aware, per the same HDC Staff Report, of the historical research on the Lakeside Cemetery's origins, and the pioneers buried there, which was submitted to the HDC by Sue Silver along with a record compiled by Eagle Scout Peter Kroff of the grave markers on head stones dating from 1850 to 1995. Both monographs should accompany any application to the National Register of Historic Places.

Folsom Municipal Code 17.52.480 Accessory Structures.

... an accessory structure is any freestanding roofed structure located on a parcel on which another larger structure (main structure) has been constructed...An accessory structure shall not be larger than the main structure in square footage or height. Design review is not required for accessory structures smaller than 60 square feet or which are below required fence height. (Ord. 890 § 2 (part), 1998)

Staff was aware, per the same HDC Staff Report, that the main structure on the Cemetery parcel was/is 975 sq ft. The proposed Storage Shed was/is 1,600 sq ft, (40'X40') and was **larger** than the main structure on the parcel and couldn't qualify as an Accessory Structure.

In conclusion, since the Applicant's clear purpose for constructing a new Storage Shed was to clear the way for the use of the "main" 975 sq ft structure for a future crematorium, the illegally-permitted 1,600 sq ft Storage Shed (40'X40') and the proposed crematorium in the Appeal are inexorably linked. The latter cannot go forward without the former. And, based on the foregoing analysis, the Storage Shed constructed on the grounds of the historic Lakeside Memorial Cemetery was not properly permitted by the City in 2020 and never should have been constructed.

To allow the Crematorium to go forward would be to reward the illegality of the whole underlying scheme. In equity and in good faith, such an outcome should not be allowed, and the logical remedy would be to deny the Crematorium Appeal. Otherwise, the applicant will be allowed to benefit from a 1,600 sq ft Storage Shed that has robbed the community of the integrity of some of its oldest historical resources based on an illegal underpinning. The City Council should not condone such an outcome.

Sincerely,

/s/

Deborah Grassl

cc: kerri@atlanticcorrosionengineers.com; rrodriguez@folsom.ca.us; saquino@folsom.ca.us; ykc@folsom.ca.us; mkozlowski@folsom.ca.us; kmullett@folsom.ca.us; jkinkade@folsom.ca.us; sbanks@folsom.ca.us; sjohnson@folsom.ca.us; pjohns@folsom.ca.us

***NOTE:** email attachment:

From: Deborah Grassl <arm@artpass.net>

Sent: Thursday, November 18, 2021 10:42 AM

To: Josh Kinkade <jkinkade@folsom.ca.us>

Subject: New Shed build at Lakeside Lawn Cemetery and Public Records Request

Hi Josh,

I took five photos of a new slab foundation, approximately 10'X30', being finished next to the adjacent, existing shed at the proposed Lakeside Cemetery Crematorium project site. The photos were made on June 30, 2021. Shortly thereafter a metal Tuff Shed-style building was fully erected.

This newly built shed can be seen from Folsom Blvd, alongside and with the old Crematorium's Tuff Shed. At night there is a very strong light coming from the new shed that breaks up the shadows over the Cemetery and attracts drivers' eyes to the sheds. The sheds clearly stick up above the California State Dredger Tailings Preserve, and now block the riparian forest view shed, a part of the Chung Wah Chinese Cemetery's National Register of Historic Places description of site characteristics. In addition, the sightlines of the rare Lakeside Lawn's grouping of 8 contiguous pioneer cemeteries alongside the Chung Wah Cemetery and Dredger Tailings Preserve are now separated by the newly built shed. This unique grouping is described in the **Historical Significance** site description of the City's 01.15.03 Staff Report on page 2, paragraph 5.

It is noted, too, that the newly built shed corresponds to a new shed build description included in the 01/15/03 HDC Staff Report regarding the Request for Approval of a Conditional Use Permit to allow for the operation of [a] crematorium, page 2, **PROJECT DESCRIPTION**, paragraph 2. This CUP application was subsequently Withdrawn on 01/15/03 and the new shed was not built at that time.

However, when Igor Semenyuk was asked about this new building construction on the Zoom meeting LNS/Preserve residents had with owner, Igor told us not to pay any attention to this new construction. He stated that it had nothing to do with the current Lakeside Lawn Cemetery's Crematorium Conditional Use Permit application awaiting an HDC hearing date.

I did note in the current Lakeside Lawn Memorial Cemetery Crematorium IS/MND & CUP descriptions that a 10'X30' new shed build is NOT included.

Was a permit application to build this 10'X30' Tuff-style shed ever received and approved by the City? If so, I would like to put in a Public Records Request for a copy along with the environmental evaluation.

Thank you for your time researching this request.

Deborah Grassl

Art and Craft

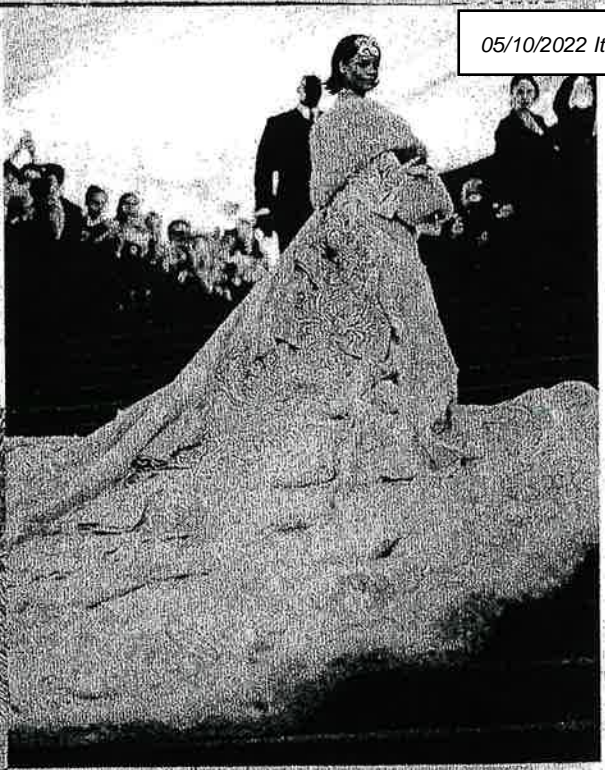
At San Francisco's Legion of Honor museum, a sumptuous showcase for Chinese couturier Guo Pei.

It was one of the great fashion moments of the internet era: For the 2015 Met gala, marking the museum's "China: Through the Looking Glass" exhibition, Rihanna electrified the red carpet in a fur-trimmed, canary yellow cape with a traffic-stopping 16-foot train. As the fashion world descended in a joyous frenzy, the designer Guo Pei found, rather instantly, a rapt Western audience. "I did not expect such a strong reaction," Pei says now. In hindsight, she attributes it in part to "people's perception that China did not have fashion or design. Many people used to think of it as a developing country." Later that year, she was invited to join the Chambre Syndicale de la Haute Couture and show on the couture week calendar in Paris.

Though you can draw a straight line from that Yellow Empress cape to "Guo Pei: Couture Fantasy," the retrospective opening this month at the Legion of Honor museum in San Francisco, you won't find it there. The new exhibition mines Pei's career—characterized by sculptural silhouettes and painstaking craftsmanship—both before 2015 and since. (Born in Beijing, Pei studied fashion at Beijing Second Light Industry School in the 1980s and designed children's clothing and womenswear before launching her own label and atelier in 1997.)

"Our Costume program highlights extraordinary designers who have changed the course of fashion history," says Thomas P. Campbell, director and CEO of the Fine Arts Museums of San Francisco (which includes the Legion of Honor). "and Guo Pei is a perfect fit: Her career is emblematic of a shift in global fashion narratives and China's rise as a fashion leader."

The show's 80-plus looks, all sourced from Pei's archives, are sometimes arranged by season or theme (nature, architecture, and China's imperial past have all been creative springboards for the designer) and sometimes coupled with Western artworks—medieval icons, chinoiserie, and examples from the Italian Baroque and French Rococo periods among them. As curator Jill D'Alessandro notes, Pei's work resonates not only across cultures, but across media.



MAXIMUM IMPACT

CLOCKWISE FROM TOP: Rihanna in Guo Pei at the 2015 Met gala; a double-barreled creation from Pei's fall 2019 collection; an embroidered silk robe from fall 2012.

Take, for instance, pieces from her fall 2007 collection, "An Amazing Journey in a Childhood Dream," which open the show. "When the ensembles first debuted, it was theater at its best—a young girl in a canopied bed above the runway dreamed, while models below acted out her dreams in pastel-colored silk confections," D'Alessandro explains. "She paired these with separates embellished with raised metallic-thread embroidery, an homage to the costumes donned by Spanish matadors"—all part of a singular body of work that the curator describes as "occupying a space between fashion, theater, performance, and sculpture." There's another element, too, related to the Chinese concept of *lin* (灵), or "spiritual resonance." For Pei, fashion can also exist in a more personal realm. "Many are moved by my work, but they also say they don't understand why. I create these unwearable clothes," she says. "I think these clothes are needed in the spiritual world." —MARLEY MARIUS

FABRIC: LEGEND OF THE DRAGON, 2012. COPYRIGHT © GUO PEI. ASIAN COUTURE FEDERATION. ALL RIGHTS RESERVED. PHOTOGRAPH BY RANDY DODSON, COURTESY OF THE FINE ARTS MUSEUMS OF SAN FRANCISCO. RIHANNA: DIMITRIOS KAMBOURIS/GETTY IMAGES. DRESS: COPYRIGHT © GUO PEI. ASIAN COUTURE FEDERATION. PHOTOGRAPH BY LIAN XU. ALL RIGHTS RESERVED.

Chinese Couture
She Pei is a perfect fit
San Francisco Legion of Honor
The AR 15 museum of San Francisco in China
Legion of Honor
New York Best in 2015
Guo Pei is
female
2015
1997
started
fashion at
Beijing
Light Industry School
1980s
1997
Agnes
Couture of
Guo Pei
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robe from fall 2012.

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East met west
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China spiritual res



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michael@lozeaudrury.com

April 25, 2022

Via E-mail

Mayor Kerri Howell
Vice-Mayor Rosario Rodriguez
Sarah Aquino
YK Chalamcherla
Mike Kozlowski
Christa Freemantle, City Clerk
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Re: Comments in Opposition to Appeal by Igor Semenyuk of a Decision by the Historic District Commission Denying a Conditional Use Permit for the Lakeside Memorial Lawn Crematorium project (PN 19-182) located at 1201 Forrest Street - April 26, 2022 City Council Meeting, Agenda Item No. 8

Dear Mayor Howell, Vice-Mayor Rodriguez, Councilmembers Aquino, Chalamcherla, and Kozlowski, Director Johns, Ms. Freemantle, and Mr. Kinkade,

These comments are submitted on behalf of Tim & Mariko McGarry, Dave & Kim Higgins, Sean & Niki Gates and Kyal & Bre Von Gunten (hereinafter "Residents"), all of whom reside in the Folsom Historic District in close proximity to the proposed Lakeside Memorial Lawn Crematorium project. The Residents request that the Council deny Mr. Semenyuk's appeal. The record supports the Historic Commission's application of its discretion to find that the proposed crematorium, if allowed, would impact the historic character of the historic Lakeside Memorial Lawn as well as the unique historic assemblage it shares with the Chung Wah Cemetery, a site included on the National Register of Historic Places. As a result, the Council should uphold the Commission's finding that the proposed crematorium would be detrimental or injurious to property and improvements in the neighborhood and the general welfare of the City.

The Council also should uphold the Commission's denial because the project's proposed expansion of the accessory maintenance shed and its accessory use as a crematorium violates Municipal Code §§ 17.52.680 and 17.52.690 and the Folsom Historic District Design Guidelines. The City's Code and Design Guidelines prohibit any increases in scope of nonconforming structures or uses within the Historic District. The proposed crematorium would increase a non-

Folsom City Council
 April 25, 2022
 Page 2 of 8

conforming use as well as the nonconforming maintenance building by adding an exhaust stack that, according to the drawings attached to the MND would extend at least 19 feet above the existing roof line of the maintenance shed.

In addition, the Council should uphold the Commission's decision because the Initial Study and Mitigated Negative Declaration ("IS/MND") prepared for the crematorium project does not comply with the California Environmental Quality Act ("CEQA"), Pub. Res. Code §21000, *et seq.* First, the IS/MND's description of the project is unstable and inconsistent by evaluating a 19.5 feet high exhaust stack in the toxic pollutant dispersion analysis while at the same time depicting a 29.5 feet exhaust stack in the drawings of the project. Second, the record contains substantial evidence of a fair argument that the crematorium project may have a significant impact on historic resources requiring the preparation of an environmental impact report ("EIR") before the project may be considered for approval.

The following comments elaborate on these deficiencies. The Council should defer to the thorough evaluation and findings prepared by the Commission and should further uphold the denial of the crematorium by finding that the project would be inconsistent with the Municipal Code and Design Guidelines and the IS/MND is deficient pursuant to CEQA.

A. The Crematorium Project Would Violate the Municipal Code and Folsom Historic District Design Guidelines.

The City's Municipal Code governing the Historic District prohibits the proposed nonconforming crematorium use and the proposed expansion of the maintenance building structure. The City's Zoning Code establishes a comprehensive program not only to preserve the historic character of the Historic District but also to enhance it. Thus, core purposes of the zoning for the historic district include "[t]o preserve and enhance the historic, small-town atmosphere of the historic district as it developed between the years 1850 and 1950; ... [t]o ensure that new ... commercial development is consistent with the historical character of the historic district as it developed between the years 1850 and 1950; ... and "[t]o preserve and enhance open space areas." (Folsom Muni. Code § 17.52.010(B)(1), (5) & (7).)

The existing maintenance shed in which the crematorium is proposed to be located is an "accessory structure" as defined by the Guidelines, §4.06. The shed was constructed sometime between 1993 and August 1998. (Agenda Packet, p. 323.) The shed predates the City's historic district zoning ordinance which was enacted in October 1998. Thus, the shed was constructed at the time without consideration of the criteria established in the Code and the October 1998 Design Guidelines. Indeed, the pictures of the shed confirm that it was not designed to comply with any historic design criteria or guidelines. Rather than reflect any historic character of the District between 1850 and 1950, the shed is a metal shed constructed in the late 1990s.

As a result, the shed appears to be a Nonconforming Structure pursuant to the Guidelines. "A structure or part thereof is determined to be legally nonconforming if it was legally in place on the effectiveness date of Ordinance No. 890 but by reason of style, construction or placement it does not meet the standards or intent of the Primary Area or Subarea cause a structure or part thereof to be deemed nonconforming." (Guidelines, §4.16.02.) The Code and the Guidelines make the continuation of a nonconforming structure, like the shed, contingent on several conditions,

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including that “[t]he nonconformity is not increased....” (Code §17.52.690(B)(2); Guidelines § 4.16.02.)

The crematorium project proposes to expand the nonconforming, late-1990’s shed structure to include a prominent exhaust stack. As a result, the project proposes to expand the nonconformity of the existing shed. The addition of the vent stack is significantly more prominent than suggested in the renderings provided by the applicant. The residents are informed and believe that the rear of the maintenance shed, where the proposed crematorium would be positioned, is 10 feet in height. The drawings of Hartwick Combustion Technologies’ Apex 250 crematory clearly depict an exhaust stack extending 354.11 inches, *i.e.*, 29.5 feet, from ground level. (Agenda Packet, p. 255.) As a result, the exhaust stack will extend about 19.5 feet above the roof of the existing shed. The rendering attached to the IS/MND appears to suggest a stack extending a few feet above the rear roof of the shed. (*Id.*, pp. 261. 262.) In either event, the addition of the exhaust stack, whether extending 19.5 feet or a few feet above the shed, is an expansion of the nonconforming structure and hence prohibited by the Code and Guidelines.

The staff report prepared for the Historic Commission suggests that “[t]he height of the stack is determined during the SCAQMD permitting process in order to meet their air quality standards.” (Agenda Packet, p. 220.) However, the City Council staff report now notes that the “[Air District] representative noted that the Air District does not advocate for a specific height unless the height proposed by the applicant does not meet the district’s air quality standards for nearby sensitive receptors.” (*Id.*, p. 194.) In preparing its air pollution modeling, “HELIX commented that the analysis done for the IS/MND assumed a height of 19.5 feet from finished grade based on specifications provided to them by the applicant.” (*Id.*) However, the only stack height reflected in the materials attached to the IS/MND is that provided by Hartwick showing a 29.5 feet stack.

Whatever the actual height of the stack may be, it is a prohibited expansion of a nonconforming structure. The Guidelines recognize that mechanical equipment, such as a crematorium exhaust stack, are generally incompatible “with the design time frames of much of the Historic District and [have] inherent aesthetic drawbacks....” (Guidelines, §4.03.) For this reason alone, the Council should uphold the Commission and deny the permit.

The Historic District Zoning Code also provides for nonconforming uses if the use was “legally in place as of the effective date of the ordinance codified in this chapter[,]” *i.e.* October 1998. (Code §17.52.680.) As a result, no new nonconforming use can be approved by the City. The open space areas of the historic district permit a “cemeteries” use. (Code §17.52.550(A)(4). Cemetery is defined as “a burial ground.” The New Oxford American Dictionary, p. 275 (Oxford University Press (2d ed. 2005). On the other hand, a “crematorium” or “crematory” is “a place where a dead person’s body is cremated.” (*Id.*, p. 398.) The only accessory uses contemplated by the Historic District open space zoning are “[r]esidential uses accessory to a public use.” (*Id.*, §17.52.550(A)(7).) Whether a crematory would qualify as an accessory use to a cemetery in general, as discussed by staff in the Historic District Commission staff report, does not address the specific language of the City’s zoning code which specifies only one type of permitted accessory use for this subarea of the historic district. (Agenda Packet, p. 209.) Indeed, there is no general authority at all for accessory uses in the zoning code for the Historic District, with the exception of accessory uses associated with accessory structures for residences. (*See* Code §17.52.540.) The “[r]esidential uses accessory to a public use” is consistent with the limited accessory use permitted

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in the open space subarea. The Council cannot, however, extend that authorization to other new non-residential accessory uses, such as a crematory. For this reason, the Council should uphold the Commission's denial of the permit.

B. The IS/MND is Insufficient and Cannot be Relied Upon to Reverse the Commission's Decision and Approve the Project.

The IS/MND is unlawful pursuant to CEQA because the project description is unstable and because substantial evidence shows the project may have a significant impact on historic resources requiring the preparation of an EIR. Because the IS/MND is inadequate, the Council cannot overturn the Commission's denial and approve the Project without first preparing an EIR.

1. The IS/MND's description of the project is unstable and incomplete.

As discussed above, the City's zoning code of the Historic District highlights the importance of not expanding structures that are not consistent with the historic period the District is intended to preserve and enhance. Despite the importance of such proposed changes, the IS/MND fails to adequately describe the extent of the exhaust stack that must be installed for the project. The one hard piece of evidence attached to the IS/MND depicts the stack with an accompanying measured height of 354.11 inches, *i.e.*, 29.5 feet. (Agenda Packet, p. 255.) The IS/MND's air modeling assumes a height of 19.5 feet based on specifications provided by the applicant. (*Id.*, pp. 194, 305, 395, 430.) But no other written height specifications besides the manufacturer's drawing depicting a measured height of 29.5 feet are in the record. The importance of the stack height is highlighted by the comments by the Historic District Commissioner's uncertainty over the height of the stack. (*Id.*, p. 194.) The manufacturer's drawing indicates that the stack would extend 19.5 above the shed roof. (*Id.*, p. 255.) The IS/MND indicates that the stack would extend 10 feet above the roof. (*Id.*, p. 296.) However, as noted above, the rendering provided by the applicant shows a stack of no more than 2 to 3 feet. (*Id.*, p. 261.)

A negative declaration must accurately describe the proposed project. (*Christward Ministry v. Superior Court* (1986) 184 Cal.App.3d 180; CEQA Guidelines §15071(a).) The initial study must "provide documentation of the factual basis for the finding in a Negative Declaration that a project will not have a significant effect on the environment." (CEQA Guidelines § 15063(c)(5).) "An accurate, stable and finite project description is the sine qua non of an informative and legally sufficient [CEQA document]." (*County of Inyo v. City of Los Angeles* (1977) 71 Cal.App.3d 185, 193.) "[A] project description that gives conflicting signals to decision makers and the public about the nature and scope of the project is fundamentally inadequate and misleading." (*Washoe Meadows Community v. Department of Parks & Rec.* (2017) 17 Cal.App.5th 277, 287, quoting *Citizens for a Sustainable Treasure Island v. City and Cty. of San Francisco* (2014) 227 Cal.App.4th 1036, 1052.) "A curtailed or distorted project description may stultify the objectives of the reporting process." (*Cty. of Inyo*, 71 Cal.App.3d at 192.) "Only through an accurate view of the project may affected outsiders and public decision-makers balance the proposal's benefit against its environmental cost, consider mitigation measures, assess the advantage of terminating the proposal ..., and weigh other alternatives in the balance." (*Id.*)

Where a lead agency relies on an insufficient project description, "the problem ... is not confined to 'the informative quality of the [environmental document]'s environmental forecasts.'" (*Stopthemillenniumhollywood.com v. City of Los Angeles* (2019) 39 Cal.App.5th 1, 17, citing

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Washoe Meadows, 17 Cal.App.5th at 288.) “A curtailed, enigmatic or unstable project description draws a red herring across the path of public input.” (*City of Inyo*, 71 Cal.App.3d at 198.) “The omission of relevant information is deemed prejudicial regardless of whether a different outcome would have resulted if the public agency had complied with those provisions.” (*Stoepthemillennium*, 39 Cal.App.5th at 17 [citations omitted].)

The IS/MND’s failure to provide a stable description of the exhaust stack for the proposed crematorium is fatal to the IS/MND. Because one cannot discern the actual height of the proposed stack, the project’s aesthetic impacts were not properly evaluated. Likewise, the extent of the project’s impacts on historic resources is not disclosed given the uncertainty of the stack height and impedes the IS/MND’s evaluation of the project’s consistency with the zoning code restrictions. Lastly, the air modeling is flawed, relying on a stack height that is 10 feet shorter than depicted and unsupported by any substantial evidence in the record. Accordingly, the IS/MND cannot be relied upon for any approval of the Project.

2. Substantial evidence in the record demonstrates a fair argument that the project may have significant impacts on cultural resources.

The IS/MND also is fatally flawed because substantial evidence in the record establishes a fair argument that the project may have a significant impact on historic resources. As a result, an EIR rather than a MND must be prepared for the project prior to any approval.

In 2003, the City’s staff prepared a report concluding that a crematorium in the same location would have significant impacts on historic resources. The 2003 staff recommendation concluded that adding a crematory use does not maintain the historic use of the site. (Agenda Packet, p. 777.) The report also found that adding a crematorium would discourage the inclusion of the Lakeside Cemetery on the State and National Historic Registers. (*Id.*) The report further cited evidence that a crematory use is not a consistent use with a historic cemetery. (*Id.*) As a result of the staff’s report and the recommendation to deny the permit for the proposed crematory, the applicant withdrew the project. Nevertheless, the staff’s report remains substantial evidence. (*See, e.g. Young v. City of Coronado* (2017) 10 Cal.App.5th 408, 433 (staff report included substantial evidence to support City’s ultimate decision to designate a property as a historic resource).)

As the California Supreme Court held, “[i]f no EIR has been prepared for a nonexempt project, but substantial evidence in the record supports a fair argument that the project may result in significant adverse impacts, the proper remedy is to order preparation of an EIR.” (*Communities for a Better Environment v. South Coast Air Quality Management Dist.* (2010) 48 Cal.4th 310, 319-320 [“*CBE v. SCAQMD*”], citing, *No Oil, Inc. v. City of Los Angeles* (1974) 13 Cal.3d 68, 75, 88; *Brentwood Assn. for No Drilling, Inc. v. City of Los Angeles* (1982) 134 Cal.App.3d 491, 504–505.) “The ‘foremost principle’ in interpreting CEQA is that the Legislature intended the act to be read so as to afford the fullest possible protection to the environment within the reasonable scope of the statutory language.” (*Communities for a Better Environment v. Calif. Resources Agency* (2002) 103 Cal.App.4th 98, 109 [“*CBE v. CRA*”].)

The EIR is the very heart of CEQA. (*Bakersfield Citizens for Local Control v. City of Bakersfield* (2004) 124 Cal.App.4th 1184, 1214; *Pocket Protectors v. City of Sacramento* (2004) 124 Cal.App.4th 903, 927.) The EIR is an “environmental ‘alarm bell’ whose purpose is to alert the public and its responsible officials to environmental changes before they have reached the

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ecological points of no return.” (*Bakersfield Citizens*, 124 Cal.App.4th at 1220.) The EIR also functions as a “document of accountability,” intended to “demonstrate to an apprehensive citizenry that the agency has, in fact, analyzed and considered the ecological implications of its action.” (*Laurel Heights Improvements Assn. v. Regents of University of California* (1988) 47 Cal.3d 376, 392.) The EIR process “protects not only the environment but also informed self-government.” (*Pocket Protectors*, 124 Cal.App.4th at 927.)

An EIR is required if “there is substantial evidence, in light of the whole record before the lead agency, that the project may have a significant effect on the environment.” (Pub. Resources Code, § 21080(d); see also *Pocket Protectors*, 124 Cal.App.4th at 927.) In limited circumstances, an agency may avoid preparing an EIR by issuing a negative declaration, a written statement briefly indicating that a project will have no significant impact thus requiring no EIR (14 Cal. Code Regs., § 15371 [“CEQA Guidelines”]), only if there is not even a “fair argument” that the project will have a significant environmental effect. (Pub. Resources Code, §§ 21100, 21064.) Since “[t]he adoption of a negative declaration . . . has a terminal effect on the environmental review process,” by allowing the agency “to dispense with the duty [to prepare an EIR],” negative declarations are allowed only in cases where “the proposed project will not affect the environment at all.” (*Citizens of Lake Murray v. San Diego* (1989) 129 Cal.App.3d 436, 440.)

Where an initial study shows that the project may have a significant effect on the environment, a mitigated negative declaration may be appropriate. However, a mitigated negative declaration is proper *only* if the project revisions would avoid or mitigate the potentially significant effects identified in the initial study “to a point where clearly no significant effect on the environment would occur, and . . . there is no substantial evidence in light of the whole record before the public agency that the project, as revised, may have a significant effect on the environment.” (Public Resources Code §§ 21064.5 and 21080(c)(2); *Mejia v. City of Los Angeles* (2005) 130 Cal.App.4th 322, 331.) In that context, “may” means a *reasonable possibility* of a significant effect on the environment. (Pub. Resources Code, §§ 21082.2(a), 21100, 21151(a); *Pocket Protectors*, 124 Cal.App.4th at 927; *League for Protection of Oakland's etc. Historic Resources v. City of Oakland* (1997) 52 Cal.App.4th 896, 904–905.)

Under the “fair argument” standard, an EIR is required if any substantial evidence in the record indicates that a project may have an adverse environmental effect—even if contrary evidence exists to support the agency’s decision. (CEQA Guidelines, § 15064(f)(1); *Pocket Protectors*, 124 Cal.App.4th at 931; *Stanislaus Audubon Society v. County of Stanislaus* (1995) 33 Cal.App.4th 144, 150–15; *Quail Botanical Gardens Found., Inc. v. City of Encinitas* (1994) 29 Cal.App.4th 1597, 1602.) The “fair argument” standard creates a “low threshold” favoring environmental review through an EIR rather than through issuance of negative declarations or notices of exemption from CEQA. (*Pocket Protectors*, *supra*, 124 Cal.App.4th at 928.)

The “fair argument” standard is virtually the opposite of the typical deferential standard accorded to agencies. As a leading CEQA treatise explains:

This ‘fair argument’ standard is very different from the standard normally followed by public agencies in making administrative determinations. Ordinarily, public agencies weigh the evidence in the record before them and reach a decision based on a preponderance of the evidence. [Citations]. The fair argument standard, by contrast, prevents the lead agency from weighing competing evidence to determine who has a better

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argument concerning the likelihood or extent of a potential environmental impact. The lead agency's decision is thus largely legal rather than factual; it does not resolve conflicts in the evidence but determines only whether substantial evidence exists in the record to support the prescribed fair argument.

(Kostka & Zishcke, *Practice Under CEQA*, §6.29, pp. 273-274.) The Courts have explained that "it is a question of law, not fact, whether a fair argument exists, and the courts owe no deference to the lead agency's determination. Review is de novo, with a **preference for resolving doubts in favor of environmental review.**" (*Pocket Protectors*, 124 Cal.App.4th at 928 [emphasis in original].)

Because the prior staff report concluded that essentially the same crematorium project would significantly affect the surrounding historic resources, that report is substantial evidence of a fair argument that the current crematorium project may have a significant adverse effect on the same historic resources. In *Stanislaus Audubon Society, Inc. v. County of Stanislaus* (1995) 33 Cal.App.4th 144, the court rejected a county's argument that a revised initial study prepared by the county which contradicted the findings of the first initial study had not "relegated the first initial study to oblivion." (*Id.* at 154.) The court stated, "We analogize such an untenable position to the unringing of a bell. The first initial study is part of the record. The fact that a revised initial study was later prepared does not make the first initial study any less a record entry nor does it diminish its significance...." (*Id.* at 154) The City cannot conclude that a project may have significant impacts and then, when such admission is no longer convenient, simply change its conclusion to better suit its needs. Here, the analysis from the 2003 staff report, itself substantial evidence, creates a "fair argument" that the crematory project may have significant impacts on the surrounding historic resources, despite other evidence to the contrary. (*See, Id.; Gentry v. Murietta* (1995) 36 Cal.app.4th 1359 (petitioner may rely on statements made in initial study to establish fair argument, even in the face of contradictory evidence).)

The unsupported assertion that cremation furnace technology has significantly evolved since 2003 is not supported by the record. The type of equipment and size described for the 2003 project is essentially the same as that proposed for the current project. (Compare Agenda Packet, pp. 783-84 and 255-257.) There is no evidence in the record pointing to any differences in the performance of the two brands of equipment. (*Id.*) As a result, the assertion does not provide any evidence to distinguish the 2003 staff report's conclusions from the current project.

The City Council Staff Report further confirms that there is a fair argument that the project may have significant impacts on historic resources. Staff identifies the letters "sent by the Cemetery Advisory Commission [which] stated that the crematorium would be inappropriate near the surrounding cultural resources of historic importance and could threaten the nomination of the Young Wo Cemetery and adversely affect the historic features and burials of those buried nearby." (Agenda Packet, p. 198.) Likewise, the fact that the Historic Commission itself, the City's own expert agency on evaluating historic impacts in the Historic District, determined that the project would have significant impacts on the adjacent historic resources is itself substantial evidence of a fair argument that the project may have such impacts, requiring an EIR. (*See Stanislaus Audubon Society*, 33 Cal.App.4th at 154.)

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C. The Commission's Findings are Supportedd by the Record.

The above reasons that the City Council cannot overturn the Commission's denial are in addition to reasons articulated by the Commission in its findings and which are supported by substantial evidence in the record. In addition to the evidence provided by the Sacramento County Historic Cemetery Commission and the City's prior staff analysis, additional substantial evidence from expert organizations has been submitted. The Chinese Historical Society of America has reviewed the proposed project and submitted comments describing evidence of impacts to persons' experience of the cultural significance of the adjacent cemeteries. (Agenda Packet, pp. 554-556.) Likewise, the Heritage Preservation League of Folsom provided comments describing shortcomings in the IS/MND and potential impacts to the surrounding historic resources. (*Id.*, pp. 611-612.)

For all of these reasons, the Residents respectfully request that the City Council deny the appeal and uphold the Historic Commission's denial of the conditional use permit for the proposed crematorium. Thank you for your consideration of these comments.

Sincerely,



Michael R. Lozeau

Lozeau Drury LLP

on behalf of Tim & Mariko McGarry, Dave & Kim Higgins,
Sean & Niki Gates and Kyal & Bre Von Gunten



CITY OF
FOLSOM
DISCOVERED BY NATURE

Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	Public Hearing
SUBJECT:	<p>Folsom Corporate Center Apartments – South side of Iron Point Road, east of the intersection of Iron Point Road and Oak Avenue Parkway (PN 21-120)</p> <p>i. Resolution No. 10849 - A Resolution to Adopt a Mitigated Negative Declaration, Approve a General Plan Amendment, and Approve a Planned Development Permit for the Folsom Corporate Center Apartments project</p> <p>ii. Ordinance No. 1327 – An Uncodified Ordinance to amend the zoning designation for a 7.24-acre parcel (Lot 1) from M-L PD to R-4 PD and to amend the zoning designation for a 4.68-acre parcel (Lot 6) from BP PD to R-4 PD for the Folsom Corporate Center Apartments project (Introduction and First Reading)</p>
FROM:	Community Development Department

RECOMMENDATION / CITY COUNCIL ACTION

Move to Adopt Resolution No. 10849 - A Resolution to Adopt a Mitigated Negative Declaration, Approve a General Plan Amendment, and Approve a Planned Development Permit for the Folsom Corporate Center Apartments project

And

Move to Introduce and Conduct First Reading of Ordinance No. 1327 - An Uncodified Ordinance to amend the zoning designation for a 7.24-acre parcel (Lot 1) from M-L PD to R-4 PD and to amend the zoning designation for a 4.68-acre parcel (Lot 6) from BP PD to R-4 PD for the Folsom Corporate Center Apartments project (Introduction and First Reading)

BACKGROUND / ISSUE

On August 15, 2000, the City Council approved a Tentative Subdivision Map and Planned Development for development of a 1.425-million-square-foot professional office center known as the Folsom Corporate Center. On May 1, 2002, the Planning Commission approved a Planned Development Permit and Conditional Use Permit for development of a 255,795-square-foot retail shopping center known as Folsom Gateway within the eastern portion of the previously approved Folsom Corporate Center. That approval resulted in the reduction of 395,000 square feet of office space within the Folsom Corporate Center.

A total of four professional office buildings have been developed within the Folsom Corporate Center with major tenants including HDR Engineering, Kaiser Permanente, Micron Technology, and SAFE Credit Union.

- On January 26, 2016, the City Council approved the development of the 126-unit senior retirement community known as the Iron Point Retirement Community on a 4.68-acre property located at 2275 Iron Point Road.
- On October 4, 2017, the Planning Commission approved an extension to the previous approval for the Iron Point Retirement Community project.
- On February 6, 2019, the Planning Commission approved an additional one-year extension to the project. Subsequently, the applicant decided not to pursue project development and withdrew their application. It is important to note that the 4.68 parcel associated with Iron Point Retirement Community project is one of the parcels (Lot 6) included with the proposed Folsom Corporate Center Apartments project.
- On October 7, 2020, the Planning Commission approved a 11,716-square-foot single-story medical building (Kidney Dialysis Treatment Center) on a 2.77-acre site located near the southwest corner of the intersection of Iron Point Road and Rowberry Drive within the Folsom Corporate Center. The Kidney Dialysis Treatment Center is currently under construction and is located directly to the east of one of the parcels (Lot 1) associated with the proposed Folsom Corporate Center Apartments project.

The applicant, FCC 50, LLC (Cole Partners), is requesting approval of a General Plan Amendment, Rezone, and Planned Development Permit for development of a 253-unit market-rate apartment community on two parcels (Lot 1: 7.24-acre parcel and Lot 6: 4.68-acre parcel) within the Folsom Corporate Center, which is generally located on the south side of Iron Point Road, east of the intersection of Iron Point Road and Oak Avenue Parkway.

The applicant's request for approval of a General Plan Amendment, Rezone, and Planned Development Permit was considered by the Planning Commission at its April 6, 2022 meeting. At this meeting, the Planning Commission discussed several items associated with the proposed project including land use impacts, regional housing needs and affordability, traffic impacts, pedestrian connectivity, vehicle parking, and oak tree preservation and mitigation.

In relation to land use, the Commission indicated that they were supportive of the proposed change in land use from commercial office to multi-family residential based on changing market dynamics relative to the office market in Folsom and the region. The Commission also commented that the proposed change in land use could potentially create a more vibrant mixed-use environment. A detailed discussion regarding the project's land use impacts is contained within the General Plan Amendment and Rezone section of this staff report. Below is a summary of the issues discussed at the Planning Commission meeting (a more detailed discussion of these items is found in the analysis section below):

- **RHNA**- The City's progress with meeting the RHNA numbers was discussed. City staff shared a table with the Commission that showed that 523 affordable housing units have been constructed in the City to date this year, with a total remaining RHNA obligation for construction of 5,840 housing units across all income levels. The Commission inquired as to whether the applicant had considered incorporating affordable housing units into the proposed apartment development. The applicant responded that their expertise is in developing and managing market-rate apartment communities and that their business model for this particular project would not accommodate any affordable units.
- **Traffic**- The traffic consultant for the project provided an overview of the Transportation Impact Study (Attachment 21) that was prepared for the proposed project and stated that the project is anticipated to generate a low volume of vehicle trips including 81 AM peak hours trips and 104 PM peak hour trips. Based on the low number of vehicle trips, the traffic consultant stated that the project is not expected to have a significant impact on level of service (LOS) at any of the 17 study intersections including the Iron Point Road/East Bidwell Street intersection. The traffic consultant also noted that the proposed project would not have a significant impact relative to Vehicle Miles Traveled (VMT).
- **Pedestrian Connectivity** - the Commission discussed whether it would be feasible to provide additional pedestrian connections from Lot 1 and Lot 6 to the existing sidewalk located along the south side of Iron Point Road. The applicant indicated that they had evaluated the possibility of providing these additional pedestrian connections but because of significant grade changes and because the sidewalk would be on an adjacent property not controlled by the applicant it was determined to be infeasible.
- **Parking** - The Commission discussed the parking requirements of the proposed project and inquired whether City staff thought that the project may be overparked. City staff responded that the proposed project meets the parking requirements established by the Folsom Municipal Code and the parking recommendations of the Design Guidelines for Multifamily Development. City staff further commented that the City does not require development projects to exceed the parking requirements established by the

Folsom Municipal Code and the Design Guidelines, but rather meet the minimum parking requirements and recommendations of these documents.

- **Oak Tree Mitigation** - The Commission engaged in a lengthy discussion regarding oak tree preservation and mitigation. The applicant provided an overview regarding the initial planning and development of the Folsom Corporate Center and how designated oak tree preserve areas were created to preserve as many oak trees as possible. With respect to this specific project, the applicant indicated that they are preserving as many trees as possible including preservation of a 41” diameter Heritage Blue Oak tree on Lot 1. The applicant also noted that they were planning to mitigate for the impact to oak trees by planting 35 Mitigation Oak trees on the project site.

Ultimately, the Planning Commission voted 4-1-0 (2 Commissioners absent) to recommend to the City Council approval of the proposed project, subject to the findings included with this report.

POLICY / RULE

The Folsom Municipal Code (FMC) requires that applications for General Plan Amendments and Rezones be forwarded to the City Council for final action. City Council actions regarding General Plan Amendments and Rezones are covered under Section 17.68.050 of the Folsom Municipal Code.

ANALYSIS

As noted above, the applicant is requesting approval of three entitlements to allow for development of the proposed apartment community. The first entitlement is a request for approval of a General Plan Amendment to change the General Plan land use designation for the two project parcels (Lot 1 and Lot 6) from IND (Industrial/Office Park) to MHD (Multi-Family High Density). The second entitlement is a request for approval of a Rezone to change the zoning designation for Lot 1 from M-L PD (Limited Manufacturing, Planned Development District) to General Apartment, Planned Development District (R-4 PD) and to change the zoning designation of Lot 6 from BP PD (Business and Professional, Planned Development District) to General Apartment, Planned Development District (R-4 PD). The third entitlement is a request for approval of a Planned Development Permit to establish project-specific development standards, review the project site design, evaluate the architectural design of the multi-family apartment and clubhouse buildings, and establish signage criteria.

The proposed Folsom Corporate Center Apartments project, which includes development of 11 three-story apartment buildings and two clubhouse buildings (three-story and one-story buildings respectively), is comprised of 253 market rate apartments within a gated community. The apartment buildings include a combination of 16-plex buildings, 21-plex buildings, 26-plex building, and 32-plex buildings with a total of 16 studio units (564 square feet), 126 one-bedroom units (687 square feet), 97 two-bedroom units (990-1057 square feet), and 14 three-

bedroom units (1,412 square feet). All apartment units are proposed to be accessible from interior hallways and include a full kitchen, living space, storage closets, bedrooms, bathrooms, and an outdoor patio/balcony. The one and three-story clubhouse buildings include a recreation room, a fitness center, a yoga studio, a spa room, a mail room, a bike storage facility, leasing offices, a storage room, and restroom facilities. Outdoor amenities associated with the clubhouse buildings include a pool, a spa, and deck areas. Additional outdoor amenities include two dog parks.

In relation to site design, Lot 1 includes seven rectangular apartment buildings that are evenly spaced within the eastern portion of parcel due to constraints associated with overhead transmission lines situated in the western portion of the parcel. Lot 6 includes four rectangular apartment buildings which are centrally located on the parcel.

The applicant proposes a modern contemporary architectural design theme intended to compliment the surrounding commercial buildings within the Folsom Corporate Center. Modern and unique design elements include angular building shapes and forms, varied roof heights, flat rooftops, recessed building elements, metal canopies, and extensive use of glass. Proposed building materials include stucco walls, stone veneer wainscoting, metal canopies, glass railing, and metal railing. The color scheme for the buildings is proposed to be generally earth tone, with extensive use of gray and brown colors accented by a mixture of lighter colors including white and tan.

General access to the project area is provided by three existing driveways located on the south side of Iron Point Road. Primary vehicle access to Lot 1 is provided by a new driveway on south side of an existing private ring road with secondary access accommodated by two emergency vehicle access driveways also situated on the south side of the ring road. Primary vehicle access to Lot 6 is provided by a new driveway on the north side of the private ring road with secondary access served by an emergency vehicle access driveway also positioned on the north side of the ring road. Each of the project driveways will accommodate all vehicle turning movements into and out of the respective sites. In addition, all project driveways will have access controlled by vehicle gates.

Proposed internal vehicle circulation consists of 27-foot-wide drive aisles to facilitate movement in and around the project sites. Pedestrian circulation is provided by a combination of new sidewalks and existing sidewalks located along the private ring road and along Iron Point Road. Internal pedestrian circulation is accommodated by a series of new pedestrian pathways that provide connectivity to the apartment buildings, the clubhouse building, the perimeter sidewalks, and the future Class I trail to the south. Additional site improvements include: 491 parking spaces (includes combination of garage, carport, and uncovered spaces), 51 bicycle parking spaces, 5 electric vehicle charging stations, underground utilities, drainage basins, site lighting, site landscaping, retaining walls, fencing, and project identification signs.

A. General Plan Amendment and Rezone

General Plan Amendment and Rezone

The Folsom Corporate Center Apartments project is comprised of two separate parcels, Lot 1, which is 7.24-acres in size and Lot 6, which is 4.68-acres in size. Lot 1 and Lot 6 each have a General Plan land use designation of IND (Industrial/Office Park). As shown on Attachment 6, the proposed project includes a request to change the General Plan land use designation for both parcels from IND (Industrial/Office Park) to MHD (Multi-Family High Density). Lot 1 currently has a Zoning designation M-L PD (Limited Manufacturing, Planned Development District), while Lot 6 has a zoning designation of BP PD (Business and Professional, Planned Development District). As shown on Attachment 7, the proposed project includes a request to change the zoning designation for Lot 1 from M-L PD (Limited Manufacturing, Planned Development District) to R-4 PD (General Apartment, Planned Development District) and to change the zoning designation of Lot 6 from BP PD (Business and Professional, Planned Development District) to R-4 PD (General Apartment, Planned Development District). With approval of the proposed amendments and rezones, the entire project site will have a General Plan land use designation of MHD and a Zoning designation of R-4 PD.

The project is consistent with both the proposed General Plan land use designations and the proposed zoning designations, as multi-family apartments are identified as a permitted land use within the Folsom Municipal Code (FMC, Section 17.18.020 Permitted Uses). The proposed project includes a density of 21.2 dwelling units per acre, is consistent with the allowable density range (20-30 dwelling units per acre) established by the General Plan for Multi-Family High Density (Table LU-1: Residential Designations). In addition, the proposed project meets the development requirements established by the Folsom Municipal Code (FMC, Chapter 17.18, General Apartment District) and the Folsom Corporate Center Planned Development Guidelines with some minor modifications (discussed within the Planned Development Permit section of this staff report). Proposed modifications to development standards include lot area, lot width, building coverage, building height, building setbacks, and parking, which are discussed in the Planned Development Permit section of this staff report.

In reviewing the proposed General Plan Amendment and the Rezone, staff took into consideration community benefits that the proposed apartment project will provide relative to the supply of new housing units. City staff also considered the changes in the region's office and housing markets over the past 10 to 15 years. According to the California Department of Housing and Community Development (HUD), the state of California is facing a severe shortage regarding housing supply, with some estimates indicating a shortfall of up to 3.5 million housing units. The housing shortage has a number of significant negative effects including but not limited to causing housing prices to rise which limits affordability and increasing the homeless population in communities. The benefit of the proposed project is that

it will increase the City's housing supply by providing 253 new market-rate rental units along the Iron Point Road corridor in close proximity to jobs and services in that area of the City.

Cole Partners, who is the original developer of the 900,000-square-foot Folsom Corporate Center, described efforts to bring new medical and office uses to the Folsom area over the last two decades. Since inception of the Corporate Center in 2000, the development has attracted prominent medical and office companies including Kaiser Permanente, Micron, and SAFE Credit Union. However, the applicant describes changing regional market dynamics over the last decade (changes in technology, acceptable of telecommuting, etc.) with the interest in housing projects far outpacing the demand for new office development. It has been more than 12 years since any new major office buildings (Waste Connections/SAFE Credit Union and Numonyx/Micron) were constructed within the Corporate Center. Notably, these two office buildings are the last privately developed larger suburban office buildings completed not only in Folsom, but along the Highway 50 corridor. While the office market dynamic has changed in a negative way, the regional demand for housing (single-family and multi-family) continues to remain extremely strong, especially in Folsom with a range of multi-family projects (Alder Creek Apartments, Avenida Senior Apartments, Mangini Ranch Apartments, Scholar Way Apartments, etc.) being approved recently. Based on these factors, staff has determined that the proposed changes in land use and zoning are warranted.

Land Use Compatibility

In evaluating the General Plan Amendment and the Rezone, staff also took into consideration the compatibility of the proposed project relative to existing land uses in the project area. The proposed project is located on two undeveloped parcels within the Folsom Corporate Center. The project site is bounded by Iron Point Road to the north with single-family residential development (Broadstone Unit. No. 2) and multi-family residential development (Sherwood Apartments) beyond, U.S. Highway 50 to the south with undeveloped properties within the Folsom Plan Area beyond, multi-family development (Revel Senior Living and CountryHouse Memory Care) to the west with future Oak Avenue Parkway extension and commercial development beyond, and commercial development to the east with East Bidwell Street beyond.

The most prominent land uses in the immediate project area are professional office-related and include SAFE Credit Union, Micron, Kaiser Permanente, and HDR. Residential land uses in close proximity to the site include the Broadstone Unit No. 2 Subdivision (approximately 150 feet to the north across Iron Point Road), Sherwood Apartments (approximately 400 feet to the northeast across Iron Point Road), and Revel Senior Living Apartments (approximately 500 feet to the west). Medical-office related land uses in the project vicinity include the aforementioned Kaiser Permanente Medical Office facility and the Kaiser Permanente Surgery Center. The nearest retail commercial development (Folsom Gateway Shopping Center, which was also developed by a Cole-related entity) is located approximately 1,200 feet to the east of

the project site. Additional retail commercial development is located north of Iron Point Road (Palladio at Broadstone), approximately 3,100 feet east of the project site.

As described above, the project site is situated in a unique location that includes a wide array of land uses including professional offices, medical offices, retail shopping, multi-family apartments, single-family residences, and a memory care facility. As mentioned within the project description, the Folsom Corporate Center Apartments project is a market-rate apartment community providing living opportunities for residents within 253 apartment units. Given the residential nature of the proposed use, staff has determined that the proposed project will be complimentary to the existing multi-family and single-family residential land uses located in the immediate project vicinity. In addition, considering the basic needs of the apartment residents, staff has determined that the proposed project is well-situated to take advantage of the numerous goods (grocery store, restaurants, and retail shops) and services (medical offices) and job opportunities that are located within walking distance of the site.

B. Planned Development Permit

The purpose of the Planned Development Permit process is to allow greater flexibility in the design of integrated developments than otherwise possible through strict application of land use regulations. The Planned Development Permit process is also designed to encourage creative and efficient uses of land. The following are proposed as part of the applicant's Planned Development Permit:

- Development Standards
- Building Architecture and Design
- Signage

Development Standards

The Folsom Corporate Center includes development standards that were intended to guide commercial development and did not consider that residential development might occur within the boundaries of the Corporate Center. As a result, the applicant's intent with the subject application is to create a set of unique set of development standards that are better suited for multi-family residential development, yet still generally comply with the development standards established for properties within the Folsom Corporate Center as well as being consistent with the development standards established for properties within the General Apartment (R-4) zoning district. Table 1 on the following page lists the existing and proposed development standards for the Folsom Corporate Center Apartments project.

TABLE 1: DEVELOPMENT STANDARDS TABLE

Development Standards Table Folsom Corporate Center Apartments						
	Lot Area	Lot Width	Front Yard Setback	Rear Yard Setback	Side Yard Setbacks	Building Height
Existing Standards	0.5-Acres	NA	30 Feet Iron Point Rd.	NA	5 Feet	60 feet
R-4 District Standards	6,000 S.F.	60 Feet	20 Feet	10 Feet	5 Feet/10 Feet	50 Feet
Proposed Standards	0.5-Acres	60 Feet	40 Feet Iron Point Rd. 20 Feet	15 Feet	15 Feet	41 feet

As shown in Table 1, the proposed project meets or exceeds all development standards established for the Folsom Corporate Center and for the R-4 (General Apartment) zoning district. However, the proposed project does deviate from one guideline that is not shown in the table above. The Folsom Corporate Center Planned Development Guidelines recommend that a 30-foot-wide landscape buffer be provided along the Iron Point Road frontage. Due to site constraints (topography, shape, etc.), the applicant is proposing to reduce the width of the landscape buffer (17-21 feet) along the eastern portion of the Lot 6 frontage with Iron Point Road, while at the same time expanding the width of the buffer (41-43 feet) along a greater length of the western portion of the Lot 6 frontage with Iron Point Road. With this proposed landscape modification, the average width of the landscape buffer along Iron Point Road would exceed 30 feet. Staff supports this landscape modification as the total amount of landscaping along the Iron Point Road frontage will be increased.

Building Architecture and Design

As detailed in the Project Description section of this report, the proposed project includes development of 11 three-story apartment buildings and two clubhouse buildings on two separate parcels within the Folsom Corporate Center. The design concept for the apartment building and clubhouse buildings features a modern contemporary architectural style with strong articulation of building forms and massing, both of which are used to break up the scale of the buildings. Proposed building materials include stucco walls, stone veneer wainscoting, metal canopies, glass railing, and metal railing. The color scheme for the buildings is proposed to be primarily earth tone, with prominent use of gray and brown colors accented by a mixture of lighter colors including white and tan. Proposed elevations and renderings of the apartment and clubhouse buildings are shown in the exhibits on the following pages.

FIGURE 1: BUILDING ELEVATIONS (16-PLEX)



FIGURE 2: BUILDING ELEVATIONS (21-PLEX)



FIGURE 3: BUILDING ELEVATIONS (26-PLEX)



Front Elevation



Perspective



Left Elevation

FIGURE 4: BUILDING ELEVATIONS (32-PLEX)



Rear Elevation



Perspective



Left Elevation

FIGURE 5: CLUBHOUSE BUILDING ELEVATIONS (LOT 1)



FIGURE 6: CLUBHOUSE BUILDING ELEVATIONS (LOT 6)



FIGURE 7: BUILDING RENDERINGS (LOT 1)



FIGURE 8: BUILDING RENDERINGS (LOT 6)



The proposed project is subject to the Folsom Corporate Center Design Guidelines. The Design Guidelines, in respect to overall architectural design concepts, are intended to provide a framework for design, while not restricting creativity. The following are design parameters recommended by the Design Guidelines to ensure a high-level quality of development:

- Buildings should be responsive to views from all four elevations.
- Building masses should be made human in scale, present varied elevations, and use accent materials to add variety.
- Building materials such as tile, stone, glass, metal panels, and concrete should be utilized together to reflect the area's modernity, diversity, and traditions.
- Building entries shall be distinguished with accent materials such as stone, slate, color metal panels, or concrete.

In addition to the Folsom Corporate Center Design Guidelines, the proposed project is subject to the City's Design Guidelines for Multi-Family Development. The Design Guidelines for Multi-Family Development recommend that multi-family projects be designed in a manner that compliments the surrounding community. The following are some of the specific design recommendations suggested by the Design Guidelines:

- Variety and distinctness in design are desirable.
- Expanses of uninterrupted wall area and unbroken roof forms shall be discouraged. Balconies, porches, bay windows, chimneys, and other design elements with projections and varied setbacks shall be used to break up the physical characteristics of structures.
- The use of a variety and combination of building materials is encouraged. Building materials selected for multi-family projects shall be very durable and require low maintenance including, but not limited to, stucco, stone, and brick. Building materials shall integrate quality design elements consistent with the design of the development and the surrounding neighborhood.
- Exterior building colors shall be compatible with the surrounding neighborhood setting and shall not be out of character or in visual competition with the existing surrounding design elements.
- All accessory structures, including carports, garages, and solid waste enclosures, shall be designed with materials and in a manner consistent with the architectural design characteristics of the development.

As illustrated on the building elevations and color renderings (Attachments 15 and 16), the proposing apartment and clubhouse buildings incorporate many of the key design features recommended by the Folsom Corporate Center Design Guidelines and the Design Guidelines for Multi-Family Development including the use of rectilinear building shapes to create a sense of depth, use of varied forms to create visual relief, use of staggered building elements to create visual interest, and the inclusion of unique design details to reinforce the modern contemporary residential design theme.

As shown on the color and materials board (Attachment 17), the proposed project utilizes a variety of modern building materials to enhance the appearance of the building including the use of stucco on the walls, stone veneer wainscoting, glass windows and doors, metal canopies, glass railing, and metal railing. As recommended by the Design Guidelines, the proposed project features a natural color scheme with extensive use of earth tone colors including gray and brown, complimented with lighter colors including white and tan.

Based on the analysis, staff has determined that the proposed project represents a high-quality design that is consistent with the design recommendations of the Folsom Corporate Center Design Guidelines and the Design Guidelines for Multi-Family Development. In addition, staff has determined that the project design is complimentary to the design of existing commercial and residential buildings in the immediate project area. As a result, staff recommends approval of the applicant's design with the following conditions:

1. This approval is for 11 three-story apartment buildings and two clubhouse buildings associated with the Folsom Corporate Center Apartments project. The applicant shall submit building plans that comply with this approval and the attached building elevations and color renderings dated November 16, 2021.
2. The design, materials, and colors of the proposed Folsom Corporate Center apartment and clubhouse buildings shall be consistent with the submitted building elevations, color renderings, materials samples, and color scheme to the satisfaction of the Community Development Department.
3. Brick pavers or another type of colored masonry material (ADA compliant) shall be used to designate pedestrian crosswalks on the project site, in addition to where pedestrian paths cross drive aisles, and shall be incorporated as a design feature at the two primary driveway entrances for Lot 1 and Lot 6 to the satisfaction of the Community Development Department.
4. Roof-mounted mechanical equipment, including satellite dish antennas, shall not extend above the height of the parapet walls. Ground-mounted mechanical equipment shall be shielded by landscaping or trellis type features.
5. Utility equipment such as transformers, electric and gas meters, electrical panels, and junction boxes shall be screened by walls and or landscaping.

These recommendations are included in the conditions of approval (Condition No. 60) presented for consideration by the Planning Commission.

Signage

The proposed project includes placement of three monument signs at strategic locations within the project site. The first monument sign is proposed to be located on a decorative six-foot-tall wall within a landscaped area at the southwest corner of Iron Point Road and private driveway entrance into the Folsom Corporate Center. The second and third monument signs

are proposed to be located on decorative six-foot-tall walls at their respective driveway entrances to Lot 1 and Lot 6. In terms of design, the monument signs will include individual letters made of metal with copy reading "Iron Point Apartment Homes". The monument signs, which are six-feet-tall and will include approximately 24 square feet of sign area each, will be indirectly illuminated. Staff has determined that the design of the proposed monument identification signs is complementary to the design of the proposed Folsom Corporate Center Apartments.

The Folsom Municipal Code (FMC, Section, 17.50.040 D) states that monument identification signs are an acceptable form of identification for multi-family residential projects. The Folsom Municipal Code also states that multi-family residential projects are permitted one freestanding sign that is a maximum of six-feet-tall with a maximum sign area of 32 square feet. Through the Planned Development Permit process, the applicant is seeking approval for three monument signs to provide identification for the proposed project. Staff has determined that three monument signs are appropriate based on several factors including lack of direct access to the project site from Iron Point Road, the project having two distinct driveway entrances in different locations, and the large physical scale of the apartment community. Staff recommends that the owner/applicant obtain a sign permit prior to installation of the three monument signs. Condition No. 62 is included to reflect this requirement.

C. Traffic/Access/Circulation

Existing Roadway Network

General access to the Folsom Corporate Center and the project parcels is provided by three existing driveways located on the south side of Iron Point Road. The westerly driveway is restricted to vehicle right-turn in and right-turn out movements only. The central driveway, which is located at the signalized intersection of Iron Point Road and Rowberry Drive, allows all vehicle turning movements. The easterly driveway allows vehicle right-turn in, right-turn out, and left-turn in movements only. Lots 1 and 6 both have new driveways with full access turning movements directly from existing private loop roads connecting to existing driveways on Iron Point Road.

Significant roadways in the project vicinity include Iron Point Road, Oak Avenue Parkway, Broadstone Parkway, and Rowberry Drive. Iron Point Road is an east-west arterial roadway with a raised median that runs from Folsom Boulevard to the eastern city limit along the north side of U.S. Highway 50. Within the vicinity of the project site, Iron Point Road (45 mph posted speed limit) has six lanes, bike lanes, sidewalk, curb, and gutter. Oak Avenue Parkway (45 mph posted speed limit) is a north-south arterial that extends from Willow Creek Drive to Iron Point Road. Oak Avenue Parkway is a four-lane urban arterial road between Willow Creek Drive and Blue Ravine Road, a six-lane urban arterial road between Blue Ravine Road and Riley Street, and a four-lane urban arterial road between Riley Street and Iron Point Road. Broadstone Parkway (45 mph posted speed limit) in the project vicinity is a four-lane east-west arterial, that wraps around the back of the Palladio at Broadstone Shopping Center from Iron Point Road to connect with Empire Ranch Road near the Sacramento-El Dorado County line. Rowberry Drive is a north-south two-lane local road that runs northward from the Kaiser

Permanente Medical Offices into neighborhoods to the north of Iron Point Road. A future extension of Rowberry Drive across U.S. Highway 50 and into the Folsom Plan Area is planned.

The traffic, access, and circulation analysis associated with the proposed project is based on the results of a Transportation Impact Study that was prepared in February 2022 by T. Kear Transportation Planning and Management, Inc. The transportation study analyzed traffic operations at the following 17 study intersections in the vicinity of the project site:

- Prairie City Road/U.S Highway 50 Eastbound Ramps
- Prairie City Road/U.S. Highway 50 Westbound Ramps
- Prairie City Road/American Aggregates Road
- Prairie City Road/Iron Point Road
- Iron Point Road /Grover Road
- Iron Point Road /Oak Avenue Parkway
- Iron Point Road /West Kaiser Access Road
- Iron Point Road /Rowberry Way
- Iron Point Road /Safe Credit Union Access
- Iron Point Road /Broadstone Parkway
- Iron Point Road /East Bidwell Street
- East Bidwell Street/U.S. Highway 50 Westbound Ramps
- East Bidwell Street/U.S. Highway 50 Eastbound Ramps
- APN 072-3120-023 "Lot 6" Access
- APN 072-3120-023 "Lot 1" Access
- Oak Avenue Parkway/U.S. Highway 50 Westbound Ramps (2035 Only)
- Oak Avenue Parkway/U.S. Highway 50 Eastbound Ramps (2035 Only)

Six different scenarios were evaluated in reviewing traffic operations at the 17 aforementioned study intersections including Existing 2021 without Project Condition, Existing 2021 with Project Condition, Existing Plus Approved Projects (EPAP) 2026 without Project Condition, EPAP 2026 with Project Condition, Cumulative 2035 without Project Condition, and Cumulative 2035 with Project Condition.

The proposed Folsom Corporate Center Apartments project is expected to generate a total of 81 vehicle-trips during the weekday AM peak hour and 104 vehicle-trips during the weekday PM peak hour trips. Overall, the proposed project is projected to generate a total of 1,376 daily vehicle trips. Based on the projected volume of project-related vehicle trips, the Transportation Study concluded that the proposed project would not have a significant impact on vehicle level of service (LOS) at any of the 17 study intersections. In addition, the Transportation Study determined that the proposed project would not have a significant impact relative to Vehicle Miles Traveled (VMT).

While the Transportation Study determined that the proposed project would not have any significant impacts on study intersections relative to LOS and VMT, the Study did indicate

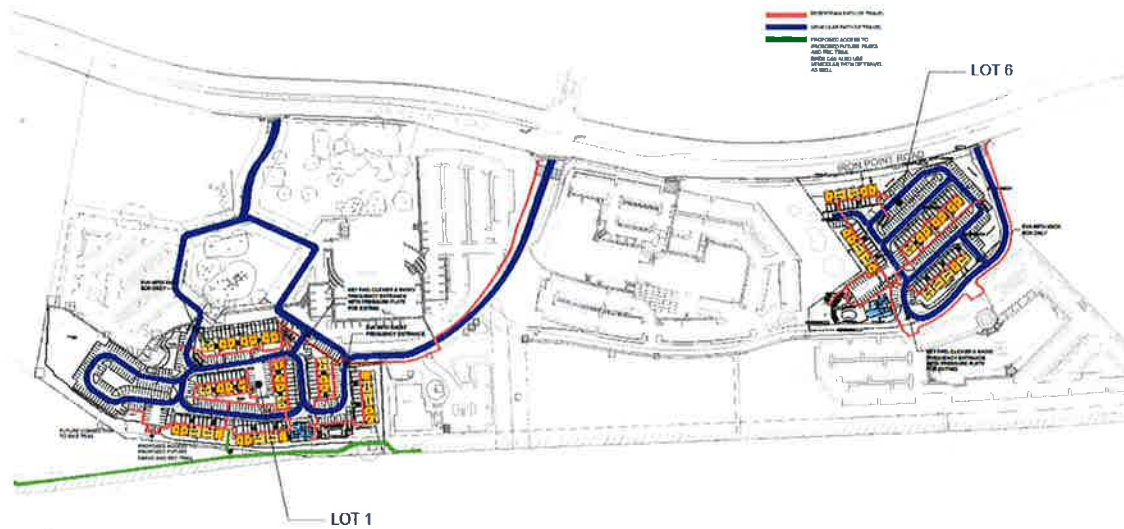
that the project would result in a queueing deficiency (project would add 1 vehicle to a queue that already exceeds available storage) in the AM Peak Hour for the westbound left-turn lanes at the intersection of Prairie City Road and Iron Point Road under two different study scenarios (Existing 2021 Conditions with Project and EPAP 2026 Conditions with Project). To address this impact and reduce the vehicle queuing caused by the proposed project, the Transportation Study recommends the following measure (Condition No. 51) be implemented:

- The owner/applicant shall modify Prairie City Road/ Iron Point Road signal timing plan by shifting 1 second from the eastbound through movement to the westbound left turn movement, reduce the vehicle extension setting from adding five to six additional seconds to the green phase for through movements to adding four seconds to the green phase for through movements for each vehicle passing the detector after the minimum green phase length has been exceeded. This mitigation measure shall be implemented by the City through the reimbursement agreement with the owner/applicant to cover any City costs. The implementation of this mitigation measure shall occur prior to issuance of the first building permit.

Project Access and On-Site Circulation

As shown on the submitted site plans (Attachments 8 and 9), access to the project area (Folsom Corporate Center) is provided by three existing driveways located on the south side of Iron Point Road. Primary vehicle access to Lot 1 is provided by a new driveway on south side of an existing private ring road with secondary access provided by two emergency vehicle access driveways also situated on the south side of the ring road. Primary vehicle access to Lot 6 is provided by a new driveway on the north side of the private ring road with secondary access served by an emergency vehicle access driveway also positioned on the north side of the ring road. Each of the project driveways from the private loop roads will accommodate all vehicle turning movements into and out of the respective sites. In addition, all project driveways will have access controlled by a vehicle gate. Internal vehicle circulation is provided by 27-foot-wide drive aisles that accommodate movement in and around the project sites. Pedestrian circulation is provided by a combination of new sidewalks and existing sidewalks located along the private ring road and also along Iron Point Road. Internal pedestrian circulation is accommodated by a series of new pedestrian pathways that provide connectivity to the apartment buildings, the clubhouse building, and the perimeter sidewalks. Access and circulation exhibits for the proposed project are shown in the figures on the following pages.

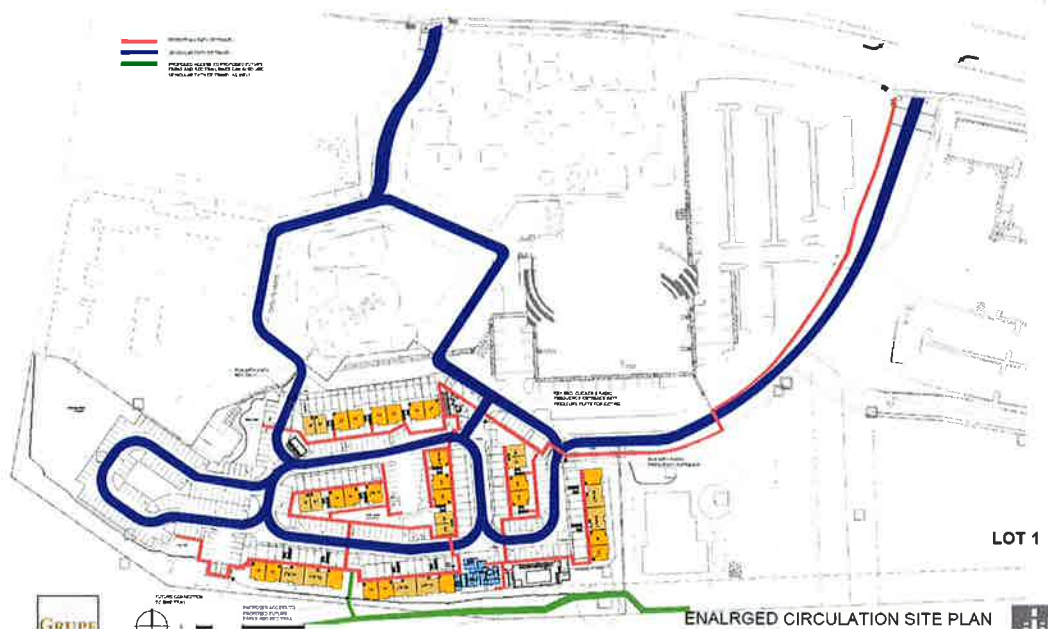
FIGURE 9: OVERALL ACCESS AND CIRCULATION EXHIBIT



Legend

- Blue Line: Vehicle Access
- Red Line: Pedestrian Access
- Green Line: Future Trail and Connection

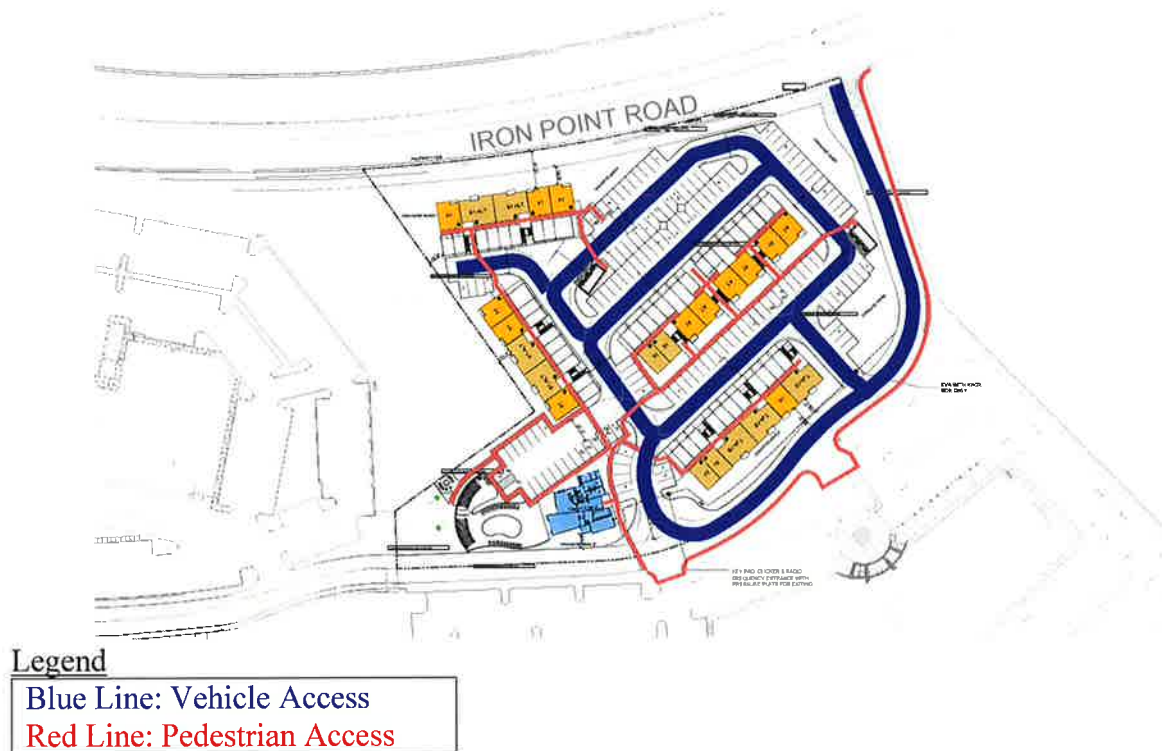
FIGURE 10: LOT 1 ACCESS AND CIRCULATION EXHIBIT



Legend

- Blue Line: Vehicle Access
- Red Line: Pedestrian Access
- Green Line: Future Trail and Connection

FIGURE 11: LOT 6 ACCESS AND CIRCULATION EXHIBIT L 6



The Transportation Impact Study prepared for the proposed project evaluated the internal operation and configuration of the project access system in terms of right-turn deceleration lanes and tapers for driveways, minimum required driveway throat depth, emergency vehicle access, and entry gate queuing. As referenced previously within this report, the project parcels are accessed via private roadways within the Folsom Corporate Center. Access to City streets (Iron Point Road) is not being modified by the proposed project, thus the City's requirements for right-turn tapers and deceleration lanes are not applicable. Additionally, the Study determined that vehicle speeds and volumes within the Folsom Corporate Center's internal roadway network do not create a safety issue that would necessitate right-turn tapers and deceleration lanes at either of the internal project driveways.

As noted earlier, access to the two project parcels is provided by an existing private roadway network within the Folsom Corporate Center. As a result, the City's minimum required throat depth is not applicable. The Study determined that the design and throat depth of each of the proposed project driveways was acceptable and would function appropriately. In terms of emergency vehicle access, there are three gated emergency vehicle access driveways proposed to serve the proposed project. In addition, the project's internal drive isles have 25-foot inner/50-foot outer minimum turning radii to accommodate all fire and police department access. Based on this information, the Study determined that adequate emergency vehicle access is being provided for the project.

Primary vehicle access to Lot 1 is provided by a new driveway on south side of an existing private ring road and primary vehicle access to Lot 6 is provided by a new driveway on the north side of the private ring road. Both of these project driveways will have access controlled by a vehicle gate. As shown on the submitted Individual Site Plans and Details (Attachment 9), the two project driveways have been designed to accommodate queuing of up to three vehicles for entry into the respective sites. The Study determined that the design of the two project driveways provides adequate queuing space for vehicles entering the project sites.

To ensure implementation of the traffic control and pedestrian circulation measures identified on the submitted site plans, staff recommends the following recommendations be included as conditions of approval for the project (Condition No. 52):

- A “stop” sign and appropriate pavement markings shall be installed at the internal approach to the private ring road at the two primary project driveways.
- The vehicle entry gates at the two primary project driveway locations shall open inward, away from the private ring road or retract sideways. In addition, the design of the vehicle entry gates and the vehicle entry gate area shall conform to all requirements established by the City of Folsom for gated multi-family residential developments.
- If vehicles are observed backing up into the private ring road at either of the two gated primary project entries, City staff will evaluate and require appropriate measures to alleviate the traffic congestion including but not limited to requiring the two project entry gates to remain open during the AM (7:00 a.m. to 9:00 a.m.) and PM (4:00 p.m. to 6:00 p.m.) peak hours on weekdays.
- Residents of the Folsom Corporate Center Apartments project shall be issued remote transmitters to allow them to open the entry gates without needing to stop to enter a code in the keypad at either entrance location.
- The owner/applicant shall provide at least one pedestrian connection from Lot 1 to the southern property boundary to allow for a connection to the future Class I bicycle trail expected to be located within the 50-foot-wide landscape easement between the project site and U.S. Highway 50.

D. Parking

The Folsom Municipal Code (Section 17.18.110 Parking) requires 1.5 parking spaces per unit for multi-family structures and complexes located within the R-4 (General Apartment Zoning District) zoning district. The Design Guidelines for Multi-Family Development require that multi-family apartment developments provide 1.5 parking spaces for studio and one-bedroom units, 1.75 parking spaces for two-bedroom units, 2.0 parking spaces for three-bedroom units, and 1 guest parking space for every 5 apartment units.

As noted in the Project Description, the proposed project includes a total of 253 apartment units including 16 studio units, 126 one-bedroom units, 97 two-bedroom units, and 14 three-bedroom units. As shown and described on the submitted site plan, the proposed project provides a total of 491 parking spaces including 120 integrated garage parking spaces, 133 carport covered parking spaces, and 238 uncovered surface parking spaces. Based on this parking information, Staff has determined that the proposed project meets the parking requirements established by the Folsom Municipal Code by providing 491 parking spaces whereas 379 parking spaces are required. In addition, staff has determined that the proposed project meets the parking recommendations of the Design Guidelines by providing 491 parking spaces whereas 462 parking spaces are recommended.

The Folsom Municipal Code (FMC, Section 17.57.090) requires multi-family residential developments to provide one bicycle parking space for every five dwelling units. The proposed project features 55 bicycle parking spaces including 31 bicycle storage room in the Lot 6 clubhouse building, 20 bicycle parking spaces in bicycle storage room in the Lot 1 clubhouse building, and 4 additional bicycle parking distributed throughout both project parcels. In addition to the dedicated bicycle storage facilities, bicycle parking opportunities are provided in each of the 120 integrated garages on the project site. Staff has determined that the proposed project meets the bicycle parking requirements established by the Folsom Municipal Code (FMC, Section 17.57.090) by providing 55 bicycle parking spaces whereas 51 bicycle parking spaces are required.

E. Noise Impacts

Based on the proximity of the project site to U.S. Highway 50, Iron Point Road, and existing commercial land uses within the immediate project vicinity, acoustical measurements and modeling were preliminarily prepared by Bollard Acoustical on May 3, 2021 and bolstered by Helix Environmental Planning on February 23, 2022 to analyze potential noise impacts at the proposed Folsom Corporate Center Apartments project site. The purpose of the noise analysis was to quantify existing noise levels associated with traffic on U.S. Highway 50 and Iron Point Road, and to compare those noise levels against the applicable City of Folsom noise standards for acceptable noise exposure at the project site. In addition, noise generated by the proposed project including construction activities, on-site parking/circulation, and mechanical equipment noise, was also evaluated in the noise analysis.

Two aspects of noise impacts were evaluated relative to the proposed apartment project, noise directed at the proposed project, and noise caused by the proposed project. As noted previously, the predominant existing noise sources in the project vicinity that cause an impact to the project site are from vehicles traveling on U.S. Highway 50 and Iron Point Road, as well as background noises from adjacent nearby commercial land uses. Potential noise impacts that might result from development of the Folsom Corporate Center Apartments project community are construction-related activities and operational activities. Construction-related noise would have a short-term effect, while operational noise would continue throughout the lifetime of the project.

The Noise Element of the City of Folsom General Plan regulates noise emissions from public roadway traffic on new development of residential or other noise sensitive land uses. The Noise Element states that noise from traffic on public roadways shall not exceed 65 CNEL for outdoor use areas and 45 CNEL for interior use areas. To evaluate such potential noise impacts to the proposed project, Bollard Acoustical conducted ambient noise measurements to calibrate the predictive noise modeling program that estimates noise levels based on estimated future traffic noise affecting the project site.

As stated above, a significant direct noise impact would occur if traffic-related noise levels exceed 65 CNEL at the proposed project's designated outdoor use areas (outdoor pool/amenity areas). The noise modeling program determined that the outdoor noise level at the clubhouse area on Lot 1 would be 65 CNEL, while the outdoor noise level at the clubhouse area on Lot 6 would be 63 CNEL. Based on these projected noise levels at the project two exterior use areas, staff has determined that the proposed project would comply with the City's exterior noise threshold.

As referenced above, a significant direct noise impact would also occur if the project's interior use areas would be exposed to noise levels greater than 45 CNEL from roadway traffic. A 45 CNEL interior limit would be achieved if exterior locations are exposed to a noise level of 60 CNEL or less, based on a typical attenuation of 15-20 dB by standard residential building construction. The noise modeling program determined that three buildings on Lot 1 (Buildings 1, 2, and 7) and two buildings on Lot 6 (Buildings 2 and 5) would potentially exceed the City's interior noise level standard of 45 CNEL. To reduce these potential noise impacts to a less than significant level and comply with the City's interior noise level standards, staff recommends that the following measures be implemented (Condition No. 56).

For habitable areas (both living rooms and bedrooms) with a direct line-of-sight to U.S. Highway 50 for Lot 1 and Iron Point Road for Lot 6, the following measures shall be incorporated in the design of the project to reduce interior noise levels to 45 CNEL or less:

- Lot 1 (Buildings 1 and 2) and Lot 6 (Building 2) – Minimum exterior wall requirement of STC 46.
- Lot 1 (Buildings 1 and 2) and Lot 6 (Building 2) – Minimum window and glass sliding door requirement of STC 35.
- Lot 1 (Building 7) and Lot 6 (Building 5) – Minimum window and glass sliding door requirement of STC 28.
- The building design shall include a mechanical ventilation system that meets the criteria of the International Building Code (Chapter 12, §1203.3 of the 2013 California Building Code) to ensure that windows would be able to remain permanently closed.

Construction of the Folsom Corporate Center Apartments project would temporarily increase noise levels in the project vicinity during the construction period, which would take approximately 20 to 26 months. Construction activities, including site clearing, excavation, grading, building construction, and paving, would be considered an intermittent noise impact throughout the construction period of the project. The City's Noise Ordinance excludes construction activities from meeting the General Plan Noise Element standards, provided that all phases of construction are limited to the hours between 7:00 a.m. and 6:00 p.m. on weekdays, and between 8:00 a.m. and 5:00 p.m. on Saturdays. To ensure compliance with the City's Noise Control Ordinance and General Plan Noise Element, staff recommends that hours of construction operation be limited from 7:00 a.m. to 6:00 p.m. on weekdays and 8:00 a.m. to 5:00 p.m. on Saturdays with no construction permitted on Sundays or holidays. In addition, staff recommends that construction equipment be muffled and shrouded to minimize noise levels. Condition No. 55 is included to reflect these requirements.

Operational noises generated by the proposed project include sounds associated with new vehicle trips, vehicle parking, and mechanical equipment associated with the apartment project. Persons and activities potentially sensitive to noise in the project vicinity include residents within the Broadstone Unit No. 2 Subdivision (150 feet north across Iron Point Road) across Iron Point Road to the north of the project site, residents within the Sherwood Apartments (approximately 450 northeast of the project site across Iron Point Road), and residents of the Revel Senior Living Apartments (approximately 500 feet to the west). Due to the limited volume of project-generated vehicle trips (81 weekday AM peak hour trips and 104 weekday PM peak hour trips), vehicle noise exposure would increase only slightly as compared to existing conditions in the project vicinity. Based on the significant distance and buffers between the project site and the nearby residential land uses, staff has determined that potential noise impacts relative to these operational noise sources will not be significant.

F. Walls/Fencing

The proposed project includes the construction of retaining walls and fencing. As shown on the submitted Grading and Drainage Plans (Attachment 11), retaining walls that predominantly range from 1-8 feet in height, with a maximum height of 15 ft at Lot 6 at the northeast corner. The walls are proposed to be constructed in various locations on Lot 1 and Lot 6 due to substantial changes in elevation on the sites. As shown the submitted Landscape Plan and Details (Attachment 12), decorative six-foot-tall metal open view fencing is proposed to be placed around the perimeter of Lots 1 and 6. In addition to the perimeter fencing, vehicle gates and pedestrian gates are also proposed at various locations on the Lots 1 and 6. Staff recommends that the final location, design, height, materials, and colors of the retaining walls, fences, and gates be subject to review and approval by the Community Development Department. Condition No. 59 is included to reflect this requirement.

G. Site Lighting

As shown on the Preliminary Lighting Plan (Attachment 14), the applicant is proposing to use a combination of pole-mounted parking lot lighting, carport lighting, building-attached

lighting, and bollard lights along the walkways on the project site. All lighting would be designed to minimize light/glare impacts to the adjacent properties by ensuring that all exterior lighting is shielded and directed downward. Staff recommends that the final exterior building and site lighting plans be submitted for review and approval by Community Development Department for location, height, aesthetics, level of illumination, glare and trespass prior to the issuance of any building permits. In addition, staff recommends all lighting is designed to be shielded and directed downward onto the project site and away from adjacent properties and public rights-of-way. Condition No. 23 is included to reflect these requirements.

H. Trash/Recycling

The proposed project includes three trash/recycling enclosures to manage trash, recycling, and organics associated with the apartment community. Lot 1 includes one trash/recycling enclosure and one trash compactor, while Lot 6 includes two trash/recycling enclosures. The proposed trash/recycling enclosures, which are constructed of textured concrete masonry blocks with a decorative trim cap, feature metal gates to control access. Staff recommends that the final location, design, materials, and colors of the trash/recycling enclosures be subject to review and approval by the Community Development Department. Condition No. 58 is included to reflect these requirements.

I. Existing and Proposed Landscaping

Lot 1, which is largely undisturbed, is predominantly comprised of non-native annual grassland with a single oak tree situated in the southeast corner of the site. Lot 1 does include small parking lot area with associated landscaping in the northwest corner of the project site. This small parking lot and landscaped area, which is associated with the adjacent Kaiser Permanente Medical Office Complex, is proposed to remain in place. A 50-foot-wide landscape easement, which is located between the southern boundary of Lot 1 and U.S. Highway 50, is steeply sloped and contains non-native grasses. Lot 6, which has been greatly disturbed by prior grading and stockpiling activities, features non-native grasses with a small stand of oak trees located in the southwest corner of the site. A 20-foot-wide landscape easement, which is located within the northern portion of Lot 6 adjacent to Iron Point Road, features a rockery retaining wall and sidewalk with minimal landscaping and non-native grasses.

As shown on the Preliminary Landscape Plans (Attachment 12), the applicant is proposing to install landscaping that features California-native and low water-use trees, shrubs, and groundcover selections intended to comply with the requirements of the Model Water Efficiency Landscape Ordinance (MWELo). Proposed landscape improvements include a variety of drought-tolerant trees, shrubs, and groundcover. Among the proposed trees are; Chinese Pistache, Coast Live Oak, Dwarf Strawberry Tree, Interior Live Oak, Red Crape Myrtle, Redpointe Maple, Sweet Bay, and Swan Hill Olive. Proposed shrubs and groundcover include; Australian Bluebell Creeper, Autumn Sage, Deer Grass, Dwarf Bottlebrush, Dwarf Hawthorne, Heavenly Bamboo, Manzanita, Red Fountain Grass, and Biofiltration Sod. The

preliminary landscape plan meets the CALgreen and City shade requirement by providing 50 percent shade in the parking lot area within fifteen years. Staff recommends that the final landscape plans be reviewed and approved by the Community Development Department. Condition No. 36 is included to reflect this requirement.

Oak Tree Preservation and Removal

Chapter 12.16 of the Folsom Municipal Code, the Tree Preservation Ordinance, regulates the cutting or modification of trees, including oaks and specified other trees; requires a Tree Permit prior to cutting or modification; and establishes mitigation requirements for cut or damaged trees. The Tree Preservation Ordinance establishes policies, regulations, and standards necessary to ensure that the City will continue to preserve and maintain its “urban forests”.

An Arborist Report and Arborist Inventory prepared for the proposed project found that the project parcels contain a total of 11 protected native oak trees (oak trees measuring six inches in diameter or larger) including nine Blue Oaks and two Valley Oaks. Of the 11 oak trees mentioned above, one Blue Oak tree located on Lot 6 is recommended for removal due to compromised health and structural defects. The remaining ten native oak trees, which are located on Lot 6, are identified as being in fair to good condition by the Arborist Report.

As shown on the submitted Landscape Plan, the applicant is proposing to preserve three oak trees on the project site including a 41” diameter Blue Oak tree (Heritage Tree) on Lot 1 and two Blue Oak trees (30” and 26” in diameter respectively) on Lot 6. The remaining eight oak trees on the project site (southwest corner of Lot 6) are proposed to be removed to allow for development of the proposed project. To offset the loss of the protected native oak trees, the applicant is proposing to plant 35 Mitigation Oak trees (Coast Live Oak and Interior Live Oak) in appropriate locations (through consultation with the City Arborist) on the project site and to pay in-lieu fees for any outstanding Oak tree mitigation that is required. To mitigate the impact to the protected native Oak trees, staff recommends that the following measures be implemented (Condition No. 37) in accordance with requirements of the Tree Preservation Ordinance:

- A Tree Permit Application containing an Application Form, Tree Protection and Mitigation Plan, and Arborist Report shall be submitted to the City of Folsom by the owner/applicant for issuance of a Tree Work Permit and Tree Removal Permit prior to commencement of any grading or site improvement activities. The tree protection and mitigation plan shall be prepared in collaboration with a qualified arborist and shall be subject to review and approval by the City. The tree protection and mitigation plan shall contain the contact information of the project arborist and shall be included in all associated plan sets for the project.
- Removal of any protected tree shall be mitigated by planting replacement trees and/or payment of “In-Lieu” fees on a diameter inch basis in accordance with FMC, Section 12.16.150. The proposed method of mitigation shall be subject to review and approval by the City.

- Prior to starting construction, oak trees to be preserved shall be fenced with high visibility fencing consistent with the city-approved tree protection and mitigation plan. Parking of vehicles, equipment, or storage of materials is prohibited within the Tree Protection Zone of Protected Trees at all times. Signs shall be posted on exclusion fencing stating that the enclosed trees are to be preserved. Signs shall state the penalty for damage to, or removal of, the protected tree.
- The owner/applicant shall retain the services of a project arborist for the duration of the development project to monitor the health of oak trees to be preserved and carry out the City-approved tree protection plan. All regulated activity conducted within the Critical Root Zone of protected trees, as that term is defined in Folsom Municipal Code (FMC) 12.16.020, shall be performed under the direct supervision of the project arborist. A copy of the executed contract for these arboricultural services shall be submitted to the City prior to the issuance of any tree or grading permits
- Certification letters by the project arborist attesting compliance with the tree protection and mitigation plan and tree permit conditions shall be submitted to the City at the following stages of the project:
- The owner/applicant shall plant 35 Mitigation Oak Trees on the project site in the locations as shown on the Preliminary Landscape Plans. The final number, location, and type of Mitigation Oak Trees shall be subject to review and approval by the Community Development Department. The owner/applicant shall pay in-lieu fees for any outstanding required Oak Tree Mitigation that is not satisfied through planting of Mitigation Oak Trees.

J. Conformance with Relevant General Plan Goals and Policies

The City of Folsom General Plan (2035) outlines a number of goals, policies, and implementation programs designed to guide the physical, economic, and environmental growth of the City. Staff has determined that the proposed project is consistent with the General Plan goals and policies as outlined and discussed below:

APPLICABLE GENERAL PLAN GOALS AND POLICIES

GP GOAL LU 1.1 (Land Use/Growth and Change)

Retain and enhance Folsom's quality of life, unique identity, and sense of community while continuing to grow and change.

GP POLICY LU 1.1.12-1 (Infill Development)

Respect the local context: New development should improve the character and connectivity of the neighborhood in which it occurs. Physical design should respond to the scale and features of the surrounding community, while improving critical elements such as transparency and permeability.

The proposed project is consistent with this policy in that the project features significant site and design improvements which will enhance the overall character of the area including

introducing new market rate apartment units with a contemporary modern residential design intended to complement the architecture and design of existing residential and commercial buildings in the project vicinity.

GP POLICY LU 1.1.15 (SACOG Blueprint Principles)

Strive to adhere to the Sacramento Regional Blueprint Growth Principles.

The proposed project is consistent with this policy in that the project has been designed to adhere to the primary SACOG Blueprint Principles including Compact Development, Housing Choice and Diversity, Use of Existing Assets, and Quality Design. Compact Development involves creating environments that are more compactly built and use space in an efficient but attractive manner to encourage more walking, biking, and transit use and shorter auto trips. Housing Choice and Diversity includes providing a variety of places where people can live (apartments, townhomes, condominiums, and single-family detached homes) and also creating opportunities for the variety of people who need them such as families, singles, seniors, and people with special needs. Use of Existing Assets entails intensification of the existing use or redevelopment in order to make better use of existing public infrastructure, including roads. Quality Design focuses on the design details of any land development (such as relationship to the street, placement of buildings, sidewalks, street widths, landscaping, etc.), which are all factors that influence the attractiveness of living in a compact development and facilitate the ease of walking within and in and out of a community.

APPLICABLE GENERAL PLAN GOALS AND POLICIES

GP GOAL LU 6.1 (Residential Neighborhoods)

Allow for a variety of housing types and mix of uses that provide choices for Folsom residents, create complete and livable neighborhoods, and encourage walking and biking.

GP POLICY LU 6.1.3 (Efficiency through Density)

Support an overall increase in average residential densities in identified urban centers and mixed-use districts. Encourage new housing types to shift from lower-density, large-lot developments to higher-density, small-lot and multifamily developments, as a means to increase energy efficiency, conserve water, reduce waste, as well as increase access to services and amenities (e.g., open space) through an emphasis on mixed uses in these higher-density developments.

The proposed project is consistent with this policy in that the project is a new market-rate multi-family residential project developed at a residential density of 21.2 units per acre. Its location within Folsom Corporate Center and proximity to the Folsom Gateway retail center will create a compact/horizontal mixed-use development. The proposed project design will be consistent with California Green Building Standards Code (CALGreen), and the residential units are being designed to be all-electric, and the project intends to participate the SMUD SolarShares program. In addition, the proposed project includes electric vehicle charging stations, and will meet or exceed the percentage of electric vehicle capable parking spaces per CALGreen code.

GP GOAL M 4.1 (Vehicle Traffic and Parking)

Ensure a safe and efficient network of streets for cars and trucks, as well as provide an adequate supply of vehicle parking.

GP POLICY M 4.1.3 (Level of Service)

Strive to achieve a least traffic Level of Service “D” (or better) for local streets and roadways throughout the City. In designing transportation improvements, the City will prioritize use of smart technologies and innovative solutions that maximize efficiencies and safety while minimizing the physical footprint. During the course of Plan buildout, it may occur that temporarily higher Levels of Service result where roadway improvements have not been adequately phased as development proceeds. However, this situation will be minimized based on annual traffic studies and monitoring programs. Staff will report to the City Council at regular intervals via the Capital improvement Program process for the Council to prioritize projects integral to achieving Level of Service D or better.

The proposed project is consistent with this policy in that the project will not result in a change in the level of service (LOS) at any of the 17 study intersections. In addition, the proposed project is anticipated to generate less than 82% of the regional per capita Vehicle Miles Traveled (VMT), consistent with new State Law that took effect July 1, 2020 (SB 743).

GP GOAL M 4.2 (Vehicle Traffic and Parking)

Provide and manage a balanced approach to parking that meets economic development and sustainability goals.

GP POLICY M 4.2.4 (Electric Vehicle Charging Stations)

Encourage the installation of electric vehicle charging stations in parking spaces throughout the city, prioritizing installations at multi-family residential units.

The proposed project is consistent with this policy in that the project includes five electric vehicle charging stations to serve electric vehicles of residents and guests. In addition, the applicant has committed to having at least 10 percent of parking spaces be EV Capable. The number of proposed electric vehicle charging stations (5) and percentage of EV Capable parking spaces is consistent with the California Green Buildings Standards Code’s provisions (10 percent of all parking spaces) required to be EV Capable) for multi-family residential development.

GP GOAL H-2 (Removing Barriers to the Production of Housing)

To minimize governmental constraints on the development of housing for households of all income levels.

GP POLICY H 2.7

The City shall educate the community on the needs, the realities and the benefits of affordable and high-density housing.

The proposed project is consistent with this policy in that the project will result in development of a high-density market-rate apartment community on parcels that are not currently zoned for multi-family high density residential development.

K. Native American Consultation (SB 18/AB52)

Senate Bill (SB) 18 was signed into law in September 2004 and became effective in March 2005. SB 18 requires city and county governments to consult with California Native American tribes early in the planning process with the intent of protecting traditional tribal cultural places. In accordance with Government Code 65352.3(a)(2), the City sent project notifications to each of listed tribes on October 26, 2021 and afforded them 90 days to respond and request consultation. The City received a response from one tribe (UAIC-United Auburn Indian Community) who expressed a desire to consult regarding the proposed project. During the consultation process, the City provided UAIC with a Cultural Resources Assessment document that indicated there are no known Tribal Cultural Resources present on the project site. Subsequently, UAIC submitted information to the City that stated that heritage trees, in general, are an important Tribal Cultural Resource. The City responded to UAIC that there is one Heritage Oak Tree on the project site (41” diameter Oak tree on Lot 1) that is intended to be preserved. City staff also responded to UAIC that a mitigation measure (Condition No. 39) will be placed on the project to protect any unanticipated discovery of Tribal Cultural Resources on the project site.

On March 9, 2022, and in accordance with Government Code §65352(a)(11), the City mailed the 45-day referral notices to the listed tribes. No tribes provided comment within that timeframe. The City will mail specific details of the pending City Council public hearing to listed tribes at least 10 days in advance of the meeting, in accordance with Government Code §65092. In summary, the City has assumed and concluded consultation responsibilities in accordance with the *Tribal Consultation Guidelines: Supplement to General Plan Guidelines* (November 14, 2005) published by the Governor’s Office of Planning and Research.

Assembly Bill (AB 52), which was signed into law in July 2015, requires City or County Governments to consult with California Native American Tribes to identify Tribal Cultural Resources that may be significantly impacted by development projects and to avoid or mitigate those impacts. On September 21, 2021, the City sent project notification letters to the three California Native American tribes named on the City’s AB 52 contact list, with the United Auburn Indian Community (UAIC) being the only tribe to respond. The City subsequently initiated consultation with UAIC concurrently with respect to AB 52 and SB 18 as the issues raised by UAIC under these two sets of State regulations were identical. On February 4, 2022, the City concluded the consultation with UAIC with the acknowledgement that measures would be included with the project to ensure protection of the Heritage Oak Tree on Lot 1 and the protection of previously unknown Tribal Cultural Resources on the project site during construction activities.

FINANCIAL IMPACT

No financial impact is anticipated with approval of the Folsom Corporate Center Apartments project as the project will be subject to all applicable development impact fees.

ENVIRONMENTAL REVIEW

Staff has prepared an Initial Study, Mitigated Negative Declaration, and Mitigation Monitoring and Reporting Program (Attachment 23) for the project in accordance with the California Environmental Quality Act (CEQA) and associated regulations and determined that with the proposed mitigations, the project will not have a significant effect on the environment. The Mitigated Negative Declaration has been prepared and noticed for public comment, and mitigation measures have been included as Conditions of Approval. One written comment was received from the public during the Mitigated Negative Declaration public review period (March 8, 2022 to April 6, 2022).

On March 24, 2022, the Sacramento Metropolitan Air Quality Management District (SMAQMD) submitted a response letter (Attachment 23) regarding the Initial Study and Mitigated Negative Declaration that was prepared for the proposed project. In the response letter, SMAQMD recommends that additional measures be implemented to protect residents from exposure to toxic air contaminant emissions produced by vehicles traveling on U.S. Highway 50. Specifically, SMAQMD recommends that a continuous landscape buffer or dense landscape plantings be provided along the southern, western, and eastern edges of the project site consistent with the Air District's *Landscaping Guidance for Improving Air Quality Near Roadways*. As shown on the submitted Preliminary Landscaped Plans (Attachment 12), the project includes a robust amount of landscaping along the perimeter of the site (Lot 1) adjacent to U.S. Highway 50. However, to further reduce residents' exposure to air contaminant emissions, staff recommends additional landscape plantings be provided where feasible along the southern, western, and eastern perimeter of Lot 1 to the satisfaction of the Community Development Department. Condition No. 45 is included to reflect this requirement. It is important to note that each of the apartment buildings will have a mechanical ventilation system that accommodates air filters with a minimum efficiency rating to reduce residents' exposure to air contaminant emissions.

In their letter, SMAQMD also recommends that the proposed project consider implementing additional energy related measures to help reduce the urban heat island effect. Specifically, SMAQMD recommends that certified cool roofs be installed on all of the apartment buildings and that solar photovoltaic shade structures be placed over the parking spaces in the area under the overhead power lines in the western portion of Lot 1. The applicant has indicated that they will be installing certified cool roofs on all the apartment buildings consistent with CALgreen code requirements. Unfortunately, the placement of solar photovoltaic shade structures over parking spaces in the power line easement area is not feasible because these types of structures are not permitted by the responsible utility agencies (PG&E and SMUD). However, it is important to reiterate that the applicant intends to participate in the SMUD SolarShares program.

ATTACHMENTS

1. Resolution No. 10849 - A Resolution to Adopt a Mitigated Negative Declaration, Approve a General Plan Amendment, and Approve a Planned Development Permit for the Folsom Corporate Center Apartments project
2. Ordinance No. 1327 - An Uncodified Ordinance to amend the zoning designation for a 7.24-acre parcel (Lot 1) from M-L PD to R-4 PD and to amend the zoning designation for a 4.68-acre parcel (Lot 6) from BP PD to R-4 PD for the Folsom Corporate Center Apartments project (Introduction and First Reading)
3. Planning Commission Staff Report, dated April 6, 2022
4. Minutes from April 6, 2022 Planning Commission Meeting
5. Vicinity Map
6. General Plan Amendment Exhibits, dated November 16, 2021
7. Rezone Exhibits, dated November 16, 2021
8. Overall Site Plan, dated November 16, 2021
9. Individual Site Plans and Details, dated February 8, 2022
10. Preliminary Utility Plans, dated November 16, 2021
11. Preliminary Grading and Drainage Plans, dated November 16, 2021
12. Preliminary Landscape Plans and Details, dated November 16, 2021
13. Preliminary Access and Circulation Plan, dated November 16, 2021
14. Preliminary Lighting Plan and Details, dated November 16, 2021
15. Building Elevations, Floor Plans, and Details dated November 16, 2021
16. Color Renderings and Perspectives, dated November 16, 2021
17. Color and Materials Board, dated November 16, 2021
18. Signage Details, dated November 16, 2021
19. Building and Parking Summary, dated February 8, 2022
20. Site Photographs
21. Transportation Impact Study, dated February, 2022
22. Initial Study, Mitigated Negative Declaration, and Mitigation Monitoring and Reporting Program, dated March 2022 (electronic version available at www.folsom.ca.us/government/community-development/planning-services/current-project-information)
23. SMAQMD ISMND Response Letter, dated March 24, 2022
24. Attachment 24 - Folsom Corporate Center Planned Development Guidelines

Submitted,



PAM JOHNS
Community Development Director

Attachment No. 1

**Resolution No. 10849 - A Resolution to Adopt a Mitigated
Negative Declaration, Approve a General Plan Amendment, and
Approve a Planned Development Permit for the Folsom
Corporate Center Apartments project**

RESOLUTION NO. 10849**A RESOLUTION TO ADOPT A MITIGATED NEGATIVE DECLARATION,
APPROVE A GENERAL PLAN AMENDMENT, AND APPROVE A PLANNED
DEVELOPMENT PERMIT FOR THE FOLSOM CORPORATE CENTER
APARTMENTS PROJECT**

WHEREAS, the Planning Commission on April 6, 2022, held a public hearing on the proposed General Plan Amendment, considered public comment and determined that the development of a market rate apartment community on the project site is consistent with and compatible to the existing land uses in the project vicinity which are a mixture of commercial and residential land uses, thus providing sufficient justification for changing the General Plan land use designations from IND (Industrial/Office Park) to MHD (Multi-Family High Density); and

WHEREAS, the Planning Commission on April 6, 2022 held a public hearing on the proposed Planned Development Permit, considered public comment and determined that based on the proposed building design, building heights, building setbacks, lot configuration, lot areas, building coverage, density, and parking, the project is consistent with the City's General Plan, the Folsom Municipal Code, and the Folsom Corporate Center Planned Development Guidelines; and

WHEREAS, notice has been given at the time and in the manner required by State Law and City Code; and

WHEREAS, staff has prepared a Mitigated Negative Declaration in accordance with the California Environmental Quality Act (CEQA).

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Folsom that the City Adopt the Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program for the Folsom Corporate Center Apartments, Amend the General Plan land use designation for a 7.24-acre parcel (Lot 1) and a 4.68-acre parcel (Lot 6) from IND (Industrial/Office Park) to MHD (Multi-Family High Density, and Approve a Planned Development Permit for the development of a 253-unit market rate apartment community for the Folsom Corporate Center Apartment Community project, with the General Plan map exhibit as set forth on Exhibit A and the conditions of approval as set forth on Exhibit B and the following findings:

GENERAL FINDINGS

- A. NOTICE OF HEARING HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE PROJECT IS CONSISTENT WITH THE GENERAL PLAN, THE ZONING CODE OF THE CITY, AND THE FOLSOM CORPORATE CENTER PLANNED DEVELOPMENT GUIDELINES AS AMENDED.

CEQA FINDINGS

- C. A MITIGATED NEGATIVE DECLARATION HAS BEEN PREPARED FOR THE PROJECT IN ACCORDANCE WITH CEQA.
- D. THE CITY COUNCIL HAS CONSIDERED THE PROPOSED MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING AND REPORTING PROGRAM BEFORE MAKING A DECISION REGARDING THE PROJECT.
- E. ON THE BASIS OF THE WHOLE RECORD BEFORE THE CITY COUNCIL, THERE IS NO SUBSTANTIAL EVIDENCE THAT THE PROJECT, AS CONDITIONED, WILL HAVE A SIGNIFICANT EFFECT ON THE ENVIRONMENT.
- F. THE MITIGATED NEGATIVE DECLARATION REFLECTS THE INDEPENDENT JUDGMENT AND ANALYSIS OF THE CITY OF FOLSOM.
- G. THE MITIGATED NEGATIVE DECLARATION HAS DETERMINED THAT THE PROPOSED PROJECT, AS CONDITIONED AND CONSISTENT WITH THE REQUIRED MITIGATION MONITORING AND REPORTING PROGRAM, WOULD NOT HAVE A SIGNIFICANT EFFECT ON THE ENVIRONMENT WITH MITIGATION MEASURES.
- H. THE LOCATION AND CUSTODIAN OF THE DOCUMENTS WHICH CONSTITUTE THE RECORD OF PROCEEDINGS UPON WHICH THE DECISION IS BASED ARE: CITY OF FOLSOM COMMUNITY DEVELOPMENT DEPARTMENT, 50 NATOMA STREET, FOLSOM, CA 95630.

GENERAL PLAN AMENDMENT FINDINGS

- I. THE PROPOSED GENERAL PLAN AMENDMENT IS CONSISTENT WITH THE GOALS, POLICIES AND OBJECTIVES OF THE CITY OF FOLSOM GENERAL PLAN
- J. THE PROPOSED GENERAL PLAN AMENDMENT IS CONSISTENT WITH THE OBJECTIVES OF THE LAND USE ELEMENT OF THE CITY'S GENERAL PLAN AND DEVELOPMENT POLICIES.
- K. THE PROPOSED GENERAL PLAN AMENDMENT WILL NOT RESULT IN A NET LOSS OF RESIDENTIAL CAPACITY.
- L. THE PROPOSED GENERAL PLAN AMENDMENT IS IN THE PUBLIC INTEREST.

- M. PURSUANT TO GOVERNMENT CODE SECTION 65352.3, THE CITY CONTACTED ALL CALIFORNIA NATIVE AMERICAN TRIBES ON THE CONTACT LIST MAINTAINED BY THE NATIVE AMERICAN HERITAGE COMMISSION IN ASSOCIATION WITH THIS PROJECT. THE CITY RECEIVED ONE REQUEST FOR CONSULTATION FROM A NATIVE AMERICAN TRIBE, INITIATED CONSULTATION, AND SUBSEQUENTLY CONCLUDED CONSULTATION ON FEBRUARY 4, 2022

REZONE FINDING

- N. THE PROJECT IS CONSISTENT WITH THE CITY'S GENERAL PLAN, THE FOLSOM MUNICIPAL CODE, AND THE FOLSOM CORPORATE CENTER PLANNED DEVELOPMENT GUIDELINES AS AMENDED.

PLANNED DEVELOPMENT PERMIT FINDINGS

- O. THE PROPOSED PROJECT COMPLIES WITH THE INTENT AND PURPOSES OF CHAPTER 17.38 (PLANNED DEVELOPMENT DISTRICT) OF THE FOLSOM MUNICIPAL CODE AND OTHER APPLICABLE ORDINANCES OF THE CITY.
- P. THE PROPOSED PROJECT IS CONSISTENT WITH THE OBJECTIVES, POLICIES AND REQUIREMENTS OF THE DEVELOPMENT STANDARDS OF THE CITY.
- Q. THE PHYSICAL, FUNCTIONAL AND VISUAL COMPATIBILITY BETWEEN THE PROPOSED PROJECT AND EXISTING AND FUTURE ADJACENT USES AND AREA CHARACTERISTICS IS ACCEPTABLE.
- R. THERE ARE AVAILABLE PUBLIC FACILITIES, INCLUDING BUT NOT LIMITED TO, WATER, SEWER AND DRAINAGE TO ALLOW FOR THE DEVELOPMENT OF THE PROJECT SITE IN A MANNER CONSISTENT WITH THIS PROPOSAL.
- S. THE PROPOSED PROJECT WILL NOT CAUSE UNACCEPTABLE VEHICULAR TRAFFIC LEVELS ON SURROUNDING ROADWAYS, AND THE PROPOSED PROJECT WILL PROVIDE ADEQUATE INTERNAL CIRCULATION.
- T. THE PROPOSED PROJECT WILL NOT BE DETRIMENTAL TO THE HEALTH, SAFETY AND GENERAL WELFARE OF THE PERSONS OR PROPERTY WITHIN THE VICINITY OF THE PROJECT SITE, AND THE CITY AS A WHOLE.
- U. ADEQUATE PROVISION IS MADE FOR THE FURNISHING OF SANITATION SERVICES AND EMERGENCY PUBLIC SAFETY SERVICES TO THE PROJECT.

PASSED AND ADOPTED this 10th day of May, 2022, by the following roll-call vote:

AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Kerri M. Howell, MAYOR

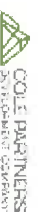
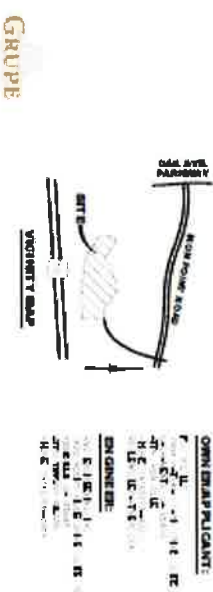
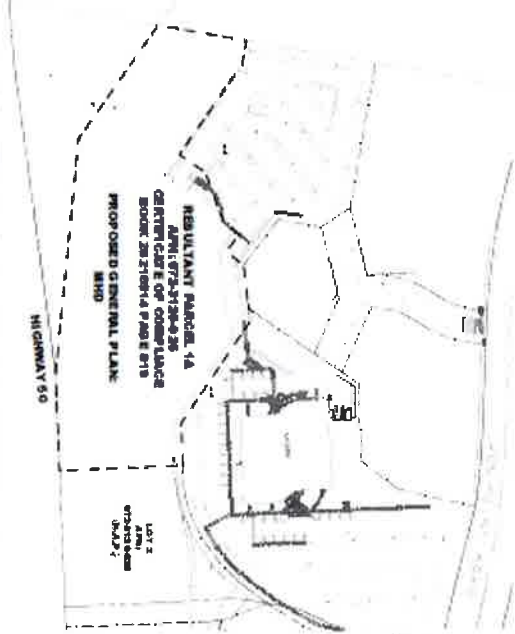
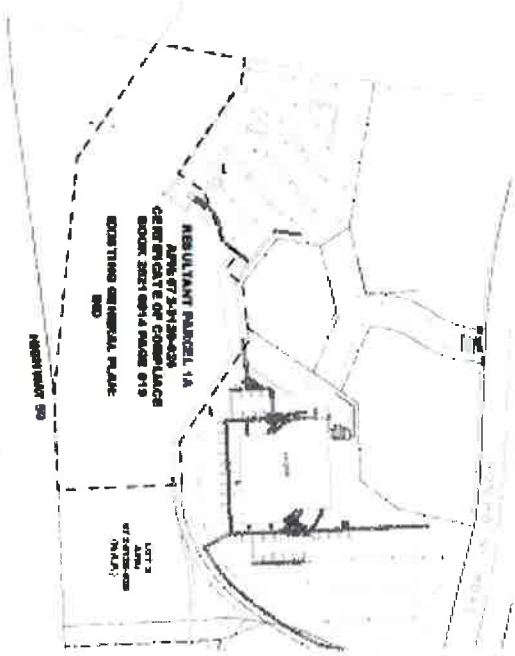
ATTEST:

Christa Freemantle, CITY CLERK

Exhibit A

General Plan Amendment Exhibits

General Plan Amendment Exhibit (Lot 1)

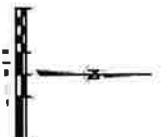


GRUPE

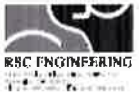
GENERAL PLAN AMENDMENT **GPA**
IRON POINT ROAD APARTMENTS - LOT 1 **FO.S01M.0A**

OWNERS/PLACANT:

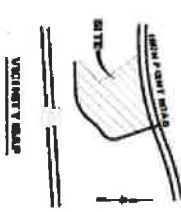
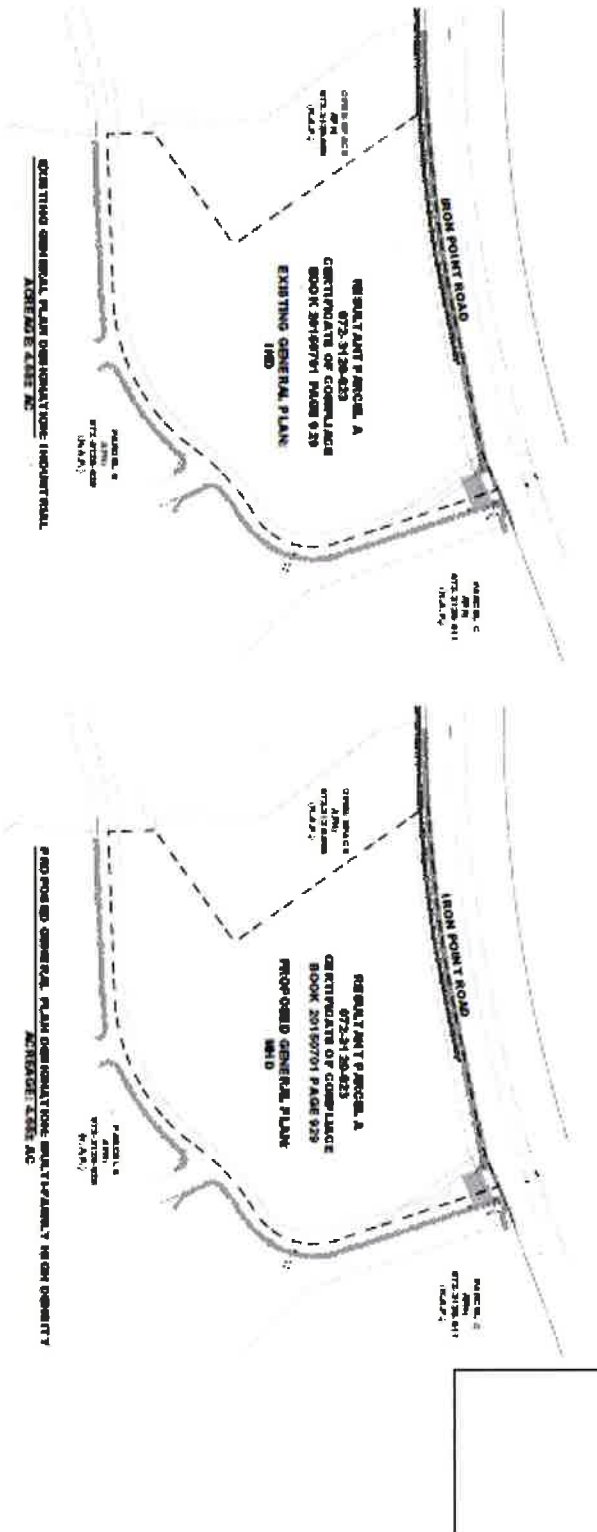
APPLICANT:	APPLICANT:
OWNER:	OWNER:
ENGINEER:	ENGINEER:
DATE:	DATE:
SCALE:	SCALE:



NOVEMBER 19, 2021 | MR.2023.00.00

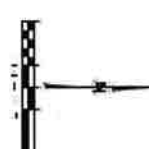


General Plan Amendment Exhibit (Lot 6)



GENERAL PLAN ZONING

RESIDENTIAL SINGLE A	RESIDENTIAL SINGLE B	RESIDENTIAL SINGLE C	RESIDENTIAL SINGLE D	RESIDENTIAL SINGLE E	RESIDENTIAL SINGLE F	RESIDENTIAL SINGLE G	RESIDENTIAL SINGLE H	RESIDENTIAL SINGLE I	RESIDENTIAL SINGLE J	RESIDENTIAL SINGLE K	RESIDENTIAL SINGLE L	RESIDENTIAL SINGLE M	RESIDENTIAL SINGLE N	RESIDENTIAL SINGLE O	RESIDENTIAL SINGLE P	RESIDENTIAL SINGLE Q	RESIDENTIAL SINGLE R	RESIDENTIAL SINGLE S	RESIDENTIAL SINGLE T	RESIDENTIAL SINGLE U	RESIDENTIAL SINGLE V	RESIDENTIAL SINGLE W	RESIDENTIAL SINGLE X	RESIDENTIAL SINGLE Y	RESIDENTIAL SINGLE Z
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26



GENERAL PLAN AMENDMENT GPA

IRON POINT ROAD APARTMENTS - LOT 6

FOLSOM, CA



1001 Orange Street, Suite 200, Folsom, CA 95630
 Tel: 916.451.1100 Fax: 916.451.1101
 www.colepartners.com

NOVEMBER 10, 2021 | MR.2021.01.00



Exhibit B

Conditions of Approval

CONDITIONS OF APPROVAL FOR THE FOLSOM CORPORATE CENTER APARTMENTS PROJECT (PN 21-120) GENERAL PLAN AMENDMENT, REZONE, AND PLANNED DEVELOPMENT PERMIT SOUTH SIDE OF IRON POINT ROAD, SLIGHTLY EAST OF OAK AVENUE PARKWAY				
1.		<p>The applicant shall submit final site development plans to the Community Development Department that shall substantially conform to the exhibits referenced below:</p> <ol style="list-style-type: none"> 1. Vicinity Map 2. General Plan Amendment Exhibits, dated November 16, 2021 3. Rezone Exhibits, dated November 16, 2021 4. Overall Site Plan, dated November 16, 2021 5. Individual Site Plans and Details, dated February 8, 2022 6. Preliminary Utility Plans, dated November 16, 2021 7. Preliminary Grading and Drainage Plans, dated November 16, 2021 8. Preliminary Landscape Plans and Details, dated November 16, 2021 9. Preliminary Access and Circulation Plan, dated November 16, 2021 10. Preliminary Lighting Plan and Details, dated November 16, 2021 11. Building Elevations, Floor Plans, and Details dated November 16, 2021 12. Color Renderings and Perspectives, dated November 16, 2021 13. Color and Materials Board, dated November 16, 2021 14. Signage Details, dated November 16, 2021 15. Building and Parking Summary, dated February 8, 2022 <p>The project is approved for the development the 253-unit Folsom Corporate Center Apartment Community, which includes 11 three-story apartment buildings, two clubhouse buildings, and associated site improvements. Implementation of the project shall be consistent with the above-referenced items as modified by these conditions of approval.</p>	B	CD (P)(E)

1.		Building plans, and all civil engineering and landscape plans, shall be submitted to the Community Development Department for review and approval to ensure conformance with this approval and with relevant codes, policies, standards and other requirements of the City of Folsom.	I, B	CD (P)(E)(B)
2.		The project approvals (Planned Development Permit) granted under this staff report shall remain in effect for two years from final date of approval (May 10, 2024). Failure to obtain the relevant building (or other) permits within this time period, without the subsequent extension of this approval, shall result in the termination of this approval.	B	CD (P)
3.		<p>The owner/applicant shall defend, indemnify, and hold harmless the City and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul any approval by the City or any of its agencies, departments, commissions, agents, officers, employees, or legislative body concerning the project. The City will promptly notify the owner/applicant of any such claim, action or proceeding, and will cooperate fully in the defense. The City may, within its unlimited discretion, participate in the defense of any such claim, action or proceeding if both of the following occur:</p> <ul style="list-style-type: none"> • The City bears its own attorney’s fees and costs; and • The City defends the claim, action or proceeding in good faith <p>The owner/applicant shall not be required to pay or perform any settlement of such claim, action or proceeding unless the settlement is approved by the owner/applicant.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD
4.	✓	The owner/applicant shall be required to participate in a mitigation monitoring and reporting program pursuant to City Council Resolution No. 2634 and Public Resources Code 21081.6. The mitigation monitoring and reporting measures identified in the Mitigated Negative Declaration prepared for this project have been incorporated into these conditions of approval in order to mitigate or avoid significant effects on the environment. These mitigation monitoring and reporting measures are identified with a check mark (✓) in the mitigation measure column.	G, I	CD (P)(E)
DEVELOPMENT COSTS AND FEE REQUIREMENTS				
5.		The owner/applicant shall pay all applicable taxes, fees and charges at the rate and amount in effect at the time such taxes, fees and charges become due and payable.	I, B	CD (P)(E)
6.		If applicable, the owner/applicant shall pay off any existing assessments against the property, or file necessary segregation request and pay applicable fees.	B	CD (E)

7.		The City, at its sole discretion, may utilize the services of outside legal counsel to assist in the implementation of this project, including, but not limited to, drafting, reviewing and/or revising agreements and/or other documentation for the project. If the City utilizes the services of such outside legal counsel, the applicant shall reimburse the City for all outside legal fees and costs incurred by the City for such services. The applicant may be required, at the sole discretion of the City Attorney, to submit a deposit to the City for these services prior to initiation of the services. The applicant shall be responsible for reimbursement to the City for the services regardless of whether a deposit is required.	I	CD (P)(E)
8.		If the City utilizes the services of consultants to prepare special studies or provide specialized design review or inspection services for the project, the applicant shall reimburse the City for actual costs it incurs in utilizing these services, including administrative costs for City personnel. A deposit for these services shall be provided prior to initiating review of the improvement plans or beginning inspection, whichever is applicable.	I, B	CD (P)(E)
9.		This project shall be subject to all City-wide development impact fees, unless exempt by previous agreement. This project shall be subject to all City-wide development impact fees in effect at such time that a building permit is issued. These fees may include, but are not limited to, fees for fire protection, park facilities, park equipment, Quimby, Humbug-Willow Creek Parkway, Light Rail, TSM, capital facilities and traffic impacts. The 90-day protest period for all fees, dedications, reservations or other exactions imposed on this project has begun. The fees shall be calculated at the fee rate in effect at the time of building permit issuance.	B	CD (P)(E), PW, PK
10.		The owner/applicant agrees to pay to the Folsom-Cordova Unified School District the maximum fee authorized by law for the construction and/or reconstruction of school facilities. The applicable fee shall be the fee established by the School District that is in effect at the time of the issuance of a building permit. Specifically, the owner/applicant agrees to pay any and all fees and charges and comply with any and all dedications or other requirements authorized under Section 17620 of the Education Code; Chapter 4.7 (commencing with Section 65970) of the Government Code; and Sections 65995, 65995.5 and 65995.7 of the Government Code.	B	CD (P)

SITE DEVELOPMENT REQUIREMENTS				
11.		Prior to the issuance of any grading and/or building permit, the owner/applicant shall have a geotechnical report prepared by an appropriately licensed engineer that includes an analysis of site suitability, proposed foundation design for all proposed structures, and roadway and pavement design.	G, B	CD (E)
12.		Public and private improvements, including roadways, curbs, gutters, sidewalks, bicycle lanes and trails, streetlights, underground infrastructure and all other improvements shall be provided in accordance with the current edition of the City of Folsom <u>Standard Construction Specifications</u> and the <u>Design and Procedures Manual and Improvement Standards</u> . All necessary rights-of-way and/or easements shall be dedicated to the City of Folsom for these improvements.	I, B	CD (P)(E)
13.		The applicant/owner shall submit water, sewer and drainage studies to the satisfaction of the Community Development Department and provide sanitary sewer, water and storm drainage improvements with corresponding easements, as necessary, in accordance with these studies and the current edition of the City of Folsom <u>Standard Construction Specifications</u> and the <u>Design and Procedures Manual and Improvement Standards</u> .	I	CD (E)
14.		The improvement plans for the required public and private improvements shall be reviewed and approved by the Community Development Department prior to issuance of a building permit for the project.	B	CD (E)
15.		Final lot and building configurations may be modified to allow for overland release of storm events greater than the capacity of the underground system.	B	CD (E)
16.		The owner/applicant shall coordinate the planning, development and completion of this project with the various utility agencies (i.e., SMUD, PG&E, etc.).	I	CD (P)(E)
17.		The owner/applicant shall be responsible for replacing any and all damaged or hazardous public sidewalk, curb and gutter along the site frontage and/or boundaries, including pre-existing conditions and construction damage, to the satisfaction of the Community Development Department.	O	CD (E)
18.		For any improvements constructed on private property that are not under ownership or control of the owner/applicant, a right-of-entry, and if necessary, a permanent easement shall be obtained and provided to the City prior to issuance of a grading permit and/or approval of improvement plans.	G, I	CD (E)

19.		The on-site water and sewer systems shall be privately owned and maintained. The fire protection system shall be separate from the domestic water system. The fire system shall be constructed to meet the National Fire Protection Association Standard 24. The domestic water and irrigation system shall be metered per City of Folsom <i>Standard Construction Specifications</i> .	I	CD (E)
20.		Any reimbursement for public improvements constructed by the applicant shall be in accordance with a formal reimbursement agreement entered into between the City and the owner/applicant prior to approval of the improvement plans.	I	CD (E)
21.		The owner/applicant shall dedicate a 12.5-foot-wide public utility easement for underground facilities and appurtenances adjacent to all public rights-of-way. The owner/applicant shall also dedicate any private drive, ingress, and egress easement as a public utility easement for underground facilities and appurtenances. An easement shall also be dedicated to SMUD based on the location of as constructed facilities placed beyond the limits of the private drives.	I	CD (E)
22.		Final exterior building and site lighting plans shall be submitted for review and approval by Community Development Department for location, height, aesthetics, level of illumination, glare and trespass prior to the issuance of any building permits. All lighting, including but not limited to free-standing parking lot lights, building-attached lights, and landscape lights shall be designed to be screened, shielded, and directed downward onto the project site and away from adjacent properties and public rights-of-way. The final design of the building-attached lights shall be subject to review and approval by the Community Development Department. Lighting shall be equipped with a timer or photo condenser. In addition, pole-mounted parking lot lights shall utilize a low-intensity, energy efficient lighting method.	I, B	CD (P)
STORM WATER POLLUTION/CLEAN WATER ACT REQUIREMENTS				
23.		The owner/applicant shall be responsible for litter control and sweeping of all paved surfaces in accordance with City standards. All on-site storm drains shall be cleaned immediately before the commencement of the rainy season (October 15).	G, I, B	CD (E)
24.		The storm drain swale or onsite improvement plans shall provide for “Best Management Practices” that meet the requirements of the water quality standards of the City’s National Pollutant Discharge Elimination System Permit issued by the State Regional Water Quality Control Board.	G, I, B, O	CD (E)

25.		Erosion and sedimentation control measures shall be incorporated into construction plans. These measures shall conform to the City of Folsom requirements and the County of Sacramento <i>Erosion and Sedimentation Control Standards and Specifications</i> -current edition and as directed by the Community Development Department.	G, I	CD (E)
26.		The proposed development will add new impervious area to the site; therefore, stormwater quality treatment shall be provided. The City requires developers to utilize the <i>Guidance Manual for On-Site Stormwater Quality Treatment Control Measures</i> (January 2000) (“On-Site Manual”) in selecting and designing source control and post-construction facilities to treat runoff from the project.	G, I	CD (E)
27.		Prior to issuance of grading permits, the owner/applicant shall submit detailed drainage plans for evaluation by the City. Approved plans shall be implemented prior to project occupancy. The drainage plans shall include measures to minimize the total amount of additional surface runoff and to limit the flows released to off-site receiving waters to existing pre-development levels in accordance with the requirements of the City of Folsom Public Works Department.	G, I	CD (E), PW
28.		Prior to issuance of grading permits, the owner/applicant shall submit erosion control plans and other monitoring programs for the construction and operational phases of the proposed project for review by the City. The plan shall include Best Management Practices (BMP) to minimize and control the level of pollutants in stormwater runoff, and in runoff released to off-site receiving waters. Specific techniques may be based on geotechnical reports or the Erosion and Sediment Control Handbook of the California Department of Conservation, and shall comply with current City standards.	G, I	CD (E), PW
29.		Prior to issuance of grading permits, the owner/applicant shall obtain coverage under the State Water Resources Control Board General Permit for Discharges of Storm Water Associated with Construction Activity (Order 2009-0009-DWQ), including preparation and submittal of a project-specific Storm Water Pollution Prevention Plan (SWPPP) at the time the Notice of Intent (NOI) is filed. The project applicant shall also prepare and submit any other necessary erosion and sediment control and engineering plans and specifications for pollution prevention and control to the City of Folsom.	G, I	CD (E), PW
ENVIRONMENTAL AND WATER RESOURCE REQUIREMENTS				
30.		The water system shall be protected with USC Certified and approved RPPA and RPDA devices.	I	EWR
31.		All on-site water and sewer systems shall be privately owned and maintained.	I	EWR

32.		A Sewer Manhole or cleanout shall be placed at the property line/Right of Way line to distinguish private vs public ownership.	I	EWR
33.		All proposed sewer within the Right of Way shall be 8-inch SDR-26 sewer pipe.	I	EWR
LANDSCAPE/TREE PRESERVATION REQUIREMENTS				
34.		The owner/applicant shall be responsible for on-site landscape maintenance throughout the life of the project to the satisfaction of the Community Development Department. Vegetation or planting shall not be less than that depicted on the final landscape plan, unless tree removal is approved by the Community Development Department because the spacing between trees will be too close on center as they mature. No decorative turf or sod shall be permitted to the satisfaction of the Community Development Department.	B, OG	CD (P)(E)

35.		<p>Final landscape plans and specifications shall be prepared by a registered landscape architect and approved by the City prior to the approval of the first building permit. Said plans shall include all on-site landscape specifications and details including a tree planting exhibit demonstrating sufficient diversity and appropriate species selection to the satisfaction of the Community Development Department. The tree exhibit shall include all street trees, accent trees, parking lot shading trees, and mitigation trees proposed within the development. Said plans shall comply with all State and local rules, regulations, Governor’s declarations and restrictions pertaining to water conservation and outdoor landscaping.</p> <p>Landscaping of the parking area shall meet shade requirements as outlined in the Folsom Municipal Code Chapter 17.57. The landscape plans shall comply and implement water efficient requirements as adopted by the State of California (Assembly Bill 1881) (State Model Water Efficient Landscape Ordinance) until such time the City of Folsom adopts its own Water Efficient Landscape Ordinance at which time the owner/applicant shall comply with any new ordinance. Shade and ornamental trees shall be maintained according to the most current American National Standards for Tree Care Operations (ANSI A-300) by qualified tree care professionals. Tree topping for height reduction, view protection, light clearance or any other purpose shall not be allowed. Specialty-style pruning, such as pollarding, shall be specified within the approved landscape plans and shall be implemented during a 5-year establishment and training period. The owner/applicant shall comply with city-wide landscape rules or regulations on water usage. The owner/applicant shall comply with any state or local rules and regulations relating to landscape water usage and landscaping requirements necessitated to mitigate for drought conditions on all landscaping in the Folsom Corporate Center project.</p>	I	CD(P)(E)
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36.	✓	<p>A Tree Permit Application containing an Application Form, Tree Protection and Mitigation Plan, and Arborist Report shall be submitted to the City of Folsom by the owner/applicant for issuance of a Tree Work Permit and Tree Removal Permit prior to commencement of any grading or site improvement activities. The tree protection and mitigation plan shall be prepared in collaboration with a qualified arborist and shall be subject to review and approval by the City. The tree protection and mitigation plan shall contain the contact information of the project arborist and shall be included in all associated plan sets for the project.</p> <p>Removal of any protected tree shall be mitigated by planting replacement trees and/or payment of “In-Lieu” fees on a diameter inch basis in accordance with <u>FMC, Section 12.16.150</u>. The proposed method of mitigation shall be subject to review and approval by the City.</p> <p>Prior to starting construction, oak trees to be preserved shall be fenced with high visibility fencing consistent with the city-approved tree protection and mitigation plan. Parking of vehicles, equipment, or storage of materials is prohibited within the Tree Protection Zone of Protected Trees at all times. Signs shall be posted on exclusion fencing stating that the enclosed trees are to be preserved. Signs shall state the penalty for damage to, or removal of, the protected tree.</p> <p>The owner/applicant shall retain the services of a project arborist for the duration of the development project to monitor the health of oak trees to be preserved and carry out the City-approved tree protection plan. All regulated activity conducted within the Critical Root Zone of protected trees, as that term is defined in <u>Folsom Municipal Code (FMC) 12.16.020</u>, shall be performed under the direct supervision of the project arborist. A copy of the executed contract for these arboricultural services shall be submitted to the City prior to the issuance of any tree or grading permits</p> <p>Certification letters by the project arborist attesting compliance with the tree protection and mitigation plan and tree permit conditions shall be submitted to the City at the following stages of the project:</p> <ul style="list-style-type: none"> ○ Following completion of grading, prior to issuance of Building Permits. ○ At the time of final inspection, prior to Certificate of Occupancy 	I, G, B, O	CD(P)(E)
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		The owner/applicant shall plant 35 Mitigation Oak Trees on the project site in the locations as shown on the Preliminary Landscape Plans. The final number, location, and type of Mitigation Oak Trees shall be subject to review and approval by the Community Development Department. The owner/applicant shall pay in-lieu fees for any outstanding required Oak Tree Mitigation that is not satisfied through planting of Mitigation Oak Trees.		
CULTURAL RESOURCE REQUIREMENTS				
37.	✓	It is always possible that ground-disturbing activities during project development may uncover previously unknown archaeological resources. In the event that archaeological resources are discovered during construction, construction operations shall stop within a 100-foot radius of the find and a qualified archaeologist shall be consulted to determine whether the resource requires further study. The City shall include a standard inadvertent discovery clause in every construction contract to inform contractors of this requirement. The archaeologist shall make recommendations concerning appropriate measures that will be implemented to protect the resources, including but not limited to, excavation and evaluation of the finds in accordance with Section 15064.5 of the CEQA Guidelines. Archaeological resources could consist of, but are not limited to, stone, bone, wood, or shell artifacts or features, including hearths. Any previously undiscovered resources found during construction within the project area should be recorded on appropriate Department of Parks and Recreation (DPR) 523 forms and evaluated for significance in terms of CEQA criteria.	G, I, B	CD (P)(E)
38.	✓	If any suspected TCRs are discovered during ground disturbing construction activities, all work shall cease within 100-feet of the find, or an agreed upon distance based on the Project Area and nature of the find. A Tribal Representative from a California Native American tribe that is traditionally and culturally affiliated with a geographic area shall be immediately notified and shall determine if the find is a TCR (PRC §21074). The Tribal Representative will make recommendations for further evaluation and culturally appropriate treatment as necessary. If deemed necessary by the City, a qualified cultural resources specialist meeting the Secretary of Interior’s Standards and Qualifications for Archaeology may also assess the significance of the find in joint consultation with Native American Representatives to ensure that Tribal values are considered. Work at the discovery location may not resume until the City, in consultation as appropriate and in good faith, determines that all necessary investigation and treatment of the discovery under the requirements of CEQA, including AB52, have been satisfied.	G, I, B	CD (P)(E)

39.	✓	<p>In the event of the accidental discovery or recognition of any human remains, CEQA Guidelines § 15064.5; Health and Safety Code § 7050.5; Public Resources Code § 5097.94 and § 5097.98 must be followed. If during the course of project development there is accidental discovery or recognition of any human remains, the following steps shall be taken:</p> <p>There shall be no further excavation or disturbance within a 100-foot radius of the potentially human remains until the County Coroner is contacted to determine if the remains are Native American and if an investigation of the cause of death is required. If the coroner determines the remains to be Native American, the coroner shall contact the Native American Heritage Commission (NAHC) within 24 hours, and the NAHC shall identify the person or persons it believes to be the “most likely descendant” (MLD) of the deceased Native American. The MLD may make recommendations to the landowner or the person responsible for the excavation work within 48 hours, for means of treating or disposing of, with appropriate dignity, the human remains and any associated grave goods as provided in PRC Section 5097.98.</p> <p>Where the following conditions occur, the landowner or his authorized representative shall reburial the Native American human remains and associated grave goods with appropriate dignity either in accordance with the recommendations of the most likely descendant or on the project site in a location not subject to further subsurface disturbance:</p> <ul style="list-style-type: none">○ The NAHC is unable to identify a most likely descendent or the most likely descendent failed to make a recommendation within 48 hours after being notified by the commission.○ The descendant identified fails to make a recommendation.○ The landowner or his authorized representative rejects the recommendation of the descendant, and mediation by the NAHC fails to provide measures acceptable to the landowner.	G, I, B	CD (P)(E)
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BIOLOGICAL RESOURCE REQUIREMENTS

40.	✓	<p><u>Nesting Birds:</u> Mitigation Measure BIO-2: If project (construction) ground-disturbing or vegetation clearing and grubbing activities commence during the avian breeding season (February 1 through August 31), a qualified biologist shall conduct a pre-construction nesting bird survey no more than 14 days prior to initiation of project activities and again immediately prior to construction. The survey area shall include suitable raptor nesting habitat within 500-feet of the project boundary (inaccessible areas outside of the project site can be surveyed from the site or from public roads using binoculars or spotting scopes). Preconstruction surveys are not required in areas where project activities have been continuous since prior to February 1, as determined by a qualified biologist. Areas that have been inactive for more than 14 days during the avian breeding season must be re-surveyed prior to resumption of project activities. If no active nests are identified, no further mitigation is required. If active nests are identified, the following measure is required:</p> <ul style="list-style-type: none"> • A suitable buffer (e.g., typically 300-500-feet for raptors; and 50-100-feet for passerines) shall be established by a qualified biologist around active nests and no construction activities within the buffer shall be allowed until a qualified biologist has determined that the nest is no longer active (i.e., the nestlings have fledged and are no longer reliant on the nest, or the nest has failed). Encroachment into the buffer may occur at the discretion of a qualified biologist. Any encroachment into the buffer shall be monitored by a qualified biologist to determine whether nesting birds are being impacted. • With implementation of the above mitigation measures, potential impacts to special-status species and nesting birds would be less than significant and no additional mitigation measures would be required. 	G, I	CD (E)(P)
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<p>41.</p>	<p>✓</p>	<p>Burrowing Owl Mitigation Measure BIO-1: Prior to the commencement of construction activities (which includes clearing, grubbing, or grading) a survey for burrowing owl shall be conducted by a qualified biologist. The survey shall occur within 30 days of the start of construction activities. Surveys shall be conducted in accordance with the following:</p> <ul style="list-style-type: none"> • A survey for active burrows and burrowing owls shall be conducted by walking through suitable habitat over the entire project site and in areas within 150-meters (~500-feet) of the project impact zone where accessible. • Pedestrian survey transects shall be spaced to allow 100 percent visual coverage of the ground surface. The distance between transect center lines shall be no more than 30-meters (~100-feet) and shall be reduced to account for differences in terrain, vegetation density, and ground surface visibility. Surveyor(s) shall maintain a minimum distance of 50-meters (~160-feet) from any owls or occupied burrows. It is important to minimize disturbance near occupied burrows during all seasons. • If no occupied burrows or burrowing owls are found in the survey area, a letter report documenting survey methods and findings shall be prepared and no further mitigation is necessary. • If occupied burrows or burrowing owls are found, then a complete burrowing owl survey is required. This consists of a minimum of four site visits conducted on four separate days, which must also be consistent with the Survey Method, Weather Conditions, and Time of Day sections of Appendix D of the California Fish and Wildlife “Staff Report on Burrowing Owl Mitigation” (March 2012). A survey report shall be prepared that is consistent with the Survey Report section of Appendix D of the California Fish and Wildlife “Staff Report on Burrowing Owl Mitigation” (March 2012). • If occupied burrows or burrowing owls are found, the applicant shall contact the City and consult with CDFW prior to construction and will be required to submit a Burrowing Owl Mitigation Plan (subject to the approval of the City and in consultation with California Fish and Wildlife). This plan must document all proposed measures, including avoidance, minimization, exclusion, relocation, or other measures, and include a plan to monitor mitigation success. The CDFW “Staff Report on Burrowing Owl Mitigation” (March 2012 shall be used. 	<p>G, I</p>	<p>CD (E)(P)</p>
<p>AIR QUALITY REQUIREMENTS</p>				

42.		<p>Control of fugitive dust is required by District Rule 403 and enforced by SMAQMD staff. The owner/applicant shall implement the following measures as identified by the SMAQMD:</p> <ul style="list-style-type: none"> • Water all exposed surfaces two times daily. Exposed surfaces include, but are not limited to soil piles, graded areas, unpaved parking areas, staging areas, and access roads. • Cover or maintain at least two feet of free board space on haul trucks transporting soil, sand, or other loose material on the site. Any haul trucks that would be traveling along freeways or major roadways should be covered. • Use wet power vacuum street sweepers to remove any visible trackout mud or dirt onto adjacent public roads at least once a day. Use of dry power sweeping is prohibited. • Limit vehicle speeds on unpaved roads to 15 miles per hour (mph). • All roadways, driveways, sidewalks, parking lots to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used. • Minimize idling time either by shutting equipment off when not in use or reducing the time of idling to 5 minutes [required by California Code of Regulations, Title 13, sections 2449(d)(3) and 2485]. Provide clear signage that posts this requirement for workers at the entrances to the site. • Maintain all construction equipment in proper working condition according to manufacturer’s specifications. The equipment must be checked by a certified mechanic and determine to be running in proper condition before it is operated. 	G, I, B	CD (P)(E)(B)
43.	✓	<p>The building design shall include a mechanical ventilation system that meets the criteria of the International Building Code (Chapter 12, §1203.2 of the California Building Code) to ensure that windows would be able to remain closed while maintaining adequate ventilation and temperature control. The mechanical ventilation system shall be designed to accommodate, and equipped with, filters having a Minimum Efficiency Reporting Value (MERV) rating of 13 or higher.</p>	B	CD (P)(B)

44.		Additional landscape plantings shall be provided where feasible along the southern, western, and eastern perimeter of Lot 1 to the satisfaction of the Community Development Department.	I	CD(P)(E)
GREENHOUSE GAS REQUIREMENTS				
45.	✓	In accordance with the City General Plan GHG Reduction Measure T-3, the project shall provide a minimum of five percent more bicycle parking than required in the City's Municipal Code Section 17.57.090 (for a total of 54 bicycle parking spaces).	B	CD (P)(B)
46.	✓	In accordance with the City General Plan GHG Reduction Measure T-6, the project shall use high-performance diesel (also known as Diesel-HPR or Reg-9000/RHD) for all diesel-powered equipment utilized in construction of the project.	B	CD (P)(B)
47.	✓	In accordance with the City General Plan GHG Reduction Measure T-8, the project shall provide electric vehicle capable parking spaces in ten percent of the total parking spaces on the project site (for a total of 49 EV Capable charging spaces).	B	CD (P)(B)
48.	✓	In accordance with the City General Plan GHG Reduction Measure SW-1, the project shall divert to recycle or salvage a minimum 65 of nonhazardous construction and demolition waste generated at the project site in accordance with Appendix A4 (Residential) of the as outlined in the California Green Building Standards Code (2019 CALGreen).	B	CD (P)(B)
49.	✓	In accordance with the City General Plan GHG Reduction Measure W-1, the project shall comply with all applicable indoor and outdoor water efficiency and conservation measures required under 2019 CALGreen Tier 1, as outlined in the California Green Building Standards Code.	B	CD (P)(B)

TRAFFIC, ACCESS, CIRCULATION, AND PARKING

50.	✓	<p>Based on the recommendations of the Transportation Impact Study dated February 2022 (Attachment 21), the following condition of approval shall be implemented to the satisfaction of the Community Development Department and the Public Works Department:</p> <ul style="list-style-type: none">• The owner/applicant shall modify Prairie City Road/ Iron Point Road signal timing plan by shifting 1 second from the eastbound through movement to the westbound left turn movement, reduce the vehicle extension setting from adding five to six additional seconds to the green phase for through movements to adding four seconds to the green phase for through movements for each vehicle passing the detector after the minimum green phase length has been exceeded. This mitigation measure shall be implemented by the City through the reimbursement agreement with the owner/applicant to cover any City costs. The implementation of this mitigation measure shall occur prior to issuance of the first building permit.	I	CD (P)(E), PW
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51.		<p>To further ensure safe travel within the project site, the following measures shall be implemented to the satisfaction of the Community Development Department:</p> <ul style="list-style-type: none"> • A “stop” sign and appropriate pavement markings shall be installed at the internal approach to the private ring road at the two primary project driveways. • The vehicle entry gates at the two primary project driveway locations shall open inward, away from the private ring road or retract sideways. In addition, the design of the vehicle entry gates and the vehicle entry gate area shall conform to all requirements established by the City of Folsom for gated multi-family residential developments. • If vehicles are observed backing up into the private ring road at either of the two gated primary project entries, City staff will evaluate and require appropriate measures to alleviate the traffic congestion including but not limited to requiring the two project entry gates to remain open during the AM (7:00 a.m. to 9:00 a.m.) and PM (4:00 p.m. to 6:00 p.m.) peak hours on weekdays. • Residents of the Folsom Corporate Center Apartments project shall be issued remote transmitters to allow them to open the entry gates without needing to stop to enter a code in the keypad at either entrance location. • The owner/applicant shall provide at least one pedestrian connection from Lot 1 to the southern property boundary to allow for a connection to the future Class I bicycle trail expected to be located within the 50-foot-wide landscape easement between the project site and U.S. Highway 50. 	I	CD (P)(E)
52.		A minimum of 462 on-site parking spaces shall be provided for the project.	I, O	CD (P)(E)
53.		A minimum of 51 on-site bicycle parking spaces shall be provided for the project in the two clubhouse buildings and at locations that are close proximity to the primary building entrances.	I, O	CD (P)(E)

NOISE REQUIREMENTS				
54.		Compliance with Noise Control Ordinance and General Plan Noise Element shall be required. Hours of construction operation shall be limited from 7:00 a.m. to 6:00 p.m. on weekdays and 8:00 a.m. to 5:00 p.m. on Saturdays. No construction is permitted on Sundays or holidays. Construction equipment shall be muffled and shrouded to minimize noise levels.	G, I, B	CD (P)(E)

<p>55.</p>	<p>✓</p>	<p>Construction activities shall be required to comply with the following and be noted accordingly on construction contracts:</p> <ol style="list-style-type: none"> 1. Construction hours/Scheduling: The following are required to limit construction activities to the portion of the day when occupancy of the adjacent sensitive receptors are at the lowest: <ol style="list-style-type: none"> a. Construction activities for all phases of construction, including servicing of construction equipment shall only be permitted during the hours of 7:00 a.m. and 6:00 p.m. Monday through Friday and between 8:00 a.m. to 5:00 p.m. on Saturdays. Construction is prohibited on Sundays and on all holidays. b. Delivery of materials or equipment to the site and truck traffic coming to and from the site is restricted to the same construction hours specified above. 2. Construction Equipment Mufflers and Maintenance: All construction equipment powered by internal combustion engines shall be properly muffled and maintained. 3. Idling Prohibitions: All equipment and vehicles shall be turned off when not in use. Unnecessary idling of internal combustion engines is prohibited. 4. Equipment Location and Shielding: All stationary noise-generating construction equipment, such as air compressors, shall be located as far as practical from the adjacent homes. Acoustically shield such equipment when it must be located near adjacent residences. 5. Quiet Equipment Selection: Select quiet equipment, particularly air compressors, whenever possible. Motorized equipment shall be outfitted with proper mufflers in good working order. 6. Staging and Equipment Storage: The equipment storage location shall be sited as far as possible from nearby sensitive receptors. 	<p>G, I, B</p>	<p>CD (P)(E)</p>
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56.	✓	<p>For habitable areas (both living rooms and bedrooms) with a direct line-of-sight to U.S. Highway 50 for Lot 1 and Iron Point Road for Lot 6, the following measures shall be incorporated in the design of the project to reduce interior noise levels to 45 CNEL or less:</p> <ul style="list-style-type: none"> ○ Lot 1 (Buildings 1 and 2) and Lot 6 (Building 2) – Minimum exterior wall requirement of STC 46. ○ Lot 1 (Buildings 1 and 2) and Lot 6 (Building 2) – Minimum window and glass sliding door requirement of STC 35. ○ Lot 1 (Building 7) and Lot 6 (Building 5) – Minimum window and glass sliding door requirement of STC 28. ○ The building design shall include a mechanical ventilation system that meets the criteria of the International Building Code (Chapter 12, §1203.3 of the 2013 California Building Code) to ensure that windows would be able to remain permanently closed. 	B	CD (P)(E)
ARCHITECTURE/SITE DESIGN REQUIREMENTS				
57.		The final location, design, materials, and colors of the trash/recycling enclosures be subject to review and approval by the Community Development Department.	I, B	CD (P)(E)
58.		The final location, height, design, materials, and colors for the proposed retaining walls and fencing shall be subject to review and approval by the Community Development Department.	I, B	CD (P)(E)

59.		<p>The project shall comply with the following architecture and design requirements:</p> <ol style="list-style-type: none"> 1. This approval is for 11 three-story apartment buildings and two clubhouse buildings associated with the Folsom Corporate Center Apartments project. The applicant shall submit building plans that comply with this approval and the attached building elevations and color renderings dated November 16, 2021. 2. The design, materials, and colors of the proposed Folsom Corporate Center apartment and clubhouse buildings shall be consistent with the submitted building elevations, color renderings, materials samples, and color scheme to the satisfaction of the Community Development Department. 3. Brick pavers or another type of colored masonry material (ADA compliant) shall be used to designate pedestrian crosswalks on the project site, in addition to where pedestrian paths cross drive aisles, and shall be incorporated as a design feature at the two primary driveway entrances for Lot 1 and Lot 6 to the satisfaction of the Community Development Department. 4. Roof-mounted mechanical equipment, including satellite dish antennas, shall not extend above the height of the parapet walls. Ground-mounted mechanical equipment shall be shielded by landscaping or trellis type features. 5. Utility equipment such as transformers, electric and gas meters, electrical panels, and junction boxes shall be screened by walls and or landscaping. 	I, B	CD (P)
GRADING REQUIREMENT				
61.		<p>Prior to the approval of the final facilities design and the initiation of construction activities, the applicant shall submit an erosion control plan to the City for review and approval. The plan shall identify protective measures to be taken during excavation, temporary stockpiling, any reuse or disposal, and revegetation. Specific techniques may be based upon geotechnical reports, the <u>Erosion and Sediment Control Handbook</u> of the State of California Department of Conservation, and shall comply with all updated City standards.</p>	G, I	CD (E)
SIGN REQUIREMENT				
62.		<p>The owner/applicant shall obtain a sign permit prior to installation of the three monument signs.</p>	B	CD (P)
OTHER AGENCY REQUIREMENT				

63.		The owner/applicant shall obtain all required State and Federal permits and provide evidence that said permits have been obtained, or that the permit is not required, subject to staff review and approval of any grading or improvement plan.	G, I	CD (P)(E)
FIRE DEPARTMENT REQUIREMENTS				
64.		The building shall have illuminated addresses visible from the street or drive fronting the property. Size and location of address identification shall be reviewed and approved by the Fire Marshal.	I	FD
65.		Prior to the issuance of any improvement plans or building permits, the Community Development and Fire Departments shall review and approve all detailed design plans for accessibility of emergency fire equipment, fire hydrant flow location, and other construction features.	I, B	FD
66.		All fire protection devices shall be designed to be located on site: fire hydrants, fire department connections, post indicator valves, etc. off-site devices cannot be used to serve the building. A water model analysis that proves the minimum fire flow will be required before any permits are issued. The fire sprinkler riser location shall be inside a Fire Control Room (5' X 7' minimum) with a full-sized 3'-0" door. This room can be a shared with other building utilities. The room shall only be accessible from the exterior.	I, B	FD
67.		All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material or vertical construction is allowed on site. All-weather access is defined as 6" of compacted AB from May 1 to September 30 and 2"AC over 6" AB from October 1 to April 30.	I, B	FD
POLICE/SECURITY REQUIREMENT				
68.		<p>The owner/applicant shall consult with the Police Department in order to incorporate all reasonable crime prevention measures. The following security/safety measures shall be required:</p> <ul style="list-style-type: none"> • A security guard shall be on-duty at all times at the site or a six-foot security fence shall be constructed around the perimeter of construction areas. (This requirement shall be included on the approved construction drawings). • Security measures for the safety of all construction equipment and unit appliances shall be employed. • Landscaping shall not cover exterior doors or windows, block line-of-sight at intersections or screen overhead lighting. 	G, I, B	PD

MISCELLANEOUS REQUIREMENTS				
69.		The proposed project shall comply with all State and local rules, regulations, Governor’s Declarations, and restrictions including but not limited to: Proclamation of a State of Emergency due to drought conditions issued by the Governor of California on October 19, 2021 relative to water usage and conservation, requirements relative to water usage and conservation established by the State Water Resources Control Board, and water usage and conservation requirements established within the <u>Folsom Municipal Code, (Section 13.26 Water Conservation)</u> , or amended from time to time.	I, B, OG	CD (P)(E)

CONDITIONS

See attached tables of conditions for which the following legend applies.

RESPONSIBLE DEPARTMENT		WHEN REQUIRED	
CD	Community Development Department Planning Division Engineering Division Building Division Fire Division	I	Prior to approval of Improvement Plans
(P)		M	Prior to approval of Final Map
(E)		B	Prior to issuance of first Building Permit
(B)		O	Prior to approval of Occupancy Permit
(F)		G	Prior to issuance of Grading Permit
PW	Public Works Department	DC	During construction
PR	Park and Recreation Department	OG	On-going requirement
PD	Police Department		

Attachment No. 2

Ordinance No. 1327 - An Uncodified Ordinance to amend the zoning designation for a 7.24-acre parcel (Lot 1) from M-L PD to R-4 PD and to amend the zoning designation for a 4.68-acre parcel (Lot 6) from BP PD to R-4 PD for the Folsom Corporate Center Apartments project (Introduction and First Reading)

ORDINANCE NO. 1327**AN UNCODIFIED ORDINANCE TO AMEND THE ZONING DESIGNATION FOR A 7.24-ACRE PARCEL (LOT 1) FROM M-L PD TO R-4 PD AND TO AMEND THE ZONING DESIGNATION FOR A 4.68-ACRE PARCEL (LOT 6) FROM BP PD TO R-4 PD FOR THE FOLSOM CORPORATE CENTER APARTMENTS PROJECT**

WHEREAS, the proposed Folsom Corporate Center Apartments project consists of the development of a 253-unit market-rate apartment community on an 11.92-acre site located within the Folsom Corporate Center; and

WHEREAS, the Planning Commission, at its regular meeting on April 6, 2022, considered the proposed rezone of two parcels associated with the Folsom Corporate Center Apartments project and determined that the proposed rezone was appropriate given the existing residential and commercial land uses in the project vicinity; and

WHEREAS, all notices have been given at the time and in the manner required by State Law and the Folsom Municipal Code.

NOW, THEREFORE, the City Council of the City of Folsom hereby does ordain as follows:

SECTION 1. PURPOSE

- A. A certain property, a 7.24-acre parcel (APN: 072-3120-023), located at 2275 Iron Point Road, is proposed for rezoning, from M-L PD (Limited Industrial, Planned Development District) to R-4 PD (General Apartment, Planned Development District) and a certain property, a 4.68-acre area (APN: 072-3120-026), located at 2275 Iron Point Road, is proposed for rezoning, from BP PD (Business and Professional, Planned Development District) to R-4 PD (General Apartment, Planned Development District); and
- B. The proposed rezoning is consistent with the objectives, goals and policies of the Folsom General Plan; and
- C. A duly noticed public hearing was held before the Planning Commission on April 6, 2022; and
- D. A Mitigated Negative Declaration has been prepared for the project in accordance with the California Environmental Quality Act. The Negative Declaration and the Initial Study are incorporated herein by reference; and
- E. Notice of hearing before the City Council has been given in the form and in the manner required by State statute and Folsom City Code.

SECTION 2. CHANGE OF ZONING MAP DESIGNATION

The Zoning Map designation for the subject parcels are hereby amended from M-L PD (Limited Manufacturing, Planned Development District) to BP PD (Business and Professional, Planned Development District) and BP PD (Business and Professional, Planned Development District) to R-4 PD (General Apartment, Planned Development District) as set forth on Exhibit A.

SECTION 3. SEVERABILITY

If any section, subsection, sentence, clause, or phrase in this Ordinance or any part thereof is for any reason held to be unconstitutional, invalid, or ineffective by any court of competent jurisdiction, such decision shall not affect the validity or effectiveness of the remaining portions of this Ordinance or any part thereof. The City Council declares that it would have passed each section irrespective of the fact that any one or more section, subsection, sentence, clause, or phrase be declared unconstitutional, invalid, or ineffective.

SECTION 4. EFFECTIVE DATE

This ordinance shall become effective thirty (30) days from and after its passage and adoption, provided it is published in full or in summary within twenty (20) days after its adoption in a newspaper of general circulation in the City.

This ordinance was introduced and the title thereof read at the regular meeting of the City Council on May 10, 2022, and the second reading occurred at the regular meeting of the City Council on May 24, 2022.

On a motion by _____, seconded by _____ the foregoing ordinance was passed and adopted by the City Council of the City of Folsom, State of California, this 10th day of May, 2022 by the following vote, to wit:

AYES: Council member(s):
NOES: Council member(s):
ABSTAIN: Council member(s):
ABSENT: Council member(s):

Kerri M. Howell, MAYOR

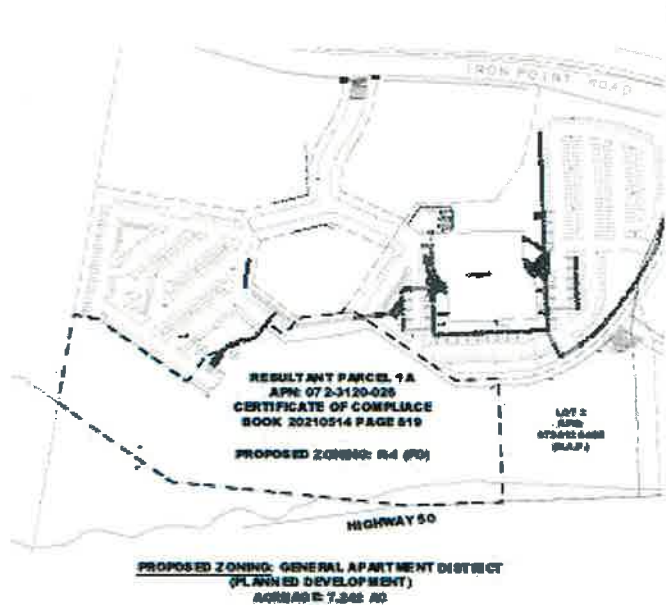
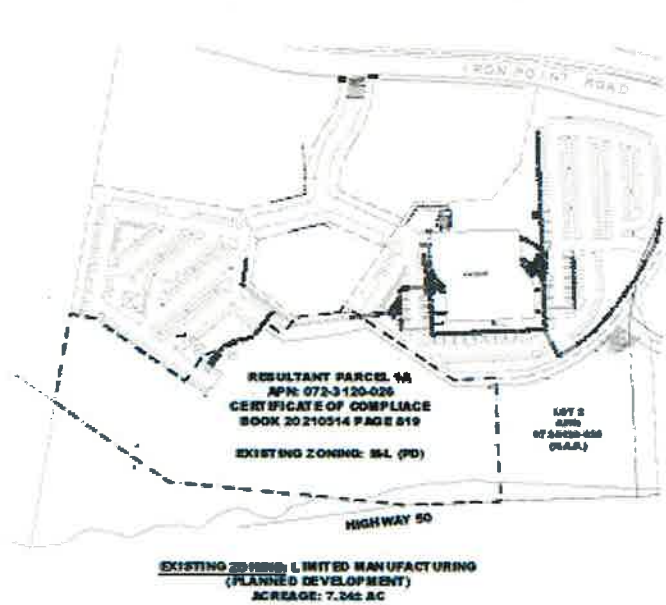
ATTEST:

Christa Freemantle, CITY CLERK

Exhibit A

Rezone Exhibits

Rezone Exhibit (Lot 1)



OWNER/APPLICANT:
 F. S. L.
 1000 S. ...
 ...
ENGINEER:
 ...
 ...

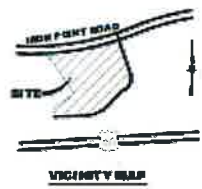
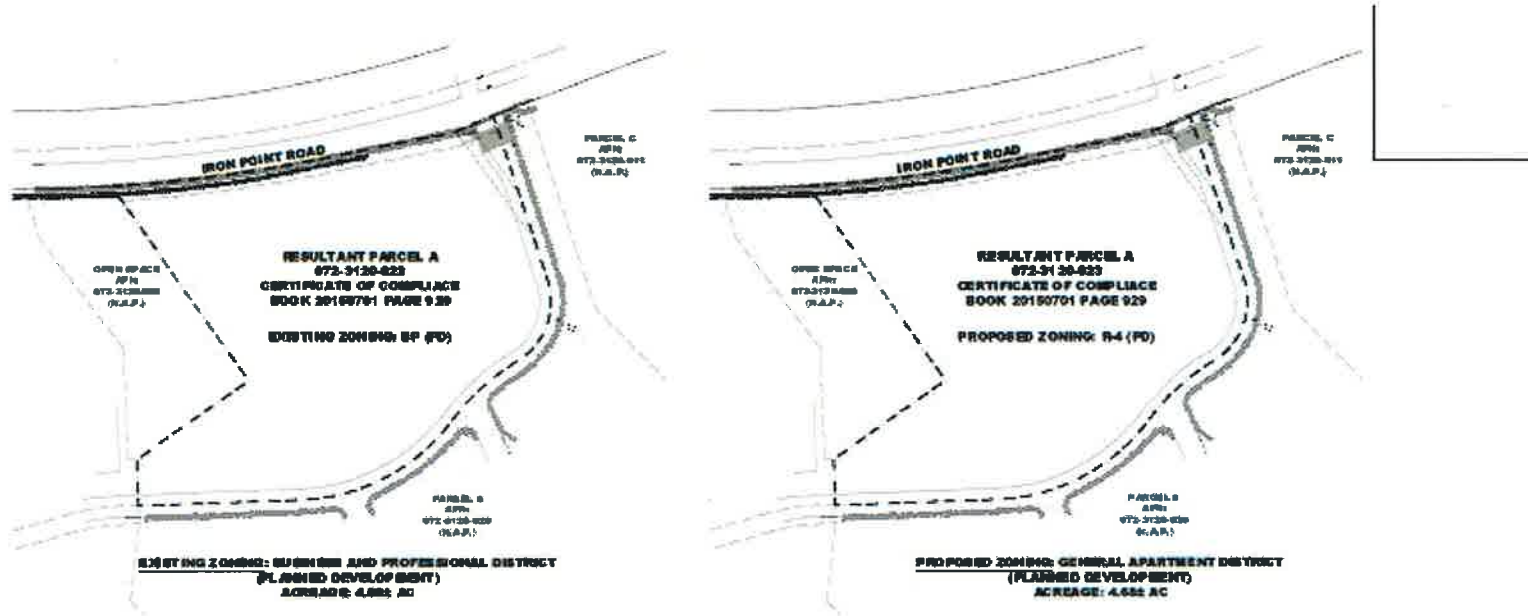


REZONE EXHIBIT **RZ**
IRON POINT ROAD APARTMENTS - LOT 1
 FOLSOM, CA

This drawing presented is a tentative of schedule and design work only, and is not subject to change without the design consultant's approval. It is subject to local, state, and federal design requirements, and other applicable codes, ASCE 2018-0528 Design, etc.

NOVEMBER 16, 2021 | MR200323.00

Rezone Exhibit (Lot 6)



OWNER/APPLICANT:
 PROJECT: IRON POINT APARTMENTS - LOT 6
 TITLE: REZONE EXHIBIT
ENGINEER:
 PROJECT: IRON POINT APARTMENTS - LOT 6
 TITLE: REZONE EXHIBIT



REZONE EXHIBIT **RZ**

IRON POINT ROAD APARTMENTS - LOT 6
 FOLSOM, CA



The drawings presented are the sole responsibility of the design team and are subject to change based upon the design development process. All drawings are for informational purposes only and are not to be used for construction without the approval of the design team.

NOVEMBER 16, 2021 | MR200320-00

Attachment No. 3

Planning Commission Staff Report, dated April 6, 2022



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

AGENDA ITEM NO. 2
Type: Public Hearing
Date: April 6, 2022

Planning Commission Staff Report

50 Natoma Street, Council Chambers
Folsom, CA 95630

Project: Folsom Corporate Center Apartments

File #: PN 21-120

Requests: General Plan Amendment

Rezone

Planned Development Permit

Location/APN: The proposed Folsom Corporate Center Apartments project is located on two parcels situated on the south side of Iron Point Road, slightly east of the intersection of Iron Point Road and Oak Avenue Parkway/APN Nos. 072-3120-023 and 072-3120-026

Staff Contact: Steve Banks, Principal Planner, 916-461-6207
sbanks@folsom.ca.us

Property Owner/Applicant

Name: FCC 50, LLC (Cole Partners)
Address: 2484 Natomas Park Drive,
Suite 101
Sacramento CA 95833

Recommendation: Conduct a public hearing and upon conclusion recommend to City Council approval of a General Plan Amendment, Rezone, and Planned Development Permit for the Folsom Corporate Center Apartments project, subject to the findings (Findings A-U) and conditions of approval (Conditions 1-69) attached to this report.

Project Summary: The proposed project includes development of a 253-unit market-rate apartment community on two sites (Lot 1: 7.24-acre parcel and Lot 6: 4.68-acre parcel) within the Folsom Corporate Center, which is located on the south side of Iron Point Road, slightly east of the intersection of Iron Point Road and Oak Avenue Parkway. The following are the specific entitlements requested with the proposed project.

- A **General Plan Amendment** to change the General Plan land use designation for the two project parcels (Lot 1 and Lot 6) from IND (Industrial/Office Park) to MHD (Multi-Family High Density).



AGENDA ITEM NO. 2

Type: Public Hearing

Date: April 6, 2022

- A **Rezone** to change the zoning designation for Lot 1 from M-L PD (Limited Manufacturing, Planned Development District) to R-4 PD (General Apartment, Planned Development District) and to change the zoning designation of Lot 6 from BP PD (Business and Professional, Planned Development District) to R-4 PD (General Apartment, Planned Development District).
- A **Planned Development Permit** which contains detailed development and architectural standards for the proposed 253-unit residential apartment community.

These proposed actions are described in detail and analyzed later in this report.

Table of Contents:

Attachment 1 - Background and Setting

Attachment 2 - Project Description

- General Plan Amendment
- Rezone
- Planned Development Permit

Attachment 3 - Analysis

- General Plan Amendment
- Rezone
- Planned Development Permit

Attachment 4 - Conditions of Approval

Attachment 5 - Vicinity Map

Attachment 6 - General Plan Amendment Exhibits, dated November 16, 2021

Attachment 7 - Rezone Exhibits, dated November 16, 2021

Attachment 8 - Overall Site Plan, dated November 16, 2021

Attachment 9 - Individual Site Plans and Details, dated February 8, 2022

Attachment 10 - Preliminary Utility Plans, dated November 16, 2021

Attachment 11 - Preliminary Grading and Drainage Plans, dated November 16, 2021

Attachment 12 - Preliminary Landscape Plans and Details, dated November 16, 2021

Attachment 13 - Preliminary Access and Circulation Plan, dated November 16, 2021

Attachment 14 - Preliminary Lighting Plan and Details, dated November 16, 2021

Attachment 15 - Building Elevations, Floor Plans, and Details dated November 16, 2021

Attachment 16 - Color Renderings and Perspectives, dated November 16, 2021

Attachment 17 - Color and Materials Board, dated November 16, 2021

Attachment 18 - Signage Details, dated November 16, 2021

Attachment 19 - Building and Parking Summary, dated February 8, 2022



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

AGENDA ITEM NO. 2
Type: Public Hearing
Date: April 6, 2022

- Attachment 20 - Site Photographs
- Attachment 21 - Transportation Impact Study, dated February, 2022
- Attachment 22 - Initial Study, Mitigated Negative Declaration, and Mitigation Monitoring and Reporting Program, dated March, 2022 (electronic version available for viewing at www.folsom.ca.us/government/community-development/planning-services/current-project-information)
- Attachment 23 - SMAQMD ISMND Response Letter, dated March 24, 2022
- Attachment 24 - Folsom Corporate Center Planned Development Guidelines
- Attachment 25 - Folsom Corporate Center Apartments Booklet (Separate Bound Document)

Submitted,

PAM JOHNS
Community Development Director

ATTACHMENT 1 BACKGROUND AND SETTING

Background:

On August 15, 2000, the City Council approved a Tentative Subdivision Map and Planned Development for development of a 1.425-million-square-foot professional office center known as the Folsom Corporate Center. On May 1, 2002, the Planning Commission approved a Planned Development Permit and Conditional Use Permit for development of a 255,795-square-foot retail shopping center known as Folsom Gateway within the eastern portion of the previously approved Folsom Corporate Center. That approval resulted the reduction of 395,000 square feet of office space within the Folsom Corporate Center. A total of four professional office buildings have been developed within the Folsom Corporate Center with major tenants including HDR Engineering, Kaiser Permanente, Micron Technology, and SAFE Credit Union.

On January 26, 2016, the City Council approved a General Plan Amendment, Rezone, Planned Development Permit, and Conditional Use Permit for development of the 126-unit senior retirement community known as the Iron Point Retirement Community on a 4.68-acre property located at 2275 Iron Point Road. On October 4, 2017, the Planning Commission approved a one-year extension to the previously approved Planned Development Permit and Conditional Use Permit associated with the Iron Point Retirement Community project. On February 6, 2019, the Planning Commission approved an additional one-year extension to the previously approved Planned Development Permit and Conditional Use Permit associated with the Iron Point Retirement Community project. Subsequently, the applicant decided not to pursue development of the project and withdrew their application. It is important to note that the 4.68 parcel associated with Iron Point Retirement Community project is one of the parcels (Lot 6) included with the proposed Folsom Corporate Center Apartments project.

On October 7, 2020, the Planning Commission approved a Design Review application for development of an 11,716-square-foot single-story medical building (Kidney Dialysis Treatment Center) on a 2.77-acre site located near the southwest corner of the intersection of Iron Point Road and Rowberry Drive within the Folsom Corporate Center. The Kidney Dialysis Treatment Center is currently under construction and is located directly to the east of one of the parcels (Lot 1) associated with the proposed Folsom Corporate Center Apartments project.

Physical Setting

The Folsom Corporate Center Apartments project site consists of two separate parcels located within the Folsom Corporate Center development, which is generally located on the south side of Iron Point Road, slightly east of the intersection of Iron Point Road and Oak Avenue Parkway. Lot 1, which is a 7.24-acre parcel located between the Kaiser Permanente Medical Office Building and U.S. Highway 50 to the south, features moderately sloped terrain covered with non-native grasses and a single native Oak tree. Lot 6, which is a 4.68-acre parcel located between Iron Point Road and the SAFE Credit Union building to the south, has gently sloped terrain and contains non-native grasses and 10 native Oak trees. An aerial photograph of the project site and surrounding land uses is shown in Figure 1 below.

FIGURE 1: AERIAL PHOTOGRAPH OF PROJECT SITE



ATTACHMENT 2 PROJECT DESCRIPTION

APPLICANT'S PROPOSAL

The applicant, FCC 50, LLC (Cole Partners), is requesting approval of a General Plan Amendment, Rezone, and Planned Development Permit for development of a 253-unit market-rate apartment community on two parcels (Lot 1: 7.24-acre parcel and Lot 6: 4.68-acre parcel) within the Folsom Corporate Center, which is generally located on the south side of Iron Point Road, slightly east of the intersection of Iron Point Road and Oak Avenue Parkway.

As noted above, the applicant is requesting approval of three entitlements to allow for development of the proposed apartment community. The first entitlement is a request for approval of a General Plan Amendment to change the General Plan land use designation for the two project parcels (Lot 1 and Lot 6) from IND (Industrial/Office Park) to MHD (Multi-Family High Density). The second entitlement is a request for approval of a Rezone to change the zoning designation for Lot 1 from M-L PD (Limited Manufacturing, Planned Development District) to General Apartment, Planned Development District (R-4 PD) and to change the zoning designation of Lot 6 from BP PD (Business and Professional, Planned Development District) to General Apartment, Planned Development District (R-4 PD). The third entitlement is a request for approval of a Planned Development Permit to establish project-specific development standards, review the project site design, evaluate the architectural design of the multi-family apartment and clubhouse buildings, and establish signage criteria.

The proposed Folsom Corporate Center Apartments project, which includes development of 11 three-story apartment buildings and two clubhouse buildings (three-story and one-story buildings respectively), is comprised of 253 market rate apartments within a gated community. The apartment buildings include a combination of 16-plex buildings, 21-plex buildings, 26-plex building, and 32-plex buildings with a total of 16 studio units (564 square feet), 126 one-bedroom units (687 square feet), 97 two-bedroom units (990-1057 square feet), and 14 three-bedroom units (1,412 square feet). All apartment units are proposed to be accessible from interior hallways and include a full kitchen, living space, storage closets, bedrooms, bathrooms, and an outdoor patio/balcony. The one and three-story clubhouse buildings include a recreation room, a fitness center, a yoga studio, a spa room, a mail room, a bike storage facility, leasing offices, a storage room, and restroom facilities. Outdoor amenities associated with the clubhouse buildings include a pool, a spa, and deck areas. Additional outdoor amenities include two dog parks.

In relation to site design, Lot 1 includes seven rectangular apartment buildings that are evenly spaced within the eastern portion of parcel due to constraints associated with overhead transmission lines situated in the western portion of the parcel. Lot 6 includes four rectangular apartment buildings which are centrally located on the parcel.

The applicant proposes a modern contemporary architectural design theme intended to compliment the surrounding commercial buildings within the Folsom Corporate Center. Modern and unique design elements include angular building shapes and forms, varied roof heights, flat rooftops, recessed building elements, metal canopies, and extensive use of glass. Proposed building materials include stucco walls, stone veneer wainscoting, metal canopies, glass railing, and metal railing. The color scheme for the buildings is proposed to be generally earth tone, with extensive use of gray and brown colors accented by a mixture of lighter colors including white and tan.

General access to the project area is provided by three existing driveways located on the south side of Iron Point Road. Primary vehicle access to Lot 1 is provided by a new driveway on south side of an existing private ring road with secondary access accommodated by two emergency vehicle access driveways also situated on the south side of the ring road. Primary vehicle access to Lot 6 is provided by a new driveway on the north side of the private ring road with secondary access served by an emergency vehicle access driveway also positioned on the north side of the ring road. Each of the project driveways will accommodate all vehicle turning movements into and out of the respective sites. In addition, all project driveways will have access controlled by vehicle gates.

Proposed internal vehicle circulation consists of 27-foot-wide drive aisles to facilitate movement in and around the project sites. Pedestrian circulation is provided by a combination of new sidewalks and existing sidewalks located along the private ring road and also along Iron Point Road. Internal pedestrian circulation is accommodated by a series of new pedestrian pathways that provide connectivity to the apartment buildings, the clubhouse building, the perimeter sidewalks, and the future Class I trail to the south. Additional site improvements include: 491 parking spaces (includes combination of garage, carport, and uncovered spaces), 51 bicycle parking spaces, 5 electric vehicle charging stations, underground utilities, drainage basins, site lighting, site landscaping, retaining walls, fencing, and project identification signs. The proposed site plans are shown in Figure 2, Figure 3, and Figure 4 on the following pages.

FIGURE 2: OVERALL SITE PLAN

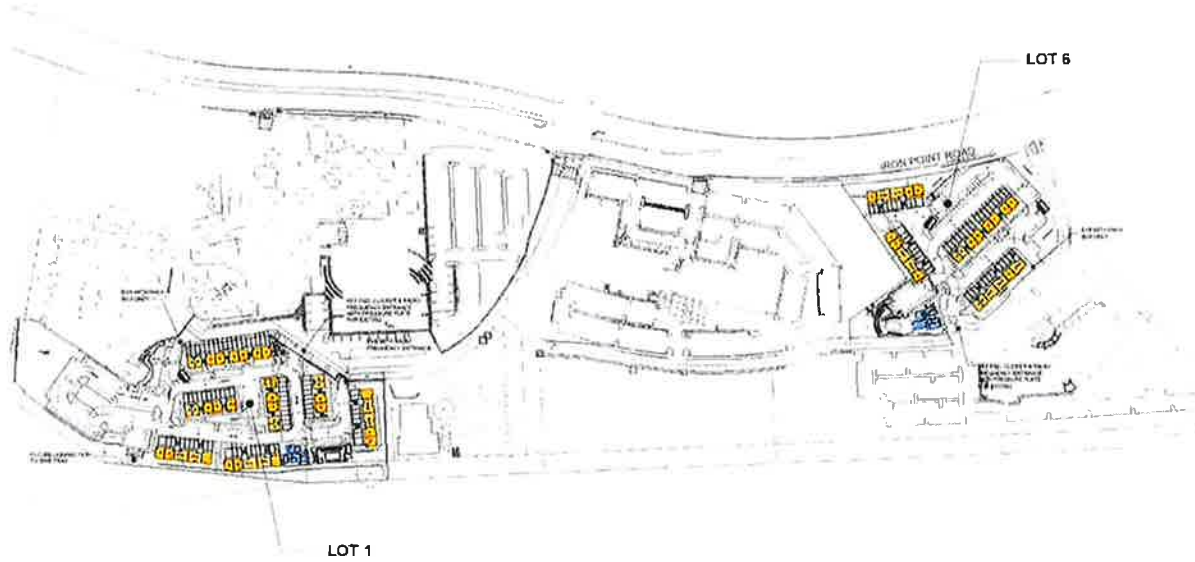
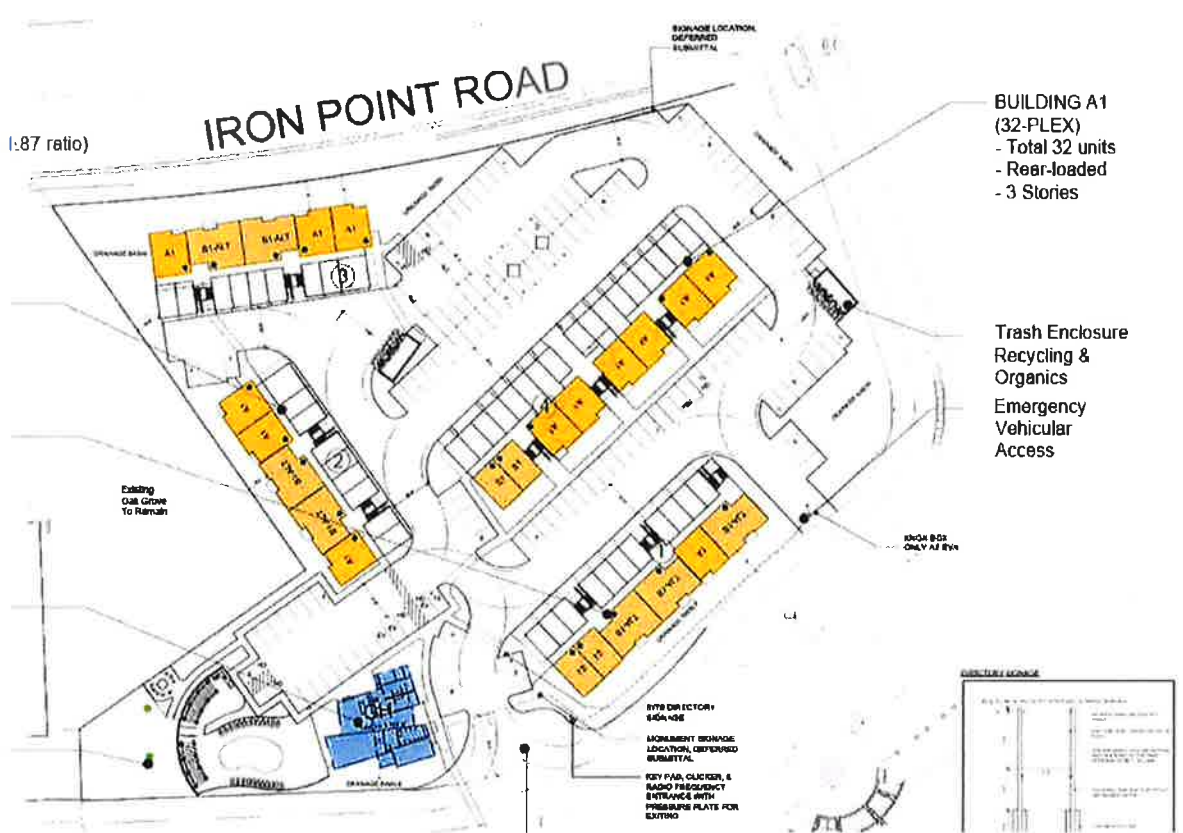


FIGURE 3: LOT 1 SITE PLAN



FIGURE 4: LOT 6 SITE PLAN



ATTACHMENT 3 ANALYSIS

The following sections provide an analysis of the applicant's proposal. Staff's analysis includes:

- A. General Plan Amendment and Rezone
- B. Planned Development Permit
 - Development Standards
 - Building Architecture and Design
 - Signage
- C. Traffic/Access/Circulation
- D. Parking
- E. Noise Impacts
- F. Walls/Fencing
- G. Site Lighting
- H. Trash/Recycling
- I. Existing and Proposed Landscaping
- J. Conformance with Relevant Folsom General Plan Objectives and Policies
- K. Native American Consultation

A. General Plan Amendment and Rezone

General Plan Amendment and Rezone

The Folsom Corporate Center Apartments project is comprised of two separate parcels, Lot 1, which is 7.24-acres in size and Lot 6, which is 4.68-acres in size. Lot 1 and Lot 6 each have a General Plan land use designation of IND (Industrial/Office Park). As shown on Attachment 6, the proposed project includes a request to change the General Plan land use designation for both parcels from IND (Industrial/Office Park) to MHD (Multi-Family High Density). Lot 1 currently has a Zoning designation M-L PD (Limited Manufacturing, Planned Development District), while Lot 6 has a zoning designation of BP PD (Business and Professional, Planned Development District). As shown on Attachment 7, the proposed project includes a request to change the zoning designation for Lot 1 from M-L PD (Limited Manufacturing, Planned Development District) to R-4 PD (General Apartment, Planned Development District) and to change the zoning designation of Lot 6 from BP PD (Business and Professional, Planned Development District) to R-4 PD (General Apartment, Planned Development District). With approval of the proposed amendments and rezones, the entire project site will have a General Plan land use designation of MHD and a Zoning designation of R-4 PD.

The project is consistent with both the proposed General Plan land use designations and the proposed zoning designations, as multi-family apartments are identified as a permitted land use within the Folsom Municipal Code (FMC, Section 17.18.020 Permitted Uses). The proposed project includes a density of 21.2 dwelling units per acre, is consistent with the allowable density range (20-30 dwelling units per acre) established by the General Plan for Multi-Family High Density (Table LU-1: Residential Designations). In addition, the proposed project meets the development requirements established by the Folsom Municipal Code (FMC, Chapter 17.18, General Apartment District) and the Folsom Corporate Center Planned Development Guidelines with some minor modifications (discussed within the Planned Development Permit section of this staff report). Proposed modifications to development standards include lot area, lot width, building coverage, building height, building setbacks, and parking, which are discussed in the Planned Development Permit section of this staff report.

In reviewing the proposed General Plan Amendment and the Rezone, staff took into consideration community benefits that the proposed apartment project will provide relative to the supply of new housing units. City staff also considered the changes in the region's office and housing markets over the past 10 to 15 years. According to the California Department of Housing and Community Development (HUD), the state of California is facing a severe shortage with regard to housing supply, with some estimates indicating a shortfall of up to 3.5 million housing units. The housing shortage has a number of significant negative effects including but not limited to causing housing prices to rise which limits affordability, and increasing the homeless population in communities. The benefit of the proposed project is that it will increase the City's housing supply by providing 253 new market-rate rental units along the Iron Point Road corridor in close proximity to jobs and services in that area of the City.

Cole Partners, who is the original developer of the 900,000-square-foot Folsom Corporate Center, described efforts to bring new medical and office uses to the Folsom area over the last two decades. Since inception of the Corporate Center in 2000, the development has attracted prominent medical and office companies including Kaiser Permanente, Micron, and SAFE Credit Union. However, the applicant describes changing regional market dynamics over the last decade (changes in technology, acceptable of telecommuting, etc.) with the interest in housing projects far outpacing the demand for new office development. It has been more than 12 years since any new major office buildings (Waste Connections/SAFE Credit Union and Numonyx/Micron) were constructed within the Corporate Center. Notably, these two office buildings are the last privately developed larger suburban office buildings completed not only in Folsom, but along the Highway 50 corridor. While the office market dynamic has changed in a negative way, the regional demand for housing (single-family and multi-family) continues to remain extremely strong, especially in Folsom with a range of multi-family projects

(Alder Creek Apartments, Avenida Senior Apartments, Mangini Ranch Apartments, Scholar Way Apartments, etc.) being approved recently. Based on these factors, staff has determined that the proposed changes in land use and zoning are warranted.

Land Use Compatibility

In evaluating the General Plan Amendment and the Rezone, staff also took into consideration the compatibility of the proposed project relative to existing land uses in the project area. The proposed project is located on two undeveloped parcels within the Folsom Corporate Center. The project site is bounded by Iron Point Road to the north with single-family residential development (Broadstone Unit. No. 2) and multi-family residential development (Sherwood Apartments) beyond, U.S. Highway 50 to the south with undeveloped properties within the Folsom Plan Area beyond, multi-family development (Revel Senior Living and CountryHouse Memory Care) to the west with future Oak Avenue Parkway extension and commercial development beyond, and commercial development to the east with East Bidwell Street Beyond.

The most prominent land uses in the immediate project area are professional office-related and include SAFE Credit Union, Micron, Kaiser Permanente, and HDR. Residential land uses in close proximity to the site include the Broadstone Unit No. 2 Subdivision (approximately 150 feet to the north across Iron Point Road), Sherwood Apartments (approximately 400 feet to the northeast across Iron Point Road), and Revel Senior Living Apartments (approximately 500 feet to the west). Medical-office related land uses in the project vicinity include the aforementioned Kaiser Permanente Medical Office facility and the Kaiser Permanente Surgery Center. The nearest retail commercial development (Folsom Gateway Shopping Center, which was also developed by a Cole-related entity) is located approximately 1,200 feet to the east of the project site. Additional retail commercial development is located north of Iron Point Road (Palladio at Broadstone), approximately 3,100 feet east of the project site. Both retail commercial developments include grocery stores and a variety of retail shops.

As described above, the project site is situated in a unique location that includes a wide array of land uses including professional offices, medical offices, retail shopping, multi-family apartments, single-family residences, and a memory care facility. As mentioned within the project description, the Folsom Corporate Center Apartments project is a market-rate apartment community providing living opportunities for residents within 253 apartment units. Given the residential nature of the proposed use, staff has determined that the proposed project will be complimentary to the existing multi-family and single-family residential land uses located in the immediate project vicinity. In addition, taking into account the basic needs of the apartment residents, staff has determined that the proposed project is well-situated to take advantage of the numerous goods (grocery stores, restaurants, and retail shops) and services (medical offices) and job opportunities

that are located within walking distance of the site.

B. Planned Development Permit

The purpose of the Planned Development Permit process is to allow greater flexibility in the design of integrated developments than otherwise possible through strict application of land use regulations. The Planned Development Permit process is also designed to encourage creative and efficient uses of land. The following are proposed as part of the applicant's Planned Development Permit:

- Development Standards
- Building Architecture and Design
- Signage

Development Standards

The Folsom Corporate Center includes development standards that were intended to guide commercial development and did not take into account that residential development might occur within the boundaries of the Corporate Center. As a result, the applicant's intent with the subject application is to create a set of unique set of development standards that are better suited for multi-family residential development, yet still generally comply with the development standards established for properties within the Folsom Corporate Center as well as being consistent with the development standards established for properties within the General Apartment (R-4) zoning district. Table 1 lists the existing and proposed development standards for the Folsom Corporate Center Apartments project.

TABLE 1: DEVELOPMENT STANDARDS TABLE

Development Standards Table Folsom Corporate Center Apartments						
	Lot Area	Lot Width	Front Yard Setback	Rear Yard Setback	Side Yard Setbacks	Building Height
Existing Standards	0.5-Acres	NA	30 Feet Iron Point Rd.	NA	5 Feet	60 feet
R-4 District Standards	6,000 S.F.	60 Feet	20 Feet	10 Feet	5 Feet/10 Feet	50 Feet
Proposed Standards	0.5-Acres	60 Feet	40 Feet Iron Point Rd. 20 Feet	15 Feet	15 Feet	41 feet

As shown in Table 1, the proposed project meets or exceeds all development standards established for the Folsom Corporate Center and for the R-4 (General Apartment) zoning district. However, the proposed project does deviate from one guideline that is not shown in the table above. The Folsom Corporate Center Planned Development Guidelines

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recommend that a 30-foot-wide landscape buffer be provided along the Iron Point Road frontage. Due to site constraints (topography, shape, etc.), the applicant is proposing to reduce the width of the landscape buffer (17-21 feet) along the eastern portion of the Lot 6 frontage with Iron Point Road, while at the same time expanding the width of the buffer (41-43 feet) along a greater length of the western portion of the Lot 6 frontage with Iron Point Road. With this proposed landscape modification, the average width of the landscape buffer along Iron Point Road would exceed 30 feet. Staff supports this landscape modification as the total amount of landscaping along the Iron Point Road frontage will be increased.

Building Architecture and Design

As detailed in the Project Description section of this report, the proposed project includes development of 11 three-story apartment buildings and two clubhouse buildings on two separate parcels within the Folsom Corporate Center. The design concept for the apartment building and clubhouse buildings features a modern contemporary architectural style with strong articulation of building forms and massing, both of which are used to break up the scale of the buildings. Proposed building materials include stucco walls, stone veneer wainscoting, metal canopies, glass railing, and metal railing. The color scheme for the buildings is proposed to be primarily earth tone, with prominent use of gray and brown colors accented by a mixture of lighter colors including white and tan. Proposed elevations and renderings of the apartment and clubhouse buildings are shown in the exhibits below and on the following pages.

FIGURE 5: BUILDING ELEVATIONS (16-PLEX)



Front Elevation



Perspective



Left Elevation

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FIGURE 6: BUILDING ELEVATIONS (21-PLEX)



FIGURE 7: BUILDING ELEVATIONS (26-PLEX)



FIGURE 8: BUILDING ELEVATIONS (32-PLEX)



Rear Elevation



Perspective



Left Elevation

FIGURE 9: CLUBHOUSE BUILDING ELEVATIONS (LOT 1)



FRONT ELEVATION



SOUTH AERIAL VIEW OF THE LOT 1 CLUBHOUSE



STREET VIEW

FIGURE 10: CLUBHOUSE BUILDING ELEVATIONS (LOT 6)



Rear Elevation



Front Elevation



Left Elevation



Right Elevation



Perspective



Perspective

FIGURE 11: BUILDING RENDERINGS (LOT 1)



FIGURE 12: BUILDING RENDERINGS (LOT 6)



The proposed project is subject to the Folsom Corporate Center Design Guidelines. The Design Guidelines, in respect to overall architectural design concepts, are intended to provide a framework for design, while not restricting creativity. The following are design parameters recommended by the Design Guidelines to ensure a high-level quality of development:

- Buildings should be responsive to views from all four elevations
- Building masses should be made human in scale, present varied elevations, and use accent materials to add variety
- Building materials such as tile, stone, glass, metal panels, and concrete should be utilized together to reflect the area's modernity, diversity, and traditions.
- Building entries shall be distinguished with accent materials such as stone, slate, color metal panels, or concrete.

In addition to the Folsom Corporate Center Design Guidelines, the proposed project is subject to the City's Design Guidelines for Multi-Family Development. The Design Guidelines for Multi-Family Development recommend that multi-family projects be designed in a manner that compliments the surrounding community. The following are some of the specific design recommendations suggested by the Design Guidelines:

- Variety and distinctness in design are desirable.

- Expanses of uninterrupted wall area and unbroken roof forms shall be discouraged. Balconies, porches, bay windows, chimneys, and other design elements with projections and varied setbacks shall be used to break up the physical characteristics of structures.
- The use of a variety and combination of building materials is encouraged. Building materials selected for multi-family projects shall be very durable and require low maintenance including, but not limited to, stucco, stone, and brick. Building materials shall integrate quality design elements consistent with the design of the development and the surrounding neighborhood.
- Exterior building colors shall be compatible with the surrounding neighborhood setting and shall not be out of character or in visual competition with the existing surrounding design elements.
- All accessory structures, including carports, garages, and solid waste enclosures, shall be designed with materials and in a manner consistent with the architectural design characteristics of the development.

As illustrated on the building elevations and color renderings (Attachments 15 and 16), the proposing apartment and clubhouse buildings incorporate many of the key design features recommended by the Folsom Corporate Center Design Guidelines and the Design Guidelines for Multi-Family Development including the use of rectilinear building shapes to create a sense of depth, use of varied forms to create visual relief, use of staggered building elements to create visual interest, and the inclusion of unique design details to reinforce the modern contemporary residential design theme.

As shown on the color and materials board (Attachment 17), the proposed project utilizes a variety of modern building materials to enhance the appearance of the building including the use of stucco on the walls, stone veneer wainscotting, glass windows and doors, metal canopies, glass railing, and metal railing. As recommended by the Design Guidelines, the proposed project features a natural color scheme with extensive use of earth tone colors including gray and brown, complimented with lighter colors including white and tan.

Based on the aforementioned analysis, staff has determined that the proposed project represents a high-quality design that is consistent with the design recommendations of the Folsom Corporate Center Design Guidelines and the Design Guidelines for Multi-Family Development. In addition, staff has determined that the project design is complimentary to the design of existing commercial and residential buildings in the immediate project area. As a result, staff recommends approval of the applicant's design with the following conditions:

1. This approval is for 11 three-story apartment buildings and two clubhouse buildings associated with the Folsom Corporate Center Apartments project. The

applicant shall submit building plans that comply with this approval and the attached building elevations and color renderings dated November 16, 2021.

2. The design, materials, and colors of the proposed Folsom Corporate Center apartment and clubhouse buildings shall be consistent with the submitted building elevations, color renderings, materials samples, and color scheme to the satisfaction of the Community Development Department.
3. Brick pavers or another type of colored masonry material (ADA compliant) shall be used to designate pedestrian crosswalks on the project site, in addition to where pedestrian paths cross drive aisles, and shall be incorporated as a design feature at the two primary driveway entrances for Lot 1 and Lot 6 to the satisfaction of the Community Development Department.
4. Roof-mounted mechanical equipment, including satellite dish antennas, shall not extend above the height of the parapet walls. Ground-mounted mechanical equipment shall be shielded by landscaping or trellis type features.
5. Utility equipment such as transformers, electric and gas meters, electrical panels, and junction boxes shall be screened by walls and or landscaping.

These recommendations are included in the conditions of approval (Condition No. 60) presented for consideration by the Planning Commission.

Signage

The proposed project includes placement of three monument signs at strategic locations within the project site. The first monument sign is proposed to be located on a decorative six-foot-tall wall within a landscaped area at the southwest corner of Iron Point Road and private driveway entrance into the Folsom Corporate Center. The second and third monument signs are proposed to be located on decorative six-foot-tall walls at their respective driveway entrances to Lot 1 and Lot 6. In terms of design, the monument signs will include individual letters made of metal with copy reading "Iron Point Apartment Homes". The monument signs, which are six-feet-tall and will include approximately 24 square feet of sign area each, will be indirectly illuminated. Staff has determined that the design of the proposed monument identification signs is complementary to the design of the proposed Folsom Corporate Center Apartments.

The Folsom Municipal Code (FMC, Section, 17.50.040 D) states that monument identification signs are an acceptable form of identification for multi-family residential projects. The Folsom Municipal Code also states that multi-family residential projects are permitted one freestanding sign that is a maximum of six-feet-tall with a maximum sign area of 32 square feet. Through the Planned Development Permit process, the applicant is seeking approval for three monument signs to provide identification for the proposed

project. Staff has determined that three monument signs are appropriate based on a number of factors including lack of direct access to the project site from Iron Point Road, the project having two distinct driveway entrances in different locations, and the large physical scale of the apartment community. Staff recommends that the owner/applicant obtain a sign permit prior to installation of the three monument signs. Condition No. 62 is included to reflect this requirement.

C. Traffic/Access/Circulation

Existing Roadway Network

General access to the Folsom Corporate Center and the project parcels is provided by three existing driveways located on the south side of Iron Point Road. The westerly driveway is restricted to vehicle right-turn in and right-turn out movements only. The central driveway, which is located at the signalized intersection of Iron Point Road and Rowberry Drive, allows all vehicle turning movements. The easterly driveway allows vehicle right-turn in, right-turn out, and left-turn in movements only.

Significant roadways in the project vicinity include Iron Point Road, Oak Avenue Parkway, Broadstone Parkway, and Rowberry Drive. Iron Point Road is an east-west arterial roadway with a raised median that runs from Folsom Boulevard to the eastern city limit along the north side of U.S. Highway 50. Within the vicinity of the project site, Iron Point Road (45 mph posted speed limit) has six lanes, bike lanes, sidewalk, curb, and gutter. Oak Avenue Parkway (45 mph posted speed limit) is a north-south arterial that extends from Willow Creek Drive to Iron Point Road. Oak Avenue Parkway is a four-lane urban arterial road between Willow Creek Drive and Blue Ravine Road, a six-lane urban arterial road between Blue Ravine Road and Riley Street, and a four-lane urban arterial road between Riley Street and Iron Point Road. Broadstone Parkway (45 mph posted speed limit) in the project vicinity is a four-lane east-west arterial, that wraps around the back of the Palladio at Broadstone Shopping Center from Iron Point Road to connect with Empire Ranch Road near the Sacramento-El Dorado County line. Rowberry Drive is a north-south two-lane local road that runs northward from the Kaiser Permanente Medical Offices into neighborhoods to the north of Iron Point Road. A future extension of Rowberry Drive across U.S. Highway 50 and into the Folsom Plan Area is planned for the future.

The traffic, access, and circulation analysis associated with the proposed project is based on the results of a Transportation Impact Study that was prepared in February 2022 by T. Kear Transportation Planning and Management, Inc. The transportation study analyzed traffic operations at the following 17 study intersections in the vicinity of the project site:

- Prairie City Road/U.S Highway 50 Eastbound Ramps
- Prairie City Road/U.S. Highway 50 Westbound Ramps
- Prairie City Road/American Aggregates Road

- Prairie City Road/Iron Point Road
- Iron Point Road /Grover Road
- Iron Point Road /Oak Avenue Parkway
- Iron Point Road /West Kaiser Access Road
- Iron Point Road /Rowberry Way
- Iron Point Road /Safe Credit Union Access
- Iron Point Road /Broadstone Parkway
- Iron Point Road /East Bidwell Street
- East Bidwell Street/U.S. Highway 50 Westbound Ramps
- East Bidwell Street/U.S. Highway 50 Eastbound Ramps
- APN 072-3120-023 "Lot 6" Access
- APN 072-3120-023 "Lot 1" Access
- Oak Avenue Parkway/U.S. Highway 50 Westbound Ramps (2035 Only)
- Oak Avenue Parkway/U.S. Highway 50 Eastbound Ramps (2035 Only)

Six different scenarios were evaluated in reviewing traffic operations at the 17 aforementioned study intersections including; Existing 2021 without Project Condition, Existing 2021 with Project Condition, Existing Plus Approved Projects (EPAP) 2026 without Project Condition, EPAP 2026 with Project Condition, Cumulative 2035 without Project Condition, and Cumulative 2035 with Project Condition.

The proposed Folsom Corporate Center Apartments project is expected to generate a total of 81 vehicle-trips during the weekday AM peak hour and 104 vehicle-trips during the weekday PM peak hour trips. Overall, the proposed project is projected to generate a total of 1,376 daily vehicle trips. Based on the projected volume of project-related vehicle trips, the Transportation Study concluded that the proposed project would not have a significant impact on vehicle level of service (LOS) at any of the 17 study intersections. In addition, the Transportation Study determined that the proposed project would not have a significant impact relative to Vehicle Miles Traveled (VMT).

While the Transportation Study determined that the proposed project would not have any significant impacts on study intersections relative to LOS and VMT, the Study did indicate that the project would result in a queueing deficiency (project would add 1 vehicle to a queue that already exceeds available storage) in the AM Peak Hour for the westbound left-turn lanes at the intersection of Prairie City Road and Iron Point Road under two different study scenarios (Existing 2021 Conditions with Project and EPAP 2026 Conditions with Project). To address this impact and reduce the vehicle queuing caused by the proposed project, the Transportation Study recommends the following measure (Condition No. 51) be implemented:

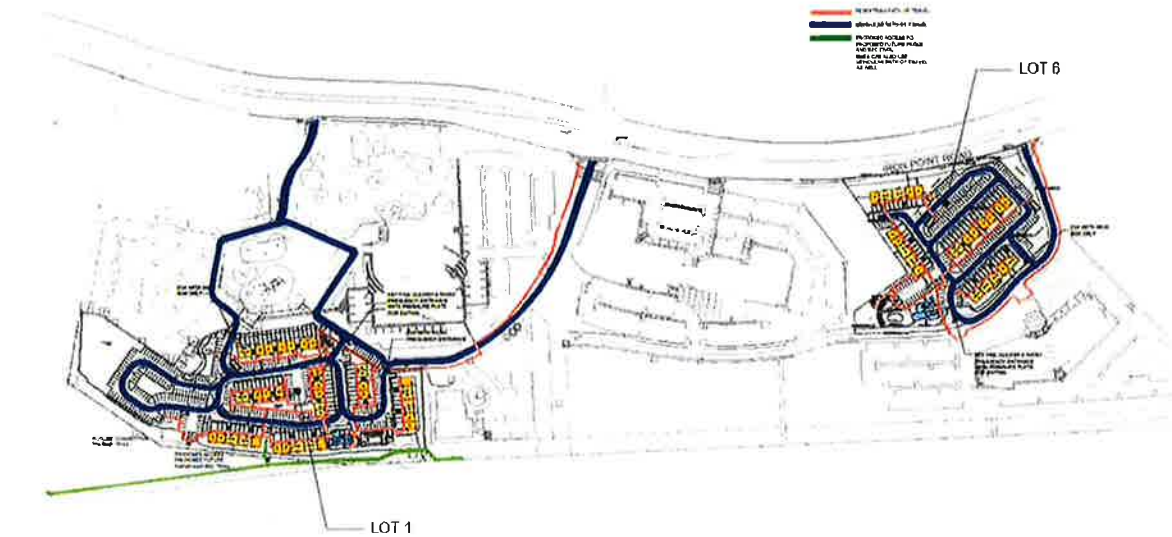
- The owner/applicant shall modify Prairie City Road/ Iron Point Road signal timing plan by shifting 1 second from the eastbound through movement to the westbound left turn movement, reduce the vehicle extension setting from adding five to six additional seconds to the green phase for through movements to adding four

seconds to the green phase for through movements for each vehicle passing the detector after the minimum green phase length has been exceeded. This mitigation measure shall be implemented by the City through the reimbursement agreement with the owner/applicant to cover any City costs. The implementation of this mitigation measure shall occur prior to issuance of the first building permit.

Project Access and On-Site Circulation

As shown on the submitted site plans (Attachments 8 and 9), access to the project area (Folsom Corporate Center) is provided by three existing driveways located on the south side of Iron Point Road. Primary vehicle access to Lot 1 is provided by a new driveway on south side of an existing private ring road with secondary access accommodated by two emergency vehicle access driveways also situated on the south side of the ring road. Primary vehicle access to Lot 6 is provided by a new driveway on the north side of the private ring road with secondary access served by an emergency vehicle access driveway also positioned on the north side of the ring road. Each of the project driveways will accommodate all vehicle turning movements into and out of the respective sites. In addition, all project driveways will have access controlled by a vehicle gate. Internal vehicle circulation is provided by 27-foot-wide drive aisles that accommodate movement in and around the project sites. Pedestrian circulation is provided by a combination of new sidewalks and existing sidewalks located along the private ring road and also along Iron Point Road. Internal pedestrian circulation is accommodated by a series of new pedestrian pathways that provide connectivity to the apartment buildings, the clubhouse building, and the perimeter sidewalks. Access and circulation exhibits for the proposed project are shown in the figures on the following pages.

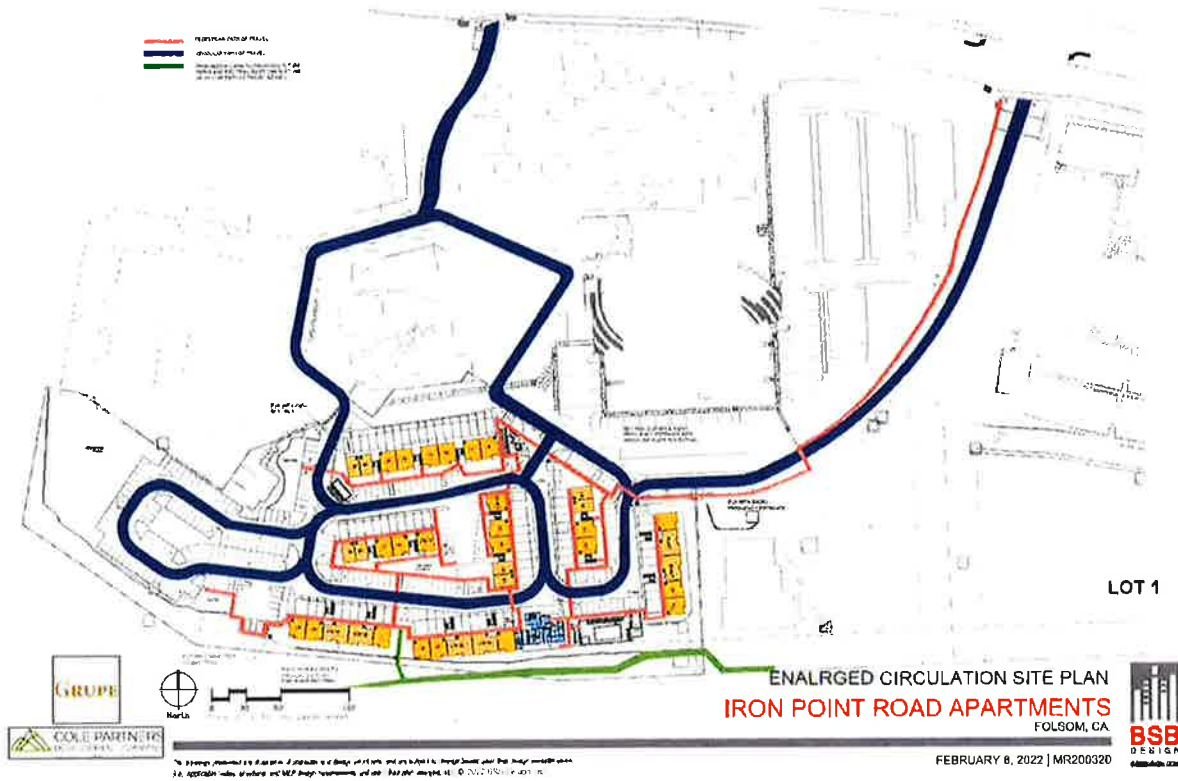
FIGURE 13: OVERALL ACCESS AND CIRCULATION EXHIBIT



Legend

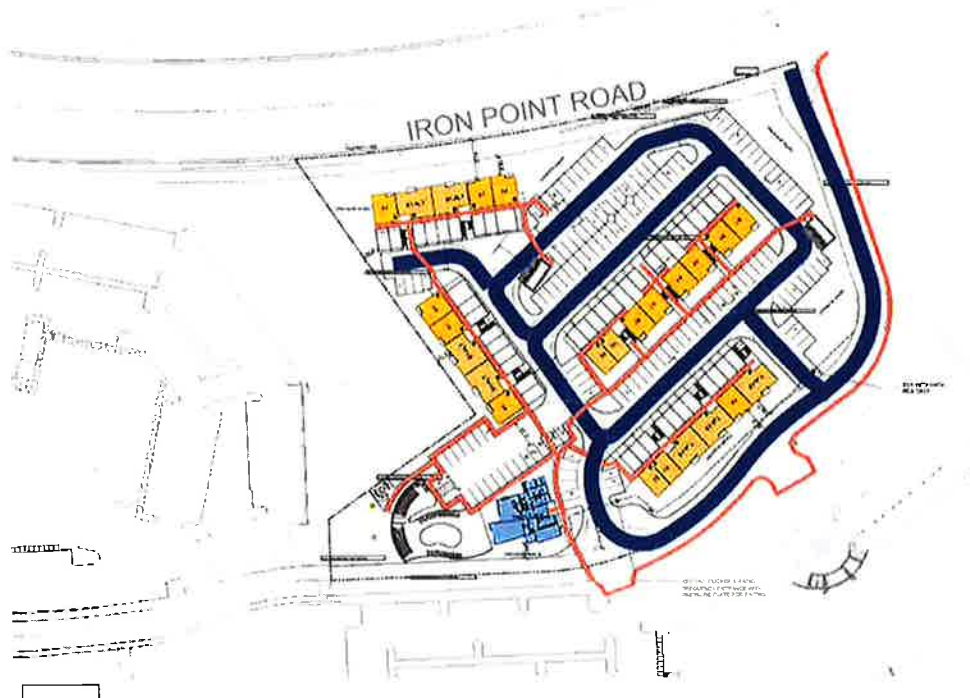
- Blue Line: Vehicle Access
- Red Line: Pedestrian Access
- Green Line: Future Trail and Connection

FIGURE 14: LOT 1 ACCESS AND CIRCULATION EXHIBIT



Legend
Blue Line: Vehicle Access
Red Line: Pedestrian Access
Green Line: Future Trail and Connection

FIGURE 15: LOT 6 ACCESS AND CIRCULATION EXHIBIT L 6



Legend

- Blue Line: Vehicle Access**
- Red Line: Pedestrian Access**

The Transportation Impact Study prepared for the proposed project evaluated the internal operation and configuration of the project access system in terms of right-turn deceleration lanes and tapers for driveways, minimum required driveway throat depth, emergency vehicle access, and entry gate queuing. As referenced previously within this report, the project parcels are accessed via private roadways within the Folsom Corporate Center. Access to City streets (Iron Point Road) is not being modified by the proposed project, thus the City's requirements for right-turn tapers and deceleration lanes are not applicable. Additionally, the Study determined that vehicle speeds and volumes within the Folsom Corporate Center's internal roadway network do not create a safety issue that would necessitate right-turn tapers and deceleration lanes at either of the internal project driveways.

As noted earlier, access to the two project parcels is provided by an existing private roadway network within the Folsom Corporate Center. As a result, the City's minimum required throat depth is not applicable. That being said, the Study determined that the design and throat depth of each of the proposed project driveways was acceptable and

would function appropriately. In terms of emergency vehicle access, there are three gated emergency vehicle access driveways proposed to serve the proposed project. In addition, the project's internal drive isles have 25-foot inner/50-foot outer minimum turning radii to accommodate all fire and police department access. Based on this information, the Study determined that adequate emergency vehicle access is being provided for the project.

Primary vehicle access to Lot 1 is provided by a new driveway on south side of an existing private ring road and primary vehicle access to Lot 6 is provided by a new driveway on the north side of the private ring road. Both of these project driveways will have access controlled by a vehicle gate. As shown on the submitted Individual Site Plans and Details (Attachment 9), the two project driveways have been designed to accommodate queuing of up to three vehicles for entry into the respective sites. The Study determined that the design of the two project driveways provides adequate queuing space for vehicles entering the project sites.

To ensure implementation of the traffic control and pedestrian circulation measures identified on the submitted site plans, staff recommends the following recommendations be included as conditions of approval for the project (Condition No. 52):

- A "stop" sign and appropriate pavement markings shall be installed at the internal approach to the private ring road at the two primary project driveways.
- The vehicle entry gates at the two primary project driveway locations shall open inward, away from the private ring road or retract sideways. In addition, the design of the vehicle entry gates and the vehicle entry gate area shall conform to all requirements established by the City of Folsom for gated multi-family residential developments.
- If vehicles are observed backing up into the private ring road at either of the two gated primary project entries, City staff will evaluate and require appropriate measures to alleviate the traffic congestion including but not limited to requiring the two project entry gates to remain open during the AM (7:00 a.m. to 9:00 a.m.) and PM (4:00 p.m. to 6:00 p.m.) peak hours on weekdays.
- Residents of the Folsom Corporate Center Apartments project shall be issued remote transmitters to allow them to open the entry gates without needing to stop to enter a code in the keypad at either entrance location.
- The owner/applicant shall provide at least one pedestrian connection from Lot 1 to the southern property boundary to allow for a connection to the future Class I bicycle trail expected to be located within the 50-foot-wide landscape easement between the project site and U.S. Highway 50.

Traffic Safety Committee

The proposed project was reviewed by the Traffic Safety Committee at its January 27, 2022 meeting. Upon a thorough review of the project's Site Plan and Access and Circulation Plan, the Committee made two recommendations relative to vehicle circulation and pedestrian circulation. With respect to vehicle circulation, the Committee recommended that the applicant evaluate implementing a traffic or right-of-way control solution (round-a-bout, stop-sign control, etc.) in the vicinity of the Lot 1 primary driveway and the two driveways across the private road on the Kaiser Permanente Medical Campus site due to the odd angles and configuration of this intersection. With regard to pedestrian circulation, the Committee recommended that the applicant evaluate providing improved pedestrian access between Lot 1 and Iron Point Road in the vicinity of the westernmost Kaiser Permanente project driveway.

Subsequent to the Traffic Safety Committee meeting, City staff met with the project applicant and the traffic consultant to discuss the two recommendations of the Committee. In relation to providing a traffic control solution near the primary entrance to Lot 1, the traffic consultant indicated that the volume of traffic at this location does not warrant the installation of a traffic control solution. In addition, it was determined that installation of any type of traffic control feature at this location would require off-site improvements on property that owned by the applicant. Based on this feedback, staff has determined that construction of traffic control feature near the Lot 1 driveway entrance is not necessary nor feasible.

In reviewing the possibility of providing improved pedestrian access between Lot 1 and Iron Point Road, City staff identified numerous challenges. Specifically, the construction of pedestrian pathway from Lot 1 to Iron Point Road near the westernmost Kaiser Permanente driveway would require a significant number of off-site improvements on property owned by Kaiser Permanente, not the applicant. In addition, construction of pedestrian walkways in this area would be extremely difficult due to the severe change in grade between Lot 1 and Iron Point Road. Lastly, the construction of a pedestrian walkway in this area would like required encroachment into a number of open space parcels containing Oak trees and sensitive habitat. Based on these factors, staff has determined that construction of new pedestrian pathways between Lot 1 and Iron Point Road is not feasible. Of note, Lot 1 in conjunction with the Dialysis Clinic (which is currently under construction) will construct additional sidewalk that would allow for pedestrian access to Iron Point along Rowberry and the eastern edge of the Kaiser Permanente property.

D. Parking

The Folsom Municipal Code (Section 17.18.110 Parking) requires 1.5 parking spaces per unit for multi-family structures and complexes located within the R-4 (General Apartment Zoning District) zoning district. The Design Guidelines for Multi-Family Development require that multi-family apartment developments provide 1.5 parking spaces for studio

and one-bedroom units, 1.75 parking spaces for two-bedroom units, 2.0 parking spaces for three-bedroom units, and 1 guest parking space for every 5 apartment units.

As noted in the Project Description, the proposed project includes a total of 253 apartment units including 16 studio units, 126 one-bedroom units, 97 two-bedroom units, and 14 three-bedroom units. As shown and described on the submitted site plan, the proposed project provides a total of 491 parking spaces including 120 integrated garage parking spaces, 133 carport covered parking spaces, and 238 uncovered surface parking spaces. Based on this parking information, Staff has determined that the proposed project meets the parking requirements established by the Folsom Municipal Code by providing 491 parking spaces whereas 379 parking spaces are required. In addition, staff has determined that the proposed project meets the parking recommendations of the Design Guidelines by providing 491 parking spaces whereas 462 parking spaces are recommended.

The Folsom Municipal Code (FMC, Section 17.57.090) requires multi-family residential developments to provide one bicycle parking space for every five dwelling units. The proposed project features 55 bicycle parking spaces including 31 bicycle storage room in the Lot 6 clubhouse building, 20 bicycle parking spaces in bicycle storage room in the Lot 1 clubhouse building, and 4 additional bicycle parking distributed throughout both project parcels. In addition to the dedicated bicycle storage facilities, bicycle parking opportunities are provided in each of the 120 integrated garages on the project site. Staff has determined that the proposed project meets the bicycle parking requirements established by the Folsom Municipal Code (FMC, Section 17.57.090) by providing 55 bicycle parking spaces whereas 51 bicycle parking spaces are required.

E. Noise Impacts

Based on the proximity of the project site to U.S. Highway 50, Iron Point Road, and existing commercial land uses within the immediate project vicinity, acoustical measurements and modeling were preliminarily prepared by Bollard Acoustical on May 3, 2021 and bolstered by Helix Environmental Planning on February 23, 2022 to analyze potential noise impacts at the proposed Folsom Corporate Center Apartments project site. The purpose of the noise analysis was to quantify existing noise levels associated with traffic on U.S. Highway 50 and Iron Point Road, and to compare those noise levels against the applicable City of Folsom noise standards for acceptable noise exposure at the project site. In addition, noise generated by the proposed project including construction activities, on-site parking/circulation, and mechanical equipment noise, was also evaluated in the noise analysis.

Two aspects of noise impacts were evaluated relative to the proposed apartment project, noise directed at the proposed project, and noise caused by the proposed project. As noted previously, the predominant existing noise sources in the project vicinity that cause

an impact to the project site are from vehicles traveling on U.S. Highway 50 and Iron Point Road, as well as background noises from adjacent nearby commercial land uses. Potential noise impacts that might result from development of the Folsom Corporate Center Apartments project community are construction-related activities and operational activities. Construction-related noise would have a short-term effect, while operational noise would continue throughout the lifetime of the project.

The Noise Element of the City of Folsom General Plan regulates noise emissions from public roadway traffic on new development of residential or other noise sensitive land uses. The Noise Element states that noise from traffic on public roadways shall not exceed 65 CNEL for outdoor use areas and 45 CNEL for interior use areas. To evaluate such potential noise impacts to the proposed project, Bollard Acoustical conducted ambient noise measurements to calibrate the predictive noise modeling program that estimates noise levels based on estimated future traffic noise affecting the project site.

As stated above, a significant direct noise impact would occur if traffic-related noise levels exceed 65 CNEL at the proposed project's designated outdoor use areas (outdoor pool/amenity areas). The noise modeling program determined that the outdoor noise level at the clubhouse area on Lot 1 would be 65 CNEL, while the outdoor noise level at the clubhouse area on Lot 6 would be 63 CNEL. Based on these projected noise levels at the project two exterior use areas, staff has determined that the proposed project would comply with the City's exterior noise threshold.

As referenced above, a significant direct noise impact would also occur if the project's interior use areas would be exposed to noise levels greater than 45 CNEL from roadway traffic. A 45 CNEL interior limit would be achieved if exterior locations are exposed to a noise level of 60 CNEL or less, based on a typical attenuation of 15-20 dB by standard residential building construction. The noise modeling program determined that three buildings on Lot 1 (Buildings 1, 2, and 7) and two buildings on Lot 6 (Buildings 2 and 5) would potentially exceed the City's interior noise level standard of 45 CNEL. To reduce these potential noise impacts to a less than significant level and comply with the City's interior noise level standards, staff recommends that the following measures be implemented (Condition No. 56).

- For habitable areas (both living rooms and bedrooms) with a direct line-of-sight to U.S. Highway 50 for Lot 1 and Iron Point Road for Lot 6, the following measures shall be incorporated in the design of the project to reduce interior noise levels to 45 CNEL or less:
 - Lot 1 (Buildings 1 and 2) and Lot 6 (Building 2) – Minimum exterior wall requirement of STC 46.
 - Lot 1 (Buildings 1 and 2) and Lot 6 (Building 2) – Minimum window and glass sliding door requirement of STC 35.

- Lot 1 (Building 7) and Lot 6 (Building 5) – Minimum window and glass sliding door requirement of STC 28.
- The building design shall include a mechanical ventilation system that meets the criteria of the International Building Code (Chapter 12, §1203.3 of the 2013 California Building Code) to ensure that windows would be able to remain permanently closed.

Construction of the Folsom Corporate Center Apartments project would temporarily increase noise levels in the project vicinity during the construction period, which would take approximately 20 to 26 months. Construction activities, including site clearing, excavation, grading, building construction, and paving, would be considered an intermittent noise impact throughout the construction period of the project. The City's Noise Ordinance excludes construction activities from meeting the General Plan Noise Element standards, provided that all phases of construction are limited to the hours between 7:00 a.m. and 6:00 p.m. on weekdays, and between 8:00 a.m. and 5:00 p.m. on Saturdays. To ensure compliance with the City's Noise Control Ordinance and General Plan Noise Element, staff recommends that hours of construction operation be limited from 7:00 a.m. to 6:00 p.m. on weekdays and 8:00 a.m. to 5:00 p.m. on Saturdays with no construction permitted on Sundays or holidays. In addition, staff recommends that construction equipment be muffled and shrouded to minimize noise levels. Condition No. 55 is included to reflect these requirements.

Operational noises generated by the proposed project include sounds associated with new vehicle trips, vehicle parking, and mechanical equipment associated with the apartment project. Persons and activities potentially sensitive to noise in the project vicinity include residents within the Broadstone Unit No. 2 Subdivision (150 feet north across Iron Point Road) across Iron Point Road to the north of the project site, residents within the Sherwood Apartments (approximately 450 northeast of the project site across Iron Point Road), and residents of the Revel Senior Living Apartments (approximately 500 feet to the west). Due to the limited volume of project-generated vehicle trips (81 weekday AM peak hour trips and 104 weekday PM peak hour trips), vehicle noise exposure would increase only slightly as compared to existing conditions in the project vicinity. Based on the significant distance and buffers between the project site and the nearby residential land uses, staff has determined that potential noise impacts relative to these operational noise sources will not be significant.

F. Walls/Fencing

The proposed project includes the construction of retaining walls and fencing. As shown on the submitted Grading and Drainage Plans (Attachment 11), retaining walls that predominantly range from 1-8 feet in height, with a maximum height of 15 ft at Lot 6 at the northeast corner. The walls are proposed to be constructed in various locations on Lot 1 and Lot 6 due to substantial changes in elevation on the sites. As shown the submitted Landscape Plan and Details (Attachment 12), decorative six-foot-tall metal

open view fencing is proposed to be placed around the perimeter of Lots 1 and 6. In addition to the perimeter fencing, vehicle gates and pedestrian gates are also proposed at various locations on the Lots 1 and 6. Staff recommends that the final location, design, height, materials, and colors of the retaining walls, fences, and gates be subject to review and approval by the Community Development Department. Condition No. 59 is included to reflect this requirement.

G. Site Lighting

As shown on the Preliminary Lighting Plan (Attachment 14), the applicant is proposing to use a combination of pole-mounted parking lot lighting, carport lighting, building-attached lighting, and bollard lights along the walkways on the project site. All lighting would be designed to minimize light/glare impacts to the adjacent properties by ensuring that all exterior lighting is shielded and directed downward. Staff recommends that the final exterior building and site lighting plans be submitted for review and approval by Community Development Department for location, height, aesthetics, level of illumination, glare and trespass prior to the issuance of any building permits. In addition, staff recommends all lighting is designed to be shielded and directed downward onto the project site and away from adjacent properties and public rights-of-way. Condition No. 23 is included to reflect these requirements.

H. Trash/Recycling

The proposed project includes three trash/recycling enclosures to manage trash, recycling, and organics associated with the apartment community. Lot 1 includes one trash/recycling enclosure and one trash compactor, while Lot 6 includes two trash/recycling enclosures. The proposed trash/recycling enclosures, which are constructed of textured concrete masonry blocks with a decorative trim cap, feature metal gates to control access. Staff recommends that the final location, design, materials, and colors of the trash/recycling enclosures be subject to review and approval by the Community Development Department. Condition No. 58 is included to reflect these requirements.

I. Existing and Proposed Landscaping

Lot 1, which is largely undisturbed, is predominantly comprised of non-native annual grassland with a single Oak tree situated in the southeast corner of the site. Lot 1 does include small parking lot area with associated landscaping in the northwest corner of the project site. This small parking lot and landscaped area, which is associated with the adjacent Kaiser Permanente Medical Office Complex, is proposed to remain in place. A 50-foot-wide landscape easement, which is located between the southern boundary of Lot 1 and U.S. Highway 50, is steeply sloped and contains non-native grasses. Lot 6, which has been greatly disturbed by prior grading and stockpiling activities, features non-native grasses with a small stand of Oak trees located in the southwest corner of the site.

A 20-foot-wide landscape easement, which is located within the northern portion of Lot 6 adjacent to Iron Point Road, features a rockery retaining wall and sidewalk with minimal landscaping and non-native grasses.

As shown on the Preliminary Landscape Plans (Attachment 12), the applicant is proposing to install landscaping that features California-native and low water-use trees, shrubs, and groundcover selections intended to comply with the requirements of the Model Water Efficiency Landscape Ordinance (MWELo). Proposed landscape improvements include a variety of drought-tolerant trees, shrubs, and groundcover. Among the proposed trees are; Chinese Pistache, Coast Live Oak, Dwarf Strawberry Tree, Interior Live Oak, Red Crape Myrtle, Redpointe Maple, Sweet Bay, and Swan Hill Olive. Proposed shrubs and groundcover include; Australian Bluebell Creeper, Autumn Sage, Deer Grass, Dwarf Bottlebrush, Dwarf Hawthorne, Heavenly Bamboo, Manzanita, Red Fountain Grass, and Biofiltration Sod. The preliminary landscape plan meets the CALgreen and City shade requirement by providing 50 percent shade in the parking lot area within fifteen years. Staff recommends that the final landscape plans be reviewed and approved by the Community Development Department. Condition No. 36 is included to reflect this requirement.

Oak Tree Preservation and Removal

Chapter 12.16 of the Folsom Municipal Code, the Tree Preservation Ordinance, regulates the cutting or modification of trees, including oaks and specified other trees; requires a Tree Permit prior to cutting or modification; and establishes mitigation requirements for cut or damaged trees. The Tree Preservation Ordinance establishes policies, regulations, and standards necessary to ensure that the City will continue to preserve and maintain its "urban forests".

An Arborist Report and Arborist Inventory prepared for the proposed project found that the project parcels contain a total of 11 protected native oak trees (oak trees measuring six inches in diameter or larger) including nine Blue Oaks and two Valley Oaks. Of the 11 oak trees mentioned above, one Blue Oak tree located on Lot 6 is recommended for removal due to compromised health and structural defects. The remaining ten native Oak trees, which are located on Lot 6, are identified as being in fair to good condition by the Arborist Report.

As shown on the submitted Landscape Plan, the applicant is proposing to preserve three Oak trees on the project site including a 41" diameter Blue Oak tree (Heritage Tree) on Lot 1 and two Blue Oak trees (30" and 26" in diameter respectively) on Lot 6. The remaining eight oak trees on the project site (southwest corner of Lot 6) are proposed to be removed to allow for development of the proposed project. To offset the loss of the protected native oak trees, the applicant is proposing to plant 35 Mitigation Oak trees (Coast Live Oak and Interior Live Oak) in appropriate locations (through consultation with the City Arborist) on the project site and to pay in-lieu fees for any outstanding Oak tree

mitigation that is required. To mitigate the impact to the protected native Oak trees, staff recommends that the following measures be implemented (Condition No. 37) in accordance with requirements of the Tree Preservation Ordinance:

- A Tree Permit Application containing an Application Form, Tree Protection and Mitigation Plan, and Arborist Report shall be submitted to the City of Folsom by the owner/applicant for issuance of a Tree Work Permit and Tree Removal Permit prior to commencement of any grading or site improvement activities. The tree protection and mitigation plan shall be prepared in collaboration with a qualified arborist and shall be subject to review and approval by the City. The tree protection and mitigation plan shall contain the contact information of the project arborist and shall be included in all associated plan sets for the project.
- Removal of any protected tree shall be mitigated by planting replacement trees and/or payment of "In-Lieu" fees on a diameter inch basis in accordance with FMC, Section 12.16.150. The proposed method of mitigation shall be subject to review and approval by the City.
- Prior to starting construction, oak trees to be preserved shall be fenced with high visibility fencing consistent with the city-approved tree protection and mitigation plan. Parking of vehicles, equipment, or storage of materials is prohibited within the Tree Protection Zone of Protected Trees at all times. Signs shall be posted on exclusion fencing stating that the enclosed trees are to be preserved. Signs shall state the penalty for damage to, or removal of, the protected tree.
- The owner/applicant shall retain the services of a project arborist for the duration of the development project to monitor the health of oak trees to be preserved and carry out the City-approved tree protection plan. All regulated activity conducted within the Critical Root Zone of protected trees, as that term is defined in Folsom Municipal Code (FMC) 12.16.020, shall be performed under the direct supervision of the project arborist. A copy of the executed contract for these arboricultural services shall be submitted to the City prior to the issuance of any tree or grading permits
- Certification letters by the project arborist attesting compliance with the tree protection and mitigation plan and tree permit conditions shall be submitted to the City at the following stages of the project:
- The owner/applicant shall plant 35 Mitigation Oak Trees on the project site in the locations as shown on the Preliminary Landscape Plans. The final number, location, and type of Mitigation Oak Trees shall be subject to review and approval by the Community Development Department. The owner/applicant shall pay in-lieu fees for any outstanding required Oak Tree Mitigation that is not satisfied through planting of Mitigation Oak Trees.

J. Conformance with Relevant General Plan Goals and Policies

The City of Folsom General Plan (2035) outlines a number of goals, policies, and implementation programs designed to guide the physical, economic, and environmental growth of the City. Staff has determined that the proposed project is consistent with the General Plan goals and policies as outlined and discussed below:

APPLICABLE GENERAL PLAN GOALS AND POLICIES

GP GOAL LU 1.1 (Land Use/Growth and Change)

Retain and enhance Folsom's quality of life, unique identity, and sense of community while continuing to grow and change.

GP POLICY LU 1.1.12-1 (Infill Development)

Respect the local context: New development should improve the character and connectivity of the neighborhood in which it occurs. Physical design should respond to the scale and features of the surrounding community, while improving critical elements such as transparency and permeability.

The proposed project is consistent with this policy in that the project features significant site and design improvements which will enhance the overall character of the area including introducing new market rate apartment units with a contemporary modern residential design intended to complement the architecture and design of existing residential and commercial buildings in the project vicinity.

GP POLICY LU 1.1.15 (SACOG Blueprint Principles)

Strive to adhere to the Sacramento Regional Blueprint Growth Principles.

The proposed project is consistent with this policy in that the project has been designed to adhere to the primary SACOG Blueprint Principles including Compact Development, Housing Choice and Diversity, Use of Existing Assets, and Quality Design. Compact Development involves creating environments that are more compactly built and use space in an efficient but attractive manner to encourage more walking, biking, and transit use and shorter auto trips. Housing Choice and Diversity includes providing a variety of places where people can live (apartments, townhomes, condominiums, and single-family detached homes) and also creating opportunities for the variety of people who need them such as families, singles, seniors, and people with special needs. Use of Existing Assets entails intensification of the existing use or redevelopment in order to make better use of existing public infrastructure, including roads. Quality Design focuses on the design details of any land development (such as relationship to the street, placement of buildings, sidewalks, street widths, landscaping, etc.), which are all factors that influence the attractiveness of living in a compact development and facilitate the ease of walking within and in and out of a community.

APPLICABLE GENERAL PLAN GOALS AND POLICIES

GP GOAL LU 6.1 (Residential Neighborhoods)

Allow for a variety of housing types and mix of uses that provide choices for Folsom residents, create complete and livable neighborhoods, and encourage walking and biking.

GP POLICY LU 6.1.3 (Efficiency through Density)

Support an overall increase in average residential densities in identified urban centers and mixed-use districts. Encourage new housing types to shift from lower-density, large-lot developments to higher-density, small-lot and multifamily developments, as a means to increase energy efficiency, conserve water, reduce waste, as well as increase access to services and amenities (e.g., open space) through an emphasis on mixed uses in these higher-density developments.

The proposed project is consistent with this policy in that the project is a new market-rate multi-family residential project developed at a residential density of 21.2 units per acre. Its location within Folsom Corporate Center and proximity to the Folsom Gateway retail center will create a compact/horizontal mixed-use development. The proposed project design will be consistent with California Green Building Standards Code (CALGreen), and the residential units are being designed to be all-electric, and the project intends to participate the SMUD SolarShares program. In addition, the proposed project includes electric vehicle charging stations, and will meet or exceed the percentage of electric vehicle capable parking spaces per CALGreen code.

GP GOAL M 4.1 (Vehicle Traffic and Parking)

Ensure a safe and efficient network of streets for cars and trucks, as well as provide an adequate supply of vehicle parking.

GP POLICY M 4.1.3 (Level of Service)

Strive to achieve a least traffic Level of Service "D" (or better) for local streets and roadways throughout the City. In designing transportation improvements, the City will prioritize use of smart technologies and innovative solutions that maximize efficiencies and safety while minimizing the physical footprint. During the course of Plan buildout, it may occur that temporarily higher Levels of Service result where roadway improvements have not been adequately phased as development proceeds. However, this situation will be minimized based on annual traffic studies and monitoring programs. Staff will report to the City Council at regular intervals via the Capital improvement Program process for the Council to prioritize projects integral to achieving Level of Service D or better.

The proposed project is consistent with this policy in that the project will not result in a change in the level of service (LOS) at any of the 17 study intersections. In addition, the proposed project is anticipated to generate less than 82% of the regional per capita Vehicle Miles Traveled (VMT), consistent with new State Law that took effect July 1, 2020 (SB 743).

GP GOAL M 4.2 (Vehicle Traffic and Parking)

Provide and manage a balanced approach to parking that meets economic development and sustainability goals.

GP POLICY M 4.2.4 (Electric Vehicle Charging Stations)

Encourage the installation of electric vehicle charging stations in parking spaces throughout the city, prioritizing installations at multi-family residential units.

The proposed project is consistent with this policy in that the project includes five electric vehicle charging stations to serve electric vehicles of residents and guests. In addition, the applicant has committed to having at least 10 percent of parking spaces be EV Capable. The number of proposed electric vehicle charging stations (5) and percentage of EV Capable parking spaces is consistent with the California Green Buildings Standards Code's provisions (10 percent of all parking spaces) required to be EV Capable) for multi-family residential development.

GP GOAL H-2 (Removing Barriers to the Production of Housing)

To minimize governmental constraints on the development of housing for households of all income levels.

GP POLICY H 2.7

The City shall educate the community on the needs, the realities and the benefits of affordable and high-density housing.

The proposed project is consistent with this policy in that the project will result in development of a high-density market-rate apartment community on parcels that are not currently zoned for multi-family high density residential development.

K. Native American Consultation (SB 18/AB52)

Senate Bill (SB) 18 was signed into law in September 2004 and became effective in March 2005. SB 18 requires city and county governments to consult with California Native American tribes early in the planning process with the intent of protecting traditional tribal cultural places. In accordance with Government Code 65352.3(a)(2), the City sent project notifications to each of listed tribes on October 26, 2021 and afforded them 90 days to respond and request consultation. The City received a response from one tribe (UAIC-United Auburn Indian Community) who expressed a desire to consult regarding the proposed project. During the consultation process, the City provided UAIC with a Cultural Resources Assessment document that indicated there are no known Tribal Cultural Resources present on the project site. Subsequently, UAIC submitted information to the City that stated that heritage trees, in general, are an important Tribal Cultural Resource. The City responded to UAIC that there is one Heritage Oak Tree on the project site (41" diameter Oak tree on Lot 1) that is intended to be preserved. City staff also responded to UAIC that a mitigation measure (Condition No. 39) will be placed on the project to protect any unanticipated discovery of Tribal Cultural Resources on the project site.

On March 9, 2022, and in accordance with Government Code §65352(a)(11), the City mailed the 45-day referral notices to the listed tribes. No tribes provided comment within that timeframe. The City will mail specific details of the pending City Council public hearing to listed tribes at least 10 days in advance of the meeting, in accordance with Government Code §65092. In summary, the City has assumed and concluded consultation responsibilities in accordance with the *Tribal Consultation Guidelines: Supplement to General Plan Guidelines* (November 14, 2005) published by the Governor's Office of Planning and Research.

Assembly Bill (AB 52), which was signed into law in July 2015, requires City or County Governments to consult with California Native American Tribes in order to identify Tribal Cultural Resources that may be significantly impacted by development projects and to avoid or mitigate those impacts. On September 21, 2021, the City sent project notification letters to the three California Native American tribes named on the City's AB 52 contact list, with the United Auburn Indian Community (UAIC) being the only tribe to respond. The City subsequently initiated consultation with UAIC concurrently with respect to AB 52 and SB 18 as the issues raised by UAIC under these two sets of State regulations were identical. On February 4, 2022, the City concluded the consultation with UAIC with the acknowledgement that measures would be included with the project to ensure protection of the Heritage Oak Tree on Lot 1 and the protection of previously unknown Tribal Cultural Resources on the project site during construction activities.

ENVIRONMENTAL REVIEW

Staff has prepared an Initial Study, Mitigated Negative Declaration, and Mitigation Monitoring and Reporting Program (Attachment 23) for the project in accordance with the California Environmental Quality Act (CEQA) and associated regulations and determined that with the proposed mitigations, the project will not have a significant effect on the environment. The Mitigated Negative Declaration has been prepared and noticed for public comment, and mitigation measures have been included as Conditions of Approval. To date, one written comment has been received from the public during the Mitigated Negative Declaration public review period (March 8, 2022 to April 6, 2022).

On March 24, 2022, the Sacramento Metropolitan Air Quality Management District (SMAQMD) submitted a response letter (Attachment 23) regarding the Initial Study and Mitigated Negative Declaration that was prepared for the proposed project. In the response letter, SMAQMD recommends that additional measures be implemented to protect residents from exposure to toxic air contaminant emissions produced by vehicles traveling on U.S. Highway 50. Specifically, SMAQMD recommends that a continuous landscape buffer or dense landscape plantings be provided along the southern, western, and eastern edges of the project site consistent with the Air District's *Landscaping Guidance for Improving Air Quality Near Roadways*. As shown on the submitted Preliminary Landscaped Plans (Attachment 12), the project includes a robust amount of landscaping along the perimeter of the site (Lot 1) adjacent to U.S. Highway

50. However, to further reduce residents' exposure to air contaminant emissions, staff recommends additional landscape plantings be provided where feasible along the southern, western, and eastern perimeter of Lot 1 to the satisfaction of the Community Development Department. Condition No. 45 is included to reflect this requirement. It is important to note that each of the apartment buildings will have a mechanical ventilation system that accommodates air filters with a minimum efficiency rating to reduce residents' exposure to air contaminant emissions.

In their letter, SMAQMD also recommends that the proposed project consider implementing additional energy related measures to help reduce the urban heat island effect. Specifically, SMAQMD recommends that certified cool roofs be installed on all of the apartment buildings and that solar photovoltaic shade structures be placed over the parking spaces in the area under the overhead power lines in the western portion of Lot 1. The applicant has indicated that they will be installing certified cool roofs on all of the apartment buildings consistent with CALgreen code requirements. Unfortunately, the placement of solar photovoltaic shade structures over parking spaces in the power line easement area is not feasible due to the fact that these types of structures are permitted by the responsible utility agencies (PG&E and SMUD). However, it is important to reiterate that the applicant intends to participate the SMUD SolarShares program

RECOMMENDATION/PLANNING COMMISSION ACTION

City staff recommends that the Planning Commission recommend City Council approval of this project, subject to the findings and conditions of approval attached to this report.

PLANNING COMMISSION ACTION:

- Adopt the Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program prepared for the Folsom Corporate Center Apartments project (PN 21-120) per Attachment 23; and
- Approve a General Plan Amendment to change the General Plan land use designation for Lot 1 (APN No. 072-3120-023) and Lot 6 (APN No. 072-3120-023) from IND (Industrial/Office Park) to MHD (Multi-Family High Density); and
- Approve a Rezone to change the zoning designation for Lot 1 (APN No. 072-3120-026) from M-L PD (Limited Manufacturing, Planned Development District) to R-4 PD (General Apartment, Planned Development District) and to change the zoning designation of Lot 6 (APN No. 072-3120-023) from BP PD (Business and Professional, Planned Development District) to R-4 PD (General Apartment, Planned Development District); and
- Approve a Planned Development Permit to establish detailed development and architectural standards for the 253-unit Folsom Corporate Center Apartments project.

These recommended approvals are subject to the proposed findings below (Findings A-U) and the conditions of approval (Conditions 1-69) attached to this report.

GENERAL FINDINGS

- A. NOTICE OF HEARING HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE PROJECT IS CONSISTENT WITH THE GENERAL PLAN, THE ZONING CODE OF THE CITY, AND THE FOLSOM CORPORATE CENTER PLANNED DEVELOPMENT GUIDELINES AS AMENDED.

CEQA FINDINGS

- C. A MITIGATED NEGATIVE DECLARATION HAS BEEN PREPARED FOR THE PROJECT IN ACCORDANCE WITH CEQA.
- D. THE PLANNING COMMISSION HAS CONSIDERED THE PROPOSED MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING AND REPORTING PROGRAM BEFORE MAKING A DECISION REGARDING THE PROJECT.
- E. ON THE BASIS OF THE WHOLE RECORD BEFORE THE PLANNING COMMISSION, THERE IS NO SUBSTANTIAL EVIDENCE THAT THE PROJECT, AS CONDITIONED, WILL HAVE A SIGNIFICANT EFFECT ON THE ENVIRONMENT.
- F. THE MITIGATED NEGATIVE DECLARATION REFLECTS THE INDEPENDENT JUDGMENT AND ANALYSIS OF THE CITY OF FOLSOM.
- G. THE MITIGATED NEGATIVE DECLARATION HAS DETERMINED THAT THE PROPOSED PROJECT, AS CONDITIONED AND CONSISTENT WITH THE REQUIRED MITIGATION MONITORING AND REPORTING PROGRAM, WOULD NOT HAVE A SIGNIFICANT EFFECT ON THE ENVIRONMENT WITH MITIGATION MEASURES.
- H. THE LOCATION AND CUSTODIAN OF THE DOCUMENTS WHICH CONSTITUTE THE RECORD OF PROCEEDINGS UPON WHICH THE DECISION IS BASED ARE: CITY OF FOLSOM COMMUNITY DEVELOPMENT DEPARTMENT, 50 NATOMA STREET, FOLSOM, CA 95630.

GENERAL PLAN AMENDMENT FINDINGS

- I. THE PROPOSED GENERAL PLAN AMENDMENT IS CONSISTENT WITH THE GOALS, POLICIES AND OBJECTIVES OF THE CITY OF FOLSOM GENERAL PLAN
- J. THE PROPOSED GENERAL PLAN AMENDMENT IS CONSISTENT WITH THE OBJECTIVES OF THE LAND USE ELEMENT OF THE CITY'S GENERAL PLAN AND DEVELOPMENT POLICIES.
- K. THE PROPOSED GENERAL PLAN AMENDMENT WILL NOT RESULT IN A NET LOSS OF RESIDENTIAL CAPACITY.
- L. THE PROPOSED GENERAL PLAN AMENDMENT IS IN THE PUBLIC INTEREST.
- M. PURSUANT TO GOVERNMENT CODE SECTION 65352.3, THE CITY CONTACTED ALL CALIFORNIA NATIVE AMERICAN TRIBES ON THE CONTACT LIST MAINTAINED BY THE NATIVE AMERICAN HERITAGE COMMISSION IN ASSOCIATION WITH THIS PROJECT. THE CITY RECEIVED ONE REQUEST FOR CONSULTATION FROM A NATIVE AMERICAN TRIBE, INITIATED CONSULTATION, AND SUBSEQUENTLY CONCLUDED CONSULTATION ON FEBRUARY 4, 2022

REZONE FINDING

- N. THE PROJECT IS CONSISTENT WITH THE CITY'S GENERAL PLAN, THE FOLSOM MUNICIPAL CODE, AND THE FOLSOM CORPORATE CENTER PLANNED DEVELOPMENT GUIDELINES AS AMENDED.

PLANNED DEVELOPMENT PERMIT FINDINGS

- O. THE PROPOSED PROJECT COMPLIES WITH THE INTENT AND PURPOSES OF CHAPTER 17.38 (PLANNED DEVELOPMENT DISTRICT) OF THE FOLSOM MUNICIPAL CODE AND OTHER APPLICABLE ORDINANCES OF THE CITY.
- P. THE PROPOSED PROJECT IS CONSISTENT WITH THE OBJECTIVES, POLICIES AND REQUIREMENTS OF THE DEVELOPMENT STANDARDS OF THE CITY.
- Q. THE PHYSICAL, FUNCTIONAL AND VISUAL COMPATIBILITY BETWEEN THE PROPOSED PROJECT AND EXISTING AND FUTURE ADJACENT USES AND AREA CHARACTERISTICS IS ACCEPTABLE.

- R. THERE ARE AVAILABLE PUBLIC FACILITIES, INCLUDING BUT NOT LIMITED TO, WATER, SEWER AND DRAINAGE TO ALLOW FOR THE DEVELOPMENT OF THE PROJECT SITE IN A MANNER CONSISTENT WITH THIS PROPOSAL.
- S. THE PROPOSED PROJECT WILL NOT CAUSE UNACCEPTABLE VEHICULAR TRAFFIC LEVELS ON SURROUNDING ROADWAYS, AND THE PROPOSED PROJECT WILL PROVIDE ADEQUATE INTERNAL CIRCULATION.
- T. THE PROPOSED PROJECT WILL NOT BE DETRIMENTAL TO THE HEALTH, SAFETY AND GENERAL WELFARE OF THE PERSONS OR PROPERTY WITHIN THE VICINITY OF THE PROJECT SITE, AND THE CITY AS A WHOLE.
- U. ADEQUATE PROVISION IS MADE FOR THE FURNISHING OF SANITATION SERVICES AND EMERGENCY PUBLIC SAFETY SERVICES TO THE PROJECT.

Attachment No. 4

Minutes from April 6, 2022 Planning Commission Meeting



FOLSOM
CITY OF FOLSOM

PLANNING COMMISSION MINUTES
April 6, 2022
CITY COUNCIL CHAMBERS
6:30 P.M.
50 Natoma Street
Folsom, CA 95630

CALL TO ORDER PLANNING COMMISSION: Bill Miklos, Ralph Peña, Barbara Leary, Eileen Reynolds, Daniel West, Bill Romanelli, Justin Raithel

ABSENT: Justin Raithel, Ralph Peña

PLEDGE OF ALLEGIANCE

CITIZEN COMMUNICATION: None

MINUTES: The minutes of the December 15, 2021 meeting were approved as submitted.

Oath of Office Administered to Bill Romanelli

Election of Chair and Vice Chair

COMMISSIONER MIKLOS MOVED TO RECOMMEND EILEEN REYNOLDS TO SERVE AS CHAIR AND DAN WEST TO SERVE AS VICE CHAIR FOR THE PLANNING COMMISSION

COMMISSIONER LEARY SECONDED THE MOTION WHICH CARRIED THE FOLLOWING VOTE:

AYES: MIKLOS, LEARY, REYNOLDS, WEST, ROMANELLI

NOES: NONE

RECUSED: NONE

ABSENT: PEÑA, RAITHEL

PRESENTATIONS

1. Draft Active Transportation Plan (Brett Bollinger, Parks and Recreation Department)

PUBLIC HEARING**2. PN 21-120, Folsom Corporate Center Apartments General Plan Amendment, Rezone and Planned Development and Determination that the Project is Exempt from CEQA**

A Public Hearing for approval of a General Plan Amendment, Rezone, and Planned Development Permit for the Folsom Corporate Center Apartments project. The proposed project includes development of a 253-unit market-rate apartment community on two sites (Lot 1: 7.24-acre parcel and Lot 6: 4.68-acre parcel) within the Folsom Corporate Center, which is located on the south side of Iron Point Road, slightly east of the intersection of Iron Point Road and Oak Avenue Parkway. A General Plan Amendment to change the General Plan land use designation for the two project parcels (Lot 1 and Lot 6) from IND (Industrial/Office Park) to MHD (Multi-Family High Density) and A Rezone to change the zoning designation for Lot 1 from M-L PD (Limited Manufacturing, Planned Development District) to R-4 PD (General Apartment, Planned Development District) and to change the zoning designation of Lot 6 from BP PD (Business and Professional, Planned Development District) to R-4 PD (General Apartment, Planned Development District). This project is exempt from the California Environmental Quality Act in accordance with Section 15315 of the CEQA Guidelines. (Principal Planner: Steve Banks / Applicant: Cole Partners

COMMISSIONER WEST MOVED TO RECOMMEND CITY COUNCIL APPROVAL OF THIS PROJECT, SUBJECT TO THE PROPOSED FINDINGS (FINDINGS A – U) AND CONDITIONS OF APPROVAL ATTACHED TO THIS REPORT (CONDITIONS 1-89). SPECIFICALLY, COMMISSIONER WEST MOVED TO RECOMMEND THAT CITY COUNCIL TAKE THE FOLLOWING ACTIONS TO APPROVE THIS PROJECT:

- ADOPT THE MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING AND REPORTING PROGRAM PREPARED FOR THE FOLSOM CORPORATE CENTER APARTMENTS PROJECT (PN 21-120) PER ATTACHMENT 23; AND
- APPROVE A GENERAL PLAN AMENDMENT TO CHANGE THE GENERAL PLAN LAND USE DESIGNATION FOR LOT 1 (APN NO. 072-3120-023) AND LOT 6 (APN NO. 072-3120-023) FROM IND (INDUSTRIAL/OFFICE PARK) TO MHD (MULTI-FAMILY HIGH DENSITY); AND
- APPROVE A REZONE TO CHANGE THE ZONING DESIGNATION FOR LOT 1 (APN NO. 072-3120-026) FROM M-L PD (LIMITED MANUFACTURING, PLANNED DEVELOPMENT DISTRICT) TO R-4 PD (GENERAL APARTMENT, PLANNED DEVELOPMENT DISTRICT) AND TO CHANGE THE ZONING DESIGNATION OF LOT 6 (APN NO. 072-3120-023) FROM BP PD (BUSINESS AND PROFESSIONAL, PLANNED DEVELOPMENT DISTRICT) TO R-4 PD (GENERAL APARTMENT, PLANNED DEVELOPMENT DISTRICT); AND
- APPROVE A PLANNED DEVELOPMENT PERMIT TO ESTABLISH DETAILED DEVELOPMENT AND ARCHITECTURAL STANDARDS FOR THE 253-UNIT FOLSOM CORPORATE CENTER APARTMENTS PROJECT.

COMMISSIONER ROMANELLI SECONDED THE MOTION WHICH CARRIED THE FOLLOWING VOTE:

AYES: MIKLOS, REYNOLDS, WEST, ROMANELLI
 NOES: LEARY
 RECUSED: NONE
 ABSENT: PEÑA, RAITHEL

PLANNING COMMISSION / PLANNING MANAGER REPORT


The next Planning Commission meeting is scheduled for April 20, 2022.

RESPECTFULLY SUBMITTED,



Karen Sanabria, ADMINISTRATIVE ASSISTANT

APPROVED:

signed for 

Eileen Reynolds, CHAIR

Attachment No. 5

Vicinity Map

Vicinity Map

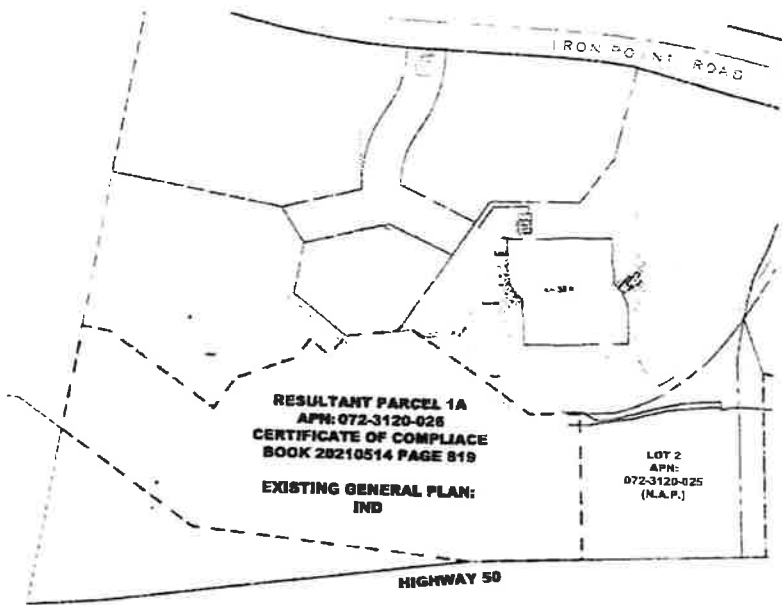


CITY OF FOLSOM

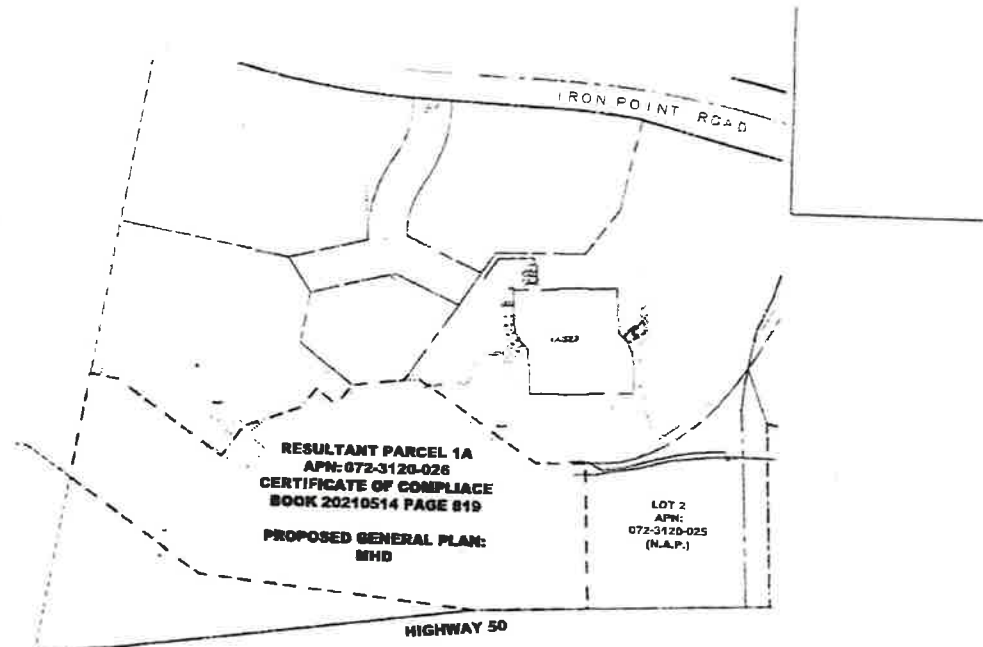


Attachment No. 6

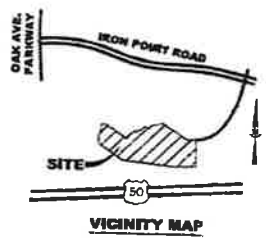
General Plan Amendment Exhibits, dated November 16, 2021



**EXISTING GENERAL PLAN DESIGNATION: INDUSTRIAL
ACREAGE: 7.24± AC**

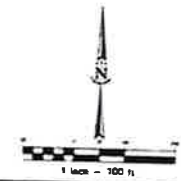


**PROPOSED GENERAL PLAN DESIGNATION: MULTI-FAMILY HIGH DENSITY
ACREAGE: 7.24± AC**



OWNER/APPLICANT:
 FCC 30, LLC
 2484 MATOMAS PARK DRIVE, SUITE 101
 SACRAMENTO, CA 95833
 ATTN: ROB COLE
 PHONE: (916)273-4020
 RCOLE@COLEPARTNERS.COM

ENGINEER:
 RSC ENGINEERING, INC.
 1420 ROCKY RIDGE DRIVE, SUITE 150
 ROSEVILLE, CA 95661
 ATTN: TIFFANY WILSON
 PHONE: (916)788-2884



RSC ENGINEERING
 1420 ROCKY RIDGE DRIVE, SUITE 150
 ROSEVILLE, CA 95661
 PHONE: (916)788-2884

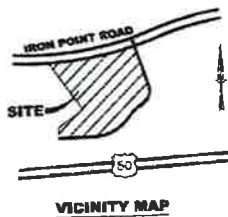
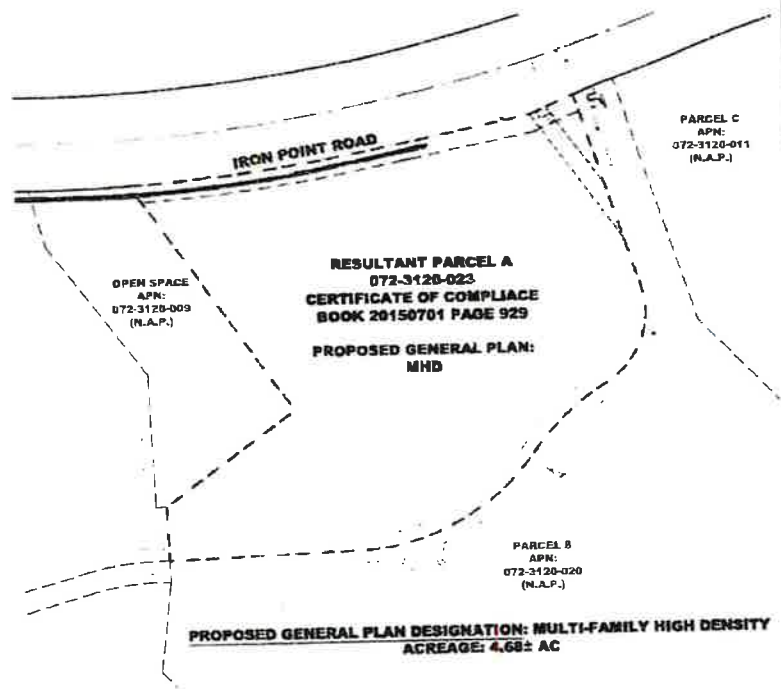
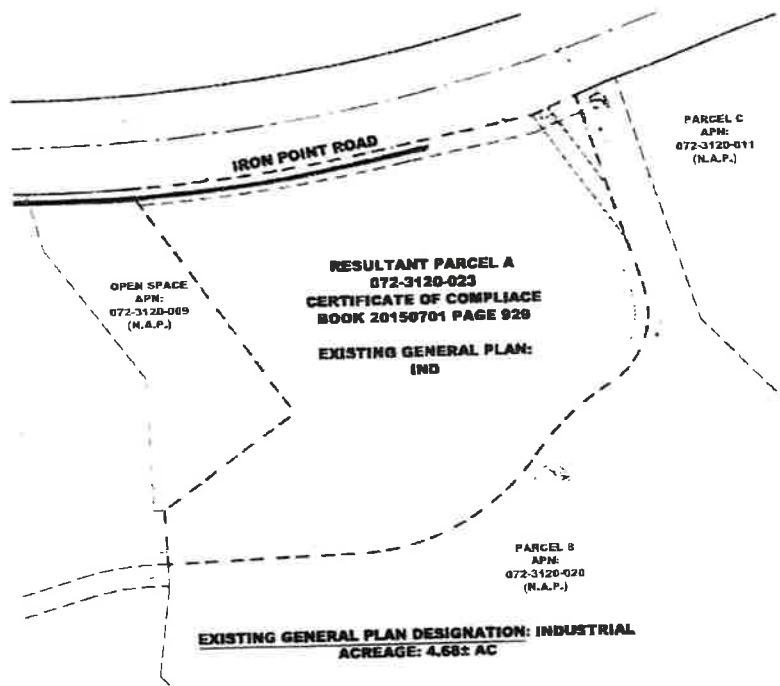


GENERAL PLAN AMENDMENT GPA

IRON POINT ROAD APARTMENTS - LOT 1
 FOLSOM, CA.



NOVEMBER 16, 2021 | MR200320 00

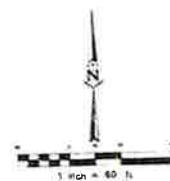


OWNER/APPLICANT:

FDC 50, LLC
2484 NATOMAS PARK DRIVE, SUITE 101
SACRAMENTO, CA 95633
ATTN: ROB COLE
PHONE: (916)273-4020
RCOLE@COLEPARTNERS.COM

ENGINEER:

RSC ENGINEERING, INC
1420 ROCKY RIDGE DRIVE, SUITE 150
ROSEVILLE, CA 95661
ATTN: TIFFANY WILSON
PHONE: (916)788-2884



GENERAL PLAN AMENDMENT GPA

IRON POINT ROAD APARTMENTS - LOT 6
FOLSOM, CA.

NOVEMBER 16, 2021 | MR200320.00

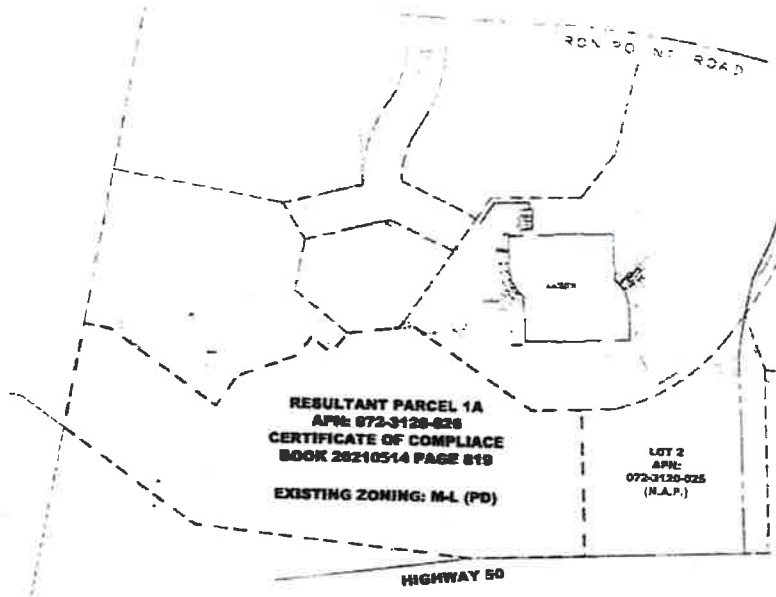


GRUPE

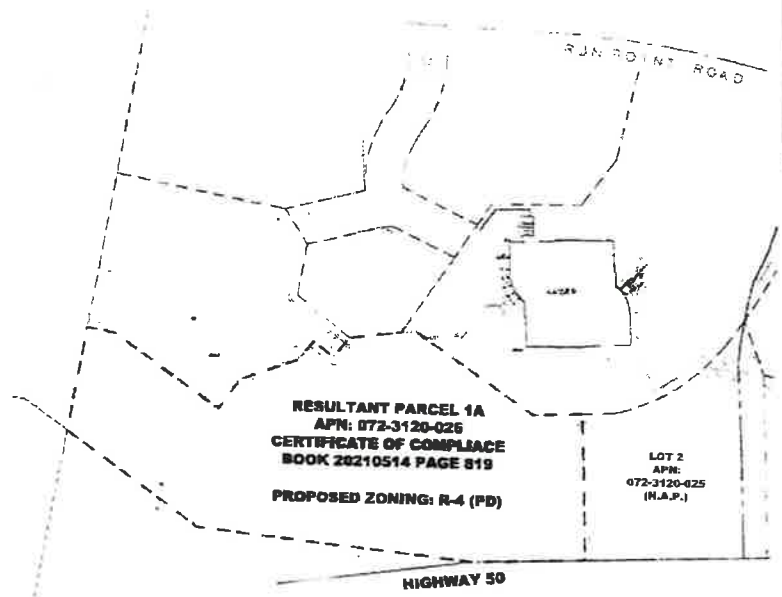


Attachment No. 7

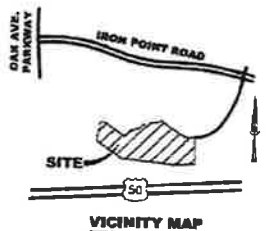
Rezone Exhibits, dated November 16, 2021



EXISTING ZONING: LIMITED MANUFACTURING (PLANNED DEVELOPMENT)
ACREAGE: 7.24± AC

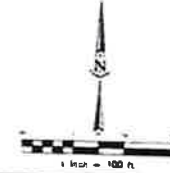


PROPOSED ZONING: GENERAL APARTMENT DISTRICT (PLANNED DEVELOPMENT)
ACREAGE: 7.24± AC



OWNER/APPLICANT:
 FCC 50, LLC
 2484 MATDMAS PARK DRIVE, SUITE 101
 SACRAMENTO, CA 95833
 ATTN: ROB COLE
 PHONE: (916)273-4020
 RCOLE@COLEPARTNERS.COM

ENGINEER:
 RSC ENGINEERING, INC.
 1420 ROCKY RIDGE DRIVE, SUITE 150
 ROSEVILLE, CA 95687
 ATTN: TIFFANY WILSON
 PHONE: (916)788-2884



RSC ENGINEERING
 1420 ROCKY RIDGE DRIVE, SUITE 150
 ROSEVILLE, CA 95687
 PHONE: (916)788-2884
 FAX: (916)788-4108

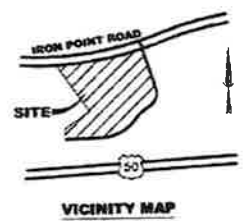
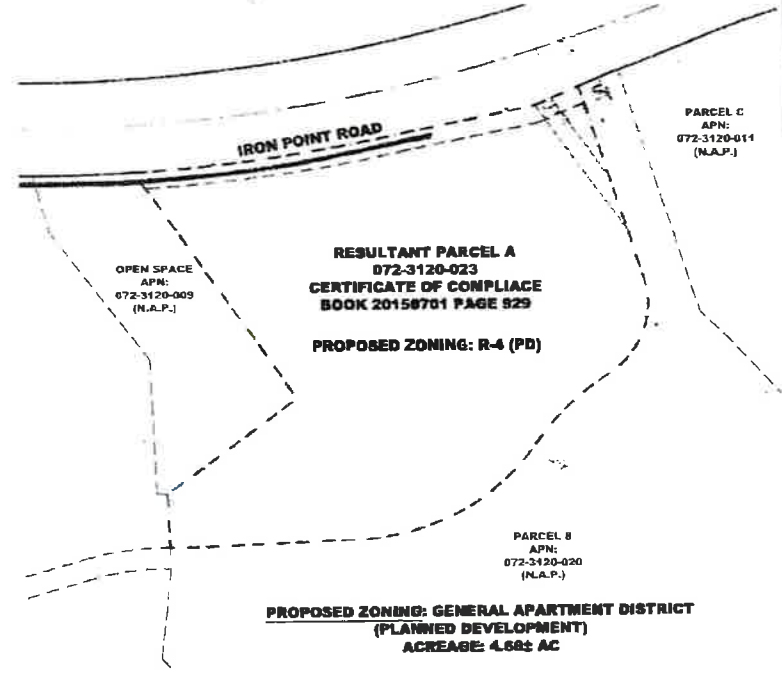
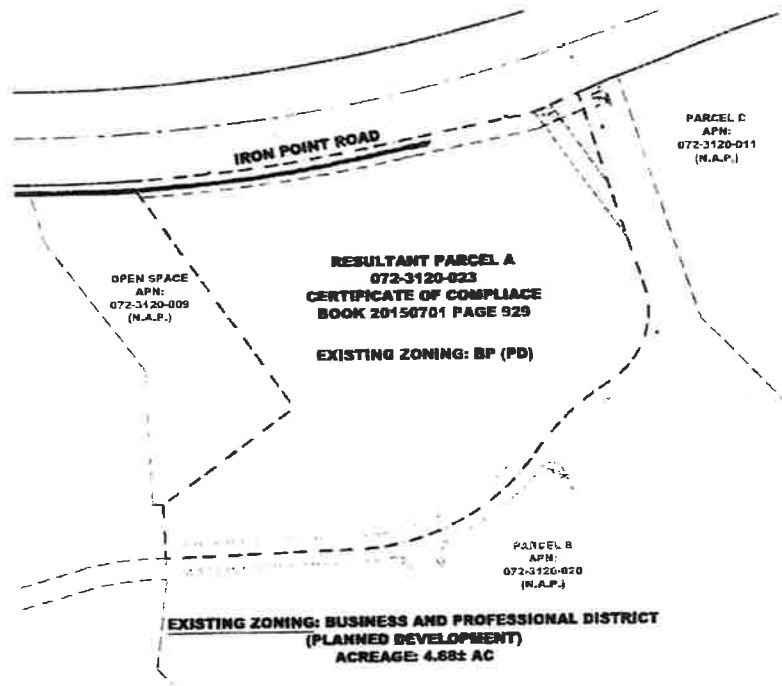


REZONE EXHIBIT **RZ**

IRON POINT ROAD APARTMENTS - LOT 1
 FOLSOM, CA.

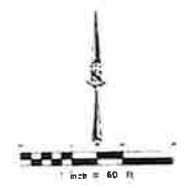
NOVEMBER 16, 2021 | MR200320.00





OWNER/APPLICANT:
 FCC SO, LLC
 2484 MATOMAS PARK DRIVE, SUITE 101
 SACRAMENTO, CA 95833
 ATTN: BOB COLE
 PHONE: (916)273-4020
 RCOLE@COLEPARTNERS.COM

ENGINEER:
 RSC ENGINEERING, INC
 1420 ROCKY RIDGE DRIVE, SUITE 150
 ROSEVILLE, CA 95661
 ATTN: TIFFANY WILSON
 PHONE: (916)788-2884



GRUPE



REZONE EXHIBIT **RZ**

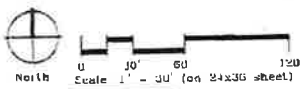
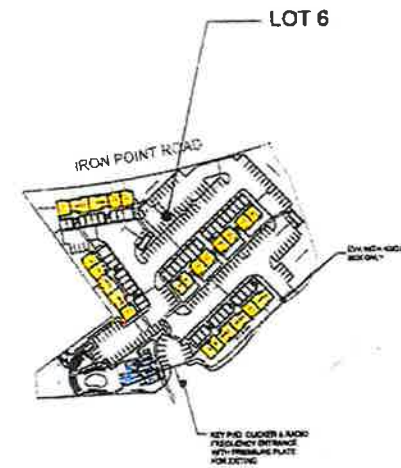
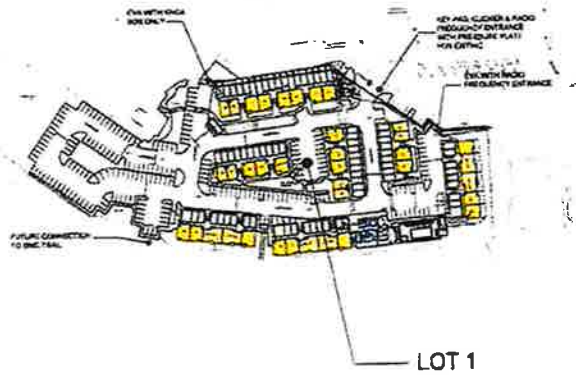
IRON POINT ROAD APARTMENTS - LOT 6
FOLSOM, CA.



NOVEMBER 16, 2021 | MR200320.00

Attachment No. 8

Overall Site Plan, dated November 16, 2021



The drawings presented are illustrative of character and design intent only, and are subject to change based upon final design considerations (i.e. applicable codes, structural, and MEP design requirements, site plan/floor plan changes, etc.) © 2019 BSB Design, Inc.

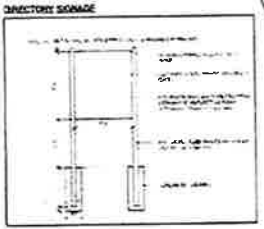
OVERALL SITE PLAN
IRON POINT ROAD APARTMENTS
 FOLSOM, CA



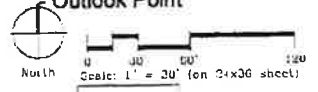
NOVEMBER 16, 2021 MR200320 00

Attachment No. 9

Individual Site Plans and Details, dated February 8, 2022



PROJECT AMENITIES
 - Dog Park
 - Outlook Point



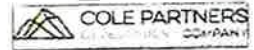
- | | | | | |
|---|--|---|---|--|
| BUILDING A1 (32-PLEX)
- Total 32 units
- 3 Stories
- 17 Garages | BUILDING B2 (21-PLEX)
- Total 21 units
- 3 Stories
- 9 Garages | BUILDING B3 (21-PLEX)
- Total 21 units
- 3 Stories
- 11 Garages | CLUBHOUSE
- Leasing Office
- 3 story, +/- 6,782 sf
- Pool | BUILDING B1 (16-PLEX)
- Total 16 units
- 3 Stories
- 9 Garages |
|---|--|---|---|--|

PROJECT SUMMARY

GROSS AREA	+/- 7.18 ac	10% OF PARKING = 30 EV CAPABLE PARKING SPACES TO BE PROVIDED
TOTAL UNITS	153 units	
DENSITY	21.31 du/ac	
OVERALL PARKING	304 spaces (1.99 ratio)	

CONCEPTUAL SITE PLAN - 7.18 ac SITE
CONCEPTUAL SITE PLAN - 153 UNITS

IRON POINT ROAD APARTMENTS
 FOLSOM, CA

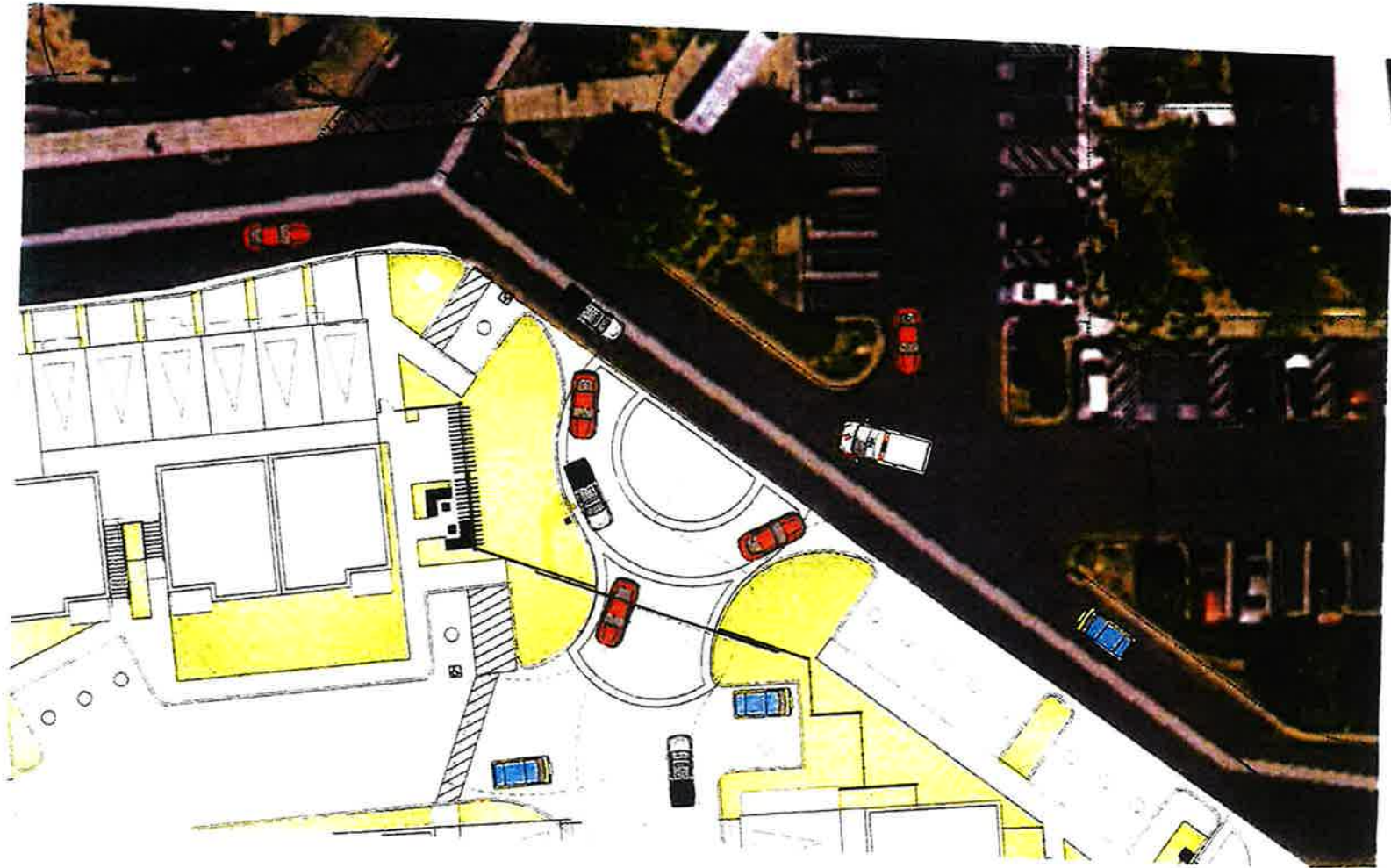


NOTE: Proposed property location, boundary lines, and shape of the parcel shown in this study are for graphic reference only and may be subject to change pending on owner's final surveying map.

The drawings presented are illustrative of character and design intent only and are subject to change based upon final design considerations in accordance with applicable codes, structural and MEP design requirements and all other floor plan changes. © 2022 BSB Design, Inc.

FEBRUARY 8, 2022 | MR200320





GRUPE
FUHRMAN LEAMY
COLE PARTNERS
DEVELOPMENT COMPANY

The drawings presented are illustrative of character and design intent only and are subject to change based upon final design considerations, applicable codes, structural and MEP design requirements, site plan / floor plan changes, etc. | © 2019 BSB Design, Inc.

FUHRMAN LEAMY
LAND GROUP
DESIGN - SERVICE - SOLUTIONS
800.193.1343 info@flandgroup.com

IRON POINT ROAD APARTMENTS

FOLSOM, CA

Preliminary Entry - Lot 1

Nov. 6th, 2021 | MR200320 00

PL1
BSB
DESIGN
BSBDESIGN.COM

PROJECT SUMMARY

GROSS AREA +/-4.13 ac
 TOTAL UNITS 100 units
 DENSITY 24.21 du/ac
 PARKING 187 spaces (1.87 ratio)
 - Garage 46 spaces
 - Covered Parking 54 spaces
 - Surface Parking 87 spaces
 10% OF PARKING = 19 EV CAPABLE
 PARKING SPACES TO BE PROVIDED

BUILDING B2 (21-PLEX)
 - Total 21 units
 - Front-loaded
 - 3 Stories

BUILDING B4 (26-PLEX)
 - Total 26 units
 - Front-loaded
 - 3 Stories

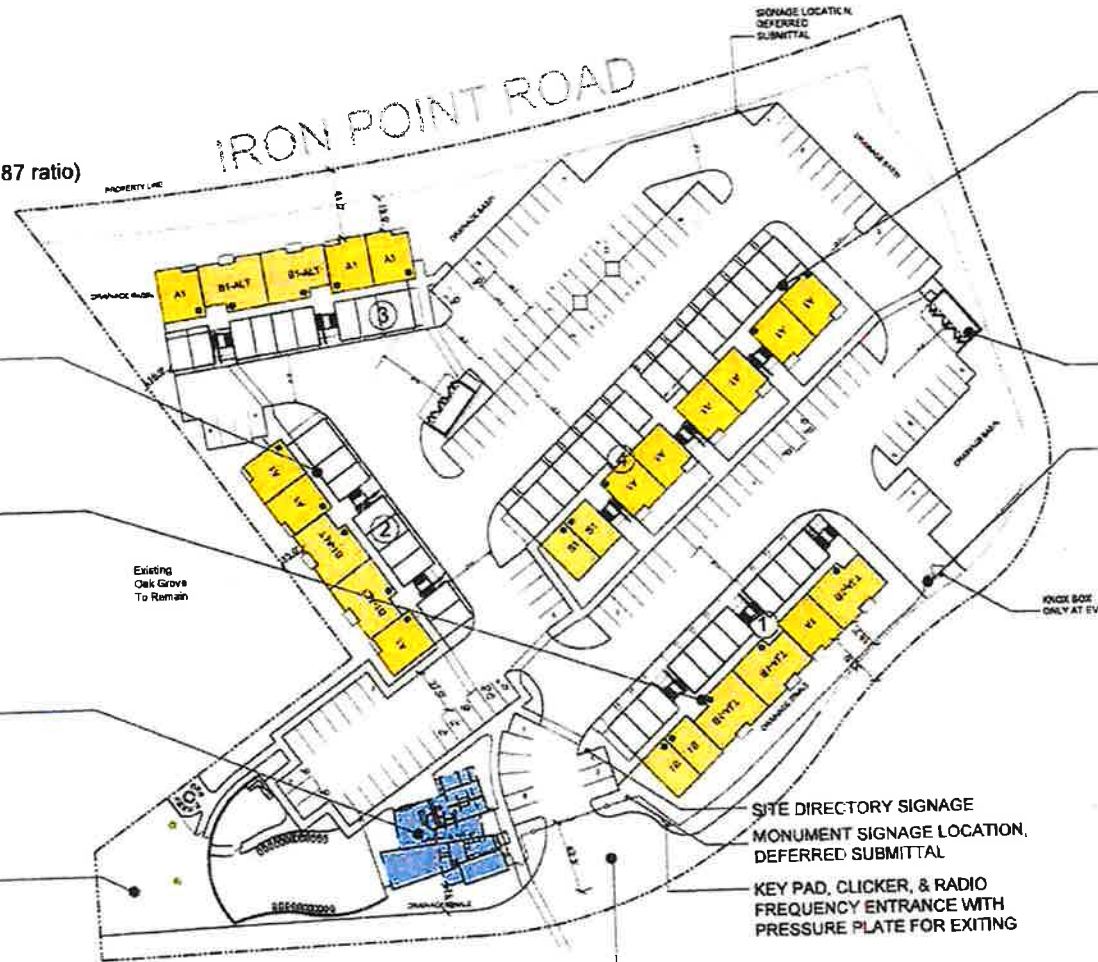
CLUBHOUSE
 - Leasing Office
 - +/- 3,098 sf
 - Pool
 - Outdoor Fire Pit

DOG PARK

BUILDING A1 (32-PLEX)
 - Total 32 units
 - Rear-loaded
 - 3 Stories

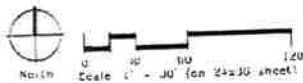
Trash Enclosure
 Recycling &
 Organics

Emergency
 Vehicular
 Access



SITE DIRECTORY SIGNAGE
 MONUMENT SIGNAGE LOCATION,
 DEFERRED SUBMITTAL

KEY PAD, CLICKER, & RADIO
 FREQUENCY ENTRANCE WITH
 PRESSURE PLATE FOR EXITING



GROUP



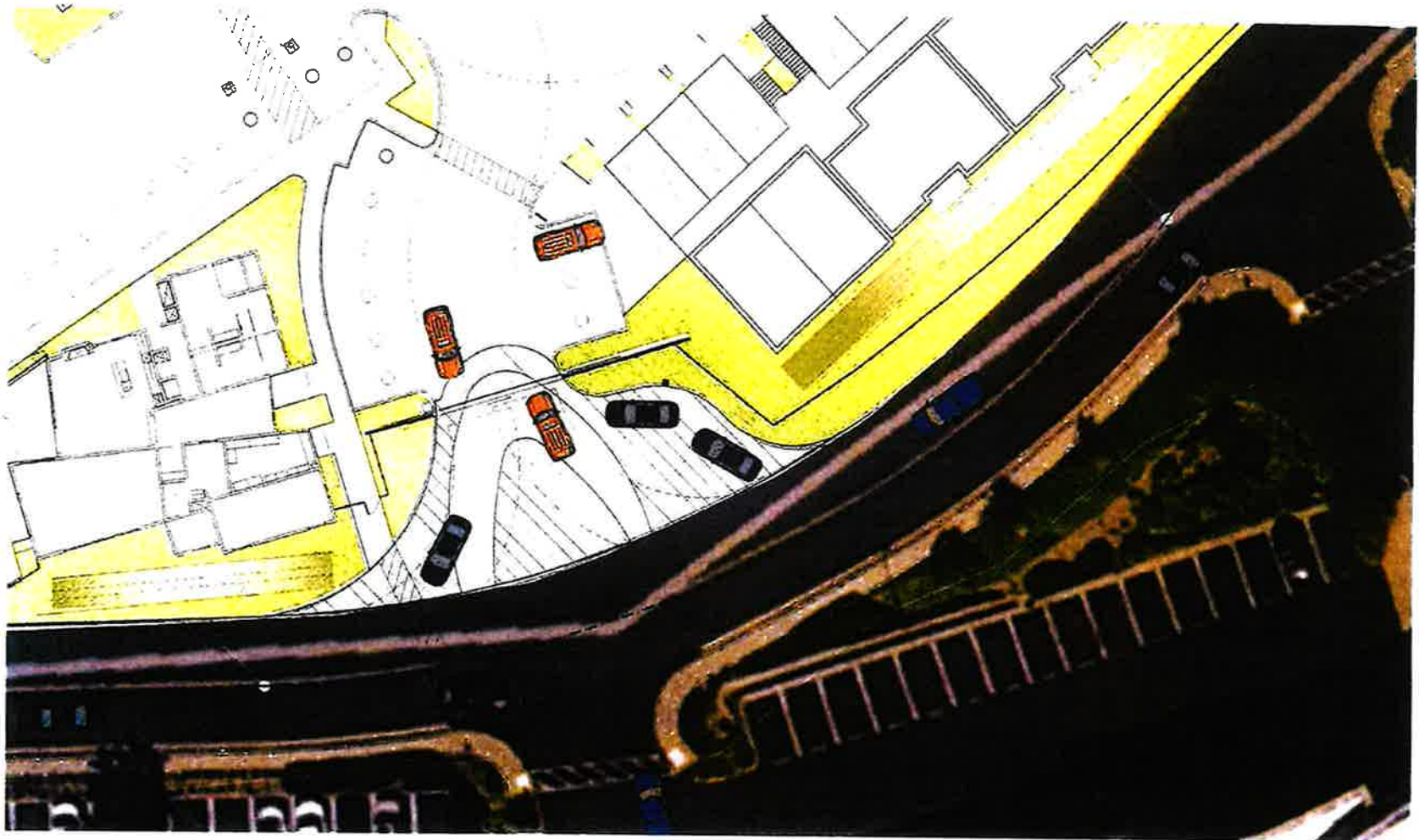
NOTE: Proposed property location, boundary lines, and shape of the parcel shown in this study are for graphic reference only and may be subject to change pending on owner's final surveying map.

**CONCEPTUAL SITE PLAN - 4.13 ac SITE
 CONCEPTUAL SITE PLAN - 100 UNITS
 IRON POINT ROAD APARTMENTS**

FOLSOM, CA.

FEBRUARY 8, 2022 | MR200320





IRON POINT ROAD APARTMENTS FOLSOM, CA.

PL2
BSB
DESIGN
669DESIGN.COM

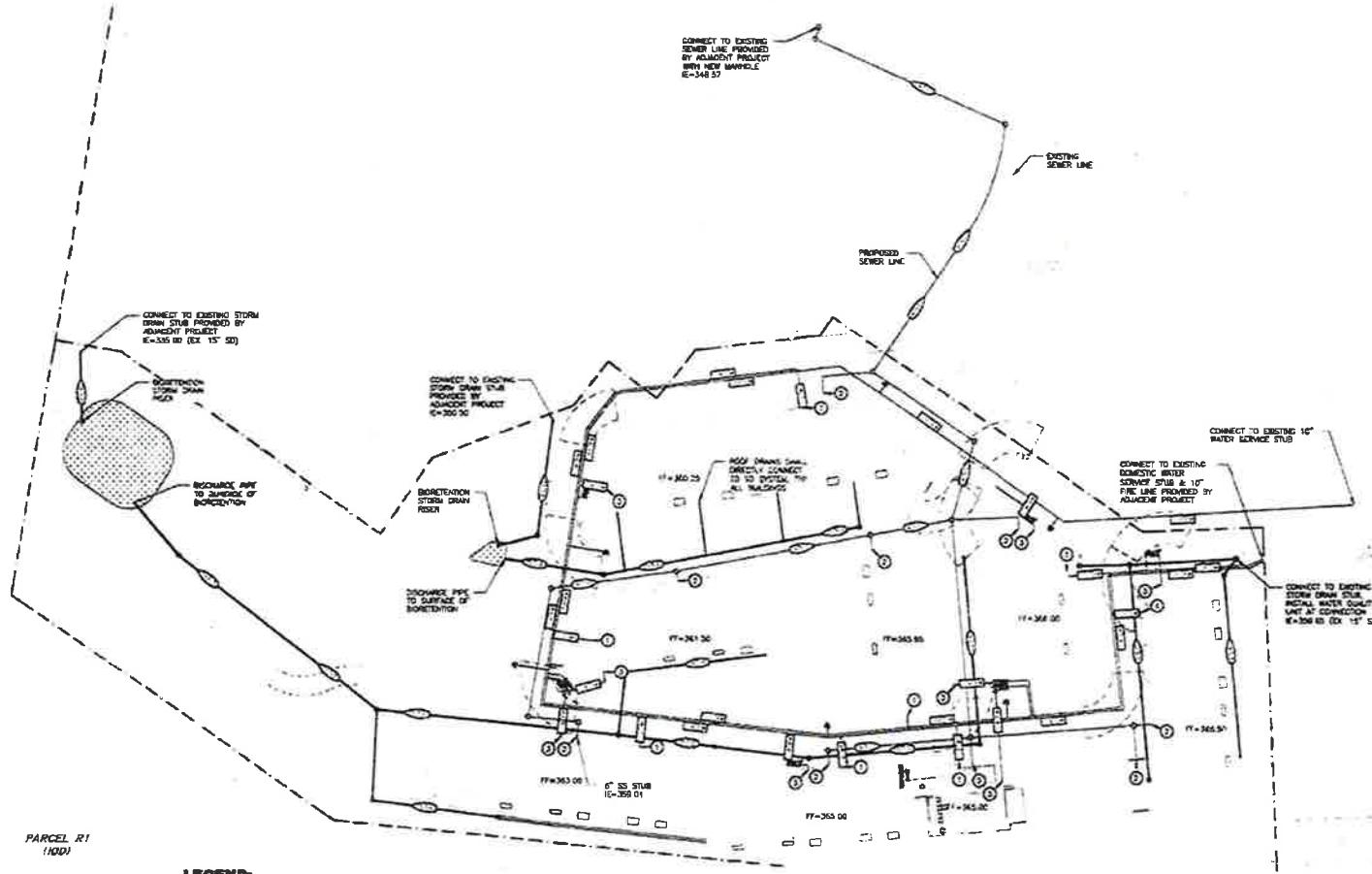
Preliminary Entry - Lot 6

Nov. 6th, 2021 | MR200320 00

These drawings are illustrative of character and design intent only and are subject to change based upon final design considerations. All applicable codes, structural and MEP design requirements, utility plans / floor plan changes, etc. | © 2019 BSB Design, Inc.

Attachment No. 10

Preliminary Utility Plans, dated November 16, 2021



PARCEL R1
(100)

LEGEND:

- PROPOSED SANITARY SEWER
- EXISTING SANITARY SEWER
- PROPOSED STORM DRAIN
- EXISTING STORM DRAIN
- PROPOSED DOMESTIC WATER LINE
- EXISTING WATER LINE
- PROPOSED FIRE SERVICE
- PROPOSED WASH WATER
- PROPOSED FIRE DEPARTMENT CONNECTION ASSEMBLY
- PROPOSED FIRE HYDRANT
- PROPOSED BIODETENTION STORM WATER PLANTER
- PROPOSED SANITARY SEWER MANHOLE
- PROPOSED STORM DRAIN MANHOLE
- PROPOSED STORM DRAIN INLET
- EXISTING PROPERTY LINE
- EXISTING P.U.E.
- 4" ABSOLUTE WATER CONNECTION
- 4" SANITARY SEWER CONNECTION
- 4" SPRINKLER SERVICE CONNECTION

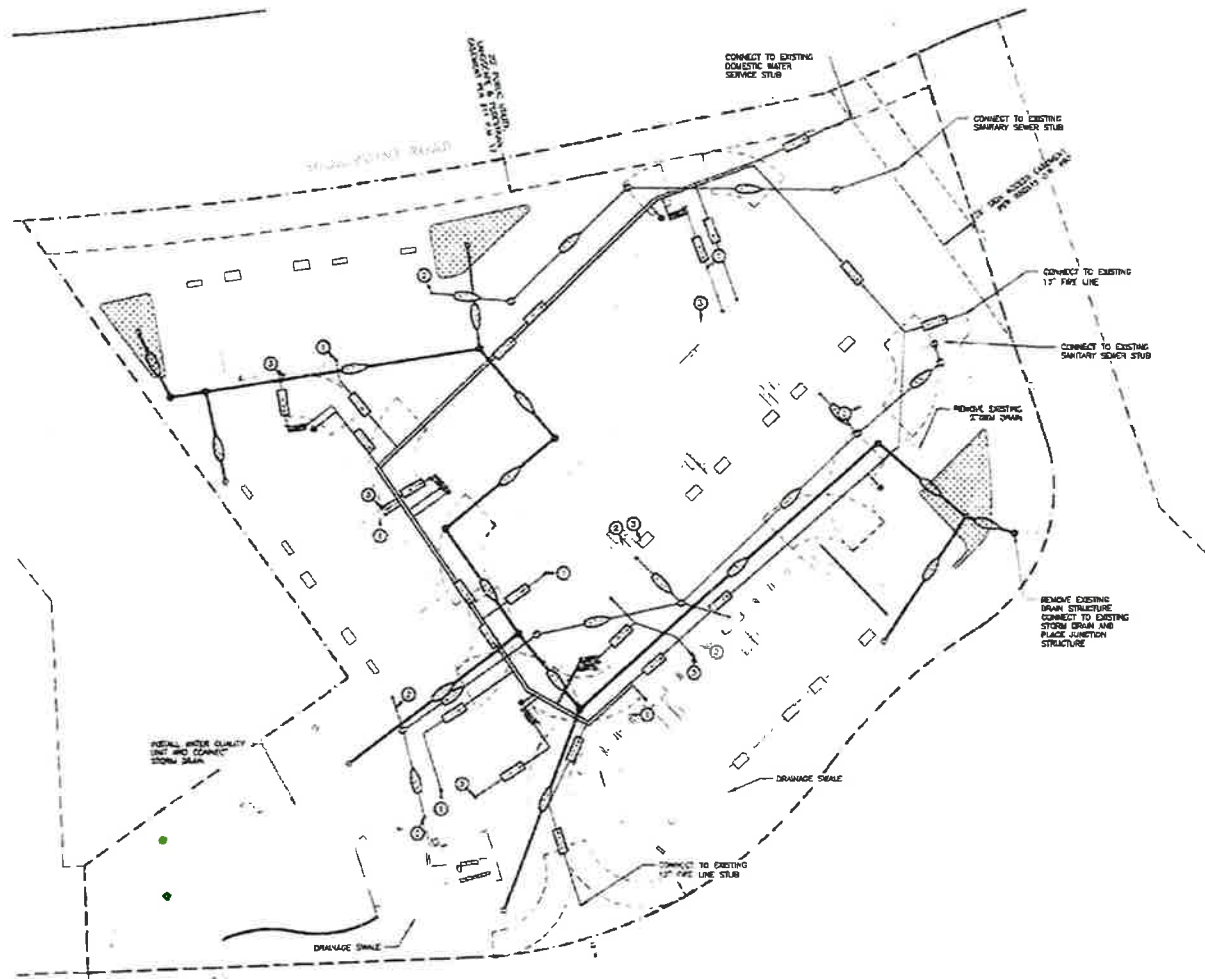


PRELIMINARY UTILITY PLAN UT1

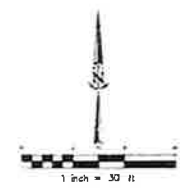
IRON POINT ROAD APARTMENTS - LOT 1
FOLSOM, CA.



NOVEMBER 16, 2021 | MR200320.00



- LEGEND:**
- PROPOSED SANITARY SEWER
 - EXISTING SANITARY SEWER
 - PROPOSED STORM DRAIN
 - EXISTING STORM DRAIN
 - PROPOSED DOMESTIC WATER LINE
 - EXISTING WATER LINE
 - PROPOSED FIRE SERVICE
 - PROPOSED WATER METER
 - PROPOSED FIRE SERVICE/CONNECTION ASSEMBLY
 - PROPOSED FIRE HYDRANT
 - ASSUMED BENEATH/STORM INLET PLANTER
 - PROPOSED SANITARY SEWER MANHOLE
 - PROPOSED STORM DRAIN MANHOLE
 - PROPOSED STORM DRAIN INLET
 - EXISTING PROPERTY LINE
 - EXISTING P.U.E.
 - 3" POSSIBLE WATER CONNECTION
 - 6" SANITARY SEWER CONNECTION
 - 4" DRAINAGE SERVICE CONNECTION



PRELIMINARY UTILITY PLAN UT1

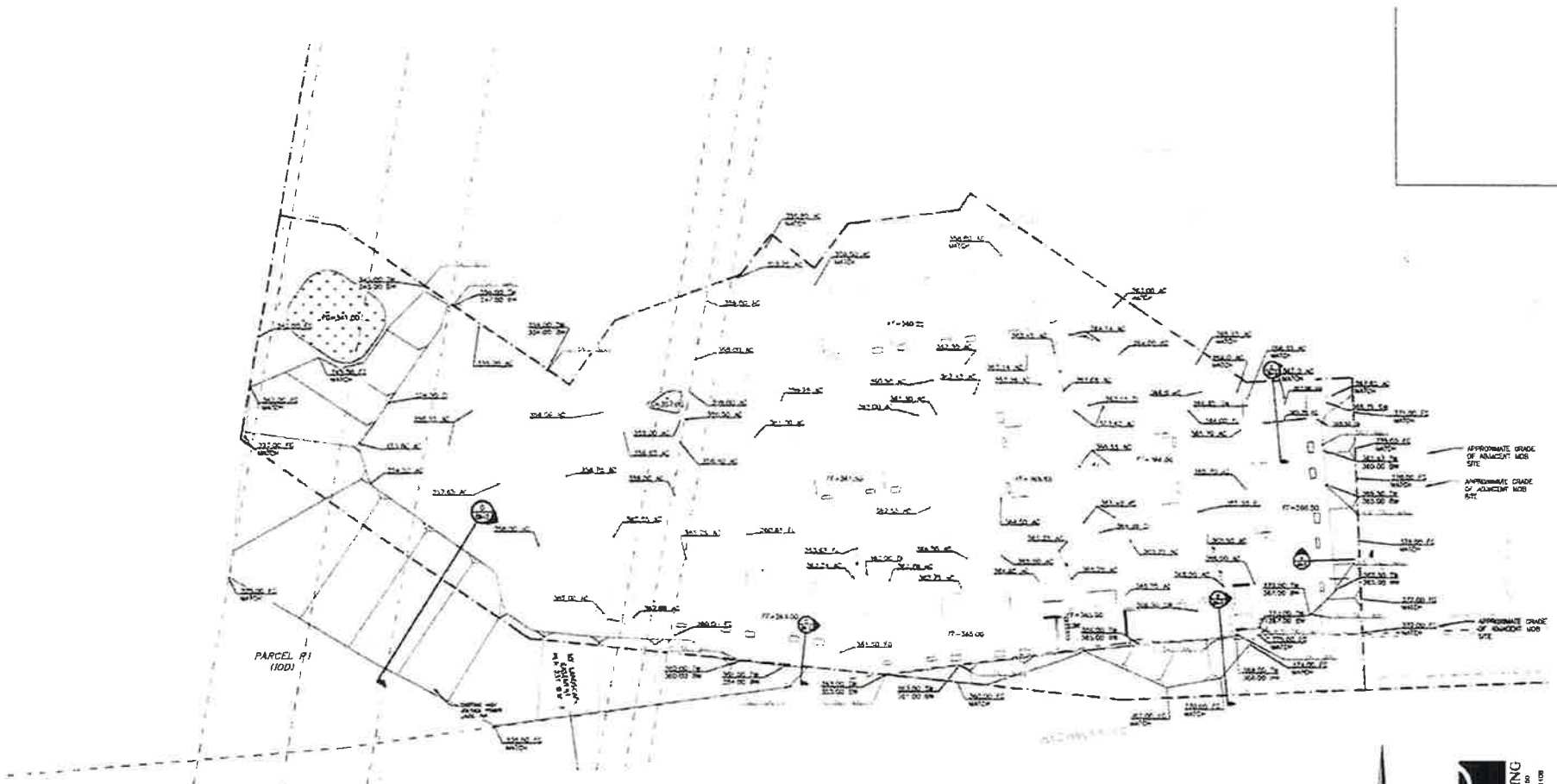
IRON POINT ROAD APARTMENTS - LOT 6
FOLSOM, CA.

NOVEMBER 16, 2021 | MR200320.00



Attachment No. 11

**Preliminary Grading and Drainage Plans, dated November 16,
2021**

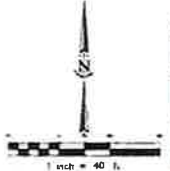


LEGEND:

- FF=13.5
- 13.0 AS
- 12.0 D
- 12.0 B
- PROPOSED 1ST FLOOR ELEVATION
- PROPOSED TOP OF FINEMENT ELEVATION
- PROPOSED TOP OF GATE ELEVATION DROP W/LS
- PROPOSED CUTTER FLOWLINE
- PROPOSED CONC. WATER PAVEMENT
- EXISTING CENTERLINE
- PROPOSED DRIVE MIDDLE
- EXISTING PROPERTY LINE
- EXISTING P.U.L.

PRELIMINARY EARTHWORK:

CUT: 10,883 CU FT
 FILL: 43,383 CU FT
 NET: 12,492 CU FT



RSC ENGINEERING
 1400 PLYMOUTH STREET, SUITE 100
 FOLSOM, CA 95630
 (916) 978-2800 FAX (916) 978-2807

PRELIMINARY GRADING PLAN GR1

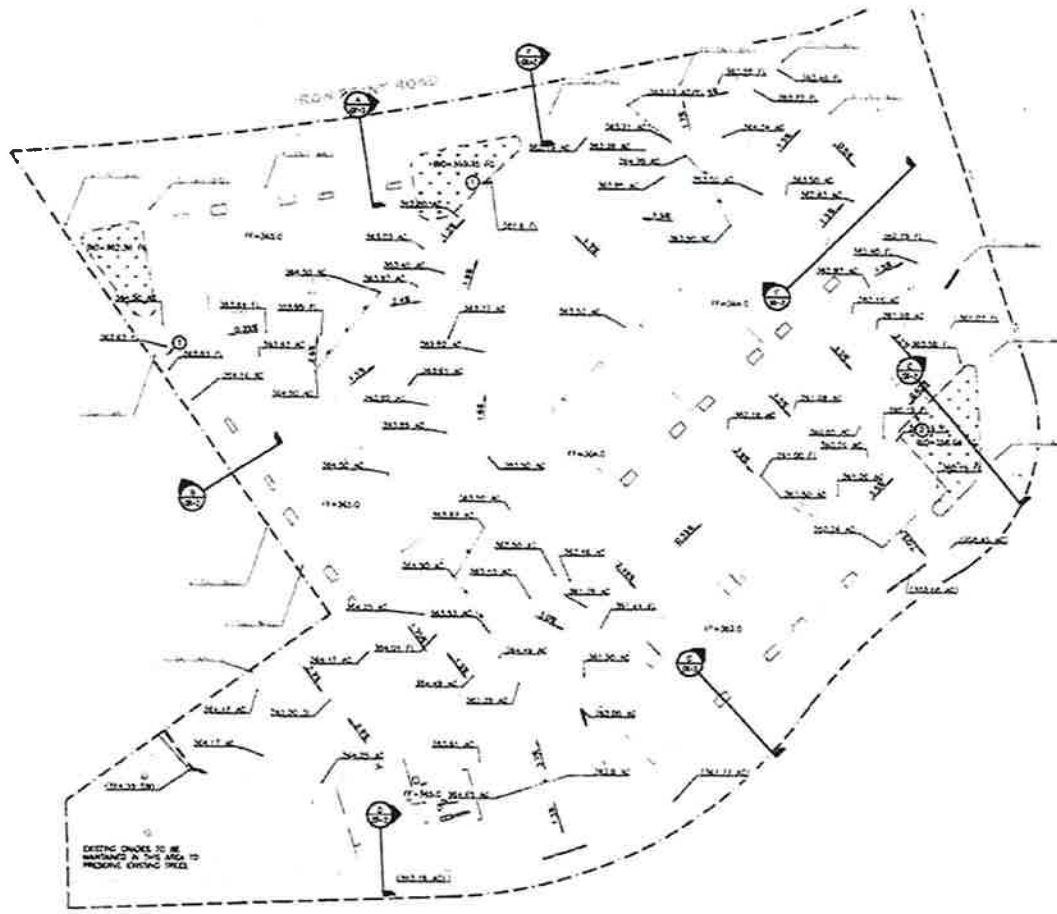
IRON POINT ROAD APARTMENTS - LOT 1
 FOLSOM, CA.

BSB DESIGN
 BSBDESIGN.COM

GRUPE

COLE PARTNERS DEVELOPMENT COMPANY

NOVEMBER 16, 2021 | MR200320.00



- LEGEND:**
- 117.11.5 PROPOSED FINISHED FLOOR ELEVATION
 - 120.00 PROPOSED TOP OF PAVEMENT ELEVATION
 - 120.00 PROPOSED TOP OF GRADE ELEVATION (PROP. HLT)
 - 122.00 PROPOSED OUTER FLOORING
- EXISTING CONTOUR
 - PROPOSED GRADE BREAK
 - EXISTING PROPERTY LINE
 - EXISTING P.U.C.
 - CURB OPENING TO DRAIN TO STORMWATER PLANTED
 - SIDEWALK UNDERDRAIN
 - PROPOSED BIODIVERSITY STORM WATER PLANTED
- CONTAIN LINE, MATCH EXISTING GRIDES

PRELIMINARY EARTHWORK:
 CUT 5,482 SF
 FILL 4,374 SF

EXISTING CHANGES TO BE MAINTAINED IN THIS AREA TO PROTECT EXISTING TREES



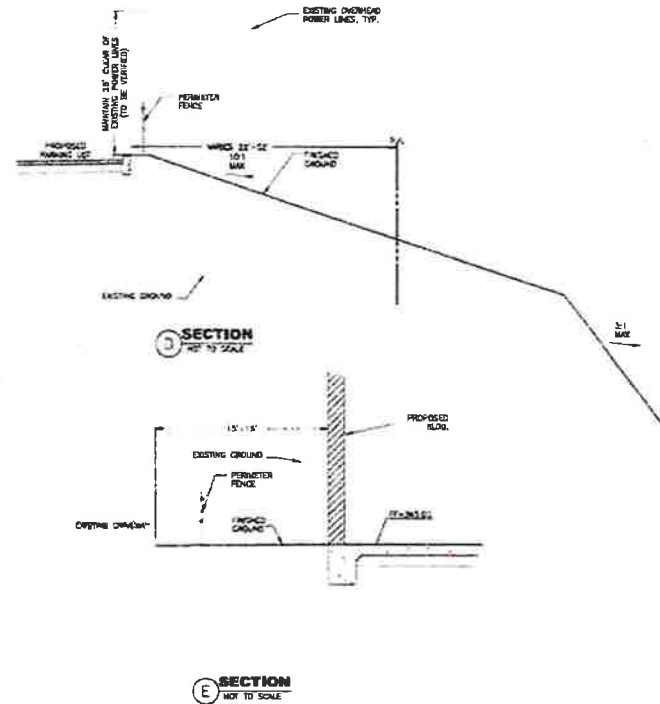
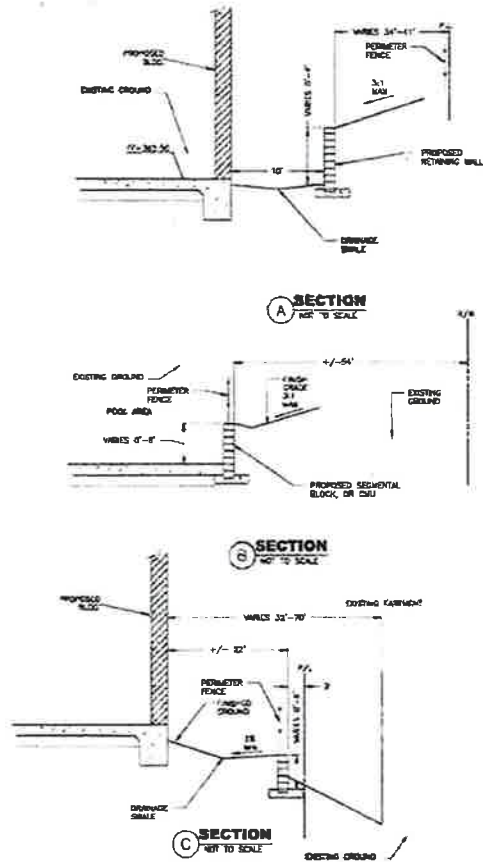
GRUPE



PRELIMINARY GRADING PLAN GR1

IRON POINT ROAD APARTMENTS - LOT 6
 FOLSOM, CA.

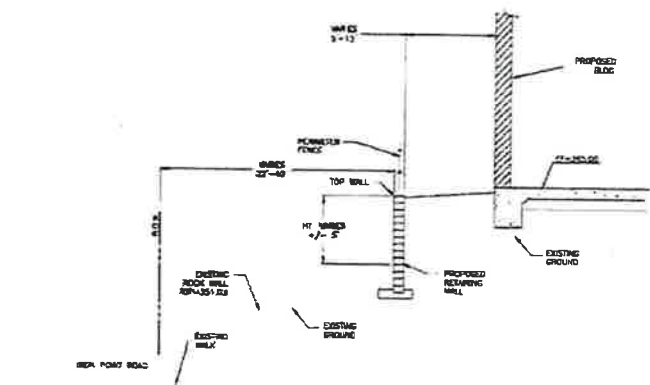
NOVEMBER 16, 2021 | MR200320.00



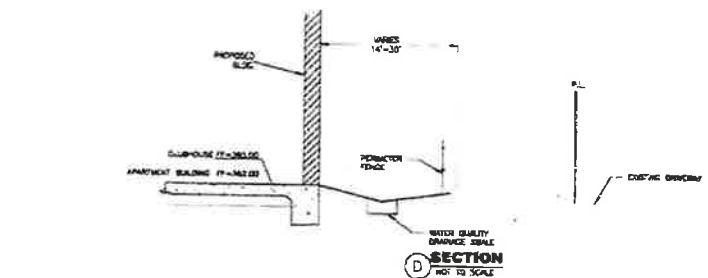
PRELIMINARY GRADING SECTIONS GR2
IRON POINT ROAD APARTMENTS - LOT 1
 FOLSOM, CA.

NOVEMBER 16, 2021 | MR200320.00

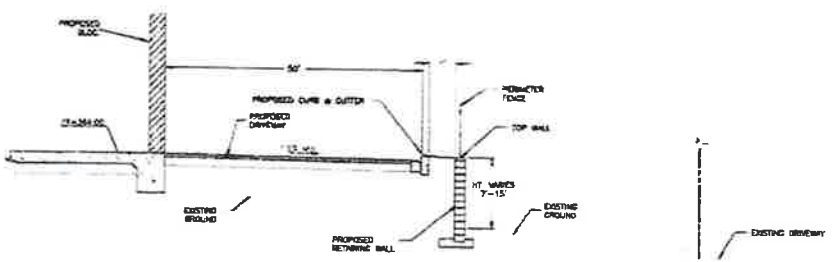




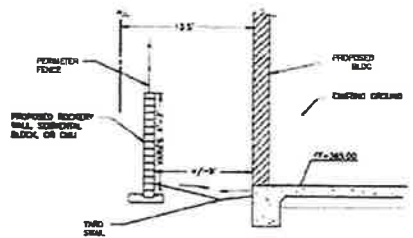
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NOT TO SCALE



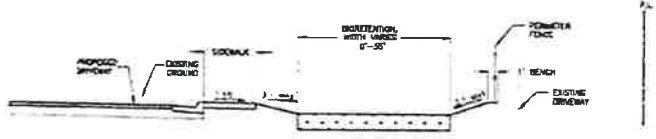
D SECTION
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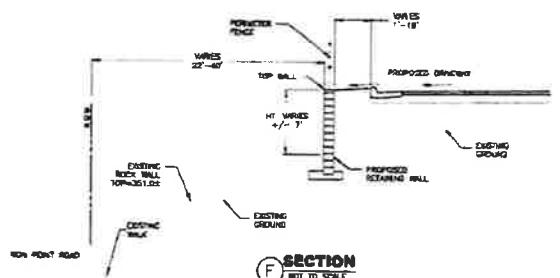
E SECTION
NOT TO SCALE



B SECTION
NOT TO SCALE



C SECTION
NOT TO SCALE



F SECTION
NOT TO SCALE

GRUPE

COLE PARTNERS
DEVELOPMENT COMPANY

PRELIMINARY GRADING SECTIONS GR2

IRON POINT ROAD APARTMENTS - LOT 6

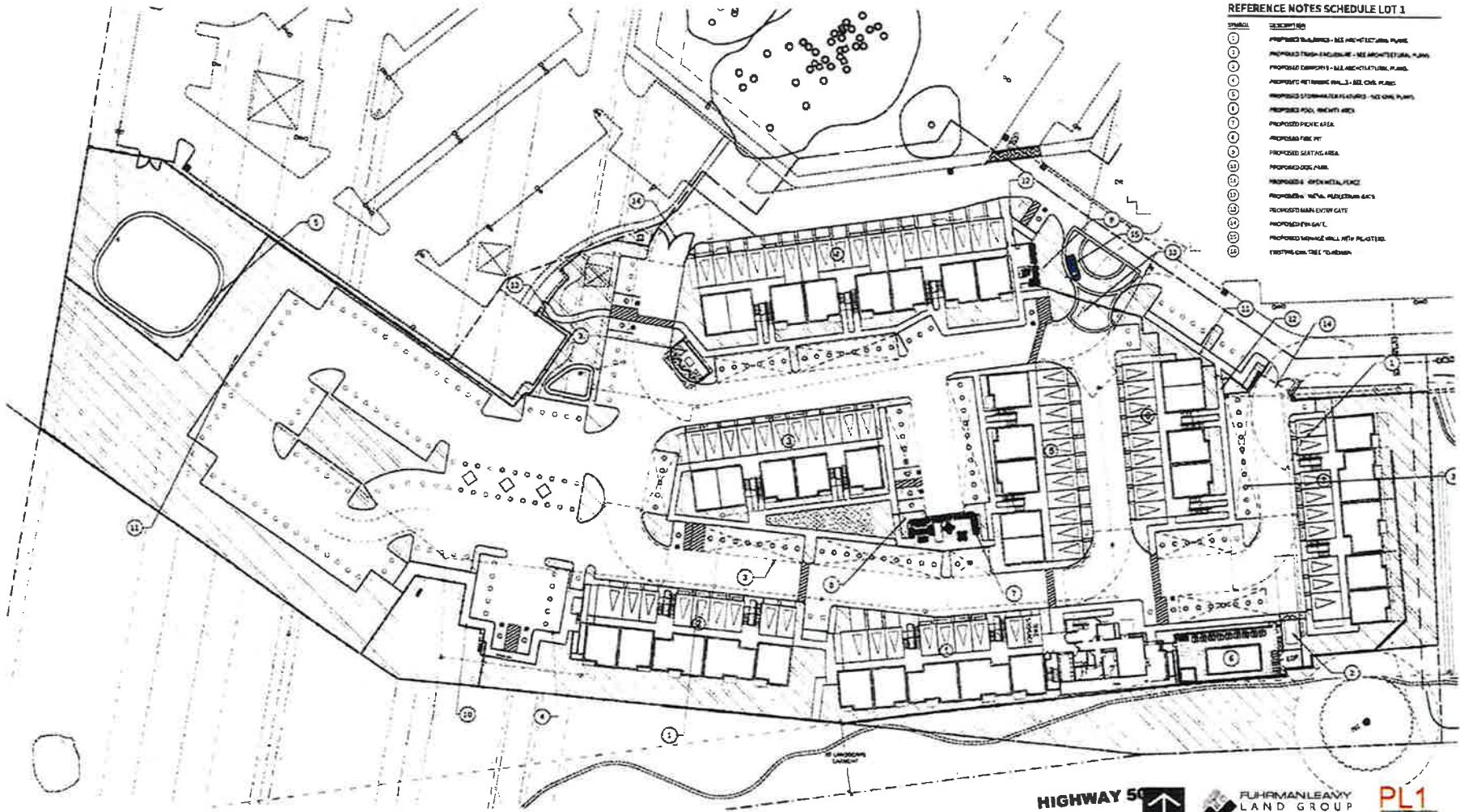
FOLSOM, CA.

NOVEMBER 16, 2021 | MR200320.00



Attachment No. 12

Preliminary Landscape Plans and Details, dated November 16, 2021



REFERENCE NOTES SCHEDULE LOT 1

SYMBOL	DESCRIPTION
(1)	PROPOSED BALCONY - SEE ARCHITECTURAL PLAN
(2)	PROPOSED TERRACE/ENCLOSURE - SEE ARCHITECTURAL PLAN
(3)	PROPOSED CORNER - SEE ARCHITECTURAL PLAN
(4)	PROPOSED RETAINING WALL - SEE CIVIL PLAN
(5)	PROPOSED STORMWATER COURSE - SEE CIVIL PLAN
(6)	PROPOSED POOL, DECK & AREA
(7)	PROPOSED PAVEMENT AREA
(8)	PROPOSED PARKING
(9)	PROPOSED SEATING AREA
(10)	PROPOSED DECK AREA
(11)	PROPOSED OPEN METAL FENCE
(12)	PROPOSED WOOD PICKET FENCE
(13)	PROPOSED MAIN ENTRY GATE
(14)	PROPOSED SIDE GATE
(15)	PROPOSED SIDEWALK WITH PAVEMENT
(16)	PROPOSED SIDEWALK WITH PAVEMENT
(17)	PROPOSED SIDEWALK WITH PAVEMENT
(18)	PROPOSED SIDEWALK WITH PAVEMENT
(19)	PROPOSED SIDEWALK WITH PAVEMENT
(20)	PROPOSED SIDEWALK WITH PAVEMENT
(21)	PROPOSED SIDEWALK WITH PAVEMENT
(22)	PROPOSED SIDEWALK WITH PAVEMENT
(23)	PROPOSED SIDEWALK WITH PAVEMENT
(24)	PROPOSED SIDEWALK WITH PAVEMENT
(25)	PROPOSED SIDEWALK WITH PAVEMENT

GRUPE



IRON POINT ROAD APARTMENTS-LOT 1
FOLSOM, CA.

Preliminary Landscape Plan NOV 16th, 2021 | MR200320.00





DESIGN STATEMENT
 THE LANDSCAPE WILL BE DESIGNED TO MEET THE COUNTY DEVELOPMENT STANDARDS AND DESIGN GUIDELINES. THE PLANTING AND IRRIGATION WILL MEET THE WMELO REQUIREMENTS BY UTILIZING LOW WATER USE PLANT MATERIAL AND IRRIGATED WITH A HIGH EFFICIENCY IRRIGATION SYSTEM. THE STORMWATER FEATURES HAVE BEEN COORDINATED WITH THE CIVIL ENGINEER FOR PLACEMENT AND SIZE TREES UNDER THE OVERHEAD UTILITY EASEMENT WILL MEET THE HEIGHT CRITERIA.

GENERAL PLANTING NOTES
 1. ALL PLANTING AREAS SHALL BE IRRIGATED WITH AN AUTOMATIC IRRIGATION SYSTEM THAT WILL BE COMPLIANT WITH THE STATE'S WATER EFFICIENT LANDSCAPE ORDINANCE.
 ALL SHRUBS AND GROUNDCOVER AREAS SHALL RECEIVE A 3" LAYER OF BARK CHIP MULCH TOP DRESSING.
 GROUND COVERS SHALL NOT BE INSTALLED WITHIN A 4' RADIUS OF THE TREE DRUM.

PLANT SCHEDULE LOT 1

SYMBOL	SYMBOL NAME	COMMON NAME	SIZE	PLANT CODE	QTY
(Symbol)	RED BURNING BUSH	EREGONIA GARDNERI	12 GAL	12R	10
(Symbol)	WAXY LEAFED SAGE	LEUCANTHOSUS GRACILIS	12 GAL	12W	10
(Symbol)	... (Other plants listed)	...	...	...	...

SYMBOL	SYMBOL NAME	COMMON NAME	SIZE	PLANT CODE	QTY
(Symbol)	... (Plant)	...	...	...	...
(Symbol)	... (Plant)	...	...	...	...

SYMBOL	SYMBOL NAME	COMMON NAME	SIZE	PLANT CODE	QTY
(Symbol)	... (Plant)	...	...	...	...
(Symbol)	... (Plant)	...	...	...	...



IRON POINT ROAD APARTMENTS-LOT 1
 FOLSOM, CA
 Preliminary Landscape Plan NOV 16th 2021 | MR200320 00

PL2
 PLURMAN LEAMY LAND GROUP
 240 PROFESSIONAL DRIVE, SUITE 100, FOLSOM, CA 95630
 (916) 783-1941 • www.pl2.com

BSB DESIGN
 bsbdesign.com

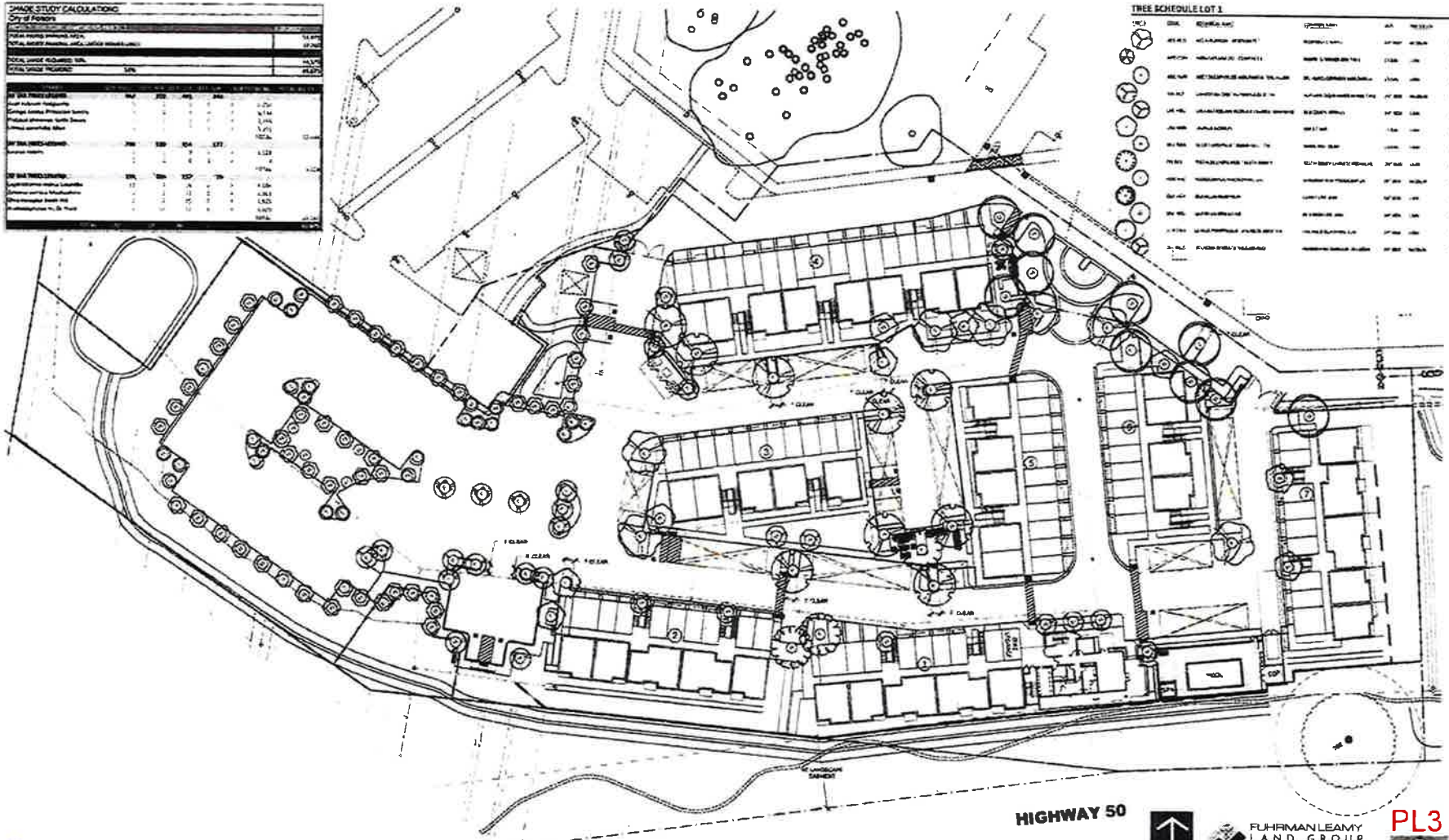
SHADE STUDY CALCULATIONS
City of Folsom

Category	Area (sq ft)	Shade Value
Existing Shade	10,000	100
Proposed Shade	20,000	200
Total Shade	30,000	300

Category	Area (sq ft)	Shade Value
Plant Species	100	1.00
Plant Species	200	2.00
Plant Species	300	3.00
Plant Species	400	4.00
Plant Species	500	5.00
Plant Species	600	6.00
Plant Species	700	7.00
Plant Species	800	8.00
Plant Species	900	9.00
Plant Species	1000	10.00

TREE SCHEDULE LOT 1

ID	Species	Quantity	Size	Notes
101	ACACIA	10	12"	
102	ALBANY	5	18"	
103	ALBANY	5	18"	
104	ALBANY	5	18"	
105	ALBANY	5	18"	
106	ALBANY	5	18"	
107	ALBANY	5	18"	
108	ALBANY	5	18"	
109	ALBANY	5	18"	
110	ALBANY	5	18"	
111	ALBANY	5	18"	
112	ALBANY	5	18"	
113	ALBANY	5	18"	
114	ALBANY	5	18"	
115	ALBANY	5	18"	
116	ALBANY	5	18"	
117	ALBANY	5	18"	
118	ALBANY	5	18"	
119	ALBANY	5	18"	
120	ALBANY	5	18"	



HIGHWAY 50

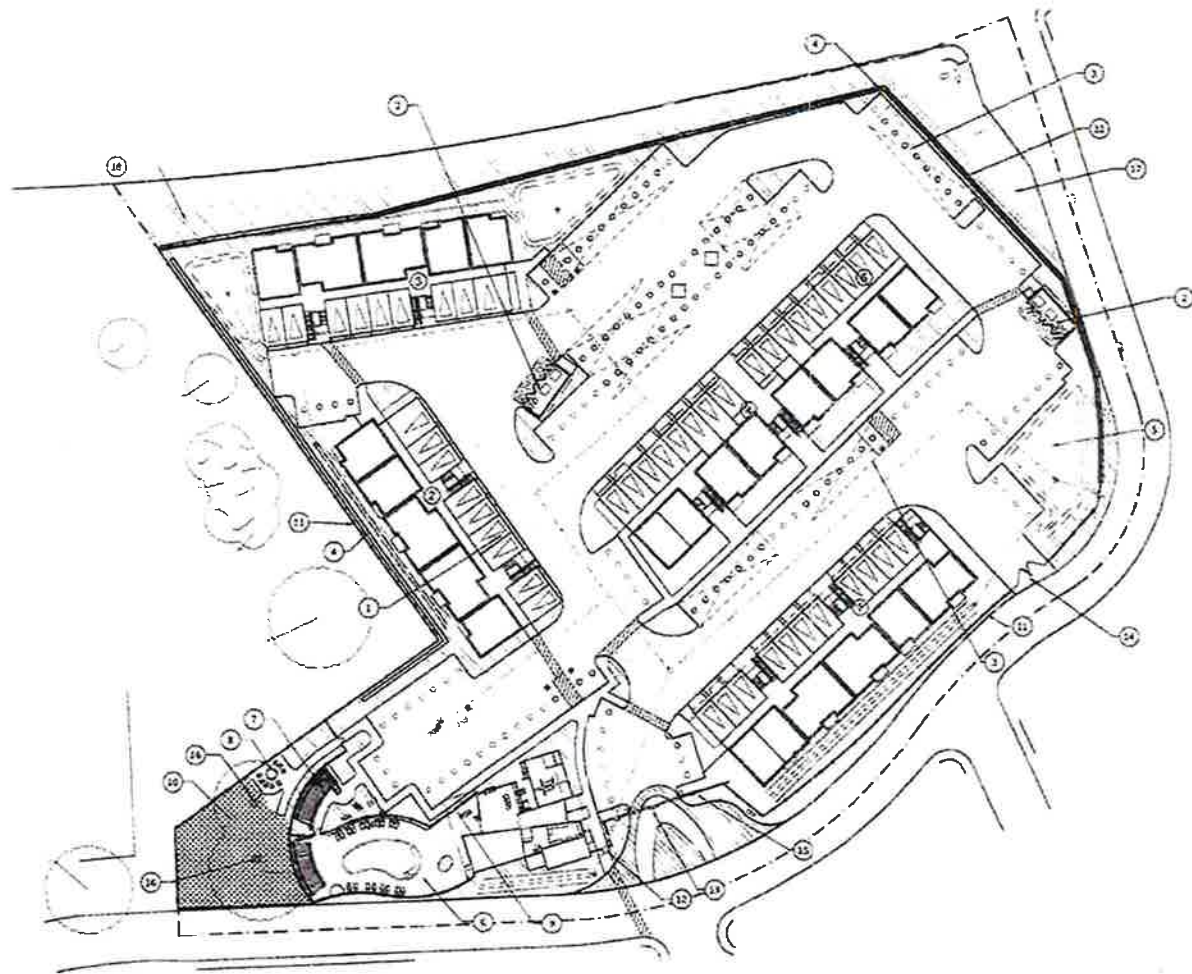


FUHRMAN LEAMY
LAND GROUP
3610 PROFESSIONAL DRIVE, SUITE 100, FOLSOM, CA 95630
PH: (916) 554-1100 | www.flh.com

IRON POINT ROAD APARTMENTS
FOLSOM, CA



Preliminary Parking Shade Plan March 16, 2022 | MR200320.00



REFERENCE NOTES SCHEDULE LOT 6

SYMBOL	DESCRIPTION
1	PROPOSED LANDSCAPING - SEE ARCHITECTURAL PLANS
2	PROPOSED FENCE ENCLOSURE - SEE ARCHITECTURAL PLANS
3	PROPOSED CHIMNEY - SEE ARCHITECTURAL PLANS
4	PROPOSED STAINING WALLS - SEE CIVIL PLANS
5	PROPOSED STORMWATER FEATURES - SEE CIVIL PLANS
6	PROPOSED FOOD SERVICE AREA
7	PROPOSED PICNIC AREA
8	PROPOSED DECK
9	PROPOSED SEATING AREA
10	PROPOSED DOG PARK
11	PROPOSED 6' OPEN METAL FENCE
12	PROPOSED 6' METAL PEDESTRIAN GATE
13	PROPOSED MINI ENTRY GATE
14	PROPOSED ADA GATE
15	PROPOSED SIGNAGE WALL WITH PLANTER
16	EXISTING ON-SITE TREE TO REMAIN
17	EXISTING LANDSCAPE TO BE REINFORCED
18	EXISTING RETAINING WALL TO REMAIN

GRUPE



IRON POINT ROAD APARTMENTS - LOT 6
FOLSOM, CA.

Preliminary Landscape Plan

NOV 16th, 2021 | MR200320.00



PLANT SCHEDULE LOT 6

TREE	SIZE	RELATIONSHIP	COMMON NAME	SPR	WTR/LEAV	SHR
(Symbol)	12' CAL.	ACQUEDUCANUS	ACQUEDUCANUS	24"	10'	6'
(Symbol)	12' CAL.	REBUTUS GRECO	COMPACTA	24"	10'	6'
(Symbol)	18' CAL.	INDICATA	BALDARD	24"	10'	6'
(Symbol)	18' CAL.	LAURESTRIATA	FUCCIFORMIS	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'
(Symbol)	18' CAL.	GLAUCOPHYLLA	BI-LOBE	24"	10'	6'

QUANTITY	DESCRIPTION	UNIT	PRICE	TOTAL
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
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1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
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1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100
1	PROPOSED / SCREEN SHRUBS - EVERGREEN, 1/2" DBH @ 6'-4" HIG, FOR PLEASANT PARKING AND WALKWAY UNDERSTORY	EA	100	100

TREE REMOVAL TABLE

PROPOSED TREE DBH (IN)	TOTAL NUMBER OF TREES TO BE REMOVED	TOTAL INCHES REMOVED
2" - 12" DBH	2248	326.7
14" DBH AND GREATER	1	2437.50
TOTAL INCHES REMOVED		326.7
TOTAL INCHES REMOVED		326.7

TREE MITIGATION TABLE

PROPOSED TREE DBH (IN)	TOTAL NUMBER OF TREES TO BE REMOVED	TOTAL INCHES REMOVED	MITIGATION FEE (\$/INCH)	TOTAL MITIGATION FEE
2" - 12" DBH	2248	326.7	168.00	54,765.60
14" DBH AND GREATER	1	2437.50	23.00	56,062.50
TOTAL MITIGATION FEE				\$110,828.10

DESIGN STATEMENT

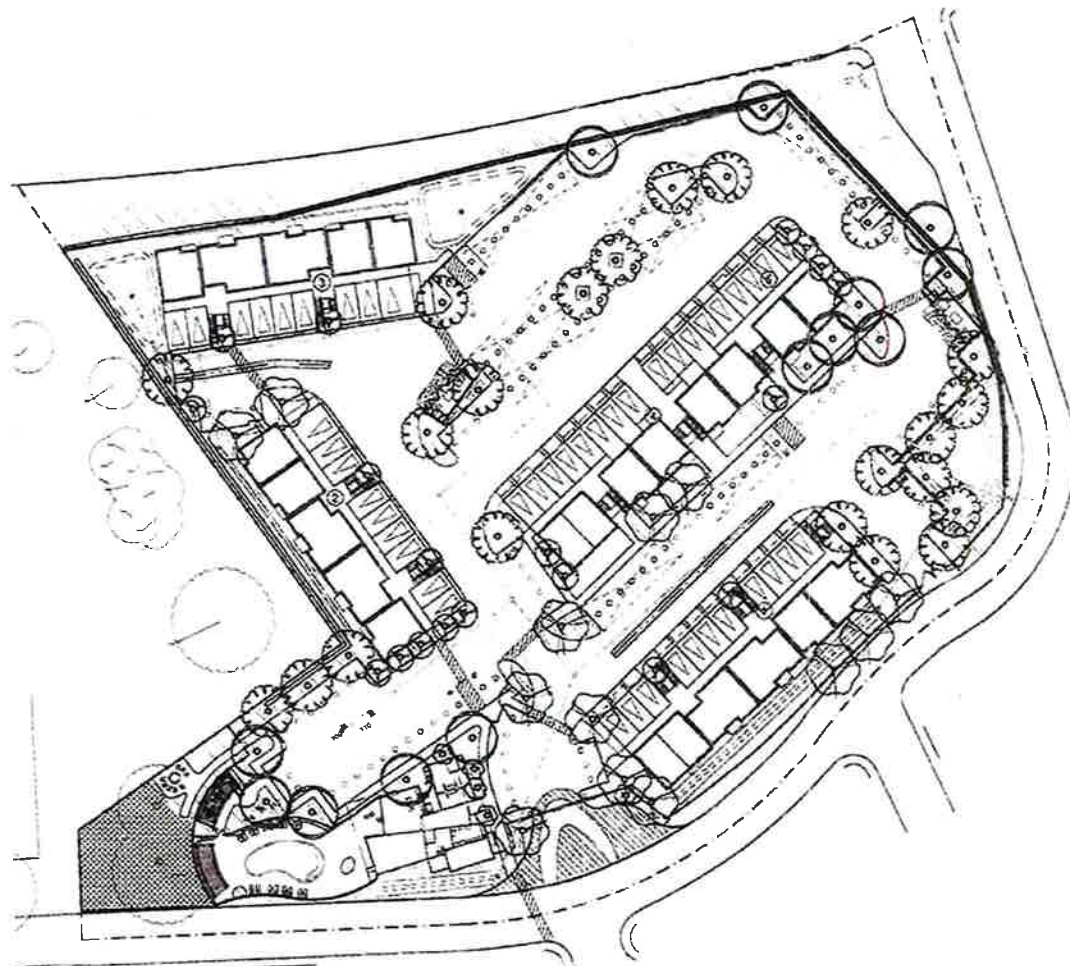
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GENERAL PLANTING NOTES

- 1. ALL PLANTING AREAS SHALL BE IRRIGATED WITH AN AUTOMATIC IRRIGATION SYSTEM THAT WILL BE COMPLIANT WITH THE STATE'S WATER EFFICIENT LANDSCAPE ORDINANCE.
- 2. ALL SHRUB AND GROUND COVER AREAS SHALL RECEIVE A 3" LAYER OF BARK CHIP MULCH TOP DRESSING.
- 3. GROUND COVERS SHALL NOT BE INSTALLED WITHIN A 4' RADIUS OF A TREE TRUNK.



IRON POINT ROAD APARTMENTS - LOT 6
 Preliminary Landscape Plan
 NOV 16th, 2021 | MR200320.00



SHADE STUDY CALCULATIONS
City of Folsom

DESCRIPTION	AMOUNT	PERCENTAGE	TOTAL
TOTAL PAVED PARKING AREA	62,500	100%	62,500
TOTAL SHADE ACQUIRED (sq. ft.)	13,740	22%	13,740
TOTAL SHADE PROVIDED	13,740	22%	13,740

SHADE STUDY CATEGORY	AMOUNT	PERCENTAGE	TOTAL
30' DIA TREES (1000)	362	0.6%	362
40' DIA TREES (1000)	722	1.2%	722
50' DIA TREES (1000)	405	0.7%	405
60' DIA TREES (1000)	246	0.4%	246
Subtotal	1735	2.8%	1735
40' DIA TREES (500)	1170	1.9%	1170
50' DIA TREES (500)	1338	2.1%	1338
60' DIA TREES (500)	1338	2.1%	1338
70' DIA TREES (500)	1338	2.1%	1338
80' DIA TREES (500)	1338	2.1%	1338
90' DIA TREES (500)	1338	2.1%	1338
100' DIA TREES (500)	1338	2.1%	1338
Subtotal	11700	18.9%	11700
40' DIA TREES (250)	1170	1.9%	1170
50' DIA TREES (250)	1338	2.1%	1338
60' DIA TREES (250)	1338	2.1%	1338
70' DIA TREES (250)	1338	2.1%	1338
80' DIA TREES (250)	1338	2.1%	1338
90' DIA TREES (250)	1338	2.1%	1338
100' DIA TREES (250)	1338	2.1%	1338
Subtotal	11700	18.9%	11700
TOTAL	13740	22%	13740

GRUPE



IRON POINT ROAD APARTMENTS - LOT 6
FOLSOM, CA

Preliminary Parking Shade

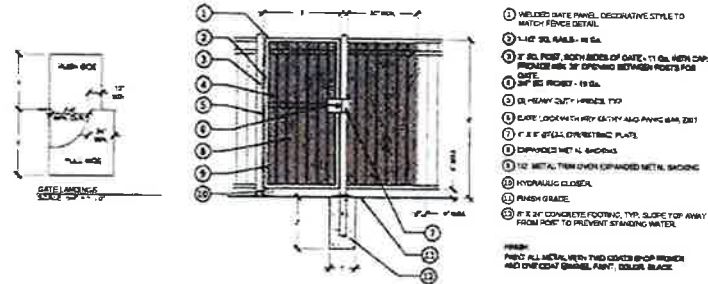


PL6



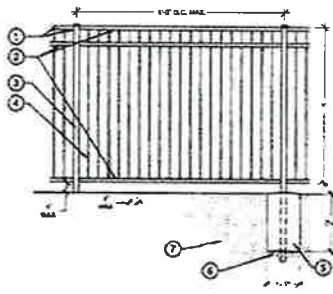
BSB DESIGN
BSBDESIGN.COM

NOV. 16th, 2021 | MR200320 00



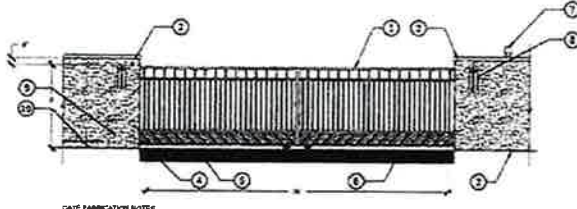
- 1 WELDED GATE PANEL, DECORATIVE STYLE TO MATCH FENCE DETAIL.
 - 2 1-1/2" SQ. RAILS - 18 GA.
 - 3 2" SQ. POST, BOTH SIDES OF GATE - 11 GA. WITH CAP PROVIDER 1/8" SP. SPACING BETWEEN POSTS FOR GATE.
 - 4 2" SQ. POST - 18 GA.
 - 5 0.1" HEAVY DUTY - FINISH, TYP.
 - 6 CLARE LOCK WITH KEY CATCH AND PIVOT BAR Z61
 - 7 1" X 1/2" STEEL EXPANDED PLATE.
 - 8 EXPANDED METAL 6"X6"
 - 9 1/2" METAL TRIM OVER EXPANDED METAL, SANDING
 - 10 HYDRAULIC CLOSER.
 - 11 FINISH GRADE.
 - 12 6" X 24" CONCRETE FOOTING, TYP. SLOPE TOP AWAY FROM POST TO PREVENT STANDING WATER.
- FINISH:
PAINT ALL METAL WITH TWO COATS SHOP PRIMER AND ONE COAT ENAMEL, PAINT COLOR: BLACK.

3 DECORATIVE METAL PEDESTRIAN GATE - 8'-0" HEIGHT



- 1 WELDED PANEL.
- 2 2" SQ. RAIL - 14 GA.
- 3 2" SQ. POST - 13 GA. WITH CAP PLATE NORTH & S.C. 1/2" DIA. X 1/2" H. S.A.
- 4 1/2" X 3/4" CONCRETE FOR DRG. TYP. SLOPE TOP AWAY FROM RIGHT TO PREVENT STANDING WATER.
- 5 2" ABSOLUTE BALK, CONSTRUCTED.
- 6 CONSTRUCTED SUBGRADE TO 10% MIN. SLOPE.
- 7 2" ABSOLUTE BALK WITH TWO COATS SHOP PRIMER AND ONE COAT ENAMEL, PAINT COLOR: BLACK.

2 TUBULAR METAL FENCE



- 1 TUBULAR STEEL GATES, TYP. OF 0.1 MINIMUM 1-1/2" SQ. FINISHING WITH 1/2" SQ. Pickets.
- 2 6" SQ. WALL WITH SECTION BETWEEN TO MATCH BUILDING.
- 3 1/2" DIA. METAL RAIL WITH 1/2" DIA. PICKETS.
- 4 CONCRETE CURB.
- 5 STREET FINISH SURFACE. SEE CIVIL PLANS FOR SECTION.
- 6 PROVIDE 1/2" GROOVE WHEELER AND TRACK INST. ALL PER MANUFACTURER'S SPEC.
- 7 PROVIDE ELECTRONIC OVER RISE SWITCH PER FIRE DEPARTMENT REQUIREMENTS.
- 8 1/2" DIA. METAL RAIL WITH 1/2" DIA. PICKETS.
- 9 1/2" DIA. METAL RAIL WITH 1/2" DIA. PICKETS.
- 10 CONCRETE DATE TRACK FOR ALIDE DATE. TOP OF CONCRETE DATE TRACK TO BE AT FINISH GRADE.

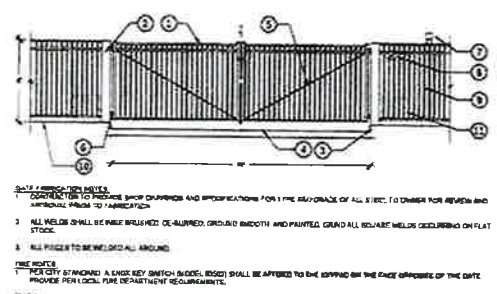
GATE FABRICATION NOTES:
CONTRACTOR TO PROVIDE SHOP DRAWINGS AND SPECIFICATIONS FOR TYPE AND GRADE OF ALL IRON TO GAINER FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.

ALL WELDS SHALL BE WELD BUILT PER LOCAL FIRE DEPARTMENT REQUIREMENTS AND PAINTED. GRIND ALL SQUARE WELDS OCCURRING ON FLAT STEEL.

ALL PICKETS TO BE WELDED ALL AROUND.

FIRE NOTES:
PER CITY STANDARD, A LENS KEY SWITCH (MODEL 8002) SHALL BE AFFIXED TO THE SETTING ON THE FACE OPPOSITE OF THE GATE. PROVIDE PER LOCAL FIRE DEPARTMENT REQUIREMENTS.

1 ENTRY GATE - SLIDING



- 1 TUBULAR STEEL GATES, TYP. OF 0.1 MINIMUM 1-1/2" SQ. FINISHING WITH 1/2" SQ. PICKETS.
- 2 6" SQ. WALL WITH SECTION BETWEEN TO MATCH BUILDING.
- 3 1/2" DIA. METAL RAIL WITH 1/2" DIA. PICKETS.
- 4 CONCRETE CURB.
- 5 STREET FINISH SURFACE.
- 6 PROVIDE 1/2" GROOVE WHEELER AND TRACK INST. ALL PER MANUFACTURER'S SPEC.
- 7 PROVIDE ELECTRONIC OVER RISE SWITCH PER FIRE DEPARTMENT REQUIREMENTS.
- 8 1/2" DIA. METAL RAIL WITH 1/2" DIA. PICKETS.
- 9 1/2" DIA. METAL RAIL WITH 1/2" DIA. PICKETS.
- 10 CONCRETE DATE TRACK FOR ALIDE DATE. TOP OF CONCRETE DATE TRACK TO BE AT FINISH GRADE.
- 11 FINISH GRADE.

4 EVA GATE - SLIDING

GATE FABRICATION NOTES:
CONTRACTOR TO PROVIDE SHOP DRAWINGS AND SPECIFICATIONS FOR TYPE AND GRADE OF ALL IRON TO GAINER FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.

ALL WELDS SHALL BE WELD BUILT PER LOCAL FIRE DEPARTMENT REQUIREMENTS AND PAINTED. GRIND ALL SQUARE WELDS OCCURRING ON FLAT STEEL.


ALL PICKETS TO BE WELDED ALL AROUND.

FIRE NOTES:
PER CITY STANDARD, A LENS KEY SWITCH (MODEL 8002) SHALL BE AFFIXED TO THE SETTING ON THE FACE OPPOSITE OF THE GATE. PROVIDE PER LOCAL FIRE DEPARTMENT REQUIREMENTS.


FINISH:
PAINT ALL METAL WITH TWO COATS SHOP PRIMER AND ONE COAT ENAMEL, PAINT COLOR: BLACK.




This drawing is preliminary and subject to all local codes and regulations. It may be altered without notice. Please refer to the preliminary site plan and landscape site plan for more information. All dimensions are in feet unless otherwise noted. All dimensions are to the centerline of the structure unless otherwise noted. All dimensions are to the centerline of the structure unless otherwise noted.



PL7



BSB DESIGN



FULMAN LEAMY LAND GROUP

 3100 MIDCOUNTRY PARK DRIVE, SUITE 100, FOLSOM, CA 95630

 (916) 437-1234 | info@fulmanleamy.com

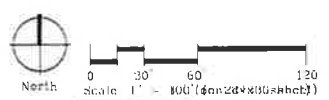
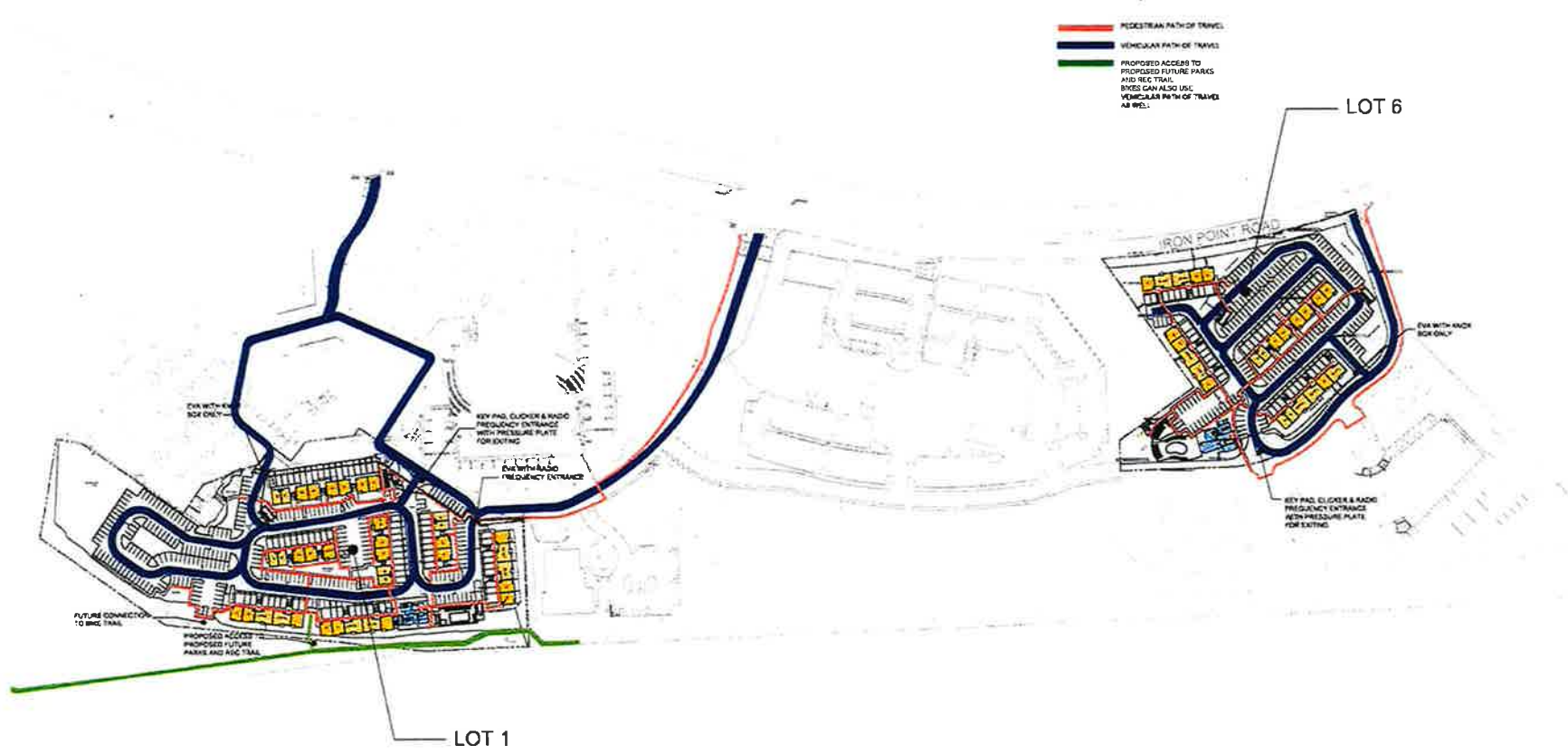
IRON POINT ROAD APARTMENTS

 FOLSOM CA.

Preliminary Landscape Details NOV 16th, 2021 | MR200320.00

Attachment No. 13

Preliminary Access and Circulation Plan, dated November 16, 2021



GRUPE

COLE PARTNERS
DEVELOPMENT COMPANY

OVERALL CIRCULATION SITE PLAN IRON POINT ROAD APARTMENTS

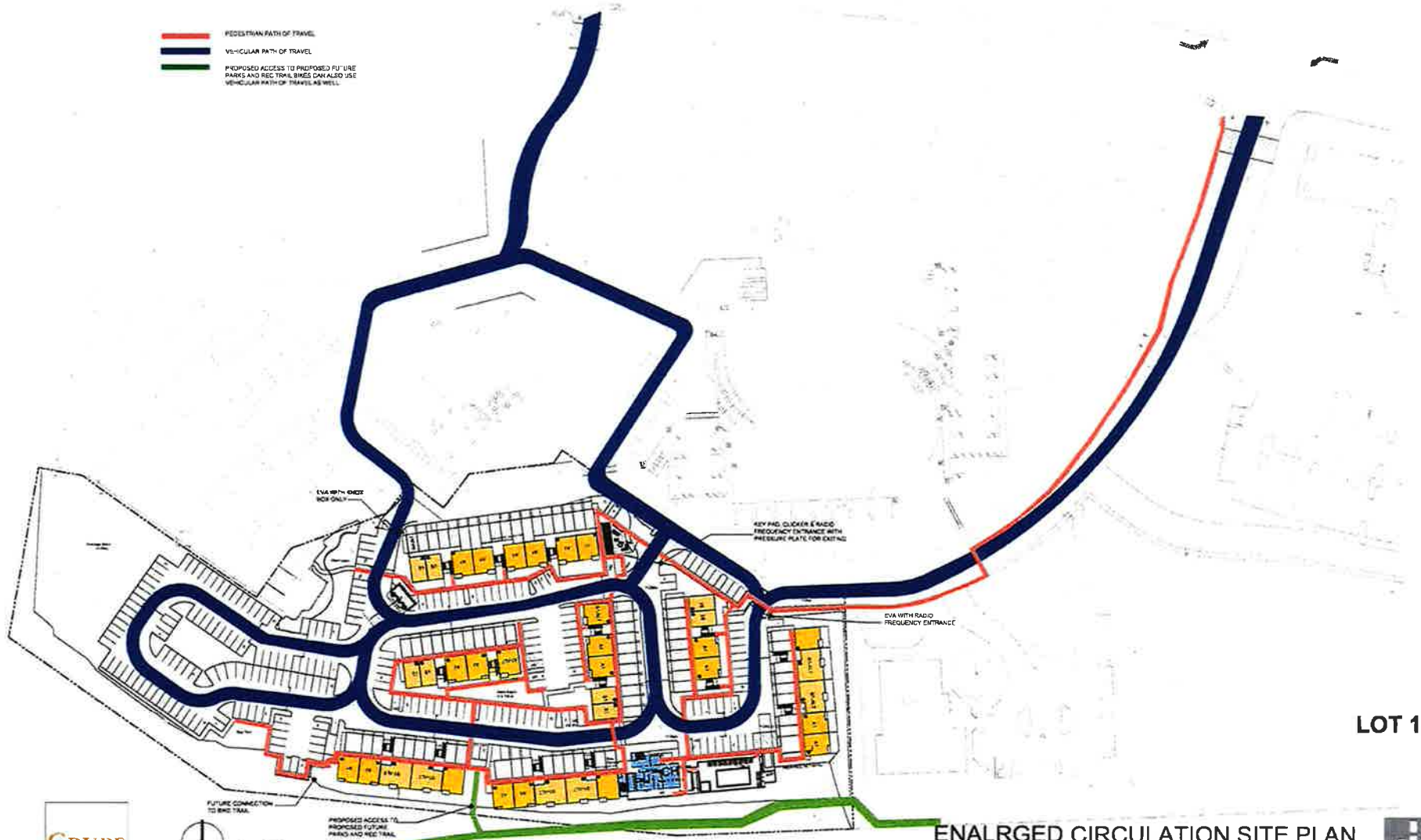
FOI SOM, CA



This plan, its contents and all drawings are the property of Grupe and Cole Partners, Inc. and are hereby design-build with the design considerations. It is applicable only to the project described herein. All other uses are subject to the design of BSB Design, Inc.

FEBRUARY 8, 2022 | MR200320

 PEDESTRIAN PATH OF TRAVEL
 VEHICULAR PATH OF TRAVEL
 PROPOSED ACCESS TO PROPOSED FUTURE PARKS AND REC TRAIL BIKES CAN ALSO USE VEHICULAR PATH OF TRAVEL AS WELL



LOT 1

GRUPE

COLE PARTNERS DEVELOPMENT COMPANY



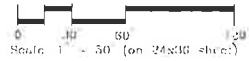
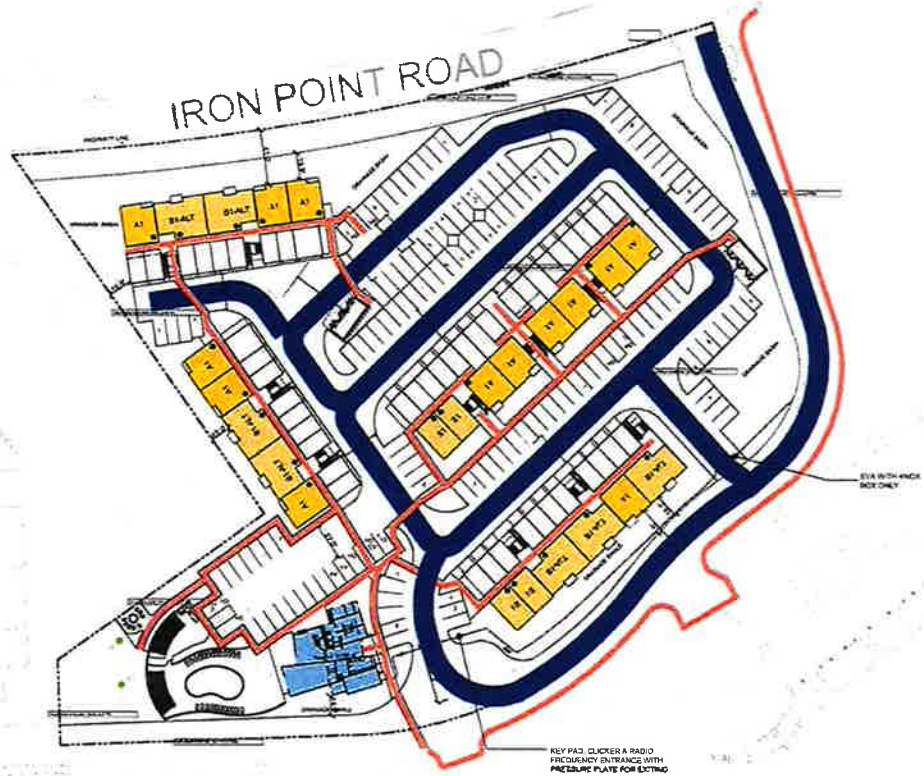
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ENLARGED CIRCULATION SITE PLAN
IRON POINT ROAD APARTMENTS
 FOLSOM, CA



BSB DESIGN

FEBRUARY 8, 2022 | MR200320



- PEDESTRIAN PATH OF TRAVEL
- VEHICULAR PATH OF TRAVEL
- PROPOSED ACCESS TO PROPOSED FUTURE PARKS AND RECREATION AREAS AS WELL AS VEHICULAR PATH OF TRAVEL AS WELL

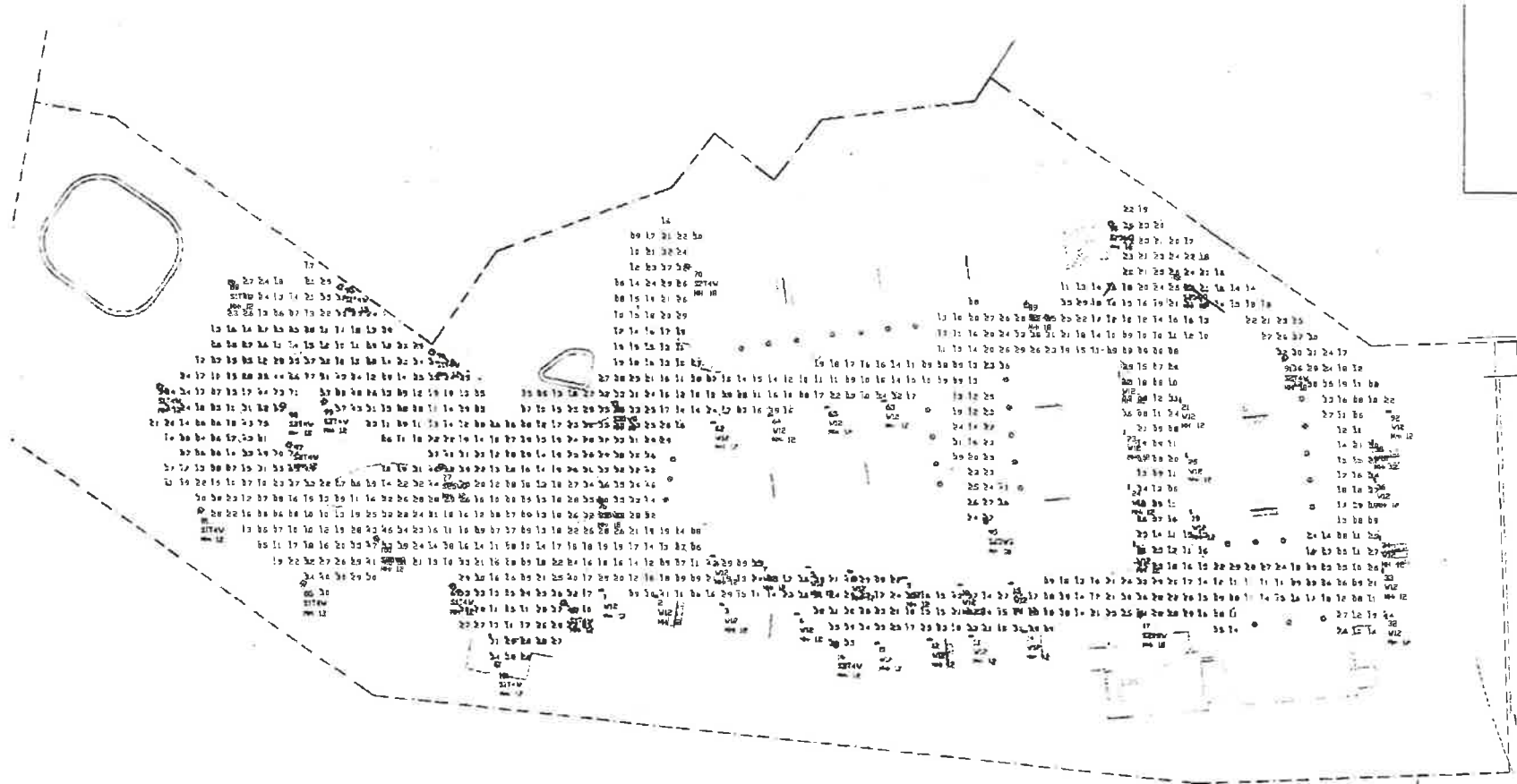
LOT 6
ENLARGED CIRCULATION SITE PLAN
IRON POINT ROAD APARTMENTS
 FOLSOM, CA



FEBRUARY 8, 2022 | MR200320

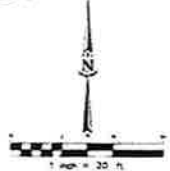
Attachment No. 14

Preliminary Lighting Plan and Details, dated November 16, 2021



Station	Lat	Long	Dist	Bearing	Remarks
1	34° 12' 30"	121° 15' 00"	100.00	S 77° 45' 00" W	Corner of Lot 1
2	34° 00' 00"	121° 15' 00"	100.00	S 77° 45' 00" W	Corner of Lot 1
3	34° 00' 00"	121° 15' 00"	100.00	S 77° 45' 00" W	Corner of Lot 1

Station	Lat	Long	Dist	Bearing	Remarks
1	34° 12' 30"	121° 15' 00"	100.00	S 77° 45' 00" W	Corner of Lot 1
2	34° 00' 00"	121° 15' 00"	100.00	S 77° 45' 00" W	Corner of Lot 1
3	34° 00' 00"	121° 15' 00"	100.00	S 77° 45' 00" W	Corner of Lot 1



GRUPE



PHOTOMETRIC PLAN

CE1

IRON POINT ROAD APARTMENTS - LOT 1
FOLSOM, CA.

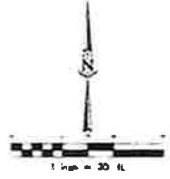


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Layer	Color	Line Style	Width	Dash	Symbol	Color
Boundary	Red	Solid	2.00			Red
Structure	Blue	Solid	1.00			Blue

Code	Layer	Symbol	Color	Line Style	Width	Dash	Symbol	Description
100	Structure	Circle	Red	Solid	1.00			Structure - 100' DIA
101	Structure	Square	Red	Solid	1.00			Structure - 100' x 100'
102	Structure	Triangle	Red	Solid	1.00			Structure - 100' x 100'
103	Structure	Circle	Blue	Solid	1.00			Structure - 100' DIA
104	Structure	Square	Blue	Solid	1.00			Structure - 100' x 100'
105	Structure	Triangle	Blue	Solid	1.00			Structure - 100' x 100'



GAUPE

COLE PARTNERS
DEVELOPMENT COMPANY

PHOTOMETRIC PLAN **CE1**

IRON POINT ROAD APARTMENTS - LOT 6
FOLSOM, CA.

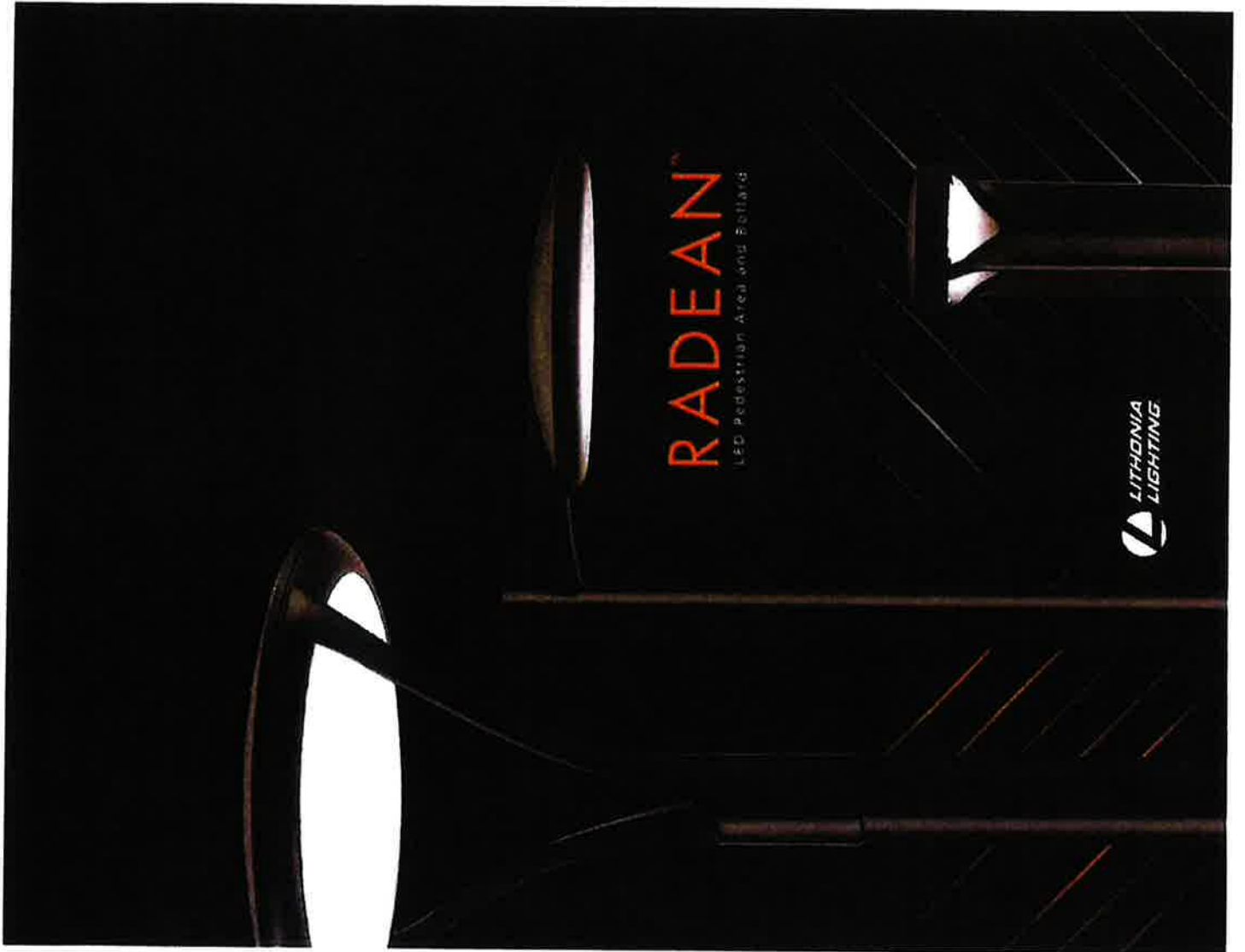
NOVEMBER 16, 2021 | MR200320.00



RADEAN BOLLARD

With an available in 1000mm height, the bollard is available in three sizes: 1000mm, 1500mm and 2000mm. The bollard is available in two finishes: polished stainless steel and powder coated steel. The bollard is available in two colors: chrome and black. The bollard is available in two finishes: polished stainless steel and powder coated steel. The bollard is available in two colors: chrome and black.







TECHNICAL INFORMATION

Motion Sensor

Non-pixel Controls

Warmer Color Palettes

Sharp Leaf Blades

DLC and DLC PREMIUM

Attachment No. 15

**Building Elevations, Floor Plans, and Details dated November 16,
2021**



FRONT ELEVATION



SOUTH AERIAL VIEW OF THE LOT 1 CLUBHOUSE



STREET VIEW

LOT 1 CLUBHOUSE/LEASING OFFICE

CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA

Scale: 1/8" = 1'-0"



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HIGHWAY 50 ELEVATION OF THE LOT 1 CLUBHOUSE



AERIAL VIEW FROM HIGHWAY 50 OF THE LOT 1 CLUBHOUSE



POOL ELEVATION OF THE LOT 1 CLUBHOUSE

LOT 1 CLUBHOUSE/LEASING OFFICE

CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS

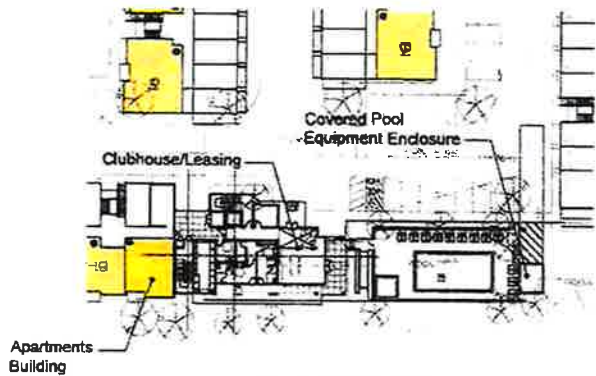
Scale: 1/8" = 1'-0"

FOLSOM, CA.

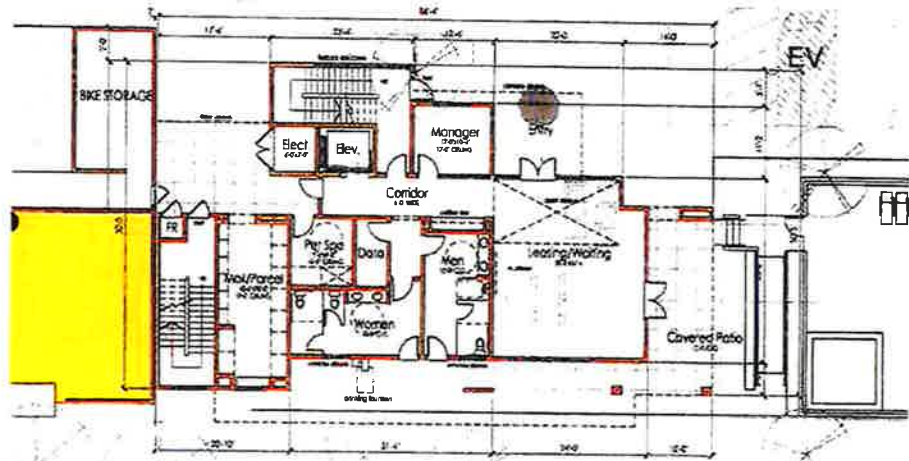


The drawings presented are illustrative of character and design intent only and are subject to change based upon final design considerations.
1. Applicable codes, structural and MEP design requirements, and plan / floor plate changes, etc. © 2019 BSB Design, Inc.

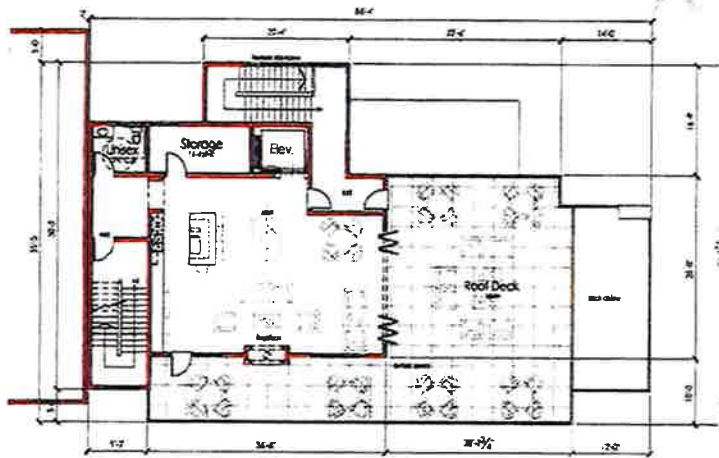
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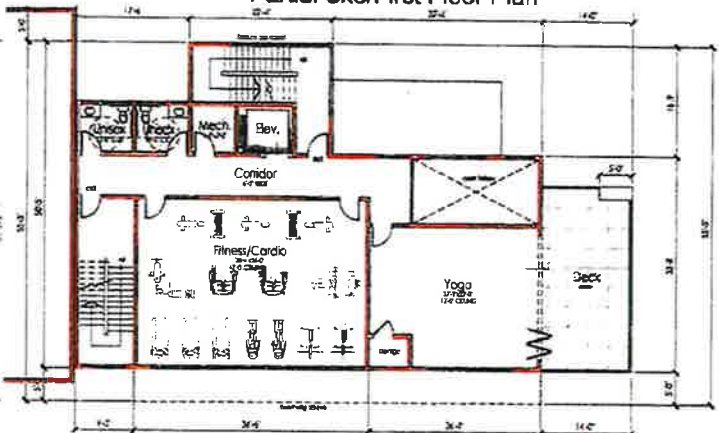
Partial Site Plan



Partial Site/First Floor Plan



Third/Roof Deck Plan



Second Floor Plan

Floor Area Calculation:

Main Clubhouse

First Floor:	2,598 s.f.
Second Floor:	2,377 s.f.
Third Floor:	1,807 s.f.
Total:	6,782 s.f.

Covered Patio:	412 s.f.
Balcony:	409 s.f.
Roof Deck:	1,395 s.f.

Lot 1 CLUBHOUSE

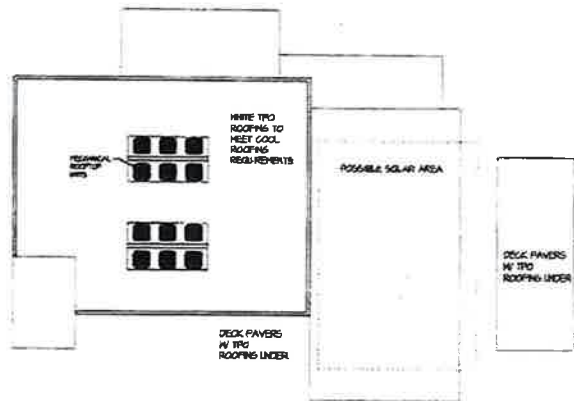
IRON POINT ROAD APARTMENTS
FOLSOM, CA

Scale: 1/8" = 1'-0"

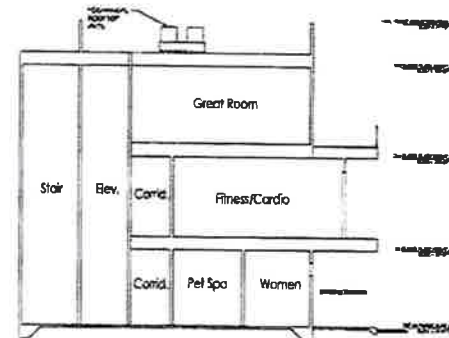


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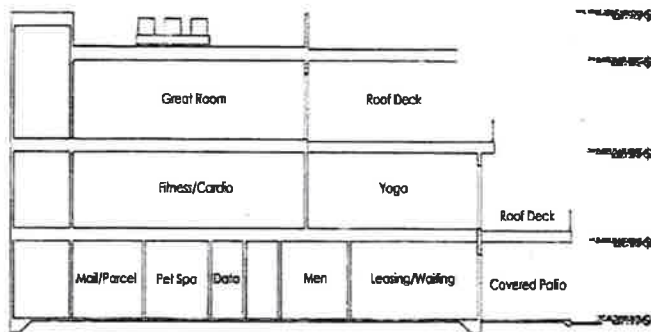
NOVEMBER 16, 2021 MR200320.00



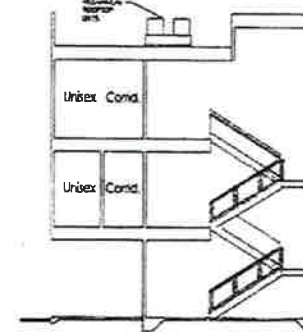
Roof Plan



Section B-B



Section A-A



Section C-C



Lot 1 CLUBHOUSE

Roof Plan and Sections
IRON POINT ROAD APARTMENTS

Scale: 1/8" = 1'-0"

FOLSOM, CA.

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Rear Elevation



Front Elevation



Left Elevation



Right Elevation



Perspective



Perspective

LOT 6 / CLUBHOUSE/LEASING OFFICE

Scale: 1/8" = 1'-0"

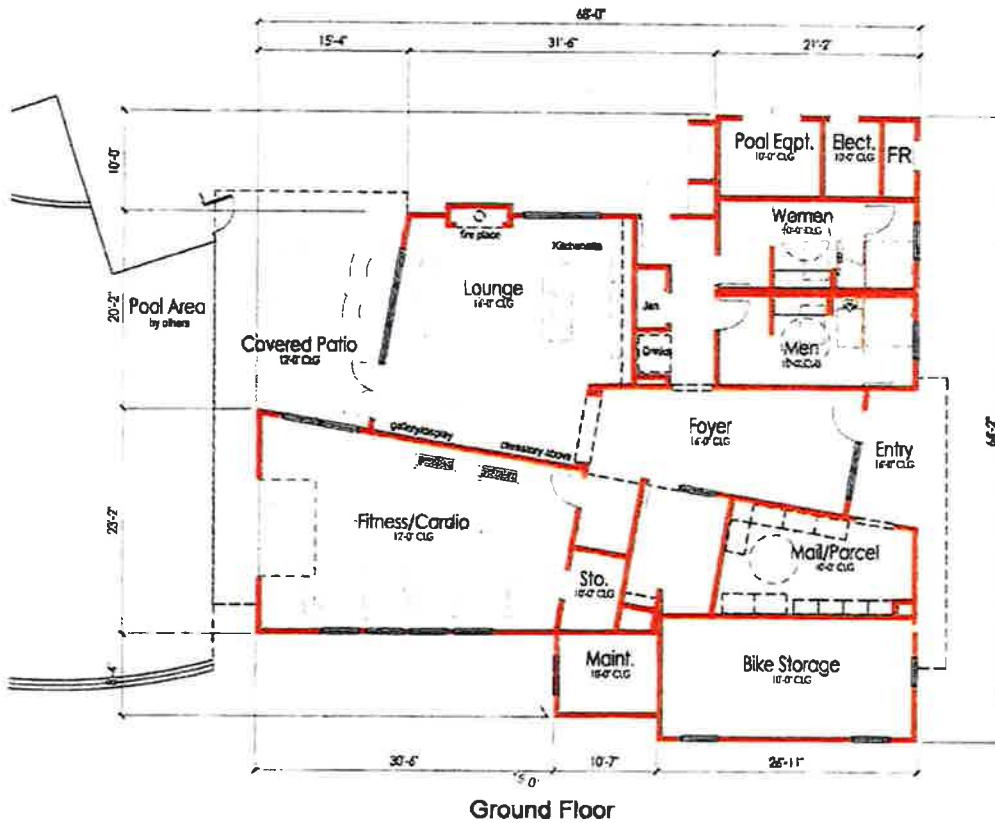
CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA.



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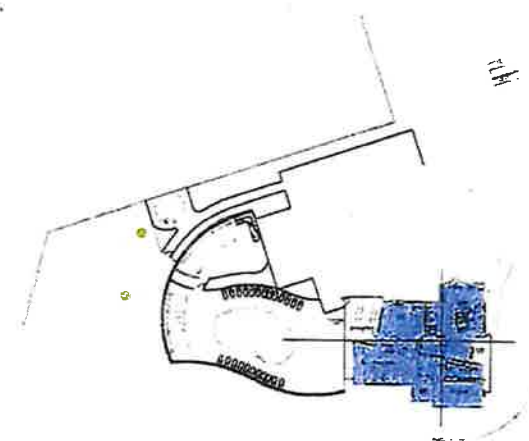
NOVEMBER 16, 2021 MR200320 00





Ground Floor

Lot 6 CLUBHOUSE



Partial Site Plan

Clubhouse: 3,098 s.f. +/-
 Covered Patio: 502 s.f. +/-
 Total Gross: 3,627 s.f. +/-

Scale: 3/16" = 1'-0"

IRON POINT ROAD APARTMENTS

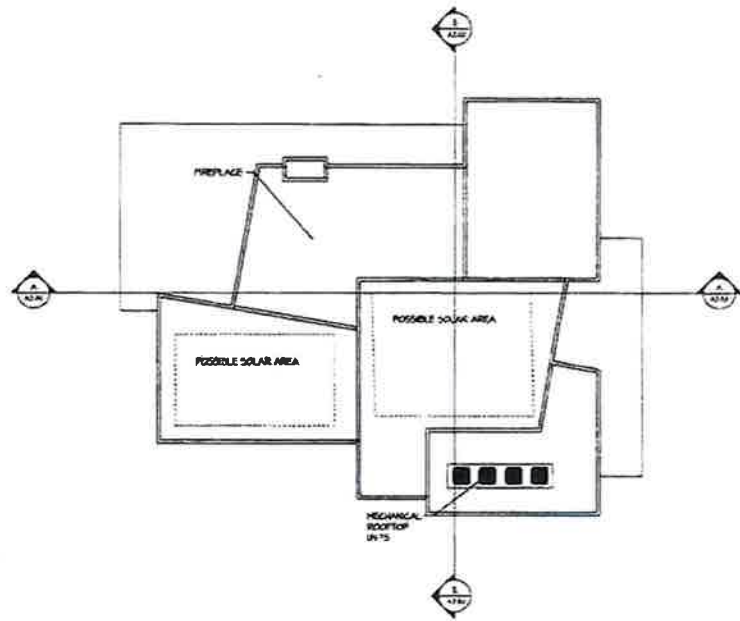
FOLSOM, CA.



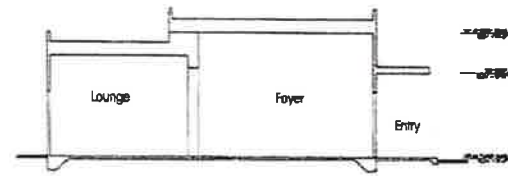
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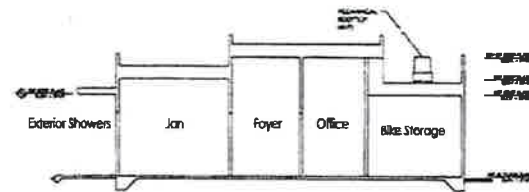
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Roof Plan



Section A-A



Section B-B

Lot 6 CLUBHOUSE

GRUPE

COLE PARTNERS DEVELOPMENT COMPANY

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Scale: 1/8" = 1'-0"
Roof Plan and Sections
IRON POINT ROAD APARTMENTS
FOLSOM, CA

NOVEMBER 16, 2021 MR200320 00





Front Elevation



Perspective



Right Elevation



BUILDING - A1 / 32PLEX

Scale: 1/8" = 1'-0"
CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA



NOVEMBER 16, 2021 MR200320 00



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Rear Elevation



Perspective



Left Elevation



BUILDING - A1 / 32PLEX

Scale: 1/8" = 1'-0"

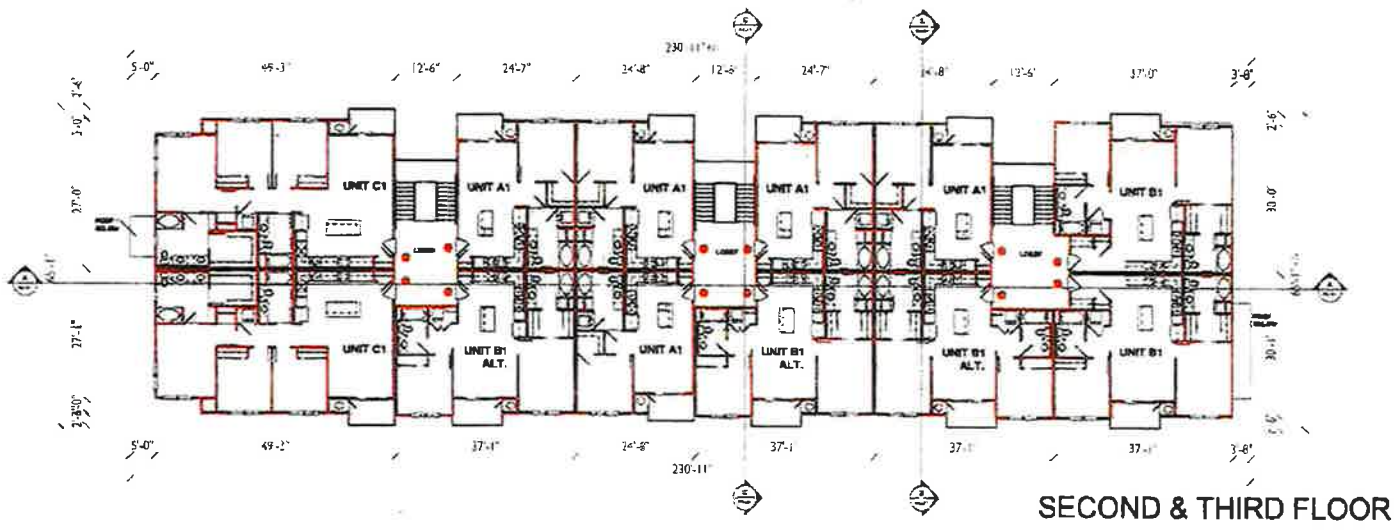
CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA



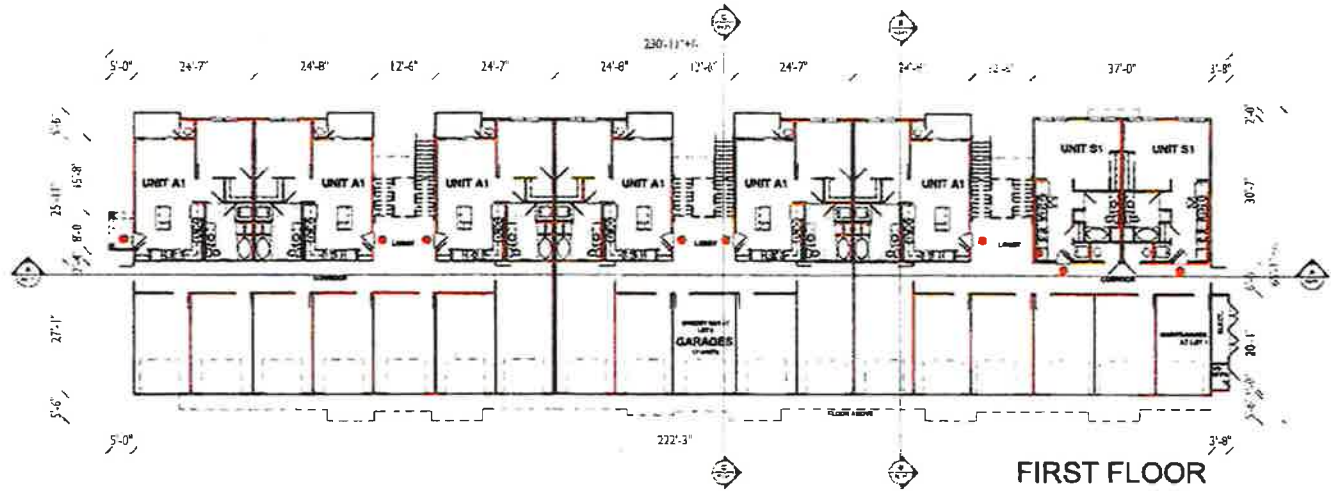
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SECOND & THIRD FLOOR



FIRST FLOOR

BUILDING A1 - 32 PLEX IRON POINT ROAD APARTMENTS

Overall Square Footage:
 1st FLOOR: 12,644 SF
 2nd FLOOR: 13,284 SF
 3rd FLOOR: 12,821 SF
TOTAL: 38,749

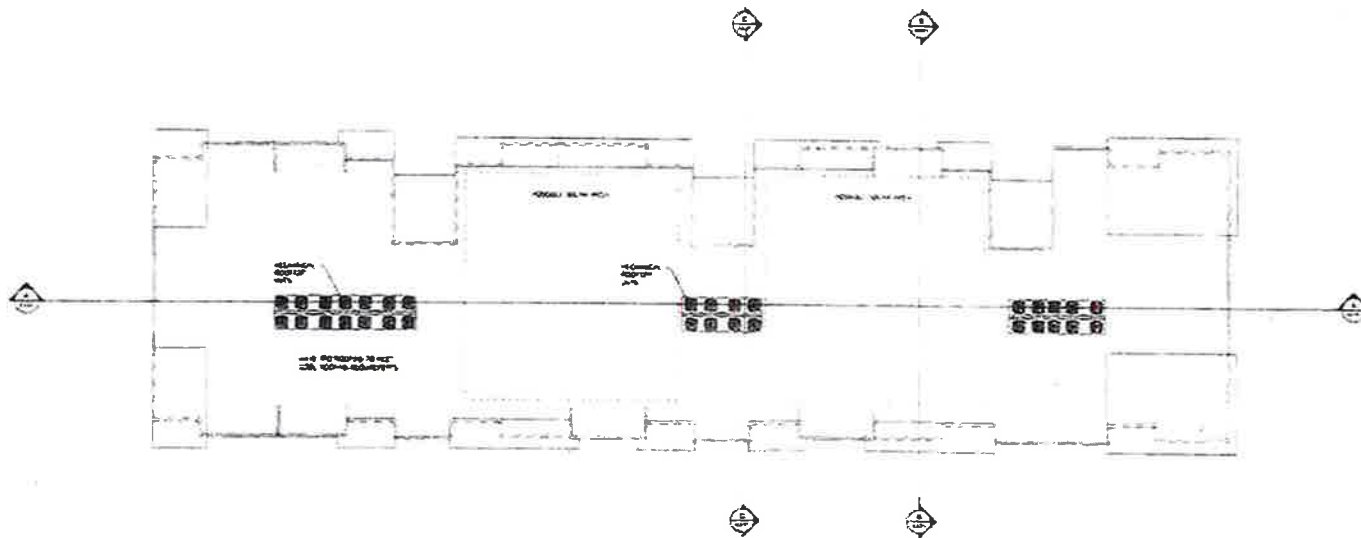
Unit Mix:
 S1: 2 units
 A1: 16 units
 B1: 4 units
 B1-Alt: 6 units
 C1: 4 units
Total: 32 units

Scale: 3/32" = 1'-0"
 FOLSOM, CA.



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ROOF PLAN

BUILDING A1 - 32 PLEX

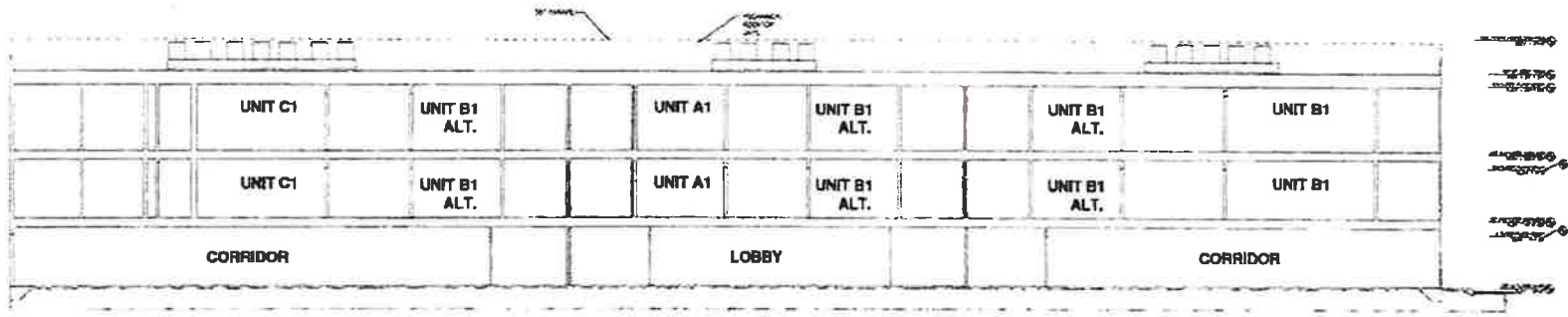
IRON POINT ROAD APARTMENTS
FOLSOM, CA.

Scale: 3/32" = 1'-0"

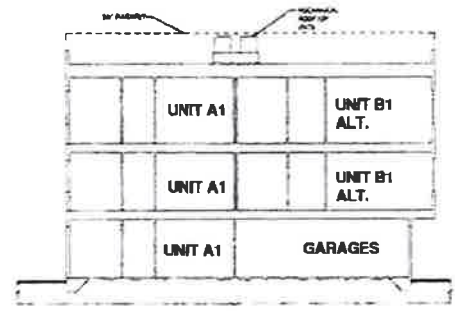


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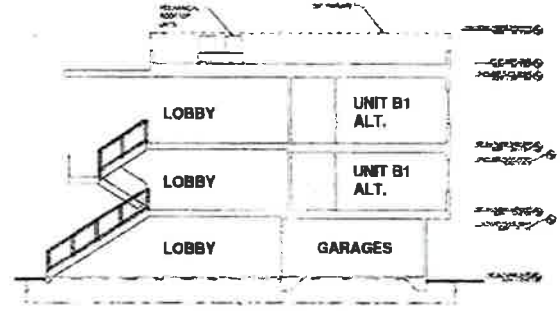
NOVEMBER 16, 2021 MR200320.00



SECTION A-A



SECTION B-B



SECTION C-C

BUILDING A1 - 32PLEX

IRON POINT ROAD APARTMENTS

Scale: 1/8" = 1'-0"
FOLSOM, CA.



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Front Elevation



Perspective



Left Elevation



BUILDING - B1 / 16PLEX

Scale: 1/8" = 1'-0"
CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA



NOVEMBER 16, 2021 MR200320 00



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Rear Elevation



Perspective



Right Elevation



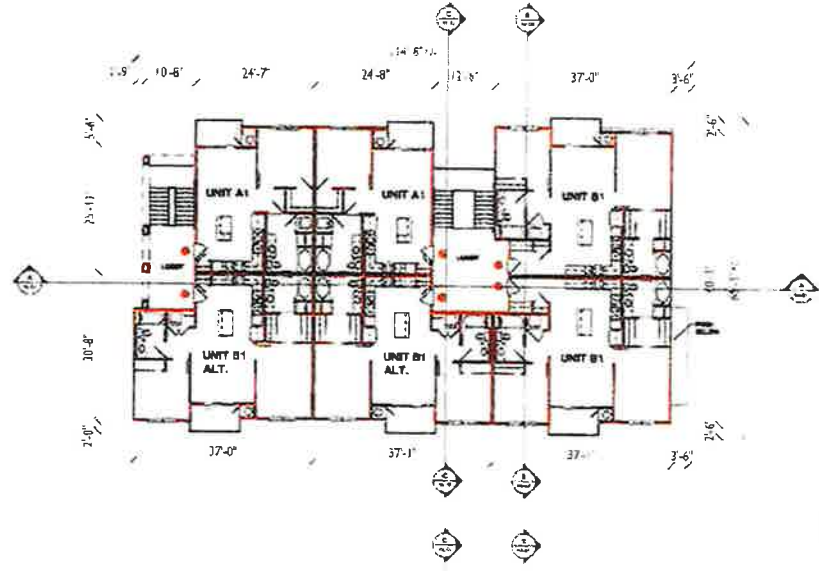
BUILDING - B1 / 16PLEX

Scale: 1/8" = 1'-0"
CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA

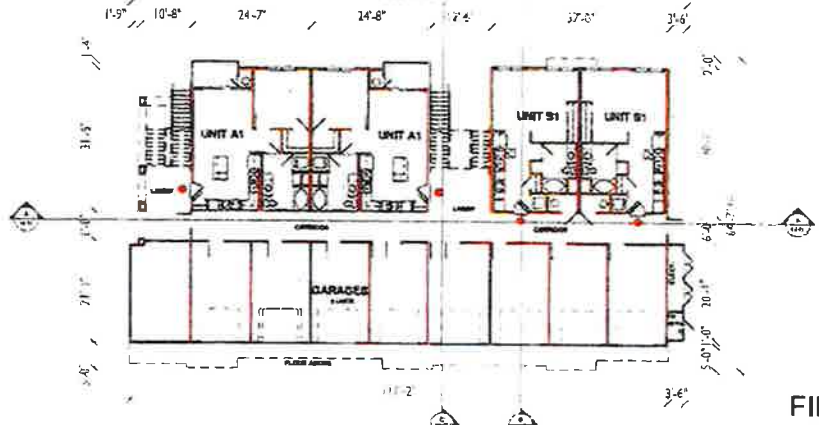


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SECOND & THIRD FLOOR



FIRST FLOOR

BUILDING B1 - 16PLEX IRON POINT ROAD APARTMENTS

Scale: 3/32" = 1'-0"
FOLSOM, CA

Overall Square Footage

1st FLOOR:	6,241 SF
2nd FLOOR:	6,307 SF
3rd FLOOR:	6,589 SF
TOTAL:	19,137 SF

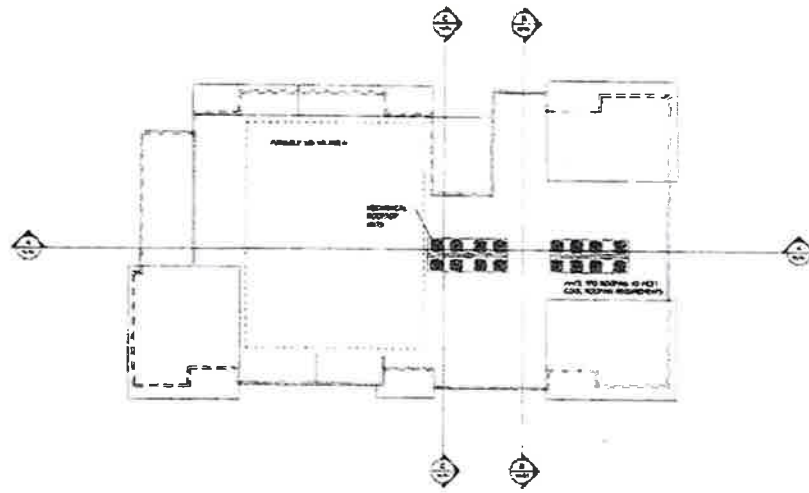
Unit Mix:

S1:	2 units
A1:	6 units
B1:	4 units
B1-Alt:	4 units
Total:	16 units



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ROOF PLAN

BUILDING B1 - 16PLEX

IRON POINT ROAD APARTMENTS

Scale: 3/32" = 1'-0"

FOLSOM, CA.

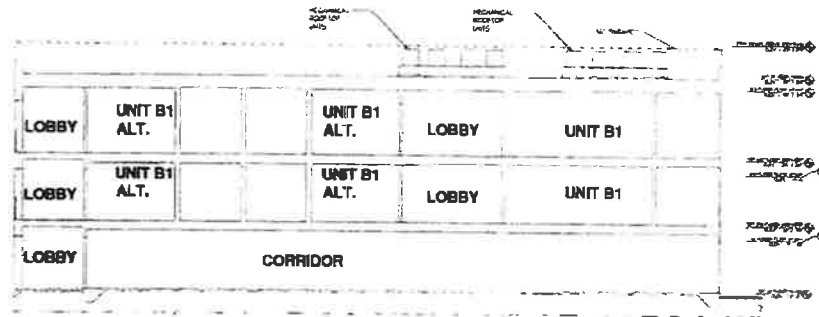


GRUPE

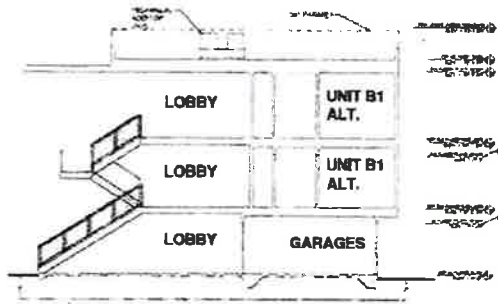


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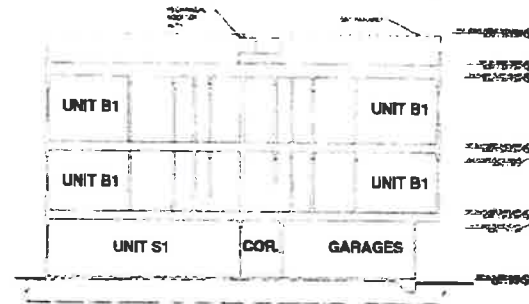
NOVEMBER 16, 2021 MR200320.00



SECTION A-A



SECTION B-B



SECTION C-C

BUILDING B1 - 16PLEX

IRON POINT ROAD APARTMENTS

Scale: 1/8" = 1'-0"
FOLSOM, CA



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Front Elevation



Perspective



Left Elevation



BUILDING - B2 / 21PLEX

CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA.

Scale: 1/8" = 1'-0"



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Rear Elevation



Perspective



Right Elevation

BUILDING - B2 / 21PLEX



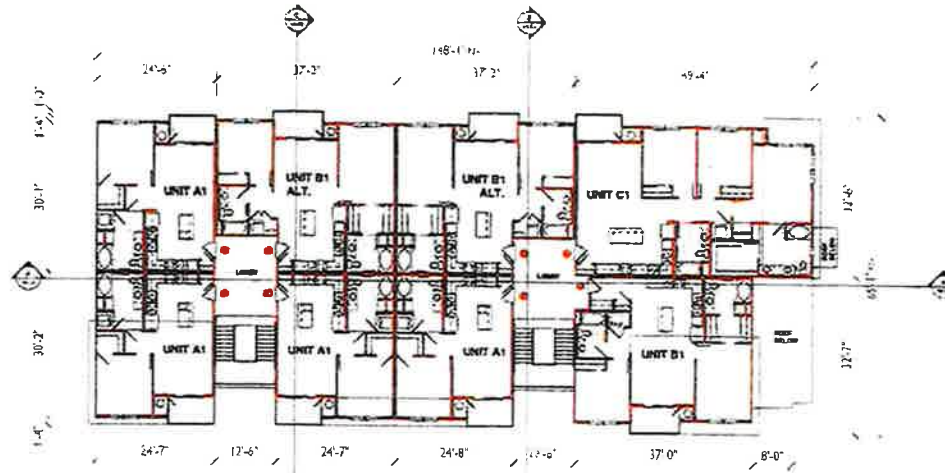
Scale: 1/8" = 1'-0"

CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA.

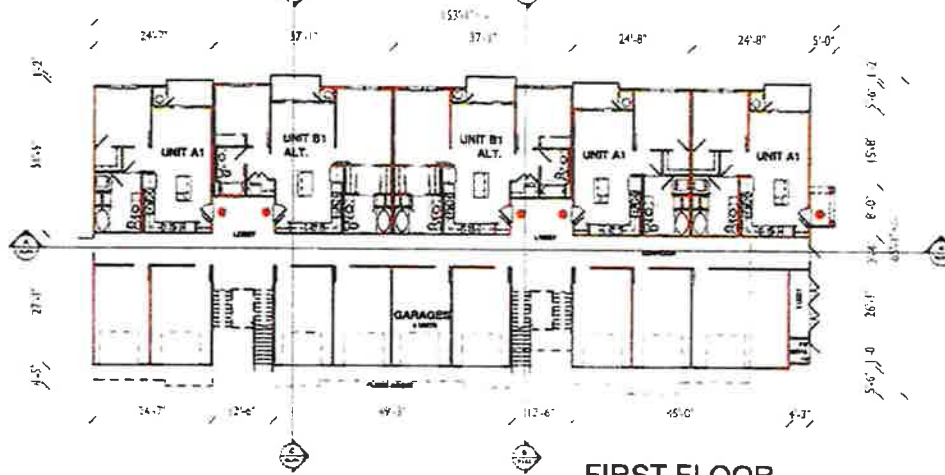


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SECOND & THIRD FLOOR



FIRST FLOOR

BUILDING B2 - 21PLEX IRON POINT ROAD APARTMENTS

Scale: 3/32" = 1'-0"
FOLSOM, CA

Overall Square Footage:
1st FLOOR: 8,532 SF
2nd FLOOR: 8,518 SF
3rd FLOOR: 8,210 SF
TOTAL: 25,260

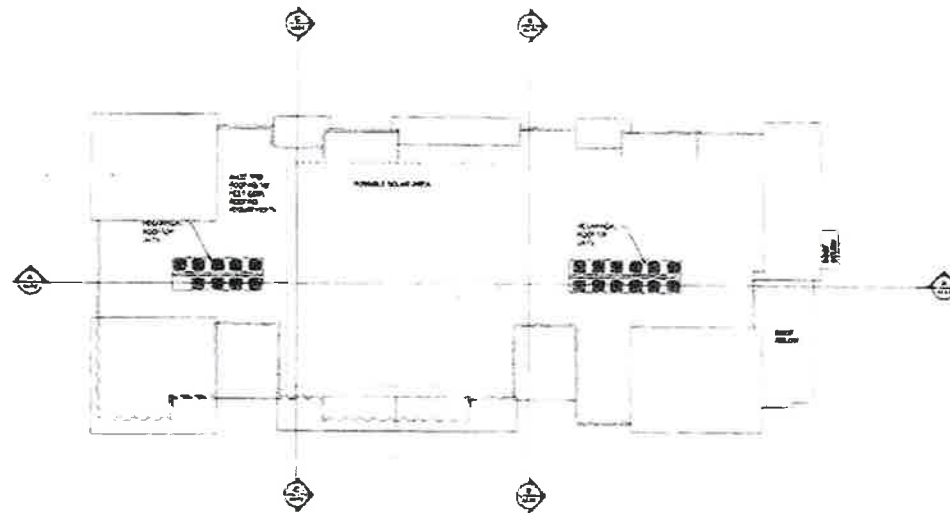
Unit Mix
A1: 11 units
B1: 2 units
B1-ALT: 6 units
C1: 2 units
Total: 21 units



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ROOF PLAN

GRUPE



BUILDING B2 - 21PLEX

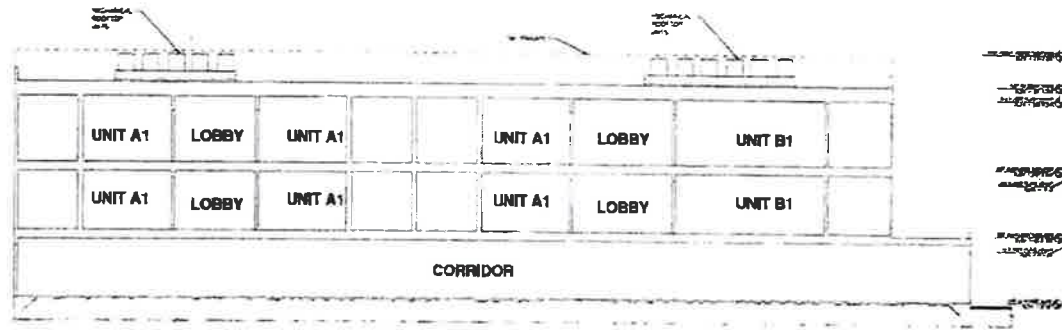
IRON POINT ROAD APARTMENTS

Scale: 3/32" = 1'-0"
FOLSOM, CA

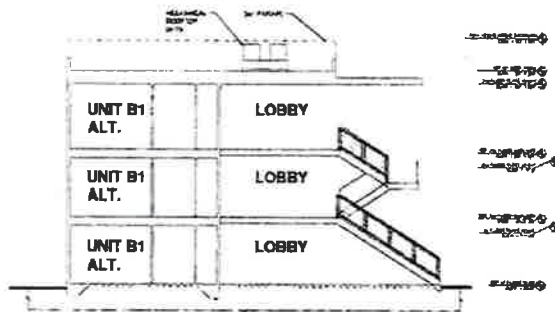


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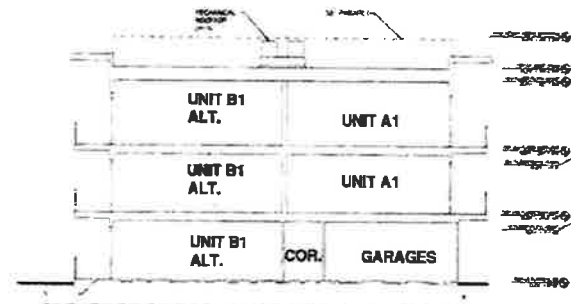
NOVEMBER 16, 2021 MR200320.00



SECTION A-A



SECTION B-B



SECTION C-C

BUILDING B2 - 21PLEX

IRON POINT ROAD APARTMENTS

Scale: 1/8" = 1'-0"

FOLSOM, CA.

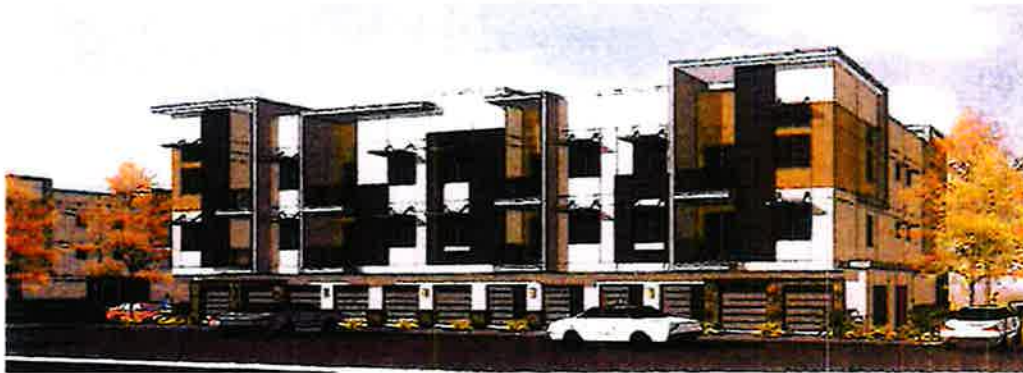


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Rear Elevation



Perspective



Right Elevation



BUILDING - B3 / 21PLEX

Scale: 1/8" = 1'-0"

CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA



NOVEMBER 16, 2021 MR200320.00



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Front Elevation



Perspective



Left Elevation



BUILDING - B3 / 21PLEX

Scale: 1/8" = 1'-0"

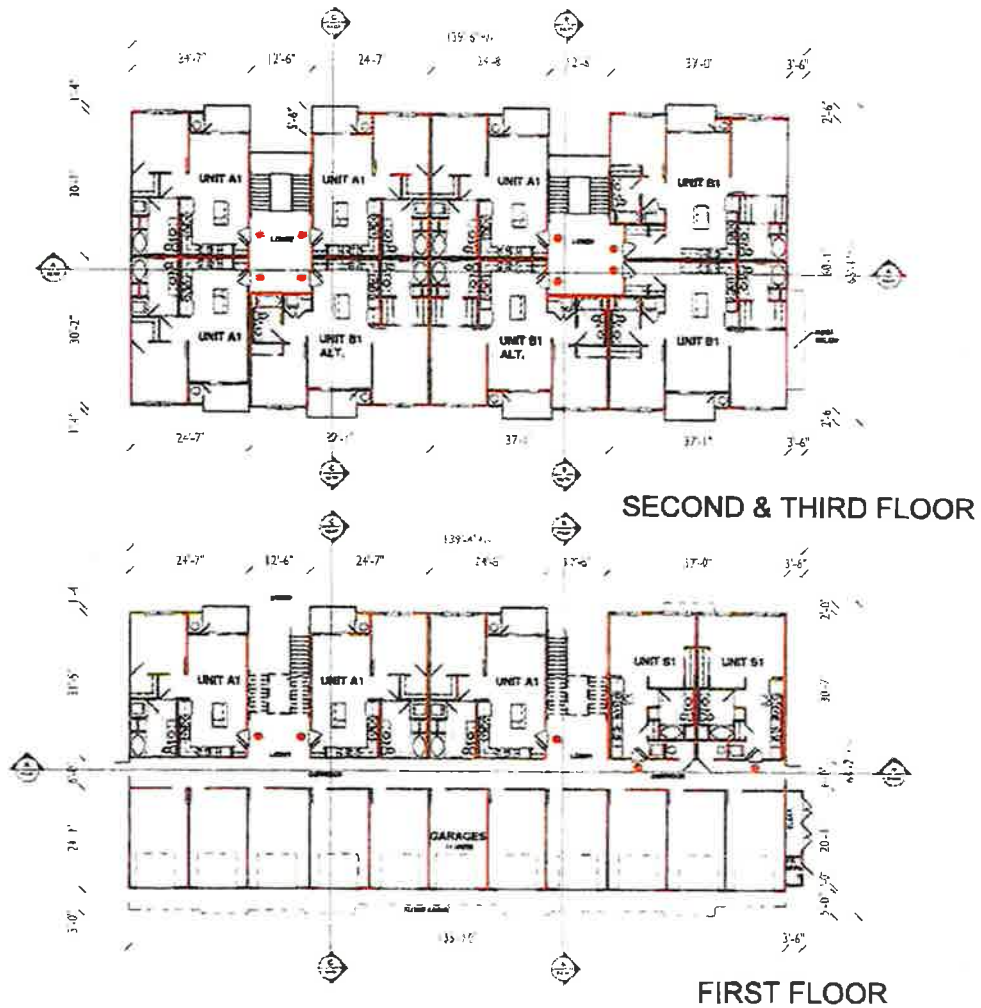
CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA



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Overall Square Footage
 1st FLOOR: 7,711 SF
 2nd FLOOR: 8,148 SF
 3rd FLOOR: 7,840 SF
 TOTAL: 23,699

Unit Mix
 S1: 2 units
 A1: 11 units
 B1: 4 units
 B1-Alt: 4 units
 Total: 21 units

Scale: 3/32" = 1'-0"

BUILDING B3 - 21PLEX IRON POINT ROAD APARTMENTS

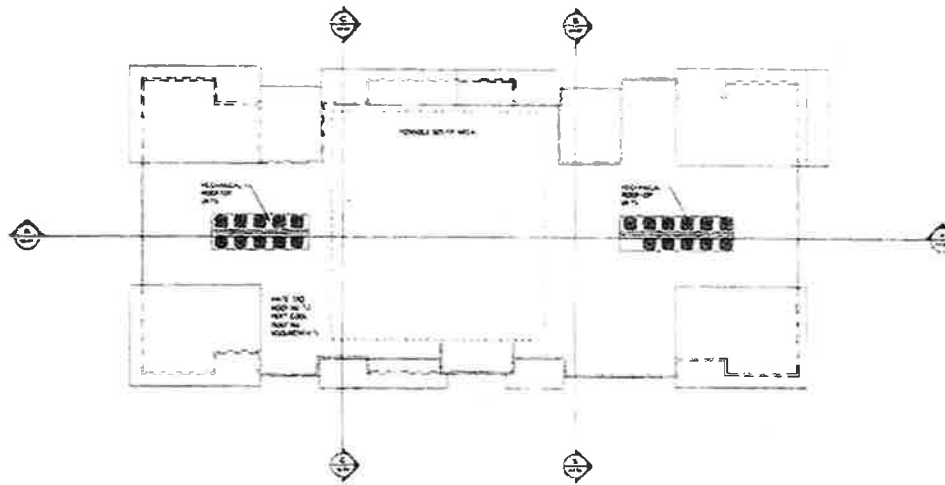
FOLSOM, CA.



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ROOF PLAN

GRUPE

BUILDING B3 - 21 PLEX

IRON POINT ROAD APARTMENTS

Scale: 3/32" = 1'-0"

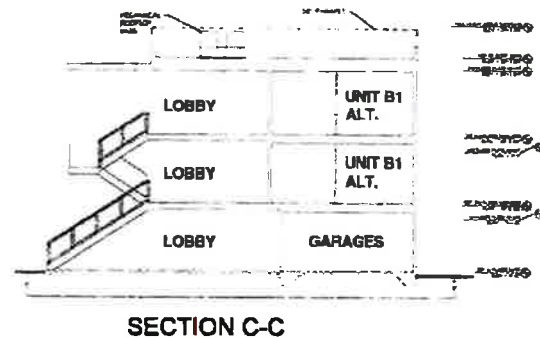
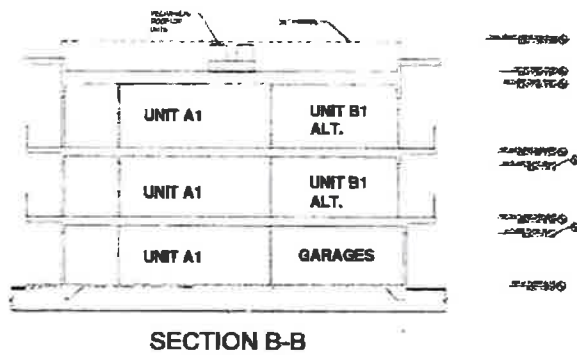
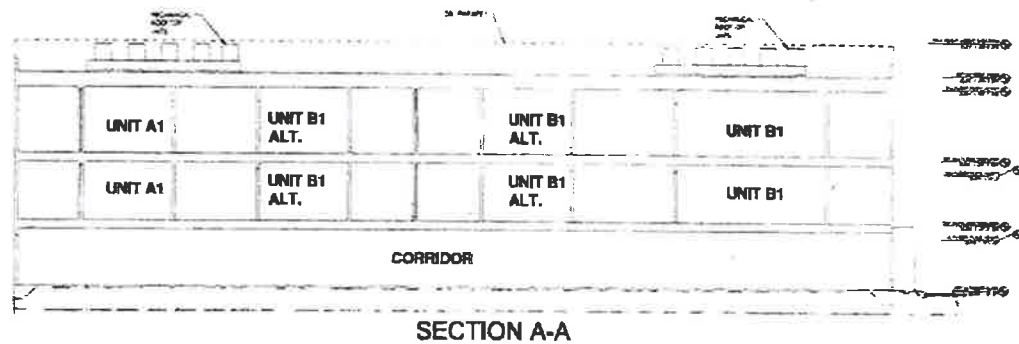
FOLSOM, CA.



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BUILDING B3 - 21PLEX

IRON POINT ROAD APARTMENTS

Scale: 1/8" = 1'-0"
FOLSOM, CA.



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NOVEMBER 16, 2021 MR200320 00





Front Elevation



Perspective



Left Elevation



BUILDING - B4 / 26PLEX

CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM CA



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NOVEMBER 18, 2021 MR200320 00



Rear Elevation



Perspective



Right Elevation



BUILDING - B4 / 26PLEX

CONCEPTUAL EXTERIOR ELEVATION
IRON POINT ROAD APARTMENTS
FOLSOM, CA

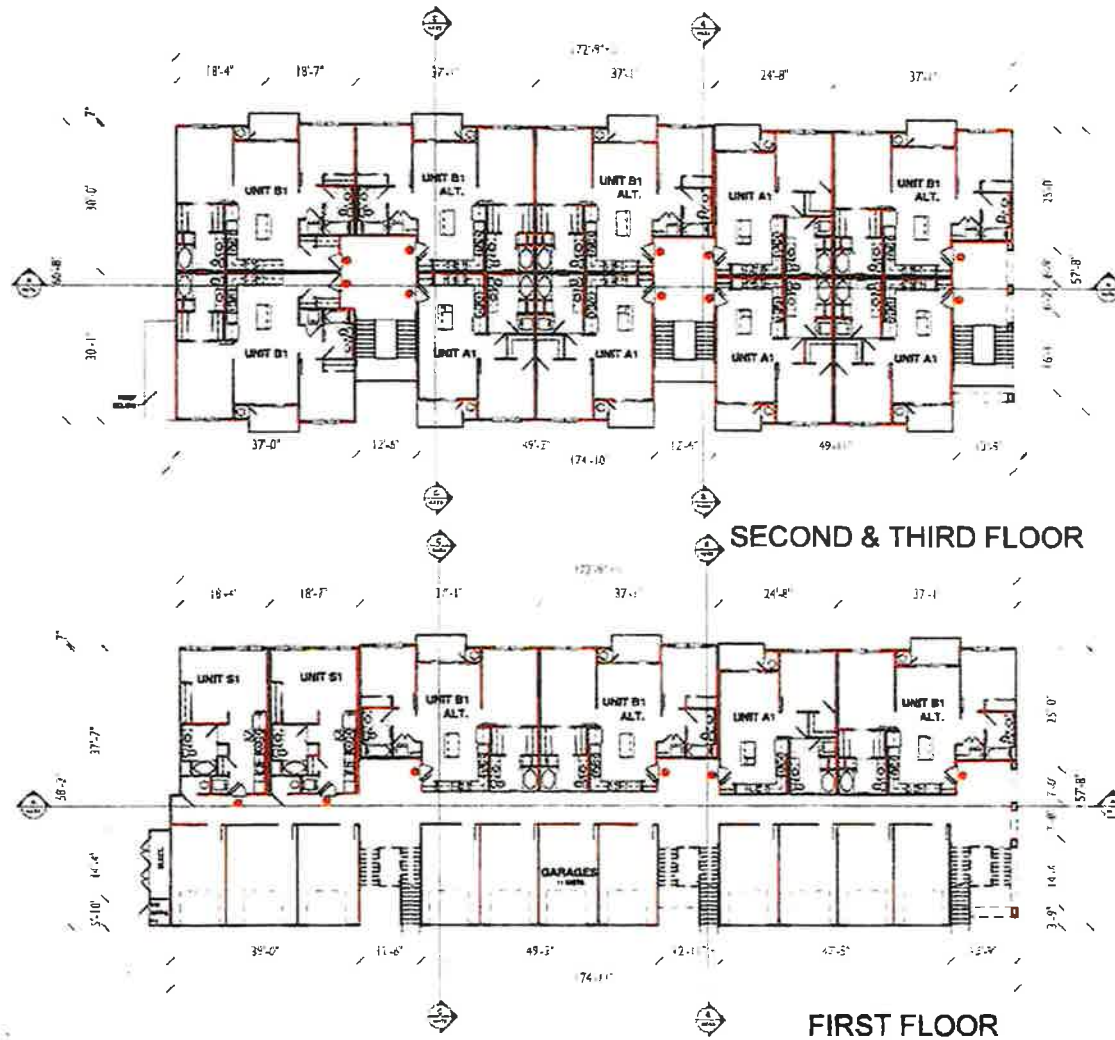


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DESIGN
BSBDESIGN.COM



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NOVEMBER 16, 2021 MR200320 00



Overall Square Footage:
 1st FLOOR: 9,942 SF
 2nd FLOOR: 9,801 SF
 3rd FLOOR: 9,801 SF
 TOTAL: 29,544

Unit Mix:
 S1: 2 units
 A1: 11 units
 B1: 4 units
 B1-Alt: 9 units
 Total: 26 units

BUILDING B4 - 26PLEX **FIRST FLOOR** **IRON POINT ROAD APARTMENTS**
 FOLSOM CA

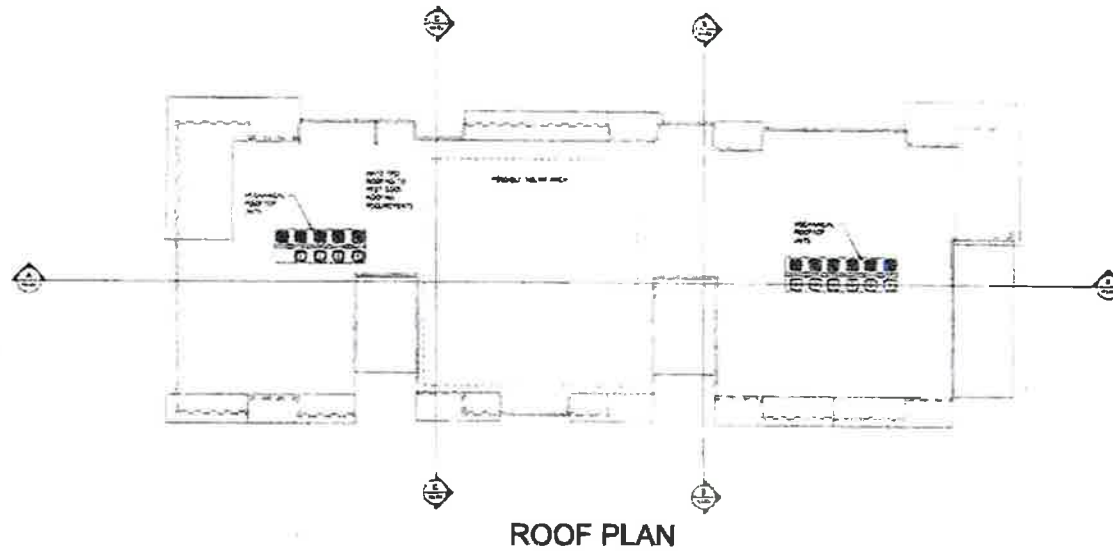
Scale: 3/32" = 1'-0"



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GRUPE

BUILDING B4 - 26PLEX

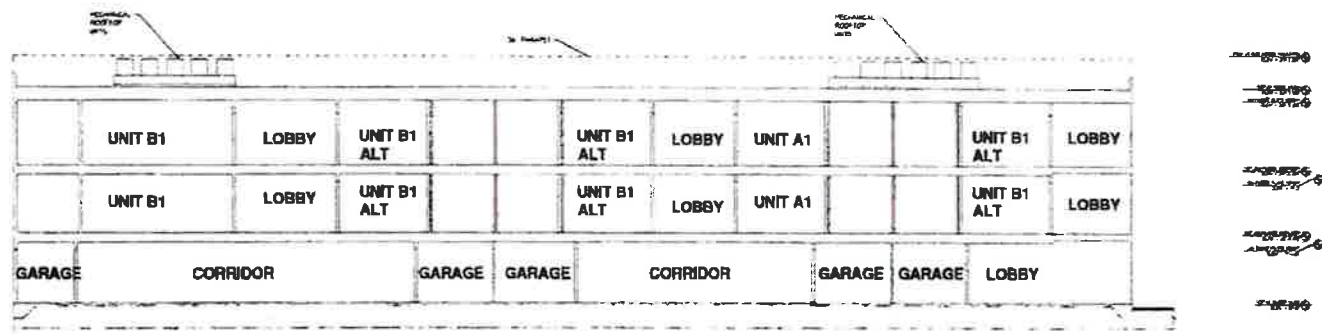
IRON POINT ROAD APARTMENTS

Scale: 3/32" = 1'-0"
FOLSOM, CA.

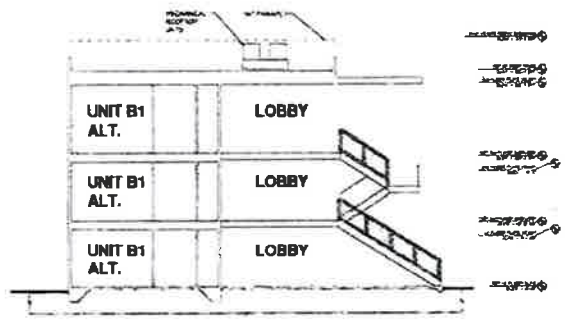


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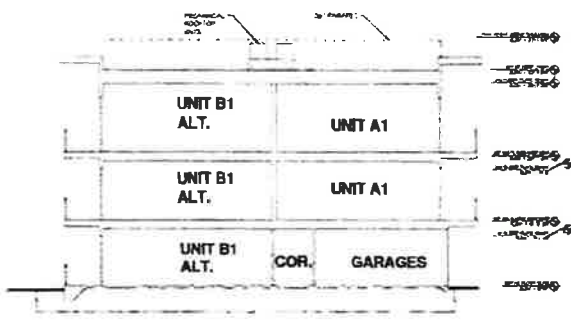
NOVEMBER 16, 2021 MR200320.00



SECTION A-A



SECTION B-B



SECTION C-C

BUILDING B4 - 26PLEX

IRON POINT ROAD APARTMENTS

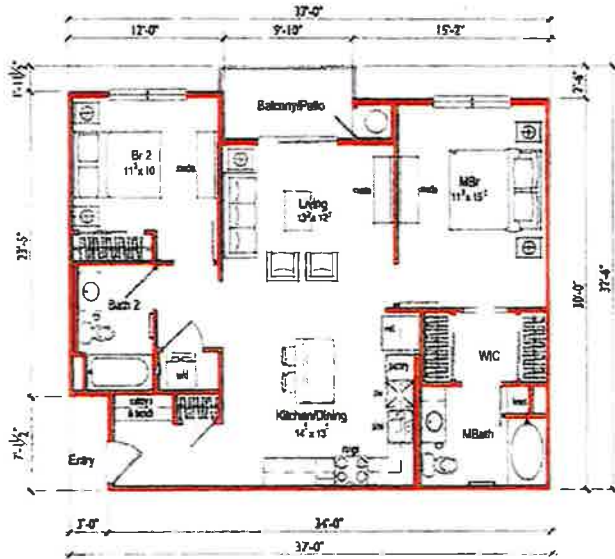
Scale: 1/8" = 1'-0"
FOLSOM, CA



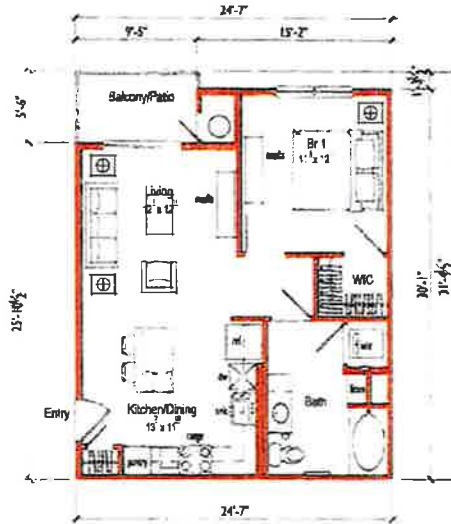
The drawings presented are illustrative of character and design intent only, and are subject to change based upon final design considerations (i.e. applicable codes, structural, and MEP design requirements, unit plan / floor plan changes, etc.) © 2019 BSB Design, Inc.

NOVEMBER 16, 2021 MR200320.00

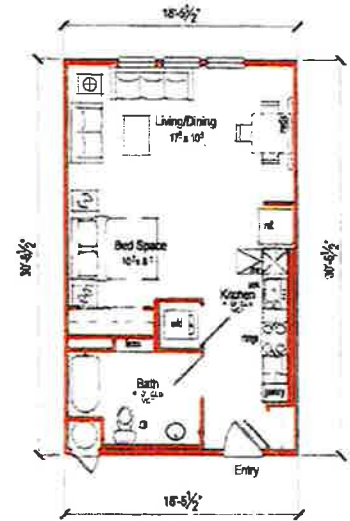




UNIT-B1
 2 BEDROOM + 2 BATH
 GROSS: 1,122 SQ. FT.
 NET: 1,057 SQ. FT.
 PATIO/BALC/WHL: 65 SQ. FT.



UNIT-A1
 1 BEDROOM + 1 BATH
 GROSS: 752 SQ. FT.
 NET: 687 SQ. FT.
 PATIO/BALC/WHL: 65 SQ. FT.



UNIT-S1
 STUDIO + 1 BATH
 GROSS: 564 SQ. FT.
 NET: 564 SQ. FT.

UNIT PLAN

IRON POINT ROAD APARTMENTS
 FOLSOM, CA.

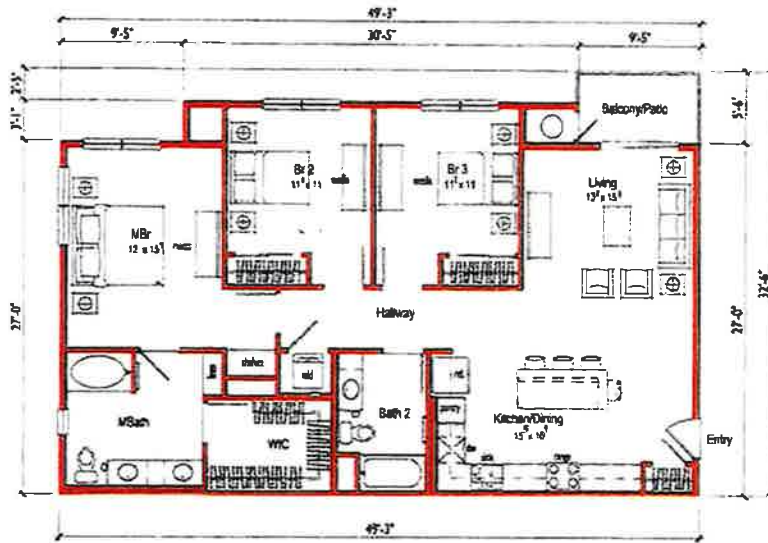
Scale: 1/4" = 1'-0"



The drawings presented are illustrations of character and design intent only, and are subject to change based upon final design considerations. No applicable codes, structural and MEP design requirements and plan/door plan changes. UC | © 2019 BSE Design, Inc.

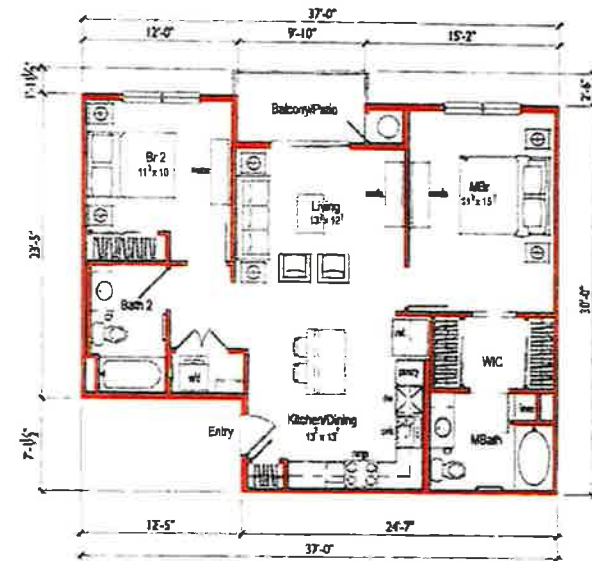
NOVEMBER 16, 2021 MR200320.00





UNIT C1

3 BEDROOM + 2 BATH
 GROSS: 1,476 SQ. FT.
 NET: 1,414 SQ. FT.
 PATIO/BALC/WH.: 62 SQ. FT.



UNIT-B1 ALT.

2 BEDROOM + 2 BATH
 GROSS: 1,055 SQ. FT.
 NET: 990 SQ. FT.
 PATIO/BALC/WH.: 65 SQ. FT.

UNIT PLAN

Scale: 1/4" = 1'-0"

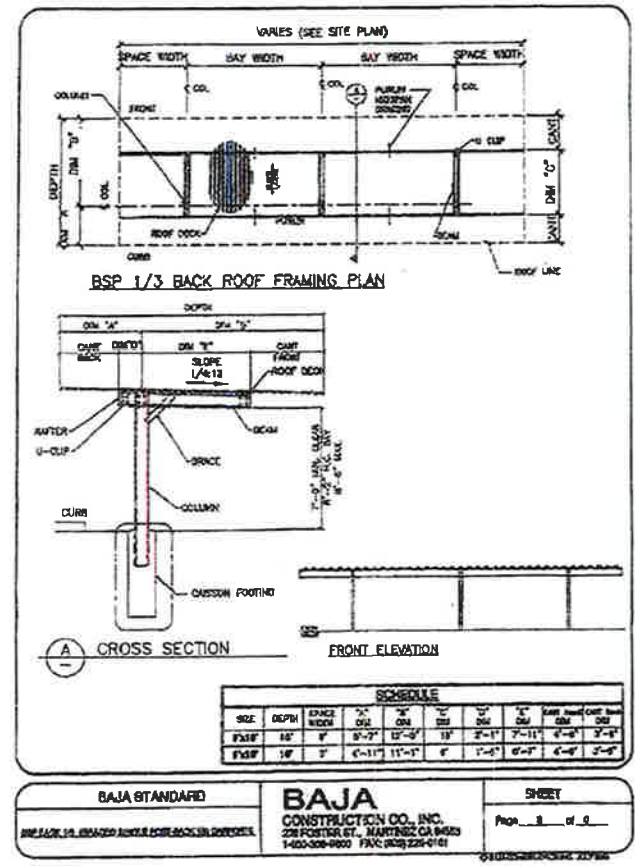
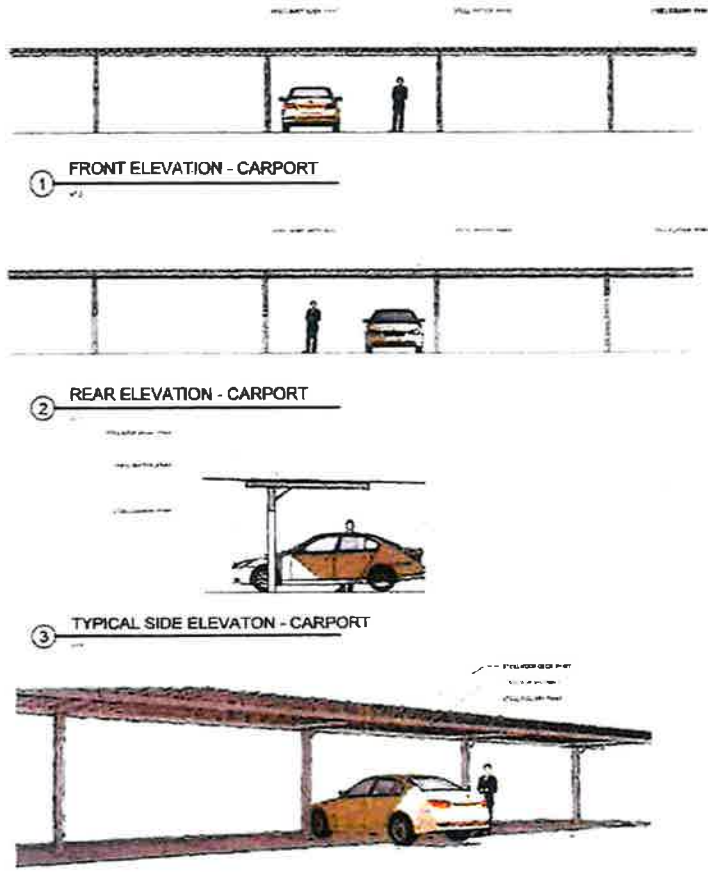
IRON POINT ROAD APARTMENTS
 FOLSOM, CA.



NOVEMBER 16, 2021 MR200320 00

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CARPORT



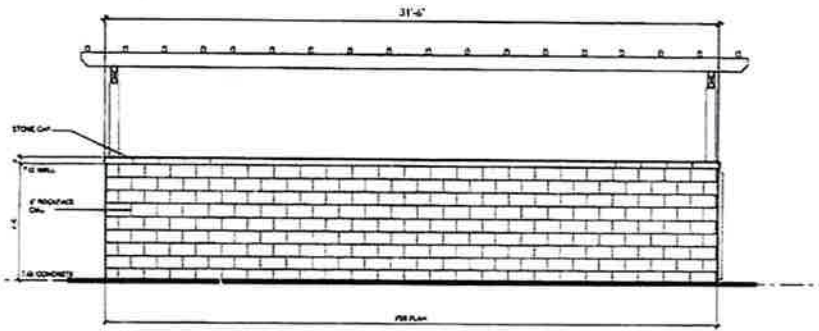
The drawings presented are illustrative of character and design intent only and are subject to change based upon final design considerations (i.e. applicable codes, structural and MEP design requirements, utility plan / floor plan changes, etc.) © 2019 BSB Design, Inc.

IRON POINT ROAD APARTMENTS

FOLSOM, CA.

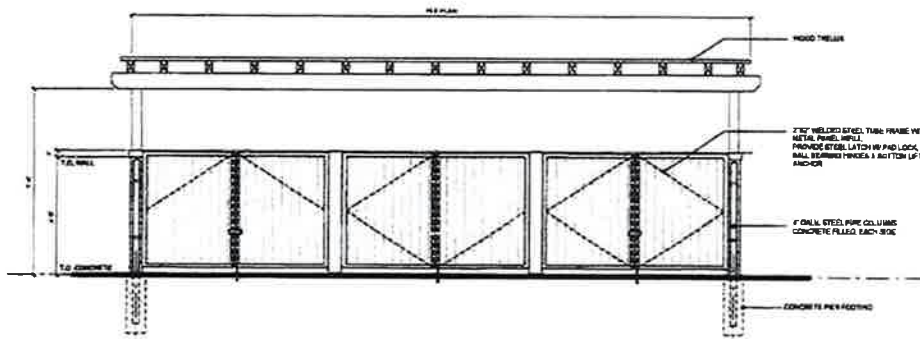
NOVEMBER 16, 2021 MR200320.00





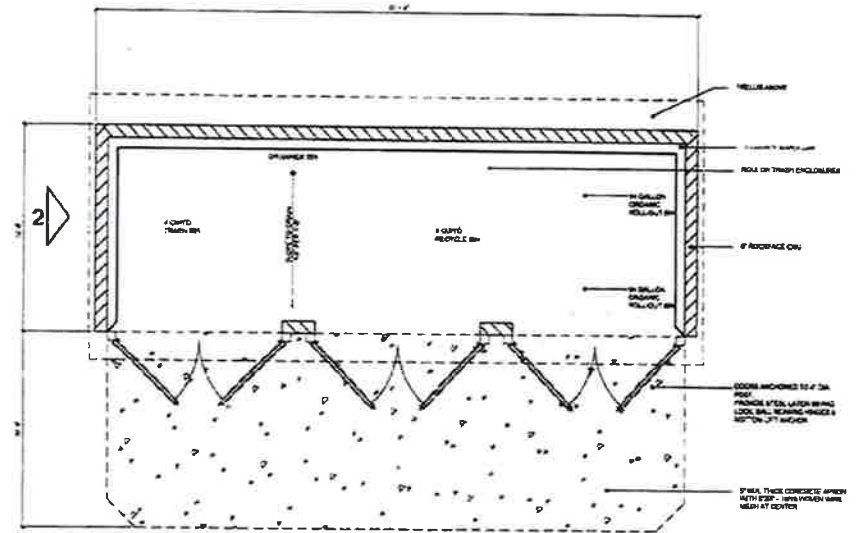
SIDE ELEVATION - 2

SCALE 1/4\"/>



FRONT ELEVATION - 1

SCALE 1/4\"/>



FLOOR PLAN

SCALE 1/4\"/>

TRASH ENCLOSURE

IRON POINT ROAD APARTMENTS

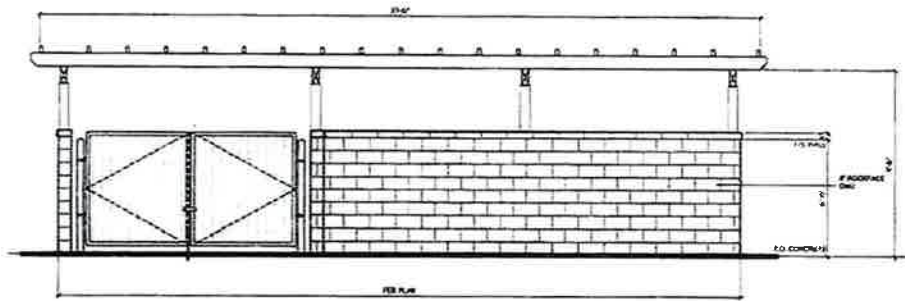
FOLSOM, CA.



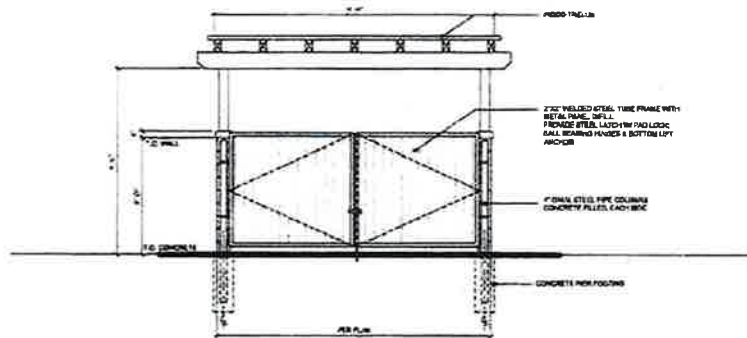
The drawings presented are illustrative of character and design intent only, and are subject to change based upon final design considerations (i.e. applicable codes, structural and MEP design requirements, unit plan / floor plan changes, etc.) © 2019 BSB Design, Inc.

NOVEMBER 16, 2021 MR200320.00

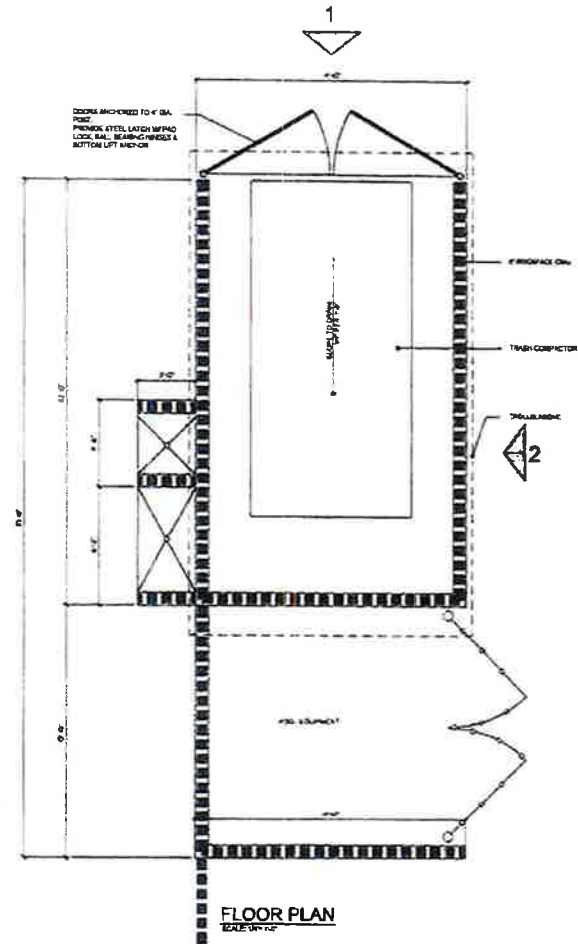




SIDE ELEVATION - 2
SCALE: 1/8\"/>



FRONT ELEVATION - 1
SCALE: 1/8\"/>



FLOOR PLAN
SCALE: 1/8\"/>

TRASH COMPACTOR ENCLOSURE

IRON POINT ROAD APARTMENTS
FOLSOM, CA.



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NOVEMBER 16, 2021 MR200320 00

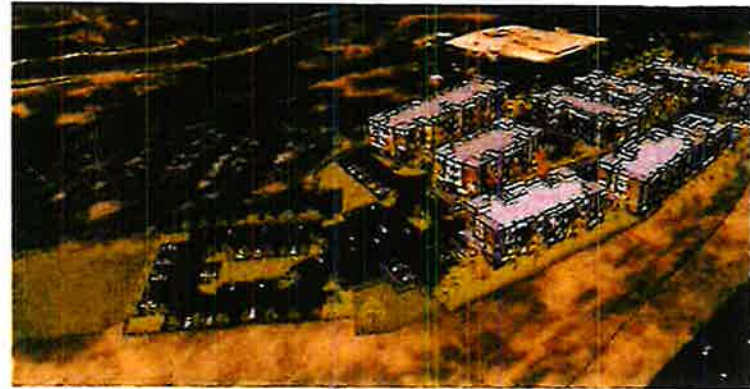


Attachment No. 16

Color Renderings and Perspectives, dated November 16, 2021



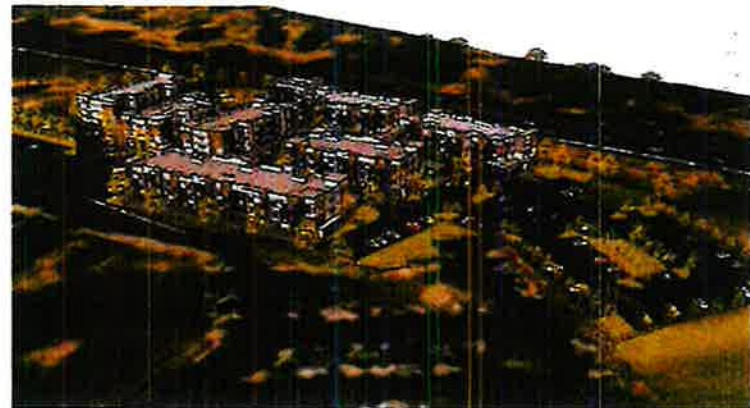
SOUTHEAST - 3



SOUTHWEST - 1



NORTHEAST - 4



NORTHWEST - 2



LOT 1 / AERIAL VIEWS



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IRON POINT ROAD APARTMENTS
FOLSOM, CA.

NOVEMBER 16, 2021 MR200320.00





SOUTHEAST - 3



SOUTHWEST - 1



NORTHEAST - 4



NORTHWEST - 2

LOT 6 / AERIAL VIEWS



IRON POINT ROAD APARTMENTS
FOLSOM, CA.



NOVEMBER 16, 2021 MR200320.00



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VIEW-1 - FROM ENTRY GATE OF LOT 1



VIEW-2 - FROM HWY 50 LOOKING INTO LOT 1



VIEW-3 - FROM SAFE CREDIT UNIT
PARKING LOOKING INTO LOT 6



VIEW-4 - FROM IRON POINT RD.
LOOKING INTO LOT 6

SITE VIGNETTES



IRON POINT ROAD APARTMENTS
FOLSOM, CA



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NOVEMBER 16, 2021 MR200320.00



Attachment No. 17

Color and Materials Board, dated November 16, 2021



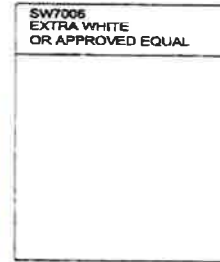
APARTMENTS



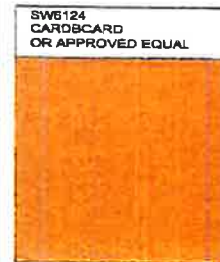
LOT 1 - CLUBHOUSE/LEASING



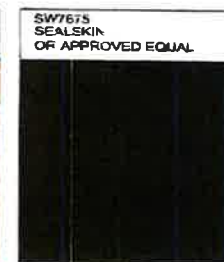
LOT 6 - CLUBHOUSE



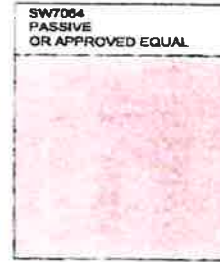
1 STUCCO WALL - MAIN



2 STUCCO WALL - ACCENT



3 STUCCO WALL - ACCENT



4 GARAGE DOOR



5 WALL ACCENT



6 METAL RAILING



7 GLASS RAILING



8 METAL CLADDING CANOPY/AWNING

MATERIAL & COLOR BOARD

IRON POINT ROAD APARTMENTS
FOLSOM CA



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NOVEMBER 16, 2021 MR200320 00



Attachment No. 18

Signage Details, dated November 16, 2021



NORTH VIEW OF LOT 6 FROM IRON POINT RD. CORNER



STREET ELEVATION ALONG IRON POINT RD



WEST VIEW OF LOT 6 FROM IRON POINT RD. CORNER



FRONT ELEVATION
SCALE 3/8\"/>

IRON POINT ROAD / MONUMENT SIGNAGE

GRUPE

COLE PARTNERS
DEVELOPMENT COMPANY

IRON POINT ROAD APARTMENTS
FOLSOM, CA.

BSB
DESIGN
95605104.CCU

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NOVEMBER 16, 2021 MR200320 00



STREET ELEVATION



FRONT ELEVATION

SCALE: 3/8" = 1'-0"



PERSPECTIVE



PERSPECTIVE

LOT 1 / MONUMENT SIGNAGE

GEUPE



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IRON POINT ROAD APARTMENTS
FOLSOM, CA

NOVEMBER 16 2021 MR200320.00





STREET ELEVATION



FRONT ELEVATION
SCALE: 3/8" = 1'-0"



PERSPECTIVE



PERSPECTIVE

LOT 6 / MONUMENT SIGNAGE

CRUPE



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IRON POINT ROAD APARTMENTS
FOLSOM, CA

NOVEMBER 16, 2021 MR200320.00



Attachment No. 19

Building and Parking Summary, dated February 8, 2022

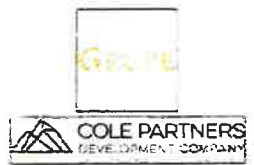
COLE PARTNERS - FOLSOM APARTMENTS - E01_1 1/13/2022

SITE INFORMATION			
TOTAL SITE AREA	7.18	AC	
TOTAL UNITS	153	UNITS	
DENSITY	21.31	du/ac	

BUILDING SUMMARY	A1	B1	B1-10	S1	T1	Total
BUILDING A1 - 3 story (12 plex stair back loaded)						
Total	16	4	6	2	4	32
Percentage per each unit type	50%	13%	19%	6%	13%	100%
	50.00%	11.92%	17.74%	6.25%	12.50%	100%
BUILDING B1 - 3 story (16 plus stair back loaded)						
Total	6	2	6	2	0	16
Percentage per each unit type	38%	13%	38%	13%	0%	100%
	37.50%	13.33%	50.00%	12.50%	0.00%	100%
BUILDING B2 - 3 story (21 plex)						
Total	11	2	0	0	2	15
Percentage per each unit type	52%	10%	29%	0%	10%	100%
	52%	13%	0%	0%	10%	100%
BUILDING B3 - 3 story (21 plex Stair back loaded)						
Total	11	1	4	2	0	18
Percentage per each unit type	52%	10%	19%	10%	0%	100%
	52%	10%	38%	10%	0%	100%
TOTAL						
	# of Bldg					
BUILDING A1 - 32 PLEX	2	16	4	6	2	32
BUILDING B1 - 16 PLEX	1	6	2	6	0	16
BUILDING B2 - 21 PLEX	1	11	0	0	2	21
BUILDING B3 - 21 PLEX	2	22	1	4	0	42
TOTAL	77	20	38	8	10	153
Percentage per each unit type	50.3%	13.1%	24.8%	5.2%	6.6%	100%
	50.33%	13.10%	24.84%	5.23%	6.54%	100%

Clubhouse: +/-6,782 sf.
 Total Overall Parking: 304 spaces
 Overall Parking Ratio: 1.99 spaces/unit
 Total Parking Outside Powerlines: 189 spaces

Integrated Garage: 74 spaces
 Covered Surface Parking: 79 spaces
 Uncovered Surface Parking: 151 spaces
 Uncovered Surface Parking Outside Power Lines: 36
 Uncovered Surface Parking Under Power Lines: 115
 Guest Parking (Included): 9 spaces



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SITE 1 PROJECT SUMMARY - 153 UNITS
IRON POINT ROAD APARTMENTS
 FOLSOM, CA



February 8, 2022 | MR200320.00

PARKING STATISTICS					
10% OF PARKING = 30 EV CAPABLE PARKING SPACES TO BE PROVIDED PER CALIFORNIA MANDATORY REQUIREMENTS					
Parking Required	Unit Type	No. of Units	Ratio Required per City Guidelines	Stalls Required per City Guidelines	Total Provided
	1 Bedroom	77	1.50	116	
	2 Bedroom	58	1.75	102	
	3 Bedroom	10	2.00	20	
	Studio	8	1.50	12	
	Visitors	153	1 per 5 units	31	
Total Required Spaces				280	304
Parking Ratio				1.83	1.99

BICYCLE PARKING STATISTICS							
Parking Required	Unit Type	No. of Units	City Requirement 1 per 5 units	Req's spaces	In Bike Storage	Additional external spaces	Total Provided Spaces
Total Required Spaces		153	30.6	31	31	2	33
Parking Ratio							0.22

Per California, the most restrictive application should be used. California Non-Residential requirement is 5% of total parking so we have applied the City of Folsom bike requirement.

UNIT MIX	Unit	Unit Type	Unit Loc.	Net Unit Area*	Balcony	Gross Unit Area**	Quantity	%	Total (Net Unit)	Total (Gross Unit)
	A1	1 br/1 ba	Interior	687	65	752	77	50%	52,899	57,904
	B1	2 br/2 ba	Corner	1,057	64	1,121	20	13%	21,140	22,420
	B1-Alt	2 br/2 ba	Interior	990	64	1,054	38	25%	37,620	40,052.00
	S1	Studio	Interior/Corner	564	-	558	8	5%	4,512	4,460
	C1	3 br/2 ba	Corner	1,412	62	1,474	10	7%	14,120	141,200
	Total						153	100%	130,291	266,036.00
				Average unit size					851.58	922.88



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SITE 1 PARKING/UNIT SUMMARY
IRON POINT ROAD APARTMENTS
 FOLSOM, CA



February 8, 2022 | MR200320.00

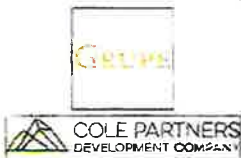
COLE PARTNERS - FOLSOM APARTMENTS - LOT 6

11/3/2021

SITE INFORMATION			
TOTAL SITE AREA	4.13	AC	
TOTAL UNITS	100	UNITS	
DENSITY	24.21	du/ac	

BUILDING SUMMARY							
		A1	B1	B1-alt	S1	C1	Total
BUILDING B2 - 3 story (21 plex)							
Total		11	4	4	2	0	21
Percentage per each unit type		52%	19%	19%	10%	0%	100%
		52.38%	38.10%		9.52%	0.00%	100%
BUILDING B3 - 3 story (32 plex Stair back loaded)							
Total		16	4	5	2	4	32
Percentage per each unit type		50%	13%	19%	6%	13%	100%
BUILDING B4 - 3 story (26 plex Stair back loaded)							
Total		11	4	9	2	0	26
Percentage per each unit type		42%	15%	35%	8%	0%	100%
		42%	50%		8%	0%	100%
TOTAL							
	# of Bldg						
BUILDING B2 -21 PLEX	2	22	8	8	4	0	42
BUILDING B3 - 32 PLEX	1	16	4	6	2	4	32
BUILDING B4 - 26 PLEX	1	11	4	9	2	0	26
TOTAL		49	16	23	8	4	100
Percentage per each unit type		49.0%	16.0%	23.0%	8.0%	4.0%	100%
		49.00%	39.00%		8.00%	4.00%	100%

- Clubhouse: +/-3,098 sf.
- Total Overall Parking: 188 spaces
- Overall Parking Ratio: 1.88 spaces/unit
- Integrated Garage: 46 spaces
- Covered Surface Parking: 54 spaces
- Uncovered Surface Parking: 88 spaces



LOT 6 PROJECT SUMMARY - 100 UNITS
IRON POINT ROAD APARTMENTS
 FOLSOM, CA.



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FEBRUARY 8, 2022 | MR200320

PARKING STATISTICS 10% OF PARKING = 19 EV CAPABLE PARKING SPACES TO BE PROVIDED PER CALGREEN MANDATORY REQUIREMENTS

Parking Required	Unit Type	No. of Units	Ratio Required per City Guidelines	Stalls Required per City Guidelines	Total Provided
	1 Bedroom	49	1.50	74	
	2 Bedroom	39	1.75	68	
	3 Bedroom	4	2.00	8	
	Studio	8	1.50	12	
	Visitors	100	1 per 5 units	20	
Total Required Spaces				182	187
Parking Ratio				1.82	1.87

BICYCLE PARKING STATISTICS

Parking Required	Unit Type	No. of Units	City Requirement 1 per 5 units	Req's spaces	In Bike Storage	Additional external spaces	Total Provided Spaces
Total Required Spaces		100	20.0	20	20	2	22
Parking Ratio							0.22

Per CalGreen, the most restrictive application should be used. CalGreen Non-Residential requirement is 5% of total parking so we have applied the City of Folsom bike requirement

UNIT MIX

Unit	Unit Type	Unit Loc.	Net Unit Area*	Balcony	Gross Unit Area**	Quantity	%	Total (Net Unit)	Total (Gross Unit)
A1	1 br/1 ba	Interior	687	65	752	49	49%	33,663	36,848
B1	2 br/2 ba	Corner	1,057	64	1,121	16	16%	16,912	17,936
B1-Alt	2 br/2 ba	Interior	990	64	1,054	23	23%	22,770	24,242
S1	Studio	Interior/Corner	564	-	558	8	8%	4,512	4,460
C1	3 br/2 ba	Corner	1,412	62	1,474	4	4%	5,648	5,896
	Total					100	100%	83,595	89,382
Average unit size								835.05	893.82



LOT 6 PARKING/UNIT SUMMARY
IRON POINT ROAD APARTMENTS
FOLSOM, CA



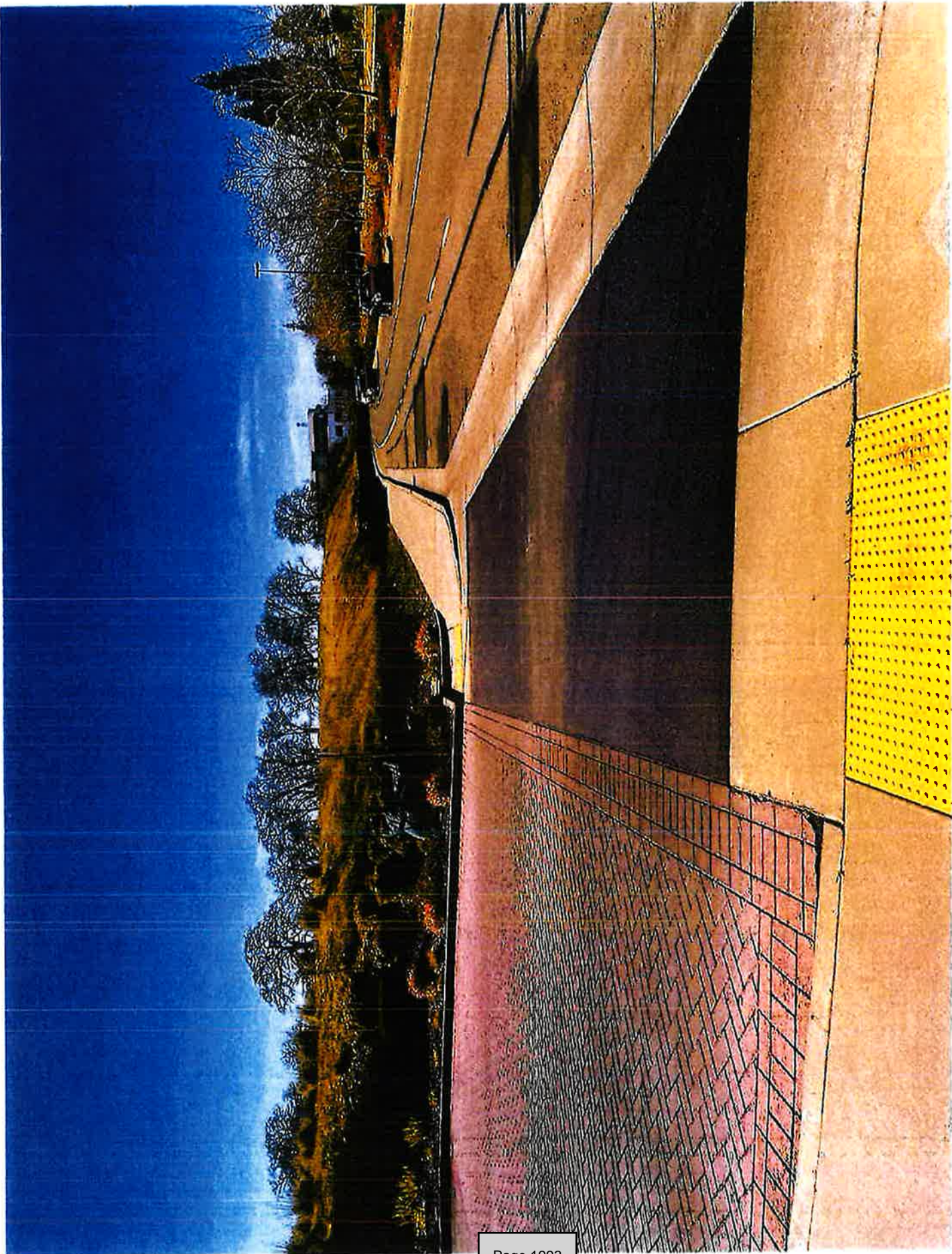
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FEBRUARY 8, 2022 | MR200320

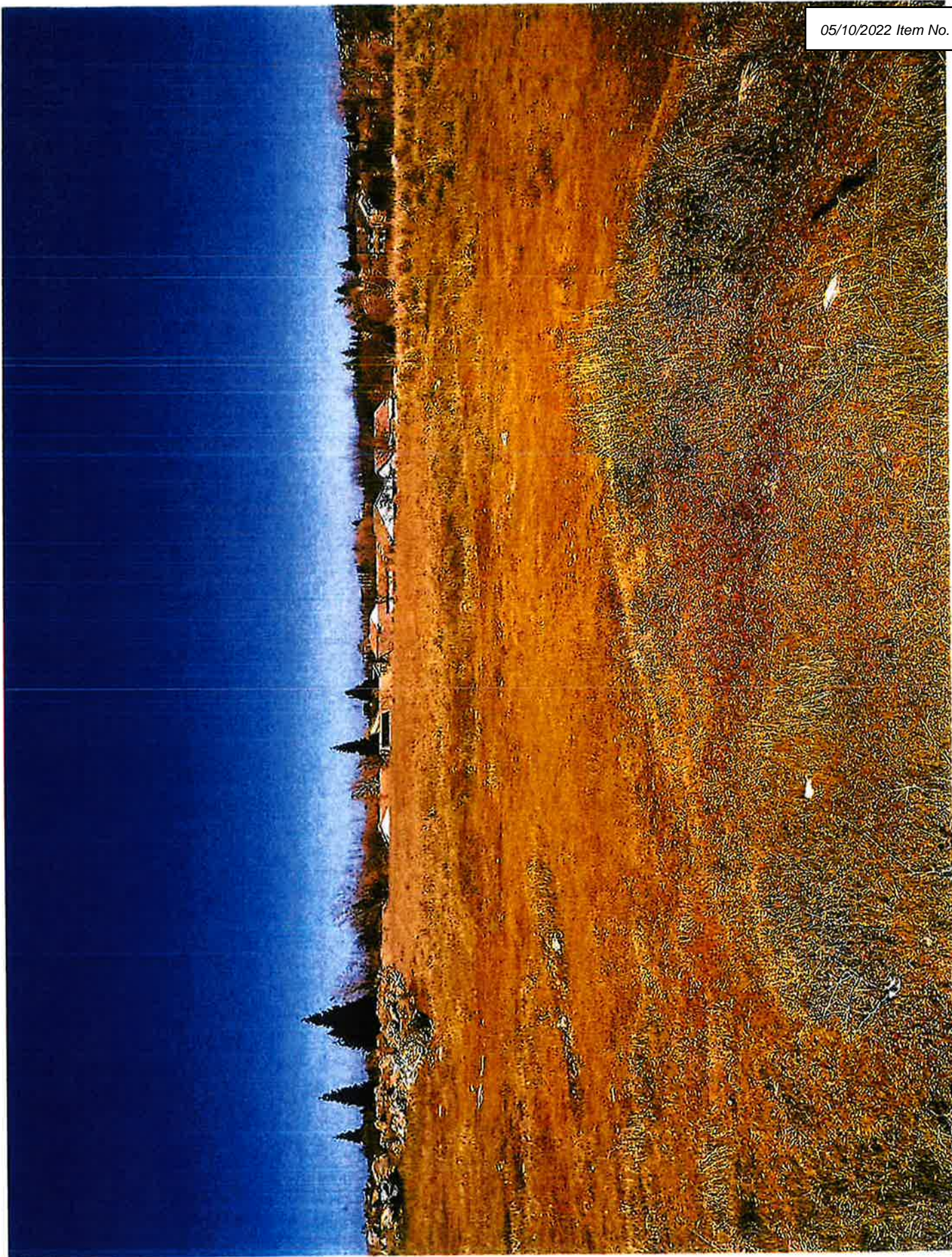
Attachment No. 20

Site Photographs



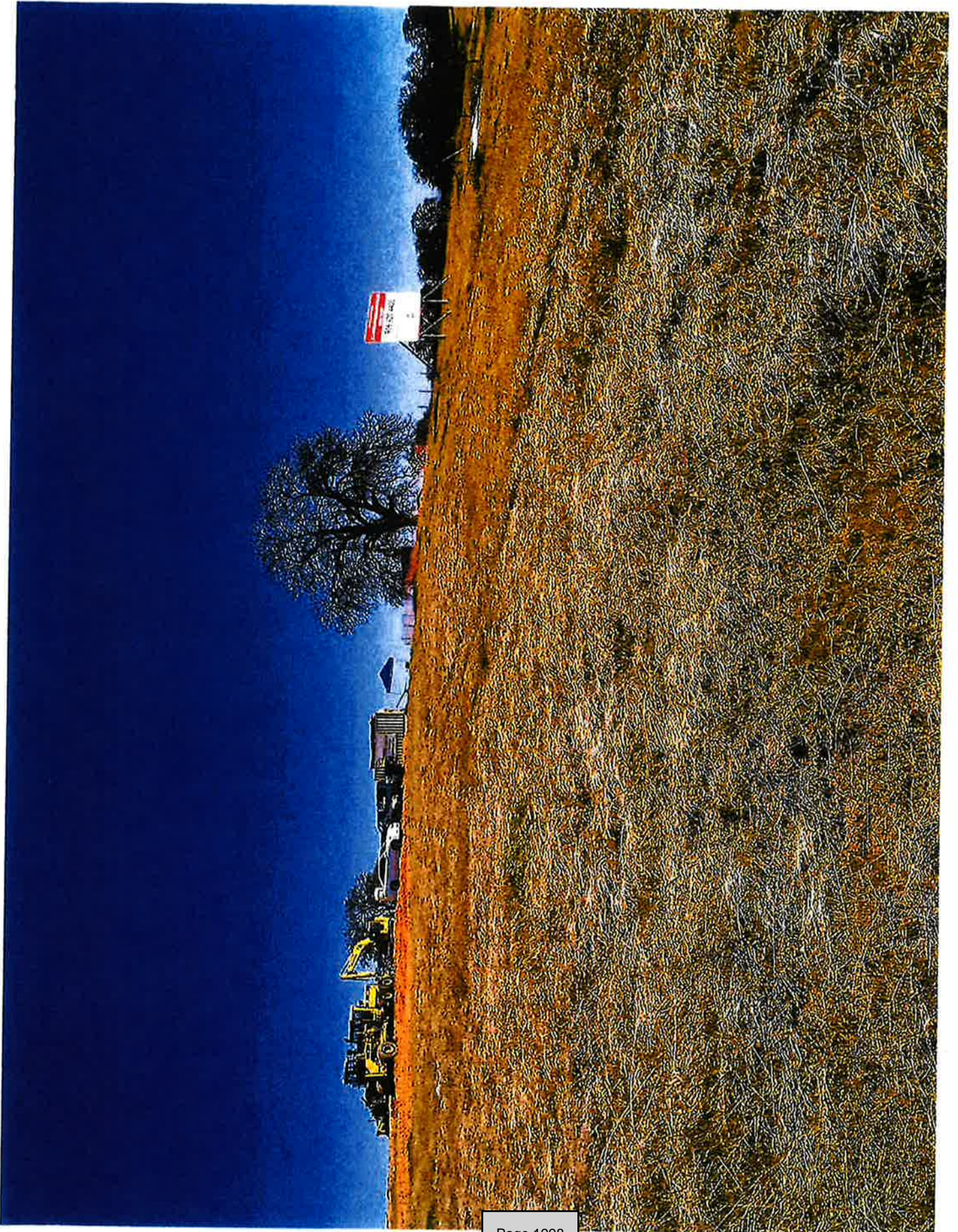


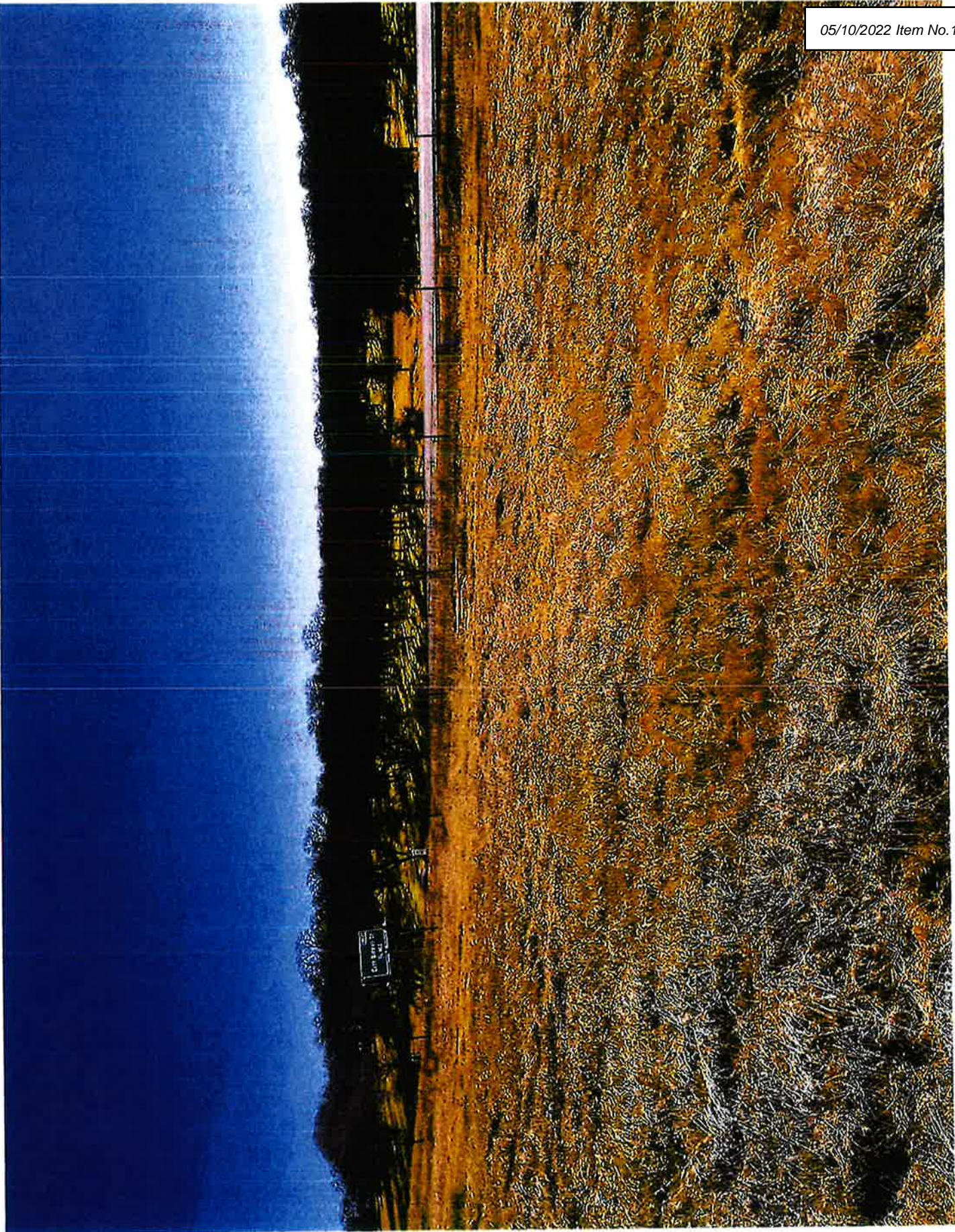












Attachment No. 21

Transportation Impact Study, dated February, 2022

P21090

Folsom Corporate Center Apartments Transportation Impact Study
Folsom, California

Prepared for:
City of Folsom
Helix Environmental, Inc., and
FCC 50, LLC

Prepared By



TRANSPORTATION PLANNING
& MANAGEMENT, INC.

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February 2022

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REVISION HISTORY

Date	Title	Comment
Dec 7, 2021	Draft TIS	
Feb 3, 2022	Final Report	Updated parking per revised site plan, clarified gate queue storage. Clarified "mitigation" vs "abatement".

EXECUTIVE SUMMARY

This Transportation Impact Study identifies impacts of the proposed Folsom Corporate Center Apartments project (the Project) on the motorized and unmotorized transportation systems in Folsom, California. This study has been prepared for the City of Folsom, Helix Environmental Inc., and FCC 50, LLC.

Project Description

Figure ES-1 provides a Project vicinity map. The Project consists of 253 apartment units on two separate parcels within the Folsom Corporate Center. The two Project parcels are Accessors Parcel number 072-3120-001 (referred to as "Lot 1") and 072-3120-023 (referred to as "Lot 6"). The Project parcels are generally located east of Oak Avenue Parkway, south of Iron Point Road, and north of U.S. Highway 50. One portion of the Project will be located on a 4.13-acre parcel situated in front of the Safe Credit Union Building and adjacent to Iron Point Road (Lot 6). The second portion of the project will be located on a 7.18-acre parcel situated directly behind the Kaiser Permanente office building (Lot 1). The Project offers walkable access to employment opportunities within the Folsom Corporate Center and is less than a mile from excellent shopping and entertainment options at the Palladio. 491 parking spaces are proposed for an overall parking ratio of 1.94 spaces per dwelling unit. A preliminary site plan is provided in **Figure ES-2**, with driveway queue storage detail shown in **Figure ES-3** and **Figure ES-4**.

Analysis Scope

The analysis considers CEQA Vehicle Miles of Travel impacts and the traffic operations at intersections in Folsom that could potentially be impacted by Project traffic. Study intersections and segments are shown in **Figure ES-5** and listed in **Table ES-1** and **Table ES-2**. This Transportation Impact Study considers six study scenarios:

- Existing 2021 without Project Condition;
- Existing 2021 with Project Condition;
- Existing Plus Approved Projects (EPAP) 2026 without Project Condition;
- EPAP 2026 with Project Condition;
- Cumulative 2035 without Project Condition; and
- Cumulative 2035 with Project Condition.

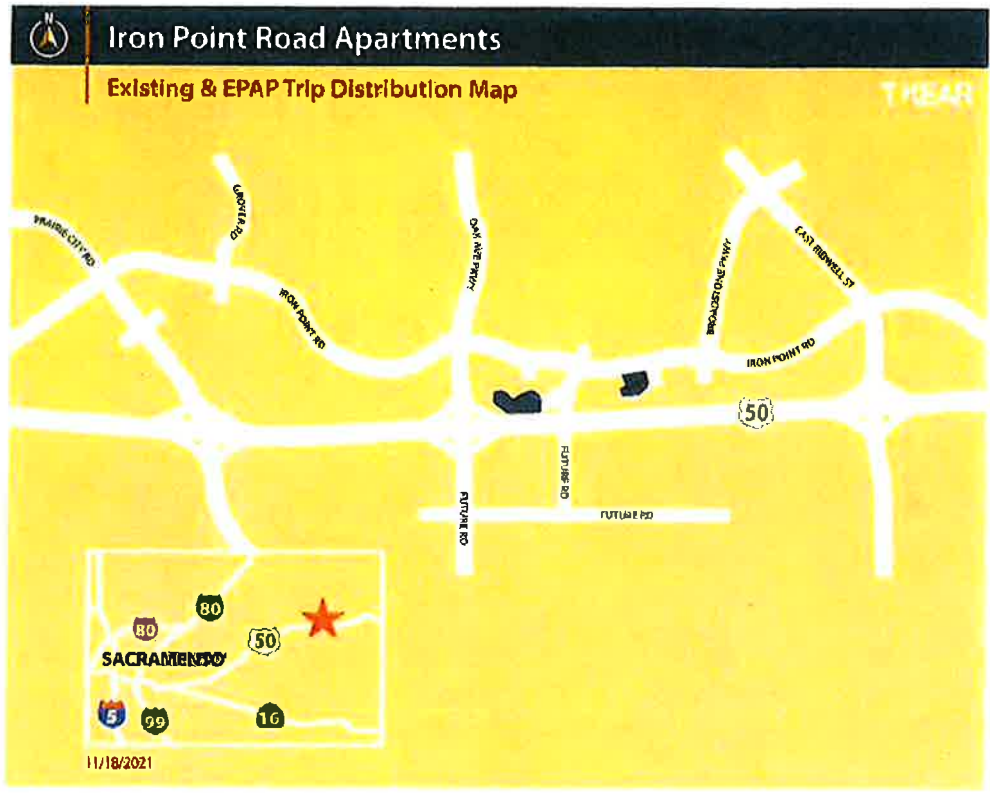


Figure ES-1. Iron Point Road Apartment Vicinity Map

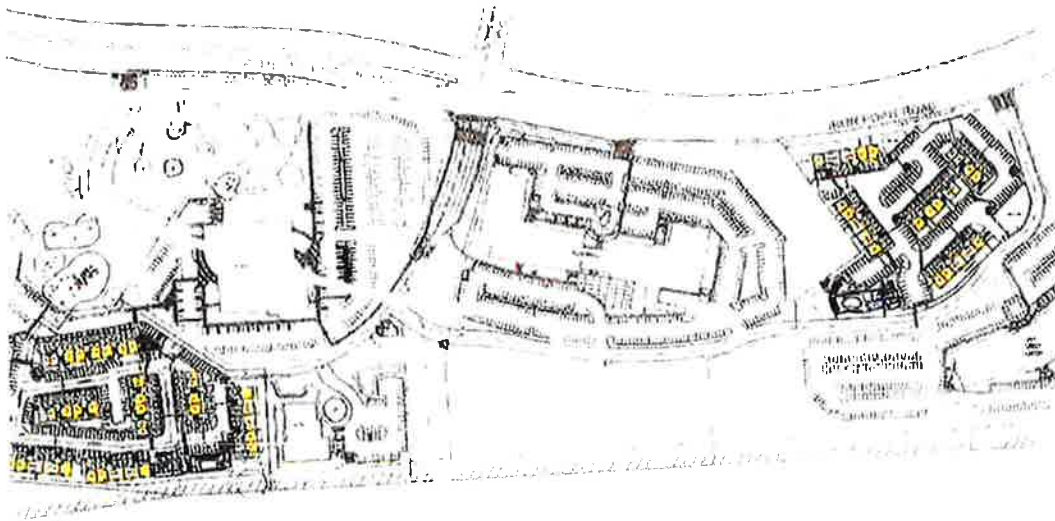


Figure ES-2. Preliminary Site Plan (note that updated entry detail is provided in Figure ES-3 and Figure ES-4)



Figure ES-3. Entry Gate Detail At For "Lot 1" (Western Portion Of Project) Showing Queue Storage At Entry Gate

**Folsom Corporate Center Apartments
Transportation Impact Study**

**Folsom,
California**

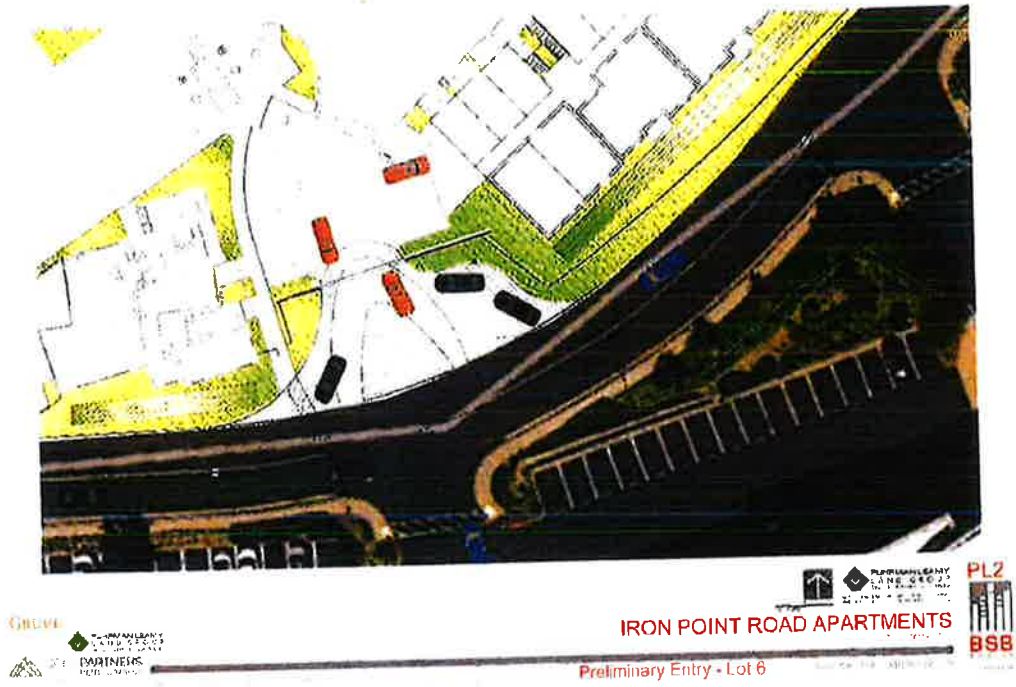


Figure ES-4. Entry Gate Detail At For "Lot 6" (Eastern Portion Of Project) Showing Queue Storage At Entry Gate



Figure ES-5. Project area roadways including study intersections and study road segments

**Folsom Corporate Center Apartments
Transportation Impact Study**

**Folsom,
California**

Table ES-1. Study Intersections

Intersection	Control
1. Prairie City Rd/US 50 eastbound ramps	Signal
2. Prairie City Rd/US 50 westbound ramps	Signal
3. Prairie City Rd/American Aggregates Rd	Signal
4. Prairie City Rd/Iron Point Rd	Signal
5. Iron Point Rd /Grover Rd	Signal
6. Iron Point Rd /Oak Avenue Pkwy	Signal
7. Iron Point Rd /West Kaiser access road	TWSC*
8. Iron Point Rd /Rowberry Way	Signal
9. Iron Point Rd /Safe Credit Union access	TWSC*
10. Iron Point Rd /Broadstone Pkwy	Signal
11. Iron Point Rd /East Bidwell St	Signal
12. East Bidwell St/US 50 westbound ramps	Signal
13. East Bidwell St/US 50 eastbound ramps	Signal
14. APN 072-3120-023 "Lot 6" access	TWSC*
15. APN 072-3120-023 "Lot 1" access	TWSC*
16. Oak Avenue Pkwy/US 50 westbound ramps (2035 Only)	Signal
17. Oak Avenue Pkwy/US 50 eastbound ramps (2035 Only)	Signal

* Two Way Stop Control

Table ES-2. US 50 Study Segments

US 50 Segment	Segment Type	Applicable Years
1. US 50 westbound East Bidwell offramp	Diverge	All
2. US 50 westbound East Bidwell loop onramp	Merge	All
3. US 50 westbound East Bidwell slip onramp	Merge	All
4. US 50 westbound East Bidwell to Oak Ave	Basic	All
5. US 50 westbound Oak Avenue offramp	Diverge	2035
6. US 50 westbound Oak Avenue loop onramp	Merge	2035
7. US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	weave	2035
8. US 50 westbound Prairie City offramp	Diverge	2021/2026
9. US 50 westbound Prairie City loop onramp	Merge	All
10. US 50 westbound Prairie City diagonal onramp	Merge	All
11. US 50 eastbound Prairie City offramp	Diverge	All
12. US 50 eastbound Prairie City diagonal onramp	Merge	All
13. US 50 eastbound Prairie City fly-over onramp	Merge	2021/2026
14. US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	2035
15. US 50 eastbound Oak Avenue loop onramp	Merge	2035
16. US 50 eastbound Oak Avenue diagonal onramp	Merge	2035
17. US 50 eastbound Oak Ave to East Bidwell	Basic	All
18. US 50 eastbound East Bidwell offramp	Diverge	All
19. US 50 eastbound East Bidwell loop onramp	Merge	All
20. US 50 eastbound East Bidwell slip onramp	Merge	All

Findings and Recommendations

The Project is anticipated to generate 1376 daily vehicle trips, 81 AM peak-hour vehicle trips, and 104 PM peak-hour vehicle trips. There are no anticipated Project related level-of-service deficiencies.

The Project is anticipated to have a **less-than-significant** impact on vehicle level-of-service, bike and pedestrian activity and facilities, transit operations and facilities, and VMT.

Parking supply at an overall ratio of 1.94 spaces per apartment exceeds the City requirements and is sufficient to meet the anticipated parking demand. Lot 1 has a parking ratio of 1.99 spaces per apartment and Lot 6 has a parking ratio of 1.87 spaces per apartment. All of which exceed the City requirement of 1.5 spaces per dwelling unit.

Storage for two or more vehicles is provided in front of entry gates, which is adequate to store the anticipated 95% gate queues.

As described in section 8.3 Queueing (page 74), Project related queueing deficiencies are anticipated on the westbound left-turn from Iron Point Rd to Prairie City Rd during the AM peak hour in under Existing 2021 with Project and EPAP 2026 with Project conditions (Deficiency 1 and Deficiency 2, respectively). To avoid confusion, General Plan deficiencies are labeled as "deficiencies" rather than (CEQA) "impacts", and the related improvements are labeled as "abatement measures" rather than "mitigation measures". This is done to emphasize that any level-of-service and/or queueing concerns are not considered to be impacts under CEQA.

Abatement 1 and Abatement 2 (also described in Section 8.3) are anticipated to reduce queues such that the Project has a less-than-significant effect on traffic operations. These two Abatement measures are identical. The project should be conditioned to coordinate with the City to implement Abatement 1 and 2, prior to issuance of the first building permit:

Abatement 1 and Abatement 2

(Prior to issuance of the First building permit, at applicants expense): "Modify Prairie City Rd/Iron Point Rd signal timing plan by shifting 1 second from the eastbound through movement to the westbound left turn movement, reduce the vehicle extension setting from adding five to six additional seconds to the green phase for through movements to adding four seconds to the green phase for through movements for each vehicle passing the detector after the minimum green phase length has been exceeded."

Otherwise, the City's standard approval conditions and fees are adequate.

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1. INTRODUCTION

This transportation impact study identifies impacts of the proposed Folsom Corporate Center Apartments project (the Project) on the motorized and unmotorized transportation systems in Folsom, California. This study has been prepared for the City of Folsom, Helix Environmental Inc., and the applicant FFC 50, LLC.

1.1 Project Description

Figure 1 provides a project vicinity map. The applicant is requesting approval of a General Plan Amendment, Rezone, Planned Development Permit Modification, and Design Review for development of a 253-unit multi-family market rate apartment community on two separate parcels within the Folsom Corporate Center. The two Project parcels are Accessors Parcel number 072-3120-001 (referred to as "Lot 1") and 072-3120-023 (referred to as "Lot 6"). The project parcels are generally located east of Oak Avenue Parkway, south of Iron Point Road, and north of U.S. Highway 50. One portion of the project will be located on a 4.13-acre parcel situated in front of the Safe Credit Union Building and adjacent to Iron Point Road (Lot 6). The second portion of the project will be located on a 7.18-acre parcel situated directly behind the Kaiser Permanente Office Building (Lot 1). The proposed apartment community is comprised of 12 three-story apartment buildings containing between 20 and 31 rental units. The applicant is requesting a General Plan amendment, Rezone, Planned Development Permit Modification, and Design Review.

The proposed apartments, which include a combination of one, two, and three bedroom units, range in size from 690 square feet to 1,325 square feet. In addition, the proposed Project includes two clubhouse buildings featuring indoor and outdoor amenities. Access to the two Project parcels is proposed to be provided by three existing driveways located along the south side of Iron Point Road. The proposed project includes 491 parking spaces including garage parking spaces, carport covered parking spaces, and uncovered parking spaces. Additional site improvements include drive aisles, curbs, gutters, sidewalks, internal walkways, underground utilities, retaining walls, site lighting, site landscaping, and monument signs.

A preliminary site plan is provided in **Figure 2**, with driveway detail in **Figure 3** and **Figure 4**. Lot 1 will accommodate 153 dwelling units and 304 parking spaces. Lot 6 will accommodate 100 dwelling units and 187 parking spaces. Each portion of the development will be gated with full access driveways to Folsom Corporate Center's private roadways. Two of three Folsom Corporate Center driveways onto Iron Point Road have restricted access (either limiting left turns out or limiting left turns both in and out) and are side street stop controlled. The Folsom Corporate Center driveway aligned with Rowberry Drive is a full access intersection with signal control. Under cumulative conditions, Rowberry Drive is assumed to be extended across US 50 to Alder Creek Parkway in Folsom Ranch.

1.2 Report Organization

The following sections are discussed after this Introduction: Setting and Study Area (key roadways and intersections, the regulatory setting, and analysis scenarios); Methodology (detailing the

analysis procedures); six analysis sections; and, the final sections summarizing project impacts, mitigations, triggers for those mitigations, and recommended conditions of approval.



Figure 1. Vicinity Map

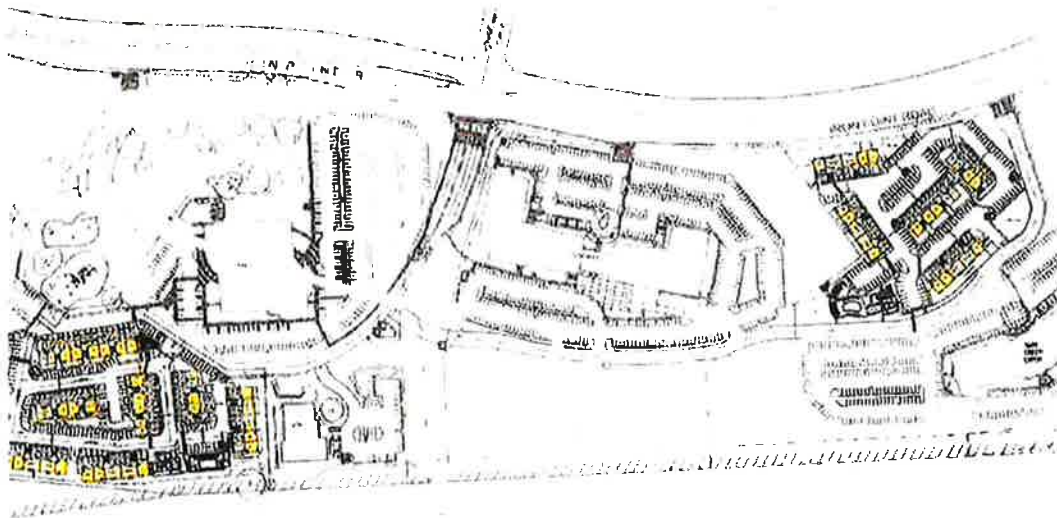


Figure 2. Preliminary Site Plan (Updates Entry Detail is Provided in Figure 3 For Lot 1 On The West And Figure 4 For Lot 6 On The East)



Figure 3. Entry Gate Detail At For "Lot 1" (Western Portion Of Project) Showing Queue Storage At Entry Gate

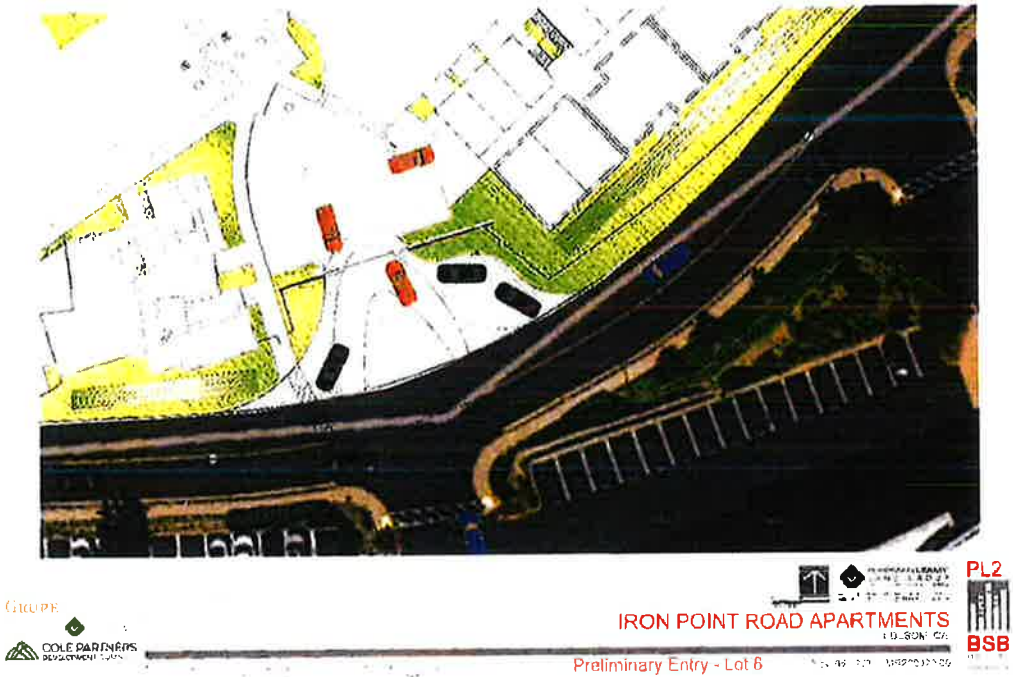


Figure 4. Entry Gate Detail At For "Lot 6" (Eastern Portion Of Project) Showing Queue Storage At Entry Gate

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2. SCENARIOS, SETTING AND STUDY AREA

The Transportation Impact Study area generally consists of the region along the portion of East Bidwell Street from Folsom Lake College to US 50, and along Cavitt Drive from Broadstone Parkway to Iron Point Road within the City of Folsom, California. Key roadways within the study area, and study intersections, are shown in Figure 5.

2.1 Study Scenarios

Four scenarios were identified for inclusion in this Transportation Impact Study through consultation with City of Folsom staff. The study determines the weekday AM peak-hour and PM peak-hour level-of-service at study intersections under the following scenarios:

- Existing 2021 without Project Condition;
- Existing 2021 with Project Condition;
- Existing Plus Approved Projects (EPAP) 2026 without Project Condition;
- EPAP 2026 with Project Condition;
- Cumulative 2035 without Project Condition; and
- Cumulative 2035 with Project Condition.

Existing 2021, and Existing 2021 with Project Condition

Analysis of the existing condition reflects the traffic volumes and roadway geometry at the time the study began. These two scenarios (with and without the Project) quantify performance measures, serve as a known reference point for those familiar with the study area, and identify project related impacts anticipated to occur if the project opened in 2021.

EPAP 2026 Condition, and EPAP 2026 with Project Condition

EPAP scenarios, with and without the Project, analyze conditions with the addition of traffic from approved and reasonably foreseeable projects that affect study intersections and segments. These scenarios are intended to reflect anticipated traffic approximately five years into the future, when the project could reasonably be anticipated to be constructed. This "phasing analysis" is intended to assist the City of Folsom in phasing of improvements at study intersections which may be necessary to accommodate traffic from all approved and anticipated tentative maps over the next five years.

Cumulative 2035 Condition, and Cumulative 2035 with Project Condition

Cumulative scenarios, with and without the Project, analyze anticipated conditions at the General Plan 2035 horizon year. These scenarios are intended to reflect anticipated traffic from Folsom Ranch, and shifts in traffic patterns anticipated after construction of two new interchanges and US 50 overcrossings.



Figure 5. Project Area Roadways Including Study Intersections and Study Road Segments

2.2 Project Area Roadways

Brief descriptions of the key roadways serving the Project site are provided below.

Iron Point Road is an east-west arterial roadway with a raised median that runs from Folsom Boulevard to the eastern city limit along the north side of US 50. Within the vicinity of the Project, Iron Point Road has six lanes, bike lanes, sidewalk, curb, and gutter. The posted speed limit is 45 mph. Turn pockets are provided at intersections.

Oak Avenue Parkway is a north-south arterial that extends from Willow Creek Drive to Iron Point Road. It is a four-lane urban arterial road between Willow Creek Drive and Blue Ravine Road. It is a six-lane urban arterial road between Blue Ravine Road and Riley Street. It is a four-lane urban arterial road between Riley Street and Iron Point Road. Oak Avenue Parkway will be extended across US 50 into Folsom Ranch and a new interchange will be constructed prior to the cumulative analysis scenarios.

Rowberry Drive is a north-south two-lane local road that runs northward from the Kaiser Permanente Folsom Medical Offices into neighborhoods to the north of Iron Point Road. A future extension of Rowberry across US 50 to Folsom Ranch is planned for the future.

Broadstone Parkway in the project vicinity is a four-lane east-west arterial, that wraps around the back of the Palladio shopping center from Iron Point Road to connect with Empire Ranch Road near the Sacramento-El Dorado county line. Broadstone Parkway has bike lanes, sidewalk, curb, and gutter. Turn pockets are provided at intersections.

East Bidwell Street runs through the City of Folsom from White Rock Road to Riley Street. East Bidwell Street becomes Scott Road south of US 50. Near the Project area, East Bidwell Street is a six-lane arterial roadway with bike lanes, sidewalk, curb, and gutter. Turn pockets are provided at intersections. The speed limit on East Bidwell Street north of US 50 is 45 mph.

Prairie City Road is a north-south arterial that extends from Blue Ravine Road to White Rock Road, north of Blue Ravine Road it is called Sibley Street. It is a five-lane urban arterial road between Blue Ravine Road and Iron Point Road. Prairie City Road is a six-lane urban arterial road between Iron Point Road and Highway 50. It is a two-lane rural road between Highway 50 and White Rock Road.

2.3 Study Intersections

There are twenty study segments on US 50 (Table 1) and seventeen study intersections (Table 2). The Oak Avenue Parkway interchange will be constructed by the cumulative analysis year, resulting in changes to some study US 50 segments.

Table 1. US 50 Study Segment

US 50 Segment	Segment Type	Applicable Years
1. US 50 westbound East Bidwell offramp	Diverge	All
2. US 50 westbound East Bidwell loop onramp	Merge	All
3. US 50 westbound East Bidwell slip onramp	Merge	All
4. US 50 westbound East Bidwell to Oak Ave	Basic	All
5. US 50 westbound Oak Avenue offramp	Diverge	2035
6. US 50 westbound Oak Avenue loop onramp	Merge	2035
7. US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	weave	2035
8. US 50 westbound Prairie City offramp	Diverge	2021/2026
9. US 50 westbound Prairie City loop onramp	Merge	All
10. US 50 westbound Prairie City diagonal onramp	Merge	All
11. US 50 eastbound Prairie City offramp	Diverge	All
12. US 50 eastbound Prairie City diagonal onramp	Merge	All
13. US 50 eastbound Prairie City fly-over onramp	Merge	2021/2026
14. US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	2035
15. US 50 eastbound Oak Avenue loop onramp	Merge	2035
16. US 50 eastbound Oak Avenue diagonal onramp	Merge	2035
17. US 50 eastbound Oak Ave to East Bidwell	Basic	All
18. US 50 eastbound East Bidwell offramp	Diverge	All
19. US 50 eastbound East Bidwell loop onramp	Merge	All
20. US 50 eastbound East Bidwell slip onramp	Merge	All

Table 2. Study Intersections and Control

Intersection	Control
1. Prairie City Rd/US 50 eastbound ramps	Signal
2. Prairie City Rd/US 50 westbound ramps	Signal
3. Prairie City Rd/American Aggregates Rd	Signal
4. Prairie City Rd/Iron Point Rd	Signal
5. Iron Pt Road/Grover Rd	Signal
6. Iron Pt Road/Oak Avenue Pkwy	Signal
7. Iron Pt Road/West Kaiser access road	TWSC*
8. Iron Pt Road/Rowberry Way	Signal
9. Iron Pt Road/Safe Credit Union access	TWSC*
10. Iron Pt Road/Broadstone Pkwy	Signal
11. Iron Pt Road/East Bidwell St	Signal
12. East Bidwell St/US 50 westbound ramps	Signal
13. East Bidwell St/US 50 eastbound ramps	Signal
14. APN 072-3120-023 "Lot 6" access	TWSC*
15. APN 072-3120-023 "Lot 1" access	TWSC*
16. Oak Avenue Pkwy/US 50 westbound ramps (2035 Only)	Signal
17. Oak Avenue Pkwy/US 50 eastbound ramps (2035 Only)	Signal

* Two Way Stop Control

2.4 Transit

City of Folsom's public transportation includes bus and dial-a-ride service provided by the City through "Folsom Stage Lines" and light rail service provided by Sacramento Regional Transit (RT). El Dorado County Transit (EDC Transit) also provides limited bus connections to El Dorado County.

Folsom Stage Lines and Dial-A-Ride

The Folsom Stage Line buses run Monday through Friday. Since February 4, 2019 Folsom Stage Lines has been operated by Sacramento RT. There is no weekend service available. There are currently ten buses running on three routes. They are routes 10, 20 and 30 (Figure 6). Routes 10 and 20 intersect at Folsom Lake College. There is no charge to transfer from one Folsom Stage Line route to the other.

- Route 10 - Services Historic Folsom, E. Bidwell St., the Broadstone Market Place, Broadstone Plaza, Folsom Aquatics Center, Folsom Lake College, Intel, Kaiser Permanente, Folsom Premium Outlets, Mercy Hospital, Palladio Mall, and Century Theatres. It connects to light rail and with the RT bus service Line 24. Service with a one-hour headway starts at 5:25 AM with the last pickup at 7:25 PM.
- Route 20 - Services Empire Ranch Road, East Natoma Street, Vista del Lago High School, Folsom Lake College and transfers to Route 10. There are one morning bus and two afternoon buses on Route 20.

- **Route 30 - Services Folsom State Prison, City Hall, and Woodmere Drive during peak hours (6 AM – 8:10 AM and 2:35 PM – 4:55 PM) with four AM peak-period buses and five PM peak-period buses.**

Dial-A-Ride is a curb-to-curb transportation service that operates within the Folsom City limits. It provides transportation to residents who have a physical, developmental, or mental disability. Senior citizens who are 55 years of age or older also qualify for this program.

Sacramento RT

Sacramento Regional Transit (RT) light rail provides service via the Gold Line connecting the Historic Folsom, Glenn, and Iron Point light rail stations to downtown Sacramento and points in between. Service is provided from 5 AM to 7 PM on a 30-minute headway. There is also a connection to RT bus route 24 from Folsom Stage Lines route 10 at the Madison/Main stop. RT route 24 provides service to Sunrise Mall on a (roughly) hourly headway from 6 AM to 7 PM.

El Dorado County Transit

The EDC Transit route 50X (the 50 Express) operates every hour from 6 AM until 7 PM Monday through Friday, with service from Missouri Flat Transfer Center in El Dorado County to the Folsom Iron Point light rail station, Folsom Lake College, and back.

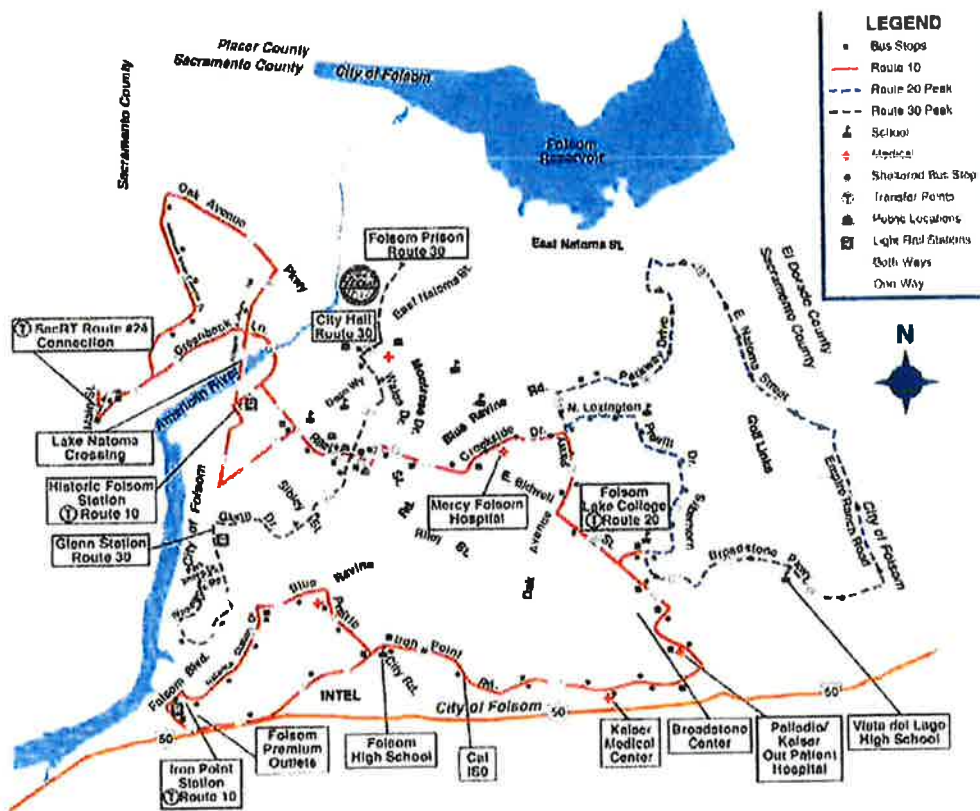


Figure 6. Folsom Stage Lines Routes 10, 20 and 30

2.5 Bicycle Facilities

The City of Folsom is one of the most bike friendly settings in California, with an existing comprehensive bikeway system that is extensive and connects to a vast number of historical and recreational attractions. Existing and planned bicycle facilities within the project area are described in the 2007 Folsom Bikeway Master Plan¹ which provide a framework for the design of a bikeway system that meets the California Street and Highway Code Section 890-894.2 - Bicycle Transportation Act and improves safety and convenience for all users. (Note that there is an updated bike plan under development as part of the Folsom Active Transportation Plan.) There are four types of bicycle facilities (Class 1, 2, 3, and 4) used in Folsom. Figure 7 provides a Folsom bike map. All road segments in the study area include Class 2 bike lanes. There are existing and planned Class 1 trails along Iron Point Road, as well as a class 1 trail connecting under US 50 paralleling the rail line located to the east of East Bidwell Street. The different classes of bicycle facilities are described after Figure 7.

¹ Folsom (2007) Bikeway Master Plan, www.folsom.ca.us/city_hall/depts/parks/parks_n_trails/trails/bikeway_master_plan.asp.

Folsom Corporate Center Apartments
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Figure 7. Folsom Bike Map

Class I Bikeway (Bike Trail)

Class I bikeways, unless adjacent to an adequate pedestrian facility, are for the exclusive use of bicycles and pedestrians, therefore any facility serving pedestrians must meet accessibility requirements. Note that sidewalks are not Class I bikeways because they are primarily intended to serve pedestrians, generally cannot meet the design standards for Class I bikeways, and do not minimize vehicle cross flows. Motor vehicles are prohibited from bike paths per the California Vehicle Code (CVC). These prohibitions can be reinforced with signs. Within the Project vicinity there are Class 1 trails along the east side of the American River/Lake Natoma, the east side of Folsom Boulevard, and connections between those two trails both north and south of the Project site.

Generally, bike paths should be used to serve corridors not served by streets and highways or where a wide right-of-way exists, permitting such facilities to be constructed away from the influence of parallel streets. Bike paths should offer opportunities not provided by the road system. They can either provide a recreational opportunity, or in some instances, can serve as direct high-speed commute routes if cross flow by motor vehicles and pedestrian conflicts can be minimized. The most common applications are along rivers, ocean fronts, canals, utility right of way, abandoned railroad right of way, within school campuses, or within and between parks. There may also be situations where such facilities can be provided as part of planned developments. Another common application of Class I facilities is to close gaps to bicycle travel caused by construction of freeways or because of the existence of natural barriers (rivers, mountains, etc.).

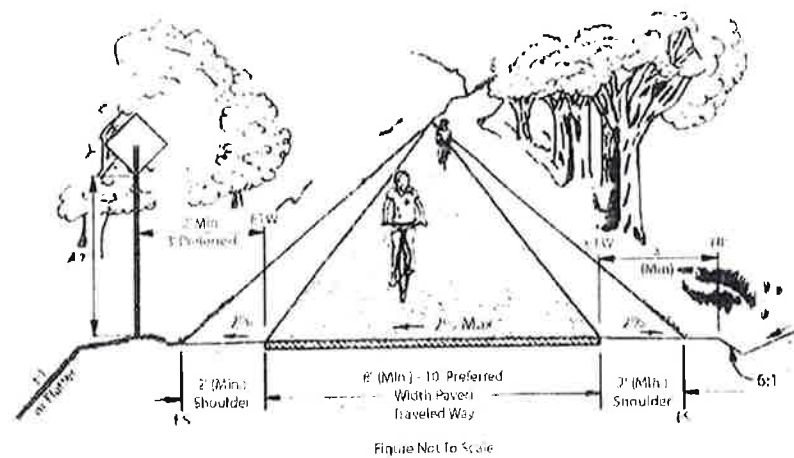


Figure 8. Two-Way Class I Bikeway (Source: Caltrans 2012 HDM Figure 1003.1A)

Class II Bikeway (Bike Lane)

Class II Bikeways are bike lanes generally striped along streets in corridors where there is significant bicycle demand, and where there are distinct needs that can be served by them. The purpose should be to improve conditions for bicyclists in the corridors. Bike lanes are intended to delineate the right-of-way assigned to bicyclists and motorists and to provide for more predictable movements by each. But a more important reason for constructing bike lanes is to better accommodate bicyclists through corridors where insufficient room exists for side-by-side sharing of existing streets by motorists and bicyclists. This can be accomplished by reducing the number of lanes, reducing lane width, or prohibiting or reconfiguring parking on given streets in order to delineate bike lanes. In addition, other things can be done on bike lane streets to improve the situation for bicyclists that might not be possible on all streets (e.g., improvements to the surface, augmented sweeping programs, special signal facilities, etc.). Generally, pavement markings alone will not measurably enhance bicycling.

If bicycle travel is to be provided by delineation, attention should be made to assure that high levels of service are provided with these lanes. It is important to meet bicyclist expectations and increase bicyclist perception of service quality where capacity analysis demonstrates service quality measures are improved, from the bicyclist's point of view.

Class III Bikeway (Bike Route)

Bike routes are unstriped, shared facilities which serve either to:

- Provide continuity to other bicycle facilities (usually Class II bikeways); or
- Designate preferred routes through high demand corridors.

As with bike lanes, designation of bike routes should indicate to bicyclists that there are advantages to using these routes as compared with alternative routes. This means that responsible agencies have taken actions to assure that these routes are suitable as shared routes and will be maintained in a manner consistent with the needs of bicyclists. Normally, bike routes are shared with motor vehicles.

A variant on Class III bikeways, shared lanes, or "sharrow" lanes, are becoming more common. Sharrows are a form of Class III bikeways where the general-purpose lane is too narrow for a bicycle and a vehicle to travel safely side-by-side within the same lane. A sharrow symbol painted (Figure 9) on the roadway is used to indicate the likely lateral location of bikes in the lane to inform motor vehicles.



Figure 9. Sharrow

Class IV Bikeway (Separated Bikeway or "Cycle Track")

The Protected Bikeways Act of 2014 (Assembly Bill 1193 - Ting, Chapter 495) established Class IV bikeways for California. Class IV bikeways provide a right-of-way designated exclusively for bicycle travel adjacent to a roadway and which are protected from vehicular traffic. Types of separation include, but are not limited to, grade separation, flexible posts, inflexible physical barriers, or on-street parking. An example is shown in Figure 10.

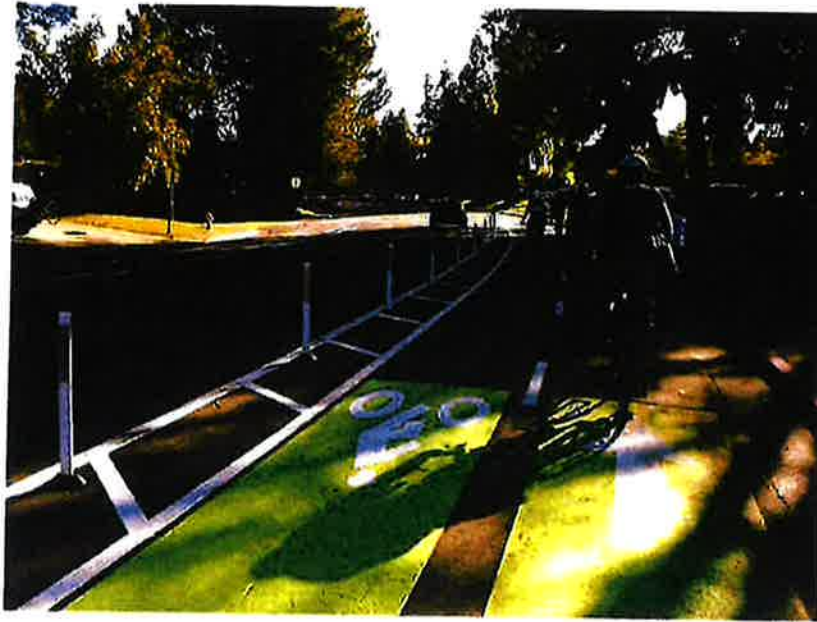


Figure 10. Class IV Bikeway

(source: Gary Kavanagh image 1272: <https://flic.kr/p/hxp5e1>)

3. METHODOLOGY

This section provides a process overview, describes traffic forecasting, and discusses the methods/criteria used to evaluate level-of-service. A discussion of the significance criteria is also included.

3.1 Process Overview

The overall analysis process was structured to identify potential adverse transportation effects related to the proposed project.

- Traffic volumes and turning movements for the Existing 2021 Condition were determined from observed traffic counts taken on Thursday May 5, 2020 (pre pandemic); Tuesday May 18, 2021, and Thursday August 26, 2021. Consistent with other recent Folsom traffic studies, “post pandemic” counts were factored up to account for the impact of COVID 19 closures on the transportation system. AM peak-hour counts were increased by 52% and PM peak-hour counts were increased by 28%.
- EPAP 2026 volumes without the Project were based on growth from all reasonably foreseeable projects effecting the study intersections based on the greater of two forecasting approaches:
 - Trips from approved projects and reasonably foreseeable projects, or five years of growth based on the City of Folsom General Plan travel demand model. Travel demand model growth was based on linear interpolation between the model base year and cumulative year, with the cumulative year trip tables assigned to the base year network to eliminate the effects of the future Oak Avenue Parkway interchange and Empire Ranch interchange.
 - Travel demand model growth was used in this study because it resulted in higher traffic volumes than growth from identified projects. Particularly at the intersections of Iron Point Road and Prairie City Road.
 - The travel demand model was calibrated to local conditions using the traffic counts and travel demand model forecasts interpolated to 2021. The NCHRP 255 adjustment was applied to all future volume forecasts at intersections 1-13. Volumes at intersections 14 and 15 were scaled up based on growth on travel demand model growth on their TAZ’s centroid connectors. 2021 traffic counts were used as a floor to protect against negative growth
- Cumulative 2035 traffic volumes were based on existing traffic counts adjusted for growth from the City of Folsom General Plan travel demand model. Local calibration and NCHRP adjustments were applied similar to the 2026 methodology described above. Turning movements at the Oak Avenue Parkway interchange (intersections 16 and 17) were taken directly from the travel demand model.

- Study intersection and segment traffic operations were analyzed both with and without the proposed project to identify potential violations of General Plan level-of-service policies.
- California Environmental Quality ACT (CEQA) VMT impacts were evaluated using screening tools published by the Sacramento Area Council of Governments (SACOG).

3.2 Level-of-Service Methodology

Level-of-service (LOS) is a qualitative indication of the level of delay and congestion experienced by motorists using an intersection. Levels-of-service are designated by the letters A through F, with A being the best conditions and F being the worst (high delay and congestion). Calculation methodologies, measures of performance, and thresholds for each letter grade differ for road segments, signalized intersections, and unsignalized intersections.

Based on guidance from City of Folsom staff, the following procedures described below for intersection and segment traffic operations analysis were selected for this study.

Intersection Traffic Operations Analysis

Signalized Intersections

The methodology from the Highway Capacity Manual (HCM) 6th Edition², are used to analyze signalized intersections. Level-of-service can be characterized for the entire intersection, each approach, or by lane group. Control delay alone (the weighted average delay for all vehicles entering the intersection) is used to characterize level-of-service for the entire intersection or an approach. Control delay and volume to capacity ratio are used to characterize level-of-service for lane groups. The average delay criteria used to determine the level-of-service at signalized intersections is presented in Table 3. The HCM 2010 methodology is used as the primary method. HCM 2000 methods are only utilized where the signal phasing is incompatible with HCM 2010 methods.

Table 3. Level-of-Service Criteria for Signalized Intersections

Level -of- Service	Description	Average Delay ¹ (Sec. /Vehicle.)
A	Very Low Delay: This level-of-service occurs when progression is extremely favorable, and most vehicles arrive during a green phase. Most vehicles do not stop at all.	≤ 10.0
B	Minimal Delays: This level-of-service generally occurs with good progression, short cycle lengths, or both. More vehicles stop than at LOS A, causing higher levels of average delay.	10.1-20.0
C	Acceptable Delay: Delay increases due to only fair progression, longer cycle lengths, or both. Individual cycle failures (<i>to service all waiting vehicles</i>) may begin to appear at this level of service. The number of vehicles stopping is significant, though many still pass through the intersection without stopping.	20.1-35.0

² Transportation Research Board (2016) Highway Capacity Manual, Washington, D.C.

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D	Approaching Unstable/Tolerable Delays: The influence of congestion becomes more noticeable. Longer delays may result from some combination of unfavorable progression, long cycle lengths, or high v/c ratios. Many vehicles stop, and the proportion of vehicles not stopping declines. Individual cycle failures are noticeable.	35.1-55.0
E	Unstable Operation/Significant Delays: This is considered by many agencies the upper limit of acceptable delays. These high delay values generally indicate poor progression, long cycle lengths, and high v/c ratios. Individual cycle failures are frequent occurrences.	55.1-80.0
F	Excessive Delays: This level, considered to be unacceptable to most drivers, often occurs with oversaturation (i.e., when arrival flow rates exceed the capacity of the intersection). It may also occur at high v/c ratios below 1.00 with many individual cycle failures. Poor progression and long cycle lengths may also contribute to such delay levels.	> 80.0 or v/c >1.0

Note 1: Weighted average of delay on all approaches. This is the measure used by the Highway Capacity Manual to determine level-of-service. Any movement with a volume-to-capacity ratio (v/c) greater than 1.0 is considered to be level-of-service F.

Source: Transportation Research Board (2016) Highway Capacity Manual 6th Edition, Washington D.C.

Unsignalized Intersections

The methodology from HCM 6th Edition is used for the analysis of unsignalized intersections. At an unsignalized intersection, most of the main street traffic is un-delayed, and by definition have acceptable conditions. The main street left-turn movements and the minor street movements are all susceptible to delay of varying degrees. Generally, the higher the main street traffic volumes, the higher the delay for the minor movements. Separate methods are utilized for Two-Way Stop-Controlled (TWSC) intersections and All-Way Stop-Controlled (AWSC) intersections.

- **TWSC:** The methodology for analysis of two-way stop-controlled intersections calculates an average total delay per vehicle for each minor street movement and for the major street left-turn movements, based on the availability of adequate gaps in the main street through traffic. A level-of-service designation is assigned to individual movements or combinations of movements (in the case of shared lanes) based upon delay, it is not defined for the intersection as a whole. Unsignalized intersection level-of-service reported herein is for each movement (or group of movements) based upon the respective average delay per vehicle. Table 4 presents the average delay criteria used to determine the level-of-service at TWSC and AWSC intersections.
- **AWSC:** At all-way stop-controlled intersections, the level-of-service is determined by the weighted average delay for all vehicles entering the intersection. The methodologies for these types of intersections calculate a single weighted average delay and level-of-service for the intersection as a whole. The average delay criteria used to determine the level-of-service at all-way stop intersections is the same as that presented in Table 4. Level-of-service for specific movements can also be determined based on the TWSC methodology.

It is not unusual for some of the minor street movements at unsignalized intersections to have level-of-service D, E, or F conditions while the major street movements have level-of-service A, B, or C conditions. In such a case, the minor street traffic experiences delays that can be substantial for individual minor street vehicles, but the majority of vehicles using the intersection have very little delay. Usually in such cases, the minor street traffic volumes are relatively low. If the minor street volume is large enough, improvements to reduce the minor street delay may be justified, such as channelization, widening, or signalization.

Table 4. Level-of-Service Criteria for Unsignalized Intersections

Level of Service (LOS)	Description	TWSC ¹	AWSC ²
		Average Delay by Movement (seconds / vehicle)	Intersection Wide Average Delay (seconds / vehicle)
A	Little or no delay	< 10	< 10
B	Short traffic delay	> 10 and < 15	> 10 and < 15
C	Average traffic delays	> 15 and < 25	> 15 and < 25
D	Long traffic delays	> 25 and < 35	> 25 and < 35
E	Very long traffic delays	> 35 and < 50	> 35 and < 50
F	Extreme delays potentially affecting other traffic movements in the intersection	> 50 (or, v/c > 1.0)	> 50

Note 1: Two-Way Stop-Control (TWSC) level-of-service is calculated separately for each minor street movement (or shared movement) as well as major street left turns using these criteria. Any movement with a volume to capacity ratio (v/c) greater than 1.0 is considered to be level-of-service F.

Note 2: All-Way Stop-Control (AWSC) assessment of level-of-service at the approach and intersection levels is based solely on control delay.

Source: Transportation Research Board (2016) Highway Capacity Manual 6th Edition, Washington D.C.

Signal Warrants

At each unsignalized intersection, the potential need for a traffic signal was evaluated. Traffic signal warrants are a series of standards that provide guidelines for determining if a traffic signal is appropriate. Signal warrant analyses are typically conducted at intersections of uncontrolled major streets and stop sign-controlled minor streets. If one or more signal warrants are met, signalization of the intersection may be appropriate. However, a signal should not be installed if none of the warrants are met, since the installation of signals would increase delays on the previously uncontrolled major street, and, may increase the occurrence of particular types of accidents.

As stated in the 2014 California Edition of the Manual on Uniform Traffic Control Devices (California MUTCD 2014)³, "An engineering study of traffic conditions, pedestrian characteristics,

³ Caltrans (2019) California Manual on Uniform Traffic Control Devices - FHWA's MUTCD 2009 Edition as amended for use in California - 2014 Edition - Revision 4, March 29, 2019. Section 4C.

and physical characteristics of the location shall be performed to determine whether installation of a traffic control signal is justified at a particular location.

The investigation of the need for a traffic control signal shall include an analysis of factors related to the existing operation and safety at the study location and the potential to improve these conditions, and the applicable factors contained in the following traffic signal warrants:

- *Warrant 1, Eight-hour Vehicular Volume*
- *Warrant 2, Four-hour Vehicular Volume*
- *Warrant 3, Peak-hour*
- *Warrant 4, Pedestrian Volume*
- *Warrant 5, School Crossing*
- *Warrant 6, Coordinated Signal System*
- *Warrant 7, Crash Experience*
- *Warrant 8, Roadway Network*
- *Warrant 9, Intersection Near a Grade Crossing*

The satisfaction of a traffic signal warrant or warrants shall not in itself require the installation of a traffic control signal."

Consistent with the industry standard of practice, this Traffic Impact Analysis did not evaluate the full panoply of warrants for traffic signals, but instead focused on the peak-hour warrant. The MUTCD states that, "*This [peak-hour] signal warrant shall be applied only in unusual cases, such as office complexes, manufacturing plants, industrial complexes, or high-occupancy vehicle facilities that attract or discharge large numbers of vehicles over a short time.*" So, the peak-hour warrant is being used in this impact analysis study as an "indicator" of the likelihood of an unsignalized intersection warranting a traffic signal in the future. Intersections that exceed the peak-hour warrant are considered (for the purposes of this impact analysis) to be likely to meet one or more of the other signal warrants (such as the 4-hour or 8-hour warrants). This peak-hour analysis is not intended to replace a rigorous and complete traffic signal warrant analysis by the responsible jurisdiction.

Unsignalized intersections were evaluated using the Peak-hour Volume Warrant (Warrant No. 3) in the California MUTCD 2014. The Peak-hour Volume Warrant was applied where the minor street experiences long delays in entering or crossing the major street for at least one hour in a day.

Even if the Peak-hour Volume Warrant is met, a more detailed signal warrant study is recommended before a signal is installed. The more detailed study should consider volumes during the daily peak-hours of roadway traffic, pedestrian traffic, and accident histories.

Freeway Segment Analysis

Freeway merge/diverge segments and basic segments were analyzed utilizing the methodologies outlined in Chapters 12 and 13 of the Highway Capacity Manual, 2010 (HCM 2010)⁴.

Basic Segments

Basic freeway segments operations and level-of-service is defined by density (passenger cars per mile per lane) which depends upon traffic volumes, and segment, characteristics. These characteristics include the geometry, grade, free flow speeds, and heavy vehicles. Table 6 shows the relationship of level-of-service to freeway density for merge, diverge, and weaving segments.

Table 5. Level-of-Service Criteria – Basic Freeway Segments

Level of Service	Maximum Density (passenger vehicles per mile per lane)
A	<11
B	18
C	26
D	35
E	45
F	> 45, or Demand exceeds capacity

Source: Transportation Research Board (2010) Highway Capacity Manual, Chapter 11, Washington, D.C.

Merge, Diverge, and Weave Segments

Freeway merge and diverge segments operations and level-of-service is defined by density (passenger cars per mile per lane) which depends upon traffic volumes and the ramp characteristics. These characteristics include the length and type of acceleration/deceleration lanes, free-flow speeds, number of lanes, grade, heavy vehicles, and types of facilities. Table 6 and Table 7 shows the relationship of level-of-service to freeway density for merge, diverge, and weaving segments.

Table 6. Level-of-Service Criteria – Freeway Ramp Merge/Diverge Areas

Level of Service	Maximum Density (passenger vehicles per mile per lane)
A	<10
B	20
C	28
D	35
E	> 35
F	Demand exceeds capacity

Source: Transportation Research Board (2010) Highway Capacity Manual, Chapter 13, Washington, D.C.

⁴ Transportation Research Board (2010) Highway Capacity Manual, Washington, D.C.

Table 7. Level-of-Service Criteria - Freeway Weaving Areas

Level of Service	Maximum Density (passenger vehicles per mile per lane)
A	0-10
B	>10-20
C	>20-28
D	>28-35
E	> 35-43
F	>43, or demand exceeds capacity

Source: Transportation Research Board (2016) Highway Capacity Manual, Chapters 13, Washington, D.C.

3.3 Standards of Significance

Level-of-service impacts of the proposed project were determined based on the methods described above and identified as either "significant" or "less-than-significant" in the following thresholds:

City of Folsom

Policy M 4.13 of the City of Folsom General Plan (adopted August 28, 2018) calls for the City to:

Strive to achieve at least traffic Level of Service "D" (or better) for local streets and roadways throughout the City. In designing transportation improvements, the City will prioritize use of smart technologies and innovative solutions that maximize efficiencies and safety while minimizing the physical footprint. During the course of plan buildout, it may occur that temporarily higher levels-of-service result where roadway improvements have not been adequately phased as development proceeds. However, this situation will be minimized based on annual traffic studies and monitoring programs. City Staff will report to the City Council at regular intervals via the Capital Improvement Program process for the Council to prioritize projects integral to achieving level-of-service D or better.

Consistent with historical practice within the City of Folsom, the General Plan EIR also includes a criterion addressing potential impacts at locations that operate at level-of-service E or F under no-project conditions. Under that standard, a significant impact would occur if the proposed project would:

Increase the average delay by five seconds or more at an intersection that currently operates (or is projected to operate) at an unacceptable level-of-service under "no-project" conditions.

For the purposes of this analysis, an impact is considered potentially significant if implementation of the Project would result in any of the following:

- Cause an intersection in Folsom that currently operates (or is projected to operate) at level-of-service D or better to degrade to level-of-service E, or worse;

- Increase the average delay by five seconds or more at an intersection in Folsom that currently operates (or is projected to operate) at an unacceptable level-of-service E or F.

Freeway Facilities

An impact is considered significant on freeway facilities if the project causes the facility to change from an acceptable to unacceptable level-of-service. For facilities that are or will be operating at unacceptable level-of-service without the project, an impact is considered significant if:

- The existing level-of-service cannot be maintained with the addition of project traffic;
- The project traffic increases vehicle density on a freeway mainline segment or freeway ramp junction by 0.1 passenger cars per lane per mile;
- The project increases the number of peak-hour vehicles on a freeway mainline segment or freeway ramp junction by more than 1 percent.

Per the Caltrans' Guide for the Preparation of Traffic Impact Studies, Caltrans strives to maintain a target level of service at the transition between level-of-service C and level-of-service D on state highway facilities. However, for the effected portion of US 50, Caltrans has established a concept level-of-service E threshold⁵. For consistency with other traffic impact studies performed in the City of Folsom that considered US 50, level-of-service E was selected as the minimum standard for all study freeway facilities.

Bicycle/Pedestrian/Transit Facilities

An impact is considered significant if implementation of the Project would:

- Inhibit the use of bicycle, pedestrian, or transit facilities;
- Eliminate existing bicycle, pedestrian, or transit facilities;
- Prevent the implementation of planned bicycle, pedestrian, or transit facilities.

3.6 Analysis Tools

Macroscopic Intersection Analysis

Control delay and level-of-service for study intersections were calculated using the PTV Vistro⁶ analysis software (Version 2022 SP 0-0). Vistro is a software package for modeling vehicle delay and optimizing traffic signal timings. Version 6 implements the methodologies of the 2000 (4th Ed.), 2010 (5th Ed.), and the 6th Ed. of the HCM for signalized and unsignalized intersections. Vistro requires data on road characteristics (geometric), traffic counts, and the signal timing data for each analysis intersection.

⁵ Caltrans (2014) Transportation Concept Report and Corridor System Management Plan, United States Route 50, district 3, California Department of Transportation, June 27, 2014

⁶ PTV (2021) Vistro, PTV America, Portland OR.

Macroscopic Freeway Analysis

Basic freeway segments, merge, and diverge segments were analyzed using FREEVAL 2015e⁷. FREEVAL provides freeway planning-level capacity analyses based on HCM 6th Edition for undersaturated and oversaturated conditions for estimating vehicle density and level-of-service.

⁷ Lake Trask, Aghdashi, B., Schroeder, S., and Roupail, N. (2015) Freeway Facilities And Reliability Analysis Computational Engine For The HCM 6th Edition: A Guide For Multimodal Mobility Analysis, North Carolina State University, Raleigh NC, <http://freeval.org/#home>.

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4. EXISTING 2021 CONDITIONS

This section presents the Existing Condition. For purposes of this study, Existing Conditions represent typical midweek, non-holiday, traffic volumes in late August/early September of 2021 adjusted to negate the impact of the COVID-19 pandemic on traffic volumes.

4.1 Existing 2021 Condition

Data Sources

The analysis tools require a variety of data to generate the evaluation criteria. The following sections describe data collection procedures for Existing Conditions. There were three primary data elements (roadway characteristics, intersection turning movement counts, and traffic control data); and two supplementary elements (other recent studies, and field data) that comprised the data collection program for this traffic analysis.

Roadway Geometry and Usage Characteristics

The geometry and usage data for the analysis were collected through aerial photographs, field visits, and prior studies. Current intersection geometry was field validated. Table 8 shows the key items included in the geometric data and the source for each item.

Table 8. Key Items and Sources for Geometry and Usage Data

Key Item	Source
Lane configurations and width	Aerial photographs and field visits
Lane utilization	Prior studies, aerial photographs, and field visits
Intersection spacing	Aerial photographs and field visits
Length of storage bays	Aerial photographs and field visits
Transit stops and routes	Transit schedules, aerial photographs, and field visits
Turn prohibitions or allowance	Aerial photographs, field visits, and traffic counts

Lane configurations and width – These data specify the number of lanes and the width of the roadway in each direction, and the directional turns that are allowed from each lane.

Lane utilization – These data specify how lanes are used by drivers, such as traffic distribution between lanes on a multi-lane roadway.

Intersection spacing – These data refer to the distance (in feet) between intersections.

Length of storage bays – These data refer to the length (in feet) of available storage for left-turning or right-turning vehicles where exclusive turn lanes are available. It is collected for right-turn lanes when the parking lane is used as a right-turn lane.

Transit stops and routes – A transit stop is an area where passengers await, board, alight, and transfer between transit vehicles. A transit route is the roadway that transit vehicles operate on.

Turn prohibitions or allowance – These data specify if right turns on red (RTOR) are allowed on the roadway.

Intersection Turning Movement Counts

Existing morning and evening peak-period vehicle and pedestrian turning movement counts were collected at study intersections on Thursday May 5, 2020; Tuesday May 18, 2021; and Thursday August 26, 2021. Pre COVID-19 pandemic counts, collected along East Bidwell Street on March 5, 2020, were used to factor up the 2021 counts to account for short term traffic reductions caused by the economic effect of COVID-19. AM peak hour counts were factored up by 52% and PM peak-hour counts were factored up by 28%. Traffic count data sheets are provided in **Appendix A** of this report. Peak-hour traffic counts were used to conduct the intersection level-of-service analysis. Turning movement counts at consecutive intersections were balanced and adjusted where appropriate to conservatively reflect existing traffic flows. Observed intersection peak hour factors (PHF) were applied. **Figure 11** provides a summary of the intersection lane geometry and peak-period turning movements under Existing Conditions.

Existing Condition Intersection and Segment Level-of-Service

Table 9 and **Table 10** present a summary of level-of-service results for the study intersections under Existing Conditions. The results indicate that all study segments are anticipated to operate at an acceptable level-of-service. Three study intersections exceed the General Plan level-of-service standard prior to the addition of Project traffic.

- Prairie City Rd/American Aggregate Dr would operate at a deficient level-of-service during the AM peak if not for the Covid-19 related traffic reductions.
- Prairie City Rd/Iron Point Rd would operate at a deficient level-of-service during the AM and PM peak if not for the Covid-19 related traffic reductions.
- East Bidwell St/Iron Point Rd would operate at a deficient level-of-service during the PM peak if not for the Covid-19 related traffic reductions.

These locations are shown in orange highlight in the tables below. Calculation sheets for intersection delay and level-of-service are provided in **Appendix B**.

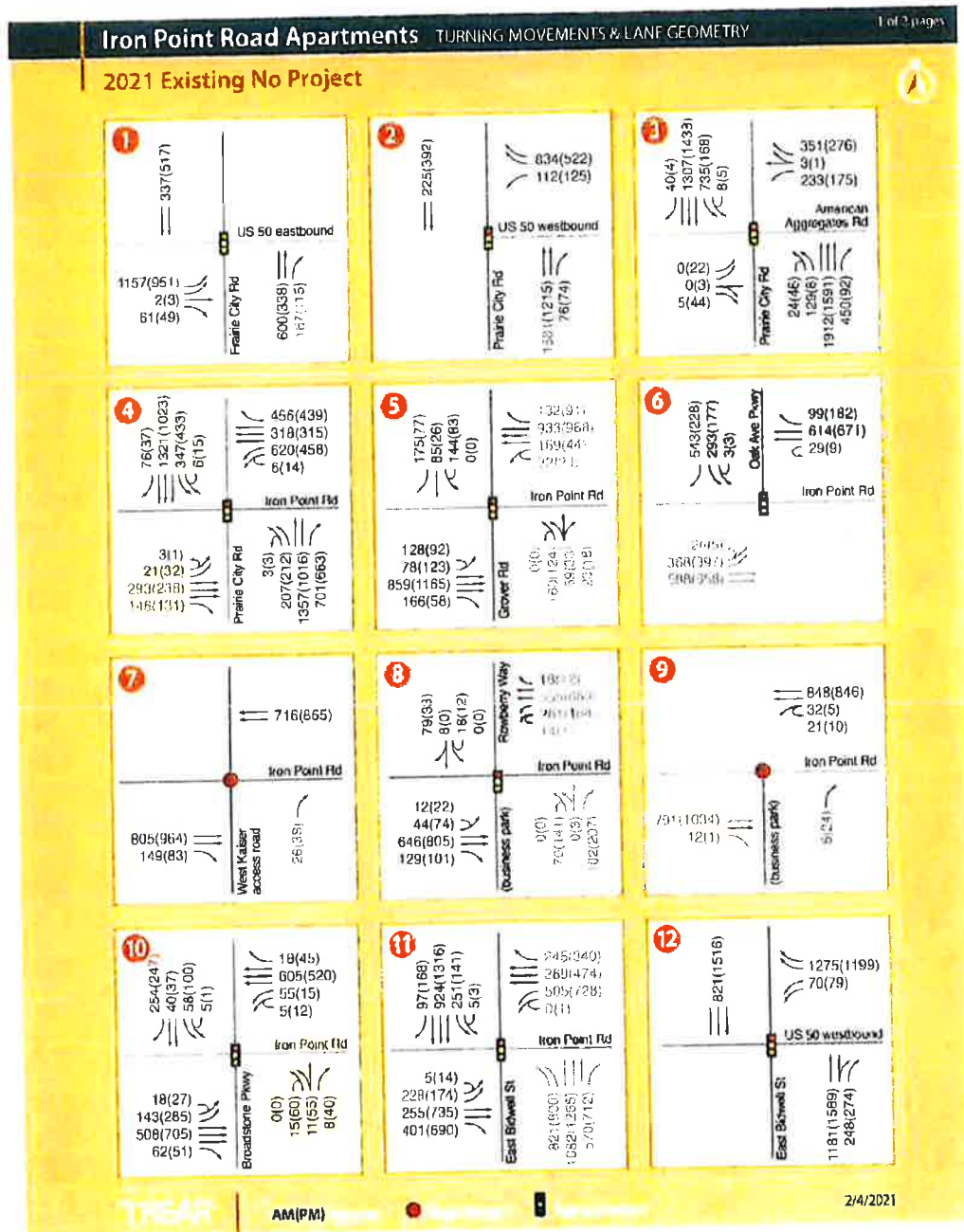


Figure 11. Existing Condition Turn Movements and Geometry

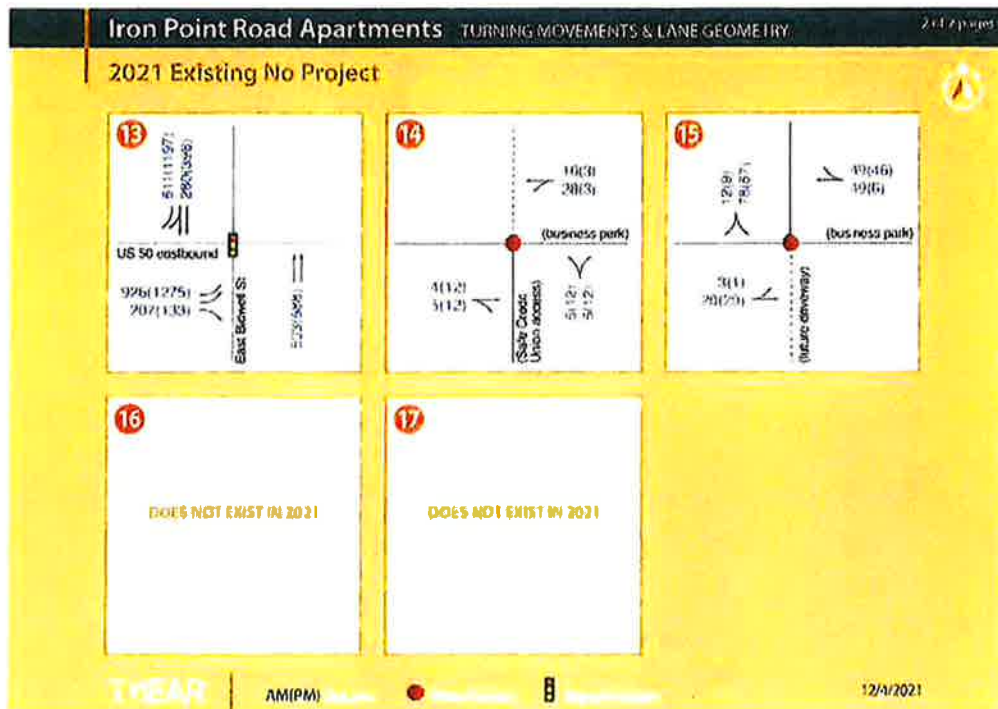


Figure 11. Existing Condition Turn Movements and Geometry (continued)

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Table 9. Existing 2021 Intersection Delay and Level-of-Service (LOS)

Intersection	Control	Without Project	
		AM (Delay LOS*)	PM (Delay LOS*)
1. Prairie City Rd/US 50 eastbound ramps	Signal	10.3 B	8.3 A
2. Prairie City Rd/US 50 westbound ramps	Signal	19.4 B	8.9 A
3. Prairie City Rd/American Aggregates Rd	Signal	66.1 E	28.8 C
4. Prairie City Rd/Iron Point Rd	Signal	88.7 F	64.5 E
5. Iron Point Rd /Grover Rd	Signal	50.9 D	42.3 D
6. Iron Point Rd /Dak Avenue Pkwy	Signal	36.2 D	37.8 D
7. Iron Point Rd /West Kaiser access road	TWSC**	11.9 B Northbound	12.9 B Northbound
8. Iron Point Rd /Rowberry Way	Signal	14.3 B	14.2 B
9. Iron Point Rd /Safe Credit Union access	TWSC**	15.6 C WB left/U	23.1 C WB left/U
10. Iron Point Rd /Broadstone Pkwy	Signal	15.6 B	19.6 B
11. Iron Point Rd /East Bidwell St	Signal	45.5 D	94.3 F
12. East Bidwell St/US 50 westbound ramps	Signal	29.5 C	35.1 D
13. East Bidwell St/US 50 eastbound ramps	Signal	10.2 B	21.5 C
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.6 A Southbound	9.3 A Southbound

* Level of Service

** Two Way Stop Control: LOS is defined by delay for the worst movement/shared movement, which is listed with the LOS results.

Table 10. Existing 2021 US 50 Segment Density and Level-of-Service (LOS)

US 50 Segment	Segment Type	Without Project	
		AM (Density LOS*)	PM (Density LOS*)
1. US 50 westbound East Bidwell offramp	Diverge	24.5 C	17.3 B
2. US 50 westbound East Bidwell loop onramp	Merge	22.9 C	17.1 B
3. US 50 westbound East Bidwell slip onramp	Merge	24.3 C	19.0 B
4. US 50 westbound East Bidwell to Oak Ave	Basic	24.8 C	18.8 C
5. US 50 westbound Oak Avenue offramp	Diverge	Not applicable to this scenario	
6. US 50 westbound Oak Avenue loop onramp	Merge		
7. US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	Weave		
8. US 50 westbound Prairie City offramp	Diverge	32.0 D	26.1 C
9. US 50 westbound Prairie City loop onramp	Merge	24.1 C	21.6 C
10. US 50 westbound Prairie City diagonal onramp	Merge	24.5 C	21.5 C
11. US 50 eastbound Prairie City offramp	Diverge	28.6 D	31.0 D
12. US 50 eastbound Prairie City diagonal onramp	Merge	18.6 B	23.2 C
13. US 50 eastbound Prairie City fly-over onramp	Merge	19.6 B	25.4 C
14. US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	Not applicable to this scenario	
15. US 50 eastbound Oak Avenue loop onramp	Merge		
16. US 50 eastbound Oak Avenue diagonal onramp	Merge		
17. US 50 eastbound Oak Ave to East Bidwell	Basic	17.5 B	23.5 C
18. US 50 eastbound East Bidwell offramp	Diverge	10.4 B	16.5 B
19. US 50 eastbound East Bidwell loop onramp	Merge	9.3 A	13.9 B
20. US 50 eastbound East Bidwell slip onramp	Merge	7.5 A	13.1 B

* Level of Service

4.2 Assessment of Proposed Project

Trip Generation

Traffic generated by the proposed project was based on Institute of Transportation Engineers (ITE) Trip Generation Manual, 10th Edition (2017), and is provided in Table 11 below.

Table 11. Project Trip Generation

Location	Quantity	Units	Metric	Daily	AM Peak-Hr			PM Peak-Hr		
					Tot	In	Out	Tot	In	Out
"Lot 6"	100	du	Rate	5.44	0.32	27%	73%	0.41	60%	40%
			Trips	544	32	9	23	41	25	16
"Lot 1"	153	du	Rate	5.44	0.32	27%	73%	0.41	60%	40%
			Trips	832	49	13	36	63	38	25
Total	253	du	Rate	5.44	0.32	27%	73%	0.41	60%	40%
			Trips	1376	81	22	59	104	62	42

Source: ITE (2017) Trip Generation Manual, 10th Ed, Institute of Transportation Engineers, Washington DC.

Trip Distribution and Assignment

Trip distribution was based on observed traffic counts and select zone analysis within the travel demand model, and nearby projects. Because of the planned additions of freeway crossings and interchanges by 2035, separate distributions and assignments were done for existing 2021/EPAP 2026 conditions and Cumulative 2035 condition.

Project trip distribution and assignment for existing 2021 and EPAP 2026 conditions are shown in Figure 12 and Figure 13. Project trip distribution and assignment for existing 2021 and EPAP 2026 conditions are shown in Figure 14 and Figure 15.



Figure 12. Project Trip Distribution for Existing 2021 and EPAP 2026 Conditions



Figure 13. Project Trip Assignment for Existing 2021 and EPAP 2026 Conditions

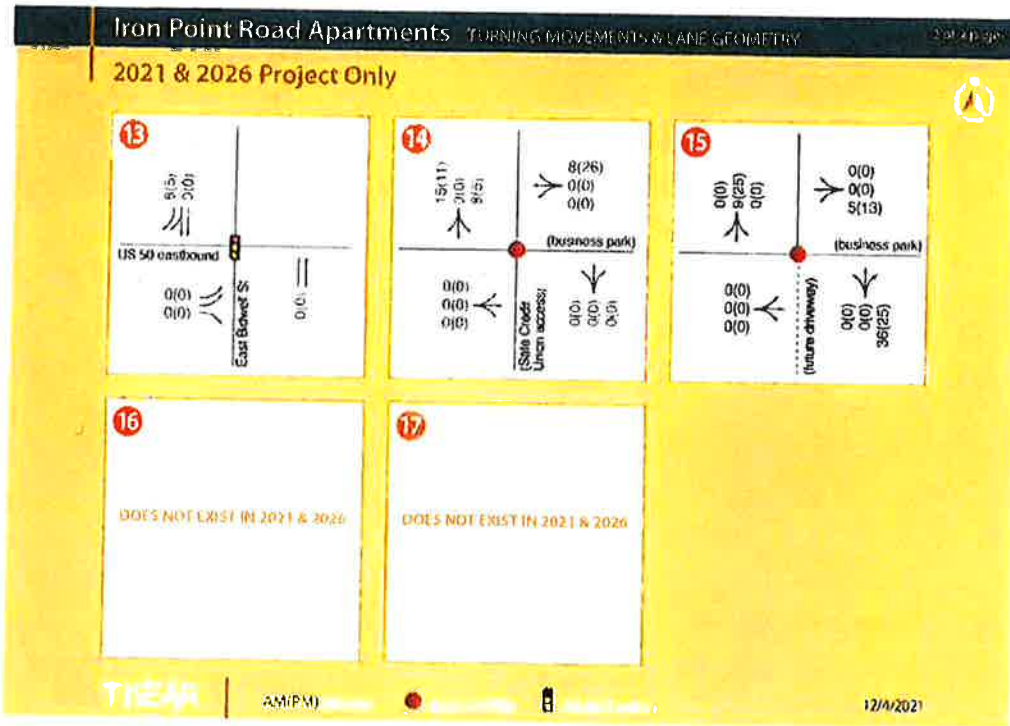


Figure 13. Project Trip Assignment for Existing 2021 and EPAP 2026 Conditions (continued)

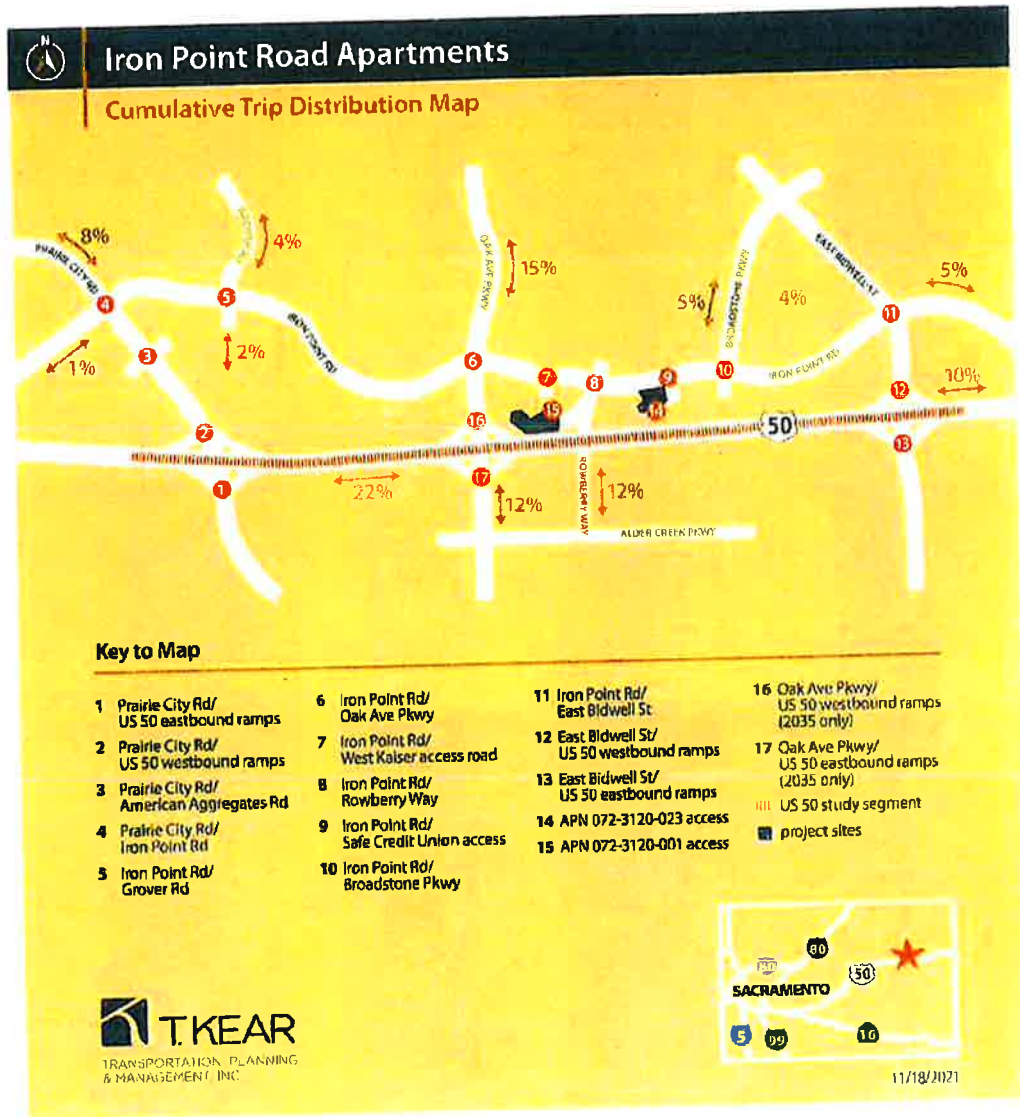


Figure 14. Project Trip Distribution for Cumulative 2035 Conditions

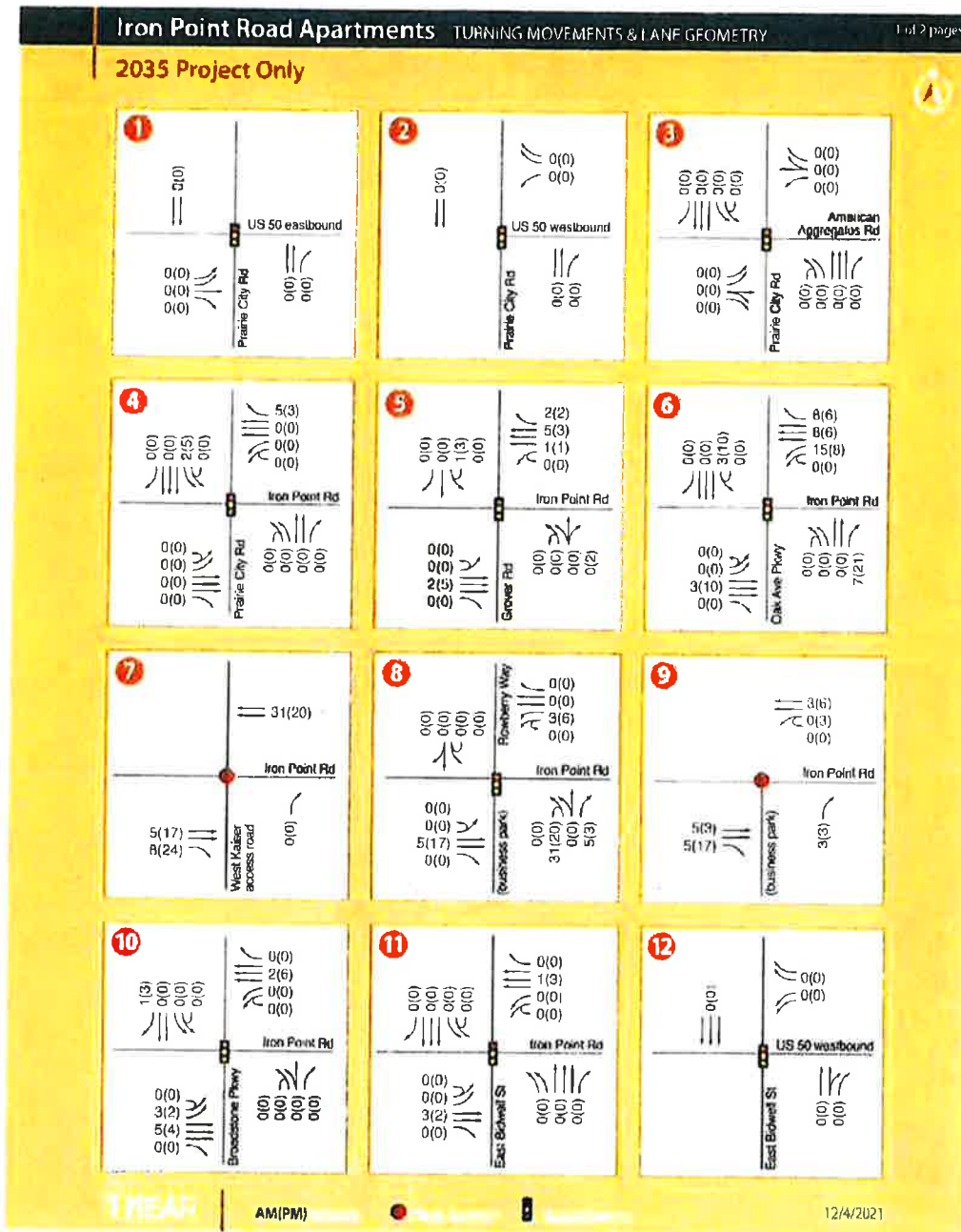


Figure 15. Project Trip Assignment for Cumulative 2035 Conditions

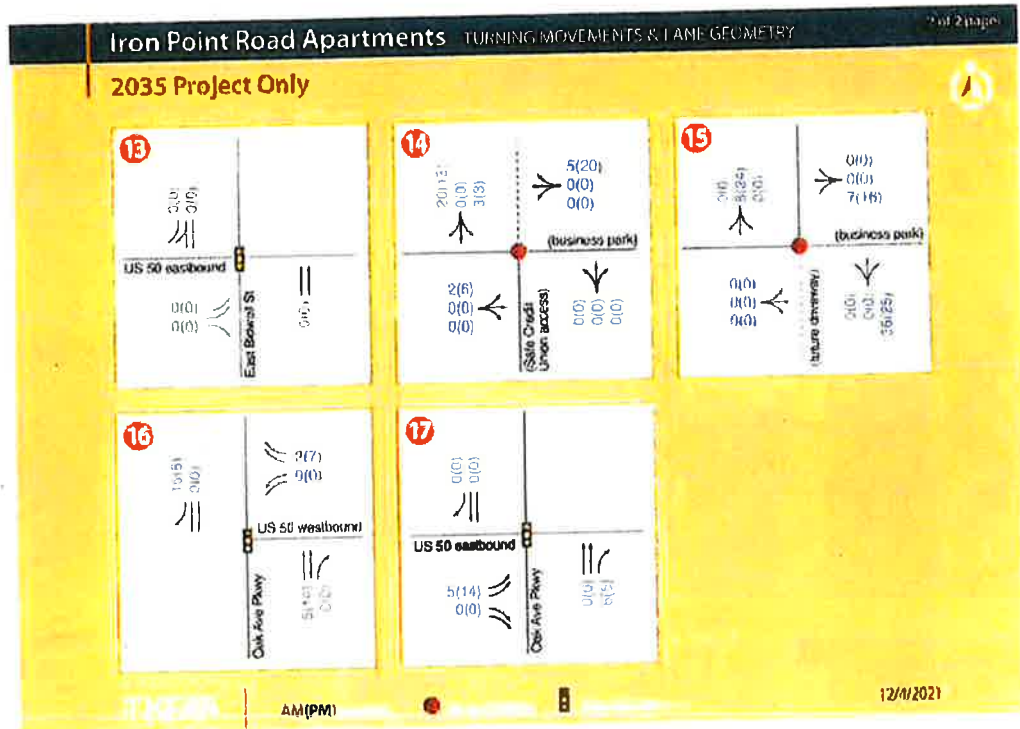


Figure 15. Project Trip Assignment for Cumulative 2035 Conditions (continued)

4.3 Existing 2021 with Project Conditions

Peak-hour traffic associated with the Project was added to the Existing 2021 turning volumes at each intersection. Delay and level-of-service were determined at the study intersections and segments. **Figure 16** summarizes the turning movements and lane configurations for the Existing with Project Condition. **Table 12** and **Table 13** presents a summary of the level-of-service results for the study intersections and segments.

The results indicate that all study segments are anticipated to operate at an acceptable level-of-service; three study intersections exceed the General Plan level-of-service standard prior to the addition of Project traffic.

- Prairie City Rd/American Aggregate Dr would operate at a deficient level-of-service during the AM peak if not for the Covid-19 related traffic reductions.
- Prairie City Rd/Iron Point Rd would operate at a deficient level-of-service during the AM and PM peak if not for the Covid-19 related traffic reductions.
- East Bidwell St/Iron Point Rd would operate at a deficient level-of-service during the PM peak if not for the Covid-19 related traffic reductions.

These locations are shown in orange highlight in the tables below. Because the increase in delay is less than five seconds, these violations of the General Plan level-of-service policy is not considered a Project impact. Calculation sheets for intersection delay and level-of-service are provided in **Appendix B**.

In addition to level-of-service, the 95th percentile left turn queues with and without the project were reviewed to identify any study intersections with Project queueing impacts. One location, the westbound left turn movement at Intersection #4 Prairie City Rd/Iron Point Rd during the AM peak has a queueing deficiency that Project traffic is anticipated to add more than one vehicle length to. This is considered a Project Related deficiency. An Abatement Measure⁸ to address this deficiency is provided in Section 8.

⁸ To avoid confusion, General Plan deficiencies are labeled as “deficiencies” rather than (CEQA) “impacts”, and the related improvements are labeled as “abatement measures” rather than “mitigation measures”. This is done to emphasize that level-of-service and/or queueing concerns are not considered to be impacts under CEQA.



Figure 16. Existing 2021 with Project Condition Turning Movements and Lane Geometry

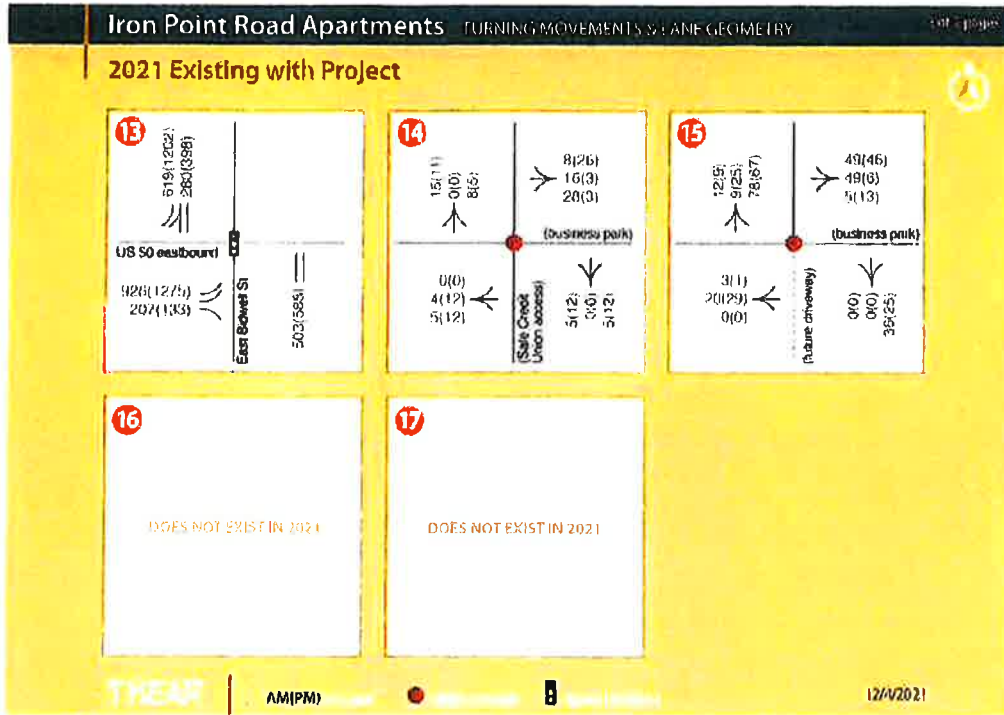


Figure 16. Existing 2021 with Project Condition Turning Movements and Lane Geometry (continued)

Folsom Corporate Center Apartments
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Table 12. Existing 2021 Intersection Delay and Level-of-Service (LOS), with and without Project

Intersection	Control	Without Project		With Project	
		AM (Delay LOS*)	PM (Delay LOS*)	AM (Delay LOS*)	PM (Delay LOS*)
1. Prairie City Rd/US 50 eastbound ramps	Signal	10.3 B	8.3 A	10.4 B	8.4 A
2. Prairie City Rd/US 50 westbound ramps	Signal	19.4 B	8.9 A	19.5 B	8.9 A
3. Prairie City Rd/American Aggregates Rd	Signal	66.1 E	28.8 C	66.3 E	28.9 C
4. Prairie City Rd/Iron Point Rd	Signal	88.7 F	64.5 E	90.6 F	66.1 E
5. Iron Pt Road/Grover Rd	Signal	50.9 D	42.3 D	51.4 D	42.5 D
6. Iron Point Rd /Oak Avenue Pkwy	Signal	36.2 D	37.8 D	36.4 D	38.4 D
7. Iron Point Rd /West Kaiser access road	TWSC**	11.9 B Northbound	12.9 B Northbound	11.9 B Northbound	13 B Northbound
8. Iron Point Rd /Rowberry Way	Signal	14.3 B	14.2 B	14.8 B	14.5 B
9. Iron Point Rd /Safe Credit Union access	TWSC**	15.6 C WB left/U	23.1 C WB left/U	16 C WB left/U	23.6 C WB left/U
10. Iron Point Rd /Broadstone Pkwy	Signal	15.6 B	19.6 B	15.7 B	19.7 B
11. Iron Point Rd /East Bldwell St	Signal	45.5 D	94.3 F	46 D	95.3 F
12. East Bldwell St/US 50 westbound ramps	Signal	29.5 C	35.1 D	29.6 C	35.7 D
13. East Bldwell St/US 50 eastbound ramps	Signal	10.2 B	21.5 C	10.2 B	21.7 C
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound	9.2 A Northbound	8.9 A Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.6 A Southbound	9.3 A Southbound	10.3 B Southbound	10.2 B Southbound

* Level of Service

** Two Way Stop Control: LOS is defined by delay for the worst movement/shared movement, which is listed with the LOS results.

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California**

Table 13. Existing 2021 US 50 Segment Density and Level-of-Service (LOS), with and without Project

US 50 Segment	Segment Type	Without Project		With Project	
		AM (Density LOS*)	PM (Density LOS*)	AM (Density LOS*)	PM (Density LOS*)
1. US 50 westbound East Bidwell offramp	Diverge	24.5 C	17.3 B	24.5 C	17.4 B
2. US 50 westbound East Bidwell loop onramp	Merge	22.9 C	17.1 B	22.9 C	17.1 B
3. US 50 westbound East Bidwell slip onramp	Merge	24.3 C	19.0 B	24.3 C	19.0 B
4. US 50 westbound East Bidwell to Oak Ave	Basic	24.8 C	18.8 C	24.8 C	18.8 C
5. US 50 westbound Oak Avenue offramp	Diverge	Not applicable to this scenario		Not applicable to this scenario	
6. US 50 westbound Oak Avenue loop onramp	Merge				
7. US 50 westbound Oak Avenue diagonal onramp to Prairie City fly-over	Merge				
8. US 50 westbound Prairie City offramp	Diverge	32.0 D	26.1 C	32.0 D	26.1 C
9. US 50 westbound Prairie City loop onramp	Merge	24.1 C	21.6 C	24.1 C	21.6 C
10. US 50 westbound Prairie City diagonal onramp	Merge	24.5 C	21.5 C	24.6 C	22.1 C
11. US 50 eastbound Prairie City offramp	Diverge	28.6 D	31.0 D	28.6 D	31.1 D
12. US 50 eastbound Prairie City diagonal onramp	Merge	18.6 B	23.2 C	18.6 B	23.2 C
13. US 50 eastbound Prairie City fly-over onramp	Merge	19.6 B	25.4 C	19.6 B	25.4 C
14. US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Merge	Not applicable to this scenario		Not applicable to this scenario	
15. US 50 eastbound Oak Avenue loop onramp	Merge				
16. US 50 eastbound Oak Avenue diagonal onramp	Merge				
17. US 50 eastbound Oak Ave to East Bidwell	Basic	17.5 B	23.5 C	17.5 B	23.5 C
18. US 50 eastbound East Bidwell offramp	Diverge	10.4 B	16.5 B	10.4 B	16.5 B
19. US 50 eastbound East Bidwell loop onramp	Merge	9.3 A	13.9 B	9.3 A	13.9 B
20. US 50 eastbound East Bidwell slip onramp	Merge	7.5 A	13.1 B	7.6 A	13.1 B

* Level of Service

5. EXISTING PLUS APPROVED PROJECTS (EPAP) 2026 CONDITION WITH AND WITHOUT PROJECT

This section presents Existing Condition traffic plus traffic from planned and approved projects that are reasonably expected to be constructed by the time the project is constructed, corresponding to five years' worth of growth.

5.1 EPAP 2026 Growth Increment

Five-year traffic forecasts were developed using two different methodologies, and the higher (more conservative) volume projections were used for this analysis.

- The first method was based on the traffic anticipated from approved projects that have not been fully built as of August 2021.
- The second method used the City of Folsom General Plan travel demand model to estimate growth through 2026. Base year (2015) and Cumulative year (2035) trip tables were both assigned to the base year model network. The resulting 2015 and 2035 volumes interpolated to 2021 and compared with counts to calibrate the model to conditions in the immediate project vicinity. Results were then interpolated to 2026 and the NCHRP 255 adjustment methodology applied⁹. Supporting material for Traffic forecasting calculations are provided in **Appendix C**.

The second method resulted in higher traffic volumes and was therefore used as the bases for EPAP 2026 condition analysis.

5.2 EPAP 2026 Conditions

EPAP Conditions analysis utilizes lane configurations and signal timing plans from the Existing Conditions. **Figure 17** summarizes the turning movements and lane configurations for the EPAP 2026 Conditions scenario. **Table 14** and **Table 15** present a summary of level-of-service results for the study intersections under EPAP 2026 Conditions.

The results indicate that all study segments are anticipated to operate at an acceptable level-of-service; three study intersections exceed the General Plan level-of-service standard prior to the addition of Project traffic.

- Prairie City Rd/American Aggregate Dr would operate at a deficient level-of-service during the AM peak if not for the Covid-19 related traffic reductions.
- Prairie City Rd/Iron Point Rd would operate at a deficient level-of-service during the AM and PM peak if not for the Covid-19 related traffic reductions.
- East Bidwell St/Iron Point Rd would operate at a deficient level-of-service during the AM and PM peak if not for the Covid-19 related traffic reductions.

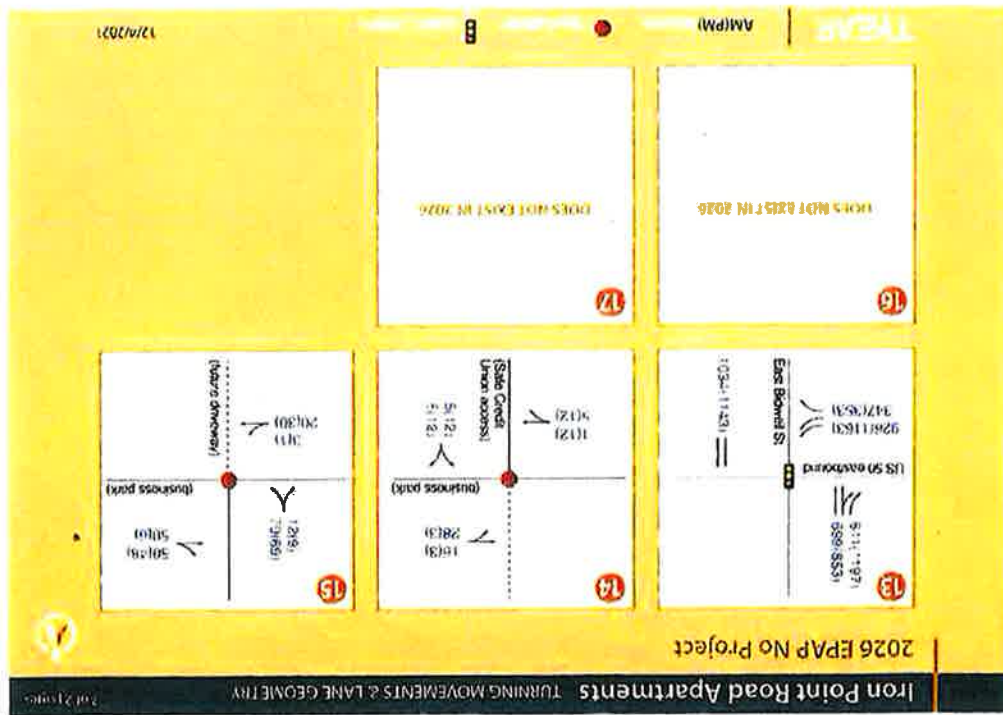
⁹ The NCHRP 255 adjustment uses anticipated traffic growth on each intersections approach and departure legs and observed traffic counts to estimate future year turning movements.

These locations are shown in orange highlight in the tables below. Calculation sheets for intersection delay and level-of-service are provided in Appendix B.



Figure 17. EPAP 2026 Condition Turn Movements and Geometry

Figure 17. EPAP 2026 Condition Turn Movements and Geometry (continued)



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Table 14. EPAP 2026 Intersection Delay and Level-of-Service

Intersection	Control	Without Project	
		AM (Delay LOS*)	PM (Delay LOS*)
1. Prairie City Rd/US 50 eastbound ramps	Signal	15.2 B	10.5 B
2. Prairie City Rd/US 50 westbound ramps	Signal	60.5 E	10.2 B
3. Prairie City Rd/American Aggregates Rd	Signal	110.5 F	30.8 C
4. Prairie City Rd/Iron Point Rd	Signal	123.4 F	72.4 E
5. Iron Point Rd /Grover Rd	Signal	52 D	43.4 D
6. Iron Point Rd /Oak Avenue Pkwy	Signal	36.8 D	40.4 D
7. Iron Point Rd /West Kaiser access road	TWSC**	12.4 B Northbound	13.7 B Northbound
8. Iron Point Rd /Rowberry Way	Signal	14.4 B	14.3 B
9. Iron Point Rd /Safe Credit Union access	TWSC**	16.9 C WB left/U	27 D WB left/U
10. Iron Point Rd /Broadstone Pkwy	Signal	16.3 B	20.5 C
11. Iron Point Rd /East Bidwell St	Signal	67.1 E	143.4 F
12. East Bidwell St/US 50 westbound ramps	Signal	46.9 D	53.5 D
13. East Bidwell St/US 50 eastbound ramps	Signal	12.9 B	25.4 C
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.6 A Southbound	9.8 A Southbound

* Level of Service

** Two Way Stop Control: LOS is defined by delay for the worst movement/shared movement, which is listed with the LOS results.

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California**

Table 15. EPAP 2026 US 50 Segment Density and Level-of-Service (LOS)

US 50 Segment	Segment Type	Without Project	
		AM (Density LOS*)	PM (Density LOS*)
1. US 50 westbound East Bidwell offramp	Diverge	25.9 C	17.8 B
2. US 50 westbound East Bidwell loop onramp	Merge	24.4 C	18.1 B
3. US 50 westbound East Bidwell slip onramp	Merge	25.9 C	21.2 C
4. US 50 westbound East Bidwell to Oak Ave	Basic	26.9 D	21.2 C
5. US 50 westbound Oak Avenue offramp	Diverge	Not applicable to this scenario	
6. US 50 westbound Oak Avenue loop onramp	Merge		
7. US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	weave		
8. US 50 westbound Prairie City offramp	Diverge	33.7 D	28.7 D
9. US 50 westbound Prairie City loop onramp	Merge	25.5 C	23.4 C
10. US 50 westbound Prairie City diagonal onramp	Merge	26.0 C	23.2 C
11. US 50 eastbound Prairie City offramp	Diverge	30.5 D	33.3 D
12. US 50 eastbound Prairie City diagonal onramp	Merge	19.6 B	24.1 C
13. US 50 eastbound Prairie City fly-over onramp	Merge	21.1 C	26.3 C
14. US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	Not applicable to this scenario	
15. US 50 eastbound Oak Avenue loop onramp	Merge		
16. US 50 eastbound Oak Avenue diagonal onramp	Merge		
17. US 50 eastbound Oak Ave to East Bidwell	Basic	18.8 C	24.7 C
18. US 50 eastbound East Bidwell offramp	Diverge	11.8 B	17.6 B
19. US 50 eastbound East Bidwell loop onramp	Merge	9.3 A	13.9 B
20. US 50 eastbound East Bidwell slip onramp	Merge	8.5 A	14.2 B

* Level of Service

5.3 EPAP 2026 with Project Condition

Peak-hour traffic associated with the Project was added to anticipated EPAP 2026 turning volumes at each intersection. Delay and level-of-service were then determined at the study intersections. **Figure 18** summarizes the turning movements and lane configurations for the EPAP 2026 with Project condition. **Table 16** and **Table 17** present a summary of the level-of-service results for the study intersections.

The results indicate that all study segments are anticipated to operate at an acceptable level-of-service; three study intersections exceed the General Plan level-of-service standard prior to the addition of Project traffic.

- Prairie City Rd/American Aggregate Dr would operate at a deficient level-of-service during the AM peak if not for the Covid-19 related traffic reductions.
- Prairie City Rd/Iron Point Rd would operate at a deficient level-of-service during the AM and PM peak if not for the Covid-19 related traffic reductions.
- East Bidwell St/Iron Point Rd would operate at a deficient level-of-service during the AM and PM peak if not for the Covid-19 related traffic reductions.

These locations are shown in orange highlight in the tables below. Because the increase in delay is less than five seconds, these violations of the General Plan level-of-service policy is not considered a Project impact. Calculation sheets for intersection delay and level-of-service are provided in **Appendix B**.

In Addition to level-of-service, the 95th percentile left turn queues with and without the project were reviewed to identify any study intersections with Project queueing impacts. One location, the westbound left turn movement at Intersection #4 Prairie City Rd/Iron Point Rd during the AM peak has a queueing deficiency that Project traffic is anticipated to add more than one vehicle length. This is considered a Project related deficiency. This deficiency is identical to the Project related deficiency previously identified under Existing 2021 with Project conditions. An Abatement measure to address this deficiency is provided in **Section 8**¹⁰.

¹⁰ To avoid confusion, General Plan deficiencies are labeled as "deficiencies" rather than (CEQA) "impacts", and the related Improvements are labeled as "abatement measures" rather than "mitigation measures". This is done to emphasize that level-of-service and/or queueing concerns are not considered to be impacts under CEQA.

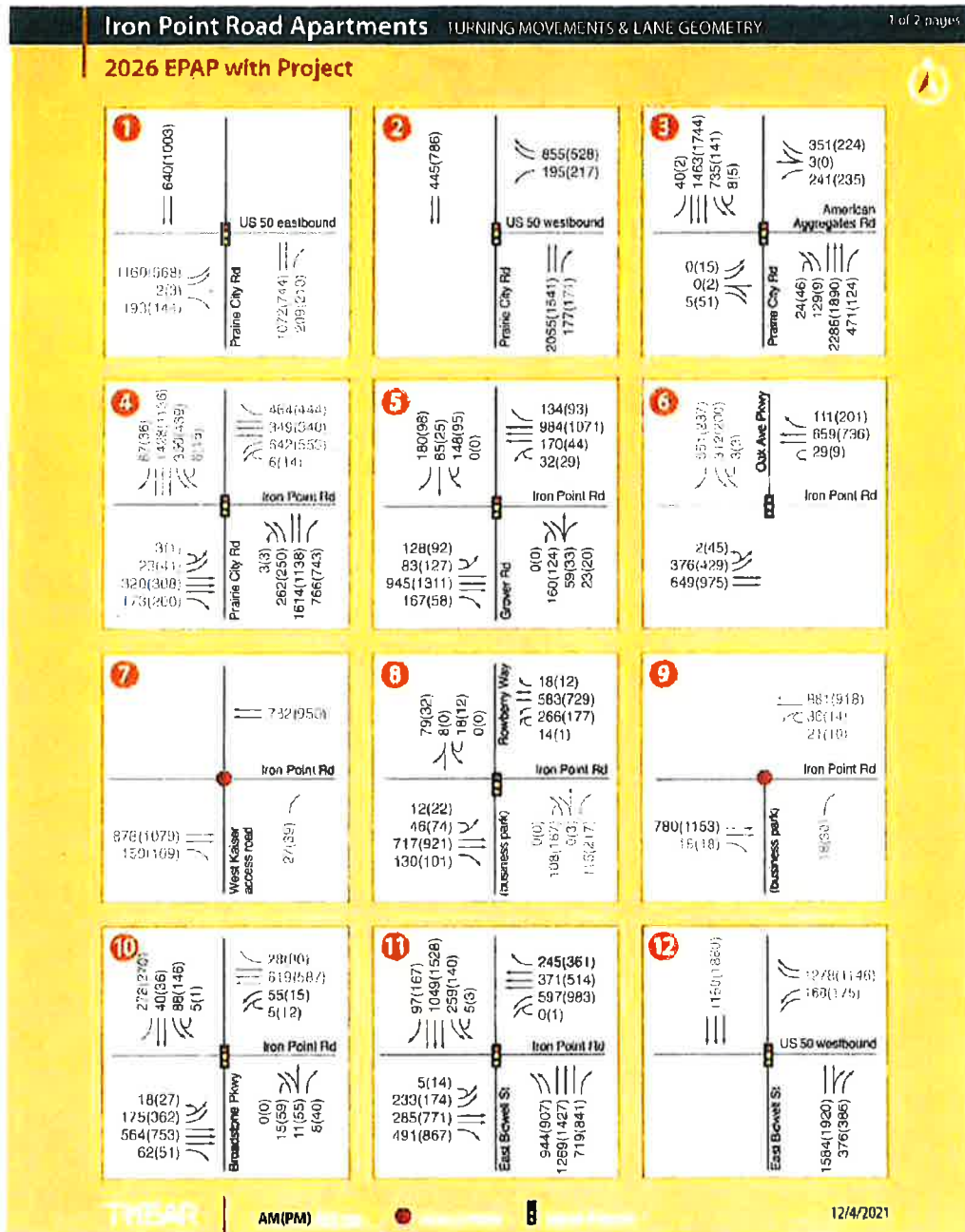


Figure 18. EPAP 2026 with Project Turning Movements and Lane Geometry

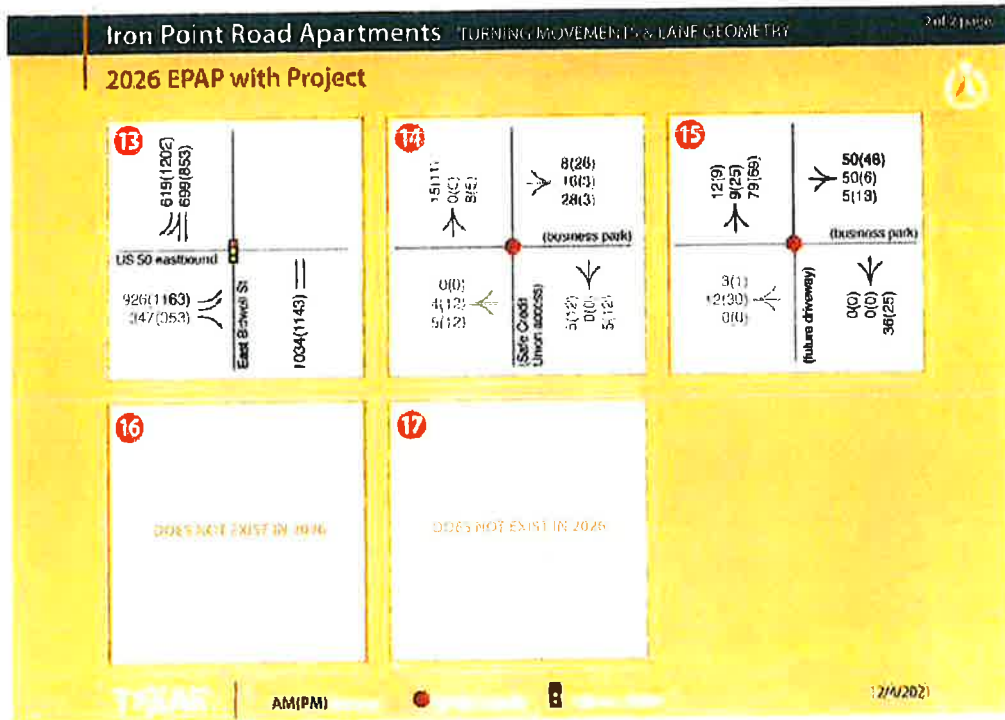


Figure 18. EPAP 2026 with Project Turning Movements and Lane Geometry (Continued)

Table 16. EPAP 2026 Intersection Delay and Level-of-Service, with and without Project

Intersection	Control	Without Project		With Project	
		AM (Delay LOS*)	PM (Delay LOS*)	AM (Delay LOS*)	PM (Delay LOS*)
1. Prairie City Rd/US 50 eastbound ramps	Signal	15.2 B	10.5 B	15.3 B	10.6 B
2. Prairie City Rd/US 50 westbound ramps	Signal	60.5 E	10.2 B	60.8 E	10.3 B
3. Prairie City Rd/American Aggregates Rd	Signal	110.5 F	30.8 C	110.6 F	30.8 C
4. Prairie City Rd/Iron Point Rd	Signal	123.4 F	72.4 E	125.2 F	74.1 E
5. Iron Point Rd /Grover Rd	Signal	52 D	43.4 D	52.5 D	43.7 D
6. Iron Point Rd /Oak Avenue Pkwy	Signal	36.8 D	40.4 D	37.1 D	41.4 D
7. Iron Point Rd /West Kaiser access road	TWSC**	12.4 B Northbound	13.7 B Northbound	12.4 B Northbound	13.8 B Northbound
8. Iron Point Rd /Rowberry Way	Signal	14.4 B	14.3 B	15 B	14.6 B
9. Iron Point Rd /Safe Credit Union access	TWSC**	16.9 C WB left/U	27 D WB left/U	17.3 C WB left/U	27.7 D WB left/U
10. Iron Point Rd /Broadstone Pkwy	Signal	16.3 B	20.5 C	16.4 B	20.6 C
11. Iron Point Rd /East Bidwell St	Signal	67.1 E	143.4 F	68 E	144.5 F
12. East Bidwell St/US 50 westbound ramps	Signal	46.9 D	53.5 D	47 D	53.8 D
13. East Bidwell St/US 50 eastbound ramps	Signal	12.9 B	25.4 C	12.9 B	25.5 C
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound	9.2 A Northbound	8.9 A Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.6 A Southbound	9.8 A Southbound	10.3 B Southbound	10.2 B Southbound

* Level of Service

** Two Way Stop Control: LOS is defined by delay for the worst movement/shared movement, which is listed with the LOS results.

Table 17. EPAP 2026 US 50 Segment Density and Level-of-Service (LOS), with and without Project

US 50 Segment	Segment Type	Without Project		With Project	
		AM (Density LOS*)	PM (Density LOS*)	AM (Density LOS*)	PM (Density LOS*)
1. US 50 westbound East Bidwell offramp	Diverge	25.9 C	17.8 B	26.0 C	17.9 B
2. US 50 westbound East Bidwell loop onramp	Merge	24.4 C	18.1 B	24.4 C	18.1 B
3. US 50 westbound East Bidwell slip onramp	Merge	25.9 C	21.2 C	25.9 C	21.2 C
4. US 50 westbound East Bidwell to Oak Ave	Basic	26.9 D	21.2 C	26.9 D	21.2 C
5. US 50 westbound Oak Avenue offramp	Diverge	Not applicable to this segment			
6. US 50 westbound Oak Avenue loop onramp	Merge				
7. US 50 westbound Oak Avenue diagonal onramp to Prairie City rd offramp	Diverge				
8. US 50 westbound Prairie City offramp	Diverge	33.7 D	28.7 D	33.7 D	28.7 D
9. US 50 westbound Prairie City loop onramp	Merge	25.5 C	23.4 C	25.5 C	23.4 C
10. US 50 westbound Prairie City diagonal onramp	Merge	26.0 C	23.2 C	26.1 C	23.3 C
11. US 50 eastbound Prairie City offramp	Diverge	30.5 D	33.3 D	30.5 D	33.3 D
12. US 50 eastbound Prairie City diagonal onramp	Merge	19.6 B	24.1 C	19.6 B	24.1 C
13. US 50 eastbound Prairie City fly-over onramp	Merge	21.1 C	26.3 C	21.1 C	26.3 C
14. US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Merge	Not applicable to this segment			
15. US 50 eastbound Oak Ave fly-over onramp	Merge				
16. US 50 eastbound Oak Ave to East Bidwell	Merge				
17. US 50 eastbound Oak Ave to East Bidwell	Basic	18.8 C	24.7 C	18.8 C	24.7 C
18. US 50 eastbound East Bidwell offramp	Diverge	11.8 B	17.6 B	11.8 B	17.6 B
19. US 50 eastbound East Bidwell loop onramp	Merge	9.3 A	13.9 B	9.4 A	14.0 B
20. US 50 eastbound East Bidwell slip onramp	Merge	8.5 A	14.2 B	8.5 A	14.3 B

* Level of Service

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6. CUMULATIVE 2026 CONDITION WITH AND WITHOUT PROJECT

This section presents Cumulative Condition traffic.

6.1 Cumulative 2035 Growth Increment

The City of Folsom General Plan travel demand model was used to estimate growth through 2035. The travel demand model was calibrated to the immediate project vicinity by using Base year (2015) and Cumulative year (2035) trip tables, both assigned to the base year model network. The resulting 2015 and 2035 volumes were interpolated to 2021 and compared with the counts to calibrate the model to conditions in the immediate project vicinity. The calibrated model was then applied using the cumulative 2035 trip tables and network to estimate Cumulative condition volumes. The NCHRP 255 adjustment methodology applied¹¹ was used to refine forecast turning movements. Supporting material for traffic forecasting calculations are provided in Appendix C.

6.2 Cumulative 2035 Conditions

The Cumulative Conditions analysis accounts for several planned changes to Folsom's transportation system:

- Addition of a third northbound through lane at intersection #4 (Prairie City Rd/Iron Point Rd);
- Widening of Iron Point Rd to six lanes on all segments between Prairie City Rd and East Bidwell St (effecting intersections 6-9);
- Construction of the Rowberry Way overcrossing of US 50;
- Construction of the Empire Ranch Rd interchange;
- Construction of the Oak Avenue Pkwy interchange; and
- The extension of Alder Creek Pkwy through Oak Avenue Pkwy (along with other Folsom Ranch infrastructure).

Figure 19 summarizes the turning movements and lane configurations for the Cumulative 2035 Conditions scenario. Table 18 and Table 19 present a summary of level-of-service results for the study intersections under EPAP 2026 Conditions. All study intersections and segments are anticipated to operate at an acceptable level-of-service. Calculation sheets for intersection delay and level-of-service are provided in Appendix B.

¹¹ The NCHRP 255 approach is an iterative algorithm that uses anticipated traffic growth on each intersections' approach and departure legs, and observed traffic counts, to estimate future year turning movements.

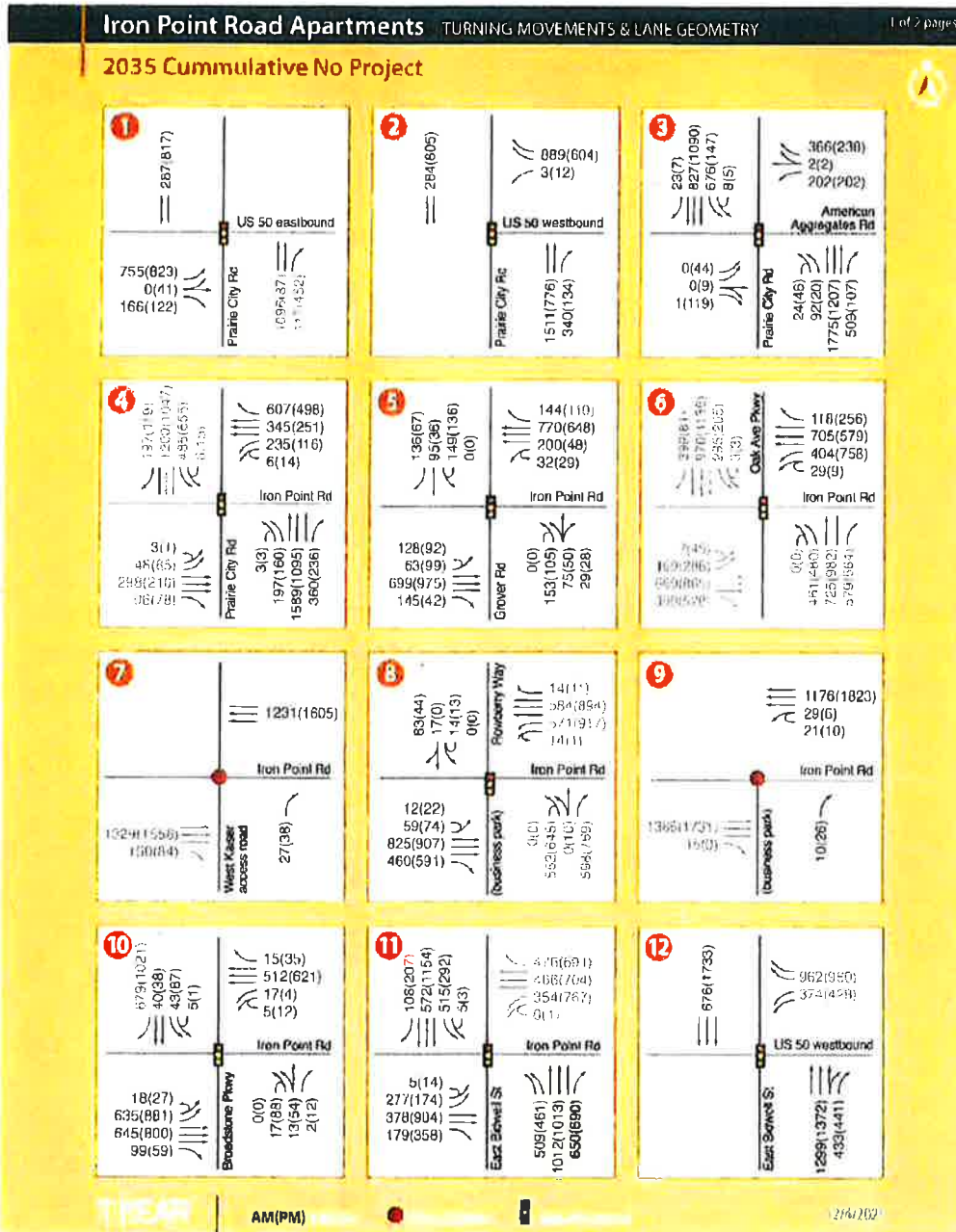


Figure 19. Cumulative 2035 Condition Turn Movements and Geometry

Attachment No. 22

**Initial Study, Mitigated Negative Declaration, and Mitigation
Monitoring and Reporting Program, dated March, 2022 (electronic
version available for viewing at
[www.folsom.ca.us/government/community-
development/planning-services/current-project-information](http://www.folsom.ca.us/government/community-development/planning-services/current-project-information))**

Folsom Corporate Center Apartments

Initial Study/Mitigated Negative Declaration

Prepared by:
City of Folsom
Community Development Department
50 Natoma Street
Folsom, CA 95630

With technical support from:
HELIX Environmental Planning, Inc.
11 Natoma Street, Suite 155
Folsom, CA 95630

March 2022

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1.0 INTRODUCTION

This Initial Study addresses the proposed Folsom Corporate Center Apartments (proposed project) and whether it may cause significant effects on the environment. These potential environmental effects are further evaluated to determine whether they were examined in the 2035 City of Folsom General Plan Environmental Impact Report (EIR; City of Folsom 2018) as amended by Code (PRC) §21083.3. This Initial Study focuses on any effects on the environment which are specific to the proposed project and were not analyzed as potentially significant effects in the 2035 City of Folsom General Plan EIR as amended by the EIR for the East Area Facilities Plan, or for which substantial new information shows that identified effects would be more significant than described in the previous EIRs. For additional information regarding the relationship between the proposed project and the previous EIRs, see Section 6.0 of this Initial Study.

The Initial Study is also intended to assess whether any environmental effects of the project are susceptible to substantial reduction or avoidance by the choice of specific revisions in the project, by the imposition of conditions, or by other means [Section 15152(b)(2)] of the California Environmental Quality Act (CEQA) Guidelines. If such revisions, conditions, or other means are identified, they will be identified as mitigation measures.

This Initial Study relies on CEQA Guidelines Sections 15064 and 15064.4 in its determination of the significance of environmental effects. According to Section 15064, the finding as to whether a project may have one or more significant effects shall be based on substantial evidence in the record, and that controversy alone, without substantial evidence of a significant effect, does not trigger the need for an EIR.

2.0 PROJECT BACKGROUND

The following project specific technical reports quantified analysis and or surveys were used in preparation of this Initial Study and are incorporated by reference:

- Air Quality, Greenhouse Gases Analysis, prepared by HELIX (2022)
- Health Risk Assessment, prepared by HELIX (2021)
- Biological Resources Memo, prepared by SCS Engineers (2021)
- Biological Resources Inventory, prepared by HELIX (2021)
- Arborist Report, prepared by Arborwell (2021)
- Noise Analysis, prepared by Bollard Acoustical, May 3, 2021 – revised by HELIX (2021)
- Transportation Impact Study, prepared by T. Kear Transportation Planning and Management, Inc. (2021).
- Tribal Cultural Resource technical memo, prepared by ECORP (2021)
- Cultural Resources Assessment, prepared by HELIX (2021)
- Preliminary Water Quality Report, prepared by RSC Engineering (2021)
- Geotechnical Investigation, Folsom Senior Living Facility, Geocon Consultants (2017)
- Sewer Capacity Analysis, prepared by Water Works Engineers (2021)

3.0 PROJECT DESCRIPTION

3.1 Project Location

The project site consists of two parcels situated in south/central City of Folsom in northeastern Sacramento County, California (Figures 1-2 in Appendix A). The first parcel, referred to as Lot 1 (APN: 072-3120-026), is an estimated 7.24-acre parcel located south of Rowberry Drive at a point south of Iron Point Road. The second parcel, referred to as Lot 6 (APN 072-3120-023), is a 4.68-acre parcel located south of Iron Point Road between Broadstone Parkway and Rowberry Drive, approximately 1,400-feet northeast of Lot 1. The street address is currently unnumbered. The project site is located within Section 7, 8, 17 & 18, Township 9 North, Range 8 East (Mount Diablo Base and Meridian, United States Geological Survey 7.5 minute "Folsom Quadrangle").

3.2 Project Setting and Surrounding Land Uses

The project site is located within the Folsom Corporate Center, a commercial business center containing a combination of commercial office buildings and open space areas. The area in which the project is located is characterized by suburban residential development, commercial business centers, transportation, and open space and undeveloped lots. Neighboring land uses are summarized in Table 1.

Table 1. Neighboring Land Uses

DIRECTION	LAND USE
North	Lot 1: Office buildings, oak woodland, and medical offices Lot 6: Iron Point Road, residential development north of Iron Point Road
East	Lot 1: vacant land Lot 6: constructed ponds/wetland, office buildings
South	Lot 1: US Highway 50, vacant land Lot 6: office buildings, US Highway 50, undeveloped land containing scattered oaks
West	Lot 1: commercial buildings, memory care facility, and undeveloped land Lot 6: office buildings, stand of oaks

Lot 1 is largely undeveloped, and is bordered by office buildings, oak woodland, and medical offices to the north, vacant land to the east, US Highway 50 and vacant land to the south, and commercial buildings, a memory care facility, an active-adult apartment community, and undeveloped land to the west. The parcel slopes from east to west with elevations ranging from 371 feet above mean sea level (amsl) in the eastern portion of the parcel to 317 feet amsl in the western portion of the parcel. The parcel is raised above the adjacent properties to the north and south. Several electrical transmission and telecommunications easements cross through the western portion of the parcel within an approximately 377.5-foot-wide restricted building and use area. Overhead transmission lines and utility poles occur on the parcel within the easements. A small area of the northwestern portion of the parcel is developed with parking, landscaping, and a walkway associated with the existing adjacent medical offices, located north and northeast of the parcel. A 50-foot landscape easement lines the southern parcel boundary. An existing US Highway 50 right-of-way fence is located along the southern parcel boundary. Additionally, one existing oak tree is located in the southeastern corner of the parcel.

Lot 6 is largely undeveloped and is bordered by Iron Point Road and residential development to the north, a constructed pond/wetland and office buildings to the east, office buildings and undeveloped land containing scattered oaks to the south, and office buildings to the west. An unnamed road borders the parcel along its eastern and southern boundaries. The parcel slopes from west to east, with elevation ranging from 370-feet amsl in the western portion of the site to 358-feet amsl in the eastern portion of the site. The parcel is elevated above the surrounding properties. An existing sidewalk with a curb and gutter, and an existing retaining wall, are located in a 20-foot-wide public utility, landscape, and pedestrian easement that lines the northern parcel boundary along Iron Point Road. The parcel frontage with the unnamed roadway is landscaped within an existing 20-foot-wide access easement. Additional areas of the parcel are undeveloped and sparsely vegetated. A group of oak trees are located in the southwestern portion of the parcel. Seven oak trees are proposed to be removed, and two oak trees would remain and become incorporated into the landscape design.

3.3 Project Characteristics

The proposed project includes the construction of a new multi-family apartment community on two separate parcels (referred to as Lot 1 and Lot 6) within the Folsom Corporate Center. The apartment community in total would consist of 253 apartment units, two clubhouses, 491 parking spaces, and indoor and outdoor amenities unique to each parcel. On-site parking would include garage parking spaces, carport covered parking spaces, and uncovered parking spaces. The units would be available as one-, two-, or three-bedroom apartments, and would range from 690-square feet (sf) to 1,325-sf. The proposed project would require a General Plan Amendment, Rezone, Planned Development Permit, Design Review, and Tree Removal Permit.

Lot 1 is a 7.24-acre parcel and would develop seven, 3-story apartment buildings with a total of 153 units (**Figure 3 in Appendix A**). The site would have 304 parking spaces provided as carports and uncovered spaces throughout the parcel. The parcel would include an approximately 6,700-sf, 3-story clubhouse with a pool located in the southeastern portion of the parcel. Additional amenities would include a dog park in the southwest portion of the parcel, fire pit with seating and a picnic area located near the center of the parcel, and a landscaped seating area near the main entrance at the northeastern portion of the parcel. Bicycle parking would be in an enclosed structure adjacent to the clubhouse. The existing oak tree in the southeast corner of the parcel would remain.

Lot 6 is a 4.86-acre parcel and would develop five, 3-story apartment buildings with a total of 100 units (**Figure 4 in Appendix A**). The site would have 187 parking spaces provided as carports and uncovered spaces throughout the parcel. The parcel would include an approximately 3,200-sf, one story clubhouse with a pool and amenity area located in southwestern portion of the parcel, east of the main entrance driveway. Additional amenities would include proposed seating areas, picnic areas, a fire pit, and a dog park in the southwestern portion of the parcel. Bicycle parking would be located in a dedicated room in the clubhouse. A group of oak trees are located in the southwestern corner of the parcel. Seven of the trees on the parcel are proposed to be removed, while the remaining two would remain and be incorporated into the landscape design.

Additional proposed improvements include drive aisles, curbs, gutters, sidewalks, internal walkways, underground utilities, retaining walls, site lighting, site landscaping, and monument signs. Building materials would consist of stucco, fiber cement siding and stone veneer. The height of each building would be approximately 38 feet with a parapet roof system to blend with the commercial buildings and

to screen the mechanical equipment (HVAC) on the roof. The project features are summarized in **Table 2** and are described in detail in the following paragraphs.

Table 2. Summary of Project Features

PROJECT FEATURE	UNITS/ PARKING SPACES	SITE COVERAGE (square feet)
Lot 1		
Total residential building units	153 units	
Clubhouse		6,782
Total parking spaces/paved areas	304 spaces	98,849
Landscaping/Shaded Area		34,945
Subtotal Lot 1		
Lot 6		
Total residential building units	100 units	
Clubhouse		3,098
Total parking spaces/paved areas	187 spaces	67,868
Landscaping/Shaded Area		34,186
Subtotal Lot 6		
Total project	253 units/491 parking spaces	

Source: BSB Design, Folsom Corporate Center Apartments Site Plan (2021).

Parking and Circulation

Parking proposed on both Lot 1 and Lot 6 currently meet the Zoning Ordinance requirement of 1.5 stalls per unit. Under the current multi-family guidelines, Lot 1 exceeds the Folsom Design guidelines by providing 304 parking spaces (1.99 ratio); inclusive of 74 garaged spaces and 79 covered stalls. Lot 6 also meets the guidelines with 187 spaces (1.87 ratio); inclusive of 46 garaged spaces and 54 covered stalls. The overall parking ration of Lots 1 and 6 together exceed the City's current multi-family guidelines with a parking ratio of 1.94.

Car Parking and Circulation

Lot 1

Lot 1 would have one gated main access driveway with two gated emergency vehicle access driveways. The main gated entrance would be located on the northern parcel boundary and would connect to Rowberry Drive. Additionally, a gated emergency vehicle access driveway entrance would be located approximately 640-feet west of the main entrance and would connect to the existing parking associated with the medical office north of the parcel. A secondary gated, emergency vehicle access driveway would connect to Rowberry Drive at a point 640-feet east of the main driveway. On-site circulation would consist of a circular driveway that would connect directly with the main public entrance driveway on the northern parcel boundary. Lot 1 includes sidewalk pedestrian connections to the Kaiser outer parking lot to the north of the parcel, and to the planned dialysis clinic to the east of the parcel. The two emergency vehicle access driveways would connect with the main on-site circulation driveway that would provide access to the proposed buildings and clubhouse located in the southeastern portion of the parcel.

A 6-foot height metal pedestrian gate would be located next to each entrance, the main access driveway, and the two emergency access driveways. Pedestrian circulation would consist of sidewalks throughout the parcel, and crosswalks providing pedestrian access to the apartment units, clubhouse and pool, and the main and emergency entrances.

Lot 6

Lot 6 would be accessed by one main access driveway and one emergency vehicle access driveway. The gated main entrance would be located on the southern parcel boundary and would connect to the unnamed road that borders the parcel to the south and east. A gated emergency vehicle access driveway would be located 170-foot east of the main access driveway and would connect to the unnamed road that borders the parcel to the south and east. On-site circulation would consist of a circular driveway that provides access to the proposed buildings and clubhouse, the amenities, the emergency access driveway, and the main entrance/ exit driveway.

One 6-foot metal pedestrian gate would be located next to each entrance, the main entrance driveway, and the emergency access driveway. Pedestrian circulation would consist of sidewalks throughout the parcel, and crosswalks providing pedestrian access to the apartment units, clubhouse and pool, and the main and emergency entrances.

Bicycle Parking

The proposed project would provide bicycle parking spaces throughout Lot 1 and Lot 6 that would exceed City and Title 24 requirements. Lot 1 bicycle parking would be in an enclosed structure adjacent to the main clubhouse. Lot 6 would include bicycle parking within a dedicated room in the clubhouse. By exceeding the bicycle parking standards, the intent is to help offset the need for motorized vehicles. In addition, the proposed project plans to provide some community-owned bicycles for use by residents between Lot 1 and Lot 6, or for easier access to nearby amenities such as the wetland and oak preserves, Folsom Gateway, or the shops at the Palladio. Of note, Lot 6 is located less than 0.25-mile from Folsom Gateway and 0.6-mile from Palladio, and Lot 1 is located approximately 0.5-mile from Folsom Gateway and 0.9-mile from Palladio.

Trash and Recycling Service Access

For Lot 1, the trash compactor would be serviced by entering through the emergency vehicle access and exiting the main access point. Recycling would enter and exit through the main access driveway. For Lot 6, trash and recycling would use the main access to enter and exit.

Grading and Drainage

Lot 1

Nearly the entire parcel of Lot 1 would be disturbed during site preparation and grading. Lot 1 would be terraced to the extent possible to account for significant existing elevation change from the eastern to western boundaries. Due to the topography of the parcel and surrounding areas, retaining walls would be installed along portions of the southern and eastern parcel boundaries, as well as along the northwestern parcel boundary. An existing oak tree in the southeastern portion of the parcel would remain.

Stormwater generated in Lot 1 would be collected by storm drain inlets throughout the parcel. The parcel would contain multiple drainage management areas that would manage the stormwater with bioretention facilities and/or Contech stormfilter units as necessary for compliance with the City of Folsom standards.

Lot 6

A majority of Lot 6 would be disturbed during site preparation and grading. An existing retaining wall along the northern boundary of the parcel would remain. Due to the topography of the parcel, a retaining wall would be installed along portions of the northern and eastern parcel boundaries, and a rockery wall would be installed along the western parcel boundary. The existing grade in the southwestern corner of the parcel would be maintained, to preserve the existing oak trees beyond the parcel boundary. Seven oak trees located within the parcel boundary would be removed, and two oak trees would remain and would be incorporated into landscaping.

Stormwater generated in Lot 6 would be collected by several storm drain inlets, gutter flowlines and sidewalk underdrains throughout the parcel. The parcel would contain multiple drainage management areas that would manage the stormwater through the use of disconnected roof drains, bioretention facilities and/or Contech stormfilter units as necessary for compliance with the City of Folsom standards.

Utilities

Lot 1

Both lots contain utility stubs for water and sewer, which would tie into existing water and sewer lines that were provided when the previous phase of the Folsom Corporate Center development project were completed. Multiple existing storm drain stubs located on the northern portion of the site will be used to connect the proposed storm drain system. Proposed water line stubs would connect to existing water service stubs located east of the parcel and on the eastern boundary line. Additionally, proposed sewer line stubs would connect to an existing sewer line with a new manhole provided by a parcel located just north. Stormwater planters and Contech Stormfilter Units are proposed on the project site to address the stormwater quality requirement of the City. Additionally, dry utilities (electric, gas, telephone, and cable TV) would be provided. An easement would be provided and centered over their facilities. An existing 12.5-foot public utility easement is located along Iron Point Road. Proposed fire service lines as well as proposed fire hydrants are located throughout the parcel. Each junction of the utility stubs would be covered by an existing or proposed manhole.

Lot 6

Both lots contain utility stubs for water and sewer, which would tie into existing water and sewer lines that were provided when the previous phase of the Folsom Corporate Center development project were completed. An existing storm drain stub would connect to the proposed site storm drain system. Proposed water lines would connect to an existing domestic water service stub located in the northeastern corner of the parcel. Additionally, proposed sewer line stubs would connect to existing sewer lines stubs located in the eastern portion of the parcel. Stormwater planters, Contech Stormfilter Units, and Disconnected Roof Drains are proposed on the project site to address the stormwater quality requirements of the City. Additionally, dry utilities (electric, gas, telephone, and cable TV) would be provided. An easement would be provided and centered over their facilities. An existing 12.5-foot public

utility easement is located along Iron Point Road. Proposed fire service lines would connect to existing fire lane stubs and fire hydrants are proposed throughout the parcel. Each junction of the utility stubs would be covered by an existing or proposed manhole.

Lighting

Lighting on Lot 1 and Lot 6 would be comprised of 12 and 18-foot-tall light poles with a dark bronze finish in the parking lot that have photo-controlled shut-off, with auto-schedule and motion sensors along with down lighting at 8-feet under the car ports. There would also be building wall sconces at 8-feet above finished floor. All lighting would be designed to minimize light/glare impacts to the adjacent properties by ensuring that all exterior lighting and pole-mounted parking lot and driveway lighting be shielded and directed downward. Light-emitting diode luminaires would be used for all of the proposed outdoor lighting.

Landscaping

Lot 1

The project applicant proposed a landscaping plan for Lot 1 that included a variety of new and existing trees, shrubs, and groundcover. Seasonal accented trees and shrubs would be planted the main entrance to Lot 1, and the parking areas would be populated with a canopy of trees and an understory of low shrubs and groundcovers. The proposed project is requesting a deviation from the 50 percent shade requirement on Lot 1 due to the restrictions associated with the power line easements that prohibit full size shade trees. Small trees that meet the standards within the easements have been clustered within these planters to maximize shade patterns. Evergreen shrub clusters would be planted along the eastern and southern parcel boundaries to screen adjacent properties. Purple crape myrtle would line the parking lot in the western portion of Lot 1. Red oak trees would line the southern and eastern parcel boundaries, and several Chinese pistache trees would provide additional cover along walkways between the apartment complexes. An existing oak tree in the southeastern corner of the parcel would remain. Masonry walls would be constructed to provide privacy for the fire pit and picnic area, which are situated between two apartment buildings in the center of the parcel, and for the seating area, which is located adjacent to the main entrance in the northeast portion of the parcel.

Lot 6

The project applicant proposed a landscaping plan for Lot 6 that includes a variety of new and existing trees, shrubs, and groundcover. The main entrance to Lot 6 would be defined by seasonal accented trees and shrubs. Chinese pistache trees would provide a canopy of shade in conjunction with the parking area. Understory planting within the parking lot would consist of low shrubs and groundcover. Lacebark elms would line the bioretention filter in the southeast corner of the parcel, and along the additional carports in the northwest corner of the parcel. Red oak trees would line the northern boundary of the Lot. The planting and irrigation would be designed to meet the Model Water Efficient Landscape Ordinance requirements by utilizing low water use plant material and a high efficiency irrigation system. Seven oak trees in the southwest corner of the project site would be removed, while two oak trees would be incorporated into the landscape design. Masonry walls would be constructed to provide privacy for the fire pit and picnic area, adjacent to the pool area in the southwestern corner, and for the seating area, adjacent to the main entrance in the southern portion of the parcel.

Fencing

A 6-foot height metal fence would be installed along the northeastern, eastern, southern, and western boundaries of Lot 1. A 6-foot height metal fence would be installed along the eastern, southern, and western boundary of Lot 6.

Signage

Project signage would be installed on masonry walls at the main entrance driveway of Lot 1 and Lot 6. In addition, directional signage would be provided on each parcel.

3.4 General Plan Land Use Designation and Zoning

The City of Folsom updated their General Plan 2035 in August 2018. The General Plan is a long-term planning document that guides growth and land development in the City. It provides the foundation for establishing community goals and supporting policies, and directs appropriate land uses for all land parcels within the City.

General Plan Land Use Designation

The General Plan is a long-term planning document that guides growth and land development in the City. It provides the foundation for establishing community goals and supporting policies, and directs appropriate land uses for all land parcels within the City. Under the current General Plan, both project parcels have a land use designation of Industrial/Office Park (IND). However, the proposed project would require a General Plan Amendment from IND to multi-family high density residential (MHD) for both Lot 1 and Lot 6. The MHD designation provides for multifamily residential units in apartment buildings. The proposed multi-family apartment complex and related amenities on Lot 1 and Lot 6 are identified as permitted uses under the MHD designation in the General Plan.

Zoning Ordinance

Developed land uses in the City of Folsom are regulated specifically by the City's Zoning Code (Title 17 of the City's Municipal Code), in addition to the other adopted regulations and programs that apply to all proposed development within the City. In more detail than the General Plan, the Zoning Code regulates land uses on a parcel-by-parcel basis throughout the City. To achieve this regulation, the City assigns each parcel within the City to a zoning district, such as a district for single-family homes. Regulations for each district apply equally to all properties within the district.

Current zoning for Lot 1 is Limited Manufacturing, Planned Development District (M-L PD), and current zoning for Lot 6 is Business and Professional, Planned Development District (B-P PD). The proposed project would require a rezone at Lot 1 from M-L PD to R-4 PD, and a rezone at Lot 6 from B-P PD to R-4 PD. The Planned Development combining zone would remain.

Chapter 17.17 of the Zoning Code outlines use standards for Multi-Family High Density (MHD). The purpose of the MHD zone is to designate areas where group dwellings and apartments are a logical and desirable use. This designation allows for multi-family residential units with 20 to 30 dwelling units per acre.

3.5 City Regulation of Urban Development

Other City Regulation of Urban Development

The City of Folsom further regulates urban development through standard construction conditions and through mitigation, building, and construction requirements set forth in the Folsom Municipal Code. Required of all projects constructed throughout the City, compliance with the requirements of the City's standard conditions and the provision of the Municipal Code avoids or reduces many potential environmental effects. City procedures to minimize negative environmental effects and disruptions include an analysis of existing features, responsible agency and public input to the design process, engineering and design standards, and construction controls. The activities that mitigate typical environmental impacts to be implemented by the City during the project review, design, and construction phases are described in greater detail below.

Community Development Department Standard Construction Conditions

The City's standard construction requirements are set forth in the City of Folsom, Community Development Standard Construction Specifications updated in May 2020. A summary of these requirements is set forth below and incorporated by reference into the project description. Copies of these documents may be reviewed at the City of Folsom, Community Development Department, 50 East Natoma Street, Folsom, California 95630.

The Department's standard construction specifications are required to be adhered to by any contractor constructing a public or private project within the City.

Use of Pesticides – Requires contractors to store, use, and apply a wide range of chemicals consistent with all local, state, and federal rules and regulations.

Air Pollution Control – Requires compliance with all City of Folsom and County of Sacramento air pollution regulations.

Water Pollution – Requires compliance with City water pollution regulations, including National Pollutant Discharge Elimination System (NPDES) provisions.

Noise Control – Requires that all construction work comply with the Folsom Noise Ordinance (discussed further below), and that all construction vehicles be equipped with a muffler to control sound levels. The Contractor shall comply with all local sound control and noise level rules, regulations and ordinances which apply to any work performed pursuant to the Contract Documents.

Naturally Occurring Asbestos – All work involved asbestos containing material must be performed in accordance with California Labor Code, sections 6501.5 through 6510, inclusive, and California Administrative Code, Title 8, Section 5208 and all other pertinent laws, rules, regulations, codes, ordinances, decrees and orders.

Weekend, Holiday, and Night Work – Prohibits construction work during evening hours, or on Sunday or holidays, to reduce noise and other construction nuisance effects.

Public Convenience – Regulates traffic through the work area, operations of existing traffic signals, roadway cuts for pipelines and cable installation, effects to adjacent property owners, and notification of adjacent property owners and businesses.

Public Safety and Traffic Control – Regulates signage and other traffic safety devices through work zones.

Existing Utilities – Regulates the relocation and protection of utilities.

Preservation of Property – Requires preservation of trees and shrubbery and prohibits adverse effects to adjacent property and fixtures.

Cultural Resources – Requires that contractors stop work upon the discovery of unknown cultural or historic resources, and that an archaeologist be retained to evaluate the significance of the resource and to establish mitigation requirements, if necessary.

Protection of Existing Trees – Specifies measures necessary to protect both ornamental and native oak trees.

Clearing and Grubbing – Specifies protection standards for signs, mailboxes, underground structures, drainage facilities, sprinklers and lights, trees and shrubbery, and fencing. Also requires the preparation of a Stormwater Pollution Prevention Plan (SWPPP) to control erosion and siltation of receiving waters.

Reseeding – Specifies seed mixes and methods for reseeded of graded areas.

City of Folsom Municipal Code

The City regulates many aspects of construction and development through requirements and ordinances established in the Folsom Municipal Code. These requirements are summarized in **Table 3**, and hereby incorporated by reference into the Project Description as though fully set forth herein. Copies of these documents may be reviewed at the City of Folsom, Office of the City Clerk, 50 Natoma Street, Folsom, California 95630.

Table 3. City of Folsom Municipal Code Regulating Construction and Development

CODE SECTION	CODE NAME	EFFECT OF CODE
8.42	Noise Control	Establishes interior and exterior noise standards that may not be exceeded within structures, including residences; establishes time periods for construction operations.
8.70	Stormwater Management and Discharge Control	Establishes conditions and requirements for the discharge of urban pollutants and sediments to the storm-drainage system; requires preparation and implementation of Stormwater Pollution Prevention Plans.
9.34	Hazardous Materials Disclosure	Defines hazardous materials; requires filing of a Hazardous Material Disclosure Form by businesses that manufacture, use, or store such materials.
9.35	Underground Storage of Hazardous Substances	Establishes standards for the construction and monitoring of facilities used for the underground storage of hazardous

		substances, and establishes a procedure for issuance of permits for the use of these facilities.
12.16	Tree Preservation	Regulates the cutting or modification of trees, including oaks and specified other trees; requires a Tree Permit prior to cutting or modification; establishes mitigation requirements for cut or damaged trees.
13.26	Water Conservation	Prohibits the wasteful use of water; establishes sustainable landscape requirements; defines water use restrictions.
14.19	Energy Code	Adopts the California Energy Code, 2010 Edition, published as Part 6, Title 24, C.C.R. to require energy efficiency standards for structures.
14.20	Green Building Standards Code	Adopts the California Green Building Standards Code (CALGreen Code), 2010 Edition, excluding Appendix Chapters A4 and A5, published as Part 11, Title 24, C.C.R. to promote and require the use of building concepts having a reduced negative impact or positive environmental impact and encouraging sustainable construction practices.
14.29	Grading Code	Requires a grading permit prior to the initiation of any grading, excavation, fill or dredging; establishes standards, conditions, and requirements for grading, erosion control, stormwater drainage, and revegetation.
14.32	Flood Damage Prevention	Restricts or prohibits uses that cause water or erosion hazards, or that result in damaging increases in erosion or in flood heights; requires that uses vulnerable to floods be protected against flood damage; controls the modification of floodways; regulates activities that may increase flood damage or that could divert floodwaters.

4.0 PROJECT OBJECTIVES

The objective of the proposed project is to develop a high-quality planned residential development on two currently vacant infill sites in the City of Folsom. The objective of providing the residential development must be achieved while minimizing environmental impacts to the maximum extent practicable and while meeting the requirements of the General Plan, as amended.

5.0 REQUIRED APPROVALS

A listing and brief description of the regulatory permits and approvals required to implement the proposed project is provided below. This environmental document is intended to address the environmental impacts associated with all the following decision actions and approvals:

- Planned Development Permit:** Because the proposed project would be sited within a Planned Development overlay zoning designation, the project requires a Planned Development Permit. This designation requires review by the Planning Commission from design review purposes.

- **General Plan Amendment:** Because the proposed project would include the construction of a multi-family unit apartment community, the project requires a General Plan Amendment to change the existing land use designation from Industrial (IND) to Multi-family High Density (MHD).
- **Rezone Permit:** Currently, Lot 1 is zoned for Limited Manufacturing Planned Development (M-L, PD) and Lot 6 is zoned for Business and Professional Planned Development (BP, PD). Because the proposed project would include the construction of a multi-family unit apartment community on both lots, a rezone is required to change both zones to General Apartment, Planned Development District (R-4 PD).
- **Design Review:** The proposed project of Lot 1 and Lot 6 would bring new construction to these vacant parcels. Therefore, the proposed construction of Lot 1 and Lot 6 will be subject to design review.
- **Tree Removal Permit:** The proposed project requests a tree permit to remove five trees of Lot 6. Per the Amended Arborist Report by Arborwell, one additional tree is recommended for removal due to its poor condition.

The City has the following discretionary powers related to the proposed project:

- **Certification of the environmental document:** The City Council will act as the lead agency as defined by the California Environmental Quality Act (CEQA) and will have authority to determine if the environmental document is adequate under CEQA.
- **Approval of project:** The City Council will consider approval of the project and all entitlements as described above.

California Department of Fish and Wildlife consultation would be required if active nests are found for species protected by the Migratory Bird Treaty Act, as applicable.

6.0 PREVIOUS RELEVANT ENVIRONMENTAL ANALYSIS

6.1 City of Folsom General Plan

The City of Folsom General Plan provides a framework for the long-range development of Folsom. This General Plan also covers what was previously described in the East Area Facilities Plan. The General Plan guides policy decision-making about land use, transportation improvements, public services, economic development housing, and other issues. The EIR for the 2035 City of Folsom General Plan updated and revised the environmental conclusions of the 1988 General Plan EIR, expanding analysis to include development in unincorporated areas around the City and five additional chapters on matters of local interest (City of Folsom 2018). The EIR for the 2035 General Plan provides the foundation environmental document for evaluating development throughout this part of the City.

6.2 Tiering

“Tiering” refers to the relationship between a program-level EIR (where long-range programmatic cumulative impacts are the focus of the environmental analysis) and subsequent environmental analyses such as the subject document, which focus primarily on issues unique to a smaller project within the larger program or plan. Through tiering a subsequent environmental analysis can incorporate, by reference, discussion that summarizes general environmental data found in the program EIR that establishes cumulative impacts and mitigation measures, the planning context, and/or the regulatory background. These broad-based issues need not be reevaluated subsequently, having been previously identified and evaluated at the program stage.

Tiering focuses the environmental review on the project-specific significant effects that were not examined in the prior environmental review, or that are susceptible to substantial reduction or avoidance by specific revisions in the project, by the imposition of conditions or by other means. Section 21093(b) of the Public Resources Code requires the tiering of environmental review whenever feasible, as determined by the Lead Agency.

In the case of the proposed project, this Initial Study tiers from the EIR for the City of Folsom General Plan as amended by approval of the East Area Facilities Plan. The Folsom General Plan, as amended, is a project that is related to the proposed project and, pursuant to §15152(a) of the CEQA Guidelines, tiering of environmental documents is appropriate. CEQA Guidelines §15152(e) specifically provides that:

“[w]hen tiering is used, the later EIRs or Negative Declarations shall refer to the prior EIR and state where a copy of the prior EIR may be examined. The later [environmental document] should state that the Lead Agency is using the tiering concept and that the [environmental document] is being tiered with the earlier EIR.”

The above mentioned EIRs can be reviewed at the following location:

City of Folsom
Community Development Department
50 Natoma Street
Folsom, CA 95630

Contact: Mr. Steve Banks, Principal Planner
(916) 461-6207

6.3 Incorporation of the Folsom General Plan by Reference

Due to various references to the Folsom General Plan EIR in this proposed project, and to its importance relative to understanding the environmental analysis that has occurred to date with respect to development in the Folsom area, the Folsom General Plan EIR is hereby incorporated by reference pursuant to CEQA Guidelines Section 15150.

6.4 Summary of Folsom General Plan EIR

The Folsom General Plan EIR analyzed the environmental impacts associated with adoption of the City of Folsom General Plan allowing for development, open space preservation, and provision of services land in and adjacent to the City of Folsom.

The Draft Program Environmental Impact Report for the Folsom General Plan identified 453 vacant parcels north of Highway 50 as an area of future development. The Folsom General Plan contemplates the full range of land uses that would constitute a balanced community, including residential uses at a variety of densities, as well as commercial, office, employment, and open space uses. Additionally, public or quasi-public uses are contemplated by the Folsom General Plan, including schools, parks, fire stations, government offices, and other uses.

7.0 ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that may require mitigation to reduce the impact from "Potential Impact" to "Less than Significant" as indicated by the checklist on the following pages.

An Initial Study is conducted by a Lead Agency to determine if a project may have a potentially significant effect on the environment (CEQA Guidelines Section 15063). An EIR must be prepared if an Initial Study indicates that further analysis is needed to determine whether a significant impact will occur or if there is substantial evidence in the record that a project may have a significant effect on the environment (CEQA Guidelines Section 15064(f)).

<input type="checkbox"/> Aesthetics	<input type="checkbox"/> Agriculture/Forestry Resources	<input checked="" type="checkbox"/> Air Quality
<input checked="" type="checkbox"/> Biological Resources	<input checked="" type="checkbox"/> Cultural Resources	<input type="checkbox"/> Geology/Soils
<input checked="" type="checkbox"/> Greenhouse Gas Emissions	<input type="checkbox"/> Hazards/Hazardous Materials	<input type="checkbox"/> Hydrology/Water Quality
<input type="checkbox"/> Land Use/Planning	<input type="checkbox"/> Mineral Resources	<input checked="" type="checkbox"/> Noise
<input type="checkbox"/> Population/Housing	<input type="checkbox"/> Public Services	<input type="checkbox"/> Recreation
<input checked="" type="checkbox"/> Transportation/Traffic	<input checked="" type="checkbox"/> Tribal Cultural Resources	<input type="checkbox"/> Utilities/Service Systems
<input type="checkbox"/> Mandatory Findings of Significance		

8.0 DETERMINATION

On the basis of this initial evaluation:

<input type="checkbox"/>	I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
<input checked="" type="checkbox"/>	I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
<input type="checkbox"/>	I find that the proposed project MAY have a significant effect on the environment, and an environmental impact report is required.
<input type="checkbox"/>	I find that the proposed project MAY have a "potential impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
<input type="checkbox"/>	I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.



 Signature

3/14/22

 Date

Steven Banks

 Printed Name

Principal Planner

 Title

9.0 ENVIRONMENTAL INITIAL STUDY CHECKLIST

Responses to the following questions and related discussion indicate if the proposed project will have or will potentially have a significant adverse impact on the environment, either individually or cumulatively with other projects. All phases of project planning, implementation, and operation are considered. Mandatory Findings of Significance are addressed in Section 9.19 below.

- A. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- B. "Less Than Significant With Mitigation" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Than Significant Impact." The lead agency must describe the mitigation measures and briefly explain how they reduce the effect to a less than significant level (mitigation measures from earlier analyses may be cross-referenced).
- C. "Less Than Significant Impact" applies where the project creates no significant impacts, only less than significant impacts.
- D. "No Impact" applies where a project does not create an impact in that category. "No Impact" answers do not require an explanation if they are adequately supported by the information sources cited by the lead agency which show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project would not expose sensitive receptors to pollutants, based on a project specific screening analysis).

I. AESTHETICS

AESTHETICS:		Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:					
a)	Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b)	Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c)	Substantially degrade the existing visual character or quality of the site and its surroundings?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d)	Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

Lot 1 is currently undeveloped, and is bordered by oak woodlands and the Kaiser medical clinic to the north, planned dialysis clinic to the east, and US Highway 50 to the south. The site is constrained by high tension powerlines on its west side, and commercial buildings, a memory care facility, and a vacant lot containing oak woodland to the west. Lot 1 has one existing oak tree in the southeastern corner of the parcel.

Lot 6 is currently undeveloped and is bordered by Iron Point Road to the north, a constructed pond/wetland and office buildings to the east, an office building and undeveloped land containing scattered oaks to the south, and an office building to the west. A strand of oak trees within a designated preserve separates Lot 6 from the existing office building to the west.

Evaluation of Aesthetics

Question a: No Impact. A scenic vista is defined as a viewpoint that provides expansive view of a highly valued landscape for the benefit of the public. Neither the project site nor the surrounding areas are considered to be scenic vistas due to the existing development and suburban environment typical of the area. Further, neither the project site, nor views to or from the project site, have been designated as an important scenic resource by the City of Folsom or any other public agency (Folsom 2018). Therefore, construction or operation of the proposed development would not interfere with or degrade a scenic vista. No impacts would occur, and no mitigation would be necessary.

Question b: No Impact. There are no state or locally designated scenic highways in the vicinity of the proposed project (CalTrans 2021, Folsom 2018). Implementation of the proposed project would not adversely affect scenic resources within a designated scenic highway. Although the project is bordered by US Highway 50 to the south, it is not considered a scenic highway. Therefore, no impact would occur, and no mitigation would be necessary.

Question c: Less than Significant Impact. The existing visual character of the area surrounding the project site is characteristic of suburban development and is primarily defined by commercial, business

offices, residential, and transportation land uses. Development of an apartment complex on Lot 1 and Lot 6 would be consistent with the surrounding suburban land uses and development. The project site would be visible by motorists and pedestrians travelling along Iron Point Road, and by motorists travelling along US Highway 50. Implementation of the project would result in the development of high-density residential structures on undeveloped land, surrounded by commercial, residential, and residential uses.

While the proposed project would inevitably result in a change in visual character on the vacant site, the proposed land uses are consistent with the overall suburban development in the vicinity, and the proposed developments are expected to integrate into the existing and planned development within the area. Therefore, a less than significant impact to visual character would occur and no mitigation is necessary.

Question d: Less than Significant Impact. Any new lighting associated with development of the project site would be subject to the City's standard practices regarding night lighting that would be made a condition of approval of the Planned Development Permit. Consistent with the City's practices, the lighting would be sited and designed to avoid light spillage and glare on adjacent properties, with photo-controlled shut-off, and auto-schedule and motion sensors. All lighting would be designed to minimize light/glare impacts to the adjacent properties by ensuring that all exterior lighting and pole-mounted parking lot and driveway lighting be shielded and directed downward. Light-emitting diode luminaires would be used for all of the proposed outdoor lighting. Because existing City practices would limit light spillover and intensity, this would be a less than significant impact, and no mitigation is necessary.

II. AGRICULTURE AND FORESTRY RESOURCES

AGRICULTURE AND FORESTRY RESOURCES:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non- forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

No agricultural activities or timber management occur on the project site or in adjacent areas and the site is not designated for agricultural or timberland uses. The California Important Farmlands Map prepared for Sacramento County by the California Department of Conservation classifies Lot 1 as grazing land surrounded by urban and built up and Lot 6 as other land (California Department of Conservation [CDC] 2018a). Urban and built-up land is land occupied by structures or infrastructure to accommodate a building density of at least one unit to one and one-half acres, or approximately six structures to 10-acres; grazing land is land on which vegetation is suited to the grazing of livestock; and other land is land not included in any other mapping category – typically vacant and nonagricultural lands (CDC 2018a).

Evaluation of Agriculture and Forestry Resources

Question a, b: No Impact. The project site is not considered Prime Farmland, Unique Farmland, or Farmland of Statewide importance (Farmland), pursuant to the Farmland Mapping and Monitoring Program of the California Department of Conservation (CDC 2018a). The project site is not zoned for

agricultural use or enacted into a Williamson Act contract. No impacts would occur, and no mitigation would be necessary for questions a) and b).

Question c, d: No Impact. Because no portion of the City or the project site are zoned for forest land, timberland, or zoned Timberland Production, no impact would occur, and no mitigation would be necessary for questions c) and d).

Question e: Less Than Significant Impact. Lot 1 has been identified as grazing land surrounded by urban and built-up land. This area is considered to be highly disturbed with marginal grazing opportunities due to its proximity to a main road and surrounding urban development. Because no important agricultural resources or activities exist on the project site, impacts would be less than significant, and no mitigation would be necessary.

III. AIR QUALITY

AIR QUALITY:

Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e) Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

HELIX Environmental Planning, Inc. conducted air quality modeling (CalEEMod) for the proposed project based primarily on the preliminary site plan and the Transportation Impact Study conducted by T. Kear Transportation Planning and Management, Inc. (2021). Additionally, due to the proposed project's proximity to US Highway 50 a Health Risk Assessment was performed. Air quality modeling output files and quantitative results are presented in **Appendix B**.

Environmental Setting

Climate in the Folsom area is characterized by hot, dry summers and cool, rainy winters. During summer's longer daylight hours, plentiful sunshine provides the energy needed to fuel photochemical reactions between Oxides of Nitrogen (NO_x) and Reactive Organic Gasses (ROG), which result in Ozone (O₃) formation. High concentrations of O₃ are reached in the Folsom area due to intense heat, strong and low morning inversions, greatly restricted vertical mixing during the day, and daytime subsidence that strengthens the inversion layer. The greatest pollution problem in the Folsom area is from NO_x.

The City of Folsom lies within the eastern edge of the Sacramento Valley Air Basin (SVAB). The Sacramento Metropolitan Air Quality Management District (SMAQMD) is responsible for implementing emissions standards and other requirements of federal and state laws in the project area. As required by the California Clean Air Act (CCAA), SMAQMD has published various air quality planning documents as discussed below to address requirements to bring the District into compliance with the federal and state ambient air quality standards. The Air Quality Attainment Plans are incorporated into the State Implementation Plan (SIP), which is subsequently submitted to the U.S. Environmental Protection Agency (EPA), the federal agency that administrates the Federal Clean Air Act of 1970, as amended in 1990.

Ambient air quality is described in terms of compliance with state and national standards, and the levels of air pollutant concentrations considered safe, to protect the public health and welfare. These standards are designed to protect people most sensitive to respiratory distress, such as people with asthma, the elderly, very young children, people already weakened by other disease or illness, and persons engaged in strenuous work or exercise. The EPA has established national ambient air quality standards (NAAQS) for seven air pollution constituents. As permitted by the Clean Air Act, California has adopted more stringent air emissions standards (California Ambient Air Quality Standards, or CAAQS) and expanded the number of regulated air constituents.

The California Air Resources Board (CARB) is required to designate areas of the state as attainment, nonattainment, or unclassified for any state standard. An “attainment” designation for an area signifies that pollutant concentrations do not violate the standard for that pollutant in that area. A “nonattainment” designation indicates that a pollutant concentration violated the standard at least once. The air quality attainment status of the SVAB, including the City of Folsom, is shown in **Table 4**.

Table 4. Sacramento Valley Air Basin Attainment Status

POLLUTANT	STATE OF CALIFORNIA ATTAINMENT STATUS	FEDERAL ATTAINMENT STATUS
Ozone (1-hour)	Nonattainment	No Federal Standard
Ozone (8-hour)	Nonattainment	Nonattainment
Coarse Particulate Matter (PM ₁₀)	Nonattainment	Attainment
Fine Particulate Matter (PM _{2.5})	Attainment	Nonattainment
Carbon Monoxide (CO)	Attainment	Attainment/Unclassified
Nitrogen Dioxide (NO ₂)	Attainment	Attainment/Unclassified
Lead	Attainment	Attainment/Unclassified
Sulfur Dioxide (SO ₂)	Attainment	Unclassified
Sulfates	Attainment	No Federal Standard
Hydrogen Sulfide	Unclassified	No Federal Standard
Visibility Reducing Particles	Unclassified	No Federal Standard

Sources: SMAQMD 2020a.

Sacramento County is designated as nonattainment for the state and federal ozone standards, the state PM₁₀ standards, and the federal PM_{2.5} standards. Concentrations of all other pollutants meet state and federal standards.

Ozone is not emitted directly into the environment, but is generated from complex chemical reactions between ROG, or non-methane hydrocarbons, and NO_x that occur in the presence of sunlight. ROG and NO_x generators in Sacramento County include motor vehicles, recreational boats, other transportation sources, and industrial processes. PM₁₀ and PM_{2.5} arise from a variety of sources, including road dust, diesel exhaust, fuel combustion, tire and brake wear, construction operations and windblown dust.

Toxic Air Contaminants

Toxic air contaminants (TAC) are a diverse group of air pollutants that may cause or contribute to an increase in deaths or in serious illness or that may pose a present or potential hazard to human health. TACs can cause long-term chronic health effects such as cancer, birth defects, neurological damage, asthma, bronchitis, or genetic damage, or short-term acute effects such as eye watering, respiratory irritation (a cough), runny nose, throat pain, and headaches. TACs are considered either carcinogenic or

noncarcinogenic based on the nature of the health effects associated with exposure to the pollutant. For carcinogenic TACs, there is no level of exposure that is considered safe and impacts are evaluated in terms of overall relative risk expressed as excess cancer cases per one million exposed individuals. Noncarcinogenic TACs differ in that there is generally assumed to be a safe level of exposure below which no negative health impact is believed to occur. These levels are determined on a pollutant-by-pollutant basis.

The Health and Safety Code (§39655[a]) defines TAC as “an air pollutant which may cause or contribute to an increase in mortality or in serious illness, or which may pose a present or potential hazard to human health.” All substances that are listed as hazardous air pollutants pursuant to subsection (b) of Section 112 of the CAA (42 United States Code Sec. 7412[b]) are designated as TACs. Under State law, the California Environmental Protection Agency (CalEPA), acting through CARB, is authorized to identify a substance as a TAC if it determines the substance is an air pollutant that may cause or contribute to an increase in mortality or an increase in serious illness, or that may pose a present or potential hazard to human health.

Diesel engines emit a complex mixture of air pollutants, including both gaseous and solid material. The solid material in diesel exhaust is referred to as diesel particulate matter (DPM). Almost all DPM is 10 microns or less in diameter, and 90 percent of DPM is less than 2.5 microns in diameter (CARB 2021a). Because of their extremely small size, these particles can be inhaled and eventually trapped in the bronchial and alveolar regions of the lung. In 1998, CARB identified DPM as a TAC based on published evidence of a relationship between diesel exhaust exposure and lung cancer and other adverse health effects. DPM has a notable effect on California’s population—it is estimated that about 70 percent of total known cancer risk related to air toxics in California is attributable to DPM (CARB 2021a).

Air Quality Monitoring

The SMAQMD operates a network of ambient air monitoring stations throughout the Sacramento region. The purpose of the monitoring stations is to measure ambient concentrations of criteria air pollutants and determine whether the ambient air quality meets state and federal standards, pursuant to the CAAQS and the NAAQS. The nearest ambient monitoring station to the project site is the East Natoma Street monitoring station located approximately 3-miles northwest of the project site. The closest monitoring station with data for PM₁₀ is the Sacramento – Branch Center Road 2 monitoring station, approximately 13.2-miles southwest of the project site. Air quality data collected at these monitoring stations for the years 2018 through 2020 are shown in **Table 5**.

Table 5. Summary of Annual Air Quality Data for Folsom Area Air Quality Monitoring Stations

POLLUTANT	2018	2019	2020
<i>Ozone (O₃): Monitoring location: Folsom – East Natoma Street</i>			
Maximum concentration 1-hour period (ppm)	0.105	0.087	0.038
Maximum concentration 8-hour period (ppm)	0.094	0.073	0.036
Days above 1-hour state standard (>0.09 ppm)	5	0	0
Days above 8-hour state/federal standard (>0.070 ppm)	19	2	0
<i>Coarse Particulate Matter (PM₁₀): Monitoring location: Sacramento – Branch Center Road 2</i>			
Maximum 24-hour concentration (µg/m ³)	200.0	53.0	201.0
Measured Days above 24-hr state standard (>50 µg/m ³)	4	1	10
Measured Days above 24-hr federal standard (>150 µg/m ³)	1	0	1
Annual average (µg/m ³)	26.5	18.4	33.2
Exceed state annual standard (20 µg/m ³)	Yes	No	Yes
<i>Fine Particulate Matter (PM_{2.5}): Monitoring location: Folsom – East Natoma Street</i>			
Maximum 24-hour concentration (µg/m ³)	104.5	25.4	19.6
Measured Days above 24-hour federal standard (>35 µg/m ³)	9	0	0
Annual average (µg/m ³)	10.2	*	*
Exceed state and federal annual standard (12 µg/m ³)	No	*	*
<i>Nitrogen Dioxide (NO₂): Monitoring location: Folsom – East Natoma Street</i>			
Maximum 1-hour concentration (ppm)	0.029	0.015	*
Days above state 1-hour standard (0.18 ppm)	0	0	*
Days above federal 1-hour standard (0.100 ppm)	0	0	*
Annual average (ppm)	0.003	*	*
Exceed annual federal standard (0.053 ppm)	No	*	*
Exceed annual state standard (0.030 ppm)	No	*	*

Source: CARB 2021b.

ppb = parts per billion; ppm = parts per million; µg/m³ = micrograms per cubic meter, * = insufficient data available.

As Shown in **Table 5**, the state 1-hour ozone standard was exceeded on five days in 2018, the state/federal 8-hour ozone standard was exceeded on 19 days in 2018 and two days in 2019, and the state/federal PM₁₀ standards were exceeded on multiple day in 2018 through 2020 and the federal PM_{2.5} standard was exceeded on nine days in 2018. There were no exceedances of NO₂ standards in 2018 through 2020.

Air Quality Attainment Planning

In order to work towards attainment for ozone, PM₁₀ and PM_{2.5}, the EPA Office of Air Quality Planning and Standards requires that each state containing nonattainment areas develop a written plan for cleaning the air in those areas. The plans developed combine to make up the SIP. Through these plans, states outline efforts they will make to try to correct the levels of air pollution and bring their areas back into attainment. The status of air quality attainment planning for the Sacramento area is listed below (SMAQMD 2017):

- **8-Hour O₃.** The Sacramento region was classified by the EPA as a “serious” nonattainment area on June 15, 2004 for the federal 8-hour ozone standard, with an attainment deadline of June 15, 2013. Emission reductions needed to achieve the air quality standard were identified using an

air quality modeling analysis. An evaluation of proposed control measures and associated ROG and NO_x emission reductions concluded that no set of feasible controls were available to provide the needed emission reductions before the attainment deadline year. Given the magnitude of the shortfall in emission reductions, and the schedule for implementing new control measures, the earliest possible attainment demonstration year for the Sacramento region is determined to be the "severe" area deadline of 2019. Section 181(b)(3) of the Clean Air Act permits a state to request that the EPA reclassify a nonattainment area to a higher classification and extend the time allowed for attainment. This process is appropriate for areas that must rely on longer-term strategies to achieve the emission reductions needed for attainment. The EPA approved this request on May 5, 2010. The Sacramento Regional 8-Hour Ozone Attainment and Reasonable Further Progress Plan was developed by the air districts in the Sacramento region to bring the region into attainment for the ozone NAAQS and CAAQS. The plan is a joint project between the SMAQMD, and four other air districts in the Sacramento region (SMAQMD 2017).

- **1-Hour O₃.** On May 9, 2011, EPA proposed to determine that California is no longer required to implement or submit a CAA Section 185 fee program for 1-hour ozone as a revision to the SIP for the Sacramento Metro 1-hour ozone nonattainment area. EPA has also taken an "interim final" action to stop sanctions from applying to the Sacramento Metro Area.
- **PM₁₀.** In March 2002, the EPA officially determined that Sacramento County had attained the PM₁₀ standards. In November 2010, the SMAQMD formally requested that the EPA redesignate Sacramento County from nonattainment to attainment for PM₁₀. The EPA approved this request effective October 28, 2013. The SMAQMD additionally adopted a PM₁₀ Maintenance Plan. The first Maintenance Plan showed maintenance from 2012 through 2022. A Second Maintenance Plan will be prepared and submitted by The SMAQMD to demonstrate maintenance for ten additional years, through 2032.
- **PM_{2.5}.** The Sacramento PM_{2.5} nonattainment area designation met the PM_{2.5} NAAQS by December 31, 2011. On May 9, 2012, CARB submitted a request that EPA find the Sacramento region in attainment for the 2006 24-hour PM_{2.5} NAAQS. EPA issued a proposed rule for Determination of Attainment for the Sacramento Nonattainment Area on October 26, 2012 and a final rule for Determination of Attainment on July 15, 2013. EPA used the updated 2010-2012 ambient air quality data for determination and the final rule became effective on August 14, 2013 (SMAQMD 2017) (EPA 2013). On May 10, 2017, the EPA found the area attained the 2006 24-hour NAAQS by the attainment date of December 31, 2015 based on monitoring data for 2013-2015. The 2013 Maintenance Plan and will be updated and submitted in the future based on the clean data finding made by the EPA.
- **CO.** The region is currently designated attainment for 1-hour and 8-hour CO standards. The Maintenance Plan developed for CO in 1996 was revised in 2004 to extend the 1996 CO Maintenance Plan demonstration to 2018.

Evaluation of Air Quality

While the final determination of whether or not a project has a significant effect is within the purview of the lead agency pursuant to CEQA Guidelines Section 15064(b), SMAQMD recommends that its air pollution thresholds be used to determine the significance of project emissions. The criteria pollutant

thresholds and various assessment recommendations are contained in SMAQMD's *Guide to Air Quality Assessment in Sacramento County* (CEQA Guide; 2020, revised), and are discussed under the checklist questions below.

Question a: Less than Significant Impact. In accordance with SMAQMD's CEQA Guide, construction-generated NO_x , PM_{10} , and $\text{PM}_{2.5}$, and operational-generated ROG and NO_x (all ozone precursors) are used to determine consistency with the Ozone Attainment Plan. The Guide states (SMAQMD 2020a p. 4-6):

By exceeding the District's mass emission thresholds for operational emissions of ROG, NO_x , PM_{10} , or $\text{PM}_{2.5}$, the project would be considered to conflict with or obstruct implementation of the District's air quality planning efforts.

As shown in the discussion for question b) below, the project's construction-generated emissions of NO_x , PM_{10} , and $\text{PM}_{2.5}$ and operation-generated emissions ROG and NO_x would not exceed SMAQMD thresholds. The project would not conflict with or obstruct implementation of the applicable air quality plan and the Impact would be less than significant.

Question b: Less than Significant Impact. The Sacramento region is in non-attainment for ozone (ozone precursors NO_x and ROG) and particulate matter ($\text{PM}_{2.5}$ and PM_{10}). The project's emissions of these criteria pollutants and precursors during construction and operation are evaluated below.

Construction Emissions

The California Emissions Estimator Model (CalEEMod) version 2020.4.0 was used to quantify project-generated construction emissions. The model output sheets are included in **Appendix B**. Construction activities were assumed to commence as early as May 2022 and be completed in early 2024. The quantity, duration, and intensity of construction activity influence the amount of construction emissions and related pollutant concentrations that occur at any one time. As such, the emission forecasts provided herein reflect a specific set of conservative assumptions based on the expected construction scenario wherein a relatively large amount of construction activity is occurring in a relatively intensive manner. Because of this conservative assumption, actual emissions could be less than those forecasted. If construction is delayed or occurs over a longer time period, emissions could be reduced because of: (1) a more modern and cleaner-burning construction equipment fleet mix than assumed in CalEEMod; and/or, (2) a less intensive buildout schedule (i.e., fewer daily emissions occurring over a longer time interval).

Construction emissions would be generated by vehicle engine exhaust from off-road construction equipment, on-road hauling trucks, vendor trips, and worker commuting trips. Grading cut/fill would be balanced on-site—no import or export of soil would be required. During paving approximately 289 truckloads (578 one-way truck trips) of aggregate/asphalt would be imported to the site. Model defaults were used for all construction activities with the following modifications:

- The project site is vacant, and no demolition would be required.
- An additional activity for excavation/installation of underground utilities was added, assumed to require one month.
- The use of a water truck for four hours per workday was assumed for the site preparation, grading, and underground utilities activities.

- Architectural coating (e.g., painting) was assumed to occur concurrently with the last three months of physical building construction.

The project's construction period emissions of ROG, NO_x, PM₁₀, and PM_{2.5} are compared to the SMAQMD construction thresholds in **Table 6**. The SMAQMD does not have a recommended threshold for construction-generated ROG. However, quantification and disclosure of ROG emissions is recommended. The SMAQMD considers any emissions of PM₁₀ and PM_{2.5} to be significant unless the Basic Construction Emissions Control Practices are implemented, also known as Best Management Practices (BMP). The project would implement all of the SMAQMD BMPs to control fugitive dust in accordance with SMAQMD Rule 403. The modeling accounts for emissions reductions resulting from watering exposed surfaces twice daily. As shown in **Table 6**, the proposed project construction period emissions of the ozone precursor NO_x, PM₁₀, and PM_{2.5} would not exceed the SMAQMD thresholds. Impacts related to construction-generated emissions of ROG, NO_x, PM₁₀, and PM_{2.5} would be less than significant.

Table 6. Construction Criteria Pollutant and Precursor Emissions

ACTIVITY	NO_x (pounds/day)	ROG (pounds/day)¹	PM₁₀ (pounds/day)	PM_{2.5} (pounds/day)
Site Preparation	35.1	3.5	10.7	6.1
Grading	40.9	4.0	6.0	3.3
Underground Utilities	10.2	1.1	0.5	0.4
Paving	16.6	1.8	1.2	0.7
Building Construction	19.5	2.8	3.1	1.4
Architectural Coatings	1.4	51.2	0.5	0.2
Concurrent 2023 Building Construction and Architectural Coating	19.1	53.7	3.5	1.5
Maximum Daily Emissions	40.9	53.7²	10.7	6.1
<i>SMAQMD Threshold</i>	<i>None</i>	<i>85</i>	<i>80</i>	<i>82</i>
Threshold exceeded?	No	No	No	No

Source of emissions estimates: CalEEMod output (Appendix B).

Source of threshold: SMAQMD 2020a.

¹ Maximum daily emissions of ROG would occur in summer, maximum daily emissions of all other analyzed pollutants would occur in winter or are not seasonally dependent.

² Maximum daily emissions of ROG would be the combined emissions from Building Construction and Architectural Coating which would occur concurrently in 2023.

Operational Emissions

Regional Emissions

SMAQMD provides screening levels to identify when additional analysis is necessary to determine potential significance for operational ROG, NO_x, PM₁₀, or PM_{2.5} emissions. The operational screening

levels represent the development size at which the operational emissions thresholds of significance would not be exceeded. According to the screening thresholds, if a proposed mid-rise apartment project is less than 740 dwelling units, then the project would not have the potential to exceed SMAQMD's recommended mass emission thresholds for NO_x or ROG during operation. The PM₁₀ and PM_{2.5} screening level is 1,485 dwelling units. The proposed project would develop 253 dwelling unit, less than the screening thresholds and project-specific modeling for operational emissions is not required. Therefore, impacts related to project long-term operational emissions of ROG, NO_x, PM₁₀, and PM_{2.5}, would be less than significant.

Impact Conclusion

The project would not result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment, and the impact would be less than significant.

Question c: Less than Significant Impact. CARB and the Office of Environmental Health Hazard Assessment (OEHHA) have identified the following groups of individuals as the most likely to be affected by air pollution: the elderly over 65, children under 14, infants (including in utero in the third trimester of pregnancy), and persons with cardiovascular and chronic respiratory diseases such as asthma, emphysema, and bronchitis (CARB 2005, OEHHA 2015). Some land uses are considered more sensitive to air pollution than others due to the types of population groups or activities involved and are referred to as sensitive receptor locations. Examples of these sensitive receptor locations are residences, schools, hospitals, and daycare centers.

The closest existing sensitive receptor sites to the project site are multi-family senior housing buildings approximately 70 feet west of Lot 1, and single-family residences approximately 150 feet north (across Iron Point Road) of Lot 6. The closest school to the project site is the Gold Ridge Elementary School approximately 1,700 feet (0.32 mile) north of the project site. There are no hospitals or daycare centers located within 0.5-mile of the project site.

Implementation of the project would result in the use of heavy-duty construction equipment, haul trucks, and construction worker vehicles. These vehicles and equipment would generate the TAC DPM. Generation of DPM from construction projects typically occurs in a localized area (e.g., at the project site) for a short period of time. Because construction activities and subsequent emissions vary depending on the construction activity (e.g., grading, building construction), the construction-related emissions to which nearby receptors are exposed to would also vary throughout the construction period. During some equipment-intensive activities such as grading and excavation, construction-related emissions would be higher than other less equipment-intensive activities such as building construction.

The dose (of TAC) to which receptors are exposed is the primary factor used to determine health risk. Dose is a function of the concentration of a substance in the environment and the extent of exposure a person has with the substance; a longer exposure period to a fixed quantity of emissions would result in higher health risks. Current models and methodologies for conducting cancer health risk assessments are associated with longer-term exposure periods (typically 30 years for individual residents based on guidance from OEHHA) and are best suited for evaluation of long duration TAC emissions with predictable schedules and locations. These assessment models and methodologies do not correlate well with the temporary and highly variable nature of construction activities. Cancer potency factors are based on animal lifetime studies or worker studies where there is long-term exposure to the carcinogenic agent. There is considerable uncertainty in trying to evaluate the cancer risk from projects

that will only last a small fraction of a lifetime (OEHHA 2015). In addition, concentrations of mobile source DPM emissions disperse rapidly and are typically reduced by 70 percent at approximately 500-feet (CARB 2005). Considering this information, the highly dispersive nature of DPM, and the fact that construction activities would occur at various locations throughout the project site, it is not anticipated that construction of the project would expose sensitive receptors to substantial DPM concentrations.

According to the SMAQMD, land use development projects do not typically have the potential to result in localized concentrations of criteria air pollutants that expose sensitive receptors to substantial pollutant concentrations. This is because criteria air pollutants are predominantly generated in the form of mobile-source exhaust from vehicle trips associated with the land use development project. These vehicle trips occur throughout a paved network of roads, and, therefore, associated exhaust emissions of criteria air pollutants are not generated in a single location where high concentrations could be formed (SMAQMD 2020a). Therefore, localized concentration of CO from exhaust emissions, or “CO hotspots,” would only be a concern on high-volume roadways where vertical and/or horizontal mixing is substantially limited, such as tunnels or below grade highways. There are no high-volume roadways in the region with limited mixing that would be affected by project generated traffic. Once operational, the project would not be a significant source of TACs. Therefore, the project would not expose sensitive receptors to substantial pollutant concentrations, and the impact would be less than significant.

The project would site new sensitive receptors within 1,000-feet of US Highway 50. High volume roads (roads that carry 100,000 or more vehicles per days) are considered substantial sources of TACs, including DPM and other TACs contained in vehicle exhaust Total Organic Gases (TOG) emissions, including benzene, ethylbenzene, and formaldehyde. The SMAQMD does not consider the health risk to sensitive receptors sited by a land use development project from high volume roadways to be a CEQA analysis requirement in accordance with the 2015 California Supreme Court decision in the case of California Building Industry Association v. Bay Area Air Quality Management District (SMAQMD 2019). The SMAQMD recommends that lead agencies use their Mobile Sources Air Toxics Protocol to evaluate the potential increased health risks to receptors near high-volume roadways (SMAQMD 2020b). The increased health risks to future project residents were evaluated using the guidance and tools in the Mobile Sources Air Toxics Protocol and were found to be potentially significant. To reduce health risk associated with concentrations of TACs along US Highway 50, it is recommended that the project be conditioned to require the installation of heating, ventilation, and air conditioning (HVAC) systems equipped with filters having a minimum efficiency reporting value (MERV) of 13 or better. A letter summarizing the methodology, results, and risk reduction recommendations from the Mobile Sources Air Toxics Protocol analysis is included in **Appendix B**.

Question d: Less than Significant Impact with Mitigation. The project is located in proximity to US Highway 50; Lot 1 located approximately 90-feet from the nearest travel lane and Lot 6 is located approximately 370-feet from the nearest travel lanes. The increase in health risks to future project residents resulting from proximity to US Highway 50 was estimated using the SMAQMD’s Mobile Sources Air Toxics Protocol (MSAT Protocol).

Using the MSAT Protocol Mapping Tool, the project Lot 1 apartments are in an area with increased cancer risks ranging from 19 in 1 million to 32 in 1 million, and PM_{2.5} concentrations ranging from 0.49 µg/m³ to 0.91 µg/m³. Lot 6 has cancer risk ranging from 30 in 1 million to 47 in 1 million and PM_{2.5} concentrations ranging from 0.8 µg/m³ to 1.3 µg/m³. Note: Lot 6 has higher cancer risks even though it is further from US Highway 50. This result is likely due to the terrain—Lot 6 is close to the same elevation as the freeway and Lot 1 is elevated 30 to 40 feet above the freeway. The cancer risk increase would

exceed both the Bay Area Air Quality Management District's (BAAQMD) threshold of 10 in 1 million and the San Joaquin Air Pollution Control District's (SJCAPCD) threshold of 20 in 1 million. PM_{2.5} concentrations would exceed the BAAQMD's threshold of 08. $\mu\text{g}/\text{m}^3$. Therefore, the increase health risk to future project residents would be potentially significant. Accordingly, the proposed project shall be conditioned with the following health risk reduction measure:

Mitigation Measure AIR-1: Mechanical Ventilation System

- The building design shall include a mechanical ventilation system that meets the criteria of the International Building Code (Chapter 12, §1203.2 of the California Building Code) to ensure that windows would be able to remain closed while maintaining adequate ventilation and temperature control. The mechanical ventilation system shall be designed to accommodate, and equipped with, filters having a Minimum Efficiency Reporting Value (MERV) rating of 13 or higher.

Implementation of Mitigation Measure AIR-1 would reduce the potential impacts associated with elevated health risk due to the project's proximity to US Highway 50 to below a level of significance.

Question e: Less than Significant Impact. Odors associated with diesel exhaust and ROG from application of asphalt and architectural coatings would be emitted during project construction. The odor of these emissions is objectionable to some; however, emissions would disperse rapidly from the project site and therefore should not be at a level that would affect a substantial number of people. Further, construction activities would be temporary. As a result, impacts associated with temporary odors during construction are not considered significant.

As a residential development, operation of the project would not result in odors affecting a substantial number of people. Solid waste generated by the project would be collected by a contracted waste hauler, ensuring that any odors resulting from on-site waste would be managed and collected in a manner to prevent the proliferation of odors. The project would not result in other emissions (such as those leading to odors) adversely affecting a substantial number of people, and the impact would be less than significant.

IV. BIOLOGICAL RESOURCES

BIOLOGICAL RESOURCES:				
Would the project:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Wildlife or US Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Biological resource evaluations prepared for the proposed project have been incorporated by reference and are presented in their entirety in **Appendix C**.

Environmental Setting

The area in which the project is located is characterized by suburban residential development, commercial business centers, transportation, and small pockets of open space. US Highway 50 is immediately south of the project site. Lands in the City of Folsom surrounding the project site that lie north of US Highway 50 are largely developed with commercial and residential development, while lands across US Highway 50 to the south of the project site remain largely in open space (primarily used for cattle grazing), although development is occurring in the City of Folsom south of US Highway 50 and to the east of the project site.

Lot 1 shows no alteration in the use or condition of the property dating back to 1952 (NETR 2021). Lot 1 slopes downward from east to west with elevations ranging from 371 feet amsl in the east to 317 feet amsl in the west. Lot 1 is predominantly comprised of non-native annual grassland with a single oak tree in the southeast of the parcel. Lot 1 features a small parking lot in the northwest corner of the parcel, and a small sidewalk with minor landscaping elements connecting the parking lot to the rest of the parcel where the Kaiser Permanente Medical Offices are located. The rest of the site is vacant.

Lot 6 is dominated by ruderal/disturbed habitat, with a small stand of native oak trees (*Quercus* sp.) in the southwest corner of the parcel. The project site is not associated with any current land use; however, historic aerial imagery shows that Lot 6 was partially graded and used to store materials and debris in 2009 during the construction of the adjacent Folsom Corporate Center and much of that debris has remained on site. Lot 6 slopes down towards the east through a series of partially graded terraces, with elevations ranging from 370 feet amsl to the west and 358 feet amsl to the east.

Regulatory Framework Related to Biological Resources

Federal Regulations

Federal Endangered Species Act

The U.S. Fish and Wildlife Service (USFWS) enforces the provisions stipulated within the Federal Endangered Species Act of 1973 (FESA; 16 USC 1531 et seq.). Species identified as federally threatened or endangered (50 CFR 17.11, and 17.12) are protected from take, defined as direct or indirect harm, unless a Section 10 permit is granted to an entity other than a federal agency or a Biological Opinion with incidental take provisions is rendered to a federal lead agency via a Section 7 consultation. Pursuant to the requirements of FESA, an agency reviewing a proposed project within its jurisdiction must determine whether any federally listed species may be present in the project site and determine whether the proposed project will jeopardize the continued existence of or result in the destruction or adverse modification of critical habitat of such species (16 USC 1536 (a)[3], [4]). Other federal agencies designate species of concern (species that have the potential to become listed), which are evaluated during environmental review under the National Environmental Protection Act (NEPA) or CEQA although they are not otherwise protected under FESA.

Migratory Bird Treaty Act

The Migratory Bird Treaty Act (MBTA) of 1918 established federal responsibilities for the protection of nearly all species of birds, their eggs, and nests. The Migratory Bird Treaty Reform Act of 2004 further defined species protected under the act and excluded all non-native species. Section 16 U.S.C. 703–712 of the Act states “unless and except as permitted by regulations, it shall be unlawful at any time, by any means or in any manner, to pursue, hunt, take, capture, kill, attempt to take, capture, or kill” a migratory bird. A migratory bird is any species or family of birds that live, reproduce or migrate within or across international borders at some point during their annual life cycle. Currently, there are 836 migratory birds protected nationwide by the Migratory Bird Treaty Act, of which 58 are legal to hunt. The U.S. Court of Appeals for the 9th Circuit (with jurisdiction over California) has ruled that the MBTA does not prohibit incidental take (952 F 2d 297 – Court of Appeals, 9th Circuit 1991).

State JurisdictionCalifornia Endangered Species Act

The California Endangered Species Act (CESA) (California Fish and Game Code Sections 2050 to 2097) is similar to the FESA. The California Fish and Wildlife Commission is responsible for maintaining lists of threatened and endangered species under CESA. CESA prohibits the take of listed and candidate (petitioned to be listed) species. "Take" under California law means to hunt, pursue, catch, capture, or kill, or attempt to hunt, pursue, catch capture, or kill (California Fish and Game Code, Section 86). The California Department of Fish and Wildlife (CDFW) can authorize take of a state-listed species under Section 2081 of the California Fish and Game Code if the take is incidental to an otherwise lawful activity, the impacts are minimized and fully mitigated, funding is ensured to implement and monitor mitigation measures, and CDFW determines that issuance would not jeopardize the continued existence of the species. A CESA permit must be obtained if a project will result in the "take" of listed species, either during construction or over the life of the project. For species listed under both FESA and CESA requiring a Biological Opinion under Section 7 of the FESA, CDFW may also authorize impacts to CESA species by issuing a Consistency Determination under Section 2080.1 of the Fish and Game Code.

California Code of Regulations Title 14 and California Fish and Game Code

The official listing of endangered and threatened animals and plants is contained in the California Code of Regulations Title 14 §670.5. A state candidate species is one that the California Fish and Game Code has formally noticed as being under review by CDFW to include in the state list pursuant to Sections 2074.2 and 2075.5 of the California Fish and Game Code.

Legal protection is also provided for wildlife species in California that are identified as "fully protected animals." These species are protected under Sections 3511 (birds), 4700 (mammals), 5050 (reptiles and amphibians), and 5515 (fish) of the California Fish and Game Code. These statutes prohibit take or possession of fully protected species at any time. CDFW is unable to authorize incidental take of fully protected species unless any such take authorization is issued in conjunction with the approval of a Natural Community Conservation Plan that covers the fully protected species (California Fish and Game Code Section 2835).

California Environmental Quality Act

Under the California Environmental Quality Act of 1970 (CEQA; Public Resources Code Section 21000 et seq.), lead agencies analyze whether projects would have a substantial adverse effect on a candidate, sensitive, or special-status species (Public Resources Code Section 21001(c)). These "special-status" species generally include those listed under FESA and CESA, and species that are not currently protected by statute or regulation, but would be considered rare, threatened, or endangered under the criteria included CEQA Guidelines Section 15380. Therefore, species that are considered rare are addressed under CEQA regardless of whether they are afforded protection through any other statute or regulation. The California Native Plant Society (CNPS) inventories the native flora of California and ranks species according to rarity; plants ranked as 1A, 1B, 2A, 2B, and 3 are generally considered special-status species under CEQA.

Although threatened and endangered species are protected by specific federal and state statutes, CEQA Guidelines Section 15380(d) provides that a species not listed on the federal or state list of protected species may be considered rare if it can be shown to meet certain specified criteria. These criteria have

been modeled after the definition in FESA and the section of the California Fish and Game Code dealing with rare or endangered plants and animals. Section 15380(d) allows a public agency to undertake a review to determine if a significant effect on species that have not yet been listed by either the USFWS or CDFW (i.e., candidate species) would occur.

Native Plant Protection Act

The California Native Plant Protection Act of 1977 (California Fish and Game Code Sections 1900-1913) empowers the Fish and Game Commission to list native plant species, subspecies, or varieties as endangered or rare following a public hearing. To the extent that the location of such plants is known, CDFW must notify property owners that a listed plant is known to occur on their property. Where a property owner has been so notified by CDFW, the owner must notify CDFW at least 10 days in advance of any change in land use (other than changing from one agricultural use to another), in order that CDFW may salvage listed plants that would otherwise be destroyed. Currently, 64 taxa of native plants have been listed as rare under the act.

Nesting Birds

California Fish and Game Code Subsections 3503 and 3800 prohibit the possession, take, or needless destruction of birds, their nests, and eggs, and the salvage of dead nongame birds. California Fish and Game Code Subsection 3503.5 protects all birds in the orders of Falconiformes and Strigiformes (birds of prey). Fish and Game Code Subsection 3513 states that it is unlawful to take or possess any migratory nongame bird as designated in the Migratory Bird Treaty Act or any part of such migratory nongame bird except as provided by rules and regulations adopted by the Secretary of the Interior under provisions of the Migratory Bird Treaty Act. The Attorney General of California has released an opinion that the Fish and Game Code prohibits incidental take.

Jurisdictional Waters

Federal Jurisdiction

Unless considered an exempt activity under Section 404(f) of the Federal Clean Water Act, any person, firm, or agency planning to alter or work in "waters of the U.S.," including the discharge of dredged or fill material, must first obtain authorization from the USACE under Section 404 of the Clean Water Act (CWA; 33 USC 1344). Permits, licenses, variances, or similar authorization may also be required by other federal, state, and local statutes. Section 10 of the Rivers and Harbors Act prohibits the obstruction or alteration of navigable waters of the U.S. without a permit from USACE (33 USC 403). Activities exempted under Section 404(f) are not exempted within navigable waters under Section 10.

"Waters of the U.S." are defined as: "All waters that are currently used, or were used in the past, or may be susceptible to use in interstate or foreign commerce, including all waters that are subject to the ebb and flow of the tide; all interstate waters including interstate wetlands; all other waters such as intrastate lakes, rivers, streams (including intermittent streams), mudflats, sand flats, wetlands, sloughs, prairie potholes, wet meadows, playa lakes or natural ponds, the use, degradation, or destruction of which could affect interstate commerce; impoundments of these waters; tributaries of these waters; the territorial sea; or wetlands adjacent to these waters (33 Code of Federal Regulations [CFR] Part 328)."

Within non-tidal waters that meet the definition cited above and, in the absence of adjacent wetlands, the indicator used by the USACE to determine the lateral extent of its jurisdiction is the ordinary high

water mark (OHWM) – the line on the shore established by fluctuations of water and indicated by a clear, natural line impressed on the bank, shelving, changes in soil character, destruction of terrestrial vegetation, and/or the presence of litter and debris.

Wetlands are defined under the CFR Part 328.3 as those areas that are inundated or saturated by surface or ground water at a frequency and duration to support, and that under normal circumstances do support, a prevalence of vegetation typically adapted for life in saturated soil conditions.

The USACE has determined that not all features which meet the wetland definition are, in fact, considered to be waters of the U.S. Normally, features not considered as waters of the U.S. include (a) non-tidal drainage and irrigation ditches excavated on dry land; (b) artificially irrigated areas which would revert to upland if the irrigation ceased; (c) artificial lakes or ponds created by excavating and/or diking dry land to collect and retain water and which are used exclusively for such purposes as stock watering, irrigation, settling basins, or rice growing, (d) artificial reflecting or swimming pools or other small ornamental bodies of water created by excavating and/or diking dry land to retain water for primarily aesthetic reasons, and (e) waterfilled depressions created in dry land incidental to construction activity and pits excavated in dry land for the purpose of obtaining fill, sand, or gravel unless and until the construction or excavation operation is abandoned and the resulting body of water meets the definition of waters of the United States (see 33 CFR 328.3(a)). Other features may be excluded based on Supreme Court decisions (e.g., SWANCC and Rapanos) or by regulation.

Federal and state regulations pertaining to waters of the U.S., including wetlands, are discussed below.

Clean Water Act (33 USC 1251-1376). The CWA provides guidance for the restoration and maintenance of the chemical, physical, and biological integrity of the nation's waters.

Section 401 requires that an applicant for a federal license or permit that allows activities resulting in a discharge to waters of the U.S. must obtain a state certification that the discharge complies with other provisions of CWA. The Regional Water Quality Control Board (RWQCB) administers the certification program in California and may require State Water Quality Certification before other permits are issued.

Section 402 establishes a permitting system for the discharge of any pollutant (except dredged or fill material) into waters of the U.S.

Section 404 establishes a permit program administered by USACE that regulates the discharge of dredged or fill material into waters of the U.S. (including wetlands). Implementing regulations by USACE are found at 33 CFR Parts 320-332. The Section 404 (b)(1) Guidelines were developed by the USEPA in conjunction with USACE (40 CFR Part 230), allowing the discharge of dredged or fill material for non-water dependent uses into special aquatic sites only if there is no practicable alternative that would have less adverse impacts.

State Jurisdiction

Regional Water Quality Control Board

Any action requiring a CWA Section 404 permit, or a Rivers and Harbors Act Section 10 permit, must also obtain a CWA Section 401 Water Quality Certification. The State of California Water Quality Certification (WQC) Program was formally initiated by the State Water Resources Control Board (SWRCB) in 1990 under the requirements stipulated by Section 401 of the Federal CWA. Although the Clean Water Act is a

Federal law, Section 401 of the CWA recognizes that states have the primary authority and responsibility for setting water quality standards. In California, under Section 401, the State and Regional Water Boards are the authorities that certify that issuance of a federal license or permit does not violate California's water quality standards (i.e., that they do not violate Porter-Cologne and the Water Code). The WQC Program currently issues the WQC for discharges requiring USACE's permits for fill and dredge discharges within Waters of the United States, and now also implements the State's wetland protection and hydromodification regulation program under the Porter Cologne Water Quality Control Act.

On April 2, 2019, the SWRCB adopted a State Wetland Definition and Procedures for Discharges of Dredged or Fill Material to Waters of the State (Procedures), for inclusion in the forthcoming Water Quality Control Plan for Inland Surface Waters and Enclosed Bays and Estuaries and Ocean Waters of California. The Procedures consist of four major elements: 1) a wetland definition; 2) a framework for determining if a feature that meets the wetland definition is a water of the state; 3) wetland delineation procedures; and 4) procedures for the submittal, review and approval of applications for Water Quality Certifications and Waste Discharge Requirements for dredge or fill activities. The Office of Administrative Law approved the Procedures on August 28, 2019, and the Procedures became effective May 28, 2020.

Under the Procedures and the State Water Code (Water Code §13050(e)), "Waters of the State" are defined as "any surface water or groundwater, including saline waters, within the boundaries of the state." Unless excluded by the Procedures, any activity that could result in discharge of dredged or fill material to Waters of the State, which includes Waters of the U.S. and non-federal Waters of the State, requires filing of an application under the Procedures.

The Porter-Cologne Water Quality Control Act (Porter-Cologne Act, Water Code Section 13000 et seq.) is California's statutory authority for the protection of water quality in conjunction with the federal CWA. The Porter-Cologne Act requires the SWRCB and RWQCBs under the CWA to adopt and periodically update water quality control plans, or basin plans. Basin plans are plans in which beneficial uses, water quality objectives, and implementation programs are established for each of the nine regions in California. The Porter-Cologne Act also requires dischargers of pollutants or dredged or fill material to notify the RWQCBs of such activities by filing Reports of Waste Discharge and authorizes the SWRCB and RWQCBs to issue and enforce waste discharge requirements, National Pollution Discharge Elimination System (NPDES) permits, Section 401 water quality certifications, or other approvals.

California Department of Fish and Wildlife

The CDFW is a trustee agency that has jurisdiction under Section 1600 et seq. of the California Fish and Game Code. Under Sections 1602 and 1603, a private party must notify CDFW if a proposed project will "substantially divert or obstruct the natural flow or substantially change the bed, channel, or bank of streambeds...except when the department has been notified pursuant to Section 1601." Additionally, CDFW asserts jurisdiction over native riparian habitat adjacent to aquatic features, including native trees over four inches in diameter at breast height (DBH). If an existing fish or wildlife resource may be substantially adversely affected by the activity, CDFW may propose reasonable measures that will allow protection of those resources. If these measures are agreeable to the parties involved, they may enter into an agreement with CDFW identifying the approved activities and associated mitigation measures. Generally, CDFW recommends submitting an application for a Streambed Alteration Agreement (SAA) for any work done within the lateral limit of water flow or the edge of riparian vegetation, whichever is greater.

Local Regulations

City of Folsom Tree Preservation Ordinance

Chapter 12.16 of the Folsom Municipal Code, the Tree Preservation Ordinance, regulates the cutting or modification of trees, including oaks and specified other trees; requires a Tree Permit prior to cutting or modification; and establishes mitigation requirements for cut or damaged trees. The Tree Preservation Ordinance establishes policies, regulations, and standards necessary to ensure that the City will continue to preserve and maintain its “urban forests”. Anyone who wishes to perform “Regulated Activities” on “Protected Trees” must apply for a permit with the City. Regulated activities include:

- Removal of a Protected Tree;
- Pruning/trimming of a Protected Tree; and/or,
- Grading or trenching within the Protected zone.

Protected trees include:

- Native oak trees with a diameter at standard height (DSH; 4.5 feet above ground level) of 6 inches or larger for single trunk trees or 20 inches or larger combined diameter of native oak multi-trunk trees. Native oak species include:
 - valley oak (*Quercus lobata*)
 - blue oak (*Quercus douglasii*)
 - interior live oak (*Quercus wislizenii*)
 - coast live oak (*Quercus agrifolia*)
- Heritage oak trees - native oaks with a trunk DSH of 19 inches or greater and native oaks with a multi-trunk diameter of 38 inches or greater;
- Landmark trees identified individually by the City Council through resolution as being a significant community benefit; and/or,
- Street trees within the tree maintenance strip.

Methods

Information used in preparation of this Initial Study comes from the following sources:

- Desktop review of regionally occurring special-status species and habitats with potential to occur in the project site and/or be affected by the proposed project;
- Biological reconnaissance survey performed by HELIX biologists in October 2021;
- Biological Review for Iron Point Road Apartments Development, prepared by SCS Engineers, dated February 25, 2021; and,
- Arborist Report – Iron Point Road Apartments, Folsom CA, prepared by Arborwell Professional Tree Management, dated December 29, 2020;
- Arborist inventory of remaining trees performed by HELIX biologist/arborist in November 2021.

For the purposes of this report, special-status species are those that fall into one or more of the following categories, including those:

- listed as endangered or threatened under the Federal Endangered Species Act (FESA; including candidates and species proposed for listing);
- listed as endangered or threatened under the California Endangered Species Act (CESA; including candidates and species proposed for listing);
- designated as rare, protected, or fully protected pursuant to California Fish and Game Code;
- designated a Species of Special Concern (SSC) by the California Department of Fish and Wildlife (CDFW);
- considered by CDFW to be a Watch List species with potential to become an SSC;
- defined as rare or endangered under Section 15380 of the California Environmental Quality Act (CEQA); or,
- Having a California Rare Plant Rank (CRPR) of 1A, 1B, 2A, 2B, or 3.

In order to evaluate special-status species and/or their habitats with the potential to occur on the project site and/or be impacted by the proposed project, HELIX obtained lists of special-status species known to occur and/or having the potential to occur in the project site and vicinity from the U.S. Fish and Wildlife Service (USFWS; USFWS 2021), the California Native Plant Society (CNPS; CNPS 2021), and the California Natural Diversity Database (CNDDDB; CDFW 2021). The results of the biological database and records searches for the project site, as well as a list of species observed during the biological reconnaissance, are compiled in **Appendix C**.

Biological Reconnaissance Survey

A biological reconnaissance survey was conducted on October 13, 2021, by HELIX biologists Stephen Stringer, M.S. International Society of Arboriculture (ISA) Certified Arborist (WE-7129A) and Stephanie McLaughlin, M.S., ISA Certified Arborist (WE-12922A) between 1230 and 1430 hours. The biological reconnaissance survey was accomplished by walking meandering transects through the project site in order to obtain 100 percent visual coverage of the site. Habitats present in the project site were classified based on the dominant plant species present and identifiable at the time of the survey. The project site was also reviewed for aquatic features exhibiting characteristics of waters of the U.S. or State, including the presence of hydrophytic vegetation, hydric soil, wetland hydrology, bed and bank, or depressional topography. Following the field survey, the potential for each species identified in the database query to occur within the project site was determined based on the site survey, soils, habitats present within the project site, and species-specific information, as shown in **Appendix C**.

Arborist Inventory

The Arborist Report prepared by Arborwell Professional Tree Management and dated December 29, 2020, inventoried a majority of the trees in the project site but did not include the trees located in the landscaped strip in the southeastern corner of Lot 6.

HELIX Biologist and International Society of Arboriculture certified arborist Stephanie McLaughlin (ISA # WE-12922A) surveyed the additional trees in the southeastern corner of Lot 6 on November 2, 2021. The following data were collected for all native and non-native oak trees with a DSH of six inches or greater on the site: species, trunk diameter at 4.5-feet above the ground (DSH), dripline radius, estimated height, and overall health and structure of the tree. Overall condition was rated on a five-point scale of 0 (dead), 1 (severe decline), 2 (declining), 3 (fair), 4 (good), or 5 (excellent). Comments such as number of trunks, irregularities, scars or other growth characteristics or vigor indicators were recorded for each tree. Recommendations for preservation or removal were made based on each tree's condition. The location of each tree was recorded using an EOS Systems Arrow 100 Global Navigation Satellite System receiver with sub-meter accuracy. Trees on the site were identified in the field with pre-printed numbered tags.

Habitat Types/Vegetation Communities

Habitat types/vegetation communities on the project site include blue oak woodland, non-native annual grassland, ruderal/disturbed, and developed. Habitats and land covers are depicted on **Figure 5 in Appendix A**.

Non-Native Annual Grassland

Non-native annual grasslands are open grasslands composed primarily of annual species. Germination follows the onset of winter rains; however, growth is slow during cold weather and plants remain low in stature until spring. Grasses flower and set seed by early summer, and large amounts of standing dead thatch are present by mid-summer in the absence of grazing.

The non-native annual grassland in the project site is found on Lot 1 and is dominated by ripgut brome (*Bromus diandrus*), soft brome (*Bromus hordeaceus*), prickly lettuce (*Lactuca serriola*), and yellow-star thistle (*Centaurea solstitialis*). The majority of the species observed were non-native; however, native species on the site include doveweed (*Croton setiger*) and yellowflower tarweed (*Holocarpha virgata*). The non-native annual grassland habitat on Lot 1 is in a somewhat disturbed condition. The contours of the parcel show a history of grading and fill, with tire ruts and depressions scattered throughout the site. The project site includes 6.95-acres of non-native annual grassland, all of which is found on Lot 1.

Blue Oak Woodland

Blue oak woodland is composed of a pronounced hardwood tree layer, with a poorly developed shrub stratum, and a sparse, grassy herbaceous layer. The canopy is entirely dominated by blue oak (*Quercus douglasii*). The herbaceous layer of this community consists of similar species to what was observed in the annual grassland habitat, such as ripgut brome, prickly lettuce, and yellow-star thistle. Blue oak woodland habitat comprises 0.62-acres of the project site, all of which is found in the southwest corner of Lot 6.

Ruderal/Disturbed

Ruderal/disturbed habitat occurs in areas that are heavily disturbed by past or ongoing human activities but retain a soil substrate. Ruderal/disturbed areas may be sparsely to densely vegetated, but do not support a recognizable community or species assemblage. Vegetative cover is usually herbaceous and dominated by a wide variety of weedy non-native species or a few ruderal native species.

Ruderal/disturbed habitat, which totals 3.61-acres, comprises much of Lot 6. This habitat on the project site is dominated by a dense cover of non-native annual grasses, with small patches of native and non-native grasses and forbs and is heavily disturbed. Riggut brome, yellow-star thistle, yellowflower tarweed, and medusa head (*Elymus caput-medusae*) make up the majority of the herbaceous cover on the project site in terms of percent cover. Nearly all herbaceous plant species observed during the biological reconnaissance are non-natives associated with disturbance; however, native plants observed include coyote brush (*Baccharis pilularis*) and deer grass (*Muhlenbergia rigens*). A small sliver of landscaping borders the eastern edge of Lot 6, it consists of ornamental scrub species as well as several valley oaks (*Quercus lobata*) and cork oaks (*Quercus suber*).

The contours of Lot 6 reflect a history of fill, grading, and other modifications resulting in tire ruts, graded areas, and depressions. There are several large debris piles consisting of rock and rebar in the center of Lot 6. Stormwater from the developed areas in the surrounding business park is discharged into a small, graded depression within the ruderal/disturbed habitat on the east end of Lot 6 through a culvert outfall that enters the site from under the parking lot to the south. The graded depression and culvert outfall appears to have been constructed as part of the stormwater management system for the Folsom Corporate Center. The graded depression contains some wetland plants typical of disturbed areas but is not considered a potential waters of the U.S. or State because it was constructed on a graded pad in uplands for the purposes of managing stormwater drainage.

Developed

Developed areas on the project site includes parking lots and roadways surrounding both parcels. A paved arterial roadway runs along the eastern and southern borders of Lot 6. Developed land near Lot 1 consists of a paved roadway and a portion of a parking lot along the parcels northern border. Developed habitat in the project site is asphalt paved and completely devoid of vegetative cover. This habitat type comprises 0.86-acres of the project site.

Wildlife

In general, wildlife use of the site is expected to be limited to common disturbance-tolerant species adapted to living in urban and suburban areas in close proximity to humans. Species observed using the habitats in the project site included mourning dove (*Zenaida macroura*), acorn woodpecker (*Melanerpes formicivorus*), northern flicker (*Colaptes aurata*), black-tailed jackrabbit (*Lepus californicus*), and house finch (*Carpodacus mexicanus*).

Special-Status Species with Potential to Occur

A total of 22 regionally occurring special-status plant species and 31 regionally occurring special-status wildlife species were identified during the database queries and desktop review and are evaluated in **Appendix C**.

Special-Status Plant Species

A total of 22 regionally occurring special-status plant species were identified during the database queries and desktop review. The majority of the special-status plant species are associated with aquatic habitats, including vernal pools. The remaining species are associated with grasslands, chaparral,

cismontane woodlands, coniferous forests, and alkaline habitat, or have specific requirements for lone, gabbroic, serpentinite, or volcanic soils that were not found in the project site.

There is currently no suitable habitat for special-status plant species in the project site and there have been no reported occurrences of special-status plant species on or adjacent to the project site in the CNDDB. Special-status plant species are not expected to occur in the project site or be impacted by the proposed project.

Special-Status Animal Species

A total of 31 regionally occurring special-status wildlife species were identified during the database searches and desktop review. The majority of the special-status wildlife species are associated with aquatic habitats of the adjacent Sacramento Valley such as rivers, sloughs, and freshwater wetlands, including vernal pools. The remaining species are associated with open areas, grasslands, coniferous forests, and cliff habitat, or have specific food species requirements that were not found on the project site.

No special-status wildlife species were observed in the project site during the biological reconnaissance survey and there are no reported occurrences in the CNDDB of special-status animal species in or adjacent to the project site. Based on the evaluation of regionally occurring special-status species documented in **Appendix C**, the project site provides marginal habitat for burrowing owl (*Athene cunicularia*) and white-tailed kite (*Elanus leucurus*) as well as habitat for other nesting raptors and migratory birds. These species are discussed briefly below. There is no suitable habitat in the project site for the remainder of the regionally occurring special-status species evaluated. Species determined to have no potential to occur in the project site or be impacted by the proposed project are not discussed further in this report.

Burrowing Owl

Burrowing owls are year-round residents of most parts of California, though local seasonal movements are common and populations in northeastern California and high elevations may migrate to lower elevations during the winter. Burrowing owls inhabit underground burrows, especially those of California ground squirrels (*Otospermophilus beecheyi*), and artificial holes such as pipes, culverts, and crevices in debris piles. Suitable habitat is open and relatively flat, with short vegetation, low perches or mounds, and abundant rodent and insect prey. Common examples of suitable habitat include agricultural fields, pastures, grasslands, deserts, and disturbed places. The breeding season for burrowing owl is April through August (CDFW 2012).

No burrowing owls or sign were observed during the biological reconnaissance, which included a thorough search for this species. However, there are three reported occurrences of burrowing owl in the CNDDB within 2.5-miles of the project site. These occurrences are generally located to the southeast in annual grassland habitat across US Highway 50 (CDFW 2021).

The non-native annual grassland and ruderal/disturbed habitat in the project site provides marginally suitable habitat for burrowing owl. There are several debris piles and small mammal burrows that provide elements of suitable habitat. The project site is too small in size to support significant burrowing owl foraging and is surrounded by disturbed industrial and residential parcels. The high levels of human

presence and disturbance in the project site likely discourage occupation of the project site by burrowing owls; however, there is a low potential for this species to occur in the project site.

If burrowing owls are residing in the project site or on adjacent properties, the project would have potential for adverse effects through injury or mortality, displacement, and loss of habitat. Injury or mortality to individual adults and young, or mortality of eggs and chicks due to forced nest abandonment by adults, would be a violation of the Fish and Game Code and a significant impact. Loss of occupied habitat including nesting burrows, satellite burrows, foraging habitat, dispersal habitat, wintering habitat, and linkages is considered a potentially significant impact to the local and regional populations of burrowing owl (CDFW 2012).

The recommended mitigation measures for nesting burrowing owl in the following section would reduce potential impacts to this species to less than significant.

White-tailed Kite

White-tailed kite is a year-round resident in coastal and valley lowlands, where it inhabits herbaceous and open stages of most habitat types. Individuals forage in grasslands, farmlands, and wetlands, preying mostly on small diurnal mammals. Nests are built near the top of dense tree stands, usually near open foraging areas (Zeiner et al. 1988).

No white-tailed kites were observed during the biological reconnaissance survey conducted for the proposed project. The nearest documented occurrence of white-tailed kite is 2.2-miles south in the City of Folsom (CDFW 2021).

The blue oak woodland habitat on and adjacent to the project site provides potential nesting habitat and the small patches of undeveloped grassland habitat in the vicinity provide suitable foraging habitat. This species is known to nest in tall trees in urban areas and forage in small habitat patches.

No adverse effects to white-tailed kite foraging are anticipated as a result of the loss of ruderal/disturbed habitat that would occur due to development of the proposed project. Non-breeding adults could readily avoid contact with construction equipment or personnel by moving out of the construction area. Displacement of non-breeding adults would not be a significant impact. The project has potential for adverse effects to white-tailed kite through nest disturbance leading to destruction of eggs or nestlings if this species were to nest in or adjacent to the project site. Eggs and young still dependent on the nest would be susceptible to injury or mortality through physical contact or through nest abandonment caused by displacement of adults. Destruction of eggs or young would be a violation of the Fish and Game Code and a significant impact.

The recommended mitigation measures for nesting migratory birds and raptors in the following section would reduce potential impacts to this species to less than significant.

Migratory Birds and Nesting Birds

As noted in the Regulatory Framework section, migratory and non-game birds are protected during the nesting season by California Fish and Game Code. The project site and immediate vicinity provides nesting and foraging habitat for a variety of native birds common to urbanized areas, such as mourning dove (*Zenaid macroura*), house finch (*Haemorrhous mexicanus*), and acorn woodpecker (*Melanerpes*

formicivorus). Nests were not observed during surveys; however, a variety of migratory birds have the potential to nest in and adjacent to the project site, in trees, shrubs and on the ground in vegetation.

Project activities such as clearing and grubbing during the avian breeding season (February 1 through August 31) could result in injury or mortality of eggs and chicks directly through destruction or indirectly through forced nest abandonment due to noise and other disturbance. Needless destruction of nests, eggs, and chicks would be a violation of the Fish and Game Code and a significant impact.

The recommended mitigation measures for nesting migratory birds and raptors in the following section would reduce potential impacts to nesting migratory birds and raptors to less than significant.

Protected Trees

Data in this section is from an Arborist Report prepared by Arborwell Professional Tree Management in December 2020 and an arborist inventory conducted by HEL in November 2021. There are a total of 14 trees found on the project site; one tree (#702) is on Lot 1 and the remaining trees are on Lot 6. Nine of the trees are blue oaks, three are cork oaks, and two are valley oaks. The majority of trees are in excellent to fair condition and one tree (#705) is in critical/poor condition. **Table 7** shows the details of all trees in the project site.

Table 7. Tree Inventory Details¹

Tree #	Species	DSH (inches)	Condition	Notes
702*	Blue Oak <i>Quercus douglasii</i>	41.1	4 - Good	Good shape, 2 Limb failures on southern side of tree, good structure. Appears to have minimal deadwood in lower part of canopy. May need to be raised up per plans for clearance.
703*	Blue Oak <i>Quercus douglasii</i>	30.4	3 - Fair	Appears to have minimal deadwood and good attachment at 6' high on trunk with 4 large limbs of attachment.
704*	Blue Oak <i>Quercus douglasii</i>	26.7	3 - Fair	Appears to have minimal deadwood, codominant at 6' with signs of included bark and V shaped crotch.
705*	Blue Oak <i>Quercus douglasii</i>	20	1 – Critical/Poor	Tree has poor structure with limb failure and is in severe decline.
706*	Blue Oak <i>Quercus douglasii</i>	19.4, 15.7	3 - Fair	Appears to have minimal deadwood, two trees at base, one to northwest is being overcrown by one to southeast.
707*	Blue Oak <i>Quercus douglasii</i>	23.1	3 - Fair	Appears to have minimal deadwood, co-dominant leader at 6' with V shaped crotch.
708*	Blue Oak <i>Quercus douglasii</i>	23.1	3 - Fair	Appears to have minimal deadwood.
709*	Blue Oak <i>Quercus douglasii</i>	20.7	3 - Fair	Appears to have minimal deadwood. Large limb near base of trees has visual signs of included bark.
710*	Blue Oak	11.4, 13.3	3 - Fair	Appears to have minimal deadwood, poor structure with co-dominant leaders at base.

	<i>Quercus douglasii</i>			
256**	Cork Oak <i>Quercus suber</i>	11.7	5- Excellent	
329**	Cork Oak <i>Quercus suber</i>	16	5-Excellent	
330**	Cork Oak <i>Quercus suber</i>	13.5	4 - Good	Co-dominant leaders
331**	Valley Oak <i>Quercus lobata</i>	8.4	4 - Good	Minor lean
332**	Valley Oak <i>Quercus lobata</i>	9.6	5-Excellent	Evidence of pruning

*Data from Arborist Report – Iron Point Road Apartments, Folsom CA, prepared by Arborwell Professional Tree Management, dated December 29, 2020.

** Data collected by HELIX November 2021.

‡Bold font indicates that a tree is protected

Eleven of the 14 trees in the study area are protected under the City of Folsom Tree Protection Ordinance, as they are native oaks and have a DSH greater than six-inches. Tree # 705 was recommended for removal due to its poor condition. Tree # 702 is considered to be a Heritage tree per City of Folsom and would be preserved on-site as part of the proposed project design. Three of the 14 trees in the study area are not protected (Trees # 256, 329, and 330) as they are not native oak species.

Jurisdictional Waters

There are no potentially jurisdictional waters of the U.S. or waters of the State on the project site. There is a small, constructed depression located on Lot 6 that appears to occasionally hold water. The constructed depression appears to be part of a larger stormwater management system that was constructed to collect runoff from the surrounding buildings, parking areas, and landscaped areas within the Folsom Corporate Center. The constructed depression receives stormwater runoff through a culvert outfall under the parking area/driveway to the south. The graded depression contains some wetland plants typical of disturbed areas but is not considered a potential waters of the U.S. or State because it was constructed on a graded pad in uplands for the purposes of managing stormwater drainage and is part of a currently functioning stormwater management system.

Wildlife Corridors

The project site is primarily surrounded by development with narrow bands of open space separating it from US Highway 50, Iron Point Road, Kaiser Permanente, and an office park. Lands north of Iron Point Road are densely developed, as are lands east of Kaiser Permanente and west of the office park; US Highway 50 is a 6-lane freeway. The project site represents an isolated island of open space with no connectivity to other suitable habitat and does not represent a significant wildlife movement corridor. Use of the site as a wildlife corridor is limited to movement of local wildlife. No native wildlife nursery sites would be affected.

Question a: Less than Significant with Mitigation Incorporated. No regionally occurring special-status plant species were identified as having the potential to occur in the project site, due to lack of suitable habitat. Therefore, impacts to special-status plant species are not anticipated as a result of the proposed project and no mitigation measures are necessary for special-status plants.

The project site provides potential marginal habitat for burrowing owl white-tailed kite and other nesting migratory birds. These species are discussed briefly below. Species determined to have no potential to occur in the project site or be impacted by the proposed project are not discussed further in this report.

Burrowing Owl

In the absence of proposed mitigation measures, potential adverse effects of the proposed project on burrowing owl could include harm to individual burrowing owls, nest disturbance/loss of occupied burrows, and loss of foraging habitat. Pre-construction surveys shall be conducted prior to project implementation to determine if burrowing owl are present on or adjacent to the project site, so that measures could be implemented if needed to avoid harming burrowing owl.

Mitigation Measure BIO-1: Avoid and Minimize Impacts to Burrowing Owl

Prior to the commencement of construction activities (which includes clearing, grubbing, or grading) a survey for burrowing owl shall be conducted by a qualified biologist. The survey shall occur within 30 days of the start of construction activities. Surveys shall be conducted in accordance with the following:

- A survey for active burrows and burrowing owls shall be conducted by walking through suitable habitat over the entire project site and in areas within 150-meters (~500-feet) of the project impact zone where accessible.
- Pedestrian survey transects shall be spaced to allow 100 percent visual coverage of the ground surface. The distance between transect center lines shall be no more than 30-meters (~100-feet) and shall be reduced to account for differences in terrain, vegetation density, and ground surface visibility. Surveyor(s) shall maintain a minimum distance of 50-meters (~160-feet) from any owls or occupied burrows. It is important to minimize disturbance near occupied burrows during all seasons.
- If no occupied burrows or burrowing owls are found in the survey area, a letter report documenting survey methods and findings shall be prepared and no further mitigation is necessary.
- If occupied burrows or burrowing owls are found, then a complete burrowing owl survey is required. This consists of a minimum of four site visits conducted on four separate days, which must also be consistent with the Survey Method, Weather Conditions, and Time of Day sections of Appendix D of the California Fish and Wildlife *"Staff Report on Burrowing Owl Mitigation"* (March 2012). A survey report shall be prepared that is consistent with the Survey Report section of Appendix D of the California Fish and Wildlife *"Staff Report on Burrowing Owl Mitigation"* (March 2012).
- If occupied burrows or burrowing owls are found, the applicant shall contact the City and consult with CDFW prior to construction and will be required to submit a Burrowing Owl Mitigation Plan (subject to the approval of the City and in consultation with California Fish and Wildlife). This plan must document all proposed measures, including avoidance, minimization, exclusion, relocation, or other measures, and include a plan to monitor mitigation success. The

CDFW "*Staff Report on Burrowing Owl Mitigation*" (March 2012) shall be used in the development of the mitigation plan.

White-tailed Kite, Other Raptors, and Migratory Birds

The project site provides suitable nesting habitat for native songbirds and large trees on and adjacent to the project site provide nesting habitat for white-tailed kite and other raptors. Removal of vegetation containing active nests would potentially result in destruction of eggs and/or chicks; noise, dust, and other anthropogenic stressors in the vicinity of an active nest could lead to forced nest abandonment and mortality of eggs and/or chicks. Needless destruction of eggs or chicks would be a violation of the Fish and Game Code and a significant impact. Pre-construction surveys shall be conducted prior to project implementation to determine if nesting birds are present on or adjacent to the project site, so that measures could be implemented if needed to avoid harming nesting birds.

The following mitigation measure shall be implemented to avoid and minimize adverse effects to nesting birds:

Mitigation Measure BIO-2: Avoid and Minimize Impacts to Nesting Birds

- If project (construction) ground-disturbing or vegetation clearing and grubbing activities commence during the avian breeding season (February 1 through August 31), a qualified biologist shall conduct a pre-construction nesting bird survey no more than 14 days prior to initiation of project activities and again immediately prior to construction. The survey area shall include suitable raptor nesting habitat within 500-feet of the project boundary (inaccessible areas outside of the project site can be surveyed from the site or from public roads using binoculars or spotting scopes). Pre-construction surveys are not required in areas where project activities have been continuous since prior to February 1, as determined by a qualified biologist. Areas that have been inactive for more than 14 days during the avian breeding season must be re-surveyed prior to resumption of project activities. If no active nests are identified, no further mitigation is required. If active nests are identified, the following measure is required:
 - A suitable buffer (e.g., typically 300-500-feet for raptors; and 50-100-feet for passerines) shall be established by a qualified biologist around active nests and no construction activities within the buffer shall be allowed until a qualified biologist has determined that the nest is no longer active (i.e., the nestlings have fledged and are no longer reliant on the nest, or the nest has failed). Encroachment into the buffer may occur at the discretion of a qualified biologist. Any encroachment into the buffer shall be monitored by a qualified biologist to determine whether nesting birds are being impacted.

With implementation of the above mitigation measures, potential impacts to special-status species and nesting birds would be less than significant and no additional mitigation measures would be required.

Question b: No Impact. There are no riparian habitats or other sensitive natural communities in the project site. Therefore, no impact would occur.

Question c: No Impact. There are no potential wetlands or other waters of the U.S. or waters of the State in the project site. Therefore, no impact would occur.

Question d: Less Than Significant Impact. The project would result in a less than significant impact to the movement of native resident wildlife or the use of native wildlife nursery sites, and no mitigation necessary.

Question e: Less than significant with Mitigation Incorporated. A total of 14 trees are found on the project site; one tree (#702) is on Lot 1 and the remaining trees are on Lot 6. Eleven of the 14 trees in the study area are protected under the City of Folsom Tree Protection Ordinance, as they are native oaks and have a DSH greater than six-inches. Tree # 705 was recommended for removal due to its poor condition. Tree # 702 is considered to be a Heritage tree per City of Folsom and will be preserved on-site as part of the proposed project design. Three of the 14 trees in the study area are not protected (Trees # 256, 329, and 330) as they are not native oak species.

Removal of protected trees requires a tree removal permit from the City of Folsom. Mitigation for tree removal includes on- or off-site replacement, payment of in-lieu fees, or credit for preservation of existing trees. Tree replacement shall be done at a ratio of one-inch DSH of tree replaced for each inch DSH of tree removed (1:1 ratio). The replacement value of planted trees is as follows:

- Sapling tree = 0.5-inch DSH
- Tree in container less than 15-gallon = 0.5-inch DSH
- A tree in a 15-gallon container = one-inch DSH.
- A tree in a 24-inch box = two-inch DSH.
- A tree in a 36-inch box or larger = three-inch DSH.

Preserved trees are eligible for a Tree Preservation Credit where a credit of 0.5-inch would be given for every one inch preserved. Mitigation for Tree #705 should not be required, due to its poor condition. Tree Preservation Credit should be given for the conservation of Tree #702, which has a DSH of 41.1-inches and results in a credit of 20.5-inches. The mitigation required for impacts to the remaining trees totals to 181-inches.

The following mitigation measure would be implemented to avoid and minimize impacts to protected trees:

Mitigation Measure BIO-3: Tree Permit

- A Tree Permit Application containing an application form, tree protection and mitigation plan, and arborist report shall be submitted to the City of Folsom by the owner/applicant for issuance of a Tree Work Permit and Tree Removal Permit prior to commencement of any grading or site improvement activities. The tree protection and mitigation plan shall be prepared in collaboration with a qualified arborist and shall be subject to review and approval by the City. The tree protection and mitigation plan shall contain the contact information of the project arborist and shall be included in all associated plan sets for the project.
- Removal of any protected tree shall be mitigated by planting replacement trees and/or payment of "In-Lieu" fees on a diameter inch basis in accordance with FMC 12.16.150. The proposed method of mitigation shall be subject to review and approval by the City.

- Prior to starting construction, oak trees to be preserved shall be fenced with high visibility fencing consistent with the city-approved tree protection and mitigation plan. Parking of vehicles, equipment, or storage of materials is prohibited within the Tree Protection Zone of Protected Trees at all times. Signs shall be posted on exclusion fencing stating that the enclosed trees are to be preserved. Signs shall state the penalty for damage to, or removal of, the protected tree.
- The owner/applicant shall retain the services of a project arborist for the duration of the development project to monitor the health of oak trees to be preserved and carry out the City-approved tree protection plan. All regulated activity conducted within the Critical Root Zone of protected trees, as that term is defined in Folsom Municipal Code (FMC) 12.16.020, shall be performed under the direct supervision of the project arborist. A copy of the executed contract for these arboricultural services shall be submitted to the City prior to the issuance of any tree or grading permits
- Certification letters by the project arborist attesting compliance with the tree protection and mitigation plan and tree permit conditions shall be submitted to the City at the following stages of the project:
 - Following completion of grading, prior to issuance of any building permits
 - At the time of the final inspection, prior to the Certificate of Occupancy

Question f: No Impact. No Habitat Conservation Plan, Natural Community Conservation Plan, or other local, regional, or state habitat conservation plan has been approved for the City of Folsom. Therefore, no impacts to an existing adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan would occur, and no mitigation is necessary.

V. CULTURAL RESOURCES

CULTURAL RESOURCES:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Cultural resource evaluations prepared for the proposed project have been incorporated by reference and are presented in their entirety in **Appendix D**.

Environmental Setting

State and federal legislation requires the protection of historical and cultural resources. In 1971, President's Executive Order No. 11593 required that all federal agencies initiate procedures to preserve and maintain cultural resources by nomination and inclusion on the National Register of Historic Places. In 1980, the Governor's Executive Order No. B-64-80 required that state agencies inventory all "significant historic and cultural sites, structures, and objects under their jurisdiction which are over 50 years of age and which may qualify for listing on the National Register of Historic Places." Section 15064.5(b)(1) of the CEQA Guidelines specifies that projects that cause "...physical demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of an historic resource would be materially impaired" shall be found to have a significant impact on the environment. For the purposes of CEQA, an historical resource is a resource listed in, or determined eligible for listing in the California Register of Historical Resources. When a project could impact a resource, it must be determined whether the resource is an historical resource, which is defined as a resource that:

(A) is historically or archaeologically significant, or is significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political or cultural annals of California; and,

(B) Meets any of the following criteria: 1) is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage; 2) is associated with the lives of persons important in our past; 3) embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values; or 4) has yielded, or may be likely to yield, information important in prehistory or history. The City of Folsom Standard Construction Specifications were developed and approved by the City of Folsom in May 2004 and updated in December 2014. They include Article 11 - Cultural Resources, which provides direction on

actions to be taken in the event that materials are discovered that may ultimately be identified as a historical or archaeological resource, or human remains (City of Folsom 2014).

Record Searches and Pedestrian Survey Results

This section describes the existing cultural resource setting and potential effects from project implementation on the project site and its surrounding area. The results are based on a record search conducted at the North Central Information Center on September 23, 2021 and a pedestrian field survey conducted on November 3, 2021. This section assesses potential impacts related to historic resources, archaeological resources, and human remains.

North Central Information Center Record Search

To determine the presence of cultural and historical resources within the project area and a 0.25-mile radius, a record search was conducted at the North Central Information Center (NCIC) on September 23, 2021. The record search included a review of National Register of Historic Places (NR), the California Register of Historic Resources (CRHR), the California Historical Landmarks (CHL) list, the California Points of Historical Interest list, the California State Historic Resources Inventory (HRI) listings for Sacramento County, and the Archaeological Determinations of Eligibility (ADOE). Historic maps were also examined to gain insights into past developments and changes within the project area and its surroundings.

The NCIC results indicate that 53 historic resources have been recorded within the 0.25-mile search radius; six resources were recorded as potentially occurring within the project area. The 53 historic resources are primarily scattered debris, ditches, and metal remnants from the Folsom Mining District and the Prairie Diggings Placer Mining District. The 53 historic Resources are outlined in **Table 8**.

Table 8. Previously Documented Resources within the Study Area

Primary	Trinomial	Year	Author(s)	Description
P-34-000335	CA-SAC-000308H	1995	Flint, S.	Historic- the Folsom Mining District
P-34-001480	CA-SAC-000903H	2005	Jensen, Sean Michael and Rob McCann	Historic- Segment of the Rhoads' Branch Ditch
P-34-002195	None	2008	Westwood, Lisa	Historic- 1940s era Transmission Line
P-34-002292	None	1994	Dougherty, John and David Davis	Historic- Placer mining landscape
P-34-002306	None	1994	Lindstrom, Susan, Judy D. Tordoff, and Daryl G. Noble	Historic- the Prairie Diggings Placer Mining District
P-34-004518	None	2012	Crawford, K. A.	Historic- mid-20 th century lattice tower/ part of transmission line
P-34-000461	CA-SAC-000434H	1989	Shapiro, William A.	Historic- Natomas Ditch- water conveyance system
P-34-000648	CA-SAC-000524	1990	Derr, Eleanor H. and John Dougherty	Prehistoric- lithic scatter and bedrock milling feature
P-34-000767	CA-SAC-000589H	1990	Derr, Eleanor H. and John Dougherty	Historic- debris scatter, contributing element to district 34-000335
P-34-000768	CA-SAC-000590H	1990	Derr, Eleanor H. and John Dougherty	Historic- mining camp contributing element to district 34-000335
P-34-000769	CA-SAC-000591H	1990	Derr, Eleanor H. and Ken McIvers	Historic- mining camp contributing element to district 34-000335
P-34-000770	CA-SAC-000592H	1990	Derr, Eleanor H. and Ken McIvers	Historic- mining camp contributing element to district 34-000335

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Primary	Trinomial	Year	Author(s)	Description
P-34-000774	CA-SAC-000596H	1990	Derr, Eleanor H. and Ken McIvers	Prehistoric- lithic scatter and Historic- mining camp and mines/quarries/tailings contributing element to district 34-000335
P-34-00775	CA-SAC-000597H	1990	Derr, Eleanor H. and Ken McIvers	Historic- remains of shed
P-34-00776	CA-SAC-000598H	1990	Derr, Eleanor H. and Ken McIvers	Historic- The Russi Place –foundations, privies and trash scatters, and farm/ranch
P-34-00777	CA-SAC-000599H	1990	Derr, Eleanor H. and Ken McIvers	Historic- well/cistern
P-34-00780	CA-SAC-000602H	1994	D., JW and ET	Historic- stone fence
P-34-00783	CA-SAC-000605H	1990	Derr, Eleanor H.	Historic- stone fence
P-34-00784	CA-SAC-000606H	1990	Derr, Eleanor H. and Ken McIvers	Historic- privy/dump/trash scatter
P-34-00789	None	1990	Derr, Eleanor H.	Historic- piece of chimney
P-34-00790	None	2012	Pappas, S., and D. Quivey	Historic- metal drum
P-34-001765	None	2006	Windmiller, Ric	Historic- wall
P-34-001771	None	2006	Windmiller, Ric	Historic- trash scatter, mines/quarries/tailings
P-34-001774	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings
P-34-001775	None	2006	Windmiller, Ric	Historic- roads/trails/railroad grade
P-34-001776	None	2006	Windmiller, Ric	Historic- water conveyance system
P-34-001777	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings
P-34-001778	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings
P-34-001782	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings
P-34-001795	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings
P-34-001798	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings
P-34-001799	None	2006	Windmiller, Ric	Historic- prospect pits
P-34-001800	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings
P-34-001801	CA-SAC-001019H	2006	Windmiller, Ric	Historic- foundations/structure pads
P-34-001802	None	2006	Windmiller, Ric	Historic- roads/trails/railroad grades
P-34-001803	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings
P-34-001807	None	2006	Windmiller, Ric	Historic- water conveyance system
P-34-001820	CA-SAC-001020H	2006	Windmiller, Ric	Historic- foundations/structure pads
P-34-001926	None	2006	Windmiller, Ric	Historic- drains, dams, mines/quarries/tailings, and ponds
P-34-002087	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings, part of Historic Mining landscape
P-34-002088	CA-SAC-001085H	2006	Windmiller, Ric	Historic- foundations/structure pads
P-34-002089	None	2006	Windmiller, Ric	Historic- prospect pits
P-34-002090	None	2006	Windmiller, Ric	Historic- concrete and metal debris
P-34-002091	None	2006	Windmiller, Ric	Historic- mines/quarries/tailings
P-34-002287	None	1990	Derr, Eleanor H.	Historic- mines/quarries/tailings, contributing element to district 34-000335
P-34-002288	None	1990	Derr, Eleanor H. and Randy Bethard	Historic- pick head embedded in quartz, element of district 34-000335
P-34-002291	None	1990	Dougherty, John and David Davis	Historic- mines/quarries/tailings, element of district 34-000335
P-34-002293	None	1990	Dougherty, John and David Davis	Historic- mines/quarries/tailings and water conveyance system, element of district 34-000335
P-34-002294	None	1994	Teixeria, Emanuel and John	Historic- mines/quarries/tailings – mining landscape, element of district 34-000335

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Primary	Trinomial	Year	Author(s)	Description
			Dougherty	
P-34-002295	None	1994	Dougherty, John, Jay Flaherty and David Davis	Historic- mines/quarries/tailings, element of district 34-000335
P-34-004667	None	2013	Westwood, Lisa	Historic- Rhoades' Diggings Mining District, including foundation pads, privy/dumps/trash scatters, water conveyance system, roads/trails/railroad grades/dams, mines/quarries/tailings, subsumes 34-001744
P-34-004757	None	2014	Pappas, S. and D. Quivey	Historic- water conveyance system
P-34-004758	None	2013	Pappas, S. and D. Quivey	Historic- mines/quarries/tailings

The first resource identified, the Folsom Mining District (P-34-000335), was recorded as a variety of elements from the region's historic mining period, including mines, quarries, tailings, mining equipment, habitation sites, roads, railroad grades, water conveyances, and structural foundations. The results of HELIX's NCIC records search indicated that elements of this historic district could be present within both lots of the currently proposed Area of Potential Impact (APE). Records indicate that the Folsom Mining District taken as a unified entity has been determined to be ineligible for listing on the NRHP and CRHR, but that individual elements within the district may be eligible for listing and that they should be evaluated as eligible or ineligible on a case-by-case basis. This resource was first recorded in 1995 by Sandy Flint.

The second resource identified on the project site is known as the Rhoads Branch Ditch (P-34-001480). The results of HELIX's NCIC records search indicated that elements of this ditch system could be present within the current APE's Lot 6. The ditch was used for supplying water to most of the mined areas south of Alder Creek, east of Prairie City, and south of the Willow Hill diggings. Since its initial recordation this resource has been incorporated as an element of the American River Placer Mining District, now also known as the Folsom Mining District (P-34-000335). As of the time of ECORP Consulting Inc.'s 2013 survey, the resource is believed to be heavily disturbed from the construction of houses, roads and associated facilities, though portions of the ditch may still be in good condition. NRHP and CRHR eligibility have not been determined for this resource.

The third resource, first recorded in 2008 by Lisa Westwood, this resource is a 1940s-era transmission line that extends from Halsey to Newark. It is composed of metal towers and situated directly east of, and parallel to, two higher capacity, modern transmission lines that bisect the current APE's Lot 1. Built in the early 1940s, the line is now named the Gold Hill-Bellota-Lockford 115kV line. According to maintenance logs on file with PG&E, the line was upgraded in conjunction with the construction of the Gold Hill Substation in 1963, and again in 1975 and 1983. This resource has been determined ineligible for listing on the NRHP and CRHR. Most recently revisited in 2017 by ECORP Consulting Inc. archaeologists, the resource is considered to be in good condition.

The fourth resource was first recorded in 1994 by John Dougherty and David Davis, this site consists of a placer mine located approximately 10-meters north of US Highway 50, along an ephemeral northwest flowing drainage. NCIC maps show the site as intersecting Lot 1's southeast border. This site is considered an element of the Folsom Mining District (P-34-000335), and it abuts several other resources which are also part of the District, including other mining-related ground disturbances, mining camps,

and historic debris piles associated with mining activities. P-34-002292's NRHP and CRHR eligibility has not been determined.

The fifth resource, first recorded in 1994 by Susan Lindstrom, Judy D. Tordoff, and Daryl G. Noble, this site represents the Prairie Diggings Placer Mining District which contains 35 loci of nineteenth century cultural resources pertaining to mining activities and mining camp occupations. These resources include examples of early shallow placer mines; evidence of ground sluicing, drift mining, low-pressure hydraulic mining, and dry land dredging activities; water conveyances; and artifacts and landscape features associated with mining camp operations including personal effects, mining equipment, hearths and roads. The district encompasses approximately 302-acres and represents one of the mining areas within Prairie City's sphere of influence in the 1850s and 60s. The district is situated north of Alder Creek and largely east of Prairie City Road, with Willow Hill Reservoir in its western arm, and it includes the current APE's Lot 1 within its boundaries. As the result of development in the area, the district has suffered significant losses to its site integrity and has been determined ineligible for inclusion on the NRHP and CRHR as of 2014.

The sixth resource, first recorded in 2012 by K.A. Crawford, this site consists of a steel lattice transmission tower located in a large parking lot area in the City of Folsom, immediately adjacent to the current APE's Lot 1. The base of the tower was installed by the Pacific Gas and Electric Company prior to 1967 as part of their expansion of electrical services in the Folsom area. The tower was constructed with bolted steel L-shaped profiles, and as of its recording in 2012 was still in good condition. At the time the tower was also noted as retaining its structural and historic integrity because it had not been significantly altered since its original construction. This resource has been determined ineligible for listing on the NRHP.

A total of 23 reports have been prepared within the search radius, six of which included the project area. These previous reports are outlined in **Table 9**.

Table 9. Previous Studies Conducted within the Study Area

Report	Year	Author(s)	Title	Affiliation
003925	1990	Derr, Eleanor	The Broadstone Master Plan Project: Final Report	Cultural Resources Unlimited
004520	1992	Maniery, Mary	Historic Survey Report and Historic Resource Evaluation Report for Sixteen Sites, Highway 50 Interchange Project Post Mile 18.8 TO 23.1, Sacramento County, California	PAR Environmental Services, Inc
011136	2012	Billat, Lorna	Collocation ("CO") Submission Packet FCC Form 621	EarthTouch, Inc
011161	2012	Crawford, Kathleen	Direct APE Historic Architectural Assessment for T-Mobile West, LLC Candidate SC06934A (HWY 50 - Scott Road), 2155 Iron Road, Folsom, Sacramento County, California	Michael Brandman Associates
011164	2012	Wills, Carrie	Cultural Resources Records Search and Site Visit Results for T-Mobile West, LLC Candidate SC06934A (Hwy 50 - Scott Road), 2155 Iron Point Road, Folsom, Sacramento County, California	Michael Brandman Associates
011632	2014	Pierce, Wendy	Willow Hill Reservoir Trail Project, Cultural Resource Inventory, City of Folsom, Sacramento	Pierce Archaeological Consulting
003840	1994	Tordoff, Judy	Proposed Interchange and Auxiliary Lanes Highway 50	Caltrans
004521	1994	Noble, Daryl G.	Historic Property Survey Report for a Proposed Interchange and Auxiliary Lanes on Highway 50 in Eastern Sacramento County, California 03-SAC-50 P.M. 17.1/20.1 03101-394500	State of California, Department of Transportation District 3

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Report	Year	Author(s)	Title	Affiliation
004525	1991	Maniery, Mary	Archaeological Survey Report for the Highway 50 Interchange Project, Post Mile 15.8 to Post Mile 23.1, Sacramento County, California	PAR Environmental Services
007121	2004	Clark, Matthew	The Status of Cultural Resources Research for the Kaiser Folsom Project Area in the City of Folsom, Sacramento County, CA	None Listed
008736	2006	Windmiller, Ric	Carpenter Ranch Cultural Resources Inventory, Folsom, Sacramento County, California	Consulting Archaeologist
009579	2008	Losee, Carolyn	Submission Packet, FCC Form 621, for existing Telecommunications Facility, Folsom AT&T	Professional Archaeologist
011001	2012	Westwood, Lisa and Stephen Pappas	Folsom South of US Highway 50 Specific Plan Project Preliminary Historic Properties Synthesis Report Sacramento County, California Project No. 2005-429.1	ECORP Consulting, Inc.
011337	2013	Knapp, Katherine, and Lisa Westwood	Cultural Resources Testing and Evaluation Report for the Mangini Ranch APE, Folsom South of U.S. Highway 50 Specific Plan Project, Sacramento County, California ECORP Project No. 2012-037.1	ECORP Consulting, Inc.
011408	2012	Westwood, Lisa, Katherine Knapp, Stephen Pappas, David Quivey, and Roger Mason	Cultural Resources Testing and Evaluation Report for the Carpenter Ranch Permit Area, Folsom South of U.S. Highway 50 Specific Plan Project; Cultural Resources inventory Report for the Carpenter Ranch APE within the Folsom South of Highway 50 Specific Plan	ECORP Consulting, Inc.
011728	2014	Westwood, Lisa	Historic Property Treatment Plan for the Non-Backbone Prairie City Road Business Park Permit Area, Folsom South of U.S. Highway 50 Specific Plan Project Sacramento County, California	ECORP Consulting, Inc.
011894	2014	Westwood, Lisa and Katherine Knapp	Finding of Effect Report for the Arcadian Heights APE Folsom South of U.S. Highway 50 Specific Plan Project Sacramento County, California	ECORP Consulting, Inc.
012049	2015	Westwood, Lisa	Light Detection and Ranging (LIDAR) data for the Folsom South of U.S. Highway 50 Specific Plan Project. Generated in compliance with Section 4.4 of the approved (August 2013) Historic Property Treatment Plan for the Backbone Infrastructure permit area (SPK-2007-02159).	ECORP Consulting, Inc.
012053	2015	Westwood, Lisa	Data Recovery Report for Archaeological Sites in the Backbone Infrastructure Area of Potential Effects, Folsom South of U.S. Highway 50 Specific Plan Project, Sacramento County, California, ECORP Project No. 2005-429.6	ECORP Consulting, Inc.
012088	2015	Westwood, Lisa and Katherine Knapp	Historic Property Treatment for the Non-Backbone Prairie City Road Business Park Permit Area, Folsom South of U.S. Highway 50 Specific Plan Project, Sacramento County, California (ECORP Project No. 2009-168.8)	ECORP Consulting, Inc.
012419	2013	Knapp, Katherine and Lisa Westwood	Historic Property Treatment Plan for the Backbone Infrastructure Permit Area, Folsom South of U.S. Highway 50 Specific Plan Project, Sacramento County, California	ECORP Consulting, Inc.
012458	2015	Westwood, Lisa, Jeremy Adams, Stephen Pappas, Susan Lindstrom, and Roger Mason	Folsom South of U.S. Highway 50 Specific Plan Project, Historic Properties Management Plan, Sacramento County, California	ECORP Consulting, Inc.
012520	2016	Westwood, Lisa	Cultural Resources Inventory Update for the 2.72-acre Broadstone Oaks Crossing APE Within the Broadstone Master Plan Project Area, ECORP Project No. 2015-049	ECORP Consulting, Inc.

Pedestrian Survey

On November 3, 2021, HELIX Senior Archaeologist Clarus Backes R.P.A, conducted a pedestrian survey to characterize any prehistoric or historic-era archaeological resources located on the surface of the Area of Potential Effects (APE). During the survey, the ground surface throughout both parcels of the APE were examined for the presence of historic-era artifacts (e.g, metal, glass, ceramics), prehistoric artifacts (e.g., flaked stone tools, tool-making debris), and other features that might represent human activity that took place more than 50 years ago. Further, a concerted effort was made to locate the six cultural resources identified during the NCIC records search as lying within or adjacent to the current APE. Representative photographs taken during the survey are presented in **Appendix D**. The surveys of each individual lot (Lot 1 and Lot 6) are presented separately below.

Lot 1

Lot 1's ground surface can be characterized as slightly undulating, with a gradual (5-10 percent) slope downhill to the southwest (Photos 1 and 2). There is also a short, steep downslope from Lot 1's northeastern boundary north towards the nearby medical center parking lot (Photo 3). The entire Lot was found to be covered with dense, nonnative grasses approximately 24-inches high, and as a result surface visibility for the pedestrian survey was very poor (less than five percent visibility). Ground soils that were visible, however, proved to be brownish-red sandy silt with large pebbles and small cobble inclusions that are angular and granitic. There were also loose, large quartz cobbles and small boulders scattered throughout the area.

Overall, the area showed signs of moderate ground disturbance, with recent tire tracks crossing the Lot from all directions. There were also several small borrow pits and push piles, as well as several small concentrations of broken asphalt and rounded river cobbles that appear to have been brought in from off-site (Photos 4 and 5). Further, at the time of survey, the entire Lot was covered with a thin scatter of modern roadside debris.

Five cultural resources identified during the NCIC records search were found lying within or adjacent to Lot 1. They are as follows: P-34-000335, P-34-022195, P-34-002292, P-34-002306 and P-34-004518. The pedestrian survey revealed that no elements or cultural resources that could be associated with the historic Folsom Mining District (P-34-000335), the Prairie Diggings Placer Mining District (P-34-002306), or the mining feature listed as an element of the Folsom Mining District (P-34-002292). Cultural resource P-34-022195 is a 1940s era lattice metal tower. HELIX's pedestrian survey did not encounter any evidence of that mining feature within Lot 1. P-34-004518 is a mid-twentieth century metal lattice transmission tower. HELIX's pedestrian survey of Lot 1 encountered this resource and noted that there had been no significant changes to its condition or character since its initial recordation by archaeologist K.A. Crawford in 2012.

Lot 6

Lot 6's ground surface gently rises from the northeast to the southwest through a series of low artificial terraces (Photo 7). The lot is covered with dense nonnative grasses, though they were shorter than those found on Lot 1, allowing for slightly better ground surface visibility (a little less than 10 percent). There is also a small stand of oak trees in the lot's southwest corner (Photo 8). A few disturbed areas within the lot exposed bare soils which proved to be brown sandy silt with angular large pebbles and small cobbles, and include concentrations of gray and red slate. Overall, Lot 6 is considerably more disturbed than Lot 1, with tire tracks, small, graded areas, and push piles visible throughout the survey

area. In addition, along the lot's northeastern boundary there is a 69-meter long, 18-meter wide concentration of push piles and large granitic boulders (Photo 9). These piles also contained broken up fragments of reinforced concrete. It is unclear whether these boulders originated from within the lot, or if they were imported from off-site, but in either case it is clear they are not in their original placements.

Lot 6 also exhibited a thin scatter of industrial debris across the survey area including scrap metal, plastic fragments, and pipe fragments. None of this debris, however, appeared to be indicative of activities taking place on the site more than 45 years ago.

Near the center of the lot is a small, graded depression used as a stormwater control basin. This basin, which was seen holding standing water at the time of the survey, is fed by a small culvert that runs from the Folsom Corporate Center to the south. A ditch extends from this stormwater basin for approximately 40-meters. Together these elements appear to function as a modern water-control feature, rather than one of the historic ditches that have been documented by previous studies in the project vicinity. Two cultural resources identified during the NCIC records search were identified lying within or adjacent to Lot 6. These resources are P-34-000335 and P-34-001480. The pedestrian survey revealed that no elements or cultural resources that could be associated with this historic district (P-34-000335) or historic ditch (P-34-001480) are located on the ground surface of Lot 6.

Evaluation of Cultural Resources

Question a: Less than Significant. Review of historic topographic maps (dating from 1911 to 1975) and historic aerial photographs (dating 1952 to 2018) indicate that Lots 1 and 6 have not undergone any formal development between 1952 and 2018. Characterized during these periods as undulating grassy fields with moderate to sparsely populated oak stands, only tree clearing and dirt road construction activities were made apparent within the APE during HELIX's historic maps and images review, with those activities spanning only between 2002 and 2018. Of the six previously recorded resources that are indicated by the NCIC as potentially lying within or adjacent to the current APE, only two were encountered during HELIX's survey. These include P-34-002195 and P-34-004518, two metal lattice towers constructed for use in electrical transmission lines during the mid-20th century. The proposed project is not anticipated to have impacts on either of these two resources. Although NCIC records indicate that site P-34-002292 might lie within the currently proposed APE, the only traces of historic mining activity spotted during HELIX's pedestrian survey consisted of placer mining spoil piles which lie to the southwest of Lot 1 and outside of the project's APE. Consequently, the current project is not anticipated to impact this resource.

In the event that cultural resources are exposed during any future ground-disturbing activities, construction activities should be halted in the immediate vicinity of the discovery. If the site cannot be avoided during the remainder of the construction, an archaeologist who meets the Secretary of the Interior's Professional Qualifications Standards should then be retained to evaluate the find's significance under CRHR criteria. If the discovery proves to be significant, additional work, such as data recovery excavation, may be warranted and should be discussed in consultation with the County. With implementation of this guideline, and with consideration that no historic resources are anticipated to be impacted by the project, impacts would be less than significant.

Question b: Less than Significant with Mitigation. On November 2, 2021, HELIX requested that the NAHC conduct a search of their SLF for the presence of Native American sacred sites or human remains in the vicinity of the proposed project area. HELIX received a response from NAHC on November 16,

2021, which reported that the SLF search results were negative. However, it is possible that subsurface excavation activities may encounter previously undocumented archaeological resources. The implementation of standard cultural resource construction mitigation (Mitigation Measure CUL-1) would ensure that this impact is less than significant.

Mitigation Measure CUL-1: Avoid and minimize impacts to previously unknown archaeological resources.

It is always possible that ground-disturbing activities during project development may uncover previously unknown archaeological resources. In the event that archaeological resources are discovered during construction, construction operations shall stop within a 100-foot radius of the find and a qualified archaeologist shall be consulted to determine whether the resource requires further study. The City shall include a standard inadvertent discovery clause in every construction contract to inform contractors of this requirement. The archaeologist shall make recommendations concerning appropriate measures that will be implemented to protect the resources, including but not limited to, excavation and evaluation of the finds in accordance with Section 15064.5 of the CEQA Guidelines. Archaeological resources could consist of, but are not limited to, stone, bone, wood, or shell artifacts or features, including hearths. Any previously undiscovered resources found during construction within the project area should be recorded on appropriate Department of Parks and Recreation (DPR) 523 forms and evaluated for significance in terms of CEQA criteria.

Question C: No Impact. The proposed project area is not located in an area that is considered likely to have paleontological resources present. Paleontological resources (fossils) are remains an/or traces of prehistoric life. Fossils are typically preserved in layered sedimentary rocks, and the distribution of fossils is a result of the sedimentary historic of the geologic units within which they occur.

Question D: Less than Significant with Mitigation. No human remains are known to exist within the project area, and there were no indications of human remains found during the field survey. However, there is always the possibility that subsurface construction activities associated with the proposed project, such as trenching and grading, could potentially damage or destroy previously undiscovered human remains. Accordingly, this implementation of Mitigation Measure CUL-2 would reduce this potential impact to a less than significant level.

Mitigation Measure CUL-2: Avoid and minimize impacts related to accidental discovery of human remains.

In the event of the accidental discovery or recognition of any human remains, CEQA Guidelines § 15064.5; Health and Safety Code § 7050.5; Public Resources Code § 5097.94 and § 5097.98 must be followed. If during the course of project development there is accidental discovery or recognition of any human remains, the following steps shall be taken:

1. There shall be no further excavation or disturbance within a 100-foot radius of the potentially human remains until the County Coroner is contacted to determine if the remains are Native American and if an investigation of the cause of death is required. If the coroner determines the remains to be Native American, the coroner shall contact the Native American Heritage Commission (NAHC) within 24 hours, and the NAHC shall identify the person or persons it believes to be the "most likely descendant" (MLD) of the deceased Native American. The MLD may make recommendations to the landowner or the person responsible for the excavation

work within 48 hours, for means of treating or disposing of, with appropriate dignity, the human remains and any associated grave goods as provided in PRC Section 5097.98.

2. Where the following conditions occur, the landowner or his authorized representative shall rebury the Native American human remains and associated grave goods with appropriate dignity either in accordance with the recommendations of the most likely descendant or on the project site in a location not subject to further subsurface disturbance:
 - The NAHC is unable to identify a most likely descendent or the most likely descendent failed to make a recommendation within 48 hours after being notified by the commission.
 - The descendant identified fails to make a recommendation.
 - The landowner or his authorized representative rejects the recommendation of the descendant, and mediation by the NAHC fails to provide measures acceptable to the landowner.

VI. GEOLOGY AND SOILS

GEOLOGY AND SOILS:					
Would the project:		Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a)	Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i.	Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii.	Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii.	Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv.	Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b)	Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c)	Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d)	Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e)	Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The following discussion is based in part on the approach, methodology, results, and conclusions outlined in a geotechnical investigation report prepared by Geocon Consultants, Inc. (Geocon 2017). The geotechnical report was prepared for a project located adjacent to Lot 1, and its description of the environmental setting and geographic landscape of the area is used in the following analysis, and is included as **Appendix E**. A NRCS soil report was also prepared, specific to this project (NRCS 2021).

Environmental Setting

Geology

The project area is at the base of the western Sierra Nevada foothills and is underlain by metamorphic rocks. Site geology consists of existing fill within the northern portion of the site north of the pond and Jurassic-age Gopher Ridge Volcanics (Jgo) and Salt Springs Slate bedrock (Jss) (Geocon Consulting 2018).

The Foothill fault system is located along the western slope of the Sierra Nevada which is the nearest source of seismic activity to the project site. The Bear Mountain Fault, four miles east of Folsom, is a

potentially active trace of the Foothills fault system (CDC 2018b). Although historic seismic activity has been minor along this fault, the potential for strong ground shaking is present. An earthquake on the Bear Mountain fault could cause bedrock accelerations up to 0.35 g (acceleration of gravity).

The State Division of Mines and Geology has published a map of maximum potential earthquake intensities for California. The project area is within seismic risk Zone 3 (State Division of Mines and Geology 2015). A maximum credible earthquake (Richter scale magnitude 6.5) on the Bear Mountain Fault could cause ground shaking of modified Mercalli scale intensity VII or greater, and subsequently cause major damage to structures and injury to people (Folsom, USBR 1992).

The Alquist-Priolo Earthquake Fault Zone Act was passed in 1972 to mitigate the hazard of surface faulting to structures designed for human occupancy. The purpose of the Act is to prevent the construction of buildings used for human occupancy on the surface trace of active faults. No active or potentially active faults are located within the project site or in the project vicinity as mapped under the Act (CDC 2018b).

Soils

Soils on the project site are mapped as Whiterock loam (Lot 1 and Lot 6) and Argonaut-Auburn complex (Lot 1). Whiterock loam soil is somewhat excessively drained, and Argonaut-Auburn complex soil is well-drained (NRCS 2018).

City Regulation of Geology and Soils

The City of Folsom regulates the effects of soils and geological constraints on urban development primarily through enforcement of the California Building Code, which requires the implementation of engineering solutions for constraints to urban development posed by slopes, soils, and geology. The City has additionally adopted a Grading Code (Folsom Municipal Code Section 14.29) that regulates grading citywide to control erosion, storm water drainage, revegetation, and ground movement.

Evaluation of Geology and Soils

Question a (i): No Impact. There are no active or potentially active faults located within the project site, or in the project vicinity as mapped under the Alquist-Priolo Earthquake Fault Zone Act (CDC 2018b). Because no faults underlie the project site, no impact would result, and no mitigation would be necessary.

Question a (ii): Less than Significant Impact. The project area is within seismic risk Zone 3, and a maximum credible earthquake on the Bear Mountain Fault could cause ground shaking of modified Mercalli scale intensity VII or greater, and subsequently cause major damage to structures and injury to people within the project area. While earthquake-induced ground shaking could occur in the project vicinity, historically, seismic activity in the Folsom area has been limited. Further, the proposed project would be constructed in accordance with standards imposed by the City of Folsom through the Grading Code, and in compliance with California Building Code requirements. Potential impacts would be reduced to levels considered acceptable in the City and region. As a result, the project would not expose people or structures to substantial adverse effects of seismic events. This would be a less than significant impact and no mitigation would be required.

Question a (iii) Less than Significant Impact. Liquefaction is a process by which water-saturated materials, such as soil and sediment lose strength and fail during strong ground shaking. Liquefaction occurs when granular material is transformed from a solid state into a liquefied state as a consequence of increased water pressure. Liquefaction is most commonly induced by strong ground shaking associated with earthquakes.

Factors that contribute to liquefaction potential include soil type, the level and duration of seismic ground motions, the type and consistency of soils, and the depth to groundwater. Liquefaction can occur where unconsolidated sediments and a high-water table coincide. Loose sands and peat deposits are susceptible to liquefaction, while clayey silts, silty clays, and clays deposited in freshwater environments are generally stable under the influence of seismic ground shaking. According to the soils mapping for the site, both the Argonaut-Auburn complex soils (present on Lot 1) and the Whiterock loam soils (present on Lot 1 and Lot 6) onsite have a depth to the water table greater than 80 inches (NRCS 2018).

The soils on both parcels do not contain the characteristics typical of soils most susceptible to liquefaction, and because the depths to groundwater are more than 80 inches below the ground surface, it is unlikely that the proposed project would be exposed to liquefaction hazards. Further, the proposed project would be constructed in accordance with standards imposed by the City through the Grading Code, and in compliance with California Building Code requirements. Compliance with these regulations would further reduce potential impacts related to liquefaction. Impacts as a result of seismic-related ground failure or liquefaction hazard at the project site would be less than significant and no mitigation would be required.

Question a (iv): Less than Significant Impact. There is a potential that the proposed project could be exposed to the effects of earthquake-induced ground shaking; however, standards imposed by the City of Folsom through the Grading Code and compliance with California Building Code requirements would reduce this potential impact to levels considered acceptable in the City and region. Likewise, the moderate potential effects from weak soils and water erosion hazards would be minimized through implementation of these standards. There would be no potential for impacts associated with rupture of a known earthquake fault, and less than significant impacts associated with strong seismic ground shaking, seismic-related ground failure, landslides, soil erosion or loss of topsoil, unstable soils, and expansive soils. Overall impacts would be less than significant, and no mitigation would be required.

Question b: Less than Significant Impact. Soils on the project site are well drained; however, Argonaut-Auburn soil has a high runoff potential, which would indicate a higher potential for water erosion. Ground disturbing activities during construction of the project would further increase the potential for soil erosion.

The California Building Code and the City's Grading Code and standard conditions for approval contain requirements to minimize or avoid potential effects from water erosion hazards. As a condition of approval, prior to the issuance of a grading or building permit, the City would require the applicant to prepare a soils report, a detailed grading plan, and an erosion control plan by a qualified and licensed engineer. The soils report would identify soil hazards, including potential impacts from erosion. The City would be required to review and approve the erosion control plan based on the State of California Department of Conservation's "Erosion and Control Handbook." The erosion control plan would identify protective measures to be implemented during excavation, temporary stockpiling, disposal, and revegetation activities.

Table 10. Global Warming Potentials and Atmospheric Lifetimes

GREENHOUSE GAS	ATMOSPHERIC LIFETIME (years)	GLOBAL WARMING POTENTIAL (100-year time horizon)
Carbon Dioxide (CO ₂)	50.0–200.0	1
Methane (CH ₄)	12.0	25
Nitrous Oxide (N ₂ O)	114.0	298
HFC-134a	14	1,430
PFC: Tetrafluoromethane (CF ₄)	50,000.0	7,390
PFC: Hexafluoroethane (C ₂ F ₆)	10,000.0	12,200
Sulfur Hexafluoride (SF ₆)	3,200.0	22,800

HFC: hydrofluorocarbons; PFC: perfluorocarbons.

Source: IPCC 2007.

Regulatory Framework Relating to Greenhouse Gas Emissions

AB 32, the California Global Warming Solutions Act of 2006, recognizes that California is a source of substantial amounts of GHG emissions. The statute states that:

Global warming poses a serious threat to the economic wellbeing, public health, natural resources, and the environment of California. The potential adverse impacts of global warming include the exacerbation of air quality problems, a reduction in the quality and supply of water to the state from the Sierra snowpack, a rise in sea levels resulting in the displacement of thousands of coastal businesses and residences, damage to marine ecosystems and the natural environment, and an increase in the incidences of infectious diseases, asthma, and other human health-related problems.

In order to help avert these potential consequences, AB 32 established a State goal of reducing GHG emissions to 1990 levels by the year 2020, which was a reduction of approximately 16 percent from forecasted emission levels, with further reductions to follow. In addition, AB 32 required CARB develop a Scoping Plan to help the state achieve the targeted GHG reductions. In 2015, Executive Order (EO) B-30-15 established California GHG emission reduction targets of 40 percent below 1990 levels by 2030 and 80 percent below 1990 levels by 2050. The EO aligns California's GHG emission reduction targets with those of leading international governments, including the 27 nation European Union. California met the target of reducing greenhouse gas emissions to 1990 levels by 2020, as established in AB 32. As a follow-up to AB 32 and in response to EO-B-30-15, Senate Bill (SB) 32 was passed by the California legislature in 2016 to codify the EO's California GHG emission reduction target of 40 percent below 1990 levels by 2030.

In December 2008, CARB adopted its first version of its Climate Change Scoping Plan (Scoping Plan), which contained the main strategies California was to implement to achieve the mandate of AB 32 to reduce statewide GHG emissions to 1990 levels by 2020. The Scoping Plan establishes an overall framework for the measures to be adopted to reduce California's GHG emissions. The Scoping Plan evaluates opportunities for sector-specific reductions, integrates all CARB and Climate Action Team early actions and additional GHG reduction measures by both entities, identifies additional measures to be pursued as regulations, and outlines the role of a cap-and-trade program.

On December 14, 2017, CARB adopted the 2017 Climate Change Scoping Plan (2017 Scoping Plan), which lays out the framework for achieving the mandate of SB 32 (2016) to reduce statewide GHG emissions to at least 40 percent below 1990 levels by the end of 2030 (CARB 2017).

The 2017 Scoping Plan includes guidance to local governments in Chapter 5, including plan-level GHG emissions reduction goals and methods to reduce communitywide GHG emissions. In its guidance, CARB recommends that “local governments evaluate and adopt robust and quantitative locally-appropriate goals that align with the statewide per capita targets and the State’s sustainable development objectives and develop plans to achieve the local goals.” CARB further states that “it is appropriate for local jurisdictions to derive evidence-based local per capita goals [or some other metric] that the local jurisdiction deems appropriate, such as mass emissions or per service population, based on local emissions sectors and population projections that are consistent with the framework used to develop the statewide per capita targets” (CARB 2017).

As part of the 2035 General Plan, the City prepared an integrated Greenhouse Gas Emissions Reduction Strategy (Appendix A to the 2035 General Plan; adopted August 28, 2018). The purpose of the Greenhouse Gas Emissions Reduction Strategy (GHG Strategy) is to identify and reduce current and future community GHG emissions and those associated with the City’s municipal operations. The GHG Strategy includes GHG reduction targets to reduce GHG emissions (with a 2005 baseline year) by 15 percent in 2020, 51 percent in 2035, and 80 percent in 2050. The GHG Strategy identifies policies within the City of Folsom General Plan that would decrease the City’s emissions of greenhouse gases. The GHG Strategy also satisfies the requirements of CEQA to identify and mitigate GHG emissions associated with the General Plan Update as part of the environmental review process and serves as the City’s “plan for the reduction of greenhouse gases”, per Section 15183.5 of the CEQA Guidelines, which provides the opportunity for tiering and streamlining of project-level emissions for certain types of discretionary projects subject to CEQA review that are consistent with the General Plan (City 2018).

Evaluation of Greenhouse Gas Emissions

The final determination of whether or not a project has a significant effect is within the purview of the lead agency pursuant to CEQA Guidelines Section 15064(b). The City’s GHG Strategy, described above, is a qualified plan for the reduction of greenhouse gases pursuant to CEQA Guidelines Section 15183.5. Consistency with the GHG Strategy may be used to determine the significance of the project’s GHG emissions.

The City’s 2035 General Plan Policy NCR 3.2.8 and GHG Strategy include criteria to determine whether the potential greenhouse gas emissions of a proposed project are significant (City 2018).

NCR 3.2.8 Streamlined GHG Analysis for Projects Consistent with the General Plan

Projects subject to environmental review under CEQA may be eligible for tiering and streamlining the analysis of GHG emissions, provided they are consistent with the GHG reduction measures included in the General Plan and EIR. The City may review such projects to determine whether the following criteria are met:

- Proposed project is consistent with the current general plan land use designation for the project site;

Further, projects resulting in one or more acre of ground disturbance require a General Construction Activity Stormwater Permit and a National Discharge Elimination System (NPDES) permit from the State Water Resources Control Board (SWRCB). Use of the permit requires the preparation of a Storm Water Pollution Prevention Plan (SWPPP) for approval by the SWRCB. The plan would contain best management practices to reduce potential impacts to water quality during construction of the project. Compliance with the City's regulations, the California Building Code requirements, and implementation of the SWPPP would reduce potential impacts related to soil erosion from water to less than significant and no mitigation would be required.

Question c: Less than Significant Impact. Lot 1 is mapped as Argonaut-Auburn soil (91.9%), and Whiterock loam (8.1 percent), and Lot 6 is mapped as Whiterock loam. The NRCS does not have information regarding the stability of Argonaut-Auburn complex soils, nor Whiterock loam (NRCS 2018). However, the project area is not noted for unstable geologic formations susceptible to landslide, lateral spreading, subsidence, liquefaction, or collapse. Compliance with the City's regulations and the California Building Code would minimize potential impacts from weak or unstable soils. Therefore, impacts related to unstable soils would be less than significant, and no additional mitigation would be necessary.

Question d: Less than Significant Impact. Expansive soils shrink and swell in response to changes in moisture levels. The changes in soil volumes can result in damage to structures including building foundations, and infrastructure, if the project design does not appropriately accommodate the changing soil conditions. The parcels are mapped as Argonaut-Auburn complex (Unit 107) and Whiterock loam (Unit 237), and NRCS does not have information regarding the shrink-swell of this soil type (NRCS 2018). The geotechnical report noted that soils of the study area (Argonaut-Auburn complex) do not have a high potential for shrink and swell (Geocon 2017). The proposed project would be designed to meet seismic safety requirements specified in the California Building Code, including standards to minimize impacts from expansive soils. Therefore, impacts related to the potential hazards of construction on expansive soils would be less than significant, and no mitigation would be required.

Question e: No Impact. The proposed project would tie into the City of Folsom's wastewater system and no on-site wastewater disposal would occur. No significant impacts from or to geophysical features or hazards would occur with implementation of the proposed project and no mitigation is required.

VII. GREENHOUSE GAS EMISSIONS

GREENHOUSE GAS EMISSIONS:				
Would the project:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

HELIX Environmental Planning, Inc. completed the City’s Greenhouse Gas Reduction Strategy Consistency Checklist for the proposed project. This checklist is presented in **Appendix B**.

Environmental Setting

Climate change refers to any significant change in measures of climate, such as average temperature, precipitation, or wind patterns over a period of time. Climate change may result from natural factors, natural processes, and human activities that change the composition of the atmosphere and alter the surface and features of the land. Significant changes in global climate patterns have recently been associated with global warming, which is an average increase in the temperature of the atmosphere near the Earth’s surface; this is attributed to an accumulation of greenhouse gas emissions (GHG) in the atmosphere. GHGs trap heat in the atmosphere which, in turn, increases the Earth’s surface temperature. Some GHGs occur naturally and are emitted to the atmosphere through natural processes, while others are created and emitted solely through human activities. The emission of GHGs through fossil fuel combustion in conjunction with other human activities appears to be closely associated with global warming.

GHGs, as defined under California’s Assembly Bill 32 (AB 32), include carbon dioxide (CO₂), methane (CH₄), nitrous oxide (N₂O), hydrofluorocarbons (HFC), perfluorocarbons (PFC), and sulfur hexafluoride (SF₆). General discussions on climate change often include water vapor, ozone, and aerosols in the GHG category. Water vapor and atmospheric ozone are not gases that are formed directly in the construction or operation of development projects, nor can they be controlled in these projects. Aerosols are not gases. While these elements have a role in climate change, they are not considered by either regulatory bodies, such as CARB, or climate change groups, such as the Climate Registry, as gases to be reported or analyzed for control. Therefore, no further discussion of water vapor, ozone, or aerosols is provided.

GHGs vary widely in the power of their climatic effects; therefore, climate scientists have established a unit called global warming potential (GWP). The GWP of a gas is a measure of both potency and lifespan in the atmosphere as compared to CO₂. For example, since CH₄ and N₂O are approximately 25 and 298 times more powerful than CO₂, respectively, in their ability to trap heat in the atmosphere, they have GWPs of 25 and 298, respectively (CO₂ has a GWP of 1). Carbon dioxide equivalent (CO₂e) is a quantity that enables all GHG emissions to be considered as a group despite their varying GWP. The GWP of each GHG is multiplied by the prevalence of that gas to produce CO₂e. The atmospheric lifetime and GWP of selected GHGs are summarized in **Table 10**.

- Proposed project incorporates all applicable GHG reduction measures (as documented in the Climate Change Technical Appendix to the General Plan EIR) as mitigation measures in the CEQA document prepared for the project; and,
- Proposed project clearly demonstrates the method, timing and process for which the project will comply with applicable GHG reduction measures and/or conditions of approval, (e.g., using a CAP/GHG reduction measures consistency checklist, mitigation monitoring and reporting plan, or other mechanism for monitoring and enforcement as appropriate).

Question a: Less than Significant Impact with Mitigation. GHG emissions would be generated by the project during construction (vehicle engine exhaust from construction equipment, on-road hauling trucks, vendor trips, and worker commuting trips) and during long-term operation (electricity and natural gas use, electricity resulting from water consumption; solid waste disposal, and vehicle engine exhaust). To determine significance of the project's GHG emissions, the City's Greenhouse Gas Reduction Strategy Consistency Checklist was completed (City of Folsom 2021a; included as **Appendix B**):

Part 1: Land Use Consistency

The proposed project is consistent with the City's 2035 General Plan land use and zoning designations?

Both project parcels are designated as Industrial/Office Park (IND) in the Folsom 2035 General Plan. The project proposes a General Plan Amendment to change the land use designation for both parcels to multi-family high density residential (MHD). Current zoning for Lot 1 is Limited Manufacturing Planned Development (M-L, PD), and current zoning for Lot 6 is Business and Professional Planned Development (B-P, PD). The proposed project would require a rezone at Lot 1 from M-L to R-4, and a rezone at Lot 6 from B-P to R-4. The Planned Development combining zone would remain. In accordance with the Greenhouse Gas Reduction Strategy Consistency Checklist, if the project would require a change in land use designation or a rezone, consistency is determined by calculating the estimated the GHG emissions resulting from maximum buildout of the project site allowed using the current zoning and using the proposed zoning change. If the land use designation/zoning change would not result in an increase in annual GHG emissions, the project would be consistent (City 2021a).

An office building would be an allowable use for both the M-L and B-P zones. The maximum allowable lot coverage for an office building is 60 percent and a maximum of two stories are allowed. The resulting maximum buildout of both project parcels under the existing zoning would be office buildings totaling 623,600-SF of floor space. Using CalEEMod and all model defaults, 623,600-SF of general office building would result in approximately 6,075-MT CO₂e per year.

Under the proposed land use designation/zoning, one apartment per 1,700-SF of lot area would be allowed, resulting in a maximum buildout of 304 apartments. Using CalEEMod and model defaults, 304 low-rise apartments would result in approximately 2,431-MT CO₂e per year. This would be 60 percent lower than the GHG emissions for maximum buildout under the current land use designation/zoning, and the project would be consistent with GHG emissions

generated by buildout of the 2035 General Plan. The CalEEMod output files are included in **Appendix B.**

Part 2: GHG Reduction Measures Consistency (only applicable measures shown):

E-1 Building energy Sector: The project will exceed the requirements of the 2016 California Building Energy Efficiency Standards (Title 24, Part 6) by 15 percent or more?

Consistent. The project would meet the requirement of the 2019 California Building Energy Efficiency Standards (Title 24, Part 6), including the requirements for onsite photovoltaic electricity generations (solar panels). According to the California Energy Commission (CEC), once rooftop solar electricity generation is factored in, homes built under the 2019 standards will use about 53 percent less energy than those under the 2016 standards (CEC 2018).

T-1 Mix of Uses: The project is a mixed-use building with two or more uses (i.e., residential, commercial, office, etc.) or if the site is 5-acres or larger there are two or more uses on the site connected by protected pedestrian paths (e.g., sidewalks, elevated walkways) excluding driveways?

Consistent. The project is larger than 5-acres and is located within the Folsom Corporate Center. With implementation of the project, the Folsom Corporate Center would contain a mix of uses including residential, office, medical office, and light manufacturing/research and development. Sidewalks and/or pedestrian paths would connect the project residences with adjacent land uses.

T-3 Bicycle Parking: Project provides five percent more bicycle parking spaces than required in the City's Municipal Code?

Consistent with mitigation. Mitigation Measure GHG-1 would require the installation of bicycle parking 5 percent or more higher than the requirements of City Code section 17.57.090 (for a total of 54 bicycle parking spaces).

T-6 High-Performance Diesel (Construction only): Use high-performance diesel (also known as Diesel-HPR or Reg-9000/RHD) for construction equipment?

Consistent with mitigation. Mitigation Measure GHG-2 would require the use of high-performance diesel for all project construction activities.

T-8 Electric Vehicle Charging (Residential): For multifamily projects with 17 or more dwelling units, provide electric vehicle charging in five percent of total parking spaces?

Consistent with mitigation. Mitigation Measure GHG-3 would require installation of electrical vehicle charging stations in a minimum of five percent of the total parking spaces on the project site.

SW-1 Enhanced Construction Waste Diversion: Project diverts to recycle or salvage at least 65 percent of nonhazardous construction and demolition waste generated at the project site in accordance with Appendix A4 (Residential) of CALGreen?

Consistent with mitigation. Mitigation Measure GHG-4 would require a minimum of 65 percent

of nonhazardous construction and demolition waste to be diverted, recycled or salvaged.

W-1 Water Efficiency: For new residential and non-residential projects, the project will comply with all applicable indoor and outdoor water efficiency and conservation measures required under CALGreen Tier 1?

Consistent with mitigation. Mitigation Measure GHG-5 would require implementation of all 2019 CALGreen Tier 1 applicable indoor and outdoor water efficiency and conservation measures.

With implementation of Mitigation Measures GHG-1 through -5, the project would be consistent with the City's GHG Strategy. Therefore, the project would not generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment, and the impact would be less than significant with mitigation.

Mitigation Measure GHG-1: Bicycle Parking

In accordance with the City General Plan GHG Reduction Measure T-3, the project shall provide a minimum of five percent more bicycle parking than required in the City's Municipal Code Section 17.57.090 (for a total of 54 bicycle parking spaces).

Mitigation Measure GHG-2: High-Performance Diesel

In accordance with the City General Plan GHG Reduction Measure T-6, the project shall use high-performance diesel (also known as Diesel-HPR or Reg-9000/RHD) for all diesel-powered equipment utilized in construction of the project.

Mitigation Measure GHG-3: Electric Vehicle Charging

In accordance with the City General Plan GHG Reduction Measure T-8, the project shall provide electric vehicle charging stations in five percent of the total surface parking spaces on the project site (for a total of 16 EV charging stations).

Mitigation Measure GHG-4: Enhanced Construction Waste Diversion

In accordance with the City General Plan GHG Reduction Measure SW-1, the project shall divert to recycle or salvage a minimum 65 of nonhazardous construction and demolition waste generated at the project site in accordance with Appendix A4 (Residential) of the as outlined in the California Green Building Standards Code (2019 CALGreen).

Mitigation Measure GHG-5: Water Efficiency

In accordance with the City General Plan GHG Reduction Measure W-1, the project shall comply with all applicable indoor and outdoor water efficiency and conservation measures required under 2019 CALGreen Tier 1, as outlined in the California Green Building Standards Code.

Question b: Less than Significant Impact. There are numerous State plans, policies, and regulations adopted for the purpose of reducing GHG emissions. The principal overall State plan and policy is AB 32, the California Global Warming Solutions Act of 2006. The quantitative goal of AB 32 is to reduce GHG

emissions to 1990 levels by 2020. SB 32 would require further reductions of 40 percent below 1990 levels by 2030. The mandates of AB 32 and SB 32 are implanted at the state level by the CARB's Scoping Plan. Because the project's operational year is post-2020, the project aims to reach the quantitative goals set by SB 32. Statewide plans and regulations such as GHG emissions standards for vehicles (AB 1493), the LCFS, and regulations requiring an increasing fraction of electricity to be generated from renewable sources are being implemented at the statewide level; as such, compliance at the project level is not addressed. Therefore, the proposed project would not conflict with those plans and regulations.

The Metropolitan Transportation Plan/Sustainable Communities Strategy (MTP/SCS) for Sacramento County is the 2020 MTP/SCS adopted by the Sacramento Area Council of Governments (SACOG) on November 18, 2019. The 2020 MTP/SCS lays out a transportation investment and land use strategy to support a prosperous region, with access to jobs and economic opportunity, transportation options, and affordable housing that works for all residents. The plan also lays out a path for improving our air quality, preserving open space and natural resources, and helping California achieve its goal to reduce greenhouse gas emissions (SACOG 2019). The transportation sector is the largest source of GHG emissions in the state. A project's GHG emissions from cars and light trucks are directly correlated to the project's vehicle miles traveled (VMT). According to the Transportation Impact Study prepared for the project, the project is anticipated to generate 18 percent less VMT per capita than the regional residential average (T. Kear Transportation Planning and Management, Inc. 2021). This VMT reduction exceeds the 15 percent reduction required by SB 743. In addition to regional VMT projections, SACOG utilizes local growth projections to develop the strategies and measures in the 2020 MTP/SCS. As discussed in question a), above, the change in land use and zoning would result in lower maximum potential GHG emissions compared to current General Plan land use/growth assumptions. Therefore, the regional VMT and population growth resulting from implementation of the project would be consistent with the assumptions used in the 2020 MTP/SCS.

As discussed in question a), above, with implementation of Mitigation Measures GHG-01 through GHG-05, the project would be consistent with the City's GHG Strategy, a qualified plan for the reduction of greenhouse gases pursuant to CEQA Guidelines Section 15183.5. Therefore, the project would not conflict with CARB's 2017 Scoping Plan, the SACOG's 2020 MTP/SCS, or the City's GHG Strategy, and the impact would be less than significant with mitigation.

VIII. HAZARDS AND HAZARDOUS MATERIALS

HAZARDS AND HAZARDOUS MATERIALS:				
Would the project:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

Lot 1 and Lot 6 are currently undeveloped and have no past land uses associated with potentially hazardous sites. The schools located nearest to the project site are: Folsom High School, located approximately 1-mile west of the project site; Sandra J. Gallardo Elementary School, located approximately 1.20-miles west of the project site; and, Gold Ridge Elementary School, located 0.3-mile north of the project site.

The following databases were reviewed for the project site and surrounding area to identify potential hazardous contamination sites: the US EPA's EnviroStor website database (EPA 2021); and the US EPA's Superfund National Priorities List (EPA 2021). Based on the results of the databases reviewed, the project site is not listed as a hazardous waste site.

Federal and state laws include provisions for the safe handling of hazardous substances. The federal Occupational Safety and Health Administration (OSHA) administers requirements to ensure worker safety. Construction activity must also be in compliance with the California OSHA regulations (Occupational Safety and Health Act of 1970).

Evaluation of Hazards and Hazardous Materials

Question a, b, c: Less than Significant Impact. No existing hazardous materials have been identified on the project site, and the site has no history of past land uses associated with potentially hazardous sites. Development of the project site from undeveloped to residential land uses would result in an increase in the generation, storage, and disposal of hazardous wastes. During project construction, oil, gasoline, diesel fuel, paints, solvents, and other hazardous materials may be used. If spilled, these substances could pose a risk to the environment and to human health.

Following construction, household hazardous materials such as various cleansers, paints, solvents, pesticides, pool chemicals, and automobile fluids would be expected to be used. The routine transport, use, and disposal of hazardous materials are subject to local, state, and federal regulations to minimize risk and exposure. The potential risk of exposure or impacts from transport, use, and disposal of hazardous materials to schools and other nearby sensitive receptors would be minimized by implementation of regulations.

Further, the City has set forth its hazardous materials goals and policies in the Hazardous Materials Element of the General Plan. The policies protect the health and welfare of residents of Folsom through management and regulation of hazardous materials in a manner that focus on preventing problems. Additionally, the routine transport, use, and disposal of hazardous materials are subject to state and federal regulations to minimize risk and exposure. The potential for risks associated with the accidental release of hazardous materials during routine transport, use, or disposal would be less than significant for questions a) through c).

Question d: No Impact. The project site is not included on the lists of hazardous materials sites compiled and available on EnviroStor (California Department of Toxic Substances Control 2021) or the US EPA's Superfund National Priorities List (EPA 2021). Therefore, no significant hazard to the public or environment would result with project implementation. No impact would occur, and no mitigation is necessary.

Question e, f: No Impact. The project site is not located in an Airport Land Use Plan area, and no public or private airfields are within 2-miles of the project site; therefore, the project would not result in a safety hazard for people residing or working in the project area. No impact would occur, and no mitigation is necessary for questions e) and f).

Question g: Less than Significant Impact. The City of Folsom published an Evacuation Plan in 2020 (City of Folsom 2020). The project site is located in Evacuation Zone 31. Iron Point Road, which is located north of Lot 1 and Lot 6, is considered a minor evacuation route. No major evacuation routes occur within the vicinity of the project site. No aspect of the proposed project would modify traffic control points within Evacuation Zone 31 or preclude their continued use as an emergency evacuation route. The proposed project would not result in an increased concentration of large numbers of persons in any at-risk location, and the proposed project would not have a significant impact on any emergency plans. Thus, no significant impact would occur, and no mitigation would be necessary.

Question h: Less than Significant Impact. The project site is located in the City of Folsom, and it is provided by urban levels of fire protection by the City. Therefore, the proposed project would not increase the risk of wildland fires. No significant impact would occur, and no mitigation is necessary.

IX. HYDROLOGY AND WATER QUALITY

HYDROLOGY AND WATER QUALITY:					
Would the project:		Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a)	Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b)	Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c)	Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d)	Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e)	Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f)	Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g)	Place housing within a 100-year flood hazard area as mapped on a federal flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h)	Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i)	Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
j)	Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

A Preliminary Water Quality Report was prepared by RSC Engineering to develop sizing of stormwater management infrastructure for Lot 1 and Lot 6. Water Quality Reports are incorporated by reference and included as **Appendix F**.

Environmental Setting

There are no existing aquatic resources or constructed stormwater management facilities on the project site. North of Lot 1 is an existing collection of oak trees. To the northwest of Lot 1 is the Revel Senior Living Apartment Complex and Country House Memory Care Facility. The land west of Lot 1 is zoned as a General Commercial District (C-3) Planned Development and populated by an office park. Vacant, undeveloped land that is a proposed medical office building lies east of Lot 1, and Micron Technology office park is northeast of the site. These land uses also serve as the western border for Lot 6. A small man-made pond lies east of Lot 6, in an area zoned for Limited Manufacturing. The land north of Lot 6 includes existing residential development, and the land south of Lot 6 includes an existing SAFE Credit Union.

Precipitation is the only apparent source of surface water for the project site. No developed storm drainage features are constructed on the project site. Because the project site is currently undeveloped, implementation of the project would result in an increase of impervious surface area and channelization of storm water runoff, the rates and volumes of which would increase. However, this is a normal consequence associated with development, and as shown in the preliminary grading plans for the project, the drainage patterns would be designed to not impact adjoining properties. Stormwater management features for the proposed project include: bioretention basins, Contech stormwater filters, and disconnected roof drains.

The multiple drainage management areas in Lot 1 would encompass the apartment buildings, pavement areas, pool, and amenity areas. The drainage areas direct the runoff to the proposed stormwater quality facilities by an onsite storm drain system. The stormwater quality facilities used (bio retention or Contech storm filters as appropriate) will be in accordance with City of Folsom requirements.

Lot 6 would be separated into multiple drainage management areas that would encompass the carports, parking areas, apartment buildings, pool, and amenity areas. The drainage areas direct the runoff to the stormwater quality facilities by an onsite storm drain system. The stormwater quality facilities used (bio retention, disconnected roof drain or Contech storm filters as appropriate) will be in accordance with City of Folsom requirements.

The on-site stormwater control system would tie-in to an existing stormwater stub at each site. The project would incorporate standard best management practices (BMP) to maintain existing water quality in accordance with City regulations.

Federal Emergency Management Agency (FEMA) flood insurance rate maps were reviewed for the project's proximity to a 100-year floodplain. The proposed project is on FEMA panel 06067C0119H, effective August 16, 2012 (FEMA 2012). The project site is not located within a 100-year floodplain.

Neither of the parcels are located in an area of important groundwater recharge. Domestic water in the City is provided solely by surface water sources. The City is the purveyor of water to the area in which the project is located.

Regulatory Framework Relating to Hydrology and Water Quality

The City is a signatory to the Sacramento Countywide National Pollutant Discharge Elimination Program (NPDES) permit for the control of pollutants in urban stormwater. Since 1990, the City has been a

partner in the Sacramento Stormwater Quality Partnership, along with the County of Sacramento and the Cities of Sacramento, Citrus Heights, Elk Grove, Galt, and Rancho Cordova. These agencies are implementing a comprehensive program involving public outreach, construction and industrial controls (i.e., BMPs), water quality monitoring, and other activities designed to protect area creeks and rivers. This program would be unchanged by the proposed project, and the project would be required to implement all appropriate program requirements.

In addition to these activities, the City maintains the following requirements and programs to reduce the potential impacts of urban development on stormwater quality and quantity, erosion and sediment control, flood protection, and water use. These regulations and requirements would be unchanged by the proposed project.

Standard construction conditions required by the City include:

- *Water Pollution* – requires compliance with City water pollution regulations, including NPDES provisions.
- *Clearing and Grubbing* – specifies protection standards for signs, mailboxes, underground structures, drainage facilities, sprinklers and lights, trees and shrubbery, and fencing. Also requires the preparation of a Stormwater Pollution Prevention Plan (SWPPP) to control erosion and siltation of receiving waters.
- *Reseeding* – specifies seed mixes and methods for reseeding of graded areas.

Additionally, the City enforces the following requirements of the Folsom Municipal Code as presented in **Table 11**.

Table 11. City of Folsom Municipal Code Sections Regulating the Effects on Hydrology and Water Quality from Urban Development

CODE SECTION	CODE NAME	EFFECT OF CODE
8.70	Stormwater Management and Discharge Control	Establishes conditions and requirements for the discharge of urban pollutants and sediments to the storm-drainage system; requires preparation and implementation of Stormwater Pollution Prevention Plans.
13.26	Water Conservation	Prohibits the wasteful use of water; establishes sustainable landscape requirements; defines water use restrictions.
14.20	Green Building Standards Code	Adopts the California Green Building Standards Code (CALGreen Code), 2010 Edition, excluding Appendix Chapters A4 and A5, published as Part 11, Title 24, C.C.R. to promote and require the use of building concepts having a reduced negative impact or positive environmental impact and encouraging sustainable construction practices.
14.29	Grading Code	Requires a grading permit prior to the initiation of any grading, excavation, fill or dredging; establishes standards, conditions, and requirements for grading, erosion control, stormwater drainage, and revegetation.
14.32	Flood Damage Prevention	Restricts or prohibits uses that cause water or erosion hazards, or that result in damaging increases in erosion or in flood heights; requires that uses vulnerable to floods be protected against flood damage; controls the modification of floodways; regulates activities that may increase flood damage or that could divert floodwaters.
14.33	Hillside Development	Regulates urban development on hillsides and ridges to protect property against losses from erosion, ground movement and flooding; to protect significant natural features; and to provide for functional and visually pleasing development of the city's hillsides by establishing procedures and standards for the siting and design of physical improvements and site grading.

Source: Folsom Municipal Code July 2011

Evaluation of Hydrology and Water Quality

Questions a, c, d, e, f: Less than Significant Impact. Ground disturbing activities associated with construction of the proposed project would include additional clearing and grading the project site. Modifications to the existing drainage patterns may result in localized flooding, and an increase in impervious surfaces may result in an increase in the total volume and peak discharges of stormwater runoff which may contribute to downstream erosion and flooding. Construction of the proposed project has the potential to degrade water quality associated with urban runoff. Ground disturbing activities would expose soil to erosion and may result in the transport of sediments which could adversely affect water quality.

Modifications to the onsite drainage resulting in on-or off-site erosion, pollutants, flooding, and/or otherwise substantially degrade water quality would be a potentially significant impact. The proposed project would be required to comply with various State and local water quality standards which would ensure the proposed project would not violate water quality standards or waste discharge permits, or otherwise substantially degrade water quality. As the project is greater than one acre, the proposed project would be subject to NPDES permit conditions which include the preparation of a SWPPP for implementation during construction. As described above, the proposed project would also be subject to all of the City's standard Code requirements, including conditions for the discharge of urban pollutants and sediments to the storm drainage system, and restrictions on uses that cause water or erosion hazards.

Further, prior to the issuance of grading and building permits, the applicant would be required to submit to the City a drainage plan that shows how project BMPs capture storm water runoff during project operations. Compliance with these requirements would ensure that water quality standards and discharge requirements are not violated, and water quality is protected. Impacts would be less than significant, and no mitigation would be necessary for questions a), c), d), e), and f).

Question b: Less than Significant Impact. Implementation of the proposed project would not result in the use of groundwater, because domestic water in the City is provided solely from surface water sources from the Folsom Reservoir. While the proposed project would result in additional impervious surfaces on the site that could affect groundwater recharge, the site is not known to be important to groundwater recharge. Further, because the proposed project would not rely on groundwater for domestic water and irrigation purposes, and because the site is not an important area of groundwater recharge, the proposed project would not deplete groundwater supplies or interfere substantially with groundwater recharge that would result in a net deficit in aquifer volume or a lowering of the local groundwater table. Therefore, no significant impacts would occur, and no mitigation would be necessary.

Question g and h: No Impact. Because the project site is located outside of a 100-year floodplain, development of the proposed project would not place persons or structures at risk from flood hazards, nor would it interfere with existing floodway capacity. Thus, no impacts would occur, and no mitigation would be necessary for questions g) and h).

Question i: Less than Significant Impact. The proposed project would not expose new development to inundation in the event of the failure of a dam. Should either of the City's two main dams (Folsom Lake and Mormon Island) fail, failure would most likely occur with adequate warning to evacuate residents. The project is required to adhere to City established evacuation plans as outlined in the City of Folsom Evacuation Plan (City of Folsom 2020) reviewed by the Reclamation District that establish protocol in the event of the dam failure. With implementation of the evacuation plan in the event of the failure of a dam, the impact would be less than significant, and no mitigation would be necessary.

Question j: Less than Significant Impact. The City of Folsom is located approximately 95-miles from the Pacific Ocean, at elevations ranging from approximately 140- to 828-feet amsl. Due to the distance and higher elevation, inundation by tsunami would not occur. The City is located adjacent to Folsom Lake, a reservoir of the American River impounded by a main dam on the river channel and wing dikes. Areas of the City adjacent to the wing dikes could be adversely affected by a seiche as a result of an earthquake, either through sloshing within a full reservoir or by a massive landslide or earth movement into the lake. Although historic seismic activity has been minor, the potential for strong ground shaking is present and

the possibility exists of a strong earthquake occurring when lake levels are high. This could create a large enough wave to overtop or breach the wing dikes although this is considered to be a remote possibility.

Mudslides and other forms of mass wasting occur on steep slopes in areas having susceptible soils or geology, typically as a result of an earthquake or high rainfall event. Slopes associated with the edges of the building pads are located on the project site; however, City grading standards, including requirements to evaluate slope stability and implement slope stabilizing measures as necessary, would prevent this potential effect. In summary, there would be no potentially significant effect from inundation by seiche, tsunami, or mudflow and no mitigation would be necessary.

X. LAND USE AND PLANNING

LAND USE AND PLANNING:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

Land use in the project area is regulated by the City of Folsom through the various plans and ordinances adopted by the City. These include the City of Folsom General Plan and the City of Folsom Municipal Code, including the Zoning Code. The General Plan currently identifies Lot 1 as Industrial/Office Park (IND), and zoned for Limited Manufacturing, Planned Development District (M-L PD). The General Plan currently identifies Lot 6 as IND, and zoned for Business Park, Planned Development District (B-P PD).

The proposed project includes a General Plan Amendment to change the land use designation in Lot 1 and Lot 6 from Industrial (IND) to Multi-Family, High Density (MHD); as well as a rezone from M-L PD to General Apartment, Planned Development District (R-4 PD) at Lot 1 and, and a rezone from B-P to M-4 at Lot 6. The Planned Development combining zone would remain.

A Planned Development Permit would be required because the proposed project is sited within a planned development overlay zoning designation. The Planned Development Permit would allow the City to review the site plan and associated project site details to ensure the project meets the standards and requirements beneficial to the City and its residents as defined in Section 17.38.100 of the Zoning Code.

Evaluation of Land Use and Planning

Question a: No Impact. Lot 1 is largely undeveloped, and is bordered by office buildings, oak woodland, and medical offices to the north, vacant land to the east, US Highway 50 and vacant land to the south, and commercial buildings, a memory care facility and undeveloped land to the west. Lot 6 is largely undeveloped and is bordered by Iron Point Road and residential development to the north, a constructed pond/wetland and office buildings to the east, office buildings and undeveloped land containing scattered oaks to the south, and office buildings to the west. Development of the project site would not physically divide an established community as various office space, vacant land, commercial land surrounds Lot 1 and Lot 6. The residential development located north of Lot 1 and Lot 6 would not be altered. Therefore, there would be no impact and no mitigation is required.

Question b: Less than Significant. The development standard for Planned Development (PD) is that the proposed project must be designed to provide open space, circulation, off-street parking, and other conditions in such a way as to form a harmonious, integrated project of sufficient quality to justify exceptions to the normal regulations of this title.

The project would require a General Plan Amendment to change the land use designation from IND in Lot 1 and Lot 6 to MHD in Lot 1 and Lot 6. A Rezone would be required for Lot 1 from M-L PD to R-4 PD, and for Lot 6 from B-P PD to R-4 PD. The General Plan Amendment and Rezone would be reviewed and approved by the City, and the project would be reviewed by the City for consistency with the proposed land use and zoning designations prior to the City issuing permits. The project would comply with these standards and not conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project; therefore, project-related impacts would be less than significant, and no mitigation would be necessary.

Question c: No Impact. No Habitat Conservation Plan or Natural Community Conservation Plan has been approved for the project area. Implementation of the proposed project would not conflict with any conservation plan. No impact would occur, and no mitigation is necessary.

XI. MINERAL RESOURCES

MINERAL RESOURCES:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The Folsom area regional geologic structure is defined by the predominantly northwest- to southeast-trending belt of metamorphic rocks and the strike-slip faults that bound them. The structural trend influences the orientation of the feeder canyons into the main canyons of the North and South Forks of the American River. This trend is interrupted where the granodiorite plutons outcrop (north and west of Folsom Lake) and where the metamorphic rocks are blanketed by younger sedimentary layers (west of Folsom Dam) (Geotechnical Consultants, Inc 2003).

The presence of mineral resources within the City has led to a long history of gold extraction, primarily placer gold. No areas of the City are currently designated for mineral resource extraction.

Evaluation of Mineral Resources

Questions a, b: No Impact. The proposed project is not located in a zone of known mineral or aggregate resources (CDC 2021). No active mining operations are present on or near the site. Implementation of the project would not interfere with the extraction of any known mineral resources. Thus, no impacts would result, and no mitigation would be necessary.

XII. NOISE

NOISE:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a) Generation of a substantial temporary or permanent increase in the ambient noise levels in the vicinity of the project in excess of standards established in the local General Plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) For a project located in the vicinity of a private airstrip or an airport land use plan, or where such a plan has not been adopted, within two miles of a public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

The existing noise environment in the vicinity of the project site is dominated by vehicular traffic, primarily on US Highway 50, approximately 100-feet south of the project Lot 1, and Iron Point Road, approximately 20-feet north of the project Lot 6. Other noise sources include ambient urban noise sources (e.g., parking lots; heating, ventilation and air conditioning [HVAC] systems) associated with the commercial/industrial developments within the Folsom Corporate Center, including: the Kaiser Permanente medical offices on the north side of the project lot 1; Micron Technology between the project Lot 1 and Lot 6; and the SAFE Credit Union corporate office south of the project Lot 6.

Noise-sensitive land uses are land uses that may be subject to stress and/or interference from excessive noise, including residences, hospitals, schools, hotels, resorts, libraries, sensitive wildlife habitat, or similar facilities where quiet is an important attribute of the environment. Noise receptors (receivers) are individual locations that may be affected by noise. Noise-sensitive land uses in the project vicinity include multi-family residences across Iron Point Road, approximately 850-feet north of the project Lot 1 and approximately 160-feet north of the project Lot 6; and senior living apartments approximately 380 feet west of the project Lot 1.

An ambient noise survey for Lot 1 was conducted by Bollard Acoustical Consultants on February 4, 2021. A 24-hour measurement was taken with the microphone place between the proposed Lot 1 pool and building 1, approximately 210-feet from the centerline of US Highway 50. The result of the measurement was 66 dBA L_{DN}. The measurement was taken approximately 5-feet above existing ground level and does not account for project grading which would change ground level noise from US Highway 50 (Bollard 2021). The letter summarizing the noise survey is included as **Appendix G**.

Noise Metrics

All noise-level and sound-level values presented herein are expressed in terms of decibels (dB), with A weighting, abbreviated "dBA," to approximate the hearing sensitivity of humans. Time averaged noise

levels of one hour are expressed by the symbol “ L_{EQ} ” unless a different time period is specified. The Community Noise Equivalent Level (CNEL) is a 24-hour average, where noise levels during the evening hours of 7:00 p.m. to 10:00 p.m. have an added 5 dBA weighting, and sound levels during the nighttime hours of 10:00 p.m. to 7:00 a.m. have an added 10 dBA weighting. This is similar to the Day Night sound level (L_{DN}), which is a 24-hour average with an added 10 dBA weighting on the same nighttime hours but no added weighting on the evening hours.

Because decibels are logarithmic units, noise levels cannot be added or subtracted through standard arithmetic. Under the decibel scale, a doubling of sound energy corresponds to a 3 dBA increase. In other words, when two identical sources are each producing sound of the same loudness, the resulting sound level at a given distance would be 3 dBA higher than from one source under the same conditions. For example, if one automobile produces a sound pressure level (S_{PL}) of 70 dBA when it passes an observer, two cars passing simultaneously would not produce 140 dBA—rather, they would combine to produce 73 dBA. Under the decibel scale, three sources of equal loudness together produce a sound level 5 dBA louder than one source.

Under controlled conditions in an acoustical laboratory, the trained, healthy human ear is able to discern 1 dBA changes in sound levels, when exposed to steady, single-frequency (“pure-tone”) signals in the mid-frequency (1,000 Hertz [Hz]–8,000 Hz) range. In typical noisy environments, changes in noise of 1 to 2 dBA are generally not perceptible. It is widely accepted, however, that people begin to detect sound level increases of 3 dB in typical noisy environments. Further, a 5 dBA increase is generally perceived as a distinctly noticeable increase, and a 10 dBA increase is generally perceived as a doubling of loudness.

Vibration Metrics

Groundborne vibration consists of rapidly fluctuating motions or waves transmitted through the ground with an average motion of zero. Sources of groundborne vibrations include natural phenomena and anthropogenic causes (e.g., explosions, machinery, traffic, trains, construction equipment). Vibration sources may be continuous (e.g., factory machinery) or transient (e.g., explosions). Peak particle velocity (PPV) is commonly used to quantify vibration amplitude. The PPV, with units of inches per second (in/sec), is defined as the maximum instantaneous positive or negative peak of the vibration wave. Decibels are also used to compress the range of numbers required to describe vibration. Vibration velocity level (L_v) with units of VdB are commonly used to describe vibrations from transit sources.

Regulatory Framework

Noise Element

The Safety and Noise Element of the City of Folsom General Plan regulates noise emissions from public roadway traffic on new development of residential or other noise sensitive land uses. Policy SN 6.1.2 and Table SN-1 provide noise compatibility standards for land uses. For multi-family residential uses, noise due to traffic on public roadways, railroad line operations, and aircraft shall be reduced to or below 65 CNEL for outdoor activity areas and 45 CNEL for interior use areas (City 2021).

Policy SN 6.1.8 requires construction projects and new development anticipated to generate a significant amount of vibration to ensure acceptable interior vibration levels at nearby noise-sensitive uses based on Federal Transit Administration criteria. Table SN-3 provides vibration impact criteria. For

construction with infrequent vibration events, impacts would be significant if residences are subject to ground borne vibrations in excess of 80 VdB (City 2021).

Noise Ordinance

For stationary noise sources, the City has adopted a Noise Ordinance as Section 8.42 of the City Municipal Code (City of Folsom 1993). The Noise Ordinance establishes hourly noise level performance standards that are most commonly quantified in terms of the one-hour average noise level (L_{EQ}). Using the limits specified in Section 8.42.040 of the Noise Ordinance, noise levels generated by the project would be significant if they exceed 50 dBA L_{EQ} from 7:00 a.m. to 10:00 p.m. and 45 dBA L_{EQ} from 10:00 p.m. to 7:00 a.m. at off-site residential property boundaries. Noise from the project's air conditioning systems would be significant if exterior noise levels exceed 50 dBA, per Section 8.42.070 of the City Municipal Code. Section 8.42.060 exempts construction noise from these standards provided that construction does not occur before 7:00 a.m. or after 6:00 p.m. on weekdays, or before 8:00 a.m. or after 5:00 p.m. on Saturday or Sunday (City 1993).

Question a: Less than Significant with Mitigation

Construction Noise

Project construction noise was analyzed using the U.S. Department of Transportation (USDOT) Roadway Construction Noise Model ([RCNM]; USDOT 2008), which utilizes estimates of sound levels from standard construction equipment.

The nearest NSLUs to the project site area, single-family homes approximately 160 feet north of the project Lot 6. Heavy earthmoving equipment would have the potential to be used along the project's periphery, closest to NSLUs, including rubber-tired dozers, backhoes, excavators, graders, and scrapers. The noisiest construction equipment anticipated to be used near NSLUs would be a grader used during grading. Modeling shows that the noise from a grader would result in 70.9 dBA L_{EQ} at the closest residential property. Because construction equipment would be mobile as it moves across the project site, the noise level experienced by the neighboring uses would vary throughout the day. The modeling output for the grader and other anticipated construction equipment is included as **Appendix G**.

According to the City Code Section 8.42.060, noise sources associated with construction of the project which are conducted between the hours of 7:00 a.m. and 6:00 p.m., on Monday, Tuesday, Wednesday, Thursday, Friday and Saturday, and between 9:00 a.m. and 6:00 p.m. on Sunday, are exempt from the City noise standard (City 1993). Furthermore, the calculated short-term construction noise would be approximately 2 dBA higher than the calculated ambient traffic noise (see the off-site traffic noise discussions, below). A 2 dBA increase in ambient noise levels is generally not perceptible in typical outdoor environments and daytime construction noise increases would be less than significant. Nighttime construction noise is not anticipated for the project. However, nighttime construction is not exempt from the City Noise Ordinance and would exceed the nighttime standard of 45 dBA if it were to occur, resulting in a temporarily significant noise impact.

Off-Site traffic Noise

Modeling of the exterior noise environment for this report was accomplished using the Traffic Noise Model (TNM) version 2.5. TNM Version 2.5 was released in February 2004 by the U.S. Department of Transportation (USDOT) and calculates the daytime average hourly L_{EQ} from three-dimensional model

inputs and traffic data (USDOT 2004). The model-calculated one-hour L_{EQ} noise output is approximately equal to the CNEL (Caltrans 2009). The noise modeling input and output is included in **Appendix G**.

According to the Transportation Impact Study (TIS), the project is expected to generate approximately 1,376 daily trips and 104 trips during the PM peak hour (T. Kear 2021). Future traffic noise levels presented in this analysis are based on traffic volumes for five segments of Iron Point Road derived from intersection turning counts included in the TIS for four scenarios: existing (2021); existing plus project; cumulative (2035); and cumulative plus project. The traffic volumes for the five analyzed segments of Iron Point Road are included in **Appendix G**. Changes in traffic noise levels were calculated based on an average distance of 80 feet from the road centerline and adjacent residential land uses. The modeling does not account for intervening terrain or structures (e.g., sound walls, buildings).

The calculated off-site traffic noise levels are shown in **Table 12**. In typical outdoor environments, a 3 dBA increase in ambient noise level is considered just perceptible and a 5 dBA increase (a doubling of noise) is considered distinctly perceptible. In areas where existing or future ambient noise exceed the land use compatibility standards, an individual project's contribution to increases in ambient noise level could be considered significant if it exceeds 1.5 dBA. Because most of the areas along the analyzed road segments already exceed the land use noise compatibility standard listed in the city General Plan (60 dBA CNEL for low density residential; 65 dBA CNEL for multi-family residential and hotels, and 70 dBA for commercial), this analysis uses a threshold of a 1.5 dBA CNEL increase to be significant.

The maximum change in CNEL as a result of project-generated traffic would be 0.2 dBA CNEL, a change in ambient noise level that is lower than the threshold and is not discernable. Therefore, impacts related to the project generating a substantial permanent increase in ambient noise levels in the vicinity of the project in excess of General Plan standards from project-generated traffic would be less than significant.

Table 12: Off-Site Traffic Noise Levels

Roadway Segment	Existing (CNEL)	Existing + Project (CNEL)	Change in CNEL	2035 (CNEL)	2035 + Project (CNEL)	Change in CNEL
Iron Point Road.						
Grover Road to Oak Avenue Parkway	69.5	69.6	0.1	69.7	69.8	0.1
Oak Avenue Parkway to West Kaiser Access Road	68.8	69.0	0.2	71.1	71.2	0.1
West Kaiser Access Road to Rowberry Way	68.8	68.8	0.0	71.1	71.2	0.1
Rowberry Way to SAFE Credit Union Access	68.7	68.8	0.0	71.5	71.5	0.0
SAFE Credit Union Access to Broadstone Parkway	68.8	68.9	0.1	71.5	71.5	0.0

Source: TNM version 2.5

Heating, Ventilation, and Air Conditioning (HVAC)

The project includes the outdoor installation of HVAC units on the roof of the proposed project buildings. The units would be located behind a parapet wall of equal or greater height to the HVAC unit, which would provide substantial noise attenuation. Specific details on planned HVAC units were not available at the time of this analysis. A typical system for apartments in multi-story buildings would be a Carrier model 38BRC-024-34 2-ton system for each apartment which has a sound rating of 73.4 dBA S_{WL} . The closest NSLUs to project buildings systems would be the single-family homes across Iron Point Road from Lot 6. The minimum distance from potential HVAC systems and off-site residential property line would be approximately 160 feet. At 160-feet, an HVAC system producing 73.4 dBA S_{WL} would result in 35 dBA L_{EQ} , without considering reductions from the parapet walls. This noise level would not exceed the City Noise Ordinance daytime (50 dBA L_{EQ}) or nighttime (45 dBA L_{EQ}) maximum acceptable noise levels; and the impacts would be less than significant.

On-site Traffic Noise

Modeling of the exterior noise environment on the project site was accomplished using the Computer Aided Noise Abatement (CadnaA) model version 2021. The noise models used in this analysis were developed from Computer Aided Design (CAD) plans provided by the project architect. Input variables included, road alignment, elevation, area topography, projected traffic volumes, estimated truck composition percentages, and vehicle speeds. The one-hour L_{EQ} traffic noise level is calculated utilizing peak-hour traffic. The model-calculated one-hour L_{EQ} noise output is the equivalent to the CNEL (Caltrans 2009). The modeling includes the project buildings but does not account for terrain or off-site buildings and structures.

Traffic volumes on Iron Point Road were derived from the p.m. peak hour intersection turning counts reported in the TIS (T.Kear 2021). The truck composition for Iron Point Road was assumed to be typical for suburban streets: 3 percent medium trucks/busses and 1 percent heavy trucks. Traffic volumes and truck composition (2.7 percent medium trucks and 3.7 percent heavy trucks) for US-50 were modeled using data from the Caltrans traffic and truck counts for 2019 (Caltrans 2022).

Exterior Noise

As discussed above, the City General Plan Safety and Noise Element has established an exterior noise standard of 65 dBA CNEL for multi-family residential outdoor activity areas, defined as “[...] the patios or common areas where people generally congregate for multifamily development” (City 2021). The pool areas and patios surrounding the club houses would be the outdoor activity areas for the project. The modeling shows ground level noise for the clubhouse/pool area would be approximately 65 dBA CNEL in Lot 1 and 63 dBA CNEL in Lot 6. This noise level would not exceed the City exterior noise standard and the impact would be less than significant.

Interior Noise

Standard building design and construction using current building codes provides approximately 15 to 20 dBA of exterior to interior noise reduction with the windows and doors closed. The noise at the exterior facades for the project buildings was modeled for receptors on first, second, and third floors of all project residential buildings and is shown in **Table 13**.

Table 13: Building Exterior Noise Levels

Building	1 st Floor (CNEL)	2 nd Floor (CNEL)	3 rd Floor (CNEL)
Lot 1, Building 1	73.0	73.9	76.0
Lot 1, Building 2	72.5	71.8	73.4
Lot 1, Building 3	54.1	59.8	62.9
Lot 1, Building 4	56.8	58.1	60.8
Lot 1, Building 5	49.9	50.6	59.4
Lot 1, Building 6	52.7	54.7	57.1
Lot 1, Building 7	60.1	65.6	68.2
Lot 6, Building 1	62.6	62.9	63.3
Lot 6, Building 2	55.5	57.8	60.5
Lot 6, Building 3	71.0	71.0	70.7
Lot 6, Building 4	59.6	58.9	61.6
Lot 6, Building 5	65.0	68.2	68.5

Source: CadnaA version 2021

Buildings with exterior noise levels exceeding 65 dBA could result in interior noise levels in excess of the City General Plan Safety and Noise Element standard of 45 dBA CNEL. Lot 1 (buildings 1 and 2) and Lot 6 (building 3) would have exterior noise levels exceeding 70 dBA CNEL. Lot 1 building 7 and Lot 6 (building 5) would have exterior noise levels exceeding 65 dBA CNEL. Interior noise impacts would be potentially significant.

Exterior to interior noise reductions are dependent on the building exterior wall area, window area, door area, and room depth, which was not available at the time of this analysis. Calculations were made to estimate the minimum exterior wall and window sound transmissions class (STC) rating required for the project apartments to meet the City's interior noise standards. The calculations were based on an assumed typical 20-feet by 10-feet apartment room with two exterior walls, two windows measuring 3-feet by 5-feet and one sliding glass door measuring 5-feet by 7-feet. The calculation sheets are included in **Appendix G**. Lot 1 buildings 1 and 2 and, Lot 6 (building 3) would require exterior walls with line of sight to US Highway 50 or Iron Point Road to have a minimum STC 46 rating and widows/sliding glass doors to have a minimum STC 35 rating. Lot 1 building 7 and Lot 6 (building 5) would require windows/sliding glass doors to have a minimum STC 28 rating.

Impact Conclusion

Construction noise generated by the project would result in short-term substantial noise increases compared to baseline existing conditions. The implementation of Mitigation Measure NOI-1 would restrict construction to daytime and minimize noise levels to surrounding residential uses.

The addition of permanent project-generated traffic vicinity roadways would not result in a discernable increase in ambient noise levels. The project would expose residential land uses to noise levels that exceed compatibility guidelines in the General Plan and impacts would be potentially significant. The implementation of Mitigation Measure NOI-02 would ensure that noise reduction measures are included in building material specifications.

Long-term operation of project building HVAC systems would not result in noise levels exceeding the City noise ordinance standards, measured at the outdoor spaces of the closest NSLUs to the project site.

Mitigation Measure NOI-1: Construction Noise Reduction Measures

Construction activities shall be required to comply with the following and be noted accordingly on construction contracts:

1. **Construction hours/Scheduling:** The following are required to limit construction activities to the portion of the day when occupancy of the adjacent sensitive receptors are at the lowest:
 - a. Construction activities for all phases of construction, including servicing of construction equipment shall only be permitted during the hours of 7:00 a.m. and 7:00 p.m. Monday through Friday and between 9:00 a.m. to 5:00 p.m. on Saturdays. Construction is prohibited on Sundays and on all holidays.
 - b. Delivery of materials or equipment to the site and truck traffic coming to and from the site is restricted to the same construction hours specified above.
2. **Construction Equipment Mufflers and Maintenance:** All construction equipment powered by internal combustion engines shall be properly muffled and maintained.
3. **Idling Prohibitions:** All equipment and vehicles shall be turned off when not in use. Unnecessary idling of internal combustion engines is prohibited.
4. **Equipment Location and Shielding:** All stationary noise-generating construction equipment, such as air compressors, shall be located as far as practical from the adjacent homes. Acoustically shield such equipment when it must be located near adjacent residences.
5. **Quiet Equipment Selection:** Select quiet equipment, particularly air compressors, whenever possible. Motorized equipment shall be outfitted with proper mufflers in good working order.
6. **Staging and Equipment Storage:** The equipment storage location shall be sited as far as possible from nearby sensitive receptors.

Mitigation Measure NOI-2: On-site Interior Noise Level Reduction

For the project's habitable areas (both living rooms and bedrooms) with a direct line-of-sight to US Highway 50 for Lot 1 and Iron Point Road for Lot 2, the following measures shall be incorporated in the design of the project to reduce interior noise levels to 45 CNEL or less:

- Lot 1 (Buildings 1 and 2) and Lot 6 (Building 2) – minimum exterior wall requirement of STC 46.
- Lot 1 (Buildings 1 and 2) and Lot 6 (Building 2) – minimum window and glass sliding door requirement of STC 35.
- Lot 1 (Building 7) and Lot 6 (Building 5) – minimum window and glass sliding door requirement of STC 28.
- The building design shall include a mechanical ventilation system that meets the criteria of the International Building Code (Chapter 12, §1203.3 of the 2013 California Building Code) to ensure that windows would be able to remain permanently closed.

Question b: Less than Significant Impact. An on-site source of vibration during project construction would be a vibratory roller (primarily used to achieve soil compaction as part of the foundation and

paving construction), which could be used within approximately 160-feet of the single-family residences across Iron Point Road to the north. A large vibratory roller creates approximately 0.21 in/sec PPV at a distance of 25-feet, or 94.4 VdB. At a distance of 160-feet, a vibratory roller would create a PPV of 0.027 in/sec, or 77 VdB.¹ This would not exceed the City General Plan residential standard of 80 VdB for infrequent events. Once operational, the project would not be a source of groundborne vibrations. Impacts associated with construction-generated vibration would be less than significant. Therefore, the project would not result in the generation of excessive groundborne vibration or groundborne noise levels, and the impact would be less than significant.

Question c: Less than Significant Impact. The closest airports to the project site are the Cameron Park Airport, approximately 7.5-miles to the northeast, and Mather Airport, approximately 9.5-miles to the southwest. The project site is located within the review area identified in the Mather Airport Land Use Compatibility Plan (ALUCP). The project site is beneath the approach paths for runways 22 Left and 22 Right, however, the project site is not within the 60 dBA noise contour for the airport (Sacramento County Association of Governments 2020). Therefore, although the project site is subject to overflight by aircraft approaching and departing Mather Airport, the residents of the proposed project or people working in the project area would not be exposed to excessive levels of noise due to aircraft or airport operations, and the impact would be less than significant.

¹ Equipment PPV = Reference PPV * (25/D)ⁿ(in/sec), where Reference PPV is PPV at 25 feet, D is distance from equipment to the receptor in feet, and n= 1.1 (the value related to the attenuation rate through the ground); formula from Caltrans 2020. VdB = 20 * Log(PPV/4/10⁻⁶).

XIII. POPULATION AND HOUSING

POPULATION AND HOUSING:				
Would the project:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

The proposed project includes the total construction of 253 new multi-family apartment units on two separate parcels, Lot 1, and Lot 6.

Evaluation of Population and Housing

Question a: Less than Significant. Implementation of the project would result in the construction of 253 apartment units. The proposed project would accommodate the demand for housing and would not induce substantial growth in the City of Folsom. It is anticipated that the project would generate between 253 and 665 new residents (assuming 2.63 people per unit, based on projected household size in 2035 [City of Folsom 2018]). The projected household size is for single family homes, which is anticipated to be larger than the apartment units within Lot 1 and Lot 6. Existing infrastructure in the area would not be expanded or extended as a result of the project. Lot 1 and Lot 6 would require the addition of main access driveways and emergency access driveways along the parcel boundaries; however, this addition would not impact the existing roadways within the vicinity of the project site. Moreover, the population generated by the project is within the projected increase in population from planned growth as projected in the City’s Housing Element. The impact would not be significant, and no mitigation would be required.

Question b and c: No Impact. The proposed project would include the development of residential units on a currently undeveloped and vacant site. There are no existing residences on the project site; therefore, neither housing units nor people would be displaced, and no replacement housing would be required. There would be no impact and no mitigation would be necessary for questions b) and c).

XIV. PUBLIC SERVICES

PUBLIC SERVICES:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
a) Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

The proposed project is in an area currently served by urban levels of all utilities and services. Public services provided by the City of Folsom in the project area include fire, police, school, library, and park services. The site is served by all public utilities including domestic water, wastewater treatment, and storm water utilities.

The City of Folsom Fire Department provides fire protection services. There are four stations within the City of Folsom. Station 37 is nearest to the project site; it is located at 70 Clarksville Road, approximately 0.76 miles north of the project site. The Fire Department responds to over 6,000 requests for service annually with an average of 16.4 per day. The City of Folsom Police Department is located at 46 Natoma Street, approximately 3-miles northwest of the project site.

The project site is located within the Folsom Cordova Unified School District and is within the attendance area for the Gold Ridge Elementary School, Sutter Middle School, and Folsom High School. There are several parks near the project site, including Livermore Community, John Kemp Community Park, and Willow Hills Reservoir Community Park.

The Sacramento Municipal Utilities District (SMUD) would supply electricity to the project site. Pacific Gas & Electric (PG&E) provides natural gas to the area and would provide natural gas to the project site.

The City of Folsom has a program of maintaining and upgrading existing utility and public services within the City. Similarly, all private utilities maintain and upgrade their systems as necessary for public convenience and necessity, and as technology changes.

Evaluation of Public Services

Questions a, b, c, d, e: Less than Significant. The project site is within the urban area of Folsom, and there is no indication that public services are inadequate. The proposed project would increase fire and police protection service due to the addition of 253 apartment units, but the project would not substantially render the current service level to be inadequate. Additionally, the project would have the potential to increase service to schools and parks, but the project would be required to pay development impact fees as well as park fees in order to accommodate for the new development, as required by the City of Folsom. Construction and operation of the proposed project would not require the construction or expansion of parks and other public facilities or result in the degradation of those facilities. Because there are no unique aspects of the project that would render the current service level to be inadequate, no new public facilities would be necessary to serve the proposed project. The impact of the project would be less than significant, and mitigation would not be necessary.

XV. RECREATION

RECREATION:				
Would the project:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

The Folsom Parks and Recreation Department provides and maintains a full range of recreational activities and park facilities for the community. There are several parks near the project site, including the Livermore Community Park, John Kemp Community, and Willow Hills Reservoir Community Park. The proposed project would include on-site recreation facilities, including pools and clubhouses, dog parks, and sitting and picnic areas for use by the residents.

Evaluation of Recreation

Question a: Less than Significant. One component of the proposed project is to change the land use designation of Lot 1 and Lot 6 from commercial/industrial (IND) to residential (MHD). In total, the associated number of residents would not result in a substantial population increase to the City of Folsom population. An increase of 253 apartment units would generate between 253 and 665 new residents (assuming 2.63 people per unit, based on projected household size in 2035 [City of Folsom 2018]). The project proposes several recreational facilities on both parcels for use by the residences. Each apartment complex would have a pool, a fire pit, a dog park, a seating area, and a picnic area. The complex on Lot 1 would have a 3-story, 6,700 sf clubhouse, and the complex at Lot 6 would have a one-story, 3,150-sf clubhouse. The Folsom Municipal Code set a standard of 5-acres of parkland per 1,000 residents (City of Folsom Section 4.10.020). The City of Folsom Parks and Recreation Master Plan estimated that for a build-out population of 94,400 residents, there would be a total build-out of 586.6 acres of parkland (City of Folsom 2015).

Based on the projects distant location from a park and the addition of proposed recreational facilities that would be provided for the residents, the proposed project would not result in a substantial increase in the use of demand for neighborhood or regional parks, or other recreational facilities. Further, the City of Folsom charges impact fees to all new developments to abate a project’s impacts on parks and recreational facilities in the City. These impact fees are also used to address the identified future needs for the City’s park system. The impact fees and the associated funded improvements would reduce any impacts from the project to less than significant, and no mitigation would be necessary.

Question b: Less than Significant. The proposed project includes the construction of a pool, picnic area, dog park, and seating area within each apartment complex, for use by the residents. The complex on Lot

1 would have a 3-story, 6,700-sf clubhouse while the complex on Lot 6 would have a one-story, 3,150-sf clubhouse. The facilities would be for exclusive use by the residents of the proposed project. Additionally, the proposed project would be subject to park development impact fees established and collected by the City's Parks and Recreation Department to ensure that the City has sufficient park land. The construction of new recreational facilities and/or parks to meet the recreational demands of the City has been evaluated for environmental impacts through the General Plan process. Payment of the Parks and Recreation Department development impact fee offsets the potential for any significant impact related to recreation stemming from the proposed project and mitigation is not necessary. With the implementation of the impact fee, impacts to recreation would be less than significant.

XVI. TRANSPORTATION AND TRAFFIC

TRANSPORTATION AND TRAFFIC:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Conflict with adopted policies, plans or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Transportation and traffic were evaluated in the Folsom Corporate Center Apartments Transportation Impact Study as presented in **Appendix H**.

Environmental Setting**Study Scenarios**

Four scenarios were identified for inclusion in this Transportation Impact Study through consultation with City of Folsom staff. The study determines the weekday AM peak-hour and PM peak-hour level of service (LOS) at study intersections under the following scenarios:

1. Existing 2021 without Project Condition;
2. Existing 2021 with Project Condition;
3. Existing Plus Approved Projects (EPAP) 2026 without Project Condition;
4. EPAP 2026 with Project Condition;
5. Cumulative 2035 without Project Condition; and,
6. Cumulative 2035 with Project Condition.

Existing 2021, and Existing 2021 with Project Condition

Analysis of the existing condition reflects the traffic volumes and roadway geometry at the time the study began. These two scenarios (with and without the project) quantify performance measures, serve as a known reference point for those familiar with the study area, and identify project related impacts anticipated to occur if the project opened in 2021.

EPAP 2026 Condition, and EPAP 2026 with Project Condition

EPAP scenarios, with and without the project, analyze conditions with the addition of traffic from approved and reasonably foreseeable projects that affect study intersections and segments. These scenarios are intended to reflect anticipated traffic approximately five years into the future, when the project could reasonably be anticipated to be constructed. This “phasing analysis” is intended to assist the City of Folsom in phasing of improvements at study intersections which may be necessary to accommodate traffic from all approved and anticipated tentative maps over the next five years.

Cumulative 2035 Condition, and Cumulative 2035 with Project Condition

Cumulative scenarios, with and without the project, analyze anticipated conditions at the General Plan 2035 horizon year. These scenarios are intended to reflect anticipated traffic from Folsom Ranch, and shifts in traffic patterns anticipated after construction of two new interchanges and US Highway 50 overcrossings.

Roadway Systems

Brief descriptions of the key roadways serving the project site are provided below:

- **Iron Point Road** is an east-west arterial roadway with a raised median that runs from Folsom Boulevard to the eastern city limit along the north side of US Highway 50. Within the vicinity of the Project, Iron Point Road has six lanes, bike lanes, sidewalk, curb, and gutter. The posted speed limit is 45 mph. Turn pockets are provided at intersections.
- **Oak Avenue Parkway** is a north-south arterial that extends from Willow Creek Drive to Iron Point Road. It is a four-lane urban arterial road between Willow Creek Drive and Blue Ravine Road. It is a six-lane urban arterial road between Blue Ravine Road and Riley Street. It is a four-lane urban arterial road between Riley Street and Iron Point Road. Oak Avenue Parkway will be extended across US Highway 50 into Folsom Ranch and a new interchange will be constructed prior to the cumulative analysis scenarios.
- **Rowberry Drive** is a north-south two-lane local road that runs northward from the Kaiser Permanente Folsom Medical Offices into neighborhoods to the north of Iron Point Road. A future extension of Rowberry across US Highway 50 to Folsom Ranch is planned for the future.
- **Broadstone Parkway** in the project vicinity is a four-lane east-west arterial, that wraps around the back of the Palladio shopping center from Iron Point Road to connect with Empire Ranch Road near the Sacramento-El Dorado county line. Broadstone Parkway has bike lanes, sidewalk, curb, and gutter. Turn pockets are provided at intersections.

- **East Bidwell Street** runs through the City of Folsom from White Rock Road to Riley Street. East Bidwell Street becomes Scott Road south of US Highway 50. Near the Project area, East Bidwell Street is a six-lane arterial roadway with bike lanes, sidewalk, curb, and gutter. Turn pockets are provided at intersections. The speed limit on East Bidwell Street north of US Highway 50 is 45 mph.
- **Prairie City Road** is a north-south arterial that extends from Blue Ravine Road to White Rock Road, north of Blue Ravine Road it is called Sibley Street. It is a five-lane urban arterial road between Blue Ravine Road and Iron Point Road. Prairie City Road is a six-lane urban arterial road between Iron Point Road and US Highway 50. It is a two-lane rural road between US Highway 50 and White Rock Road.

Study Intersections

There are twenty study segments on US Highway 50 (Table 14) and seventeen study intersections (Table 15). The Oak Avenue Parkway interchange will be constructed by the cumulative analysis year, resulting in changes to some study US Highway 50 segments.

Table 14. US Highway 50 Study Segments

US Highway 50 Segment	Segment Type	Applicable Years
US Highway 50 westbound East Bidwell offramp	Diverge	All
US Highway 50 westbound East Bidwell loop onramp	Merge	All
US Highway 50 westbound East Bidwell slip onramp	Merge	All
US Highway 50 westbound East Bidwell to Oak Ave	Basic	All
US Highway 50 westbound Oak Avenue offramp	Diverge	2035
US Highway 50 westbound Oak Avenue loop onramp	Merge	2035
US Highway 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	Weave	2035
US Highway 50 westbound Prairie City offramp	Diverge	2021/2026
US Highway 50 westbound Prairie City loop onramp	Merge	All
US Highway 50 westbound Prairie City diagonal onramp	Merge	All
US Highway 50 eastbound Prairie City offramp	Diverge	All
US Highway 50 eastbound Prairie City diagonal onramp	Merge	All
US Highway 50 eastbound Prairie City fly-over onramp	Merge	2021/2026
US Highway 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	2035
US Highway 50 eastbound Oak Avenue loop onramp	Merge	2035
US Highway 50 eastbound Oak Avenue diagonal onramp	Merge	2035
US Highway 50 eastbound Oak Avenue to East Bidwell	Basic	All
US Highway 50 eastbound East Bidwell offramp	Diverge	All
US Highway 50 eastbound East Bidwell loop onramp	Merge	All
US Highway 50 eastbound East Bidwell slip onramp	Merge	All

Table 15. Study Intersections and Control

Intersection	Control
1. Prairie City Rd/US Highway 50 eastbound ramps	Signal
2. Prairie City Rd/US Highway 50 westbound ramps	Signal
3. Prairie City Rd/American Aggregates Rd	Signal
4. Prairie City Rd/Iron Point Rd	Signal
5. Iron Pt Road/Grover Rd	Signal
6. Iron Pt Road/Oak Avenue Pkwy	Signal
7. Iron Pt Road/West Kaiser access road	TWSC*
8. Iron Pt Road/Rowberry Way	Signal
9. Iron Pt Road/Safe Credit Union access	TWSC*
10. Iron Pt Road/Broadstone Pkwy	Signal
11. Iron Pt Road/East Bidwell St	Signal
12. Est Bidwell St/US Highway 50 westbound ramps	Signal
13. East Bidwell St/US Highway 50 eastbound ramps	Signal
14. APN 072-3120-023 "Lot 6" access	TWSC*
15. APN 072-3120-023 "Lot 1" access	TWSC*
16. Oak Avenue Pkwy/US Highway 50 westbound ramps (2035 Only)	Signal
17. Oak Avenue Pkwy/US Highway 50 eastbound ramps (2035 Only)	Signal

*Two-way Stop Control.

Level of Service Methodology

Level of service (LOS) is a qualitative indication of the level of delay and congestion experienced by motorists using an intersection. Levels-of-service are designated by the letters A through F, with A being the best conditions and F being the worst (high delay and congestion). Calculation methodologies, measures of performance, and thresholds for each letter grade differ for road segments, signalized intersections, and unsignalized intersections. Based on guidance from City of Folsom staff, the following procedures described below for intersection and segment traffic operations analysis were selected for this study.

Intersection Traffic Operations Analysis

Signalized Intersections

The methodology from the Highway Capacity Manual (HCM) 6th Edition², was used to analyze signalized intersections. LOS can be characterized for the entire intersection, each approach, or by lane group. Control delay alone (the weighted average delay for all vehicles entering the intersection) is used to characterize LOS for the entire intersection or an approach. Control delay and volume to capacity ratio are used to characterize LOS for lane groups. The average delay criteria used to determine the LOS at signalized intersections is presented in **Table 16**. The HCM 2010 methodology is used as the primary method. HCM 2000 methods are only utilized where the signal phasing is incompatible with HCM 2010 methods.

Table 16. Level of Service Criteria for Signalized Intersections

Level of Service	Description	Average Delay ¹ (Sec. /Vehicle.)
A	Very Low Delay: This level-of-service occurs when progression is extremely favorable, and most vehicles arrive during a green phase. Most vehicles do not stop at all.	≤ 10.0
B	Minimal Delays: This level-of-service generally occurs with good progression, short cycle lengths, or both. More vehicles stop than at LOS A, causing higher levels of average delay.	10.1-20.0
C	Acceptable Delay: Delay increases due to only fair progression, longer cycle lengths, or both. Individual cycle failures (<i>to service all waiting vehicles</i>) may begin to appear at this level of service. The number of vehicles stopping is significant, though many still pass through the intersection without stopping.	20.1-35.0
D	Approaching Unstable/Tolerable Delays: The influence of congestion becomes more noticeable. Longer delays may result from some combination of unfavorable progression, long cycle lengths, or high v/c ratios. Many vehicles stop, and the proportion of vehicles not stopping declines. Individual cycle failures are noticeable.	35.1-55.0
E	Unstable Operation/Significant Delays: This is considered by many agencies the upper limit of acceptable delays. These high delay values generally indicate poor progression, long cycle lengths, and high v/c ratios. Individual cycle failures are frequent occurrences.	55.1-80.0
F	Excessive Delays: This level, considered to be unacceptable to most drivers, often occurs with oversaturation (i.e., when arrival flow rates exceed the capacity of the intersection). It may also occur at high v/c ratios below 1.00 with many individual cycle failures. Poor progression and long cycle lengths may also contribute to such delay levels.	> 80.0 or v/c >1.0

Note 1: Weighted average of delay on all approaches. This is the measure used by the Highway Capacity Manual to determine LOS. Any movement with a volume-to-capacity ratio (v/c) greater than 1.0 is considered to be LOS F.

Source: Transportation Research Board (2016) Highway Capacity Manual 6th Edition, Washington D.C.

Unsignalized Intersections

The methodology from HCM 6th Edition is used for the analysis of unsignalized intersections. At an unsignalized intersection, most of the main street traffic is un-delayed, and by definition has acceptable conditions. The main street left-turn movements and the minor street movements are all susceptible to delay of varying degrees. Generally, the higher the main street traffic volumes, the higher the delay for the minor movements. Separate methods are utilized for Two-Way Stop-Controlled (TWSC) intersections and All-Way Stop-Controlled (AWSC) intersections.

- **TWSC:** The methodology for analysis of two-way stop-controlled intersections calculates an average total delay per vehicle for each minor street movement and for the major street left-turn movements, based on the availability of adequate gaps in the main street through traffic. A LOS designation is assigned to individual movements or combinations of movements (in the case of shared lanes) based upon delay, it is not defined for the intersection as a whole. Unsignalized intersection LOS reported herein is for each movement (or group of movements) based upon

the respective average delay per vehicle. **Table 17** presents the average delay criteria used to determine the LOS at TWSC and AWSC intersections.

- **AWSC:** At all-way stop-controlled intersections, the level-of-service is determined by the weighted average delay for all vehicles entering the intersection. The methodologies for these types of intersections calculate a single weighted average delay and LOS for the intersection as a whole. The average delay criteria used to determine the LOS at all-way stop intersections is the same as that presented in **Table 17**. LOS for specific movements can also be determined based on the TWSC methodology.

It is not unusual for some of the minor street movements at unsignalized intersections to have LOS D, E, or F conditions while the major street movements have LOS A, B, or C conditions. In such a case, the minor street traffic experiences delays that can be substantial for individual minor street vehicles, but the majority of vehicles using the intersection have very little delay. Usually in such cases, the minor street traffic volumes are relatively low. If the minor street volume is large enough, improvements to reduce the minor street delay may be justified, such as channelization, widening, or signalization.

Table 17. Level of Service Criteria for Unsignalized Intersections

Level of Service (LOS)	Description	TWSC ¹ Average Delay by Movement (seconds/vehicle)	AWSC ² Intersection Wide Average Delay (seconds/vehicle)
A	Little or no delay	< 10	< 10
B	Short traffic delay	> 10 and < 15	> 10 and < 15
C	Average traffic delays	> 15 and < 25	> 15 and < 25
D	Long traffic delays	> 25 and < 35	> 25 and < 35
E	Very long traffic delays	> 35 and < 50	> 35 and < 50
F	Extreme delays potentially affecting other traffic movements in the intersection	> 50 (or, v/c > 1.0)	>50

Source: Transportation Research Board (2016) Highway Capacity Manual 6th Edition, Washington D.C.

Note 1: Two-Way Stop Control (TWSC) LOS is calculated separately for each minor street movement (or shared movement) as well as major street left turns using these criteria. Any movement with a volume to capacity ratio (v/c) greater than 1.0 is considered to be LOS F.

Note 2: All-Way Stop Control (AWSC) assessment of LOS at the approach and intersection levels is based solely on control delay.

Signal Warrants

At each unsignalized intersection, the potential need for a traffic signal was evaluated. Traffic signal warrants are a series of standards that provide guidelines for determining if a traffic signal is appropriate. Signal warrant analyses are typically conducted at intersections of uncontrolled major streets and stop sign-controlled minor streets. If one or more signal warrants are met, signalization of the intersection may be appropriate. However, a signal should not be installed if none of the warrants are met, since the installation of signals would increase delays on the previously uncontrolled major street and may increase the occurrence of particular types of accidents.

As stated in the 2014 California Edition of the Manual on Uniform Traffic Control Devices (California MUTCD 2014)³, *“An engineering study of traffic conditions, pedestrian characteristics, and physical characteristics of the location shall be performed to determine whether installation of a traffic control signal is justified at a particular location.*

The investigation of the need for a traffic control signal shall include an analysis of factors related to the existing operation and safety at the study location and the potential to improve these conditions, and the applicable factors contained in the following traffic signal warrants:

- *Warrant 1, Eight-hour Vehicular Volume*
- *Warrant 2, Four-hour Vehicular Volume*
- *Warrant 3, Peak-hour*
- *Warrant 4, Pedestrian Volume*
- *Warrant 5, School Crossing*
- *Warrant 6, Coordinated Signal System*
- *Warrant 7, Crash Experience*
- *Warrant 8, Roadway Network*
- *Warrant 9, Intersection Near a Grade Crossing*

The satisfaction of a traffic signal warrant or warrants shall not in itself require the installation of a traffic control signal.”

Consistent with the industry standard of practice, the Traffic Impact Analysis did not evaluate the full panoply of warrants for traffic signals, but instead focused on the peak-hour warrant. The MUTCD states that, *“This [peak-hour] signal warrant shall be applied only in unusual cases, such as office complexes, manufacturing plants, industrial complexes, or high-occupancy vehicle facilities that attract or discharge large numbers of vehicles over a short time.”* So, the peak-hour warrant is being used in this impact analysis study as an “indicator” of the likelihood of an unsignalized intersection warranting a traffic signal in the future. Intersections that exceed the peak-hour warrant are considered (for the purposes of this impact analysis) to be likely to meet one or more of the other signal warrants (such as the 4-hour or 8-hour warrants). This peak-hour analysis is not intended to replace a rigorous and complete traffic signal warrant analysis by the responsible jurisdiction.

Unsignalized intersections were evaluated using the Peak-hour Volume Warrant (Warrant No. 3) in the California MUTCD 2014. The Peak-hour Volume Warrant was applied where the minor street experiences long delays in entering or crossing the major street for at least one hour in a day. Even if the Peak-hour Volume Warrant is met, a more detailed signal warrant study is recommended before a signal is installed. The more detailed study should consider volumes during the daily peak-hours of roadway traffic, pedestrian traffic, and accident histories.

Basic Segments

Basic freeway segments operations and level-of-service is defined by density (passenger cars per mile per lane) which depends upon traffic volumes, and segment, characteristics. These characteristics

³ Caltrans (2019) California Manual on Uniform Traffic Control Devices - FHWA’s MUTCD 2009 Edition as amended for use in California - 2014 Edition - Revision 4, March 29, 2019. Section 4C.

include the geometry, grade, free flow speeds, and heavy vehicles. **Table 18** shows the level of service criteria for basic freeway segments.

Table 18. Level of Service Criteria – Basic Freeway Segments

Level of Service	Maximum Density (passenger vehicles per mile per lane)
A	<11
B	18
C	26
D	35
E	45
F	> 45, or Demand exceeds capacity

Source: Transportation Research Board (2010) Highway Capacity Manual, Chapter 11, Washington, D.C.

Merge and Diverge Segments

Freeway merge and diverge segments operations and level-of-service is defined by density (passenger cars per mile per lane) which depends upon traffic volumes and the ramp characteristics. These characteristics include the length and type of acceleration/deceleration lanes, free-flow speeds, number of lanes, grade, heavy vehicles, and types of facilities. **Table 19** shows the relationship of level-of-service to freeway density for merge, diverge, and weaving segments.

Table 19. Level of Service Criteria – Freeway Ramp Merge/Diverge Areas

Level of Service	Maximum Density (passenger vehicles per mile per lane)
A	<10
B	20
C	28
D	35
E	> 35
F	Demand exceeds capacity

Source: Transportation Research Board (2010) Highway Capacity Manual, Chapter 13, Washington, D.C.

Standards of Significance

Consistency with General Plan LOS policies for the proposed project were determined based on the methods described above and identified as either "significant" or "less than significant". General Plan Policy M4.1.3 addresses LOS:

Strive to achieve at least traffic LOS "D" (or better) for local streets and roadways throughout the City. In designing transportation improvements, the City will prioritize use of smart technologies and innovative solutions that maximize efficiencies and safety while minimizing the physical footprint. During the course of Plan buildout, it may occur that temporally higher LOS result where roadway improvements have not been adequately phased as development proceeds. However, this situation will be minimized based on annual traffic studies and monitoring programs. City Staff will report to the City Council at regular intervals via the Capital Improvement Program process for the Council to prioritize projects integral to achieving LOS D or better.

Consistent with historical practice within the City of Folsom, the General Plan EIR also includes a criterion addressing potential impacts at locations that operate at level-of-service E or F under

no-project conditions. Under that standard, a significant impact would occur if the proposed project would:

Increase the average delay by five seconds or more at an intersection that currently operates (or is projected to operate) at an unacceptable level-of-service under “no-project” conditions.

For the purposes of the traffic analysis, an impact is considered potentially significant if implementation of the project would result in any of the following:

- Cause an intersection in Folsom that currently operates (or is projected to operate) at LOS D or better to degrade to LOS E or worse.
- Increase the average delay by five seconds or more at an intersection in Folsom that currently operates (or is projected to operate) at an unacceptable LOS E or F.

Freeway Facilities

An impact is considered significant on freeway facilities if the project causes the facility to change from an acceptable to unacceptable LOS. For facilities that are or will be operating at unacceptable LOS without the project, an impact is considered significant if:

- The existing LOS cannot be maintained with the addition of project traffic;
- The project traffic increases vehicle density on a freeway mainline segment or freeway ramp junction by 0.1 passenger cars per lane per mile;
- The project increases the number of peak-hour vehicles on a freeway mainline segment or freeway ramp junction by more than 1 percent.

Per the Caltrans Guide for the Preparation of Traffic Impact Studies, Caltrans strives to maintain a target LOS at the transition between LOS C and LOS D on state highway facilities. However, for the affected portion of US 50, Caltrans has established a concept LOS E threshold⁴. For consistency with other traffic impact studies performed in the City of Folsom that considered US Highway 50, LOS E was selected as the minimum standard for all study freeway facilities.

Bicycle/Pedestrian/Transit Facilities

An impact is considered significant if implementation of the project would:

- Inhibit the use of bicycle, pedestrian, or transit facilities.
- Eliminate existing bicycle, pedestrian, or transit facilities.
- Prevent the implementation of planned bicycle, pedestrian, or transit facilities.

Existing 2021 Conditions

Tables 20 and 21 present a summary of level-of-service results for the study intersections under Existing Conditions. The results indicate that all study segments are anticipated to operate at an acceptable LOS. Three study intersections exceed the General Plan LOS standard prior to the addition of project traffic.

- Prairie City Rd/American Aggregate Dr would operate at a deficient LOS during the AM peak if not for the Covid-19 related traffic reductions.
- Prairie City Rd/Iron Point Rd would operate at a deficient LOS during the AM and PM peak if not for the Covid-19 related traffic reductions.
- East Bidwell St/Iron Point Rd would operate at a deficient LOS during the PM peak if not for the Covid-19 related traffic reductions.

These locations are shown in orange highlight in the tables below. Calculation sheets for intersection delay and LOS are provided in **Appendix H**.

Table 20. Existing 2021 Intersection Delay and LOS

Intersection	Control	Without Project AM Delay (Sec.) and LOS	Without Project PM Delay (Sec.) and LOS
1. Prairie City Rd/ US 50 eastbound ramps	Signal	10.3 B	8.3 A
2. Prairie City Rd/ US 50 westbound ramps	Signal	19.4 B	8.9 A
3. Prairie City Rd/ American Aggregates Rd	Signal	66.1 E	28.8 C
4. Prairie City Rd/ Iron Point Rd	Signal	88.7 F	64.5 E
5. Iron Point Road/ Grover rd	Signal	50.9 D	42.3 D
6. Iron Point Road/ Oak Avenue Parkway	Signal	36.2 D	37.8 D
7. Iron Point Road/ West Kaiser access road	TWSC**	11.9 B Northbound	12.9 B Northbound
8. Iron Point Road/ Rowberry Way	Signal	14.3 B	14.2 B
9. Iron Point Rd/ Safe Credit Union access	TWSC**	15.6 C WB left/U	23.1 C WB left/U
10. Iron Point Rd/ Broadstone Pkwy	Signal	15.6 B	19.6 B
11. Iron Point Rd/ East Bidwell St	Signal	45.5 D	94.3 F
12. East Bidwell St/ US 50 westbound ramps	AWSC	29.5 C	35.1 D
13. East Bidwell St/ US 50 eastbound ramps	Signal	10.2 B	21.5 C
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.6 A Southbound	9.3 A Southbound

*Level of Service

**Two Way Stop Control: LOS is defined by delay for the worst movement/ shared movement, which is listed with the LOS results.

Table 21. Existing 2021 US Highway 50 Segment Density and LOS

US Highway 50 Segment	Segment Type	Without Project AM (Density LOS*)	Without Project PM (Density LOS*)
US 50 westbound East Bidwell offramp	Diverge	24.5 C	17.3 B
US 50 westbound East Bidwell loop onramp	Merge	22.9 C	17.1 B
US 50 westbound East Bidwell slip onramp	Merge	24.3 C	19.0 B
US 50 westbound East Bidwell to Oak Ave	Basic	24.8 C	18.8 C
US 50 westbound Oak Avenue offramp	Diverge	Not applicable to this scenario.	
US 50 westbound Oak Avenue lop onramp	Merge		

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Segment	Segment Type	2021 AM No Project Density and LOS	2021 PM No Project Density and LOS	2021 AM Plus Project Density and LOS	2021 PM Plus Project Density and LOS
US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	Not applicable to this scenario.			
US 50 eastbound Oak Avenue loop onramp	Merge				
US 50 eastbound Oak Avenue diagonal onramp	Merge				
US 50 eastbound Oak Avenue to East Bidwell	Basic	17.5 B	23.5 C	17.5 B	23.5 C
US 50 eastbound East Bidwell offramp	Diverge	10.4 B	16.5 B	10.4 B	16.5 B
US 50 eastbound East Bidwell loop onramp	Merge	9.3 A	13.9 B	9.3 A	13.9 B
US 50 eastbound East Bidwell slip onramp	Merge	7.5 A	13.1 B	7.6 A	13.1 B

Existing Plus Approved Project (EPAP) 2026 Conditions

EPAP Conditions analysis utilizes lane configurations and signal timing plans from the Existing Conditions. **Tables 25 and 26** present a summary of LOS results for the study intersections under EPAP 2026 Conditions.

The results indicate that all study segments are anticipated to operate at an acceptable LOS; three study intersections exceed the General Plan LOS standard prior to the addition of project traffic.

- Prairie City Rd/American Aggregate Dr would operate at a deficient LOS during the AM peak if not for the Covid-19 related traffic reductions.
- Prairie City Rd/Iron Point Rd would operate at a deficient LOS during the AM and PM peak if not for the Covid-19 related traffic reductions.
- East Bidwell St/Iron Point Rd would operate at a deficient LOS during the AM and PM peak if not for the Covid-19 related traffic reductions.

These locations are shown in orange highlight in the tables below. Calculation sheets for intersection delay and LOS are provided in **Appendix H**.

Table 25. EPAP 2026 Intersection Delay and LOS

Intersection	Control	Without Project AM Delay (Sec.) and LOS	Without Project PM Delay (Sec.) and LOS
1. Prairie City Rd/ US 50 eastbound ramps	Signal	15.2 B	10.5 B
2. Prairie City Rd/ US 50 westbound ramps	Signal	60.5 E	10.2 B
3. Prairie City Rd/ American Aggregates Rd	Signal	110.5 F	30.8 C
4. Prairie City Rd/ Iron Point Rd	Signal	123.4 F	72.4 E
5. Iron Point Road/ Grover rd	Signal	52 D	43.4 D
6. Iron Point Road/ Oak Avenue Parkway	Signal	36.8 D	40.4 D
7. Iron Point Road/ West Kaiser access road	TWSC**	12.4 B Northbound	13.7 B Northbound
8. Iron Point Road/ Rowberry Way	Signal	14.4 B	14.3 B
9. Iron Point Rd/ Safe Credit Union access	TWSC**	16.9 C WB left/U	27 D WB Left/ U

US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	Weave		
US 50 westbound Prairie City offramp	Diverge	32.0 D	26.1 C
US 50 westbound Prairie City loop onramp	Merge	24.1 C	21.6 C
US 50 westbound Prairie City diagonal onramp	Merge	24.5 C	21.5 C
US 50 eastbound Prairie City offramp	Diverge	28.6 D	31.0 D
US 50 eastbound Prairie City diagonal onramp	Merge	18.6 B	23.2 C
US 50 eastbound Prairie City fly-over onramp	Merge	19.6 B	25.4 C
US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	Not applicable to this scenario.	
US 50 eastbound Oak Avenue loop onramp	Merge		
US 50 eastbound Oak Avenue diagonal onramp	Merge		
US 50 eastbound Oak Avenue to East Bidwell	Basic	17.5 B	23.5 C
US 50 eastbound East Bidwell offramp	Diverge	10.4 B	16.5 B
US 50 eastbound East Bidwell loop onramp	Merge	9.3 A	13.9 B
US 50 eastbound East Bidwell slip onramp	Merge	7.5 A	13.1 B

*Level of Service

Trip Generation

Traffic generated by the proposed project was based on Institute of Transportation Engineers (ITE) Trip Generation Manual, 10th Edition (2017), and is provided in **Table 22** below.

Table 22. Project Trip Generation

Location	Quantity	Units	Metric	Daily	Am (Tot)	Am (In)	Am (out)	Pm (Tot)	PM (In)	PM (out)
Lot 6	100	du	Rate	5.44	0.32	27%	73%	0.41	60%	40%
			Trips	544	32	9	23	41	25	16
Lot 1	153	Du	Rate	5.44	0.32	27%	73%	0.41	60%	40%
			Trips	832	49	13	36	63	38	25
Total	253	Du	Rate	5.44	0.32	27%	73%	0.41	60%	40%
			Trips	1376	81	22	59	104	62	42

Source: ITE (2017) Trip Generation Manual, 10th Ed, Institute of Transportation Engineers, Washington DC.

Existing 2021 with Project Conditions

Peak-hour traffic associated with the Project was added to the Existing 2021 turning volumes at each intersection. Delay and level-of-service were determined at the study intersections and segments.

Tables 23 and 24 presents a summary of the level-of-service results for the study intersections and segments.

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Table 23. Existing 2021 Intersection Delay and LOS, with and without Project

Intersection	Control	2021 No Project AM Delay (Sec.) and LOS	2021 No Project PM Delay (Sec.) and LOS	2021 Plus Project AM Delay (Sec.) and LOS	2021 Plus Project PM Delay (Sec.) and LOS
1. Prairie City Rd/US 50 eastbound ramps	Signal	10.3 B	8.3 A	10.4 B	8.4 A
2. Prairie City Rd/US 50 westbound ramps	Signal	19.4 B	8.9 A	19.5 B	8.9 A
3. Prairie City Rd/American Aggregates Rd	Signal	66.1 E	28.8 C	66.3 E	28.9 C
4. Prairie City Rd/Iron Point Rd	Signal	88.7 F	64.5 E	90.6 F	66.1 E
5. Iron Pt Road/Grover Rd	Signal	50.9 D	42.3 D	51.4 D	42.5 D
6. Iron Point Rd /Oak Avenue Pkwy	Signal	36.2 D	37.8 D	36.4 D	38.4 D
7. Iron Point Rd /West Kaiser access road	TWSC**	11.9 B Northbound	12.9 B Northbound	11.9 B Northbound	13 B Northbound
8. Iron Point Rd /Rowberry Way	Signal	14.3 B	14.2 B	14.8 B	14.5 B
9. Iron Point Rd /Safe Credit Union access	TWSC**	15.6 C WB left/U	23.1 C WB left/U	16 C WB left/ U	23.6 C WB left/ U
10. Iron Point Rd /Broadstone Pkwy	Signal	15.6 B	19.6 B	15.7 B	19.7 B
11. Iron Point Rd /East Bidwell St	Signal	45.5 D	94.3 F	46 D	95.3 F
12. East Bidwell St/US 50 westbound ramps	Signal	29.5 C	35.1 D	29.6 C	35.7 D
13. East Bidwell St/US 50 eastbound ramps	Signal	10.2 B	21.5 C	10.2 B	21.7 C
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound	9.2 A Northbound	8.9 A Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.6 A Southbound	9.3 A Southbound	10.3 B Southbound	10.2 B Southbound

Table 24. Existing 2021 US Highway 50 Segment Density and LOS, with and without Project

Segment	Segment Type	2021 AM No Project Density and LOS	2021 PM No Project Density and LOS	2021 AM Plus Project Density and LOS	2021 PM Plus Project Density and LOS
US 50 westbound East Bidwell offramp	Diverge	24.5 C	17.3 B	24.5 C	17.4 B
US 50 westbound East Bidwell loop onramp	Merge	22.9 C	17.1 B	22.9 C	17.1 B
US 50 westbound East Bidwell slip onramp	Merge	24.3 C	19.0 B	24.3 C	19.0 B
US 50 westbound East Bidwell to Oak Ave	Basic	24.8 C	18.8 C	24.8 C	18.8 C
US 50 westbound Oak Avenue offramp	Diverge	Not Applicable to this scenario			
US 50 westbound Oak Avenue lop onramp	Merge				
US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	Weave				
US 50 westbound Prairie City offramp	Diverge	32.0 D	26.1 C	32.0 D	26.1 C
US 50 westbound Prairie City loop onramp	Merge	24.1 C	21.6 C	24.1 C	21.6 C
US 50 westbound Prairie City diagonal onramp	Merge	24.5 C	21.5 C	24.6 C	22.1 C
US 50 eastbound Prairie City offramp	Diverge	28.6 D	31.0 D	28.6 D	31.1 D
US 50 eastbound Prairie City diagonal onramp	Merge	18.6 B	23.2 C	18.6 B	23.2 C
US 50 eastbound Prairie City fly-over onramp	Merge	19.6 B	25.4 C	19.6 B	25.4 C

Intersection	Control	Without Project AM Delay (Sec.) and LOS	Without Project PM Delay (Sec.) and LOS
10. Iron Point Rd/ Broadstone Pkwy	Signal	16.3 B	20.5 C
11. Iron Point Rd/ East Bidwell St	Signal	67.1 E	143.4 F
12. East Bidwell St/ US 50 westbound ramps	Signal	46.9 D	53.5 D
13. East Bidwell St/ US 50 eastbound ramps	Signal	12.9 B	25.4 C
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.6 A Southbound	9.8 A Southbound

**Two Way Stop Control: LOS is defined by delay for the worst movement/ shared movement, which is listed with the LOS results.

Table 26. EPAP 2026 US 50 Segment Density and LOS

US Highway 50 Segment	Segment Type	Without Project AM (Density LOS*)	Without Project PM (Density LOS*)
US 50 westbound East Bidwell offramp	Diverge	25.9 C	17.8 B
US 50 westbound East Bidwell loop onramp	Merge	24.4 C	18.1 B
US 50 westbound East Bidwell slip onramp	Merge	25.9 C	21.2 C
US 50 westbound East Bidwell to Oak Ave	Basic	26.9 D	21.2 C
US 50 westbound Oak Avenue offramp	Diverge	Not applicable to this scenario.	
US 50 westbound Oak Avenue loop onramp	Merge		
US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	Weave		
US 50 westbound Prairie City offramp	Diverge	33.7 D	28.7 D
US 50 westbound Prairie City loop onramp	Merge	25.5 C	23.4 C
US 50 westbound Prairie City diagonal onramp	Merge	26.0 C	23.2 C
US 50 eastbound Prairie City offramp	Diverge	30.5 D	33.3 D
US 50 eastbound Prairie City diagonal onramp	Merge	19.6 B	24.1 C
US 50 eastbound Prairie City fly-over onramp	Merge	21.1 C	26.3 C
US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	Not applicable to this scenario.	
US 50 eastbound Oak Avenue loop onramp	Merge		
US 50 eastbound Oak Avenue diagonal onramp	Merge		
US 50 eastbound Oak Avenue to East Bidwell	Basic	18.8 C	24.7 C
US 50 eastbound East Bidwell offramp	Diverge	11.8 B	17.6 B
US 50 eastbound East Bidwell loop onramp	Merge	9.3 A	13.9 B
US 50 eastbound East Bidwell slip onramp	Merge	8.5 A	14.2 B

EPAP 2026 with Project Condition

The results indicate that all study segments are anticipated to operate at an acceptable LOS; three study intersections exceed the General Plan LOS standard prior to the addition of project traffic.

- Prairie City Rd/American Aggregate Dr would operate at a deficient LOS during the AM peak if not for the Covid-19 related traffic reductions.

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- Prairie City Rd/Iron Point Rd would operate at a deficient LOS during the AM and PM peak if not for the Covid-19 related traffic reductions.
- East Bidwell St/Iron Point Rd would operate at a deficient LOS during the AM and PM peak if not for the Covid-19 related traffic reductions.

These locations are shown in orange highlight in the tables below. Because the increase in delay is less than five seconds, these exceedance of the General Plan level-of-service policy is not considered a project impact. Calculation sheets for intersection delay and LOS are provided in **Appendix H**.

Table 17. EPAP 2026 Intersection Delay and LOS, with and without Project

Intersection	Control	2021 No Project AM Delay (Sec.) and LOS	2021 No Project PM Delay (Sec.) and LOS	2021 Plus Project AM Delay (Sec.) and LOS	2021 Plus Project PM Delay (Sec.) and LOS
1. Prairie City Rd/US 50 eastbound ramps	Signal	15.2 B	10.5 B	15.3 B	10.6 B
2. Prairie City Rd/US 50 westbound ramps	Signal	60.5 E	10.2 B	60.8 E	10.3 B
3. Prairie City Rd/American Aggregates Rd	Signal	110.5 F	30.8 C	110.6 F	30.8 C
4. Prairie City Rd/Iron Point Rd	Signal	123.4 F	72.4 E	125.2 F	74.1 E
5. Iron Pt Road/Grover Rd	Signal	52 D	43.4 D	52.5 D	43.7 D
6. Iron Point Rd /Oak Avenue Pkwy	Signal	36.8 D	40.4 D	37.1 D	41.4 D
7. Iron Point Rd /West Kaiser access road	TWSC**	12.4 B Northbound	13.7 B Northbound	12.4 B Northbound	13.8 B Northbound
8. Iron Point Rd /Rowberry Way	Signal	14.4 B	14.3 B	15.0 B	14.6 B
9. Iron Point Rd /Safe Credit Union access	TWSC**	16.9 C WB left/ U	27.0 D WB Left/ U	17.3 C WB left/U	27.7 D WB left/U
10. Iron Point Rd /Broadstone Pkwy	Signal	16.3 B	20.5 C	16.4 B	20.6 C
11. Iron Point Rd /East Bidwell St	Signal	67.1 E	143.4 F	68 E	144.5 F
12. East Bidwell St/US 50 westbound ramps	Signal	46.9 D	53.5 D	47 D	53.8 D
13. East Bidwell St/US 50 eastbound ramps	Signal	12.9 B	25.4 C	12.9 B	25.5 C
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound	9.2 A Northbound	8.9 A Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.6 A Northbound	9.8 A Southbound	10.3 B Southbound	10.2 B Southbound

** Two Way Stop Control: LOS is defined by delay for the worst movement/shared movement, which is listed with the LOS results.

Table 28. EPAP 2026 US 50 Segment Density and LOS, with and without Project

Segment	Segment Type	2021 AM No Project Density and LOS	2021 PM No Project Density and LOS	2021 AM Plus Project Density and LOS	2021 PM Plus Project Density and LOS
US 50 westbound East Bidwell offramp	Diverge	25.9 C	17.8 B	26 C	17.9 B
US 50 westbound East Bidwell loop onramp	Merge	24.4 C	18.1 B	24.4 C	18.1 B
US 50 westbound East Bidwell slip onramp	Merge	25.9 C	21.2 C	25.9 C	21.2 C
US 50 westbound East Bidwell to Oak Ave	Basic	26.9 D	21.2 C	26.9 D	21.2 C
US 50 westbound Oak Avenue offramp	Diverge	Not Applicable to this scenario			
US 50 westbound Oak Avenue lop onramp	Merge				

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Segment	Segment Type	2021 AM No Project Density and LOS	2021 PM No Project Density and LOS	2021 AM Plus Project Density and LOS	2021 PM Plus Project Density and LOS
US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	Weave				
US 50 westbound Prairie City offramp	Diverge	33.7 D	28.7 D	33.7 D	28.7 D
US 50 westbound Prairie City loop onramp	Merge	25.5 C	23.4 C	25.5 C	23.4 C
US 50 westbound Prairie City diagonal onramp	Merge	26.0 C	23.2 C	26.1 C	23.3 C
US 50 eastbound Prairie City offramp	Diverge	30.5 D	33.3 D	30.5 D	33.3 D
US 50 eastbound Prairie City diagonal onramp	Merge	19.6 B	24.1 C	19.6 B	24.1 C
US 50 eastbound Prairie City fly-over onramp	Merge	21.1 C	26.3 C	21.1 C	26.3 C
US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave				
US 50 eastbound Oak Avenue loop onramp	Merge				
US 50 eastbound Oak Avenue diagonal onramp	Merge	Not applicable to this scenario.			
US 50 eastbound Oak Avenue to East Bidwell	Basic	18.8 C	24.7 C	18.8 C	24.7 C
US 50 eastbound East Bidwell offramp	Diverge	11.8 B	17.6 B	11.8 B	17.6 B
US 50 eastbound East Bidwell loop onramp	Merge	9.3 A	13.9 B	9.4 A	14.0 B
US 50 eastbound East Bidwell slip onramp	Merge	8.5 A	14.2 B	8.5 A	14.3 B

Cumulative 2026 Conditions with or without Project

The Cumulative Conditions analysis accounts for several planned changes to Folsom's transportation system:

- Addition of a third northbound through lane at intersection #4 (Prairie City Rd/Iron Point Rd);
- Widening of Iron Point Rd to six lanes on all segments between Prairie City Rd and East Bidwell St (effecting intersections 6-9);
- Construction of the Rowberry Way overcrossing of US Highway 50;
- Construction of the Empire Ranch Rd interchange;
- Construction of the Oak Avenue Pkwy interchange; and,
- The extension of Alder Creek Pkwy through Oak Avenue Pkwy (along with other Folsom Ranch infrastructure).

Tables 29 and 30 present a summary of LOS results for the study intersections under EPAP 2026 Conditions. All study intersections and segments are anticipated to operate at an acceptable LOS. Calculation sheets for intersection delay and LOS are provided in Appendix H.

Table 29. Cumulative 2035 Intersection Delay and LOS

Intersection	Control	Without Project AM Delay (Sec.) and LOS	Without Project PM Delay (Sec.) and LOS
1. Prairie City Rd/ US 50 eastbound ramps	Signal	10.6 B	9.5 A
2. Prairie City Rd/ US 50 westbound ramps	Signal	17.2 B	9.4 A
3. Prairie City Rd/ American Aggregates Rd	Signal	53.3 D	29.5 C
4. Prairie City Rd/ Iron Point Rd	Signal	45.5 D	38 D
5. Iron Point Road/ Grover rd	Signal	48.5 D	38.9 D
6. Iron Point Road/ Oak Avenue Parkway	Signal	39.7 D	52.3 D
7. Iron Point Road/ West Kaiser access road	TWSC**	18.3 C Northbound	21.5 C Northbound
8. Iron Point Road/ Rowberry Way	Signal	24.3 C	32.7 C
9. Iron Point Rd/ Safe Credit Union access	TWSC**	23.6 C WB left/U	29.6 C WB left/ U
10. Iron Point Rd/ Broadstone Pkwy	Signal	18 B	24.3 C
11. Iron Point Rd/ East Bidwell St	Signal	37.4 D	54.5 D
12. East Bidwell St/ US 50 westbound ramps	Signal	18.7 B	21.2 C
13. East Bidwell St/ US 50 eastbound ramps	Signal	10.9 B	11.8 B
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.7 A Southbound	9.3 A Southbound
16. Oak Pkwy/ US 50 westbound ramps	Signal	13.7 B	22.7 C
17. Oak Pkwy/ US 50 eastbound ramps	Signal	9.5 A	20.4 C

**Two Way Stop Control: LOS is defined by delay for the worst movement/ shared movement, which is listed with the LOS results.

Table 30. Cumulative 2035 US 50 Segment Density and LOS

US Highway 50 Segment	Segment Type	Without Project AM (Density LOS*)	Without Project PM (Density LOS*)
US 50 westbound East Bidwell offramp	Diverge	17.3 B	14.1 B
US 50 westbound East Bidwell loop onramp	Merge	31.2 D	24 C
US 50 westbound East Bidwell slip onramp	Merge	28.6 D	22.4 C
US 50 westbound East Bidwell to Oak Ave	Basic	30.6 D	22.2 C
US 50 westbound Oak Avenue offramp	Diverge	33.7 D	27 C
US 50 westbound Oak Avenue loop onramp	Merge	28 D	24.7 C
US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	Weave	27.6 C	25.2 C
US 50 westbound Prairie City offramp	Diverge	NA	NA
US 50 westbound Prairie City loop onramp	Merge	33.2 D	31.6 D
US 50 westbound Prairie City diagonal onramp	Merge	29.3 D	27.9 C
US 50 eastbound Prairie City offramp	Diverge	35.8 E	37.5 E
US 50 eastbound Prairie City diagonal onramp	Merge	27.1 C	31 D
US 50 eastbound Prairie City fly-over onramp	Merge	NA	NA
US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	22.5 C	26 C
US 50 eastbound Oak Avenue loop onramp	Merge	24.1 C	28.2 D
US 50 eastbound Oak Avenue diagonal onramp	Merge	26.7 C	32.5 D
US 50 eastbound Oak Avenue to East Bidwell	Basic	22.1 C	30.1 D

Folsom Corporate Center Apartments ISMND

US 50 eastbound East Bidwell offramp	Diverge	15.2 B	21.7 C
US 50 eastbound East Bidwell loop onramp	Merge	11 B	16.8 B
US 50 eastbound East Bidwell slip onramp	Merge	11.7 B	19.2 B

Cumulative 2035 with Project Conditions

Peak-hour traffic associated with the project was added to anticipated EPAP 2026 turning volumes at each intersection. Delay and LOS were then determined at the study intersections. **Tables 31 and 32** present a summary of the LOS results for the study intersections. All study intersections and segments are anticipated to operate at an acceptable LOS. Calculation sheets for intersection delay and LOS are provided in **Appendix H**.

Table 31. Cumulative 2035 Intersection Delay and LOS with and without Project

Intersection	Control	No Project AM Delay (Sec.) and LOS	No Project PM Delay (Sec.) and LOS	Plus Project AM Delay (Sec.) and LOS	Plus Project PM Delay (Sec.) and LOS
1. Prairie City Rd/US 50 eastbound ramps	Signal	10.6 B	9.5 A	10.6 B	9.5 A
2. Prairie City Rd/US 50 westbound ramps	Signal	17.2 B	8.4 A	17.2 B	8.4 A
3. Prairie City Rd/American Aggregates Rd	Signal	53.3 D	29.5 C	53.3 D	29.5 C
4. Prairie City Rd/Iron Point Rd	Signal	45.5 D	38 D	45.7 D	38.1 D
5. Iron Pt Road/Grover Rd	Signal	48.5 D	38.9 D	48.7 D	39.1 D
6. Iron Point Rd /Oak Avenue Pkwy	Signal	39.7 D	52.3 D	40.8 D	54.6 D
7. Iron Point Rd /West Kaiser access road	TWSC**	18.3 C Northbound	21.5 C Northbound	18.4 C Northbound	21.7 C Northbound
8. Iron Point Rd /Rowberry Way	Signal	24.3 C	32.7 C	25 C	34 C
9. Iron Point Rd /Safe Credit Union access	TWSC**	23.6 C WB left/U	29.6 D WB left/U	23.9 C WB left/U	30.8 D WB left/U
10. Iron Point Rd /Broadstone Pkwy	Signal	18 B	24.3 C	18 B	24.4 C
11. Iron Point Rd /East Bidwell St	Signal	37.4 D	54.5 C	37.5 D	54.6 D
12. East Bidwell St/US 50 westbound ramps	Signal	18.7 B	21.2 C	18.7 B	21.2 C
13. East Bidwell St/US 50 eastbound ramps	Signal	10.9 B	11.8 B	10.9 B	11.8 B
14. APN 072-3120-023 "Lot 6" access	TWSC**	9.1 A Northbound	8.8 A Northbound	9.3 A Northbound	9 a Northbound
15. APN 072-3120-023 "Lot 1" access	TWSC**	9.7 A Southbound	9.3 A Southbound	10.4 B Southbound	10.3 B Southbound
16. Oak Avenue Pkwy/ US 50 westbound ramps	Signal	13.7 B	22.7 C	14.4 B	23.4 C
Oak Avenue Pkwy/ US 50 eastbound ramps	Signal	9.5 A	20.4 C	9.5 A	20.9 C

** Two Way Stop Control: LOS is defined by delay for the worst movement/shared movement, which is listed with the LOS results.

Table 32. Cumulative US Highway 50 Segment Density and LOS with and without Project

Segment	Segment Type	AM No Project Density and LOS	PM No Project Density and LOS	AM Plus Project Density and LOS	PM Plus Project Density and LOS
US 50 westbound East Bidwell offramp	Diverge	17.3 B	14.1 B	17.3 B	14.1 B
US 50 westbound East Bidwell loop onramp	Merge	31.2 D	24 C	31.2 D	24.0 C

Folsom Corporate Center Apartments ISMND

Segment	Segment Type	AM No Project Density and LOS	PM No Project Density and LOS	AM Plus Project Density and LOS	PM Plus Project Density and LOS
US 50 westbound East Bidwell slip onramp	Merge	28.6 D	22.4 C	28.6 D	22.5 C
US 50 westbound East Bidwell to Oak Ave	Basic	30.6 D	22.2 C	30.6 D	22.3 C
US 50 westbound Oak Avenue offramp	Diverge	33.7 D	27 C	33.7 D	27.1 C
US 50 westbound Oak Avenue loop onramp	Merge	28 D	24.7 C	28.0 D	24.7 C
US 50 westbound Oak Avenue diagonal onramp to Prairie City Rd offramp	Weave	27.6 C	25.2 C	27.7 C	25.3 C
US 50 westbound Prairie City offramp	Diverge	NA			
US 50 westbound Prairie City loop onramp	Merge	33.2 D	31.6 D	33.3 D	31.7 D
US 50 westbound Prairie City diagonal onramp	Merge	29.3 D	27.9 C	29.4 D	27.9 C
US 50 eastbound Prairie City offramp	Diverge	35.8 E	37.5 E	35.8 E	27.7 E
US 50 eastbound Prairie City diagonal onramp	Merge	27.1 C	31.0 D	27.2 C	31.1 C
US 50 eastbound Prairie City fly-over onramp	Merge	NA			
US 50 eastbound Prairie City fly-over onramp to Oak Ave offramp	Weave	22.5 C	26.0 C	22.7 C	26.1 C
US 50 eastbound Oak Avenue loop onramp	Merge	24.1 C	28.2 D	24.1 C	28.2 D
US 50 eastbound Oak Avenue diagonal onramp	Merge	26.7 C	32.5 D	26.8 C	32.5 D
US 50 eastbound Oak Avenue to East Bidwell	Basic	22.1 C	30.1 D	22.2 C	30.2 D
US 50 eastbound East Bidwell offramp	Diverge	15.2 B	21.7 C	15.3 B	21.7 C
US 50 eastbound East Bidwell loop onramp	Merge	11 B	16.8 B	11.1 B	16.9 B
US 50 eastbound East Bidwell slip onramp	Merge	11.7 B	19.2 B	11.7 B	19.2 B

Evaluation of Transportation and Traffic

Questions a, f: Less than Significant Impact with Mitigation. Under existing 2021 conditions with the project, the westbound left-turn queue during the AM peak hour exceeds available storage, and the project is anticipated to add 1 vehicle to the queue. Additional queued vehicles can contribute to LOS impacts when queues are longer than available storage and “spill-back” can affect the capacity of adjacent lanes. In order to avoid impacts to the westbound left-turn queue during the AM peak, Mitigation Measure TRA-1 shall be implemented. Additionally, under the EPAP 2026 conditions with the project, the westbound left-turn queue during the AM peak hour exceeds the available storage, and the project is anticipated to add 1 vehicle to the queue, contributing to potential LOS impacts. Similar to the existing 2021 conditions, in order to avoid impacts to the westbound left-turn queue, Mitigation Measure TRA-2 shall be implemented. With implementation of Mitigation Measures TRA-1 and TRA-2, the project would have a less than significant effect on traffic operations under 2021 conditions and under 2026 conditions with the addition of project traffic.

Mitigation Measure TRA-1: Prairie Road/ Iron Point Road Under Existing 2021 Conditions.

The applicant shall modify Prairie City Road/ Iron Point Road signal timing plan by shifting 1 second from the eastbound through movement to the westbound left turn movement, reduce the vehicle extension setting from adding five to six additional seconds to the green phase for through movements to adding four seconds to the green phase for through movements for each vehicle passing the detector after the minimum green phase length has been exceeded. This mitigation measure shall be implemented by the

City through the reimbursement agreement with the applicant to cover any City costs. The implementation of this mitigation measure shall occur prior to issuance of the first building permit.

Mitigation Measure TRA-2: Prairie Road/ Iron Point Road under EPAP 2026 Conditions.

The applicant shall modify Prairie City Rd/Iron Point Rd signal timing plan by shifting 1 second from the eastbound through movement to the westbound left turn movement, reduce the vehicle extension setting from adding five to six additional seconds to the green phase for through movements to adding four seconds to the green phase for through movements for each vehicle passing the detector after the minimum green phase length has been exceeded. This mitigation measure shall be implemented by the City through the reimbursement agreement with the applicant to cover any City costs. The implementation of this mitigation measure shall occur prior to issuance of the first building permit.

Question b: Less than Significant Impact. The Governors' Office of Planning and Research (OPR) has published guidance recommending a CEQA threshold for transportation impacts of land use projects of a 15% Vehicles Miles Travelled (VMT) reduction per capita, relative to either city or regional averages, based on the California's Climate Scoping Plan. Qualitative assessment of VMT reduction is acceptable to screen projects.

Under State Law (SB 743), VMT became the only CEQA threshold of significance for transportation impacts on July 1, 2020. Without specific General Plan guidance for VMT thresholds, this analysis uses qualitative screening against OPR's guidance of a 15 percent per capita VMT reduction. To support jurisdictions' SB743 implementation, SACOG developed thresholds and screening maps for residential projects, using outputs from the 2016 base year travel demand model run for the 2020 MTP/SCS. SACOG's travel demand model is activity/tour based and is designed to estimate an individual's daily travel, accounting for land use, transportation and demographics that influence peoples' travel behaviors. For residential projects, the threshold is defined as total household VMT per capita achieving 15% of reduction compared to regional (or any appropriate sub-area) average VMT. The map uses HEX geography. Residential VMT per capita per HEX is calculated by tallying all household VMTs, including VMT traveling outside the region, generated by the residents living at the HEX and divided by the total population in the HEX. Green hexagons denote areas where residential VMT is 50 to 85 percent of the regional average and yellow hexagons denote areas where residential VMT is 85 to 100 percent of the regional average.

The project is located within one of the green hexagons with average residential VMT of 17-miles per capita (per day). The project is anticipated to generate less than 82 percent of the regional per capita residential daily VMT of 20.82 miles. The project is therefore anticipated to have a less than significant impact on VMT.

Question c: No Impact. No private or public airports are located within the City of Folsom. The nearest public airfield is Cameron Airpark, located approximately 8.5-miles from the proposed project. The Mather Airport is located approximately 10-miles southwest of the project site. The proposed project would not result in modification to any air travel route. There would be no impact and no mitigation would be required.

Question d: Less than Significant Impact. The project would be accessed via proposed private roadways inside of the Folsom Corporate Center. Access to City streets is not being modified and Folsom's requirements for right turn tapers and deceleration lanes are not applicable. Additionally, vehicle speeds and volumes within the business park's internal roadway do not create a safety issue that would

necessitate right turn tapers and deceleration lanes. Project access is from private roadways within the Folsom Corporate Center and the City's minimum required throat depth is not applicable.

Potential geometric constraints and safety issues were evaluated in the traffic study and addressed as described above. No issues were identified that suggest atypical or unsafe frontage conditions that require additional analysis. Therefore, the proposed project would have a less than significant impact.

Question e: Less than Significant Impact. Consistent with the City of Folsom's Multi-Hazard Emergency Management Plan, the City maintains pre-designated emergency evacuation routes along major streets and thoroughfares. No aspect of the proposed project would modify these streets or preclude their continued use as an emergency evacuation route. The Project's internal drive isles have 25-foot inner/50-foot outer minimum turning radii to accommodate fire department access. In addition to the primary access to each project parcel, separate emergency vehicle access points are also provided. Lot 6 has one emergency vehicle access point located 170-feet east of the main access driveway along a private road. Lot 1 has two emergency vehicle access points located approximately 640-feet east and west of the main access driveway along a private road. The plans would be approved by the City Fire Department prior to project implementation; therefore, a less than significant impact to fire protection would occur and no mitigation would be necessary.

XVII. TRIBAL CULTURAL RESOURCES

TRIBAL CULTURAL RESOURCES:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a) Cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:				
i. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

As amended in 2014, Assembly Bill (AB 52), requires that the City of Folsom (City) provide notice to any California Native American tribes that have requested notice of projects subject to CEQA review and consult with tribes that responded to the notice within 30 days of receipt with a request for consultation. Section 21073 of the Public Resources Code (PRC) defines California Native American tribes as “a Native American tribe located in California that is on the contact list maintained by the NAHC for the purposes of Chapter 905 of the Statutes of 2004.” This includes both federally and non-federally recognized tribes. For the City of Folsom, these include the following tribes that previously submitted general request letters, requesting such noticing:

- Wilton Rancheria (letter dated January 13, 2020)
- Lone Band of Miwok Indians (letter dated March 2, 2016)
- United Auburn Indian Community (UAIC) of the Auburn Rancheria (letter dated November 23, 205 and updated per UAIC via email on September 29, 2021)

The purpose of consultation is to identify Tribal Cultural Resources (TCR) that may be significantly impacted by the proposed Project, and to allow the City to avoid or mitigate significant impacts prior to Project approval and implementation. Section 21074(a) of the PRC defines TCRs for the purpose of CEQA as: Sites, features, places, cultural landscapes (geographically defined in terms of the size and scope),

sacred places, and objects with cultural value to a California Native American tribe that are either of the following:

- a) Included or determined to be eligible for inclusion in the California Register of Historical Resources; and/or
- b) Included in a local register of historical resources as defined in subdivision (k) of Section 5020.1; and/or
- c) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Section 5024.1. In applying the criteria set forth in subdivision (c) of Section 5024.1 for the purposes of this paragraph, the lead agency shall consider the significance of the resource to a California Native American tribe.

Because criteria A and B also meet the definition of a Historical Resource under CEQA, a TCR may also require additional consideration as a Historical Resource. TCRs may or may not exhibit archaeological, cultural, or physical indicators and can only be identified by a culturally affiliated tribe, which has been determined under State law to be the subject matter expert for TCRs (ECORP 2022).

City Consultation

Assembly Bill 52

On September 21, 2021, the City of Folsom sent project notification letters to the three California Native American tribes named on the City's AB 52 contact list. The letters provided each tribe with a brief description of the Project and its location, contact information for the City's authorized representative, and a notification that the tribe has 30 days to request consultation. The 30-day response window closed on October 21, 2022.

The only tribe to respond was the UAIC. On September 29, 2021, the City received an email from Anna Cheng that acknowledged receipt of the City's notification letter and informed the City that the UAIC has a new point of all CEQA-related letters and documents, Anna Starkey. On September 30, 2021, the City received an email from Anna Starkey requesting consultation. The response indicated that there is a known TCR located west of the proposed Project boundary and requested access for a survey of the Project Area to ensure that the proposed Project does not extend into the TCR location.

On October 7, 2021, the City formally initiated consultation with UAIC and acknowledged the tribe's statement about a known TCR located in the vicinity. In the correspondence to the tribe, the City noted that a survey of the Project Area had been conducted recently and that a copy of the report would be provided to the tribe in advance of a meeting or further site visits.

On November 4, 2021, Anna Starkey responded to the City's separate SB 18 outreach (Section 2.2) and referenced AB 52 in her reply. (From this point forward, all correspondence between the City and UAIC was simultaneously under both AB 52 and SB 18.) She noted the cultural sensitivity of the property and requested a copy of the cultural resources survey report that was being prepared and indicated the tribe's desire to defer to the Shingle Springs Band of Miwok Indians, if they were consulting on the Project. The City responded on November 16, 2021 to confirm the plan to forward a copy of the cultural

resources survey report when it was completed and that Shingle Springs had already been provided the opportunity to consult.

Accordingly, on December 13, 2021, the City provided a copy of the cultural resources survey report (HELIX 2021) to UAIC for their review. Anna Starkey acknowledged receipt of the report the same day and stated that “for archaeological tribal cultural resources, UAIC believes that our standard unanticipated discoveries mitigation measure should suffice for this project.” In her response, she also inquired about the number of oak trees that are proposed for removal and how they will be mitigated for. She questioned if any heritage trees had been identified and whether an arborist report had been prepared. The City replied with a copy of the arborist report, and upon her review, she indicated that heritage trees (in general) are a significant TCR and should be protected and offered to provide language for use in the CEQA document. The City responded that staff are still awaiting information on the plans for the heritage tree, and that this information would be provided upon receipt.

On December 17, 2021, the City contacted UAIC to indicate that although there are many nonnative oaks on the property, there is a single heritage tree in the Project Area that will be preserved in place as part of the Project’s design, which is consistent with the City’s Tree Preservation Ordinance. The City provided a link to the ordinance and stated that it welcomed the submission of suggested CEQA language for staff consideration.

On January 3, 2022, UAIC provided a document to the City that expresses the UAIC’s belief that native heritage trees, in general, have significance to the Miwok and Maidu (Nisenan) people, and that conservation of heritage trees is important. The UAIC provided the language with the intent for it to be incorporated into the CEQA document, and therefore, would not be considered confidential information. A copy of the UAIC submittal is included in **Appendix I**.

Senate Bill 18

On behalf of the City, ECORP contacted the California NAHC on September 7, 2021, to request a list of tribal contacts under SB 18. The NAHC responded with the list on October 20, 2021. This list is usually different than the AB 52 list because it pulls from a different database at NAHC. Using the list provided, the City mailed project notices to the following tribes on October 26 and afforded them 90 days to respond to request consultation under SB 18 (ECORP 2022).

The 90-day response window closed on January 24, 2022.

- Buena Vista Rancheria of Me-Wuk Indians
- Chicken Ranch Rancheria of Me-Wuk Indians
- Colfax-Todds Valley consolidated Tribe
- Guidiville Indian Rancheria
- Lone Band of Miwok Indians
- Muwekma Ohlone Indian Tribe of the San Francisco Bay area
- Nashville Enterprise Miwok-Maidu-Nishinam Tribe
- North Valley Yokuts Tribe
- Shingle Springs Band of Miwok Indians

- The Confederated Villages of Lisjan
- Tsi Akim Maidu
- Tule River Indian Tribe
- United Auburn Indian Community
- Wilton Rancheria
- Yocha Dehe Wintun Nation

On November 4, 2021, Anna Starkey from UAIC responded to the notice. (From this point forward, all correspondence between UAIC and the City was carried out simultaneously relative to both AB 52 and SB 18.)

Among the remaining tribes noticed under SB 18, only one other tribe responded. On November 12, 2021, the Yocha Dehe Wintun Nation responded by email with a letter dated November 10, 2021, that stated that the Project is not within the aboriginal territories of the tribe, and referred the City to UAIC, Wilton Rancheria, and Shingle Springs. All three of these tribes had already received Project notices, as described above. None of the other tribes responded to the opportunity to consult.

Evaluation of Tribal Cultural Resources

Questions a (i): No Impact. Based on the records search at the NCIC and other efforts discussed in Section V, Cultural Resources, no resources listed or eligible for listing in the California Register of Historic resources of local register or historical resources were identified. The project would have no impact.

Question a (ii): Less than significant with mitigation. Information about tribal cultural resources under AB 52 and tribal cultural places under SB 18 was drawn from multiple sources, including the tribal consultation as summarized above, records searches and literature reviews with the California Historical Resources Information System, a review of existing ethnographic information, and a cultural resources survey (HELIX 2021) that included an analysis of buried site potential. Of these sources, most did not result in any information to indicate the presence of a tribal cultural resource or a tribal cultural place within the Project Area. Only the tribal consultation process, summarized above, produced information that requires further discussion.

The UAIC submitted information that heritage trees, in general, are important to the tribal community because they “have born witness to history and human interactions and are thought to hold a collective memory that is remembered and passed down from generation to generation. These resources also provide continuity between the past, present, and future.” UAIC also noted that “heritage trees not only provide an important ecological function, but they also play an important role in UAIC’s social and cultural identity” (**Appendix I**). According to the arborist survey for the project, one of the nine native oak trees present on the property is considered a heritage tree. This heritage tree will remain in place with a suitable buffer during construction to maintain tree integrity and minimize impact to the root zone, trunk, and canopy.

CEQA and SB 18 require that the City measure the information about the importance of heritage trees against the definitions of tribal cultural resources and tribal cultural places, as cited in Section 21074(a) of the PRC and Sections 5097.9 and 5097.995 of the PRC, respectively, while taking into account the

expert knowledge of the Tribe. First, Section 21074(a) of the PRC defines tribal cultural resource for the purpose of AB 52 and CEQA. While heritage oak trees are not resources that are made, modified, or moved by a human, and do not constitute cultural resources, and although the field survey by professional archaeologists did not reveal any indication that past human activity was associated with the specific heritage tree in the Project Area, the UAIC ascribes additional importance to heritage trees and recommended avoidance and preservation to the City. The information provided does not provide substantial evidence, as defined in PRC Section 21080, about the one oak tree would, specifically, qualify as a TCR, but the recommendation to avoid it is consistent with the City's Tree Preservation Ordinance and the Project's plans. In addition, UAIC informed the City that standard mitigation measures for unanticipated discovery would be sufficient for any TCRs that are archaeological in nature, if encountered during construction (see Mitigation Measure TCR-1, below). Second, Sections 5097.9 and 5097.993 of the PRC define the types of resources that would constitute a tribal cultural place pursuant to SB 18. Neither tribal consultation nor examination of other lines of evidence revealed the presence of any resource meeting these definitions.

Therefore, although the information provided about heritage trees does not meet the criteria for being considered a TCR under CEQA, the importance of heritage trees to the tribal community should be recognized as such, and taken into account for future project planning in Folsom. For this project, because the single heritage tree present on the property will be preserved in place, there would be no impact to a known TCR or a tribal cultural place. However, there remains the possibility that ground-disturbing activity could reveal the presence of a TCR or tribal cultural place that is archaeological in nature, and if present, the effect could be adverse. As supported by UAIC, implementation of unanticipated discovery procedures, as provided in Mitigation Measure TCR-1 below, would reduce that impact to a less than significant level (ECORP 2022).

Mitigation Measure TCR-1: Unanticipated Discovery of Tribal Cultural Resources. If any suspected TCRs are discovered during ground disturbing construction activities, all work shall cease within 100-feet of the find, or an agreed upon distance based on the Project Area and nature of the find. A Tribal Representative from a California Native American tribe that is traditionally and culturally affiliated with a geographic area shall be immediately notified and shall determine if the find is a TCR (PRC §21074). The Tribal Representative will make recommendations for further evaluation and culturally appropriate treatment as necessary. If deemed necessary by the City, a qualified cultural resources specialist meeting the Secretary of Interior's Standards and Qualifications for Archaeology may also assess the significance of the find in joint consultation with Native American Representatives to ensure that Tribal values are considered. Work at the discovery location may not resume until the City, in consultation as appropriate and in good faith, determines that all necessary investigation and treatment of the discovery under the requirements of CEQA, including AB52, have been satisfied.

XVIII. UTILITIES AND SERVICE SYSTEMS

UTILITIES AND SERVICE SYSTEMS:		Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
Would the project:					
a)	Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b)	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c)	Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d)	Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e)	Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f)	Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g)	Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Environmental Setting

Existing utilities on the project site include SMUD for electricity, PG&E underground gas lines, AT&T underground telephone lines, City of Folsom for solid waste disposal, and City of Folsom water and sewer facilities. The City of Folsom employs a design process that includes coordination with potentially affected utilities as part of project development. Identifying and accommodating existing utilities is part of the design process, and utilities are considered when finalizing public project plans. The City of Folsom coordinates with the appropriate utility companies to plan and implement any needed accommodation of existing utilities, including water, sewer, telephone, gas, electricity, and cable television lines. Based on the results of an initial request for comments from the utility providers, all utility services are able to accommodate the proposed project.

Evaluation of Utilities and Service Systems

Questions a, b, e: No Impact. The City of Folsom is responsible for managing and maintaining its wastewater collection system. This system ultimately discharges into the Sacramento Regional County Sanitation District interceptor sewer system. Wastewater is treated at the Sacramento Regional Wastewater Treatment Plant, located in Elk Grove (City of Folsom 2018).

In compliance with the 2006 State Water Resources Control Board (SWRCB) General Waste Discharge Requirements for Sanitary Sewer Systems, the City of Folsom adopted a Sewer System Management Plan (SSMP) on July 28, 2009. The SSMP has been revised every five years, with the newest version approved on July 23, 2019. The plan outlines how the municipality operates and maintains the collection system, and the reporting of all Sanitary Sewer Overflows (SSO) to the SWRCB's online SSO database. Because the City has sufficient capacity to accommodate any additional demand that could result from implementation of the proposed project, and because the City is in compliance with statutes and regulations related to wastewater collection and treatment, there would be **no impact** and mitigation would not be necessary.

Question c: Less than Significant Impact. Folsom's Public Works Department handles all stormwater management issues for the City, from design and construction of the storm drain system to operation and maintenance, and urban runoff pollution prevention (City of Folsom 2018). Stormwater drains would be installed throughout the site, and curb and gutter would be installed along the parking areas to collect stormwater flows and prevent flooding or ponding. On-site stormwater management facilities would include bioretention basins, Contech filters, and disconnected roof drains which would treat and dissipate stormwater prior to entering the City's system. With implementation of these measures, environmental impacts from expanding the stormwater facilities would be less than significant and no mitigation would be necessary.

Question d: Less than Significant Impact.

Water Supply

Folsom's Water Treatment Plant has a capacity of 50 million gallons per day. According to the City of Folsom General Plan Housing Element, the combination of treated and untreated water demands (through the time frame of the Housing Element which is 2021) are not anticipated to exceed the City's current water entitlements of 34,000 acre-feet annually (City of Folsom 2013). Because sufficient supplies are available, no additional facilities would need to be constructed or expanded and impacts would be less than significant.

Question f, g: No Impact. The City of Folsom provides solid waste, recycling, and hazardous materials collection services to its residential and business communities. In order to meet the State mandated 50 percent landfill diversion requirements stipulated under AB 939, the City has instituted several community-based programs. The City offers a door-to-door collection program for household hazardous and electronic waste, in addition to six "drop off" recycling locations within the City. An offsite sewer analysis was completed by Water Works Engineering, at the request of the City of Folsom. The analysis concluded that the backbone of the existing sewer collection system has the capacity to support the development (Water Works Engineering 2021).

After processing, solid waste is taken to the Kiefer Landfill, the primary municipal solid waste disposal facility in Sacramento County. The landfill facility sits on a site of 1,084-acres in the community of Sloughhouse. Currently 250-acres, the State permitted landfill is 660-acres in size and is of sufficient capacity to accommodate the solid waste disposal needs of the City of Folsom. Because the landfill serving the project area is of sufficient capacity to accommodate solid waste needs, no impact would occur, and no mitigation would be necessary.

XIX. MANDATORY FINDINGS OF SIGNIFICANCE

MANDATORY FINDINGS OF SIGNIFICANCE:	Potential Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No Impact
<p>Would the project:</p> <p>The lead agency shall find that a project may have a significant effect on the environment and thereby require an EIR to be prepared for the project where there is substantial evidence, in light of the whole record, that any of the following conditions may occur. Where prior to commencement of the environmental analysis a project proponent agrees to MMs or project modifications that would avoid any significant effect on the environment or would mitigate the significant environmental effect, a lead agency need not prepare an EIR solely because without mitigation the environmental effects would have been significant (per Section 15065 of the State CEQA Guidelines):</p>				
<p>a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are significant when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of past, present and probable future projects)?</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<p>c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Environmental Setting

Evaluation of Mandatory Findings of Significance

Question a: Less than Significant with mitigation. The preceding analysis indicates that the proposed project has the potential to adversely affect biological, cultural, and tribal cultural resources. See Sections IV, V, and XVII of this Initial Study for discussion of the proposed project’s potential impacts on these environmental issue areas. With implementation of the mitigation measures identified in those Sections, and compliance with City programs and requirements identified in this report, impacts would be reduced to a less than significant level. No significant or potentially significant impacts would remain.

Question b: Less than Significant with mitigation. While the project would indirectly contribute to cumulative impacts associated with increased urban development in the city and region, these impacts have previously been evaluated by the City and considered in development of the City's General Plan as set forth in this Initial Study. Key areas of concern are discussed in detail below.

Evaluation of cumulative biological resources impacts: Implementation of the proposed project, with continued growth within Folsom would contribute to continued loss of habitat for biological resources by converting undeveloped area to developed uses. There is currently no suitable habitat for special-status plant species in the project site and there have been no reported occurrences of special-status plant species on or adjacent to the project site in the CNDDB. Special-status plant species are not expected to occur in the project site or be impacted by the proposed project. No special-status wildlife species were observed in the project site during the biological reconnaissance survey and there are no reported occurrences in the CNDDB of special-status animal species in or adjacent to the project site. However, the project site provides marginal habitat for burrowing owl (*Athene cunicularia*) and white-tailed kite (*Elanus leucurus*) as well as habitat for nesting birds and raptors such as the mourning dove (*Zenaida macroura*), house finch (*Haemorhous mexicanus*), and acorn woodpecker (*Melanerpes formicivorus*). Nests were not observed during surveys; however, a variety of migratory birds have the potential to nest in and adjacent to the project site, in trees, shrubs and on the ground in vegetation. With implementation of Mitigation Measures BIO-01 and BIO-02, the potential impacts to the burrowing owl and the nesting birds and raptors due to project implementation would be reduced to a less than significant level. Additionally, there are a total of 14 trees found on the project site; one tree (#702) is on Lot 1 and the remaining trees are on Lot 6. Nine of the trees are blue oaks, three are cork oaks, and two are valley oaks. With implementation of Mitigation Measure BIO-03, trees in the project site would be protected from removal and from ground disturbance and potential impacts would be minimized. As a result, with implementation of Mitigation Measures BIO-01, -02, and -03 the proposed project would not result in significant cumulative impacts to protected biological resources, and no additional mitigation measures would be needed.

Evaluation of cumulative cultural resources impacts: A database records search was conducted for the project site, including a 0.25-mile buffer area, at the North Central Information Center at Sacramento State University. Additionally, a pedestrian survey was undertaken of the project site by a senior archaeologist. The City recognizes that sensitive and/or protected resources could be unintentionally discovered during project construction. With implementation of Mitigation Measures CUL-01 and CUL-02, the impacts relating to unanticipated discoveries would be reduced to a less than significant level and potentially cumulative effects would be avoided. No additional mitigation measures would be needed.

Evaluation of cumulative greenhouse gas (GHG) impacts: GHG emissions would be generated by the project during construction (vehicle engine exhaust from construction equipment, on-road hauling trucks, vendor trips, and worker commuting trips) and during long-term operation (electricity and natural gas use, electricity resulting from water consumption; solid waste disposal, and vehicle engine exhaust). GHG impacts were evaluated the City's Greenhouse Gas Reduction Strategy Consistency Checklist, which was completed by HELIX. The project would be consistent with the City's GHG Strategy through Mitigation Measures GHG-1 through -5. Mitigation Measure GHG-1 would provide a minimum of five percent more bicycle parking than required in the City's Municipal Code Section 17.57.090 (for a total of 54 bicycle parking spaces). Mitigation Measure GHG-2 would use high-performance diesel (also known as Diesel-HPR or Reg-9000/RHD) for all diesel-powered equipment utilized in construction of the project. Mitigation Measure GHG-3 would provide electric vehicle charging stations in five percent of

the total surface parking spaces on the project site (for a total of 16 EV charging stations). Mitigation Measure GHG-4 would divert to recycle or salvage a minimum 65 of nonhazardous construction and demolition waste generated at the project site in accordance with Appendix A4 (Residential) of the as outlined in the California Green Building Standards Code (2019 CALGreen). Mitigation Measure GHG-5 would comply with all applicable indoor and outdoor water efficiency and conservation measures required under 2019 CALGreen Tier 1, as outlined in the California Green Building Standards Code. With Mitigation Measures GHG-1 through -5, potentially cumulative impacts would be avoided, and no additional mitigation measures would be needed.

Evaluation of cumulative noise impacts: Noise impacts were evaluated in Noise Analysis, prepared by Bollard Acoustical, May 3, 2021 and revised by HELIX in 2021. Construction noise generated by the project would result in short-term substantial noise increases compared to baseline existing conditions. The implementation of Mitigation Measure NOI-1 would restrict construction to daytime and minimize noise levels to surrounding residential uses. With this mitigation, potentially cumulative impacts would be avoided, and no additional mitigation measures would be needed.

Evaluation of cumulative transportation impacts: Cumulative transportation impacts were evaluated in the Folsom Corporate Center Apartments Transportation Impact Study (T. Kear Transportation Planning and Management, Inc., 2021). Under existing 2021 conditions with the project, the westbound left-turn queue during the AM peak hour exceeds available storage, and the project is anticipated to add 1 vehicle to the queue. Additional queued vehicles can contribute to LOS impacts when queues are longer than available storage and “spill-back” can affect the capacity of adjacent lanes. In order to avoid impacts to the westbound left-turn queue during the AM peak, Mitigation Measure TRA-1 would be implemented. Additionally, under the EPAP 2026 conditions with the project, the westbound left-turn queue during the AM peak hour exceeds the available storage, and the project is anticipated to add 1 vehicle to the queue, contributing to potential LOS impacts. Similar to the existing 2021 conditions, in order to avoid impacts to the westbound left-turn queue, Mitigation Measure TRA-2 would be implemented. With implementation of Mitigation Measures TRA-1 and TRA-2, the project would have a less than significant effect on traffic operations under 2021 conditions and under 2026 conditions with the addition of project traffic. Therefore, the proposed project would result in a less than significant impact to project circulation under cumulative conditions.

Evaluation of cumulative tribal cultural resources impacts: The City of Folsom sent project notification letters to the three California Native American tribes named on the City’s AB 52 contact list. The only tribe to respond was the UAIC. On behalf of the City, ECORP contacted the California NAHC, to request a list of tribal contacts under SB 18. The two tribes to respond were UAIC and Yocha Dehe Wintun Nation. UAIC informed the City that standard mitigation measures, Mitigation Measure TCR-1, for unanticipated discovery would be sufficient for any TCRs that are archaeological in nature, if encountered during construction. As supported by UAIC, implementation of unanticipated discovery procedures, as provided in Mitigation Measure TCR-1, would reduce impacts to a less than significant level and therefore, potentially cumulative impacts would be avoided. No additional mitigation would be required.

Question c: Less than Significant Impact. Because of site conditions, existing City regulations, and regulation of potential environmental impacts by other agencies, the proposed project would not have the potential to cause substantial adverse effects on human beings as demonstrated in the evaluation contained in this Initial Study.

Table 33. LOS Summary¹ Cumulative Plus Project Conditions

Intersection	Traffic Control	AM Peak Hour						PM Peak Hour					
		Construction Year No Project			Construction Year + Project			Construction Year No Project			Construction Year + Project		
		Delay ²	LOS ³	Signal Warrant? ⁴	Delay	LOS	Signal Warrant?	Delay	LOS	Signal Warrant?	Delay	LOS	Signal Warrant?
Iron Point Road/McAdoo Drive	Signal	20.2	C	--	20.3	C	--	16.6	B	--	16.6	B	--
Iron Point Road/Oak Avenue Parkway	Signal	22.8	C	--	23.3	C	--	16.2	B	--	16.6	B	--
Iron Point Road/Rowberry Drive	Signal	16.5	B	--	16.6	B	--	24.3	C	--	24.4	C	--
Iron Point Road/Project Access	Side-St. STOP ⁵	--	--	--	11.3	B	No	--	--	--	18.0	C	No

Source: Griffin Cove Transportation Consulting 2018b.

Notes:

¹ Reference: Transportation Research Board, *Highway Capacity Manual – 6th Edition*, 2016.

² Average control delay (seconds per vehicle).

³ Level of service.

⁴ "Peak Hour" signal warrant from "Part 4 – Highway Traffic Signals" of the *California Manual on Uniform Traffic Control Devices*, November 7, 2014.

⁵ Delay value represents the worst-case movement/approach.

10.0 MITIGATION MONITORING AND REPORTING PROGRAM

A Mitigation Monitoring and Reporting Program (MMRP) has been prepared by the City per Section 15097 of the CEQA Guidelines and is presented in **Appendix J**.

11.0 INITIAL STUDY PREPARERS

City of Folsom

Steve Banks, Principal Planner

Mark Rackovan, Traffic Engineer

HELIX Environmental Planning, Inc.

Robert Edgerton, AICP CEP, Principal Planner

Julia Pano, Environmental Planner

Jason Runyan, Noise Specialist

Stephen Stringer, Senior Biologist

Stephanie McLaughlin, Field Biologist

Victor Ortiz, Air Quality Specialist

Martin Rolph, Air Quality/Energy Specialist

Clarus Backes, Senior Archaeologist

John DeMartino, GIS

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Attachment No. 23

SMAQMD ISMND Response Letter, dated March 24, 2022

SACRAMENTO METROPOLITAN



March 24, 2022

Steven Banks
 City of Folsom Community Development Department
 50 Natoma Street
 Folsom Cordova, CA 95630
sbanks@folsom.ca.us

Subject: Folsom Corporate Center Apartments Mitigated Negative Declaration (SAC202102624)

Dear Steven Banks:

Thank you for providing the Folsom Corporate Center Apartments Mitigated Negative Declaration (MND) to the Sacramento Metropolitan Air Quality Management District (Sac Metro Air District) for review. The project includes a general plan amendment, rezone, planned development permit, design review and tree removal permit, for the construction and operation of a 253-unit multi-family apartment community on two parcels in the Folsom Corporate Center. Sac Metro Air District staff comments to improve health and air quality and reduce greenhouse gas emissions (GHG) follow.

Comments on the MND

The Air Quality section of the MND includes measure AIR-1, requiring a mechanical ventilation system that accommodates filters having a Minimum Efficiency Reporting Value (MERV) rating of 13 or higher to reduce resident exposure to toxic air contaminant emissions from Highway 50. Note that this is already required by the 2019 Building Energy Efficiency Standards.

To provide further protection of residents, Sac Metro Air District recommends:

- The landscape plan includes a continuous vegetative barrier along the southern, western, and eastern perimeter of the project consistent with the Sac Metro Air District's *Landscaping Guidance for Improving Air Quality Near Roadways*.¹ If a continuous barrier along the perimeter is not feasible, provide dense plantings where feasible and especially between the outdoor gathering areas and Highway 50.

The GHG section of the MND notes that the project includes onsite photovoltaic electricity generation, demonstrating consistency with Folsom's GHG Reduction Strategy measure E-1, Building Energy Sector. Sac Metro Air District recommends the project consider additional energy related measures, which provide a co-benefit of reducing the urban heat island effect:

¹ Sac Metro Air District Landscaping Guidance:

<http://www.airquality.org/LandUseTransportation/Documents/LandscapingGuidanceforImprovingAirQualityNearRoadwaysMay2020V2.pdf>

- Install certified cool roofs. The California Energy Commission's Title 24, Part 6,² suggests an aged solar reflectance of at least 0.63 for low-sloped roofs and at least 0.20 for steep-sloped roofs, and a minimum thermal emittance of 0.75. The Cool Roof Rating Council provides a product directory³ of roofs to assist. Cool roofs reduce the temperature of the buildings, requiring less energy to keep the buildings cool in the summer.
- Install solar photovoltaic shade structures over the parking lot planned under the overhead power lines on lot 1 since tree planting will be constrained. This will reduce urban heat island effect from the parking lot, generate renewable energy, and provide shading to parked vehicles to reduce their emissions of volatile organic compounds.

Comments on Site Design

Sac Metro Air District commends the project for providing infill housing near jobs and commercial uses, which can lead to reduced vehicle miles traveled (VMT) and reduced emissions. To further provide the opportunity for residents to reduce VMT, supporting Folsom General Plan Policies M 2.1.3 – Pedestrian and Bicycle Linkages, M 3.1.1 – Access to Public Transit, and NCR 3.1.3 – Reduce Vehicle Miles Traveled, Sac Metro Air District recommends the following improvements in bicycle/pedestrian connectivity:

- Include a direct connection from the north side of lot 6 to Iron Point Road. Convenient access to the existing sidewalks and bike lanes on Iron Point Road will connect lot 6 residents to the nearby transit stop and other commercial areas along Iron Point Road.
- Include a complete sidewalk network along the unnamed road bordering lot 6 and along Rowberry Drive bordering lot 1 to minimize pedestrian barriers and provide safe, convenient connections for residents to the surrounding land uses.
- Consider including a pedestrian gate from lot 1 that could allow a future connection to the planned class 1 bicycle trail south of the project, along Highway 50.

Rules Statement

All projects are subject to Sac Metro Air District rules in effect at the time of construction. A link to a list of the most common rules that apply during construction is included in the footnote.⁴ A complete listing of rules is available at www.airquality.org or by calling 279-207-1122.

Naturally Occurring Asbestos

The project site is in an area that may contain naturally occurring asbestos, as identified on Sac Metro Air District's Naturally Occurring Asbestos in Eastern Sacramento County Parcels map.⁵ Areas identified on the map are required prior to construction to either submit an Asbestos Dust Mitigation Plan or test out of the requirements of the Airborne Toxic Control Measure for Construction, Grading, Quarrying, and Surface Operations. More information can be found on the Sac Metro Air District's website⁶ or by contacting Daniel Noakes at 916-826-6366 or dnoakes@airquality.org. Folsom's construction specifications also include a reminder of these requirements.

² California Energy Commission, Title 24, Part 6: https://www.energy.ca.gov/sites/default/files/2021-06/CEC-400-2018-020-CMF_0.pdf

³ Cool Roof Rating Council product directory: <https://coolroofs.org/directory>

⁴ Rules Statement: <http://www.airquality.org/LandUseTransportation/Documents/RulesAttachment10-2020Final.pdf>

⁵ Asbestos map: http://www.airquality.org/StationarySources/Documents/NOA_Parcels_redux.pdf

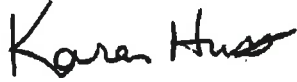
⁶ Sac Metro Air District's asbestos website: <http://www.airquality.org/businesses/asbestos/asbestos-in-soil>

Folsom Corporate Center Apartments Mitigated Negative Declaration

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Please contact me at 279-207-1131 or khuss@airquality.org if you have any questions regarding these comments.

Sincerely,



Karen Huss
Associate Air Quality Planner/Analyst

cc: Paul Philley, AICP, CEQA and Land Use Program Supervisor

Attachment No. 24

Folsom Corporate Center Planned Development Guidelines

FOLSOM CORPORATE CENTER PLANNED DEVELOPMENT GUIDELINES



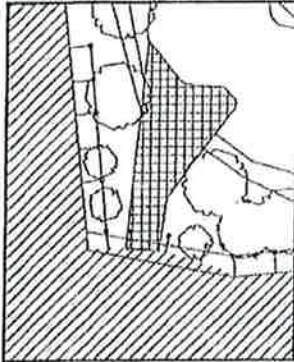
15 March 2000, revised 21 June 2000

DEVELOPMENT STANDARDS

- Total Gross Building Area: 1,425,000 square feet, provided all building and site designs meet all development standards.
- Building Setbacks:
 - Front yard: 30' along Iron Point Road.
 - Side yard: 5' or as required by building code.
 - Street side yard: 15'.
 - Rear yard: No requirement except as required by building code or other requirements.
 - Highway 50 frontage: 50 feet.
- Building Heights:
 - 4 story, not to exceed 80'* at parapets.
 - 3 story, not to exceed 60'* at parapets.
 - 2 story, not to exceed 40'* at parapets.
 - *Building height may increase at specific areas required for mechanical screening.
- Building Coverage: no requirement.
- Parking Requirements:
 - Offices: 1 space per 250 square feet of gross floor area.
 - Commercial uses and other ancillary retail: 1 space per 200 square feet of gross floor area.
- All required parking areas will meet City of Folsom requirements for dimensions, paving, disabled parking, and bicycle racks, per chapter 17.57 of the Folsom Zoning Codes.
- Pedestrian regulations:
 - Primary walkways will link street access, bus stops, parking areas, and buildings
 - Pedestrian safety and health will be a design focus.
 - Walkways will be landscaped to provide summer shade.
 - Parking areas will feature pedestrian connectors within parking stalls to facilitate safe travel through the parking area.
 - Textured/colored paving, or a change to materials, will delineate pedestrian connectors at intersections with parking areas and drives. Landscaping will delineate pedestrian walks elsewhere.
 - Disabled access will conform to State and Federal ADA regulations.
- Bicycle regulations:
 - Adequate space and access will be provided for bicycle racks per Folsom Zoning Code 17.57.090.
 - Bicycle racks will be provided near building entrances.
- Loading Areas:
 - Loading areas will be screened from public view by landscaping, walls, or other means to minimize their visibility from public streets. Where structural screen barriers are used, they shall be a minimum of 6 feet in height to adequately hide equipment and loading areas.
 - Materials used for screen barriers should be compatible and similar in quality to materials used for that site's buildings

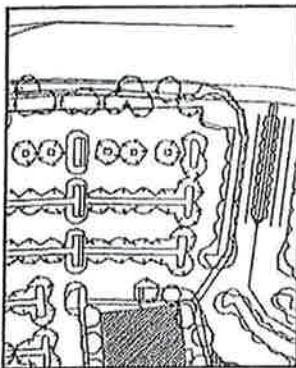
SITE DESIGN

Elements



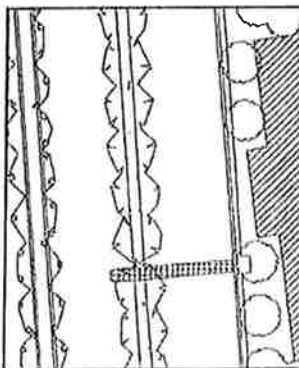
ENHANCED PAVING:

- Signifies entries, emphasizes intersections, creates informal meeting places.
- Adds clarity between pedestrian and driving routes.
- May be of embossed A.C. paving or colored concrete, or exposed aggregate.



PEDESTRIAN CONNECTORS

- Defined with landscaping and materials.
- Routes differentiated with specific pallettes of landscape materials.
- Will ease navigation through parking fields to and from a building.



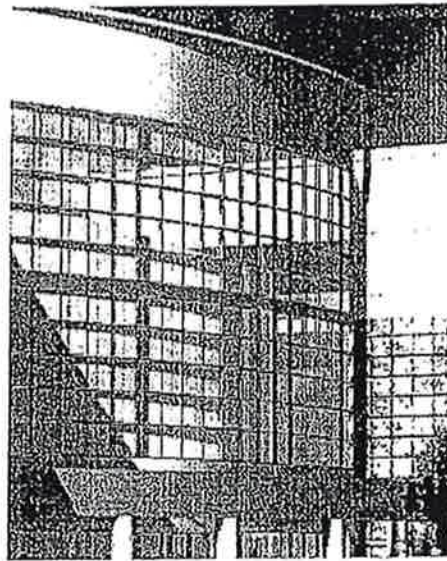
PEDESTRIAN CROSSINGS:

- Differentiated by enhanced paving.
- Located where pedestrian connectors intersect parking areas or drives.
- Not required at every row.

ARCHITECTURAL DESIGN

Concept

The Folsom Corporate Center Planned Development Guidelines, in respect to overall architectural design concepts, are intended to provide a framework for design, not restrict creativity. Building developments are encouraged to share a common architectural language, be sensitive to energy conservation, and respond to outward site forces.



Bordering by major roadways, the overall site is highly visible. Since the buildings will be visible from 360 degrees, no elevation can be called the back and design should respond accordingly. Building masses should be made human in scale, present varied elevations and use accent materials to add to the variety.

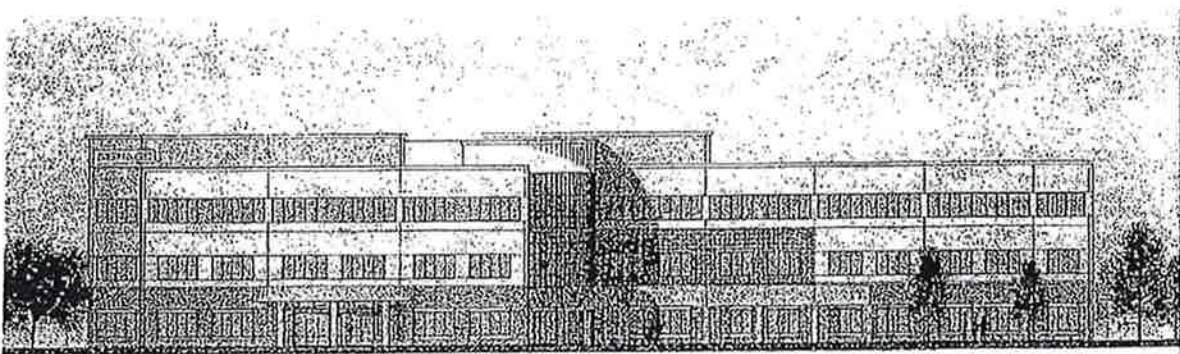
Materials such as tile, stone, glass, metal panels, and concrete, when used together, will reflect the area's modernity, diversity, and traditions, while maintaining a harmonious relationship with the other structures, developments, and communities in the vicinity. The materials used shall be consistent with those mentioned within these Planned Development Guidelines, as approved by the City of Folsom Planning Commission.

ARCHITECTURAL DESIGN

General

OVERALL PROJECT:

- Building forms relate to a specific site, providing variety and interest.
- Accentuated with color, lighting, and landscaping.
- Consistent use of elements will unify structures.
- Groves of Native Oaks are an important design element.



BUILDING WALLS:

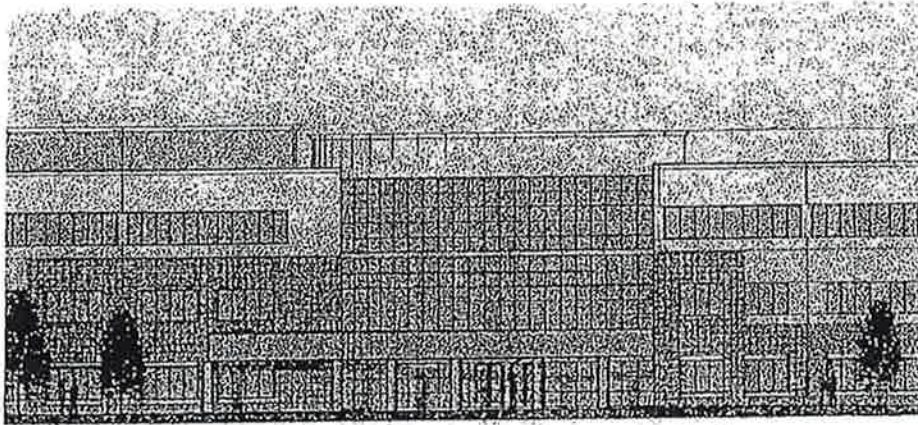
- The building base can be articulated and defined with darker colors or materials.
- Pedestrian visual interest shall be strengthened with patterns, textures or materials where appropriate, such as the use of a natural stone to accent a building's entries.
- Materials:
 - Primary surface materials shall cover no more than 80% of the exterior vertical walls and may be tilt-up concrete, pre-cast concrete, Exterior Insulated Finish System, glass, aluminum panels, aluminum window frames, or metal panels.
 - Accent materials will cover approximately 20% of the exterior vertical walls and may include stone, metal or aluminum panels, slate, or tile.

ARCHITECTURAL DESIGN

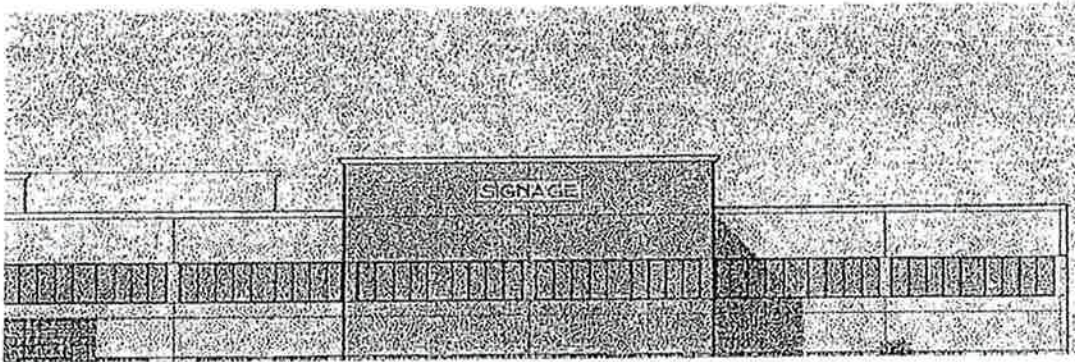
General

ENTRIES

- Entries shall be distinguished with accent materials, such as stone or slate, colored metal panels, and concrete.
- Enhanced paving shall be used at entrances, either embossed A.C. paving, colored concrete, or exposed aggregate.
- Entry facades may be accented with special lighting, secondary signage, graphics, or colors.



PARAPETS



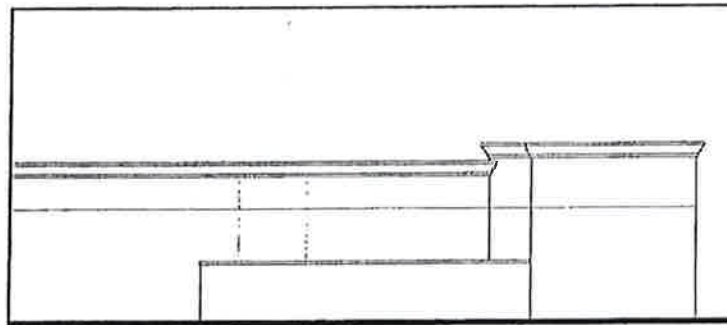
- Parapets shall conceal any roof mounted equipment as seen from adjacent roads and parking.
- Mechanical penthouse or screen forms are also acceptable when finished to match materials used on adjacent wall surfaces. The penthouse shall maintain a similar quality of construction.
- Changes in parapet height may be used to keep a human scale, accent entries, or articulate building elements.
- Cornices may be used to provide variety and express the building form at key locations.

ARCHITECTURAL DESIGN

General

SERVICE AREAS:

Service areas, loading docks, and trash enclosures shall be separated from public spaces by solid screen walls or landscape buffers. The screen wall design shall be consistent with the wall design and colors of the more prominent building elements.

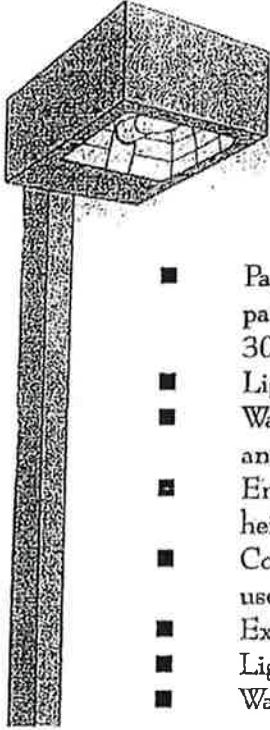


OTHER FEATURES:

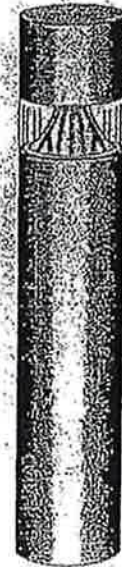
- Exposed roofing or canopies, visible from the ground, may be metal, glass, simulated slate, or Kalwall glazing.
- Sunscreens may be canvas, metal, or Kalwall.
- Exterior glass shall be high performance glazing, and may be clear, lightly tinted, reflective, or spandrel glass.

ARCHITECTURAL DESIGN

Exterior Lighting



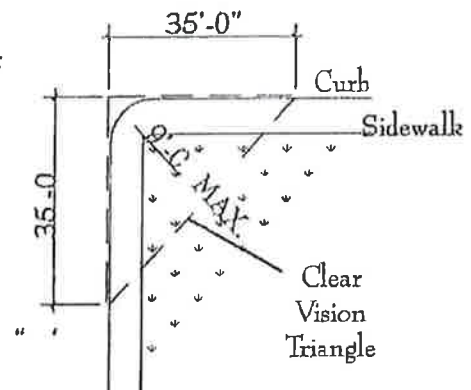
- Parking lot lighting shall be 15'-0" high at the circulation paths of the site, and no higher than 30'-0" at the parking areas.
- Lighting under power lines will be 15'-0" high.
- Wall mounted security lighting may be located at the service areas and at the perimeter of buildings.
- Entry drive lights may be decorative fixtures with a maximum height of 15'-0".
- Combination of accent, walkway & pedestrian-type lighting shall be used to illuminate pedestrian walkways.
- Exterior lighting shall be shielded to avoid off-site glare.
- Lights will be high pressure sodium.
- Wall areas may be highlighted by wall washing fixtures.



SIGNAGE

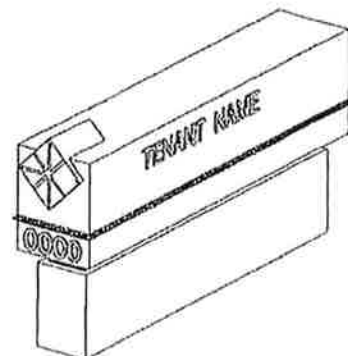
SIGN CRITERIA:

- Signs (directional and monument), visible from the street, shall be coordinated within the entire development for the mutual benefit of all occupants.
- While flexibility of design is encouraged, visual harmony shall be maintained.
- A sign application for each project shall be submitted to the City of Folsom Planning Department for review and approval prior to permit approval.
- All sign applications shall comply with the sign criteria, the City of Folsom Sign Requirements (chapter 17.59), city building codes, and the Uniform Sign Code, as from time to time amended.
- Conformity to the sign criteria shall be enforced by the landlord.
- Any non-conforming or unapproved sign shall be brought into conformity at the tenant's expense.
- Any sign between 30" and 6' above the adjacent grade at any corner formed by an intersection of 2 or more streets shall not obstruct the cross-visibility area as measured by a triangle having 2 sides 35' long and running along each curb line, and a third side connecting the ends of the other 2 lines.



MONUMENT SIGNS

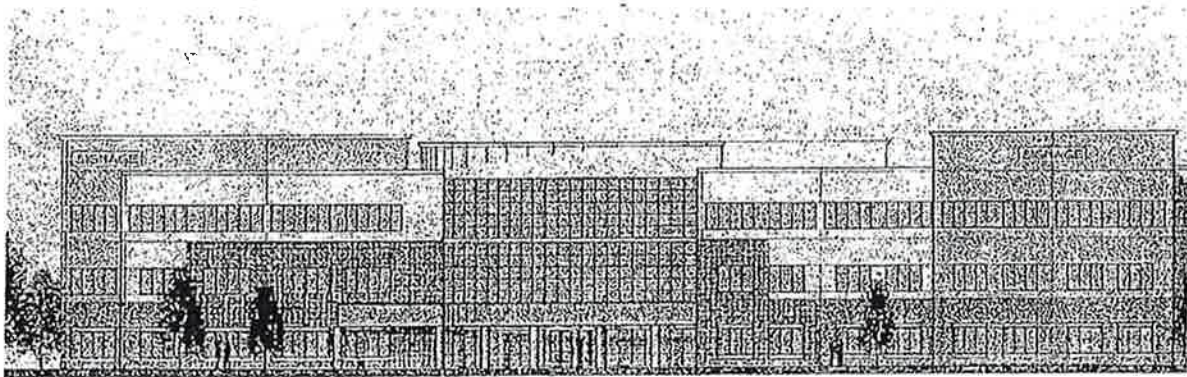
- Freestanding monument signs may be placed in the landscape areas facing common drives to identify tenants of individual buildings.
- Signs shall be adjacent to access driveways. No more than one sign per driveway shall be allowed.
- Signs shall be located a minimum of 15 feet from the back of the curb along Iron Point Road and any internal drive's right of way when space permits. Signs shall be placed outside of vehicular sight lines.
- Allowable signs are subject to all applicable ordinances of the City of Folsom Sign Ordinance.
- Sign size shall be a maximum of 6'-0" high by the maximum square footage of text for that sign.
- Monument signs shall be concrete, CMU and/or metal.
- Signs shall be illuminated by exterior ground uplighting. No internal illumination shall be permitted.



SIGNAGE

BUILDING SIGNS:

- The maximum allowable sign area is 1 1/2 square feet for each lineal foot of building frontage, up to a maximum of 150 square feet on each building.
- Wall signs and canopy signs shall count towards the maximum sign area.
- Building signs may be placed on building frontages facing a street, parking lot, or Highway 50.
- Signs shall not be located above the top of parapet, project more than 18 inches from the building wall, nor exceed 75 percent of the building frontage.
- Freestanding signs shall be set back 5 feet from the public right of way and located outside of any clear vision triangles (see page 16). Signs will comply with chapter 17.59 of the Folsom Zoning Code.



LANDSCAPE DESIGN

CONCEPT:

The Evergreen Folsom Project should serve as the transition zone between the manicured ornamental landscape of the Broadstone Development and the oak woodlands adjacent to Highway 50. The new landscape should avoid creating a hard edge between the natural and developed landscape by bringing both types of plantings into the project in a manner that allows the transition zones to occur within the parking lots and landscape easements, rather than at the perimeter of the sites.

Highway 50 passes through oak woodlands all along the south side of Folsom with the exception of the east end of this project, where no trees are present. As a part of this project, the oak woodlands will be extended east, along Highway 50, to provide a buffer from the freeway (these extended areas will not be considered a part of the "common areas").

Landscape areas will enhance the office environment. Plantings should assist users in orienting themselves on the site and keep them as comfortable as possible during the hot summer months. Attractive views should be enhanced and detrimental views should be screened.

DEVELOPMENT STANDARDS

- All work shall conform with the City of Folsom's applicable codes, including, but not limited to, the City of Folsom Landscaping Guidelines and Tree Preservation Ordinance.
- Those plants which have not performed well previously in Broadstone, particularly Alder and Red Oak, shall be used minimally, if at all.
- Native Oaks shall be retained throughout the project wherever possible. Existing Oaks which are to remain shall be protected from damage. Within a circle two times the size of the canopy diameter, irrigation systems shall be designed to minimize damage to feeder roots and plant species will be tolerant of very limited water after establishment.
- All landscape areas shall be automatically irrigated using water efficient distribution systems.
- Trees shall be a minimum of 15 gallons in size, except where smaller containers can be used to minimize damage to existing trees or promote better rooting habits among native species.
- A minimum of 50% of the total quantity of shrubs shall be 5 gallon size.
- Groundcovers and Perennials should be a minimum of 1 gallon in size.
- Plant Palettes are included in these Guidelines to provide consistency throughout the project but are not intended to prevent the addition of other species that may enhance the planting concept.

LANDSCAPE DESIGN

General

HIGHWAY FRONTAGE AND AREAS ADJACENT TO OAK WOODLANDS:

- A 50 foot landscape easement shall be provided along the Highway 50 frontage.
- An Oak Woodland shall be established along Highway 50 to act as a buffer for the Project.
- Trees should be clustered to provide controlled views of project buildings.
- Shrubs should be used as necessary to obscure the view of the freeway from parking areas and the ground floor of the buildings.
- Mowed or manicured turf should generally not be used in this area.
- Tree Palette:
 - Redbud (*Cercis spp.*) multi-trunked
 - Afghan Pine (*Pinus eldarica*)
 - Aleppo Pine (*Pinus halepensis*)
 - Stone Pine (*Pinus pinea*)
 - Chinese Pistache (*Pistachia chinensis*)
 - California Sycamore (*Platanus racemosa*) multi-trunked
 - Blue Oak (*Quercus douglasii*) some multi-trunked
 - Valley Oak (*Quercus lobata*)
 - Cork Oak (*Quercus suber*)
 - Interior Live Oak (*Quercus wislizenii*) multi-trunked
- Shrub Palette:
 - Strawberry Tree (*Arbutus spp.*)
 - Manzanita (*Arctostaphylos spp.*)
 - California Lilac (*Ceanothus spp.*)
 - Parney Cotoneaster (*Cotoneaster lacteus*)
 - Silverberry (*Eleagnus pungens "Fruitlandi"*)
 - Molate Fescue (*Festuca rubra "Molate"*)
 - Toyon (*Heteromeles arbutifolia*)
 - Assorted Ornamental Grasses (*Miscanthus sinensis, Muhlenbergia rigens, Pennisetum spp.,*)

IRON POINT ROAD FRONTAGE

- A 30 foot Landscape Easement shall be maintained along the frontage of Iron Point Road.
- A pedestrian pathway shall occur along the entire length of Iron Point Road. The pathway shall meander except in areas of restricted width where existing boulder rip-rap is installed. The pathway in constricted areas shall be adjacent to the back of curb.
- Street trees shall be randomly clustered in a quantity at least equal to one tree per 35 feet of linear frontage (excluding driveways). Areas where rock rip-rap constricts the frontage area are exempt from this requirement. Street trees will be located at least 5' from the street curb and the meandering sidewalk. Street trees will be planted a maximum of 10 feet from the street curb or the back of sidewalk, as appropriate.

LANDSCAPE DESIGN

General

IRON POINT ROAD FRONTAGE (CONTINUED):

- The street trees along Iron Point Road shall not include White Alder (*Alnus rhombifolia*), which are planted on the other side of the street, due to severe borer infestations of this species in the past 5 years.
- The primary ground-plane planting shall be turf, which will meander with the walk.
- Shrub and groundcover plantings should be kept simple in order to emphasize project entries. These areas should occur primarily behind the meandering walk.
- Parking areas shall be screened by plant material or landform to a minimum height of 30 inches at plant maturity unless such screening obscures visibility at intersections.
- Tree Palette:
 - Afghan Pine (*Pinus eldarica*)
 - Aleppo Pine (*Pinus halepensis*)
 - Stone Pine (*Pinus pinea*)
 - London Planetree (*Platanus acerifolia* "Bloodgood")
 - Burr Oak (*Quercus macrocarpa*)
 - Cork Oak (*Quercus suber*)
- Shrub and Groundcover Palette:
 - Manzanita (*Arctostaphylos* spp.)
 - California Lilac (*Ceanothus* spp.)
 - Prostrate Cotoneaster (*Cotoneaster dameri* "Lowfast")
 - Compact Escallonia (*Escallonia* x. "Terri")
 - Juniper (*Juniperus* spp.)
 - Coffeeberry (*Rhamnus californica* "Eve Case")
 - Dwarf Indian Hawthorn (*Rhaphiolepis indica* "Ballerina")
 - Evergreen Current (*Ribes viburnifolium*)
 - Prostrate Rosemary (*Rosmarinus officinalis* spp.) deep blue varieties
 - Compact Laurustinus (*Viburnum tinus* "Spring Bouquet")

DRIVEWAY INTERSECTIONS

- Plantings adjacent to the driveways at Iron Point Road should maximize seasonal color and use a variety of colors and textures to draw attention to the intersection.
- Formal arrangements of plantings should be considered to increase the contrast with the streetscape plantings.
- All intersections should not be planted in the same manner in order to assist users in orientation.
- Mature plantings should never obscure visibility for drivers.

LANDSCAPE DESIGN

General

DRIVEWAY INTERSECTIONS (CONTINUED):

- Tree Palette:
 - Crape Myrtle (*Lagerstroemia x.*) Indian tribe hybrids
 - Flowering Pear (*Pyrus calleryana* varieties)
 - Afghan Pine (*Pinus eldarica*)
 - Aleppo Pine (*Pinus halepensis*)
 - Stone Pine (*Pinus pinea*)
 - Coast Redwood (*Sequoia sempervirens*) background as space allows
- Shrubs and Groundcovers: Plants from the Frontage Palette should be used to provide visual continuity with the intersections and the streetscape. Accent plantings are to be chosen at the designer's discretion.

INTERNAL ACCESS ROADS:

- Access roads within the various sites connect the parking areas and establish a major organizing element within the project. The landscape treatment should enhance this organization by emphasizing these roads.
- Driveways connecting to parking areas should be emphasized with accent plantings, but to a lesser extent than the driveway intersections at Iron Point Road.
- Mature plantings should never obscure visibility for drivers.
- Tree Palette:
 - Strawberry Tree (*Arbutus unedo*)
 - Redbud (*Cercis spp.*) multi-trunked
 - Crape Myrtle (*Lagerstroemia x.*) Indian tribe hybrids
 - Tulip Tree (*Liriodendron tulipifera*)
 - Stone Pine (*Pinus pinea*)
 - Flowering Pear (*Pyrus calleryana* "Aristocrat")
- Shrub and Groundcover Palette:
 - Lily of the Nile (*Agapanthus orientalis*)
 - Emerald Carpet Manzanita (*Arctostaphylos x.* "Emerald Carpet")
 - Prostrate Cotoneaster (*Cotoneaster dammeri* "Lowfast")
 - Fortnight Lily (*Dietes vegeta*)
 - Dwarf Indian Hawthorn (*Rhaphiolepis indica* "Ballerina")
 - Prostrate Rosemary (*Rosmarinus officinalis spp.*) deep blue varieties
 - Star Jasmine (*Trachelospermum jasminoides*)
 - Turf (Turf-Type Tall Fescue blends)

LANDSCAPE DESIGN

General

PARKING LOTS:

- Trees shall be interspersed throughout the parking areas to shade at least 40% of the parking area, including access roads, after 15 years of growth.
- Two distinctive types of planting will occur within the project parking lots consisting of “native” areas and an ordered planting of more exotic species. Plants in “native” areas should appear to be indigenous to the area.
- The boundary between the two planting zones should meander through each project site, with the non-native plantings connecting to frontage and building areas and the native plantings connecting to the Highway 50 corridor and oak woodland plantings.
- The proportion of one type of planting in relation to the other should vary from site to site as appropriate. For example, a site immediately adjacent to an oak woodland might have 80% of the parking area devoted to more native plantings, while a site further east might have a much higher percentage of non-native plantings.
- Spacings between plants should be more random in the “native” zone.
- Where parking area divider planters are perpendicular to significant pedestrian traffic, turf grass should be considered in the non-native areas and un-mowed Molate Fescue in the native areas.
- “Native” Tree Palette:
 - Strawberry Tree (*Arbutus unedo*) some multi-trunked
 - European Hackberry (*Celtis australis*)
 - Redbud (*Cercis spp.*) multi-trunked
 - Afghan Pine (*Pinus eldarica*)
 - Aleppo Pine (*Pinus halepensis*)
 - Stone Pine (*Pinus pinea*)
 - Chinese Pistache (*Pistachia chinensis*)
 - London Planetree (*Platanus acerifolia* “Bloodgood”) most multi-trunked
 - Coast Live Oak (*Quercus agrifolia*) some multi-trunked
 - Blue Oak (*Quercus douglasii*) some multi-trunked
 - Holly Oak (*Quercus ilex*)
 - Valley Oak (*Quercus lobata*)
 - Burr Oak (*Quercus macrocarpa*)
 - Cork Oak (*Quercus suber*)
- “Native” Shrub and Groundcover Palette:
 - Emerald Carpet Manzanita (*Arctostaphylos* x. “Emerald Carpet”)
 - McMinn Manzanita (*Arctostaphylos* d. “Howard McMinn”)
 - Dwarf Coyote Brush (*Baccharis pilularis*)
 - California Lilac (*Ceanothus spp.*) lower species or varieties
 - White Rockrose (*Cistus hybridus*)
 - Molate Fescue (*Festuca rubra* “Molate”)
 - Assorted Ornamental Grasses (*Miscanthus sinensis*, *Muhlenbergia rigens*, *Pennisetum spp.*)

LANDSCAPE DESIGN

General

PARKING LOTS (CONTINUED):

- Non-Native Tree Palette:
 - Crape Myrtle (*Lagerstroemia x.*) Indian tribe hybrids
 - London Planetree (*Platanus acerifolia* "Bloodgood") standard
 - Flowering Pear (*Pyrus calleryana* "Aristocrat")
 - Chinese Elm (*Ulmus parvifolia* "Drake")
 - Sawleaf Zelkova (*Zelkova serrata*)
- Non-Native Shrub and Groundcover Palette:
 - Prostrate Cotoneaster (*Cotoneaster dammeri* "Lowfast")
 - Fortnight Lily (*Dietes vegeta*)
 - Juniper (*Juniperus spp.*)
 - Dwarf Indian Hawthorn (*Rhaphiolepis indica* "Ballerina")
 - Prostrate Rosemary (*Rosmarinus officinalis spp.*) deep blue varieties
 - Star Jasmine (*Trachelospermum jasminoides*)
 - Turf (Turf-Type Tall Fescue blends)

BUILDING AREAS:

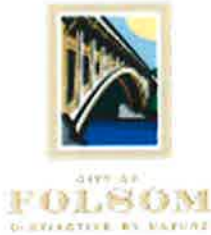
- Deciduous trees may be used to shade the south and west sides of the buildings where they will not obstruct significant architectural features.
- Trees should be located to avoid contact with buildings at maturity.
- Utility areas should be screened from view but plantings should not obstruct access to utility areas.
- Irrigation should be configured to avoid spraying windows.
- Tree Palette:
 - Maple (*Acer spp.*)
 - Strawberry Tree (*Arbutus unedo*)
 - Crape Myrtle (*Lagerstroemia x.*) Indian tribe hybrids
 - Tulip Tree (*Liriodendron tulipifera*)
 - Afghan Pine (*Pinus eldarica*)
 - Flowering Pear (*Pyrus calleryana* "Aristocrat")
 - Coast Redwood (*Sequoia sempervirens*)

LANDSCAPE DESIGN

General

BUILDING AREAS (CONTINUED)

- Shrub and Groundcover Palette:
 - Lily of the Nile (*Agapanthus orientalis*)
 - Emerald Carpet Manzanita (*Arctostaphylos* x. "Emerald Carpet")
 - McMinn Manzanita (*Arctostaphylos* d. "Howard McMinn")
 - Prostrate Cotoneaster (*Cotoneaster dammeri* "Lowfast")
 - Fortnight Lily (*Dietes vegeta*)
 - Prostrate Juniper (*Juniperus* c. "San Jose") or similar varieties
 - Deer Grass (*Muhlenbergia rigens*)
 - Heavenly Bamboo (*Nandina domestica*)
 - Dwarf Indian Hawthorn (*Rhaphiolepis indica* "Ballerina")
 - Evergreen Current (*Ribes viburnifolium*)
 - Prostrate Rosemary (*Rosmarinus officinalis* spp.) deep blue varieties
 - Star Jasmine (*Trachelospermum jasminoides*)
 - Compact Laurustinus (*Viburnum tinus* "Spring Bouquet")
 - Dwarf Periwinkle (*Vinca minor*) shade only
 - Assorted Ornamental Grasses (*Miscanthus sinensis*, *Muhlenbergia rigens*, *Pennisetum* spp.,)
 - Turf (Turf-Type Tall Fescue blends)



Folsom City Council Staff Report



MEETING DATE:	5/10/2022
AGENDA SECTION:	Old Business
SUBJECT:	Resolution No. 10831 - A Resolution of the City Council of the City of Folsom Approving an Affordable Housing Loan in the Amount of \$588,265.55 to Bidwell Place, LP for Construction of the 75-unit Bidwell Place Affordable Multifamily Project, and Appropriation of Funds
FROM:	Community Development Department

RECOMMENDATION / CITY COUNCIL ACTION

Move to Adopt Resolution No. 10831 - A Resolution of the City Council of the City of Folsom Approving an Affordable Housing Loan in the Amount of \$588,265.55 to Bidwell Place, LP for Construction of the 75-unit Bidwell Place Affordable Multifamily Project, and Appropriation of Funds.

BACKGROUND / ISSUE

On May 6, 2020, the Planning Commission approved the Bidwell Place Mixed-Use project. The Bidwell Place project is a 75-unit 100 percent affordable multifamily, mixed use housing project on the 3.24-acre Bank of America site located on East Bidwell Street between Rumsey Way and Market Street. The development, which is currently under construction, includes studio, one-bedroom, and two-bedroom floor plans and will serve a mix of extremely low, very-low- and low-income households.



The project is being financed by Federal Low Income Housing Tax Credits and tax-exempt Private Activity Bonds and all the approved project underwriting were based on a standalone asset that would be regulated as affordable housing for 55 years. As a result, the project site must be parcelized to separate the affordable housing community asset from the commercial retail property (Bank of America). At time of project approval, staff and the applicant were under the impression that separation of the Bank of America commercial use from the residential portion of the proposed project would be achieved by a simple lot line adjustment. However, through further surveying and title search it was discovered that, although there are two Assessor Parcel Numbers (APNs) associated with the site, these parcels only exist for assessment purposes and have not been mapped. As a result, the applicant is in the process of requesting Planning Commission approval for a Tentative Parcel Map to subdivide the existing 3.24-acre property into two individual parcels. The two newly created parcels will consist of a 1.11-acre parcel (403 East Bidwell Street) and a 2.07-acre parcel (425 East Bidwell Street) within the East Bidwell Mixed Use Overlay of the General Commercial zone of the City of Folsom.

In accordance with the Folsom Municipal Code, Section 16.32.040, the project is now subject to parkland dedication in-lieu fees (Quimby) as part of this subsequent Tentative Parcel Map approval request. Because this fee was not previously anticipated, the additional fees place a financial burden on the project which the affordable housing project cannot absorb. As such, City Council is being asked to approve a supplemental housing loan to the applicant in the exact amount of the levied Quimby parkland dedication in-lieu fees to offset this financial burden on the project. This loan will go directly towards the payment of the Quimby fee and will not provide any additional financial benefit to the project.

POLICY / RULE

Government Code section 66477, commonly referred to as the Quimby Act, establishes the statutory authority by which a local government may require the dedication of land or the payment of fees for park purposes (Government Code §66477(a).). In conformance with section 66477, the City of Folsom has adopted a parkland dedication ordinance, which allows the City to impose park dedication and/or fee payment obligations in conjunction with the consideration and approval of a tentative map or parcel map.

This loan requires City Council approval.

ANALYSIS

The Bidwell Place project is being financed with 4 percent non-competitive tax credits, tax-exempt bonds, developer equity, and a \$4.15 million affordable housing loan from the City. In addition, the project received a \$150,693.75 grant (Resolution No. 10630) from City to offset the loss of 60 sewer fee credits that had previously been approved for the project.

The project was financed in 2021 with a forward locked permanent debt rate. All sources and uses of financing were locked at that milestone. At the time of construction loan closing, the California Construction Cost Index (CCCI) was 7102. The CCCI, as of March of 2022, is 8736,

which represents a 23 percent increase from the previous year. And, although most of Bidwell Place's construction contracts were executed at the construction loan closing, several remaining trades experienced significant cost increases and those increased costs have been passed onto the project. In addition, it is important to note that as a 100 percent affordable housing project, all rental income is capped by federal and state agencies and cannot be increased to generate additional income for the project. For these reasons, the project cannot absorb the additional costs associated with the previously unanticipated Quimby fees.

Financial support of this affordable housing project is consistent with the City's Housing Element Goal of facilitating affordable housing. Based on City Council's previous commitment to award financial assistance to this project, staff is supportive of the developer's request for a \$588,265.55 supplemental affordable housing loan to offset the Quimby fees.

Loan Terms

A term sheet outlining the conditions of the loan has been prepared and is included in the staff report (Attachment 2). Key specifics of this term sheet include:

1. Loan amount of \$588,265.55
2. 3% simple interest
3. 50-year repayment term

FINANCIAL IMPACT

Funding for the requested affordable housing loan for this project is available from the City's Housing Fund (Fund 238). An appropriation will be required in the amount of \$588,265.55. There are sufficient funds available for this additional appropriation. This funding source does not impact the City's General Fund.

ENVIRONMENTAL REVIEW

The Bidwell Place project was determined by the Planning Commission on May 6, 2020 to be categorically exempt from the California Environmental Quality Act (CEQA) under Public Resources Code Sections 21159.21 and 21159.23 as further described in Sections 15192 and 5194 (Affordable Housing Exemption) of the CEQA Guidelines.

ATTACHMENTS

1. Resolution No. 10831 - A Resolution of the City Council of the City of Folsom Approving an Affordable Housing Loan in the Amount of \$588,265.55 to Bidwell Place, LP for Construction of the 75-unit Bidwell Place Affordable Multifamily Project, and Appropriation of Funds
2. Supplemental Subordinate Loan Term Sheet

3. Tentative Parcel Map Exhibit

Submitted,

A handwritten signature in blue ink, appearing to read "Pam Johns", followed by a long horizontal line extending to the right.

Pam Johns, Community Development Director

ATTACHMENT 1

Resolution No. 10831 - A Resolution of the City Council of the City of Folsom Approving an Affordable Housing Loan in the Amount of \$588,265.55 to Bidwell Place, LP for Construction of the 75-unit Bidwell Place Affordable Multifamily Project, and Appropriation of Funds

RESOLUTION NO. 10831

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF FOLSOM APPROVING
A LOAN IN THE AMOUNT OF \$588,265.55 TO BIDWELL PLACE, LP FOR
CONSTRUCTION OF THE 75-UNIT BIDWELL PLACE AFFORDABLE
MULTIFAMILY PROJECT, AND APPROPRIATION OF FUNDS**

WHEREAS, on May 6, 2020, the Planning Commission approved the Bidwell Place Mixed-Use project which is currently under construction and will serve a mix of extremely low, very-low- and low-income households.; and

WHEREAS, the project is financed by Federal Low Income Housing Tax Credits, tax-exempt Private Activity Bonds and an affordable housing loan and grant from the City; and

WHEREAS, all the project was financed as a stand-alone affordable housing community; and

WHEREAS, for regulatory consistency, the project site must be parcelized to separate the affordable housing community from the commercial retail property; and

WHEREAS, at the time of project approval, staff and the applicant were under the impression that separation of the residential use from the commercial portion of the proposed project would be achieved by a lot line adjustment; and

WHEREAS, through further surveying and title search, it was discovered that although there are two Assessor Parcel Numbers (APNs) associated with the site, the parcels only exist for assessment purposes and are not mapped; and

WHEREAS, the applicant is in the process of requesting Planning Commission approval for a Tentative Parcel Map to subdivide the existing 3.24-acre property into two individual parcels; and

WHEREAS, in accordance with the Folsom Municipal Code, Section 16.32.040, the project will be subject to parkland dedication in-lieu fees (Quimby) as part of the subsequent Tentative Parcel Map approval request; and

WHEREAS, to off-set the additional cost to the project associated with the previously unanticipated Quimby parkland dedication /parkland in lieu fee, the developer is requesting an affordable housing loan from the City to assist with the construction of the Bidwell Place project; and

WHEREAS, providing financial assistance to affordable housing projects is consistent with the Goal H-3: Facilitating Affordable Housing in the City's Housing Element; and

WHEREAS, the requested loan amount of \$588,265.55 is appropriate as it represents the

monetary value of the unanticipated Quimby parkland dedication / parkland in lieu fee that Bidwell Place is subject to as part of the Tentative Parcel Map request; and

WHEREAS, funding for the requested affordable housing loan is available from the City's Housing Fund (Fund 238).

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom hereby approves an affordable housing loan in the amount of \$588,265.55 to Bidwell Place, LP for construction of the 75-unit Bidwell Place affordable multifamily project.

BE IT FURTHER RESOLVED that the Finance Director is directed to appropriate \$588,265.55 from the City's Housing Fund (Fund 238) for purpose of this Resolution.

PASSED AND ADOPTED this 10th day of May 2022, by the following roll-call vote:

AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Kerri Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

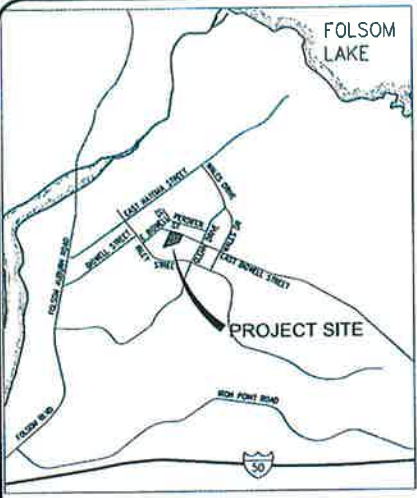
ATTACHMENT 2
Supplemental Subordinate Loan Term Sheet

Supplemental Subordinate Loan Term Sheet
Bidwell Place Apartments
City of Folsom
April 25, 2022

1. **Lender:** City of Folsom, a municipal corporation
2. **Borrower:** Bidwell Place, LP, a California limited partnership
 - i. **General Partners**
 1. St. Anton Bidwell Place, LLC (Co-GP)
 2. PacH Anton South Holdings, LLC (Managing GP) (Parent Company: Pacific Housing, Inc.)
 3. Bank of America, N.A. – Tax Credit Investor
3. **Loan Amount:** \$588,265.55
4. **Purpose:** Construction of the 75-unit Bidwell Place Affordable Multifamily Project
5. **Timing of Funding:** The City's loan will be funded in conjunction with when the Quimby Park Fee is due and payable.
6. **Interest Rate:** 3% per annum - simple
7. **Annual Payments:** Repayment of the principal amount of the loan together with accrued interest will be in a single balloon payment at loan maturity.
8. **Term:** 50 years.
9. **Balloon Payment:** At the expiration of the loan term, 100% of the principal balance of the loan and all accrued interest will be due.
10. **Refinancing:** In the event of refinancing, the City will subordinate to the new senior loan on substantially similar terms as the original financing.
11. **Security:** City's loan will be secured by a deed of trust junior to construction and permanent financing sources set forth.

ATTACHMENT 3
Tentative Parcel Map Exhibit

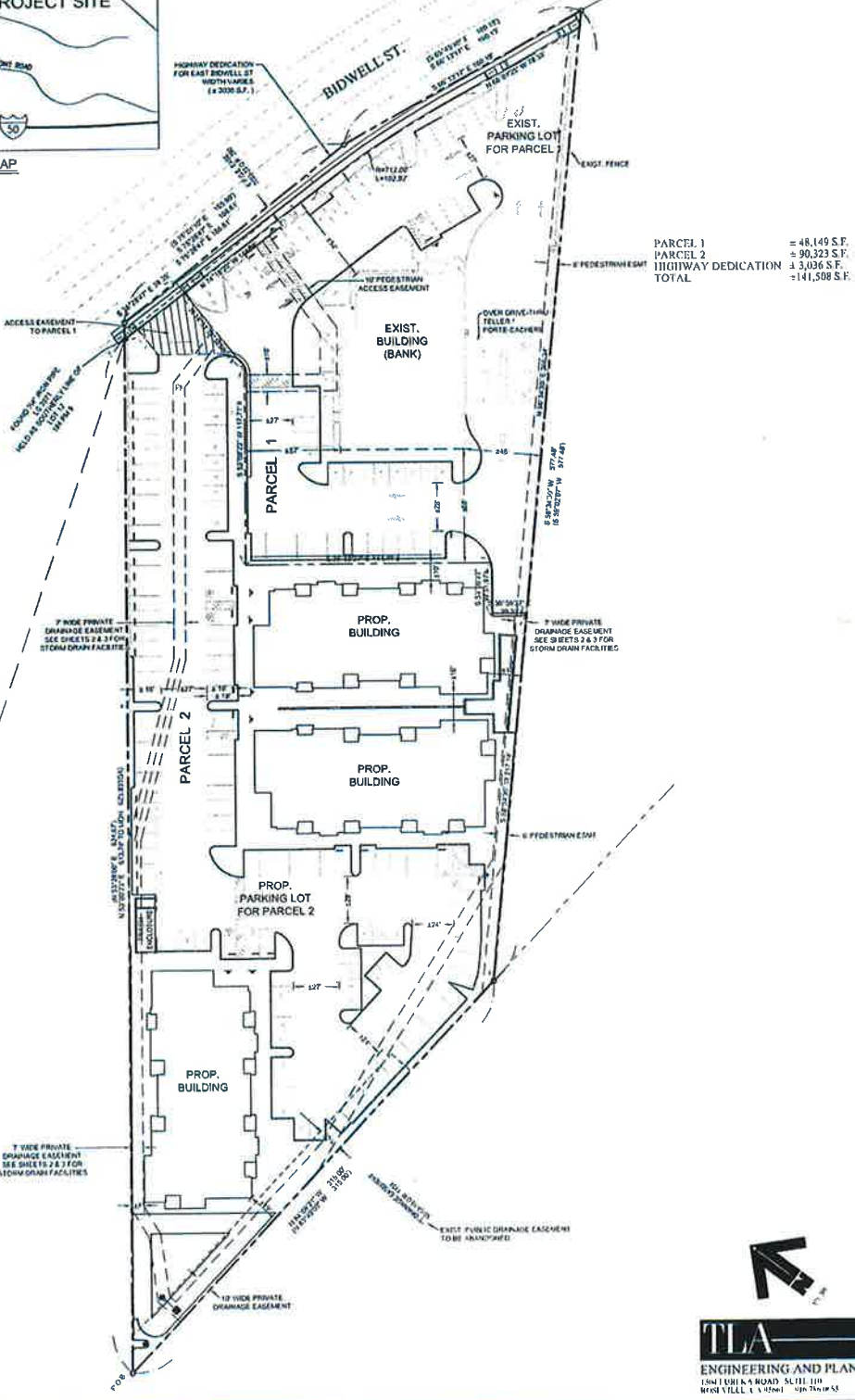
TENTATIVE PARCEL MAP
BIDWELL PLACE
 APN 071-0190-060 & 061
 403 & 425 EAST BIDWELL STREET, FOLSOM, CALIFORNIA
 LOTTING PLAN
 SHEET 1 OF 3
 DECEMBER 2021



LOCATION MAP

SHEET INDEX

SHEET 1	TENTATIVE MAP-LOTTING PLAN
SHEET 2	TENTATIVE MAP-GRADING PLAN
SHEET 3	TENTATIVE MAP-UTILITY PLAN

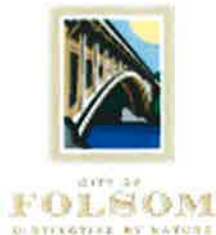


PARCEL 1 = 48,149 S.F.
 PARCEL 2 = 90,323 S.F.
 HIGHWAY DEDICATION TOTAL = 141,508 S.F.

TLA
 ENGINEERING AND PLANNING
 1841 FORTY-NINE ROAD SUITE 110
 RED HILL, CA 95670

File: C:\Users\jstevens\OneDrive\Documents\071-0190-060-061\02_Sheet1.dwg 05/10/2022 1:25pm jstevens

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Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	New Business
SUBJECT:	Ordinance No. 1326 - An Uncodified Ordinance of the City of Folsom Approving the Folsom Police Department's Military Equipment Use Policy in compliance with Assembly Bill 481 (Introduction and First Reading)
FROM:	Police Department

RECOMMENDATION / CITY COUNCIL ACTION

Staff recommends that the City Council approve the attached military equipment use policy by introducing and conducting the first reading of Ordinance No. 1326 –An Uncodified Ordinance of the City of Folsom Approving the Folsom Police Department's Military Equipment Use Policy in accordance with Assembly Bill 481.

BACKGROUND / ISSUE

On September 30, 2021, Governor Gavin Newsom signed into law Assembly Bill 481, relating to the use of military equipment by law enforcement agencies.

Assembly Bill 481 (AB 481 or the Bill), codified at Government Code sections 7070 through 7075, requires a law enforcement agency (LEA) to obtain approval from the applicable governing body, via adoption of an ordinance approving a “military equipment” use policy, prior to the LEA, acquiring, using, or seeking funds for military equipment. AB 481 defines “military equipment” broadly and creates explicit parameters for the military equipment use policy it requires.

The Folsom Police Department (FPD) seeks City Council adoption of the attached ordinance approving the Military Equipment Use Policy – FPD Policy 707 (Exhibit A), in order to allow FPD to continue to use the vital equipment specified therein, as well as acquire one piece of new equipment.

POLICY / RULE

Assembly Bill 481 requires the following in summary:

- Law enforcement agencies must obtain approval from their governing body, by ordinance adopting a military equipment use policy, before engaging in specific actions related to the use or acquisition of military style equipment, including equipment that was acquired before January 1, 2022, when AB 481 went into effect.
- The proposed military equipment use policy must be available on the law enforcement agency's public website at least 30 days before the public meeting concerning the ordinance and the policy. If approved, the policy must remain publicly available on the website for as long as the military equipment is available for use.
- The proposed ordinance and military equipment use policy must be considered as an agenda item in open session at a regular meeting of the governing body, with the opportunity for public comment in accordance with the Brown Act.
- The governing body of a law enforcement agency shall only approve the military equipment use policy if it makes the specified determinations found in Government Code section 7071(d)(1). These findings are discussed in detail below.
- The governing body must review the ordinance and vote on whether to renew it at least annually. In deciding whether to renew the ordinance, the governing body must make specific determinations as described in Government Code section 7071(e)(2).
- The law enforcement agency must submit an annual report on military equipment funding, acquisition, and use to the governing body. The report must comply with the requirements of Government Code section 7072. The governing body must rely on the report in deciding whether to renew the ordinance and in making the specific determinations described in Government Code section 7071(e)(2). The annual report must be publicly available on the website for as long as the military equipment is available for use.
- Within 30 days of submitting and publicly releasing the annual report, the law enforcement agency must hold at least one well-publicized and conveniently located community engagement meeting, at which the general public may discuss and ask questions regarding the annual military equipment report and the law enforcement agency's funding, acquisition, or use of military equipment.

ANALYSIS

The legislative intent with Assembly Bill 481 was to have each jurisdiction's governing body be aware of and approve the use or acquisition of certain 'military' equipment described in California Government Code section 7070.

AB 481 defines “military equipment” broadly to include 15 categories of equipment. The term “military equipment”, as used in AB 481, in fact does not necessarily indicate equipment that has been used by the military. Pursuant to AB 481, items deemed to be “military equipment” include, but are not limited to, unmanned aerial or ground vehicles (drones), armored vehicles, command and control vehicles, pepper balls, less lethal shotguns, less lethal 40mm projectile launchers, long range acoustic devices, and flashbangs.

Other items deemed to be “military equipment” via AB 481 include foundational equipment such as rifles.

The Folsom Police Department acquired and used “military equipment”, as defined in AB 481, in accordance with applicable rules and regulations before January 1, 2022, when the new rules went into effect. State law now requires that the Department seek City Council approval, by an ordinance adopting a military equipment use policy, before it can use existing military equipment, acquire new military equipment, or collaborate with another law enforcement agency in the deployment of military equipment within the City of Folsom. The Department may continue to use any “military equipment” acquired before January 1, 2022, as long as it begins the approval process by May 1, 2022, and the Council adopts the ordinance approving the military equipment use policy within 180 days.

AB 481 contains specific requirements for the military equipment use policy itself. These include: a description of each type of military equipment, its capabilities, expected lifespan, product description from the manufacturer, and the quantity of each type of equipment; the purposes and authorized uses for each type of equipment; the fiscal impact; the rules that govern each authorized use; the training required before the equipment may be used by Department personnel; the mechanisms to ensure compliance with the policy; and the procedures by which members of the public may register complaints or concerns or ask questions about the equipment and its use. The attached policy contains all of the required information for the Department’s existing equipment and for the one piece of new equipment to be acquired in 2022.

The proposed policy must be considered by the Council as an agenda item in open session at a regular meeting in accordance with the Brown Act, with an opportunity for public comment. The Council shall only approve the policy if it makes all of the following determinations pursuant to Government Code section 7071 (d)(1):

- A. The military equipment is necessary because there is no reasonable alternative that can achieve the same objective of officer and civilian safety.
- B. The proposed military equipment use policy will safeguard the public’s welfare, safety, civil rights, and civil liberties.
- C. If purchasing the equipment, the equipment is reasonably cost effective compared to available alternatives that can achieve the same objective of officer and civilian safety.

- D. Prior military equipment use complied with the military equipment use policy that was in effect at the time, or if prior uses did not comply with the accompanying military equipment use policy, corrective action has been taken to remedy nonconforming uses and ensure future compliance.

All sworn officers with the Folsom Police Department are bound by a stringent set of laws, policies, and procedures which are in line with the public's welfare, safety, civil rights, and civil liberties. Similarly, all officers are trained in a variety of strategies and tactics which are in line with the laws, policies, and procedures in which we are bound, prior to the use or application of any 'military' equipment.

Items deemed to be "military equipment" by AB 481 are used as a component of overall best practices for LEAs throughout the country. These tools have been tested in the field, and are used by LEAs to enhance citizen safety and officer safety. Loss of these items would jeopardize the welfare of citizens and peace officers within the City of Folsom. For example, the rifles deemed to be "military equipment" allow peace officers to address lethal threats from a greater distance and with greater precision.

The Folsom Police Department is committed to using the most up to date tools and equipment to safeguard the citizens of Folsom. Many items deemed to be "military equipment" by AB 481 are in fact employed by the Folsom Police Department, and LEA's across the country, in order to specifically reduce risk to community members. These items provide peace officers with the ability to safely resolve volatile situations which otherwise might rise to the level of a lethal force encounter. To that end, the items at issue in this report, and accompanying Military Equipment Use Policy, also provide FPD's peace officers with vital tools that facilitate compliance with its stringent use of force policy.

AB 481 also includes a focus on transparency and public engagement. To that end, the proposed policy must be posted on the Department's website at least 30 days before the public hearing on the ordinance. If approved, the policy must be publicly available on the website for as long as the military equipment is available for use.

Similarly, AB 481 stated, 'The public has a right to know about any funding, acquisition, or use of military equipment by state or local government officials, as well as a right to participate in any government agency's decision to fund, acquire, or use such equipment.'

As such, the Police Department hosted a publicized community event on 3/31/2022 at the Folsom Police Department, to display and discuss the items applicable to this new law. The feedback of the event from the over 25 attendees was favorable, and the attendees supported the use of the equipment in order to keep the community safe.

ENVIRONMENTAL REVIEW

The California Environmental Quality Act (CEQA) does not apply to activities that will not result in a direct or reasonably foreseeable indirect physical change in the environment or to activities that are not otherwise considered a "project" as defined by Public Resources Code § 21065.

(CEQA Guidelines § 15061(c)(3) and § 15378.) The Council's decision regarding the military equipment use policy meets the above criteria and is not subject to CEQA. No environmental review is required.

FINANCIAL IMPACT

There is no direct financial impact caused by adoption of the ordinance or approval of the policy. The initial and ongoing financial impacts associated with each Type of existing Military Equipment are detailed in the policy. The financial impact of any new acquisition of Military Equipment approved by the Council through the policy is stated in the policy and will also be described in the contract and any staff report associated with acquisition of the specific piece of equipment.

ATTACHMENT

Ordinance No. 1326 – An Uncodified Ordinance approving the Folsom Police Department's Military Equipment Use Policy in accordance with Assembly Bill 481

Submitted,

Richard Hillman, Chief of Police

ORDINANCE NO. 1326**AN UNCODIFIED ORDINANCE OF THE CITY OF FOLSOM APPROVING THE FOLSOM POLICE DEPARTMENT'S MILITARY EQUIPMENT USE POLICY IN COMPLIANCE WITH AB 481**

The City Council of the City of Folsom does hereby ordain as follows:

SECTION 1 PURPOSE

The purpose of this ordinance is to authorize the Folsom Police Department's acquisition and use of Military Equipment through approval of the attached Military Equipment Use Policy in accordance with AB 481 (Government Code sections 7070 through 7072.)

SECTION 2 POLICY

The proposed Military Equipment Use Policy is attached to this Ordinance as Exhibit A.

SECTION 3 FINDINGS

- A.** On September 30, 2021, Governor Gavin Newsom signed into law Assembly Bill 481, relating to the use of "military equipment" by law enforcement agencies.
- B.** Assembly Bill 481, codified at Government Code sections 7070 through 7075, requires law enforcement agencies to obtain approval from the applicable governing body, by an ordinance adopting a "military equipment" use policy, at a regular meeting held pursuant to open meeting laws, before taking certain actions related to the funding, acquisition, or use of "military equipment".
- C.** The term "military equipment" is defined in Government Code section 7070.
- D.** Assembly Bill 481 allows a law enforcement agency's governing body to approve the funding, acquisition, or use of "military equipment" within its jurisdiction only if it makes specified determinations.
- E.** Once approved, the Folsom Police Department's Military Equipment Use Policy will be contained in the Folsom Police Department's Policy Manual and identified as Policy 707- City Council Military Equipment Approval.
- F.** The proposed Military Equipment Use Policy was published on the Folsom Police Department's public website on March 25 2022.
- G.** The proposed Military Equipment Use Policy was presented to City Council on May 10, 2022.

- H. The proposed Military Equipment Use Policy meets the requirements of Government Code section 7070, subdivision (d).

SECTION 4 DETERMINATIONS

Based on the information presented to the City Council at the public meeting on May 10, 2022, and the findings above, the City Council makes the following determinations:

1. The Military Equipment identified in the proposed Military Equipment Use Policy, attached hereto as Exhibit A, is necessary because there are no reasonable alternatives that can achieve the same objectives of officer and civilian safety.
2. The proposed Military Equipment Use Policy will safeguard the public's welfare, safety, civil rights, and civil liberties.
3. The piece of Military Equipment proposed to be purchased this year is reasonably cost effective compared to available alternatives that can achieve the same objectives of officer and civilian safety.
4. Prior Military Equipment use complied with applicable policies. The Folsom Police Department did not have a Military Equipment Use Policy in effect before the date of this ordinance, but the other applicable policies were followed.
5. The proposed Military Equipment Use Policy, attached hereto as Exhibit A, is approved and adopted.

SECTION 5 NO MANDATORY DUTY OF CARE

This Ordinance is not intended to and shall not be construed or given effect in a manner that imposes upon the City or any officer or employee thereof a mandatory duty of care toward persons or property within or without the City so as to provide a basis of civil liability for damages, except as otherwise imposed by law.

SECTION 6 SEVERABILITY

If any section, subsection, sentence, clause, or phrase in this Ordinance or the attached policy or any part thereof is for any reason held to be unconstitutional, invalid, or ineffective by any court of competent jurisdiction, such decision shall not affect the validity or effectiveness of the remaining portions of this Ordinance or the attached policy or any part thereof. The City Council declares that it would have passed each section irrespective of the fact that any one or more section, subsection, sentence, clause, or phrase be declared unconstitutional, invalid, or ineffective.

SECTION 7 EFFECTIVE DATE

This ordinance shall become effective thirty (30) days from and after its passage and adoption, provided it is published in full or in summary within twenty (20) days after its adoption in a newspaper of general circulation in the City.

This ordinance was introduced and the title thereof read at the regular meeting of the City Council on May 10, 2022 and the second reading occurred at the regular meeting of the City Council on May 24, 2022.

On a motion by Council Member _____ seconded by Council Member _____, the foregoing ordinance was passed and adopted by the City Council of the City of Folsom, State of California, this 24th day of May 2022, by the following roll-call vote:

AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Kerri M. Howell, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

Military Equipment

707.1 PURPOSE AND SCOPE

The purpose of this policy is to provide guidelines for the approval, acquisition, and reporting requirements of military equipment (Government Code § 7070; Government Code § 7071; Government Code § 7072).

707.1.1 DEFINITIONS

Definitions related to this policy include (Government Code § 7070):

Military equipment – Includes but is not limited to the following:

- 1 Unmanned, remotely piloted, powered aerial or ground vehicles.
- 2 Mine-resistant ambush-protected (**MRAP**) vehicles or armored personnel carriers. Police versions of standard consumer vehicles are not Military Equipment.
- 3 High mobility multipurpose wheeled vehicles (HMMWV), two-and-one-half-ton trucks, five-ton trucks, or wheeled vehicles that have a breaching or entry apparatus attached. Unarmored all-terrain vehicles (ATVs) and motorized dirt bikes are not Military Equipment.
- 4 Tracked armored vehicles that provide ballistic protection to their occupants and use a tracked system instead of wheels for forward motion.
- 5 Command and control vehicles that are either built or modified to facilitate the operational control and direction of public safety units.
- 6 Weaponized aircraft, vessels, or vehicles of any kind.
- 7 Battering rams, slugs, and breaching apparatuses that are explosive in nature. This does not include a handheld, one-person ram. Items designed to remove a lock, such as bolt cutters, are not Military Equipment.
- 8 Firearms and ammunition of .50 caliber or greater, excluding standard-issue shotguns and standard-issue shotgun ammunition.
- 9 Specialized firearms and ammunition of less than .50 caliber, including firearms and accessories identified as assault weapons in Penal Code § 30510 and Penal Code § 30515, with the exception of standard-issue handguns and ammunition of less than .50 caliber.
- 10 Any firearm or firearm accessory that is designed to launch explosive projectiles.
- 11 Flashbang grenades, noise-flash diversionary devices, and explosive breaching tools.
- 12 Tear gas, pepper balls, and other munitions containing tear gas or OC, excluding standard, service-issued handheld pepper spray.
- 13 TASER® Shockwave, microwave weapons, water cannons, and long-range acoustic devices (LRADs).

Folsom Police Department

Folsom PD Policy Manual

Military Equipment

14 The following projectile launch platforms and their associated munitions: 40 mm projectile launchers, bean bag, rubber bullet, specialty impact munition (SIM) weapons, and any kinetic energy weapons and munitions.

15 Any other equipment as determined by the City Council to require additional oversight.

Type – Each item that shares the same manufacturer model number.

707.2 POLICY

It is the policy of the Folsom Police Department that members of this department comply with the provisions of Government Code § 7071 with respect to military equipment.

707.3 MILITARY EQUIPMENT COORDINATOR

The Chief of Police should designate a member of this department to act as the military equipment coordinator. The responsibilities of the military equipment coordinator include but are not limited to:

- (a) Acting as liaison to the City Council for matters related to the requirements of this Policy.
- (b) Identifying equipment that qualifies as Military Equipment, as defined in this Policy, whether the equipment is in the current possession of the Department or is equipment the Department intends to acquire.
- (c) Conducting an inventory of all Military Equipment, as defined in this Policy, at least annually.
- (d) Collaborating with any other law enforcement agency that may use Military Equipment within the jurisdiction of Folsom Police Department (Government Code § 7071).
- (e) Preparing for, scheduling, and coordinating the annual community engagement meeting, including:
 1. Publicizing the details of the meeting.
 2. Preparing for public questions regarding the Department's funding, acquisition, or use of Military Equipment.
- (f) Preparing the annual Military Equipment report Council and ensuring that the report is made available on the department website consistent with this Policy. (Government Code § 7072). Additionally, updating the ordinance, policy, and Military List as necessary.
- (g) Ensuring that this Policy is made available on the Department's website and that any newly proposed version of the Policy is also available on the website at least 30 days before any public hearing concerning the Policy or the associated ordinance.
- (h) Evaluating the procedures by which members of the public may register complaints or concerns or submit questions about the use of any Type of Military Equipment and updating those procedures as needed.

Folsom Police Department

Folsom PD Policy Manual

Military Equipment

- (i) Ensuring that the Department responds in a timely manner to each complaint, concern, or question about Military Equipment

707.4 MILITARY EQUIPMENT INVENTORY

Attached is a list of the Folsom Police Department's qualifying Military Equipment, including the following information (Government Code § 7070):

- (a) A description of each Type of Military Equipment, including:
 1. Quantity
 2. Capabilities
 3. Expected lifespan
 4. Product description from the manufacturer.
- (b) The purposes and authorized uses of each Type of Military Equipment
- (c) The fiscal impact of each Type of Military Equipment, including:
 1. Initial cost of obtaining the equipment
 2. Estimated annual cost of maintaining the equipment
- (d) Rules that govern each authorized use
- (e) Training that must be completed before any member of the Department is allowed to use each specific Type of Military Equipment

707.5 USE OF MILITARY EQUIPMENT

Military Equipment used by any member of this Department shall first be approved for use by the City Council. Once approved, Military Equipment must be used in accordance with all applicable Folsom Police Department policies and used only by those members trained and authorized for such use.

707.6 ACQUISITION OF NEW MILITARY EQUIPMENT

- (a) The acquisition of new Military Equipment shall be approved in advance by the City Council, in accordance with this Policy.
- (b) The Department is authorized to acquire additional stock of approved Military Equipment from other law enforcement agencies in the event of an emergency when approved in advance by the Chief of Police or authorized designee. If that occurs, the Department must obtain City Council approval as described in this Policy as soon as practicable.

Folsom Police Department

Folsom PD Policy Manual

Military Equipment

707.7 FUNDING FOR MILITARY EQUIPMENT

Funding for Military Equipment shall be approved in advance by the City Council, in accordance with the City of Folsom budget process, the procurement requirements of the Folsom Municipal Code, and this Policy.

707.8 APPROVAL BY CITY COUNCIL

- (a) The Chief of Police or the authorized designee shall obtain approval from the City Council for the acquisition and use of Military Equipment by way of an ordinance adopting the Military Equipment Use Policy. (Government Code § 7071.)
- (b) The City Council shall review the ordinance approving the Military Equipment Use Policy annually and vote on whether to renew the ordinance. (Government Code § 7071.)
- (c) As part of the initial and annual approval process, the Chief of Police or the authorized designee shall submit the proposed Military Equipment Use Policy to the City Council and make it available on the Department's website at least 30 days prior to any public hearing concerning the Military Equipment at issue (Government Code § 7071).
- (d) The ordinance approving the Military Equipment Use Policy must be adopted by the City Council before the Department engages in any of the following (Government Code § 7071):
 1. Requesting military equipment made available pursuant to 10 USC § 2576a.
 2. Seeking funds for military equipment, including but not limited to applying for a grant, soliciting or accepting private, local, state, or federal funds, in-kind donations, or other donations or transfers.
 3. Acquiring military equipment either permanently or temporarily, including by borrowing or leasing.
 4. Collaborating with another law enforcement agency in the deployment or other use of military equipment within the jurisdiction of this department.
 5. Using any new or existing military equipment for a purpose, in a manner, or by a person not previously approved by the governing body.
 6. Soliciting or responding to a proposal for, or entering into an agreement with, any other person or entity to seek funds for, apply to receive, acquire, use, or collaborate in the use of military equipment.
 7. Acquiring military equipment through any means not provided above.

707.9 PUBLIC AVAILABILITY OF POLICY

- (a) The Chief of Police or the authorized designee shall make any proposed Military Equipment Use Policy publicly available on the Department's website at least 30 days before any public hearing concerning the Military Equipment at issue. This includes any proposed changes to the Policy or the Military Equipment Inventory associated with the City Council's annual review of the ordinance. (Government Code § 7071.)

Folsom Police Department

Folsom PD Policy Manual

Military Equipment

- (b) The Chief of Police or the authorized designee shall make any Military Equipment Use Policy publicly available on the Department's website for as long as the Military Equipment is available for use. (Government Code § 7071.)

707.10 COORDINATION WITH OTHER LAW ENFORCEMENT AGENCIES

Military Equipment used by any other law enforcement agency that is providing mutual aid to or operating in conjunction with this Department or the City of Folsom in a law enforcement capacity, shall comply with the law enforcement agency's own Military Equipment use policy. Use of another law enforcement agency's Military Equipment by that agency in the City of Folsom is approved as long as the use complies with the other agency's Military Equipment use policy.

707.11 ANNUAL REPORT

- (a) Within one year after approval of the Military Equipment Use Policy, and annually thereafter, the Chief of Police or the authorized designee should submit to the City Council an annual Military Equipment report for each Type of approved Military Equipment. (Government Code § 7072).
- (b) The Chief of Police or the authorized designee should make each annual Military Equipment report publicly available on the Department's website for as long as the Military Equipment is available for use. (Government Code § 7072.)
- (c) The annual Military Equipment report shall include, at a minimum, all of the following information for the preceding calendar year for each Type of Military Equipment (Government Code § 7072):
1. A summary of how the Military Equipment was used and the purpose of its use.
 2. A summary of any complaints or concerns received concerning the Military Equipment.
 3. The results of any internal audits, any information about violations of the Military Equipment Use Policy, and any actions taken in response.
 4. The total annual cost for each Type of Military Equipment, including acquisition, personnel, training, transportation, maintenance, storage, upgrade, and other ongoing costs, and from what source funds will be provided for the Military Equipment in the calendar year following submission of the annual Military Equipment report.
 5. The quantity possessed for each Type of Military Equipment.
 6. If the Department intends to acquire additional Military Equipment in the next year, the quantity sought for each Type of Military Equipment.

Folsom Police Department

Folsom PD Policy Manual

Military Equipment

707.12 REPLACEMENT OF EXISTING MILITARY EQUIPMENT

To maintain the quantity of each Type of Military Equipment included in the Military Equipment Inventory attached to this Policy, the Department is authorized to replace any approved Military Equipment without amending this Policy or the associated ordinance, if the Military Equipment is destroyed or rendered inoperable as a result of authorized use.

707.13 COMPLIANCE

Mechanisms to ensure compliance with the Military Equipment Use Policy include, but are not limited to, the following:

- (a) The Chief of Police shall be considered the ultimate authority for the content and adoption of the provisions of this Policy and shall ensure compliance with the Policy.
- (b) Each Division Commander will ensure that members under his/her command are aware of this Policy.
- (c) Each member will be required to acknowledge that he/she reviewed the Policy and shall seek clarification from an appropriate supervisor as needed.
- (d) The Training Officer is responsible for developing, reviewing, updating, and maintaining the Department's training plan so that required training is completed.
- (e) The Military Equipment Coordinator is responsible for compliance with specific components of the Policy as described above.
- (f) Violations of this Policy shall form the basis for departmental administrative action, training, or discipline consistent with the Policy Manual, the City of Folsom Personnel Rules and Regulations, and the applicable collective bargaining agreement.
- (g) This Policy shall not be construed to create a higher standard of care for civil or criminal liability against the City of Folsom or its employees.

707.14 COMMUNITY ENGAGEMENT

- (a) Within 30 days of submitting and publicly releasing the annual Military Equipment report, the Department shall hold at least one well-publicized and conveniently located community engagement meeting, at which the Department should discuss the annual Military Equipment report and respond to public questions regarding the report and the Department's funding, acquisition, or use of Military Equipment. (Government Code § 7072.)
- (b) Members of the public may register complaints or concerns or submit questions about the use of each specific Type of Military Equipment in any form, including in writing, by email sent to the following email address: FPDmilitaryequipment@folsom.ca.us, in person, by telephone at (916) 461-6400, at pre-determined community engagement meetings related to Military Equipment, or at any City Council meeting. Complaints related to the use of Military Equipment will be handled in accordance with FPD Policy 1019- Personnel Complaints.
- (c) The Department will ensure that each complaint, concern, or question receives a response in a timely manner. The Military Equipment Coordinator is specifically tasked

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with ensuring that each complaint, concern, or question receives a response in a timely manner.

List of Folsom Police Department's Qualifying Military Equipment

DJI Matrice 210	
Description	Commercial UAS
Quantity	1
Capability	Fly, Hover, broadcast video, record video, Photography, FLIR, carry payload up to approx. 7.5lbs.
Life Span	UAS devices need to be upgraded as software becomes obsolete by vendors.
Manufacturer's Description	Commercial grade UAV equipped with a thermal imaging camera and a 30x zoom camera. 38 mins. maximum flight time. Has live stream capabilities.
Purpose	Provide Aerial Support for L.E. operations
Authorized Usage	Investigative Used by SWAT, Patrol, Search and Rescue to locate persons.
Costs	\$30,000 approx. \$2,000 anticipated yearly maintenance & battery cost
Required Training	32-hr Basic Pilots Course (or equivalent), FAA Remote Pilot Certificate (Part 107)
Authority for Authorized Use	Certificate of Authority issued by FAA - #2020-WSA-7949-COA, FAA Part 107 holder and completion of Department Training, Folsom Police Department Policy – Section 391

DJI Phantom 4 Pro	
Description	Commercial UAS
Quantity	1
Capability	Fly, Hover, broadcast video, record video, Photography
Life Span	UAS devices need to be upgraded as software becomes obsolete by vendors.
Manufacturer's Description	Commercial grade UAS equipped with UHD capable camera. Has live stream capability and 28 min. flight time.
Purpose	Provide Aerial Support for L.E. operations
Authorized Usage	Investigative Used by SWAT, Patrol, Search and Rescue to locate persons.
Costs	\$2,500 approx. \$300 anticipated yearly maintenance & battery cost
Required Training	32-hr Basic Pilots Course (or equivalent), FAA Remote Pilot Certificate (Part 107)

Authority for Authorized Use	Certificate of Authority issued by FAA - #2020-WSA-7949-COA, FAA Part 107 holder and completion of Department Training, Folsom Police Department Policy – Section 391
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DJI Mavic 2 Enterprise	
Description	Commercial UAS
Quantity	3
Capability	Fly, Hover, broadcast video, record video, Photography, broadcast instant and/or prerecorded notifications.
Life Span	UAS devices need to be upgraded as software becomes obsolete by vendors.
Manufacturer's Description	Commercial grade UAS equipped with Single 4k colored camera with Zoom capabilities. Has live stream capability and 30 min. flight time.
Purpose	Provide Aerial Support for L.E. operations
Authorized Usage	Investigative Used by SWAT, Patrol, Search and Rescue to locate persons.
Costs	\$3,600 approx. \$400 anticipated yearly maintenance & battery cost
Required Training	32-hr Basic Pilots Course (or equivalent), FAA Remote Pilot Certificate (Part 107)
Authority for Authorized Use	Certificate of Authority issued by FAA - #2020-WSA-7949-COA, FAA Part 107 holder and completion of Department Training, Folsom Police Department Policy – Section 391

DJI Mavic 2 Zoom	
Description	Commercial UAS
Quantity	1
Capability	Fly, Hover, broadcast video, record video, Photography.
Life Span	UAS devices need to be upgraded as software becomes obsolete by vendors.
Manufacturer's Description	Commercial grade UAS equipped with Single 4k colored camera with Zoom capabilities. Has live stream capability and 30 min. flight time.
Purpose	Provide Aerial Support for L.E. operations

Authorized Usage	Investigative Used by SWAT, Patrol, Search and Rescue to locate persons.
Costs	\$1,500 approx. \$400 anticipated yearly maintenance & battery cost
Required Training	32-hr Basic Pilots Course (or equivalent), FAA Remote Pilot Certificate (Part 107)
Authority for Authorized Use	Certificate of Authority issued by FAA - #2020-WSA-7949-COA, FAA Part 107 holder and completion of Department Training, Folsom Police Department Policy – Section 391

DJI Mavic Mini 2	
Description	Commercial UAS
Quantity	2
Capability	Fly, Hover, broadcast video, record video, Photography, Interior flights
Life Span	UAS devices need to be upgraded as software becomes obsolete by vendors.
Manufacturer's Description	Commerical grade UAS equipped with Single 4k colored camera. Has live stream capability and 30 min. fight time.
Purpose	Provide Aerial Support for L.E. operations
Authorized Usage	Investigative Used by SWAT, Patrol, Search and Rescue to locate persons.
Costs	\$500 approx. \$80 anticipated yearly maintenance & battery cost
Required Training	32-hr Basic Pilots Course (or equivalent), FAA Remote Pilot Certificate (Part 107)
Authority for Authorized Use	Certificate of Authority issued by FAA - #2020-WSA-7949-COA, FAA Part 107 holder and completion of Department Training, Folsom Police Department Policy – Section 391

DJI Mavic Mini	
Description	Commercial UAS
Quantity	1
Capability	Fly, Hover, broadcast video, record video, Photography, Interior flights.

Life Span	UAS devices need to be upgraded as software becomes obsolete by vendors.
Manufacturer's Description	Commerical grade UAS equipped with Single 4k colored camera. Has live stream capability and 30 min. flight time.
Purpose	Provide Aerial Support for L.E. operations
Authorized Usage	Investigative Used by SWAT, Patrol, Search and Rescue to locate persons.
Costs	\$400 approx. \$80 anticipated yearly maintenance & battery cost
Required Training	32-hr Basic Pilots Course (or equivalent), FAA Remote Pilot Certificate (Part 107)
Authority for Authorized Use	Certificate of Authority issued by FAA - #2020-WSA-7949-COA, FAA Part 107 holder and completion of Department Training, Folsom Police Department Policy – Section 391

DJI Aeroscope (Regional Item)	
Description	Commercial UAS detection device
Quantity	1
Capability	Drone detection and identification platform.
Life Span	UAS devices need to be upgraded as software becomes obsolete by vendors.
Manufacturer's Description	Drone detection platform that identifies UAV communication links, gathering information such as flight status, paths, and other information in real-time.
Purpose	Identify and track commercial drones within secured or restricted areas
Authorized Usage	Provide airspace security over restricted areas or during specilized events
Costs	Unknown – obtained through UASI grant applied for by regional partners approx. \$400 anticipated yearly maintenance & battery cost
Required Training	In house departmental training. No POST requirement.
Authority for Authorized Use	Completion of Department Training, Folsom Police Department Policy – Section 391

Blu-Link Streaming Box	
Description	Commerical streaming box
Quantity	1
Capability	Streams video in real-time HD video over a secure internet connection.
Life Span	3-5 years

Manufacturer's Description	Portable video streaming device to stream live HD video in real time over a secure Internet connection.
Purpose	Provide real-time video over a secure internet connection for situational awareness and scene assessment.
Authorized Usage	Completion of Department Training, Folsom Police Department Policy – Section 391
Costs	\$5,400 approx. \$50 anticipated yearly maintenance & battery cost
Required Training	In house departmental training. No POST requirement.
Authority for Authorized Use	Completion of Department Training, Folsom Police Department Policy – Section 391
Avatar III	
Description	Robotex INC Avatar III Robot
Quantity	1
Capability	The Avatar III Robot is a radio controlled robot on a track system of propulsion and is outfitted with Cameras, Speakers, and Microphones. The robot increases officers ability to conduct searches in high risk scenarios by providing video and audio into areas that may not be safe for officers to enter. In addition the cameras, speakers, and micorphones allow for 2 way communication between officers and subjects during critical incidents such as barricaded subjects, hostage situations, or suicidal subjects. The camera system provides additional safety for officers when placed in strategic positions to monitor doorways, hallways or access points. The Avatar III Robot is regulary used by the SWAT team during his risk search warrants, emergency calls, and during outside agency requests for assistance. The Crisis Negotiations Team (CNT) is also able to use the robot for direct communication during critical incidents when other forms of direct contact are unsuccessful.
Life Span	15 years

Manufacturer's Description	The AVATAR® enhances the capabilities of SWAT and tactical response teams by allowing them to inspect dangerous situations quickly and safely, there is no longer a need to send personnel in before you've had a chance to assess the situation. The AVATAR® saves lives by keeping first responders out of harm's way, and it does so at a fraction of the price of other robots. The AVATAR® Robots are regarded by tactical teams as a standard operational tool, like a firearm, vehicle, or piece of body armor. Departments across the United States and internationally are using the AVATAR® Robots	
Purpose	To enhance safety for officers and subjects during high risk or critical Incidents.	
Authorized Usage	Assisting in lawful searches and surveillance. Communications during critical incidents	
Costs	\$26000 anticipated yearly maintenance & battery cost	\$0
Required Training	In house departmental training. No POST requirement.	
Authority for Authorized Use	It is the policy of the FPD to utilize a robot only for official law enforcement purposes, and in a manner that respects the privacy of our community, pursuant to State and Federal law.	

Lenco Bearcat G2	
Description	Armored personnel carrier
Quantity	1
Capability	The BearCat is a large mobile armored vehicle used to conduct rescues mission for both officers and the public, transport personnel and equipment, and provide security to the public.
Life Span	25 years
Manufacturer's Description	<p>The Lenco Bearcat is an armored vehicle built on a Ford F550 frame and is manufactured for law enforcement purposes. The purpose of the Bearcat armored vehicle is to provide ballistic protection to officers and citizens from gunfire. The armored vehicle stops rifle rated rounds including .50 caliber which is commercially available and beyond the protection level of shield and personal body armor possessed by the department.</p> <p>The Bearcat is often deployed several times a month by SWAT personnel while serving high risk search warrants or assisting other agency. It can be utilized by trained patrol personnel to rescue downed officers and citizens. The Bearcat has been struck by gunfire several times and protected the officers inside, behind it, and the community.</p> <p>Protecting officers allows them to contain the suspect and reduce the immediacy of the threat while communicating and de-escalating. Crisis Negotiations Team (CNT) members have operated from inside the armored vehicles during search warrant and SWAT callouts where they communicate with the suspect and attempt to de-escalate.</p>
Purpose	Regional asset and provides armored vehicle response to critical incidents.
Authorized Usage	To protect and safely transport Police personnel to active scenes. Provide security for officers and the public. It is used by the SWAT and officers.
Costs	\$275,000 approx. \$1,000 anticipated yearly maintenance cost
Required Training	All drivers/ operators shall attend formalized instruction and be trained in vehicle operations and practical driving instruction.

Authority for Authorized Use	Use is established under FPD Policy 705. It is the policy of the Department to utilize armored vehicles only for official law enforcement purposes, and pursuant to State and Federal law.
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Mobile Command & Communications Unit (MCCU)	
Description	LDV Custom Specialty Vehicles Command Vehicle
Quantity	1
Capability	The MCCU can be utilized for SWAT/CINT and other critical incidents, preplanned large events, searching for missing persons, natural disasters, and community events
Life Span	20 years
Manufacturer's Description	The LDV Custom Specialty Vehicle is a mobile command center built on a 2006 Freightliner chassis and customized for law/fire command/communication purposes. The purpose of the LDV Mobile Command and Communication Unit (MCCU) is to provide an interior space for command staff to plan and organize responses to critical incidents and special events. The MCCU is specially equipped with an onboard dispatch center, a command area, and a radio interoperability system (RIOS) which allows radio communication between local, state, and federal law enforcement entities which currently operate under different radio systems/frequencies. The vehicle contains radios with varying frequencies including 800 megahertz, very high frequency (VHF), ultrahigh frequency (UHF), low band, and short-wave radio systems. The vehicle has internet capability and computer resources along with access to television channels allowing access to real time news/information. The vehicle is also equipped with a video downlink system allowing command staff to view live feeds from fire/law enforcement aircraft and UAS devices.
Purpose	To be used based on the specific circumstances of a given critical incident, large event, natural disaster or community event that is taking place.
Authorized Usage	Situations which the MCCU is authorized for use would include but not be limited to critical incidents, emergencies, and natural disasters.

Costs	\$750,000 \$15,000 anticipated yearly maintenance cost	approx.
Required Training	The MCCU operators will receive training in the overall operation of the vehicle to include set up and break down procedures, and skills training in the computer, dispatch, and radio systems. The drivers will receive training in the safe handling of the vehicle with the assistance of an experienced driver. Drivers will undergo California Department of Motor Vehicles commercial vehicle testing. This training will occur on a bi-monthly basis	
Authority for Authorized Use	It is the policy of the Department to use the MCCU for official fire and law enforcement purposes, and in accordance with California State law regarding operation of motor vehicles	

Colt M4 Carbine (11.5")	
Description	Enhanced Patrol Rifle
Quantity	14
Capability	These rifles fire an intermediate-power cartridge (.223) which is more powerful than a standard pistol but less powerful than a standard rifle. It's a short barreled rifle which allows SWAT Officers better control while inside of structures while still providing great accuracy.
Life Span	10-15 years
Manufacturer's Description	Built for the demanding use of those who protect our communities every day, the Colt Enhanced Patrol Rifle (EPR) is the next evolution in the world's most dependable, thoroughly field-tested patrol rifle. Featuring an extended handguard that accepts modular rail segments for mounting a wide variety of pro-grade optics, lighting, and ergonomics-enhancing accessories, as well as the highly durable Magpul® MBUS® Pro Series front and rear back up sights and B5 Bravo buttstock. The Colt EPR reestablishes the Colt AR-15® as the finest tool for local, regional, and national law enforcement agencies.
Purpose	The AR-15 can stop threats of great bodily injury or death at close and intermediate ranges. The AR-15 platform is capable of firing more accurately and quicker than a pistol while holding more rounds in the magazine and having better ballistic qualities.
Authorized Usage	To defend against an imminent threat of serious bodily injury or death. Used by SWAT
Costs	\$1,100 \$0 anticipated yearly maintenance cost
Required Training	Prior to using a rifle, officers must be certified by POST instructors in the operation of the rifle. Additionally, all members that operate any rifle are required to pass a range qualification once a year.
Authority for Authorized Use	Use is established under FPD Policy 300 and Policy 311. It is the policy of the FPD to utilize rifles only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Accuracy International AT-.308

Description	Sniper Rifle
Quantity	4
Capability	This weapon shoots a heavier round. It is utilized when there is a potential need to engage a target further away than the capabilities of our issued rifles or to engage a target behind an intermediate barrier such as glass or metal. The weapon is often deployed in an overwatch capacity to protect the public during events such as the International Marathon.
Life Span	10-15 years
Manufacturer's Description	The AT (Accuracy Tactical) continues the legacy of the combat proven AW308 and takes the AW to new levels. The AIAT has a 20-inch quick change barrel and a folding stock. The AT is ideal for Law Enforcement and civilian users.
Purpose	The AIAT is an accurate rifle for precision shooting. These rifles are equipped with magnified optics and can be utilized to take precision shots at intermediate to long ranges. The .308 caliber bullet it shoots is also bigger and heavier than a typical .223 caliber bullet from an AR-15 which means it will penetrate barriers like glass with much less deflection.
Authorized Usage	To defend against an imminent threat of serious bodily injury or death. Used by SWAT
Costs	\$5,000 \$0 anticipated yearly maintenance cost
Required Training	Prior to using a rifle, officers must be certified by POST instructors in the operation of the rifle. Additionally, all members that operate any rifle are required to pass a range qualification once a year.
Authority for Authorized Use	Use is established under FPD Policy 300 and Policy 311. It is the policy of the FPD to utilize rifles only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Benelli M3 Tactical Shotgun	
Description	SWAT Shotgun
Quantity	3

Capability	This is a 12-gauge semi-automatic pump shotgun used by SWAT officers. The semi-automatic capability reduces time between rounds versus a standard pump action shotgun. Not having to manipulate a fore-end gives SWAT officer the ability to get back on target faster increasing officer safety. The round is good for defeating barriers.
Life Span	10-15 years
Manufacturer's Description	Benelli's M3 Tactical Shotgun delivers fast cycling semi-auto fire with conventional loads or pump action for low energy loads. It is available in a 12 gauge, pistol grip model that satisfies a shooters need for both a semi-automatic and a pump action shotgun in one convertible weapon.
Purpose	SWAT
Authorized Usage	To defend against an imminent threat of serious bodily injury or death. Used by SWAT
Costs	\$800 \$0 anticipated yearly maintenance cost
Required Training	Prior to using a shotgun, officers must be certified by POST instructors in the operation of the rifle. Additionally, all members that operate any shotgun are required to pass a range qualification once a year.
Authority for Authorized Use	Use is established under FPD Policy 300 and Policy 311. It is the policy of the FPD to utilize shotguns only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Colt (11.5-inch barrel) M4 Carbine – Enhanced Patrol Rifle	
Description	Enhanced Patrol Rifle
Quantity	24
Capability	Equipped with optical sight systems and mounted flashlights, the short-barreled rifle (SBR) fires an intermediate-power cartridge (.223/5.56) which is more powerful than a standard pistol but less powerful than a standard rifle. The SBR gives police officers better maneuverability in and out of patrol vehicles and motorcycles. The SBR is ideal for close quarter deployments inside of structures and provides improved accuracy for long distance engagements.
Life Span	10-15 years

Manufacturer's Description	Built for the demanding use of those who protect our communities every day, the Colt Enhanced Patrol Rifle (EPR) is the next evolution in the world's most dependable, thoroughly field-tested patrol rifle. Featuring an extended handguard that accepts modular rail segments for mounting a wide variety of pro-grade optics, lighting, and ergonomics-enhancing accessories, as well as the highly durable Magpul® MBUS® Pro Series front and rear back up sights and B5 Bravo buttstock. The Colt EPR reestablishes the Colt AR-15® as the finest tool for local, regional, and national law enforcement agencies
Purpose	The AR-15 can stop threats of great bodily injury or death at close and intermediate ranges. The AR-15 platform is capable of firing more accurately and quicker than a pistol while holding more rounds in the magazine and having better ballistic qualities.
Authorized Usage	To defend against an imminent threat of serious bodily injury or death. Used by the Officers
Costs	\$1,190 anticipated yearly maintenance cost
Required Training	Prior to using a rifle, officers must be certified by POST instructors in the operation of the rifle. Additionally, all members that operate any rifle are required to pass a range qualification once a year.
Authority for Authorized Use	Use is established under FPD Policy 300 and Policy 311. It is the policy of the FPD to utilize rifles only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Colt (16-inch barrel) M4 Carbine AR-15	
Description	Patrol Rifle
Quantity	22
Capability	Equipped with optical sight systems and mounted flashlights, these rifles fire an intermediate-power cartridge (.223/5.56) cartridge which is more powerful than a standard pistol but less powerful than a standard rifle. Provides improved accuracy for long distance engagements.
Life Span	10-15 years

Manufacturer's Description	The civilian model 6920 is the civilian version of the current Colt M4 Carbine used by our modern military war fighters. Throughout the world today Colt's reliability, performance and accuracy provide our armed Forces with the confidence required to accomplish any mission, and this rifle can do the same for you. Colt's 6920 series shares many of the same features as it's combat-proven brother the Colt M4. From the forged aluminum upper and lower receivers, to the chrome lined barrel, even through the gas operated semi automatic firing system. Don't settle for imitations, buy the real thing, buy a Colt
Purpose	The AR-15 can stop threats of great bodily injury or death at close and intermediate ranges. The AR-15 platform is capable of firing more accurately and quicker than a pistol while holding more rounds in the magazine and having better ballistic qualities.
Authorized Usage	To defend against an imminent threat of serious bodily injury or death. Used by the Officers
Costs	\$940 \$50 anticipated yearly maintenance cost
Required Training	Prior to using a rifle, officers must be certified by POST instructors in the operation of the rifle. Additionally, all members that operate any rifle are required to pass a range qualification once a year.
Authority for Authorized Use	Use is established under FPD Policy 300 and Policy 311. It is the policy of the FPD to utilize rifles only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Black Hills Gold .308 Winchester 180 Nosler AccuBond	
Description	Specialized ammunition
Quantity	700 rounds
Capability	Penetrate Intermediate Barriers, Residential windows/Automotive Glass
Life Span	Indefinite

Manufacturer's Description	This round is loaded with a high quality Nosler AccuBond bullet for excellent down range performance. Through a proprietary bonding process that eliminates voids in the bullet core, AccuBond marries Nosler's traditional copper-alloy jacket with its special lead-alloy core. The result is a bullet that flies true, penetrates deep, won't cause extensive barrel fouling, and will retain 60-70% of its weight. The white polymer tip helps protect against deformation while initiating expansion upon impact. This round is used to penetrate intermediate barriers, residential windows, and automotive glass at 2,500 feet per second.
Purpose	To defend against an imminent threat of serious bodily injury or death. Used by SWAT
Authorized Usage	To defend against an imminent threat of serious bodily injury or death. Used by SWAT
Costs	\$45 approx. \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing specialized ammunitions are trained by POST certified firearms instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, and Policy 311. It is the policy of the FPD to utilize specialized ammunition only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Hornaday .308 Winchester Tap 168 grain ELD Match Tap Precision	
Description	Specialized ammunition
Quantity	7,600 rounds
Capability	Precision Round, Limited Penetration
Life Span	Indefinite
Manufacturer's Description	Hornady .308 Winchester Tap 168 grain ELD Match Tap Precision bullet with Heat Shield tip delivers the excellent terminal performance TAP Precision is known for, but features a resilient, heat resistant polymer tip that improves the ballistic coefficient, resulting in higher impact velocities, less drop, less wind drift, and more energy on target. The round has a muzzle velocity of 2,672 feet per second.
Purpose	Precision round with limited Penetration

Authorized Usage	To defend against an imminent threat of serious bodily injury or death. Used by SWAT
Costs	\$45 approx. \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing specialized ammunitions are trained by POST certified firearms instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, and Policy 311. It is the policy of the FPD to utilize specialized ammunition only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Ferret 37mm CS 1192	
Description	Tear Gas
Quantity	5
Capability	Can be launched via the 37mm single launcher
Life Span	5 years
Manufacturer's Description	The Ferret® 37 mm CS Round is a frangible projectile filled with chemical agent. Upon impacting the barrier, the nose cone ruptures and instantaneously delivers the .16 oz. agent payload inside a structure. The munitions is 4.8 in. by 1.5 in. and travels at 650fps within an effective range of 50 yards.
Purpose	To safely resolve critical situations such as violent civil unrest and highrisk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$40 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Ferret 37mm OC 1160	
Description	Tear Gas
Quantity	8
Capability	Can be launched via the 37mm single launcher
Life Span	5 years
Manufacturer's Description	The Ferret® 37 mm OC Round is a frangible projectile filled with chemical agent. Upon impacting the barrier, the nose cone ruptures and instantaneously delivers the .16 oz. agent payload inside a structure. These munitions are 4.8 in. by 1.5 in. and travels at 650fps within an effective range of 50 yards.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations

Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$40 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Combined Tactical Systems 37mm Riot CS Powder Muzzle Blast	
Description	Tear Gas
Quantity	1
Capability	Can be launched via the 37mm single launcher
Life Span	5 years
Manufacturer's Description	A cartridge designed to blast irritant powder directly from the muzzle toward a hostile crowd or individual. These muzzle munitions are designed with a “dual-rim” enabling the operator to chamber the round in both 37MM and 40MM Launchers. However, only 37MM will deliver optimum performance as stated in this specification sheet.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$40 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Combined Tactical Systems 2430 CS/ 12-gauge barricade
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Description	Tear Gas	
Quantity	9	
Capability	Can be launched via a 12 gauge shotgun	
Life Span	5 years	
Manufacturer's Description	Liquid filled, non-burning, fin-stabilized rounds designed to penetrate light to intermediate barriers such as windows and hollow core doors. The projectiles break upon impact and deliver agent payloads of powder or liquid throughout the adjacent target area.	
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations	
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT	
Costs	\$8	\$0
	anticipated yearly maintenance cost	
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Combined Tactical Systems 2440- 12-gauge OC powder		
Description	Tear Gas	
Quantity	5	
Capability	Can be launched via a 12 gauge shotgun	
Life Span	5 years	
Manufacturer's Description	OC Liquid filled, non-burning, fin-stabilized rounds designed to penetrate light to intermediate barriers such as windows and hollow core doors. The projectiles break upon impact and deliver agent payloads of OC powder throughout the adjacent target area.	
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations	

Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$8 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Ferret 40mm liquid barricade penetrator round, OC 2260	
Description	Tear Gas
Quantity	10
Capability	Can be launched via a 40mm launcher.
Life Span	5 years
Manufacturer's Description	The Ferret® 40mm Round is non-burning and suitable for indoor use. Used primarily by tactical teams, it is designed to penetrate barriers, such as windows, hollow core doors, wallboard and thin plywood. Upon impacting the barrier, the nose cone ruptures and instantaneously delivers a small chemical payload inside of a structure or vehicle. In a tactical deployment situation, the 40mm Ferret is primarily used to dislodge barricaded subjects from confined areas. Its purpose is to minimize the risks to all parties through pain compliance, temporary discomfort and/or incapacitation of potentially violent or dangerous subjects.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$10 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.

Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.
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Defense Technology Ferret 40 mm powder barricade round, CS 2292	
Description	Tear Gas
Quantity	16
Capability	Can be launched via a 40mm launcher.
Life Span	5 years
Manufacturer's Description	The Ferret® 40 mm Barricade Penetrating Round is filled with a CS powder chemical agent. It is a frangible projectile that is spin stabilized utilizing barrel rifling. It is non-burning and designed to penetrate barriers. Primarily used to dislodge barricaded subjects, it can also be used for area denial. Primarily used by tactical teams, it is designed to penetrate barriers, such as windows, hollow core doors, wallboard and thin plywood. Upon impact the nose ruptures and instantaneously delivers the agent payload inside a structure or vehicle.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$10 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Ferret 40 mm liquid barricade penetrator round, CS 2262	
Description	Tear Gas
Quantity	10
Capability	Can be launched via a 40mm launcher.

Life Span	5 years
Manufacturer's Description	The Ferret® 40mm Round is non-burning and suitable for indoor use. Used primarily by tactical teams, it is designed to penetrate barriers, such as windows, hollow core doors, wallboard and thin plywood. Upon impacting the barrier, the nose cone ruptures and instantaneously delivers a small chemical payload inside of a structure or vehicle. In a tactical deployment situation, the 40mm Ferret is primarily used to dislodge barricaded subjects from confined areas. Its purpose is to minimize the risks to all parties through pain compliance, temporary discomfort and/or incapacitation of potentially violent or dangerous subjects.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$10 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Ferret 40 mm powder barricade round, OC 2290	
Description	Tear Gas
Quantity	11
Capability	Can be launched via a 40mm launcher.
Life Span	5 years

Manufacturer's Description	The Ferret® 40 mm Barricade Penetrating Round is filled with an OC powder chemical agent. It is a frangible projectile that is spin stabilized utilizing barrel rifling. It is non-burning and designed to penetrate barriers. Primarily used to dislodge barricaded subjects, it can also be used for area denial. Primarily used by tactical teams, it is designed to penetrate barriers, such as windows, hollow core doors, wallboard and thin plywood. Upon impact the nose ruptures and instantaneously delivers the agent payload inside a structure or vehicle.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$10 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Triple-chaser separating canister, CS 1026	
Description	Tear Gas
Quantity	10
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years
Manufacturer's Description	The Triple-Chaser® CS consists of three separate canisters pressed together with separating charges between each. When deployed, the canisters separate and land approximately 20 feet apart allowing increased area coverage in a short period of time. This grenade can be hand thrown or launched from a fired delivery system. The grenade is 6.5 in. by 2.7 in. and holds an approximately 3.2 oz. of active agent payload. It has an approximate burn time of 20-30 seconds.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations

Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$32 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Ferret 12-gauge liquid barricade round, CS 3012	
Description	Tear Gas
Quantity	10
Capability	Can be launched via a 12 gauge shotgun
Life Span	5 years
Manufacturer's Description	The Ferret® 12-Gauge Liquid CS non pyrotechnical properties also eliminate the fire hazard common with other products. The Ferret round is available with either liquid or powder carriers for the agent. These munitions are a 2.5 in. 12-Gauge round deploying .025 oz. of active agent.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$10 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Ferret 12-gauge powder barricade round, CS 3092	
Description	Tear Gas

Quantity	10
Capability	Can be launched via a 12 gauge shotgun
Life Span	5 years
Manufacturer's Description	The Ferret® 12-Gauge Powder CS non pyrotechnic properties also eliminate the fire hazard common with other products. The Ferret round is available with either liquid or powder carriers for the agent. The powder carrier improves barricade penetration potential.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$10 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Ferret 12-gauge powder barricade round, OC 3090	
Description	Tear Gas
Quantity	10
Capability	Can be launched via a 12 gauge shotgun
Life Span	5 years
Manufacturer's Description	The Ferret® 12-Gauge Powder OC non pyrotechnical properties also eliminate the fire hazard common with other products. The Ferret® round is available with either liquid or powder carriers for the agent. These munitions are a 2.5 in. 12-Gauge round deploying .002 oz. of active agent.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT

Costs	\$8 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Defense Technology Ferret 12-gauge liquid barricade round, OC 3010		
Description	Tear Gas	
Quantity	10	
Capability	Can be launched via a 12 gauge shotgun	
Life Span	5 years	
Manufacturer's Description	The Ferret® 12-Gauge Liquid OC non pyrotechnical properties also eliminate the fire hazard common with other products. The Ferret round is available with either liquid or powder carriers for the agent. These munitions are a 2.5 in. 12-Gauge round deploying .015 oz. of active agent.	
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations	
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT	
Costs	\$10 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Defense Technology 12-gauge barricade projectile, CS 23		
Description	Tear Gas	
Quantity	10	
Capability	Can be launched via a 12 gauge shotgun	

Life Span	5 years
Manufacturer's Description	
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$10 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Smith & Wesson 12-gauge barricade projectile, CS	
Description	Tear Gas
Quantity	4
Capability	Can be launched via a 12 gauge shotgun
Life Span	5 years
Manufacturer's Description	Non-pyrotechnical 12-Gauge barricade round with a small CS charge.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$10 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Aerosol OC/CS grenade 1050

Description	Tear Gas
Quantity	3
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years
Manufacturer's Description	Designed for indoor use, this grenade contains no CFCs, is not a fire hazard and requires minimal decontamination by comparison to smoke, powders, or liquids. The Aerosol Grenade is most commonly used in tactical situations by Law Enforcement and Corrections and was designed with indoor operations in mind when a non fire-producing delivery system is desired. It is most effective when used in confined areas and close to the target area. Used to minimize the risks to all parties through pain compliance, temporary discomfort and/or incapacitation of potentially violent or dangerous subjects. The Aerosol Grenade is ideal for cell extractions or barricade situations where the use of pyrotechnic, powder, or liquid devices is not practical or desired. The OC and CS combination provide sufficient effects in confined areas of up to 1,500 square feet. The Aerosol Grenade is not recommended for outdoor use.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$35 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Spede-Heat continuous discharge chemical grenade, OC 1070	
Description	Tear Gas
Quantity	8
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years

Manufacturer's Description	The Spede-Heat™ OC Grenade is a high volume, continuous burn it expels its payload in approximately 20-40 seconds. The payload is discharged through four gas ports on top of the canister, three on the side and one on the bottom. This launchable grenade is 6.12 in. by 2.62 in. and holds approximately 1.09 oz. of active agent.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$ anticipated yearly maintenance cost \$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Riot control continuous discharge grenade, OC 1080	
Description	Tear Gas
Quantity	5
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years
Manufacturer's Description	The Riot Control OC Grenade is designed specifically for outdoor use in crowd control situations with a high volume continuous burn that expels its payload in approximately 20-40 seconds through four gas ports located on the top of the canister. This grenade can be used to conceal tactical movement or to route a crowd. The volume of smoke and agent is vast and obtrusive. This launchable grenade is 6.0 in. by 2.35 in. and holds approximately 0.88 oz. of active agent.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT

Costs	\$35 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Defense Technology Instantaneous blast CS grenade 1042		
Description	Tear Gas	
Quantity	4	
Capability	Can be hand thrown, launched, or placed into a munitions Pole	
Life Span	5 years	
Manufacturer's Description	The Instantaneous Blast CS Grenade is designed for indoor or outdoor use; this grenade's powder is expelled upon initiation of a small internal detonator that has sufficient force to split the canister at six machined groves on the outside surface. this device is well suited for affecting numerous subjects grouped within a contained portion of a prison yard or area, using wind to the advantage. This 6.12 in. by 2.62 in. grenade will deliver approximately 1.5 oz. of active agent.	
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations	
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT	
Costs	\$35 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Defense Technology Spede-Heat continuous discharge chemical grenade, CS 1072		
Description	Tear Gas	

Quantity	4
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years
Manufacturer's Description	The Spede-Heat™ CS Grenade is a high volume, continuous burn it expels its payload in approximately 20-40 seconds. The payload is discharged through four gas ports on top of the canister, three on the side and one on the bottom. This launchable grenade is 6.12 in. by 2.62 in. and holds approximately 2.9 oz. of active agent.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$35 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Flameless tri-chamber CS grenade 1032	
Description	Tear Gas
Quantity	2
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years

Manufacturer's Description	The design of the Tri-Chamber Flameless CS Grenade allows the contents to burn within an internal can and disperse the agent safely with reduced risk of fire. The grenade is designed primarily for indoor tactical situations to detect and/or dislodge a barricaded subject. This grenade will deliver approximately .70 oz. of agent during its 20-25 seconds burn time. The Tri-Chamber Flameless Grenade can be used in crowd control as well as tactical deployment situations by Law Enforcement and Corrections, but was designed with the barricade situation in mind. Its applications in tactical situations are primarily to detect and/or dislodge barricaded subjects. The purpose of the Tri-Chamber Flameless Grenade is to minimize the risks to all parties through pain compliance, temporary discomfort, and/or incapacitation of potentially violent or dangerous subjects. The Tri-Chamber Flameless Grenade provides the option of delivering a pyrotechnic chemical device indoors, maximizing the chemicals' effectiveness via heat and vaporization, while minimizing or negating the chance of fire to the structure.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$35 anticipated yearly maintenance cost \$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Flameless tri-chamber OC grenade 1030	
Description	Tear Gas
Quantity	3
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years

Manufacturer's Description	Designed for law enforcement and corrections, the OC Flameless Tri-Chamber Pyrotechnic Grenade combines the effectiveness of Oleoresin Capsicum (OC) as an incapacitating agent with the flexible delivery methods, range and area coverage of pyrotechnic munitions. The OC Flameless Tri-Chamber Grenade can be used in crowd control, or barricade situations, as a less lethal solution to incapacitate subjects through temporary respiratory discomfort, while reducing or negating the chance of fire to structures. The Tri-Chamber Flameless Grenade can be used in crowd control as well as tactical deployment situations by Law Enforcement and Corrections, but was designed with the barricade situation in mind. Its applications in tactical situations are primarily to detect and/or dislodge barricaded subjects. The purpose of the Tri-Chamber Flameless Grenade is to minimize the risks to all parties through pain compliance, temporary discomfort, and/or incapacitation of potentially violent or dangerous subjects. The Tri-Chamber Flameless Grenade provides the option of delivering a pyrotechnic chemical device indoors, maximizing the chemicals' effectiveness via heat and vaporization, while minimizing or negating the chance of fire to the structure.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$35 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Riot control continuous discharge grenade, CS 1082	
Description	Tear Gas
Quantity	14
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years

Manufacturer's Description	The Riot Control CS Grenade is designed specifically for outdoor use in crowd control situations with a high volume continuous burn that expels its payload in approximately 20-40 seconds through four gas ports located on the top of the canister. This grenade can be used to conceal tactical movement or to route a crowd. The volume of smoke and agent is vast and obtrusive. This launchable grenade is 6.0 in. by 2.35 in. and holds approximately 2.7 oz. of active agent.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$35 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Pocket tactical grenade, CS 1016	
Description	Tear Gas
Quantity	11
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years
Manufacturer's Description	The Pocket Tactical CS Grenade is small, and lightweight. The 0.9 oz. of active agent will burn approximately 20-40 seconds. At 4.75 in. by 1.4 inches in size, it easily fits in most tactical pouches. This is a launchable grenade; however it is normally used as a signaling or covering device. Though this device is slightly over four inches in length, it produces a smoke cloud so fast it appears to be an enveloping screen produced by a full size tactical grenade.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT

Costs	\$35 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Defense Technology Pocket tactical grenade, OC 1019		
Description	Tear Gas	
Quantity	15	
Capability	Can be hand thrown, launched, or placed into a munitions Pole	
Life Span	5 years	
Manufacturer's Description	The Pocket Tactical OC Grenade is a quick burning, reduced volume, continuous discharge grenade. Pelletized chemical agent is discharged through one (1) gas port located on the bottom of the canister. The Pocket Tactical Grenade is a small, lightweight, easily carried device that provides a medium volume of chemical agent or smoke for certain situations. It was designed with the tactical team in mind for distraction, concealment, rescue, or signaling. The pocket grenade is not specifically intended as a crowd management device; however, it can be used in chemical configurations in conjunction with larger smoke canisters to “piggy back” chemical agent into a predominately smoke environment. This device should be deployed utilizing wind advantage.	
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations	
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT	
Costs	\$35 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.	

Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.
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Defense Technology Muzzle blast 40 mm round, OC 6040	
Description	Tear Gas
Quantity	10
Capability	Can be launched via a 40mm launcher
Life Span	5 years
Manufacturer's Description	The 40 mm Muzzle Blast OC Round is widely used as a crowd management tool for the immediate and close deployment of chemical agent. It can also be employed in tactical operations such as barricaded subjects for area denial, area contamination, and a means of contaminating crawl spaces and attics. As a pain compliance round it is an excellent device for deploying chemical-laden OC powder at close ranges for indoor or outdoor operations. It has a maximum effective range of 30 feet /9.1 meters.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$35 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Muzzle blast 40 mm round, CS 6042	
Description	Tear Gas
Quantity	16
Capability	Can be launched via a 40mm launcher
Life Span	5 years

Manufacturer's Description	The 40 mm Muzzle Blast CS Round is widely used as a crowd management tool for the immediate and close deployment. It can also be employed in tactical operations such as barricaded subjects, room clearing, area denial, and for small space contamination, and a means of contaminating crawl spaces and attics. As a pain compliance round it is an excellent device for deploying chemical-laden CS powder at close ranges for indoor or outdoor operations. The cloud of agent is very effective in filling holes in dispersals lines or engaging crowds at close distances.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$35 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology Smoke Maximum HC Smoke 1083	
Description	Smoke
Quantity	10
Capability	Can be hand thrown, launched, or placed into a munitions Pole
Life Span	5 years
Manufacturer's Description	The Military-Style Maximum Smoke Grenade comes from the Defense Technology® #3 smoke grenade. It is a slow burning, high volume, continuous discharge grenade designed for outdoor use in crowd management situations. Emits grey-white smoke only for approximately 1.5 to 2 minutes.
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations
Authorized Usage	

Costs	\$38 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Defense Technology Triple-Chaser separating canister, SAF-Smoke 1027		
Description	Smoke	
Quantity	10	
Capability	Can be hand thrown, launched, or placed into a munitions Pole	
Life Span	5 years	
Manufacturer's Description	The Triple-Chaser® Saf-Smoke™ consists three separate canisters pressed together with separating charges between each. When deployed, the canisters separate and land approximately 20 feet apart allowing increased area coverage in a short period of time. This grenade can be hand thrown or launched from a fired delivery system and is an effective way to quickly deploy a wide blanket of agent. The grenade is 6.5 in. by 2.7 in. and delivers Saf-Smoke™. It has an approximate burn time of 20 seconds.	
Purpose	To safely resolve critical situations such as violent civil unrest and high-risk tactical operations	
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT	
Costs	\$38 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing chemical agents are trained by POST certified chemical agent instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize chemical agents only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Defense Technology 8933 Low Roll Distraction Device	
Description	Diversionsary Device (Flash Bang)
Quantity	35
Capability	This intermediate less lethal specialty munition allows for light sound diversion during tactical operations which allows for tactical advantage during high-risk situations.
Life Span	Reusable 25 times
Manufacturer's Description	The Non-Reloadable Distraction Device® unit incorporates an M201A1 type fuze with hex design gun steel body. This is compact version of the 8933 Low Roll® body Distraction Device is the newest version of the first reusable non-bursting canister that limits movement and rolling once deployed. The compact Distraction Device fits safely in your hand and packs all the power of the full-size Distraction Device. This is a smaller, lighter device with the same output.
Purpose	To safely resolve critical situations during high-risk tactical operations.
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$30 \$0 anticipated yearly maintenance cost
Required Training	Prior to use, officers must attend inhouse training conducted by POST certified instructors or attend POST certified training.
Authority for Authorized Use	Use is established under FPD Policy 300. It is the policy of the FPD to utilize diversion devices only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology 8908CI Command Initiated Distraction Device	
Description	Diversionsary Device (Flash Bang)
Quantity	8
Capability	This intermediate less lethal specialty munition allows for light sound diversion during tactical operations which allows for tactical advantage during high-risk situations.
Life Span	One time use

Manufacturer's Description	The Command Initiated Reload can be initiated on command from a remote point alleviating initiation delay when instantaneous results are desired. It is ideal for operations utilizing bang poles, deterring retreat and achieving space denial from predetermined areas. 12" of thermo tubing is included with the reload. Some assembly and accessories are required.	
Purpose	To safely resolve critical situations during high-risk tactical operations.	
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT	
Costs	\$35 anticipated yearly maintenance cost	\$0
Required Training	Prior to use, officers must attend inhouse training conducted by POST certified instructors or attend POST certified training.	
Authority for Authorized Use	Use is established under FPD Policy 300. It is the policy of the FPD to utilize diversion devices only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Remington 870 Pump Action Shotgun – Less Lethal	
Description	Less Lethal Shotgun
Quantity	10
Capability	Deploying 12 gauge less lethal flexible baton munitions (Super sock)
Life Span	15-20 years
Manufacturer's Description	The Remington 870 barrel has a fixed cylinder choke for optimum performance with buckshot and slugs at close range. A myriad of aftermarket Remington 870 accessories enables owners to customize the 870 Express for specific purposes. 870 Remington is a receiver milled from a solid billet of steel for maximum strength, and twin action bars
Purpose	Deploys 12 gauge less lethal flexible baton (Super sock) as impact munitions.
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT and Patrol.
Costs	\$500 \$50 anticipated yearly maintenance cost
Required Training	Sworn members utilizing less lethal shotguns are trained by POST certified instructors for 2 hours.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize impact munitions only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology 37MM Tactical Single Launcher	
Description	37 MM Projectile Launcher
Quantity	2
Capability	Deploying 40mm less lethal impact projectiles and 40mm chemical agent rounds
Life Span	25 years
Manufacturer's Description	The 37LMTS is a tactical 37mm single shot launcher. The Ambidextrous Lateral Sling Mount (LSM) and QD mounting systems allow both a single and two point sling attachment. The 37LMTS will fire standard 37/38mm Less Lethal ammunition, up to 8 inches in cartridge length. The Picatinny Rail Mounting System will accept a wide array of enhanced optics/sighting systems.

Purpose	Deploying 40mm less lethal impact projectiles and 40mm chemical agent rounds
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$300 anticipated yearly maintenance cost \$0
Required Training	Sworn members utilizing munitions launchers are trained by POST certified chemical agent instructors and POST certified less lethal instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4, and Policy 311. It is the policy of the FPD to utilize projectile launchers only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology 1440 40MM Tactical 4-Shot Launcher	
Description	40 MM Projectile Launcher
Quantity	2
Capability	Deploying 40mm less lethal impact projectiles and 40mm chemical agent rounds
Life Span	15 years
Manufacturer's Description	Designed for riot and tactical situations, the Defense Technology® 1440 40mm Tactical 4-Shot Launcher is low-profile and lightweight, providing multi-shot capability in an easy to carry launcher. It features the Rogers Super Stoc™ expandable gun stock, an adjustable Picatinny mounted front grip, and a unique direct-drive system to advance the magazine cylinder.
Purpose	Deploying 40mm less lethal impact projectiles and 40mm chemical agent rounds
Authorized Usage	De-escalation tool not likely to inflict serious injury Used by SWAT
Costs	\$300 anticipated yearly maintenance cost \$0
Required Training	Sworn members utilizing munitions launchers are trained by POST certified chemical agents instructors and POST certified less lethal instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4, and Policy 311. It is the policy of the FPD to utilize projectile launchers only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Penn Arms L637-1 37MM Projectile Launcher	
Description	37 MM Projectile Launcher
Quantity	1
Capability	Deploying 37mm less lethal impact projectiles
Life Span	25 years
Manufacturer's Description	A spring-advance magazine drum launcher with a six-shot capacity and a smooth barrel.
Purpose	Deploying 37mm less lethal impact projectiles
Authorized Usage	De-escalation tool not likely to inflict serious injury. Used by SWAT and patrol
Costs	\$300 \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing munitions launchers are trained by POST certified chemical agents instructors and POST certified less lethal instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4, and Policy 311. It is the policy of the FPD to utilize projectile launchers only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Sage KO1/S Impact Baton 37MM Projectile	
Description	37 MM Projectile
Quantity	17
Capability	37mm launcher
Life Span	25 years
Manufacturer's Description	The KO1 is a direct fire modular impact baton round that is designed to be used in situations where kinetic energy is preferred for the incapacitation of hostile and/or non-compliant individuals.
Purpose	To safely resolve critical situations such as high-risk tactical operations. These are necessary because there is no reasonable alternative that can achieve the same objective of officer and civilian safety/will safeguard the public's welfare, safety, civil rights, and civil liberties.
Authorized Usage	De-escalation tool not likely to inflict serious injury. Used by SWAT and patrol.

Costs	\$30 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing munitions launchers are trained by POST certified chemical agents instructors and POST certified less lethal instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize impact munitions only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Sage K03OC/S OC Impact Baton 37MM Projectile		
Description	37 MM Projectile	
Quantity	6	
Capability	37mm launcher	
Life Span	25 years	
Manufacturer's Description	The KO3 is a direct fire crush nose chemical impact baton round that is designed to be used in situations where a combination of kinetic energy and chemical agents is preferred for the incapacitation of hostile and/or non-compliant individuals.	
Purpose	To safely resolve critical situations such as high-risk tactical operations. These are necessary because there is no reasonable alternative that can achieve the same objective of officer and civilian safety/will safeguard the public's welfare, safety, civil rights, and civil liberties.	
Authorized Usage	De-escalation tool not likely to inflict serious injury. Used by SWAT and patrol.	
Costs	\$30 anticipated yearly maintenance cost	\$0
Required Training	Sworn members utilizing munitions launchers are trained by POST certified chemical agents instructors and POST certified less lethal instructors.	
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize impact munitions only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.	

Winchester Lake Erie Chemical Launcher Attachment	
Description	12 Gauge Projectile Launcher
Quantity	1

Capability	These launching cups attach to 12 gauge less lethal shotguns and allow us to launch canisters of chemical agents or smoke.
Life Span	25 years
Manufacturer's Description	
Purpose	These launching cups attach to 12 gauge less lethal shotguns and allow us to launch canisters of chemical agents or smoke.
Authorized Usage	De-escalation tool not likely to inflict serious injury. Used by SWAT.
Costs	\$500 anticipated yearly maintenance cost
Required Training	Sworn members utilizing munitions launchers are trained by POST certified chemical agents instructors and POST certified less lethal instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize impact munitions only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Defense Technology 1370 12-Gauge TkO/ Launching Cup	
Description	12 Gauge Projectile Launcher
Quantity	1
Capability	These launching cups attach to 12 gauge less lethal shotguns and allow us to launch canisters of chemical agents or smoke.
Life Span	25 years
Manufacturer's Description	The launching cup will project the Pocket Tactical Grenade beyond the normal throwing distance. This will extended the ability and safety of the officers involved. Removable shotgun forend to ensure proper stand-off when using breaching rounds.
Purpose	To limit the escalation of conflict where employment of lethal force is prohibited or undesirable.
Authorized Usage	De-escalation tool not likely to inflict serious injury. Used by SWAT.
Costs	\$40 anticipated yearly maintenance cost
Required Training	Sworn members utilizing munitions launchers are trained by POST certified chemical agents instructors and POST certified less lethal instructors.

Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.4. It is the policy of the FPD to utilize impact munitions only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.
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Defense Technology Drag Stabilized 12 gauge Bean Bag Round- 3027	
Description	Less lethal munitions
Quantity	120 rounds
Capability	This intermediate less lethal specialty munition allows for direct impact from a minimum range and a maximum effective range of 75ft
Life Span	Indefinite
Manufacturer's Description	The Drag Stabilized™ 12-Gauge Round is a translucent 12-Gauge shell loaded with a 40-Gram tear shaped bag made from a cotton and ballistic material blend and filled with #9 shot. This design utilizes four stabilizing tails and utilizes smokeless powder as the propellant. The 12-Gauge Drag Stabilized Round has secured its place as the Law Enforcement Communities' number one choice for specialty impact munitions. This round has a velocity of 270 fps with a maximum effective range of 75 feet.
Purpose	To safely resolve critical situations such as crowd control during riotous situations and high-risk tactical operations. These are necessary because there is no reasonable alternative that can achieve the same objective of officer and civilian safety/will safeguard the public's welfare, safety, civil rights, and civil liberties.
Authorized Usage	De-escalation tool not likely to inflict serious injury. Used by SWAT and patrol.
Costs	\$5 approx. \$0 anticipated yearly maintenance cost
Required Training	Sworn members utilizing munitions launchers are trained by POST certified less lethal instructors.
Authority for Authorized Use	Use is established under the FPD Policy 300, subsection 308.7, and Policy 311. It is the policy of the FPD to utilize less lethal munition only for official law enforcement purposes, and pursuant to State and Federal law regarding the use of force.

Long Range Acoustic Device (LRAD)	
Description	American Tech Cord 500
Quantity	1
Capability	Used to send messages over long range
Life Span	25 years
Manufacturer's Description	LRAD systems deliver live or recorded voice messages with exceptional clarity for any operational scenario. Optimized to the primary range of hearing, LRAD's Advanced Driver and Waveguide Technology ensure every broadcast is clearly heard and understood, even above crowd, engine, and background noise.
Purpose	Device used for public announcements.
Authorized Usage	Any critical situation to make public announcements: Critical incidents, civil unrest, search and rescue, public safety.
Costs	\$1,000 anticipated yearly maintenance cost \$0
Required Training	All operators receive training prior to operating the LRAD in the field.
Authority for Authorized Use	Use is established under FPD Policy 312. It is the policy of the FPD to utilize the LRAD only for official law enforcement purposes, and pursuant to State and Federal law.

Meridian Rapid Defense Group Archer 1200	
Description	Anti-vehicle Barrier
Quantity	16 barriers, 1 trailer, 2 haulers
Capability	<p>The Archer 1200 Anti-Vehicle Barrier is a portable barrier which can protect closed areas from vehicle-ramming attacks. These barriers replace wooden, and water filled barricades during special events and are easily deployed when there is an increase in the level of threat to a specific location or crowded area. They allow for pedestrians to move between them but can stop vehicles from entering closed areas. These barriers will be used during special events and critical incidents where pedestrian safety is a concern.</p> <p>The Folsom Police Department has applied for a grant to obtain 16 barriers but have not taken possession yet.</p>
Life Span	10 years
Manufacturer's Description	An unanchored, "drop-and-stop" barrier for a VSM (Vehicle Safety Mitigation) solution deployment on any surface. Archer 1200 barriers has the shortest stopping distance in its class, modular design, no heavy equipment required for deployment
Purpose	To be used based on the specific circumstances of a given critical incident, large event, natural disaster or community event that is taking place.
Authorized Usage	Authorized for use would include but not be limited to critical incidents, emergencies, and community events.
Costs	<p>\$150,000 approx.</p> <p>\$0 anticipated yearly maintenance cost</p>
Required Training	All officers deploying the vehicle barriers will receive training on how to properly load, unload, move, and deploy the barriers in the field.
Authority for Authorized Use	It will be the policy of the Department to use the vehicle barriers for official law enforcement purposes including road closures, and special events



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

Folsom City Council Staff Report

MEETING DATE:	5/10/2022
AGENDA SECTION:	New Business
SUBJECT:	Consideration of Letter in Response to Demand Letter Received from Scott Rafferty Regarding Alleged Non-Compliance with the Brown Act
FROM:	City Attorney's Office

RECOMMENDATION / CITY COUNCIL ACTION

Staff recommends that the City Council consider and approve the attached reply to Scott Rafferty (Attachment 1) providing an unconditional commitment to continue to comply with the Brown Act.

BACKGROUND / ISSUE

On April 20, 2022, the City received a cease and desist letter from Scott Rafferty (Attachment 2) alleging the following violations of the Brown Act on March 22, 2022:

1. The failure to permit public comment before the adoption of the consent agenda, which included the enactment of Ordinance No. 1324, as required by 54954.2(a) and 54953(a)(3);
2. The failure to recognize members of the public who raised their hands in accordance with previous instructions on how to comment prior to council action, as required by the same section; and
3. Multiple failures to announce in advance the “give notice of the means by which members of the public may access the meeting and offer public comment,” i.e., that there would be no ability to offer public comment, in violation of 54953(e)(2)(B).

While staff disagrees with the alleged non-compliance, the Brown Act provides a process for issues such as these to be resolved without further legal action. To that end, the Brown Act provides a prescribed form letter that the City Council may consider approving and sending in response to Mr. Rafferty's correspondence.

POLICY / RULE

The Brown Act provides that a response to the cease and desist letter shall be in substantially the form provided in Government Code section 54960.2(c)(1). The fact that the City Council provides an unconditional commitment shall not be construed or admissible as evidence of violation of the Brown Act. Government Code section 54960.2(c)(4).

ANALYSIS

Government Code section 54960.2 allows any interested person to submit a "cease and desist" letter to the City as a prerequisite to filing a lawsuit over alleged past non-compliance with the Brown Act. Pursuant to Section 54960.2(b), the City Council may respond to the "cease and desist" letter within thirty (30) days by providing an "unconditional commitment" not to repeat any or all of the actions challenged. By law, an "unconditional commitment" **does not** constitute admission of a violation, but it does bar a potential plaintiff from pursuing litigation and collecting attorneys' fees with respect to past non-compliance related to the specific action the City has "unconditionally committed" not to repeat.

The City Council's reply must be approved in open session as a separate item of business, not under the "Consent" portion of the agenda, and in substantially the form as prescribed by the Brown Act. Once approved, the Brown Act prohibits legal action by the potential plaintiff; however, if such an action is nonetheless filed, the court is required to dismiss the lawsuit with prejudice if it finds that the City Council has provided an unconditional commitment pursuant to the Brown Act.

FINANCIAL IMPACT

There is no legal expense associated with this item as the City Council has always complied with the Brown Act. In addition, providing the attached reply may reduce the chance of litigation and any associated legal costs.

ENVIRONMENTAL REVIEW

The California Environmental Quality Act (CEQA) does not apply to activities that will not result in a direct or reasonably foreseeable indirect physical change in the environment (CEQA Guidelines §15061(c)(3)), or is otherwise not considered a project as defined by Public Resources Code §21065 and CEQA Guidelines §15060(c)(3) and §15378. The City Council's consideration of a reply to the Brown Act cease and desist letter meets the above criteria and is not subject to CEQA. No environmental review is required.

ATTACHMENTS

1. Proposed reply from the City Council
2. Cease and desist letter dated April 20, 2022

Respectfully submitted,

Steven Wang, City Attorney

ATTACHMENT 1

May 11, 2022

Scott J. Rafferty
1913 Whitecliff Court
Walnut Creek, CA 94596

Re: Brown Act Cease and Desist Letter

To Mr. Rafferty:

The Folsom City Council has received your cease-and-desist letter dated April 20, 2022, alleging that the following described past action of the legislative body violates the Ralph M. Brown Act:

1. The failure to permit public comment before the adoption of the consent agenda, which included the enactment of Ordinance No. 1324, as required by 54954.2(a) and 54953(a)(3);
2. The failure to recognize members of the public who raised their hands in accordance with previous instructions on how to comment prior to council action, as required by the same section; and
3. Multiple failures to announce in advance the “give notice of the means by which members of the public may access the meeting and offer public comment,” i.e., that there would be no ability to offer public comment, in violation of 54953(e)(2)(B).

While the Folsom City Council strongly disputes and denies those allegations because the City has at all times complied with the Brown Act and hence there is no violation to cure or correct with respect to your demand, in order to avoid unnecessary litigation and without admitting any violation of the Ralph M. Brown Act, the Folsom City Council hereby unconditionally commits that it will cease, desist from, and not repeat the challenged past action as described above.

The Folsom City Council may rescind this commitment only by a majority vote of its membership taken in open session at a regular meeting and noticed on its posted agenda as "Rescission of Brown Act Commitment." You will be provided with written notice, sent by any means or media you provide in response to this message, to whatever address or addresses you specify, of any intention to consider rescinding this commitment at least 30 days before any such regular meeting. In the event that this commitment is rescinded, you will have the right to commence legal action pursuant to subdivision (a) of Section 54960 of the Government Code. That notice will be delivered to you by the same means as this commitment or may be mailed to an address that you have designated in writing.

Very truly yours,

Kerri Howell, Mayor

ATTACHMENT 2

SCOTT J. RAFFERTY

ATTORNEY AT LAW

1913 WHITECLIFF COURT
WALNUT CREEK CA 94596

(202)-380-5525
RAFFERTY@GMAIL.COM

April 20, 2022

Ms. Christa Freemantle
Clerk, City of Folsom
50 E. Natoma Street
Folsom CA 95630

by electronic and postal mail
cc: Mayor Kerri Howell, members of
the City Council, City Attorney

Dear Ms. Freemantle:

This letter constitutes a demand specified by Section¹ 54960.2(a)(1) that the City of Folsom cease and desist from violations of the Brown Act committed in connection with the public hearing the Council conducted on March 22, 2022. This letter also satisfies the requirement of Section 54960.2 and enables my clients to file an additional action to determine that the actions specified herein were taken in violation of the Brown Act. To the extent set forth herein, the City of Folsom may respond to the cease and desist demand by making an unconditional commitment to cease and desist from the challenged practices. This letter further constitutes a demand, pursuant to 54960.1(b) that the Council cure and correct the violation.

The violations are (1) the failure to permit public comment before the adoption of the consent agenda, which included the enactment of Ordinance No. 1324, as required by 54954.2(a) and 54953(a)(3); (2) the failure to recognize members of the public who raised their hands in accordance with previous instructions on how to comment prior to council action, as required by the same sections and (3) multiple failures to announce in advance the "give notice of the means by which members of the public may access the meeting and offer public comment," i.e., that there would be no ability to offer public comment, in violation of 54953(e)(2)(B)

On April 13, 2022, Mayor Kerri Howell signed an unconditional commitment to cease and desist from violations of 54953(e)(2)(B). My clients intend to enforce the violation of that commitment.

This letter demands that you, as City Clerk, announce that Ordinance No. 1324 is null and void.

¹ "Section" refers to the Government Code, except as noted.

Rafferty to Freemantle, Brown Act Demand Letter, April 20, 2022, page 2

Thank you from your prompt action to recognize the invalidity of this action and to remove any reference or effect of the purported statute from the municipal code or other public records of the city.

Sincerely,

A handwritten signature in black ink that reads "Scott Rafferty". The signature is written in a cursive, slightly slanted style.

Scott J. Rafferty

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